



Agenda

Planning Decisions Committee Meeting

6:30 pm, Tuesday 24 February 2026

Fitzroy Town Hall

Planning Decisions Committee

The Planning Decisions Committee is a delegated committee of Council with full authority to make decisions in relation to planning applications and certain heritage referrals.

Addressing the Committee

Planning Decisions Committee meetings are decision making forums and only Councillors have a formal role. However, Council is committed to ensuring that any person whose rights will be directly affected by a decision of Council is entitled to communicate their views and have their interests considered before the decision is made.

There is an opportunity for both applicants and objectors to make a submission to Council in relation to each matter presented for consideration at the meeting.

Before each item is considered, the meeting chair will ask people who have registered to address the committee, to come forward to the microphone, state your name clearly for the record and:

- Speak for a maximum of five minutes;
- Direct your submission to the chair;
- Confine your submission to the planning permit under consideration;
- If possible, explain your preferred decision in relation to a permit application (refusing, granting or granting with conditions) and set out any requested permit conditions and avoid repeating previous submitters;
- Refrain from asking questions or seeking comments from the Councillors, applicants or other submitters;
- If speaking on behalf of a group, explain the nature of the group and how you are able to speak on their behalf.

Once you have made your submission, please remain silent unless called upon by the chair to make further comment or to clarify any aspects.

Councillors will then have an opportunity to ask questions of submitters. Submitters may determine whether or not they wish to take these questions.

Once all submissions have been received, the formal debate may commence. Once the debate has commenced, no further submissions, questions or comments from submitters can be received.

Arrangements to ensure our meetings are accessible to the public

Access to the building is available either by the stairs or lift. Seating is provided to watch the meeting, and the room is wheelchair accessible. Accessible toilet facilities are available. Speakers at the meeting are invited to be seated at the table to address the Council, and all participants are amplified via an audio system. Meetings are conducted in English.

If you are unable to participate in this environment, we can make arrangements to accommodate you if sufficient notice is given. Some examples of adjustments are:

- a translator in your language;
- the presence of an Auslan interpreter;
- loan of a portable hearing loop; and
- reconfiguring the room to facilitate access.

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1. Acknowledgement of Country

“Yarra City Council acknowledges the Wurundjeri Woi Wurrung people as the Traditional Owners and true sovereigns of the land now known as Yarra.

We acknowledge their creator spirit Bunjil, their ancestors and their Elders.

We acknowledge the strength and resilience of the Wurundjeri Woi Wurrung, who have never ceded sovereignty and retain their strong connections to family, clan and country despite the impacts of European invasion.

We also acknowledge the significant contributions made by other Aboriginal and Torres Strait Islander people to life in Yarra.

We pay our respects to Elders from all nations here today—and to their Elders past, present and future.”

2. Attendance, apologies and requests for leave of absence

Attendance

Councillors:

- Cr Stephen Jolly Mayor
- Cr Sharon Harrison Deputy Mayor
- Cr Evangeline Aston
- Cr Edward Crossland
- Cr Andrew Davies
- Cr Kenneth Gomez
- Cr Meca Ho
- Cr Sarah McKenzie
- Cr Sophie Wade

Council staff:

- Mary Osman General Manager City Sustainability and Strategy
- Narelle Jennings Manager Statutory Planning (Acting)
- Danielle Connell Senior Coordinator Statutory Planning

Governance

- Phil De Losa Manager Governance and Integrity
- Patrick O’Gorman Senior Governance Coordinator
- Mel Nikou Governance Officer

3. Declarations of Conflict of Interest

Any Councillor who has a conflict of interest in a matter being considered at this meeting is required to disclose that interest either by explaining the nature of the conflict of interest to those present or advising that they have disclosed the nature of the interest in writing to the Chief Executive Officer before the meeting commenced.

4. Confirmation of Minutes

RECOMMENDATION

That the minutes of the Planning Decisions Committee held on Tuesday 27 January 2026 be confirmed.

5. Planning Committee Reports

5.1. – PLN21/0666.01 - 25-29 Wangaratta Street Richmond

Author Joshua Broberg – Senior Planner Statutory Planning
Authoriser General Manager City Sustainability and Strategy-Mary Osman

Executive Summary

Property	25-43 Wangaratta Street Richmond
Ward	Lennox
Application number	PLN21/0666.01
Proposal	Section 72 Amendment to Planning Permit PLN21/0666 and endorsed plans (retrospective) to allow buildings and works associated with the construction of three bicycle hoops within the front setback, removal of 1 car parking space, removal of 14 bicycle parking spaces, and deletion of Conditions 1(b) and 6(b).
Zoning & Overlay/s	<ul style="list-style-type: none"> - Mixed Use Zone - Environmental Audit Overlay - Heritage Overlay (HO332) - Development Contributions Plan Overlay – Schedule 1
Strategic setting	Land subject to future strategic work (MUZ north of Richmond Station bound by Botherambo Street, Tanner Street, Punt Road and Stewart Street).
Submissions	16 objections
Key reasons for support	<ul style="list-style-type: none"> - A planning permit is no longer required for a car parking reduction under new car parking provisions (Amendment VC277). - Reduction in bicycle parking continues to exceed planning scheme requirements. - The construction of 3 bicycle parking hoops is of no consequence with regard to the Heritage Overlay or the Mixed Use Zone.

Recommendation	That Council Issues a Notice of Decision to Grant an Amended Planning Permit, with the following key changes: - Deletion of Conditions 1(b) and 6(b).
Contact Officer	Joshua Broberg, Senior Planner

Officer Recommendation

That having considered all objections and relevant planning policies, the Committee resolves to issue a Notice of Decision to Grant an Amended Planning Permit PLN21/0666 at 25-43 Wangaratta Street Richmond for:

Control	Clause	Matter for which the permit has been granted
Mixed Use Zone	32.04-2	To use land for an office
Mixed Use Zone	32.04-10	Buildings and works associated with a Section 2 Use
Heritage Overlay	43.01-1	Construct or carry out works
Car Parking	52.06-3	To reduce the minimum number of car parking spaces required under Clause 52.06-5

subject to the following conditions (deleted conditions are shown in ~~strikethrough~~):

1. Before the use commences, amended plans to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. When approved, the plans will be endorsed and will then form part of this permit. The plans must be drawn to scale with dimensions, and must be generally in accordance with the decision plans prepared by Olvaer, Plan no. A1-101 and dated 27.08.2021, but modified to show:
 - (a) Warehouse notation removed from the plans; and
 - (b) ~~Bicycle spaces to be shown on plans generally in accordance with the bicycle details/locations within the parking impact assessment plan, but including a minimum of 5 visitor bicycle spaces and 26 employee bicycle spaces within the building (i.e. not external); and~~
 - (c) Removal of reference to installation of bollard to Wangaratta Street adjacent water meter.
2. The use as shown on the endorsed plans must not be altered (unless the Yarra Planning Scheme specifies that a permit is not required) without the prior written consent of the Responsible Authority.
3. Except with the prior written consent of the Responsible Authority, the office use authorised by this permit may only operate between the following hours:

- (a) 7.00am – 10.00pm Monday – Friday;
 - (b) 7.00am – 8.00pm Saturday and Sunday; and
 - (c) No deliveries on Sundays in lieu of legal advice.
4. Roller-doors fronting Botherambo Street to be utilised only during operating hours of the office use to the satisfaction of the Responsible Authority.
 5. All lights except for emergency lighting and for cleaning purposes to be utilised only during operating hours of the office use to the satisfaction of the Responsible Authority.
 6. Before the use commences, an amended Parking Impact Assessment to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. When approved, the amended Parking Impact Assessment will be endorsed and will form part of this permit. The amended Parking Impact Assessment must be generally in accordance with the Parking Impact Assessment prepared by Cremorne Properties Pty Ltd and dated 9 November 2021, but modified to:
 - (a) Delete reference to the use of the site as a gymnasium; ~~and~~
 - (b) ~~Show 5 visitor bicycle spaces and 26 employee bicycle spaces within the building and removal of external bicycle parking space to the satisfaction of the Responsible Authority.~~
 7. The provisions, recommendations and requirements of the endorsed Traffic Impact Assessment must be implemented and complied with to the satisfaction of the Responsible Authority.
 8. The provisions, recommendations and requirements of the endorsed Waste Management Plan must be implemented and complied with to the satisfaction of the Responsible Authority.
 9. The use must comply at all times with the noise limits specified in the Environmental Protection Regulations under the EPA Act 2017 and the incorporated Noise Protocol (Publication 1826.4, EPA, May 2021).
 10. The amenity of the area must not be detrimentally affected by the use, including through:
 - (a) the transport of materials, goods or commodities to or from land;
 - (b) the appearance of any buildings, works or materials;
 - (c) the emission of noise, artificial light, vibration, smell, fumes, smoke, vapour, steam, soot, ash, dust, waste water, waste products, grit or oil, or
 - (d) the presence of vermin,to the satisfaction of the Responsible Authority.
 11. Except with the prior written consent of the Responsible Authority, delivery and collection of goods to and from the land may only occur between 7am and 9pm on any day.
 12. This permit will expire if:
 - (a) the office use is not commenced within two years of the date of this permit; or
 - (b) The use is discontinued for a period of two years.

The Responsible Authority may extend the period referred to if a request is made in writing before the permit expires or within six months afterwards for commencement.

Notes

All future employees within the development approved under this permit will not be permitted to obtain employee or visitor parking permits.

