

Traffix Group

Traffic Engineering Assessment

Proposed Retirement Village Development
2-10 Clarke Street, Abbotsford

Prepared for
Mercy Health

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1. Introduction

Traffic Group has been engaged by Mercy Health to undertake a Traffic Engineering Assessment for the Proposed Retirement Village Development at 2-10 Clarke Street, Abbotsford.

This report provides a detailed traffic engineering assessment of the parking and traffic issues associated with the proposed development.

In the course of preparing this assessment, we inspected the subject site, reviewed development plans and background material, collected traffic and parking data, and assessed the car parking and traffic impacts of the proposal.

This report also includes responses and updates to address traffic engineering items raised by Council in their Request for Further Information (RFI) with the reference number PLN24/0680.

Our assessment is as follows.

2. Existing Conditions

2.1. Subject Site

The subject land, addressed as 2-10 Clarke Street, Abbotsford, is located in the southeastern corner of Clarke Street and St Heliers Street intersection, adjacent to the Abbotsford Convent Arts Precinct.

The site has a western abuttal of approximately 100 metres to Clarke Street and a northern abuttal to St Heliers Street of approximately 75 metres.

The site is currently occupied by an existing aged care facility, operated by Mercy Health, which includes an existing double storey building fronting Clarke Street as well as a heritage Church/Chapel fronting St Heliers Street.

The development area is the western portion of the subject site, which is the existing aged care building, with the chapel in the eastern portion to be retained.

A locality map of the site is provided at Figure 1.



Figure 1: Locality Map

2.2. Subject Site and Use

We are advised that the existing aged care facility within the development site has not been operational since April 2023. The site historically operated with 71 aged care rooms and on-site car parking for 6 cars along the site's frontage to Clarke Street.

The subject site also provides service vehicle access to the rear of the site off Clarke Street.

The subject site has 3 crossovers to Clarke Street including:

- A single width crossover of approximately 4.1 metres at the site’s southern boundary to Clarke Street providing rear and side site access for service vehicles.
- Two double width crossovers of approximately 6.6 metres and 7.5 metres providing access to the on-site parking along the Clarke Street frontage. Apron parking is provided for three spaces via each of the crossovers.

An aerial photo of the site, and nomination of the existing access/crossover locations is provided in Figure 2.

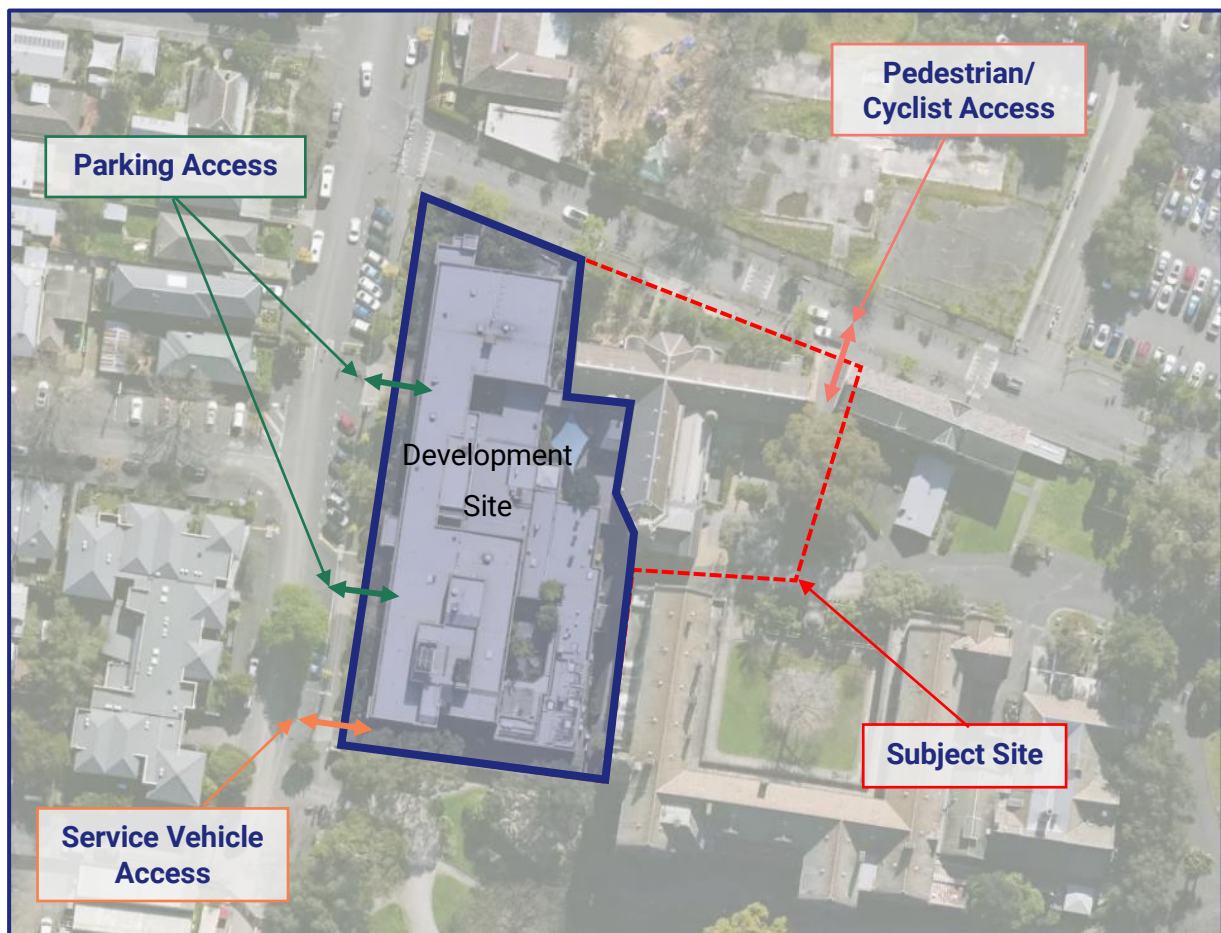


Figure 2: Aerial Photo of Site Access Points

2.3. Planning Scheme Zones & Surrounding Uses

The subject site is zoned as Special Use Zone – Schedule 2 (SUZ2) under the Yarra Planning Scheme. A planning zone map is provided at Figure 3.

Land uses in the immediate vicinity of the subject is generally residential in nature to the west, with public and recreational uses further east.