This site is subject to a Heritage Overlay. A planning permit may be required for any external works.

History and Background

1. Planning Permit PLN21/0666 was issued on 12 May 2022 and allows the use of the land as an office, along with an associated reduction in the car parking requirements. The approved office has a total floor area of 4,611 sqm and may operate between 7:00 am and 10:00 pm Monday to Friday, and 7:00 am to 8:00 pm Saturday and Sunday, in accordance with permit conditions.
2. The subject site comprises two buildings which had originally incorporated a mix of land uses, including a warehouse and office. The planning permit approved an increase to the existing office floor area by 2,612.4sqm.
3. The approval includes 25 on-site car parking spaces within an undercroft area to Wangaratta Street, representing a reduction of 47 spaces from the 72 spaces required under the Yarra Planning Scheme at the time.

Enforcement Action

4. The proposed amendment has been prompted by a planning compliance investigation, which identified that the required number of on-site bicycle spaces were not provided and that three external bicycle hoops had been installed within one of the on-site car parking spaces without the requisite planning approval.

Amendment VC277

5. Amendment VC277 was gazetted on 18 December 2025 and makes changes to Clause 52.06 (Car parking) of the Yarra Planning Scheme. The amendment updates parking requirements to align with public transport access and reduces the number of car parks required for development in locations well-served by public transport. Clause 52.06 has been updated to incorporate maximum car parking rates in some cases.
6. Due to the site's proximity to multiple public transport services, it now falls within a 'Category 4' area, where only a maximum car parking requirement is specified (no minimum requirement).
7. Under the former Clause 52.06, the use of the land for an office generated a minimum car parking requirement of 3 spaces per 100 square metres of net floor area. Under the current (new) Clause 52.06, office use in a category 4 area no longer has a minimum car parking requirement. Office now requires a maximum of 0.45 cars per 100sqm.
8. As such, there is no longer any minimum car parking requirement for the use of the subject site as an office.
9. As these changes occurred following advertising of the application, all submitters have been notified that there is no longer a planning requirement to reduce on-site car parking. Submitters have also been notified that they no longer hold rights of review and appeal for the application as it relates to Clause 52.06.

Site Context

10. The subject site is located on the eastern side of Wangaratta Street and western side of Botherambo Street. The site is 40 metres south of Tanner Street, diagonally across from the Stewart Street reserve and 80 metres north of Swan Street. The site consists of two allotments that currently form two addresses, as follows:

- (a) Lot 1 TP 672513F 25 - 29 Wangaratta Street, Richmond (southern allotment);
and
- (b) Lot 1 TP 531525M 31 - 43 Wangaratta Street, Richmond (northern allotment);
11. Collectively, the site has a northern boundary length of 35.28 metres, a Wangaratta Street frontage of 81.84 metres, a Botherambo Street frontage of 81.48 metres and a southern boundary length of 36.27 metres. The site has a total area of approximately 2,920sqm.
12. The northern site is occupied by a two-storey brick building. Where facing Wangaratta Street (Figure 1) the building is set back at the ground floor to allow for car parking, there is no formal footpath as this entire frontage is utilised as a vehicle crossover. The first floor is built to the street, cantilevering over the ground floor car parking area, creating an undercroft. Where facing Botherambo Street, the entire building is built to the boundary, with two single width vehicle crossovers providing access into the building from this street.



Figure 1 - Viewing southeast along Wangaratta Street to front of No. 31-43 Wangaratta Street. Source: Officer site visit 6 November 2025

13. The southern site is currently occupied by a two-storey brick building. Where facing Wangaratta Street the building is set back approximately 7 metres from the front boundary, with car parking accommodated in this setback. The majority of this frontage is utilised as a vehicle crossover providing access to the carparks. Where facing Botherambo Street, the entire building is built to the boundary, with two single width vehicle crossovers providing access into the building from this street. Centrally along this street is a double width vehicle crossover providing access to a small loading area.

Surrounds

14. The site is located north of Swan Street, a Major Activity Centre containing a wide range of retail, entertainment, dining and service offerings. To the south of Swan Street is the Cremorne area containing an emerging employment centre focused primarily on office but also containing a number of light industrial and warehouse uses. The MCG, AAMI Park and Rod Laver Arena, sports and entertainment precinct is located on the west side of Hoddle Street. The Melbourne CBD is located 2 kilometres north-west.
15. The Bridge Road and Church Street Activity Centres/retail precincts are located to the north and east respectively. Both these and Swan Street contain tram lines, with other public transport options including Richmond Train Station and buses on Hoddle Street.
16. The area to the east and north-east are characterised by 1-2 storey detached and semi-detached dwellings being in the Heritage Overlay and subject to the Neighbourhood Residential Zone (two storey height limit). The Mixed Use Zone (MUZ) to the west has warehouse conversions with additional built form constructed above original buildings, and contains a number of large sites that can accommodate more intensive development. The mix of zoning is demonstrated in Figure 2 and surrounding built form shown in Figure 3.



Figure 2 - The subject site and surrounding planning context (Source: VicPlan)



Figure 3 - Aerial image of the subject site (Source: Nearmap.com, 10 December 2025)

17. The site's immediate interfaces are as follows:

- (a) To the north of the subject site, at No. 23 Wangaratta Street, is a three storey dwelling that has frontages to both Wangaratta Street and Botherambo Street. The dwelling is built to the common boundary at all floors, with the windows and light courts oriented to the north, with the exception of a third floor balcony located along the common boundary;
- (b) The site to the south, at No. 45 Wangaratta Street, is a seven storey building constructed in accordance with Planning Permit PLN15/1260 which allows 'Use of the land for office, demolition, and construction of a 7 storey building containing offices and food and drink premises (café) above one basement level in the Mixed Use Zone and Heritage Overlay, reduction in the car parking requirements in clause 52.06 and in accordance with the endorsed plans';
- (c) To the east is Botherambo Street, a one-way (northbound) local road with parallel parking on the western side. On the east side of the street are dwellings generally fronting Lennox Street that are largely graded 'contributory' to the Richmond Hill heritage precinct. Both sides of Botherambo Street contains numerous vehicle crossovers and high fencing / roller doors associated with rear vehicle access; and
- (d) To the west is Wangaratta Street, a two-way local road with parallel parking on the western side. The street contains numerous vehicle crossovers and is approximately 10 metres in width including the footpaths. On the west side of the street is a mixture of commercial and residential uses between Stewart Street and Tanner Street with a number of converted factory buildings.

Proposal

18. The proposal is for a Section 72 Amendment to the permit and endorsed plans to allow buildings and works associated with the construction of three bicycle hoops within the

front setback, a further reduction of 1 car parking space, removal of 14 bicycle parking spaces, and deletion of Conditions 1b and 6b, with details as follows:

- (a) Decrease in staff bicycle spaces from 26 to 14 (-12);
- (b) Decrease in visitor bicycle spaces from 5 to 3 (-2);
- (c) A further reduction of 1 onsite car parking space (total of 24) (no permit required)
- (d) Buildings and works associated with the construction of three external bicycle hoops within the removed car parking space (northern-most car space of the northern building); and
- (e) Deletion of Conditions 1(b) and 6(b), which currently read:

1(b) *Bicycle spaces to be shown on plans generally in accordance with the bicycle details/locations within the parking impact assessment plan, but including a minimum of 5 visitor bicycle spaces and 26 employee bicycle spaces within the building (i.e. not external); and*

6(b) *Show 5 visitor bicycle spaces and 26 employee bicycle spaces within the building and removal of external bicycle parking space to the satisfaction of the Responsible Authority.*

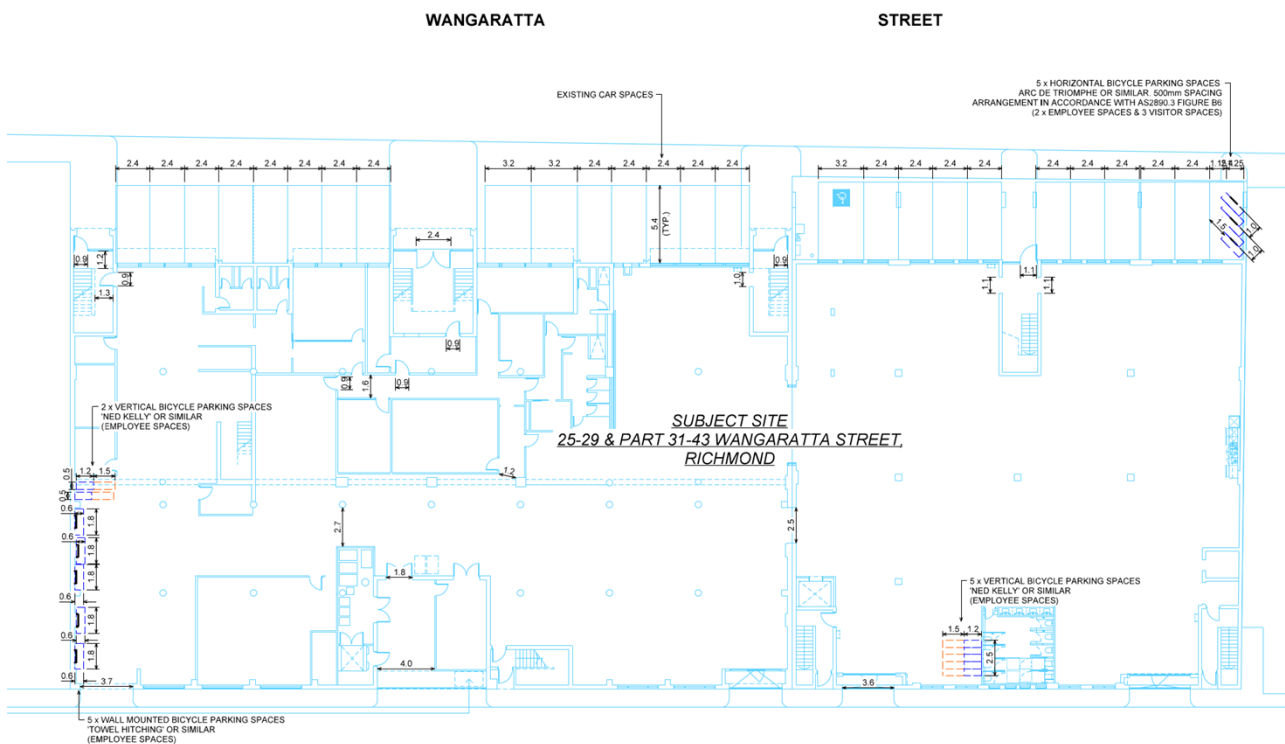


Figure 4 – Revised parking layout including external bicycle parking spaces (Source: Application plans)