Notable nearby uses include:

- Abbotsford Convent, located to the immediate east of the site, which provides for a mixture of uses and activities, including arts, functions, food and drink and market stalls on weekends.
- IGA Supermarket – Abbotsford, located approximately 750 metres walking distance to the west of the site,
- Victoria Park Oval, located approximately 700 metres north-west,
- Victoria Park Station, located approximately 800 metres north-west,

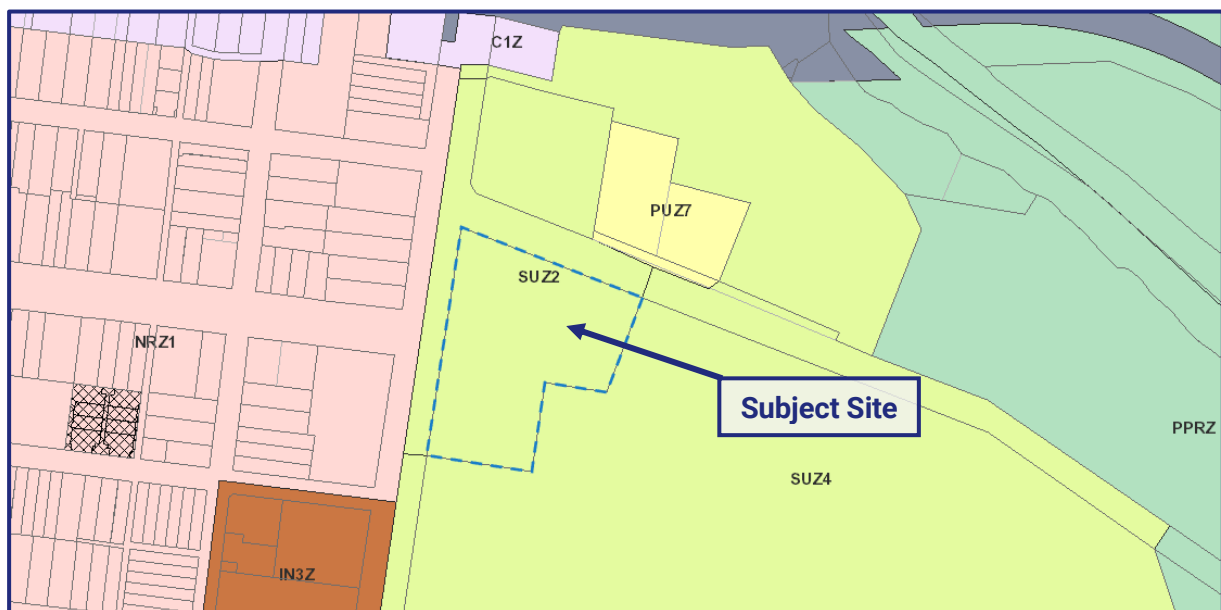


Figure 3: Planning Zone Map - Abbotsford

2.4. Road Network

Clarke Street is a local road under the control of Council generally aligned in a north-south direction between Johnston St in the north and Yarra St in the south.

In the vicinity of the site, Clarke Street has a carriageway width of approximately 14.3 metres which provides for one lane of traffic in each direction and parallel parking along the western kerb and angled parking on the eastern kerb.

It is noted that approximately 40 metres south of the intersection of Clarke Street and St Heliers Street (immediately north of the site's existing northern crossover) a kerb outstand and partial road closure restricts movements on Clarke Street. The local area traffic management restricts movements to northbound only at this point, with southbound vehicle movements not permitted (pedestrians and cyclists are permitted).

Kerbside parking on the western side of Clarke Street within the vicinity of the site is generally subject to '2P' restrictions between 7am-7pm Monday to Friday, and acts as a permit zone on public holidays and all other times. Angled parking on the eastern side of Clarke Street is generally subject to '2P' parking restrictions between 7am-7pm Monday to Friday, and unrestricted any other time.

A 40km/hr speed limit is imposed on Clarke Street.

St Heliers Street is a dead-end local road under the control of Council, running in an east-west extending approximately 225 metres east from Clarke St.

St Heliers Street operates as a shared zone.

St Heliers Street has a carriageway width of approximately 9.5 metres which provides for a usable carriageway of approximately 6.6 metres and provide for approximately 1.5 metres of landscaping and bicycle parking along both kerbs.

There is no on-street car parking along St Heliers Street within the vicinity of the site.

A 20km/hr speed limit is imposed on the shared zone along St Heliers Street.

Figure 4 to Figure 7 provide views of the road network abutting the subject site.



Figure 4: Clarke Street - View North



Figure 5: Clarke Street - View South



Figure 6: St Heliers Street - View West



Figure 7: St Heliers Street - View East

2.5. Sustainable Modes of Transport

2.5.1. Pedestrian Accessibility

Pedestrian access to surrounding areas from the subject site is excellent, with footpaths provided in both directions along Clarke Street, St Heliers Street, Johnstone Street, and all surrounding road networks in the vicinity of the site.

The pedestrian footpath network provides access to significant land uses in nearby areas including:

- Abbotsford Convent located immediately east of the site providing food and drink, restaurants, retail /shop, art galleries and health and wellbeing uses.
- Everyday shopping facilities including supermarkets, restaurants, and specialist stores, located within approximately 800 metres walk west along Johnston Street,
- Victoria Park Oval, located approximately 700 metres north-west,
- Victoria Park Station, located approximately 800 metres north-west,
- Clarke St/Johnstone St bus stop, located approximately 100 metres to the north, and,
- Yarra Bend Park, located approximately 650 metres south.

2.5.2. Bicycle Accessibility

The municipality of Yarra is very well serviced by the Principal Bicycle Network (PBN), with Johnston Street nominated as a key route which provides sections of designated bicycle lanes in each direction, with the rest nominated as a shared bike lane with other road users.

The shared zone along St Heliers Street as well as shared bicycle lanes on Clarke Street provides safe bicycle access to the site. Bicycle parking is provided along St Heliers Street.

A connection to the Main Yarra Trail is provided immediately south of the subject site on Clarke Street.

An excerpt of the City of Yarra TravelSmart Map is provided in Figure 8 illustrating the surrounding bicycle network.