Figure 5 - Proposed external bicycle parking hoops (Source: Officer site visit, 6 November 2025)

Statutory controls

19. The table contained in the Officer Recommendation sets out why a planning permit is required for this matter.
20. The following Clauses of the Yarra Planning Scheme are also relevant to the proposal:
 - (a) **Clause 32.04 - Mixed Use Zone (MUZ).** A planning permit is required under clause 32.04-2 for the office use as the total leasable floor area exceeds 250sqm. However, no changes to the approved use are proposed under this amendment. Pursuant to Clause 32.04-10, a planning permit is required for buildings and works (construction of bicycle hoops) associated with a Section 2 Use (Office);
 - (b) **Clause 43.01 - Heritage Overlay (HO332 – Richmond Hill Precinct, Richmond).** The buildings on the subject site are located within Schedule 332 and identified as 'not-contributory' to this precinct as outlined in the incorporated document *City of Yarra Review of Heritage Areas 2007 Appendix 8 (as updated from time to time)*. A planning permit is required for the buildings and works proposed under this amendment (construction of bicycle hoops);
 - (c) **Clause 45.03 - Environmental Audit Overlay (EAO).** The use of the site for an office is not a sensitive use and so the requirements of this overlay are not relevant to the application;
 - (d) **Clause 45.06 - Development Contributions Plan Overlay - Schedule 1 (DCPO1) -** the DCPO1 sets out mandatory development contributions for specific types of development. As the proposed amendment does not result in any further increase in commercial floor area, there is no impact on the requirements of the DCPO1;
 - (e) **Clause 52.06 (Car Parking).** Pursuant to Clause 52.06-3 (as at time of application made), a permit was required to reduce the number of car parking spaces under clause 52.06-3.

Amendment VC277 (state led amendment) updated Clause 52.06 across all Victorian planning schemes to better match car parking requirements with actual demand and access to public transport. The changes generally reduce minimum parking rates and introduce new maximums in areas with high-quality public transport access, using a new car parking requirement map to reflect differing

transport accessibility across the State. The intent is to reduce unnecessary parking in well-served locations, reduce development costs, support more sustainable travel and ensure parking requirements are proportionate to public transport availability and broader planning objectives.

As outlined at the outset of this report, under the updated Clause 52.06 the use of the land as an office no longer attracts a minimum car parking requirement. However, Amendment VC277 came into effect part-way through the assessment of this amendment application, and as such the amendment did not seek express permission to amend the permit preamble to reflect the updated Clause. As a result, while the previously approved parking reduction and the further reduction of one space now sought would not otherwise require a permit under Clause 52.06, the approved parking reduction will be retained within the permit preamble; and

- (f) **Clause 52.34 - Bicycle facilities** - sets out requirements for bicycle facilities, which apply to a new use or an increase in the floor area of an existing use. Where the floor area occupied by an existing use is increased, the requirement for bicycle facilities only applies to the increased floor area of the use.

A permit may be granted to vary, reduce or waive any requirement of Clause 52.34-5 and Clause 52.34-6. Under the requirements of Clause 52.34-5, an office should provide:

- 1 employee space to each 300 sqm of net floor area if the net floor area exceeds 1000 sqm; and
- 1 visitor space to each to each 1000 sqm of net floor area if the net floor area exceeds 1000 sqm.

The current permit approved an extension to the existing office use comprising an additional 2,612.4 square metres of net floor area. This additional floor area generated a requirement for 8 employee bicycle spaces and 2 visitor bicycle spaces, as well as one shower and one change room, which may be combined as end-of-trip facilities.

The amendment proposes to reduce the number of staff bicycle spaces from 26 to 14 (a reduction of 12 spaces) and the number of visitor bicycle spaces from 5 to 3 (a reduction of 2 spaces), while continuing to provide compliant end-of-trip facilities including change rooms and showers.

Accordingly, the amendment continues to meet the minimum requirements of Clause 52.34 and does not trigger the need for a permit to vary, reduce or waive the bicycle facility requirements.

Internal and External Consultation

Advertising

21. The application was advertised under the provisions of Section 52 of the *Planning and Environment Act (1987)* by 106 letters sent to surrounding owners and occupiers and by four (4) signs displayed on the Wangaratta Street and Botherambo Street frontages.
 22. Council received sixteen (16) objections, on the following key grounds:
 - (a) Endorsed reduction in car and bicycle parking is already generous and a further reduction would exacerbate parking pressure in local area;
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- (b) Amendment would legitimise/reward alleged planning breaches;
- (c) Further reduction in car and bicycle parking goes against previous Council decision; and
- (d) Endorsed bicycle parking provision required to offset parking reduction.

Policy Implications

Planning Policy Framework (PPF)

23. The following policies are of most relevance to this application:
- Clause 15 – Built environment and heritage:
 - Clause 15.01-2L – Building Design
 - Clause 15.03-1L-01 - Heritage
 - Clause 18 – Transport:
 - Clause 18.02-2L – Cycling

Officer Assessment

24. The primary considerations for this application are as follows:
- (a) Removal of car parking space;
 - (b) Reduction in bicycle parking requirements;
 - (c) Buildings and works associated with three bicycle parking hoops;
 - (d) Objector concerns; and
 - (e) Other matters.

Removal of car parking space

25. The proposed amendment seeks retrospective approval to remove one (1) onsite car parking space from the existing undercroft parking onto Wangaratta Street, resulting in a total of 24 car parking spaces. The space is proposed to be removed to accommodate three bicycle parking hoops.
26. The current planning permit had approved the provision of 25 on-site car parking spaces associated with the use of the land as an office, representing a reduction of 47 spaces from the 72 spaces that had been required under Clause 52.06 (Car Parking).
27. However, as detailed earlier in this report, Amendment VC277 has since updated Clause 52.06 across all Victorian planning schemes. As a result, the use of the land as an office at the subject site no longer attracts a minimum car parking requirement.
28. Accordingly, the approved parking reduction and further reduction of one space sought under this amendment no longer require planning permission under Clause 52.06. Therefore, the removal of a parking space to accommodate bicycle parking is acceptable.

Reduction in bicycle parking requirements

29. The proposed amendment seeks approval to reduce the number of bicycle spaces required under the permit as follows:
- (a) Decrease in staff bicycle spaces from 26 to 14 (-12); and
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- (b) Decrease in visitor bicycle spaces from 5 to 3 (-2).
30. The proposal also seeks the deletion of associated Conditions 1(b) and 6(b), which currently read:
- 1(b) *Bicycle spaces to be shown on plans generally in accordance with the bicycle details/locations within the parking impact assessment plan, but including a minimum of 5 visitor bicycle spaces and 26 employee bicycle spaces within the building (i.e. not external); and*
- 6(b) *Show 5 visitor bicycle spaces and 26 employee bicycle spaces within the building and removal of external bicycle parking space to the satisfaction of the Responsible Authority.*
31. As plans have been endorsed under condition 1, there is no requirement to delete condition 1(b) i.e.. the condition has already been met. However the applicant has requested this amendment.
32. In considering the amendment, the proposal would continue to exceed the Clause 52.34 (Bicycle Facilities) requirements, being:
- (a) 8 employee spaces, including one shower and change room; and
- (b) 2 visitor spaces.
33. Notwithstanding compliance with Clause 52.34, the policy guidelines of Clause 18.02-2L (Cycling) also seek to encourage best-practice provision of bicycle parking and end-of-trip facilities, including consistency with the Built Environment Sustainability Scorecard (BESS) administered by the Council Alliance for a Sustainable Built Environment.
34. The BESS best-practice benchmarks recommend one visitor bicycle space per 500 square metres of office floor area, and that employee bicycle parking be provided at a minimum rate of 10 per cent of building occupants (equivalent to one space per 100 square metres of office floor area). This results in a requirement of five visitor spaces and 26 employee bicycle spaces.
35. The original application had proposed to provide 25 onsite bicycle parking spaces, which substantially exceeds the Clause 52.34 requirements but falls short of the BESS best-practice benchmarks.
36. Council's original decision was based upon an officer recommendation to increase the number of onsite bicycle in line with BESS best-practice benchmarks, which would require five visitor bicycle spaces and 26 employee bicycle spaces (i.e. 6 additional bicycle parking spaces from what the applicant proposed).
37. The spaces were all required to be located internally, as any external spaces would have required a planning permit for associated buildings and works. The application at the time had only sought approval for use of the land as an office and a car parking waiver. The amendment now seeks approval for associated buildings and works to provide three bike spaces external to the building.
38. The proposed amendment would result in a shortfall of 14 total bicycle spaces when assessed against the BESS best-practice benchmarks.
39. However, it is noted that BESS is primarily intended to apply to development applications involving new buildings and works. The BESS framework describes its purpose as assisting in the assessment of planning permit applications where new buildings or
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dwellings are proposed, including non-residential and mixed-use developments. It is not intended to strictly apply to 'change of use' proposals within existing buildings.

40. The applicant has advised that the proposed reduction has been sought due to spatial constraints within the office tenancy layout and in response to the operational requirements of the office tenant.
41. Having regard to the above, the proposed reduction is considered acceptable. The amended provision would continue to exceed the minimum statutory requirements of Clause 52.34, accords with the practical limitations associated with an existing building, and retains a reasonable level of support for cycling consistent with Clause 18.02-2L.
42. It is also noted that at the time of the original permit decision, the increased provision of bicycle parking supported a broader transport response, partially offsetting the significant reduction in onsite car parking that was required under Clause 52.06 (Car Parking) at the time. However, as detailed in this report, Amendment VC277 has since amended Clause 52.06 such that the use of the land as an office no longer attracts a minimum car parking requirement.
43. In this context, the previously approved reduction in car parking, as well as the further removal of one space to accommodate external bicycle parking, no longer requires justification under the Planning Scheme. Accordingly, the bicycle parking provision is no longer required to function as an offset or mitigation measure for reduced car parking supply. This change in policy context further supports consideration of the bicycle parking numbers on their own merits.
44. On this basis, it is considered appropriate to support the deletion of Conditions 1(b) and 6(b) and to allow the reduced provision of bicycle parking as proposed. The amendment would not result in any unacceptable planning outcomes or materially undermine sustainable transport objectives for the site.

Buildings and works associated with three bicycle parking hoops

45. The amendment seeks retrospective approval for the construction of three bicycle hoops in place of a car space, which requires buildings and works permission within the Mixed Use Zone and Heritage Overlay.



Figure 6 - Proposed external bicycle parking hoops (Source: Officer site visit, 6 November 2025)

46. The three bicycle hoops accommodate five horizontal bicycle parking spaces, including all three proposed visitor spaces. The relocation of the visitor parking to within the external under-croft parking area is an improved outcome for visitors, offering convenient and legible access without requiring entry into the building. This approach aligns with the local policy direction at Clause 18.02-2L (Cycling), which encourages the provision of easily accessible and well-located visitor bicycle parking.
47. With regard to the objectives of the Mixed Use Zone and Heritage Overlay, the bicycle hoops are discretely located and do not pose any adverse impact with regard to neighbourhood character or the heritage significance of the Richmond Hill Precinct, particularly as the subject building is “not-contributory”.
48. In relation specifically to the Mixed Use Zone, the decision considerations at clause 32.04-15 state:
- Before deciding on an application, in addition to the decision guidelines in Clause 65, the responsible authority must consider, as appropriate:*
- *The Municipal Planning Strategy and the Planning Policy Framework.*
 - *The objectives set out in a schedule to this zone.*
 - *Any other decision guidelines specified in a schedule to this zone.*
 - *The impact of overshadowing on existing rooftop solar energy systems on dwellings on adjoining lots in a Housing Choice and Transport Zone, Mixed Use Zone or Residential Growth Zone.*
49. The works do not involve any alterations to the external built form or fabric that contributes to the identified heritage significance of the precinct. The bicycle hoops will be located adjacent the northern-most wall and will be largely consistent with the existing streetscape; a utilitarian corridor defined by car parking bays, vehicle crossovers and roller doors.
50. Overall, it is considered that the retrospective approval of the bicycle hoops satisfies the relevant objectives of the Mixed Use Zone and Heritage Overlay, results in no material amenity or heritage impacts, and represents an appropriate and policy-supported outcome. Accordingly, the amendment is acceptable from a planning perspective and warrants support.

Objector Concerns

51. Key objector concerns have been addressed in the assessment above. Other matters raised are addressed as follows:
- (a) **Amendment would legitimise / reward alleged planning breaches:** Alleged planning breaches are not a relevant consideration in the assessment of a planning application which must be determined on its planning merits. The amendment provides Council with the opportunity to assess the acceptability of the proposed changes under the planning scheme, separate from any enforcement matters. As set out in the report, the works are appropriate; and
 - (b) **Further reduction in car and bicycle parking goes against previous Council decision:** Applicants can seek amendments to permits. In this instance, the amendment has been sought within a new policy context where a permit is no longer required for any parking reduction. The proposal is consistent with current strategic and local policy objectives and does not warrant refusal on this basis.
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Other matters

Permit Preamble

- 52. The *Planning and Environment Amendment Regulations (2024)* (Amendment Regulations) were made on 8 October 2024. As such, the permit preamble is to be updated to reflect this, along with the changes in permit triggers.
- 53. The permit preamble is recommended to change from:
Use of the land as office and associated reduction of car parking.
 To:

Planning Scheme Clause	Matter for which the permit has been granted
32.04-2	<i>To use land for an office</i>
32.04-10	<i>Buildings and works associated with a Section 2 Use</i>
43.01-1	<i>Construct or carry out works</i>
52.06-3	<i>To reduce the minimum number of car parking spaces required under Clause 52.06-5</i>

Conditions

- 54. As discussed within the report, deletion of Condition 1(b) and Condition 6(b) can be supported.

Conclusion

- 55. The proposed amendment demonstrates compliance with policy objectives contained within the Planning Policy Framework and Municipal Strategic Statement with respect to car and bicycle parking requirements and does not undermine heritage character nor private and public amenity.
- 56. The proposal provides a balance between the range of planning objectives and imperatives and achieves an acceptable planning outcome that complies with the relevant Council policies.

Legal and Legislative Obligations

Conflict of interest disclosure

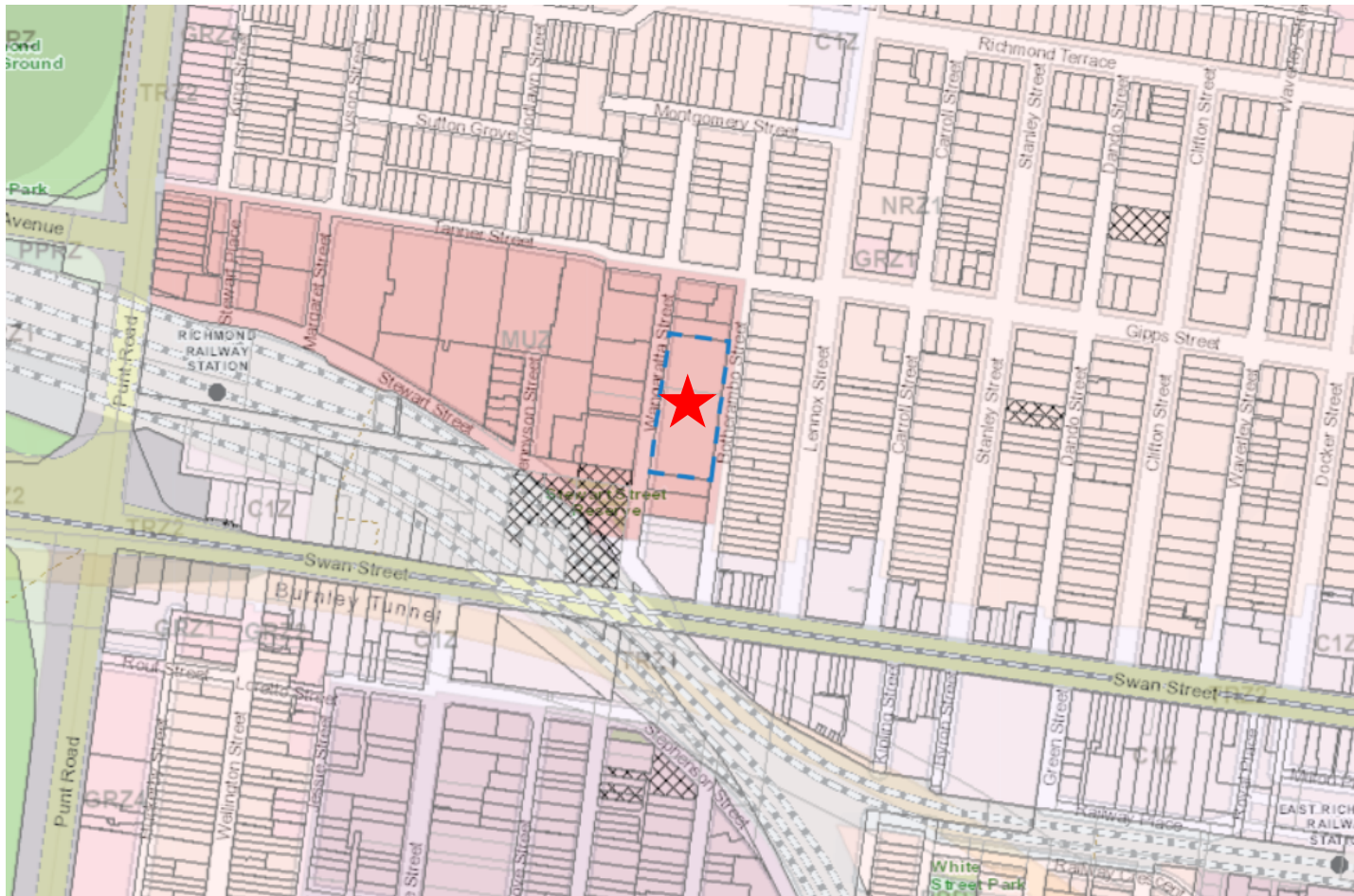
- 57. Section 130 of the Local Government Act 2020 requires members of Council staff and persons engaged under contract to provide advice to Council to disclose any conflicts of interest in a matter to which the advice relates.
- 58. The Officer reviewing this report, having made enquiries with relevant members of staff, reports that no disclosable interests have been raised in relation to this report.