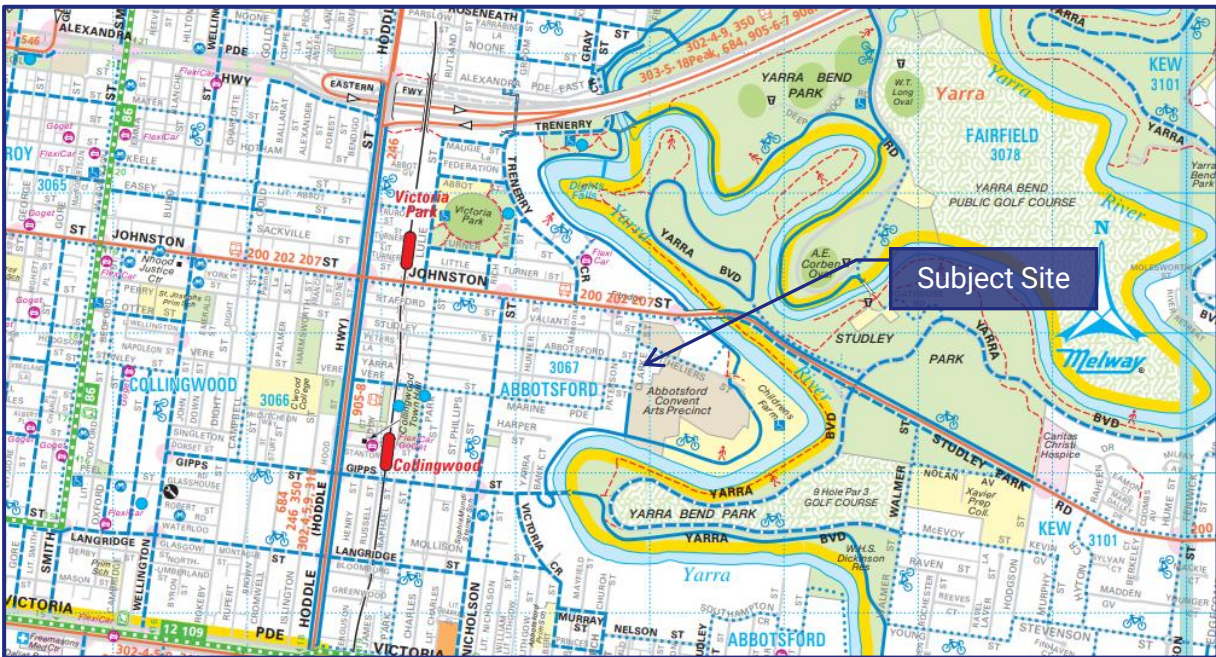


Figure 8: City of Yarra – TravelSmart Map

2.5.3. Public Transport

The site has excellent access to public transport services with two bus routes operating along Johnston Street and accessible from the Clarke St/Johnston St bus stop, located approximately 150 metres north.

Located approximately 850 metres east, a further 10 bus routes operate along Hoddle Street, accessible from Johnston St/Hoddle St bus stop.

Collingwood and Victoria Park station are within proximity of the site, locate approximately 950 metres south-west and 900 metres north-west, respectively.

Three tram services also operate along Victoria Street, accessible from the Church St/Victoria St stop, located approximately 1.2 kilometres south of the site.

Table 1 summarises the available services, whilst Figure 9 illustrates the nearby routes.

3. Proposal

3.1. The Development

The application proposes to develop the site for the purposes of a retirement village providing a total of 60 residential dwellings.

The proposed development schedule is provided in Table 2.

Table 2: Proposed Development Schedule

Use		Current Scheme
Retirement Village	1 bed dwelling	9 no.
	2 bed dwelling	46 no.
	3+ bed dwelling	5 no.
	Total	60 no.

3.2. Access

3.2.1. Pedestrian & Bicycle Access

Pedestrian and bicycle access to the site will be provided from the sites Clarke Street Frontage and a secondary access to St Heliers Street.

Bicycle parking for the development is located within the basement car park with access provided via the lift at ground floor.

3.2.2. Vehicle Access

Vehicle access will be provided to Clarke Street via modification to the existing access location. The access will provide access to a loading drop-off area at the south of the site as well as the basement car park which includes car parking for residents and staff of the development.

The existing vehicle access points providing access to the on-site car parking along Clarke Street will be removed and reinstated as part of the application. Additional on-street parking is expected with the closure of the existing crossovers.

The key vehicular, bicycle and pedestrian access points to the site are shown in Figure 10.

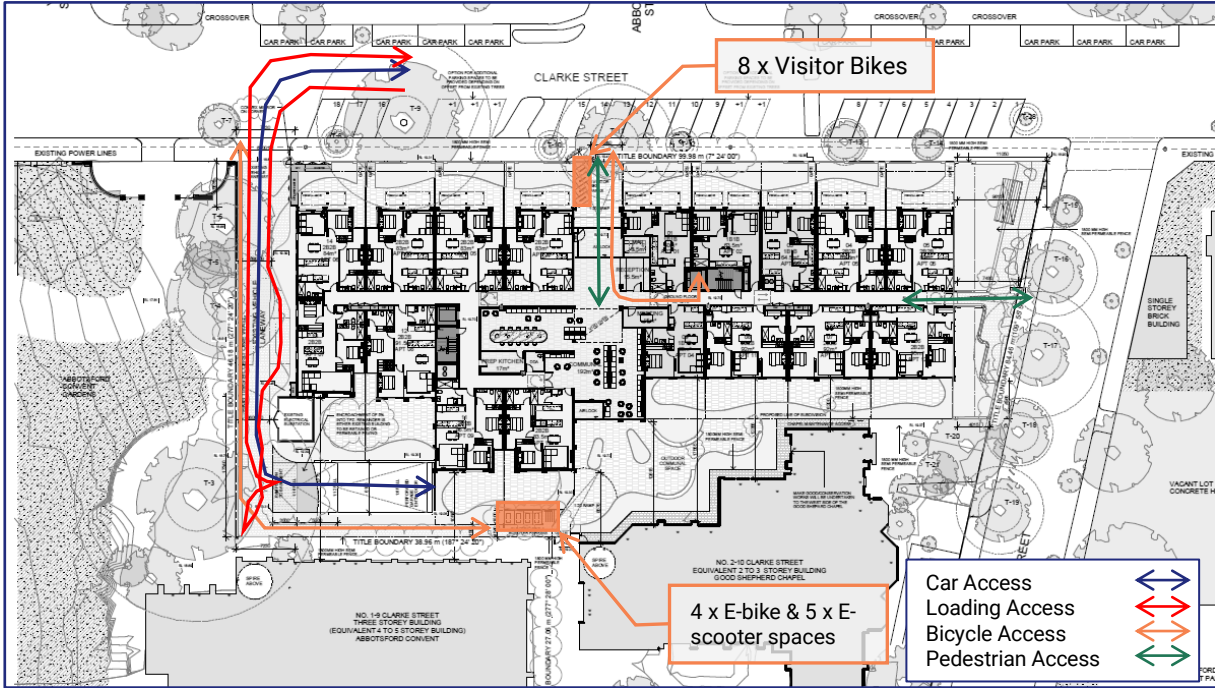


Figure 10: Proposed Site Access Arrangements

3.3. Parking Provisions and Allocations

3.3.1. Car Parking

The proposal intends to provide a total of 69 car parking spaces on-site to be allocated to be allocated to residents and staff of the development. The on-site car parking area includes the provision of 1 x DDA accessible bays and 4 x EV charging bays.

Seven pairs of tandem parking spaces are provided within the basement car park.

3.3.2. Bicycle Parking

The application proposes the provision of 28 bicycle spaces, inclusive of 20 spaces for residents within the basement car park and additional 8 spaces for visitors at the ground floor.

In addition 4 x E-bike spaces and 5 x E-scooter spaces are provided at ground floor.

3.3.3. On-Street Parking Changes

With the widening of the southern crossover, there will be a loss of one on-street parking space, however with the closure of the central and northern crossovers, there will be the opportunity to reinstate in the order of six additional on-street parking spaces for the surrounding precinct and the public.

That is, there will be a net increase of at least five spaces on-street in the area.

It is expected that these spaces would be subject to an extension of the existing on-street parking restrictions, however there is also opportunity for some spaces to be short term (pick-up/drop-off) or loading zone restricted. This will ultimately be to the satisfaction of Council.

3.4. Loading & Waste Collection

A loading area will be provided at ground floor servicing vehicles up to a 6.4 metre Small Rigid Vehicle (SRV).

Waste collection is proposed within basement via a private waste collection vehicle nominally 6.4 metre long rear loaded mini waste truck.

Access to the loading area will be provided via the existing crossover to Clarke Street in the south-west corner of the site.

4. Car Parking Considerations

4.1. Statutory Requirements – Clause 52.06

The car parking requirements for the proposed development are outlined under Clause 52.06 of the Yarra Planning Scheme. The purpose of Clause 52.06 is:

- To ensure that car parking is provided in accordance with the Municipal Planning Strategy and the Planning Policy Framework.
- To ensure the provision of an appropriate number of car parking spaces having regard to the demand likely to be generated, the activities on the land and the nature of the locality.
- To support sustainable transport alternatives to the motor car.
- To promote the efficient use of car parking spaces through the consolidation of car parking facilities.
- To ensure that car parking does not adversely affect the amenity of the locality.
- To ensure that the design and location of car parking is of a high standard, creates a safe environment for users and enables easy and efficient use.

Clause 52.06-5 states that:

“Column B rates apply to a site if any part of the land is identified as being within the Principal Public Transport Network Area as shown on the Principal Public Transport Network Area Maps”

An excerpt of the Principal Public Transport Network (PPTN) Area Map is provided at Figure 11.

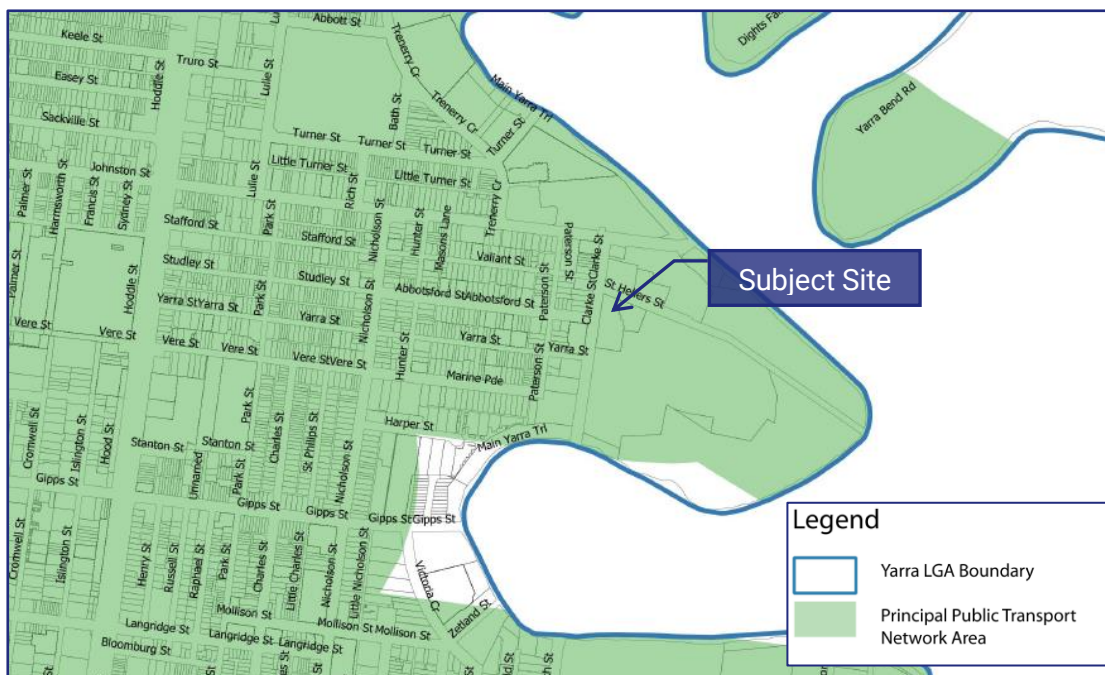


Figure 11: Yarra PPTN Area Map

The subject site falls within the PPTN area map and therefore Column B rates apply to the proposal.

A statutory assessment of the proposal under Clause 52.06 is provided at Table 3.

Table 3: Statutory Car Parking Requirements (Clause 52.06)

Use	No / Size	Statutory Requirement	No of Spaces Required
Retirement Village	55	1 car space to each 2-bed dwelling for tenants	55 spaces
	5	2 car spaces to each 3-bed dwelling for tenants	10 spaces
Total			65 spaces

Based on the table above, the development is statutorily required to provide 65 car spaces for residents.

The provision of 69 spaces onsite for residents exceeds the minimum requirements of Clause 52.06 and therefore no permit is sought under Clause 52.06.

There is no requirement to provide visitor parking and a permit is not required to provide parking above the statutory rates for residents.

4.2. Additional Parking Considerations

It is noted that in our experience, parking for these types of developments remains in common ownership by the Aged Care operator, and is leased to the individual tenants based on demands.

As new residents move in, they may have a demand for parking at rates similar to the statutory rates, however as resident’s stays extend, their need for a car often diminishes as they no longer require a car, or no longer wish to drive.

Ultimately parking is to be managed by the operator of the site, and this may include some level of parking for staff (particularly those who might work overnight), and also for resident visitors.

It should also be noted that the historical use of the site was for higher care aged care, with limited on-site parking. Staff and visitors of the historical use would have been reliant on on-street parking.

The proposed development offers an opportunity for these demands to be accommodated on-site, and therefore is likely to result in a lesser impact to on-street parking availability than the historical use.

The proposed access changes will also allow for additional on-street parking to be provided in the precinct.

Having regard to all of these considerations, the proposal is expected to result in a net positive impact to parking in the precinct.

4.3. Disabled Car Parking Requirements

The requirement for aged care facility disabled car parking spaces is set out under building class 3 of the Building Code of Australia (BCA).

The BCA sets out a rate of 1 space per 100 car parking spaces or part thereof for aged care which generates a requirement for one space. The proposal includes one disabled spaces in the basement, therefore meeting the BCA requirement.

4.4. Car Parking Layout & Access Arrangements

The car park layout and access arrangements have been developed with design advice provided to the project architect (CHT Architects) and is considered to principally meet the relevant requirements of the Yarra Planning Scheme and where applicable, the Australian Standard for Off-Street Parking (AS2890.1:2004).

A review of the car park layout reveals:

General Car Parking Layout

- Car spaces are provided with minimum dimensions of 2.6 metres width and 4.9 metres length, accessible from aisles of at least 6.4 metre wide, as per the requirements of Clause 52.06-9 (Design Standard 2).
- Car spaces adjacent to walls and structures have been widened to provide for appropriate door opening.
- A minimum headroom clearance of 2.2 metres is provided throughout the car park meeting the requirements of AS2890.1-2004.
- Columns adjacent to car spaces are generally located within 0.25-1.25 metres from the aisle end of car spaces in accordance with the car parking envelope of Clause 52.06-9 (Design Standard 2).
- Accessible parking bays are provided in accordance with the requirements of AS2890.6:2009. Dedicated bays and shared areas are dimensioned at a minimum width of 2.4 metres, minimum length of 5.4 metres and provided with a minimum headroom clearance of 2.5 metres.

Vehicle Access and Ramp

- The proposed ramp into basement is provided with a minimum width of 6.1 metres between walls (including 300mm kerb on both sides) in accordance with the requirements of Clause 52.06-9 (Design Standard 1) and AS2890.1-2004.
- Whilst not required, a 6.1 metre x 7.0 metre passing area is provided at the site's boundary to Clarke Street, narrowing to a minimum width of 3.8 metres exceeding the minimum requirements of AS2890.1-2004 for a single width ramp.
- Vehicles can enter and exit the site in a forward direction in accordance with Clause 52.06-9 (Design Standard 1).
- Entry and exit movements for the access ramp via Clarke Street have been checked for the 99th percentile design vehicle and found to be satisfactory. Swept path diagrams are attached at Appendix A.