Report Attachments

1. 5.1.1 PL N 210666.01 - 25-29 Wangaratta Street Richmond - Site Context Map
2. 5.1.2 PL N 210666.01 - 25-29 Wangaratta Street Richmond - Current Planning Permit
3. 5.1.3 PL N 210666.01 - 25-29 Wangaratta Street Richmond - Current Endorsed Plans (Condition 1)
4. 5.1.4 PL N 210666.01 - 25-29 Wangaratta Street Richmond - Current Endorsed Parking Impact Assessment (Con
5. 5.1.5 PL N 210666.01 - 25-29 Wangaratta Street Richmond - Decision Plans
6. 5.1.6 PL N 210666.01 - 25-29 Wangaratta Street Richmond - Decision Parking Impact Assessment
7. CONFIDENTIAL -5.1.7 PL N 21 0666.01 - 25-29 Wangaratta St Richmond, Submitter List

PLN21/0666.01 – 25-43 Wangaratta Street, Richmond

Site Context Map



Yarra City Council (Responsible Authority)
Yarra Planning Scheme



PLANNING PERMIT (Corrected)
PLN21/0666

Address of the land: 25-43 Wangaratta St, Richmond VIC 3121

The permit allows: Use of the land as office and associated reduction of car parking

The following conditions apply to the permit:

1. Before the use commences, amended plans to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. When approved, the plans will be endorsed and will then form part of this permit. The plans must be drawn to scale with dimensions, and must be generally in accordance with the decision plans prepared by Olvaer, Plan no. A1-101 and dated 27.08.2021, but modified to show:
 - (a) Warehouse notation removed from the plans;
 - (b) Bicycle spaces to be shown on plans generally in accordance with the bicycle details/locations within the parking impact assessment plan, but including a minimum of 5 visitor bicycle spaces and 26 employee bicycle spaces within the building (i.e. not external); and
 - (c) Removal of reference to installation of bollard to Wangaratta Street adjacent water meter.
2. The use as shown on the endorsed plans must not be altered (unless the Yarra Planning Scheme specifies that a permit is not required) without the prior written consent of the Responsible Authority.
3. Except with the prior written consent of the Responsible Authority, the office use authorised by this permit may only operate between the following hours:
 - (a) 7.00am – 10.00pm Monday – Friday;
 - (b) 7.00am – 8.00pm Saturday and Sunday; and
 - (c) No deliveries on Sundays in lieu of legal advice.
4. Roller-doors fronting Botherambo Street to be utilised only during operating hours of the office use to the satisfaction of the Responsible Authority.
5. All lights except for emergency lighting and for cleaning purposes to be utilised only during operating hours of the office use to the satisfaction of the Responsible Authority.
6. Before the use commences, an amended Parking Impact Assessment to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. When approved, the amended Parking Impact Assessment will be endorsed and will form part of this permit. The amended Parking Impact Assessment must be generally in accordance with the Parking Impact Assessment

Planning Permit PLN21/0666
Date issued: 12 May 2022

Danielle Connell
Signature for the Responsible Authority

prepared by Cremorne Properties Pty Ltd and dated 9 November 2021, but modified to:

- (a) Delete reference to the use of the site as a gymnasium; and
 - (b) Show 5 visitor bicycle spaces and 26 employee bicycle spaces within the building and removal of external bicycle parking space to the satisfaction of the Responsible Authority.
7. The provisions, recommendations and requirements of the endorsed Traffic Impact Assessment must be implemented and complied with to the satisfaction of the Responsible Authority.
 8. The provisions, recommendations and requirements of the endorsed Waste Management Plan must be implemented and complied with to the satisfaction of the Responsible Authority.
 9. The use must comply at all times with the noise limits specified in the Environmental Protection Regulations under the EPA Act 2017 and the incorporated Noise Protocol (Publication 1826.4, EPA, May 2021).
 10. The amenity of the area must not be detrimentally affected by the use, including through:
 - (a) the transport of materials, goods or commodities to or from land;
 - (b) the appearance of any buildings, works or materials;
 - (c) the emission of noise, artificial light, vibration, smell, fumes, smoke, vapour, steam, soot, ash, dust, waste water, waste products, grit or oil, or
 - (d) the presence of vermin.

to the satisfaction of the Responsible Authority.
 11. Except with the prior written consent of the Responsible Authority, delivery and collection of goods to and from the land may only occur between 7am and 9pm on any day.
 12. This permit will expire if:
 - (a) the office use is not commenced within two years of the date of this permit; or
 - (b) The use is discontinued for a period of two years.

The Responsible Authority may extend the period referred to if a request is made in writing before the permit expires or within six months afterwards for commencement.

Notes

All future employees within the development approved under this permit will not be permitted to obtain employee or visitor parking permits.

This site is subject to a Heritage Overlay. A planning permit may be required for any external works.

THIS PERMIT HAS BEEN CORRECTED AS FOLLOWS:

Date of amendment	Brief description of amendment
11 April 2024	Pursuant to Section 71 of the <i>Planning and Environment Act</i> (1987) the Condition 1 Plans re-endorsed to include the ground and first-floor plans.

PLANNING PERMIT

IMPORTANT INFORMATION ABOUT THIS PERMIT

WHAT HAS BEEN DECIDED?

The responsible authority has issued a permit. Note: This is not a permit granted under Division 5 or 6 of Part 4 of the *Planning and Environment Act 1987*.

WHEN DOES A PERMIT BEGIN?

A permit operates:

- From the date specified in the permit; or
- If no date is specified, from:
 - (i) The date of the decision of the Victorian Civil and Administration Tribunal, if the permit was issued at the direction of the Tribunal; or
 - (ii) The date on which it was issued, in any other case

WHEN DOES A PERMIT EXPIRE?

1. A permit for the development of land expires if:
 - the development or any stage of it does not start within the time specified in the permit; or
 - the development requires the certification of a plan of subdivision or consolidation under the *Subdivision Act 1988* and the plan is not certified within two years of the issue of the permit, unless the permit contains a different provision; or
 - the development or any stage is not completed within the time specified in the permit, or, if no time is specified, within two years after the issue of the permit or in the case of a subdivision or consolidation within five years of the certification of the plan of subdivision or consolidation under the *Subdivision Act 1988*.
2. A permit for the use of land expires if:
 - the use does not start within the time specified in the permit, or if no time is specified, within two years after the issue of the permit; or
 - the use is discontinued for a period of two years.
3. A permit for the development and use of land expires if:
 - the development or any stage of it does not start within the time specified in the permit; or
 - the development or any stage of it is not completed within the time specified in the permit, or, if no time is specified, within two years after the issue of the permit; or
 - the use does not start within the time specified in the permit, or, if no time is specified, within two years after the completion of the development; or
 - the use is discontinued for a period of two years.
4. If a permit for the use of land or the development and use of land or relating to any of the circumstances mentioned in section 6A(2) of the *Planning and Environment Act 1987*, or to any combination of use, development or any of those circumstances requires the certification of a plan under the *Subdivision Act 1988*, unless the permit contains a different provision:
 - the use or development of any stage is to be taken to have started when the plan is certified; and
 - the permit expires if the plan is not certified within two years of the issue of the permit.