- Access to and from each of the critical car spaces has been checked for the 85th percentile design vehicle and found to be satisfactory.
- Grades no steeper than 1 in 10 for the first 5 metres are provided from the property boundary for all three accesses, satisfying the requirements of the Planning Scheme (Design Standard 3).
- Internal ramps are designed with a maximum grade of 1 in 5 with minimum transitions of 1 in 8 for 2 metres exceeding the requirements Clause 52.06.
- Due to the location of the Clarke Street access on the southern boundary a sight triangle has not been provided on the exit side. To ensure the provision of sufficient sight lines to pedestrians a convex mirror is to be provided at the site's boundary.
- The proposal removes two vehicle crossovers to Clarke Street improving the pedestrian connection through the area and to the site from both Clarke Street and St Heliers Street.

In this regard, the above access arrangements, grades, transitions and clearances have been assessed and, in our view, meet the intent of the relevant standards.

Based on the foregoing, the car park layout and access is considered satisfactory.

5. Traffic Considerations

5.1. Consideration of Access Location

The proposal seeks to utilise the southern access point for basement and loading access.

This location has been chosen for a number of reasons. From a traffic and parking perspective, it has a number of benefits, including:

- It offers a very efficient location for the layout of basement parking due to the shape of the site;
- It allows for basement parking and access to be located in a more 'back of house' location, meaning that the presentation of the building to the northern end of the site (ie near St Heliers Street) is of 'active' building frontage;
- The northern part of the site is constrained in relation to a potential ramp location due to the historical chapel; and
- It is located further away from the northern part of the site where there is understood to be higher pedestrian activity associated with the Abbotsford Convent.

Whilst it is acknowledged that the location of the access requires drivers to utilise Nicholson Street and Abbotsford Street, on balance, and when considering the above items, the location of the access is considered acceptable.

A discussion of the likely traffic generation and impact is further discussed below.

5.2. Traffic Generation

When considering the traffic generation of the proposal a retirement village does not typically generate peak traffic during the road network peak hours, as residents do not need to go to work or undertake the typical commuter peak hour trips.

To assess the likely traffic generation of the low care facility, reference is made to the NSW RMS (formerly RTA) Guide to Traffic Generating Developments 2002 and the more recent "Updated traffic surveys" from 2013 which provides guidance to expected traffic generation rates for 'housing for aged and disabled persons'.

The RMS Guide sets out the following relevant traffic generation rates for 'housing for aged and disabled persons':

- Daily vehicle trips = 1 – 2 per dwelling
- Weekday peak hour vehicle trips = 0.1 – 0.2 per dwelling

Application of the upper rate to the 60 dwellings proposed as part of the development results in an expected traffic generation of up to 120 movements per day inclusive of up to 12 movements during the peak hour to Clarke Street.

Given the type of development, AM and PM peak traffic splits are likely to be relatively evenly split in the peak hours. This accounts for the different types of trips undertaken by residents, likely staff arrivals and departures, and also potential visitor trips.

5.3. Traffic Impact

Based on the preceding, we expect that the proposal could generate some 12 movements during the network peak hours and 120 vehicles per day, two-way.

This level of traffic generation is very low in traffic engineering terms of a peak hour, equivalent to an average of not more than one additional vehicle movement being generated every 5 minutes during the peak periods.

It is noted that due to the local area traffic management located to the north of the proposed crossover vehicles travelling to the site will most likely use Nicholson Street and Abbotsford Street.

This level of increased traffic to Abbotsford Street and Nicholson Street will be split between arrivals and departures and will not have a discernible impact on the operation of the surrounding network.

From a daily volume perspective, the proposal is likely to generate in the order of 60-80 vehicles per day to/from Nicholson Street and Abbotsford, and not more than 60 vehicles per day to the northern section of Clarke Street and out to Johnston Street.

This quantum of traffic will not significantly impact on the capacity or safety of these roads, and they will continue to remain within their daily target volumes.

We note that when considering the historical use on the site, the proposed development is likely to generate a comparable amount of traffic.

6. Bicycle Considerations

There are no requirements for bicycle parking under Clause 52.34 for proposed retirement village. However, the proposed development provides 28 bicycle spaces, inclusive of 20 spaces to be used by residents at the basement level carpark and additional 8 spaces for visitors at the ground level.

The proposal also includes the provision of 4 x E-bike spaces and 5 x E-scooter spaces at ground floor.

Bicycle parking has been provided in accordance with AS2890.3-2015 as horizontal rails with dimensions of 1.8 metre length and spaced at 1.0 metre centres, accessible from a 1.5 metre aisle.

The proposed bicycle parking facilities are therefore considered appropriate.

7. Loading Considerations

Clause 65.01 of the Planning Scheme states that the responsible authority must consider a number of matters as appropriate including:

- *The adequacy of loading and unloading facilities and any associated amenity, traffic flow and road safety impacts.*

The proposal includes an on-site loading area at ground floor accessed via the Clarke Street access at the south-western corner of the site. The loading area has been designed to accommodate vehicles up to a 6.4 metre Small Rigid Vehicle.

The loading areas within the basement car park will be accessed via Point Nepean Road and will be utilised for waste collection only. Waste collection will be undertaken by a 6.4 metre Waste-Wise Hino truck.

Vehicle accessibility has been demonstrated via swept paths attached at Appendix A.

Accordingly, we are satisfied that appropriate loading and waste provisions can be accommodated in accordance with the objectives of the Planning Scheme.

We note that loading demands associated with the proposed redevelopment would be comparable with those of the historical aged care use on the site. That is, there is not expected to be any material increase in loading activity within the precinct as a result of the future use.

Furthermore, the proposed plans will allow for loading to take place on-site, whereby it historically took place from the Clarke Street frontage, either on-site, or within the apron parking areas.

8. Council Request for Further Information

In correspondence dated 30th October 2024, Council’s Traffic Engineering Department issued a Request for Further Information (RFI) based on the original application.

Council’s Strategic Transport Team issues a response with recommendations dated 9th December 2024, in relation to bicycle parking.

The relevant commentary received from Council and our response is provided at Table 4.

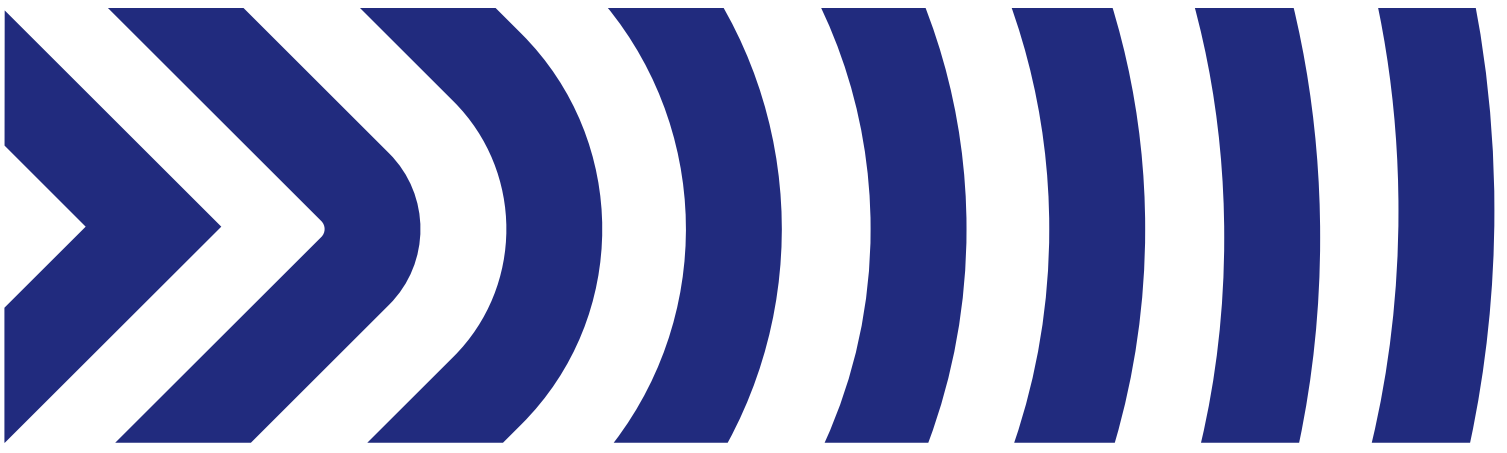
Table 4: Council Request for Further Information and Response

Council Item	Traffic Response
<i>Provide a convex mirror at the development entrance to assist exiting motorists in viewing the Clarke Street footpath.</i>	Plans have been updated to include a convex mirror on the northern side of the crossover to provide improved sight lines to the south as a result of the bluestone wall provided along the southern boundary of the site.
<i>The shared area of the accessible parking space is to be hatched line marked in accordance with the Australian/New Zealand Standard AS/NZS 2890.6:2009.</i>	Linemarking on the plans has been updated to reflect the requirements of AS2890.6-2022. An additional note has been included to ensure linemarking is delivered in accordance with AS2890.6-2022.
<i>Dimension the minimum headroom clearance of the ramp such that it be shown perpendicular to the surface of the ramp.</i>	Plans have been updated to show the dimensions perpendicular to the surface ramp. The dimension shows a minimum headroom clearance in excess of 2.7 metres, exceeding the requirements of Clause 52.06.
<i>Prepare a cross-sectional drawing for the proposed vehicle crossing to ensure that a B99 design vehicle can enter and exit the property via Clarke Street without scraping or bottoming out. For guidance in preparing the cross-sectional drawing, please refer to the information appended to this memo. A ground clearance check for each vehicle crossing using the B99 design vehicle is to be provided.</i>	A cross-sectional drawing of the proposed vehicle crossing has been prepared and provide in drawing TP3-03 dated 13/01/2024. A ground clearance check has been undertaken with a B99 design vehicle which demonstrates that no scraping is expected to occur at the proposed vehicle crossing. A copy of the ground clearance assessment is provided at Appendix B. The proposed site access is therefore acceptable.
Strategic Transport Comments	Traffic Response
<i>Identify dimensions for visitor parking on the plans.</i>	Plans have been updated to include dimensions for the visitor bicycle parking spaces at the ground floor.
<i>Identify the locking mechanism to be installed at the bicycle storage area.</i>	Plans have been updated to illustrate a lockable sliding door to the basement staff bicycle parking area.

9. Conclusions

Having undertaken a detailed traffic engineering assessment of the proposed retirement village development at 2-10 Clarke Street, Abbotsford, we are of the opinion that:

- a. the proposed development has a statutory car parking requirement of 65 car spaces under Clause 52.06-5 of the Planning Scheme and the provision of 69 car spaces exceeds this requirement.
- b. The proposal will result in a net increase of on-street parking and improvements to pedestrian amenity along the site frontage.
- c. the proposed parking layout and access arrangements accord with the requirements of the Planning Scheme, AS2890.1:2004 (where relevant) and current practice,
- d. the level of traffic generated as a result of this proposal is low and will not have a detrimental impact on the surrounding road network,
- e. There is no requirement for bicycle parking, however the proposal includes the provision of 28 spaces, exceeding the requirements set out at Clause 52.34 of the Planning Scheme,
- f. the on-site loading area has been designed to meet the objectives of Clause 65.01 of the Planning Scheme, and
- g. there are no traffic engineering reasons why a planning permit for the proposed retirement village development at 2-10 Clarke Street, Abbotsford, should be refused, subject to appropriate conditions.

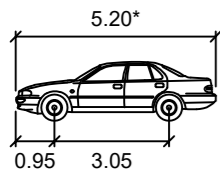


Appendix A

Swept Paths

VEHICLE PROFILE

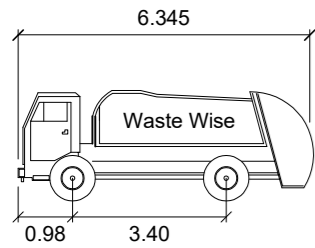
VEHICLE USED IN SIMULATION
(VEHICLE SPEED - 5KM/H)



99th percentile
(AS/NZS 2890.1:2004)

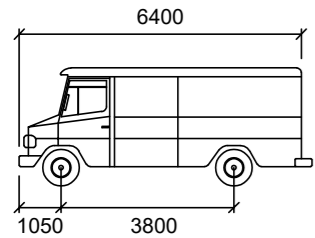
Width : 1.94
Track : 1.84
Kerb to Kerb Radius : 6.3m

* actual template based on 'relevant longitudinal dimensions that affect swept path' as set out in Section B2.1 of AS/NZS 2890.1:2004



Waste Wise Mini (Hino 300)