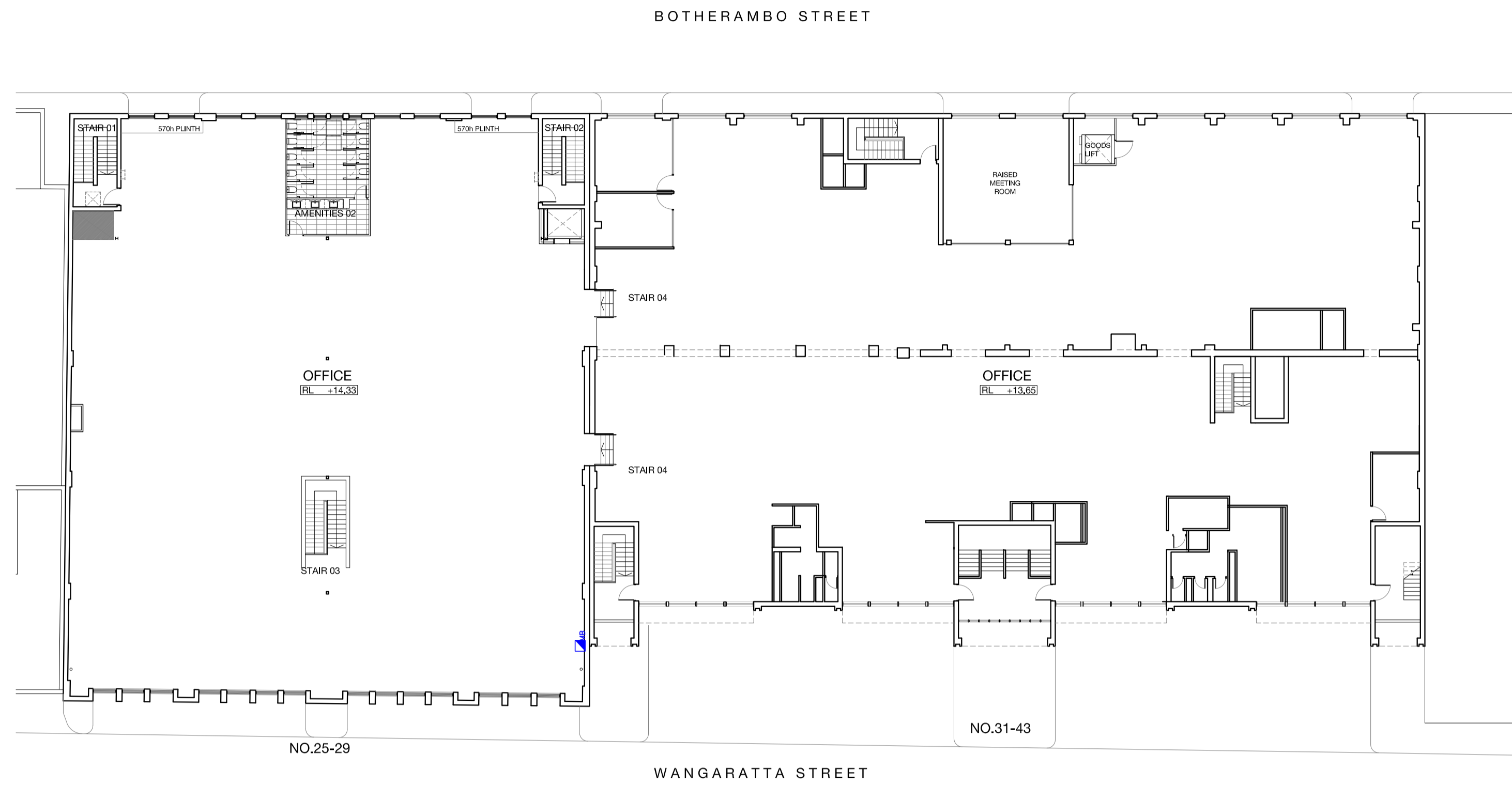
5. The expiry of a permit does not affect the validity of anything done under that permit before the expiry.

WHAT ABOUT REVIEWS?

- The person who applied for the permit may apply for a review of any condition in the permit unless it was granted at the direction of the Victorian Civil and Administrative Tribunal, in which case no right of review exists.
- An application for review must be lodged within 60 days after the permit was issued, unless a notice of decision to grant a permit has been issued previously, in which case the application for review must be lodged within 60 days after the giving of that notice.
- An application for review must be lodged with the [Victorian Civil and Administrative Tribunal \(VCAT\)](#).
- An application for review must be made on the relevant form which can be obtained from [Victorian Civil and Administrative Tribunal \(VCAT\)](#), and must be accompanied by the applicable fee.
- An application for review must state the grounds upon which it is based.
- A copy of an application for review must also be served on the responsible authority.

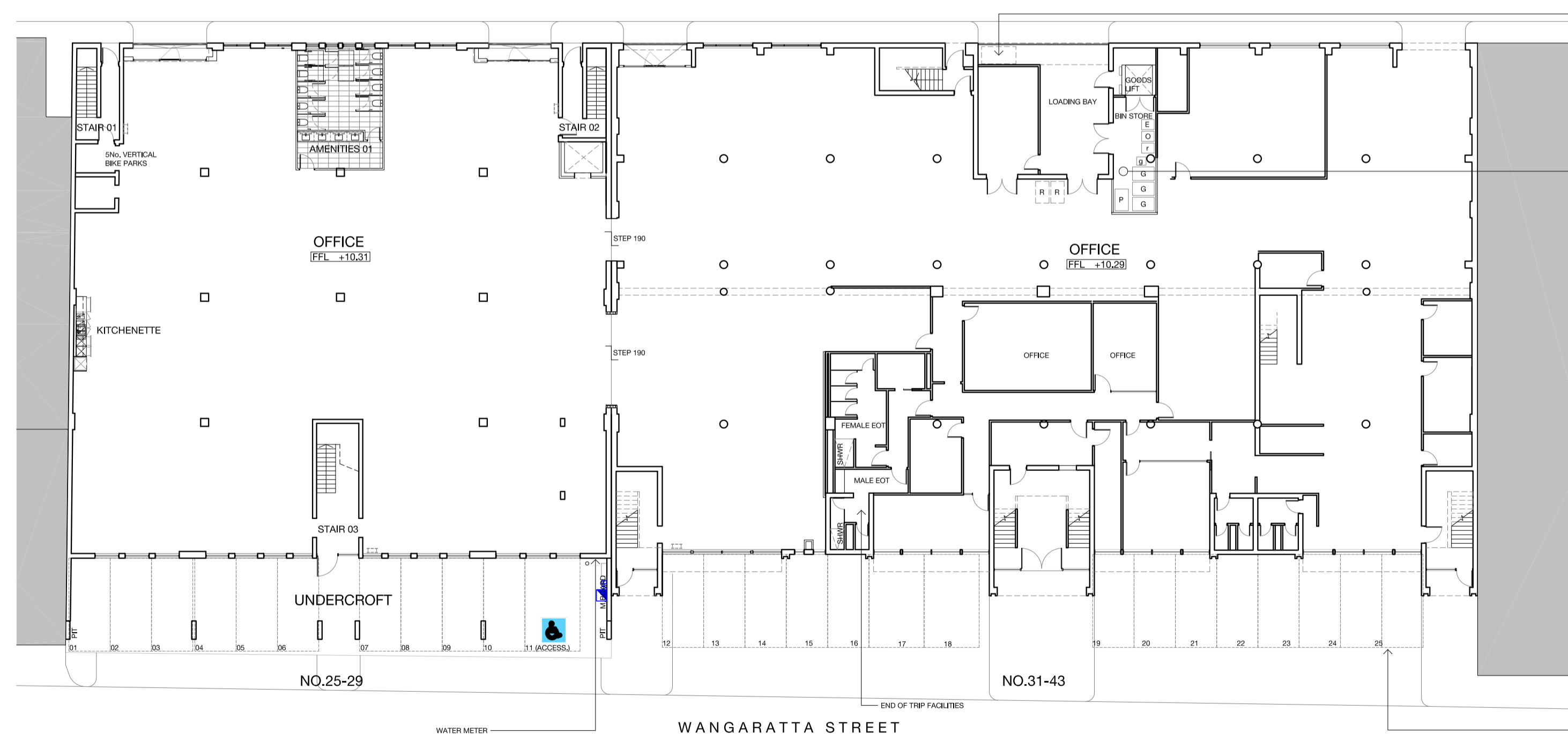
Information about the VCAT process, applications for review and the fees payable to the Victorian Civil and Administrative Tribunal you can look at Council's website <https://www.yarracity.vic.gov.au/services/planning-and-development/decisions-and-appeals>, [download the Planning proceedings at VCAT factsheet](#) (PDF) or visit the [VCAT website](#).

OLAVER



02 PROPOSED FIRST FLOOR PLAN 1:200

PLANNING ENVIRONMENT ACT 1987 YARRA PLANNING SCHEME Plan referred to in Permit No: PLN21/0666
 Approved by Council's Delegate Ally Huynh DATE 11/04/2024 PLAN 1 of 2
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01 PROPOSED GROUND FLOOR PLAN 1:200

29m² BULKY WASTE & WASTE STORE

BN STORE TO BE PROVIDED WITH HOT AND COLD WATER AND FLOOR WASTE AND MECHANICALLY VENTED

BN NOMINATIONS AS FOLLOWS:

- G 29m² BULKY WASTE STORE COLLECTED 3 X WEEKLY
- R 29m² RECYCLED RECYCLING BIN COLLECTED 3 X WEEKLY
- C 29m² CO-MINGLED RECYCLING BIN COLLECTED 3 X WEEKLY
- P 29m² DEDICATED PAPER BIN COLLECTED 3 X WEEKLY
- G 29m² GLASS ONLY BIN COLLECTED AS REQUIRED TO ENSURE BINS ARE MAINTAINED AND SERVICEABLE
- O 29m² ORGANIC FOOD WASTE COLLECTED 3 X WEEKLY
- E 29m² WASTE BIN COLLECTED AS REQUIRED TO ENSURE BINS ARE MAINTAINED AND SERVICEABLE

GROUND FLOOR BICYCLE PARKING LAYOUT TO BE IN ACCORDANCE WITH THE BICYCLE PARKING PLAN PREPARED BY IMPACT DATED 09.02.2024

TOTAL 23 CAR PARKS, 10% ACCESSIBLE

THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL RELEVANT CONTRACT SPECIFICATIONS AND DRAWINGS, DO NOT SCALE OFF DRAWINGS, USE FINISHED DIMENSIONS ONLY

CHECK AND VERIFY ALL DIMENSIONS ON SITE PRIOR TO COMMENCING WORK, ANY DISCREPANCIES BETWEEN DRAWINGS AND/OR SITE CONDITIONS MUST BE BROUGHT TO THE ARCHITECT'S ATTENTION BEFORE PROCEEDING.