Width : 1.7m
Front Track : 1.4m
Rear Track : 1.44m
Kerb to Kerb Radius : 6.2m

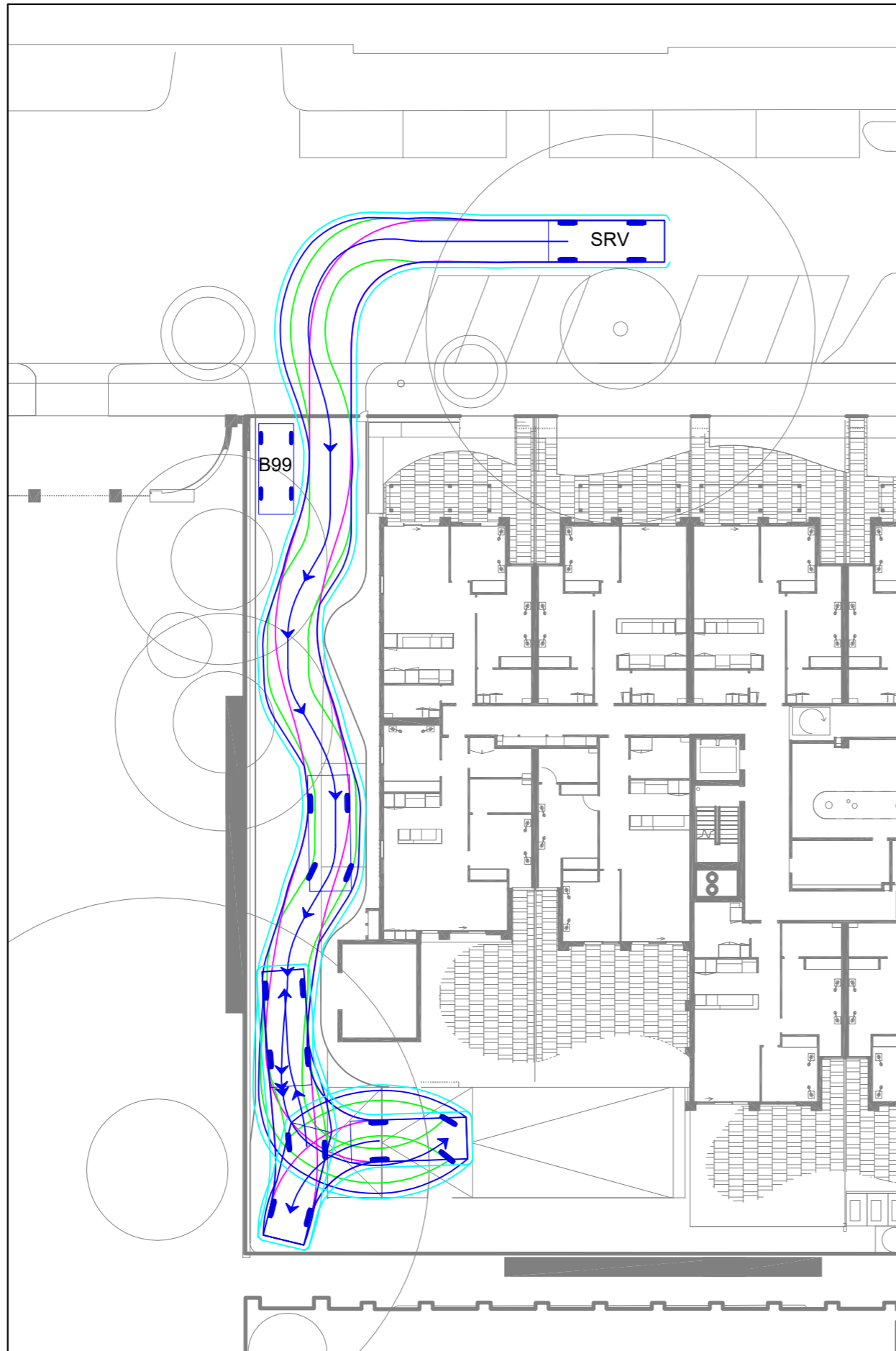


SRV (AS 2890.2) mm
Width : 2300
Track : 2300
Lock to Lock Time : 6.0
Steering Angle : 38.0