COPYRIGHT OLAVER ARCHITECTURE.

2016 25WSR

ADDRESS: 25-29 & 31-43 WANGARATTA STREET RICHMOND, VIC 3121

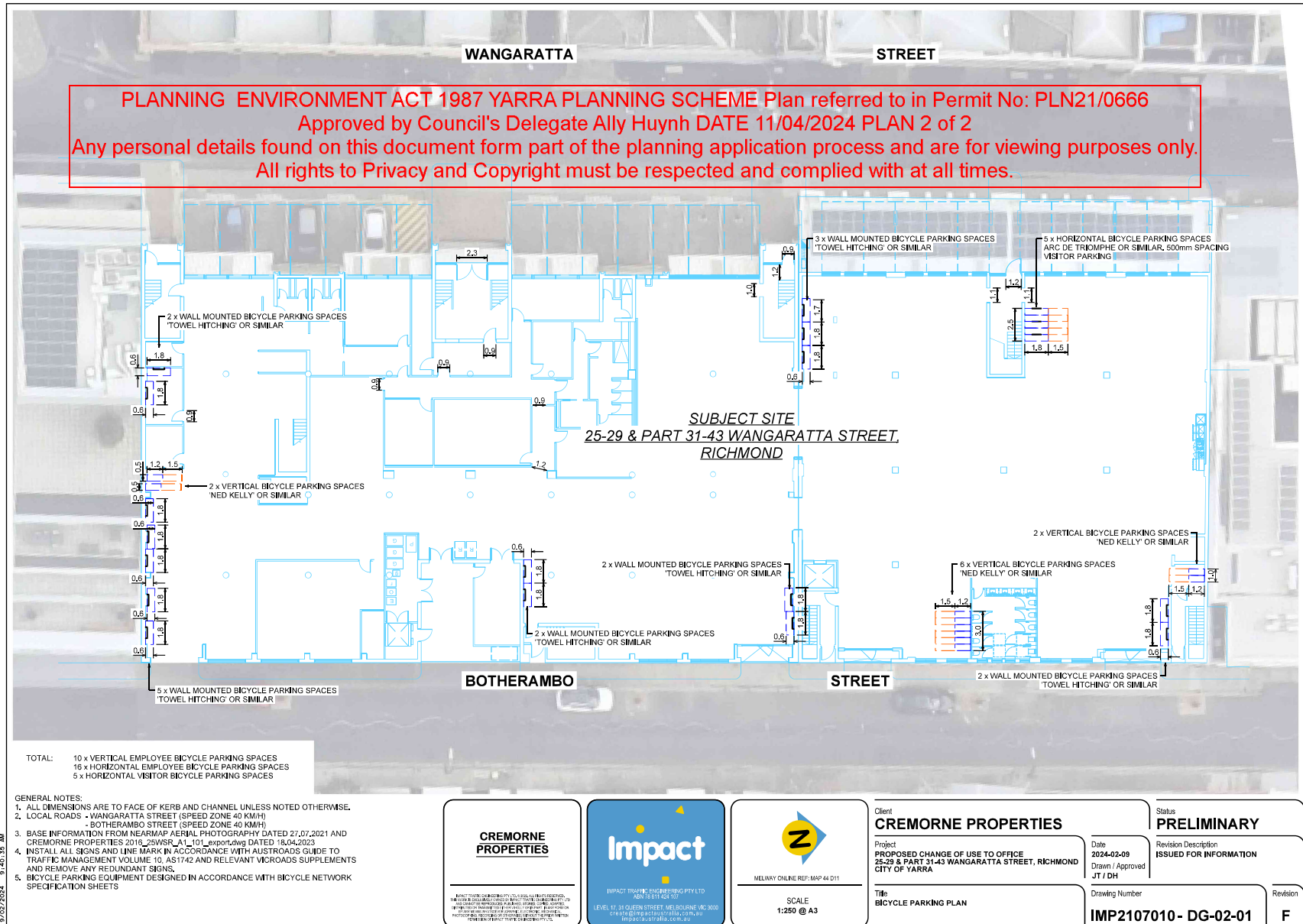
CLIENT: WANGARATTA SOUTH PTY LTD

STAGE: CHANGE OF USE APP

REV	DATE	DRAWN	ISSUE
00	12.08.21	EO	FOR COMMENT
01	14.08.21	EO	FOR COMMENT
02	18.05.22	EO	FOR ENDORSEMENT
03	18.05.22	MJL	FOR ENDORSEMENT
04	03.12.22	TH	FOR ENDORSEMENT
05	23.02.23	TH	FOR ENDORSEMENT
06	17.05.23	TH	FOR ENDORSEMENT
07	18.02.23	TH	FOR ENDORSEMENT
08	27.02.24	EO	FOR ENDORSEMENT

PROPOSED PLANS
 SCALE: 1:1000 MAX

A1-101



PLANNING ENVIRONMENT ACT 1987 YARRA PLANNING SCHEME Parking Impact Assessment referred to in Permit No: PLN21/0666 relating to Condition No: 6 Approved by Council's Delegate Nish Goonetilleke DATE 18/03/2024 PAGES 1 - 65 of 65 Any personal details found on this document form part of the planning application process and are for viewing purposes only.
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37°49'26"S
144°59'34"E

Change of Use: Retail to Office: 25 - 43 Wangaratta Street, Richmond



Parking Impact Assessment

1 February 2024
Prepared for Cremorne Properties Pty Ltd

IMP2107010TTA01F07

Impact

Change of Use:
Retail to Office: 25 - 43 Wangaratta Street, Richmond

Parking Impact Assessment

1 February 2024

Company Information

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Document Information

Client Cremorne Properties Pty Ltd
Report Title Change of Use:
Retail to Office: 25 - 43
Wangaratta Street, Richmond
Report Reference IMP2107010TTA01F07
Date of Issue 1 February 2024
Approved By John-Paul Maina

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Version	Date	Author
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FINAL	31 August 2021	Jake Townley
DRAFT	19 July 2021	Jake Townley



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Change of Use:
Retail to Office: 25 - 43 Wangaratta Street, Richmond

Parking Impact Assessment

1 February 2024

Appendices

- APPENDIX A Green Travel Plan
- APPENDIX B Functional Layout Plan
- APPENDIX C Bicycle Parking - Specification Sheets

1 IMPACT[®] Snap Shot

Development Proposition

Location	37°49'26"S 144°59'34"E	25-43 Wangaratta Street, Richmond
Use	Change of use of the existing tenancies within the building at 25-43 Wangaratta Street, Richmond to enable the building to be used for the purpose of office use The proposed change in land uses as compared to existing approved uses can be summarised as follows:	
	Existing office use:	1,998.8 sq.m
	Conversion of existing warehouse to office use:	444.4 sq.m
	Undefined use to office use:	2,168 sq.m
Car Parking	No Changes in Quantum of Parking.	
Bicycle Parking	Additional 31 bicycle parking spaces provided	

Statutory Controls

Particular Provisions

Clause 52.06 - Car Parking

Requirement	The parking provision implications of the proposal reveals a net parking requirement for 72 spaces This calculation of the parking requirement (net difference) results in a conservatively high outcome, noting that the undefined areas have historically operated for a range of uses including Warehousing, which attracts a statutory requirement.
--------------------	--

Adequacy of Provision	The site has excellent access to alternative transport modes which provide convenient and sufficient alternatives to private motor vehicle use. The reduced parking provision is consistent with City of Yarra's strategic transport objectives, and there are precedents of similar scale developments approved (by both VCAT and Council) with a similar parking ratios. The proposed provision is considered acceptable in this context. Based on the foregoing considerations, we consider that the development proposition satisfies the purpose of Clause 52.06, specifically: <ul style="list-style-type: none"> — To ensure that the car parking does not adversely affect the amenity of the locality. — To ensure the provision of an appropriate number of car parking spaces having regard to the demand likely to be generated, the activities on the land and the nature of the locality. — To support sustainable transport alternatives to the motor car.
------------------------------	--

Clause 52.34 - Bicycle Facilities

Requirement	The bicycle parking provision implications of the proposal reveals a requirement for 10 additional spaces.
--------------------	--



	This calculation of the parking requirement net difference results in a conservatively high outcome, noting that the undefined areas have historically operated for a range of uses which attract a statutory requirement.
Adequacy of Provision	A total of 31 bicycle spaces are planned. The proposed provision comfortably exceeds the statutory requirement for bicycle parking and will contribute positively to encouraging cycling as a mode of transport to the site.
Design	Assessed against Clause 52.34 and AS2890.3:2015 and determined to have satisfied the relevant design guidelines.
Conclusion	

— There are no traffic and transport grounds that should prohibit the issue of a permit.

2 Introduction

2.1 Engagement

IMPACT[®] have been engaged by Cremorne Properties Pty Ltd to undertake a Parking Impact assessment of the proposal to use the existing building at 25-43 Wangaratta Street, Richmond for the purpose of office use.

2.2 Scope of Engagement

This Traffic and Transport Assessment report has been prepared to accompany a town planning submission. In preparing this assessment we have referenced the following:

- As Built and Proposed Change of Use Plans;
- City of Yarra Planning Scheme, specifically:
 - Clause 21.06 - Transport;
 - Clause 52.06 - Car Parking;
 - Clause 52.34 - Bicycle Facilities; and
 - Clause 65.01 - Approval of an Application or Plan.