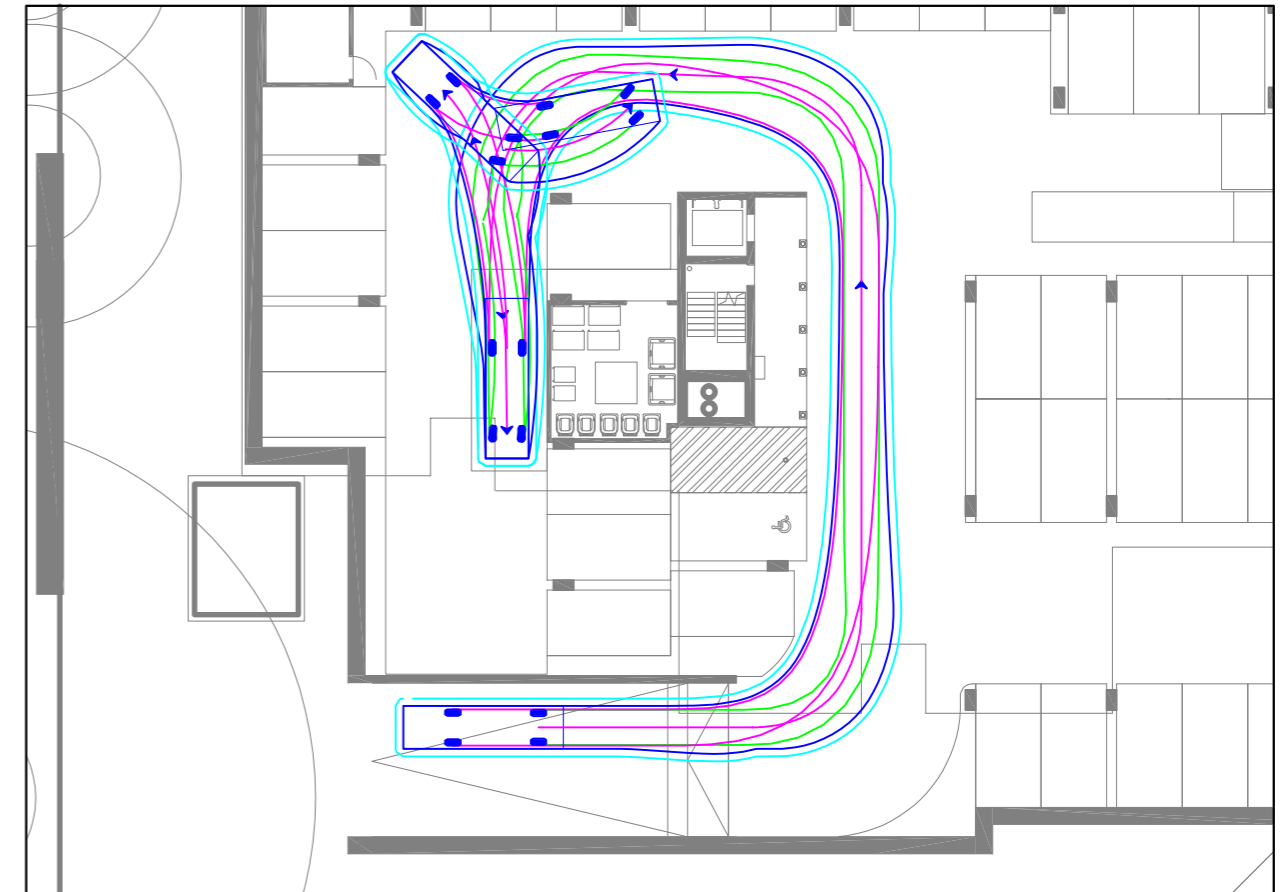
LEGEND

- REAR WHEELS
- FRONT WHEELS
- VEHICLE BODY
- BODY CLEARANCE

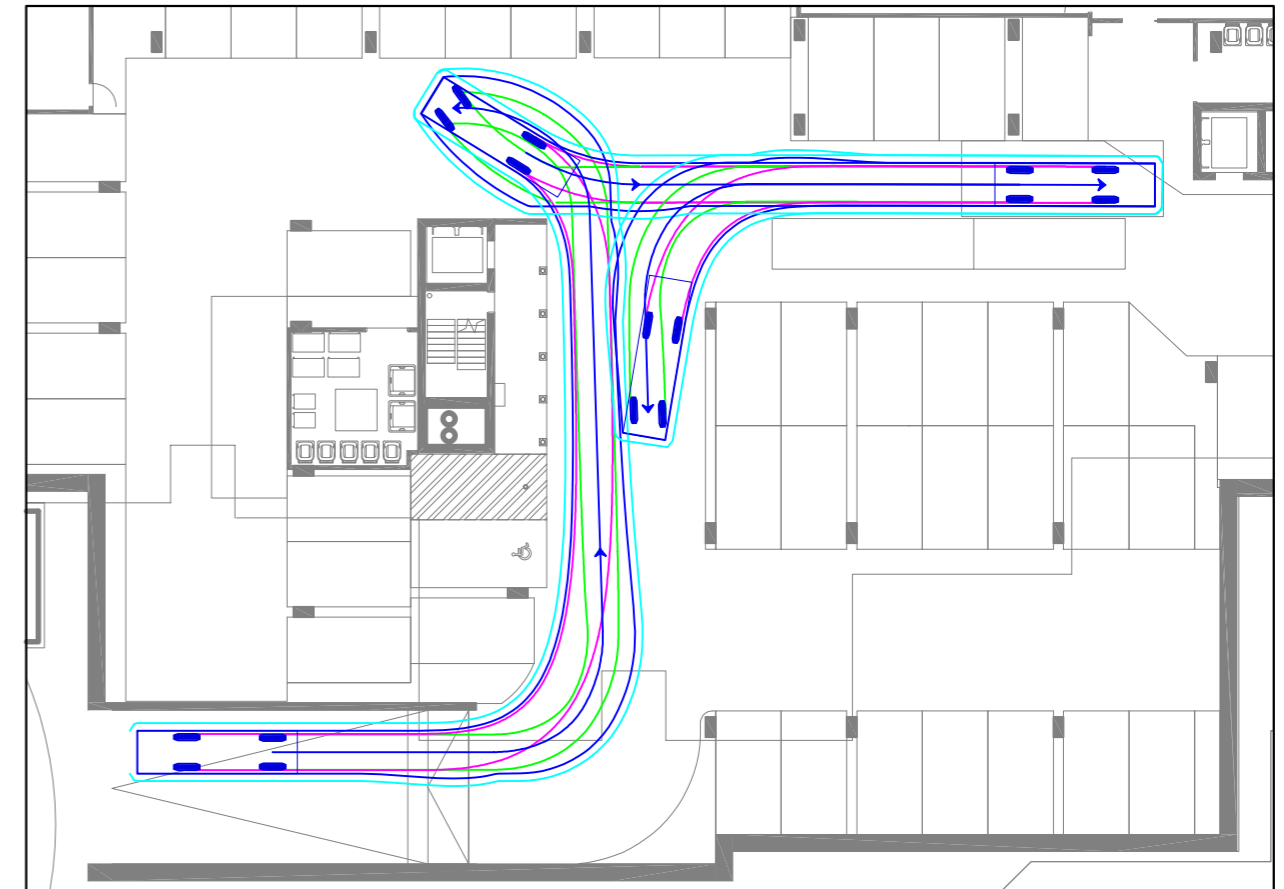
6.4m SRV SITE ACCESS - B99 PASSING



WASTE AREA 1 ACCESS



WASTE AREA 2 ACCESS



REV	DATE	NOTES	DESIGNED BY	CHECKED BY
A	15/08/2024	TOWN PLANNING	G. SINGH	J. COSSINS (RPE11787)
B	19/12/2024	RFI	N. MCCAFFREY	J. COSSINS (RPE11787)

2-10 CLARKE STREET, ABBOTSFORD
PROPOSED SENIOR RESIDENTIAL DEVELOPMENT

GENERAL NOTES:
BASED ON PLANS PREPARED BY CHT ARCHITECTS, DATED 05/12/2024.

FILE NAME: G34092-01
SHEET NO.: 01



SCALE: 1:300 (A3)
0 3 6

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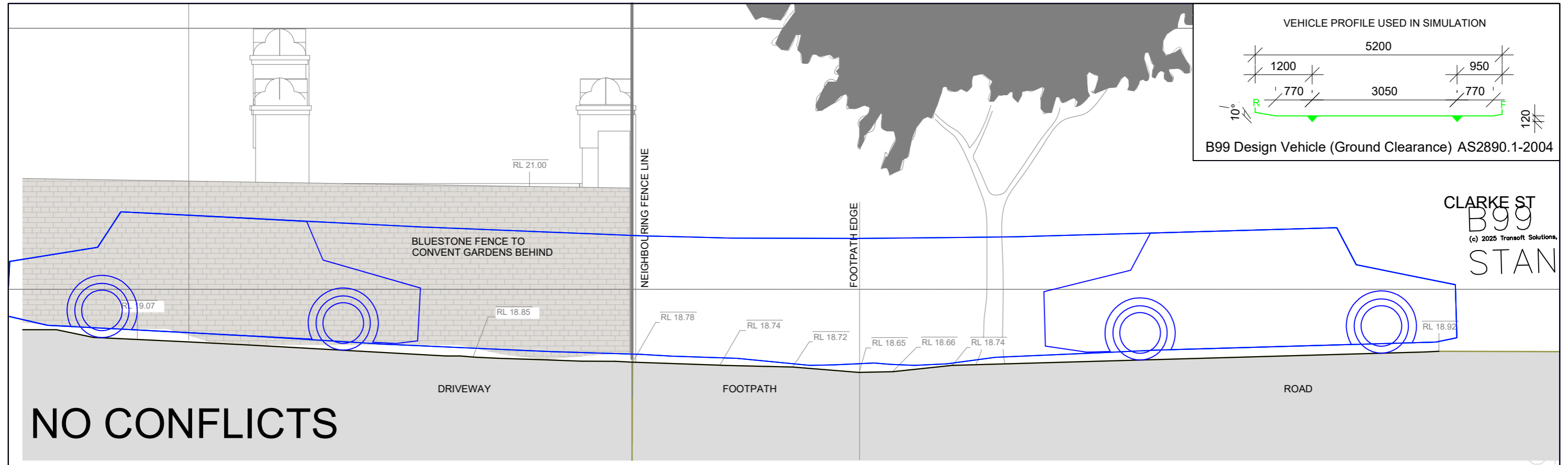
Appendix B

Ground Clearance Check

GROUND CLEARANCE CHECK - INGRESS



GROUND CLEARANCE CHECK - EGRESS



REV	DATE	NOTES	DESIGNED BY	CHECKED BY
A	15/01/2025	COUNCIL RFI	J. LEWIS	J. COSSINS (RPE11767)

2-10 CLARKE STREET, ABBOTSFORD
PROPOSED SENIOR RESIDENTIAL DEVELOPMENT

GENERAL NOTES:
BASED ON PLANS PREPARED BY CHT ARCHITECTS, DATED 13/01/2025

FILE NAME: G34092-01
SHEET NO.: 01



SCALE: 1:50 (A3)

0 0.5 1

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