3 Existing Conditions

3.1 Location

3.1.1 General

The subject site is located on the eastern side of Wangaratta Street, Richmond as shown in the locality plan and aerial photograph represented in Figure 1 and Figure 2 respectively. The site is located in the Richmond Station Precinct within The Swan Street Major Activity Centre.

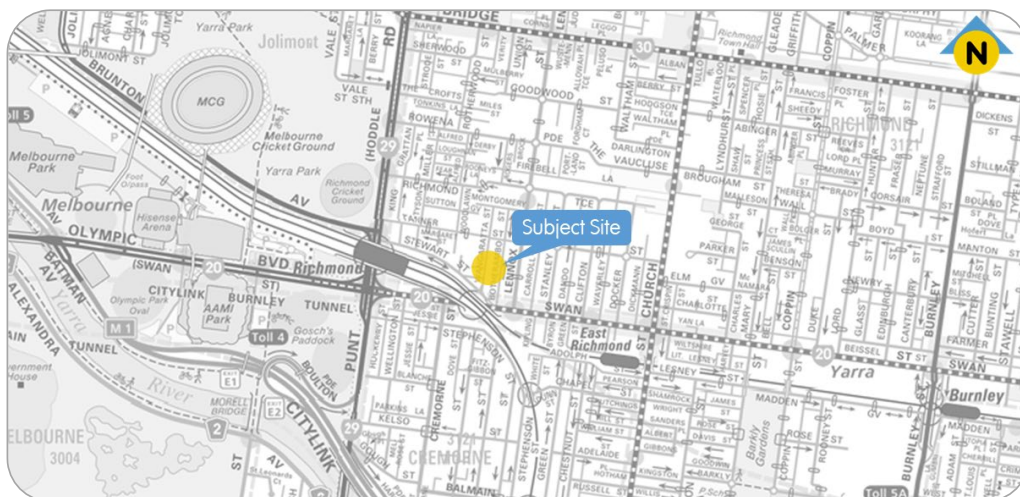


Figure 1 Location of Subject Site





Figure 2 Aerial Photography (Dated 23 February 2019)

The site is rectangular in shape and covers an area of approximately 2,920 square metres. Frontages of approximately 80 metres are provided to both Botherambo Street and Wangaratta Street, forming the eastern and western site boundaries respectively.

Land use in the area is diverse in nature, with mixed-use, neighbourhood residential and commercial uses typically surrounding the subject site.

3.1.2 Swan Street Major Activity Centre

The Swan Street area is an important Major Activity Centre (MAC) in the City of Yarra. It has a unique, strategic position within the metropolitan area offering (amongst other features):

- Unrivalled transport access by all modes of transport;
- Access to major parklands, the city centre, nearby activity centres and the Yarra River; and
- A successful retail core associated with more diverse retail, business and industrial precincts.

3.2 Planning Zone

As illustrated in Figure 3, the subject site is located within the Mixed-use Zone (MUZ).

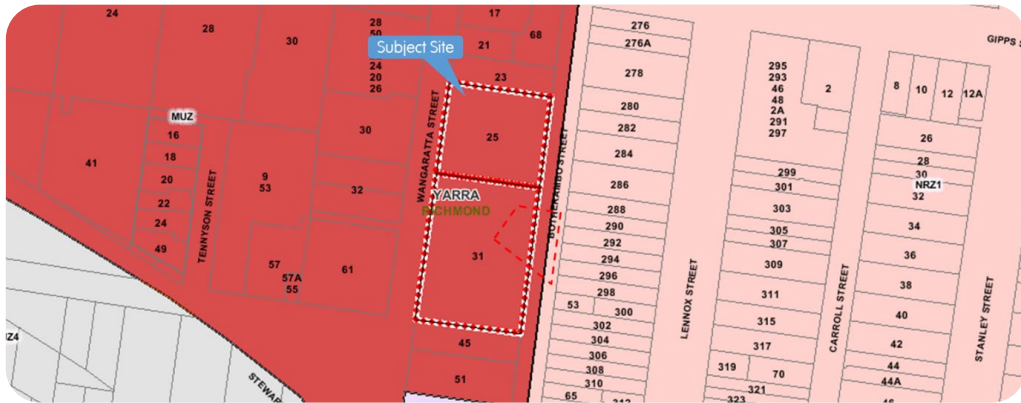


Figure 3 Land Use Planning Zone

The purpose of the MUZ is, in part, to provide for a range of residential, commercial, industrial and other uses which complement the mixed-use function of the locality.

3.3 Road Network

3.3.1 Wangaratta Street

Wangaratta Street is classified as a local access street. Aligned in a north-south direction, Wangaratta Street connects Montgomery Street to the north and Stewart Street to the south.

Wangaratta Street comprises an approximate 6.7 metre road pavement that provides for two-way traffic and kerbside parallel parking along the western side of the carriageway only (between crossovers). Parking spaces are typically subject to either Loading Zone restrictions between 8:00am-6:00pm on Monday-Friday or 2P restrictions between 8:00am-5:00pm on Monday-Friday.

A default speed limit of 50 kilometres per hour applies to Wangaratta Street along the site frontage. Paved footpaths for pedestrian connectivity are provided along both sides of Wangaratta Street. Its typical cross-section is illustrated at Figure 4.



Figure 4 Views of Wangaratta Street facing South adjacent the Subject Site

3.3.2 Botherambo Street

Botherambo Street is classified as a local access street. Aligned in a north-south direction, Botherambo Street connects Montgomery Street to the north and Swan Street to the south.

Botherambo Street comprises an approximate 6.4 metre road pavement that provides for one way traffic in the northbound direction. Kerbside parallel parking is available along the western side of the carriageway, with spaces subject to 4P restrictions between 7:00am-7:00pm on Monday-Friday and public holidays.

A posted speed limit of 40 kilometres per hour applies to Botherambo Street along the site frontage. Its typical cross-section is illustrated in Figure 5.



Figure 5 Views of Botherambo Street facing North adjacent the Subject Site

3.4 Sustainable Transport

3.4.1 Public Transport

The site is well connected to public transport services. This is facilitated primarily via the Richmond Railway Station located 300 metres west of the subject site.

Tram route 70 runs along Swan Street, just south of the subject site. The nearest stop associated with this route is located at the corner of Lennox Street and Swan Street, situated approximately 150 metres walking distance southeast of the subject site.

Figure 6 depicts a map of surrounding public transport options, with relevant services described at Table 1.



Figure 6 Public Transport Map

Table 1 Public Transport Options

Service	Route	Description	Nearest Stop	Walking Distance
Train		Alamein, Belgrave, Cranbourne, Frankston, Glen Waverley, Lilydale, Pakenham and Sandringham lines	Richmond Railway Station	300 metres (4 minutes)
Tram	48	North Balwyn - Victoria Harbour Docklands	Corner Bridge Road and Punt Road	1 kilometre (13 minutes)
	75	Etihad Stadium Docklands - Vermont South		
	70	Waterfront City Docklands - Wattle Park	Corner Lennox Street and Swan Street	150 metres (2 minutes)
	78	North Richmond - Balaclava via Prahran	Corner Swan Street and Church Street	550 metres (8 minutes)
Bus	246	Elsternwick - Clifton Hill via St Kilda	Corner Rowena Parade and Punt Road	550 metres (8 minutes)

In recognition of the sites accessibility via public transport services, the subject site is included within the State Principal Public Transport Network Area as illustrated at Figure 7.

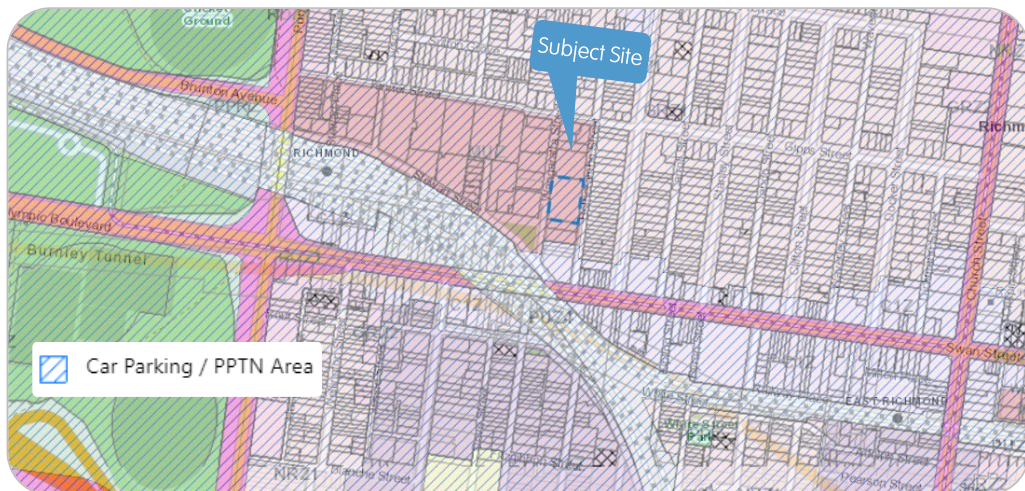


Figure 7 Subject Site location in relation to the surrounding Yarra PPTN

The Principal Public Transport Network reflects the routes where high-quality public transport services are or will be provided.

This network is intended to support integrated transport and land use planning by encouraging more diverse and dense development near high-quality public transport to help support public transport usage.

3.4.2 Cycling

The site is easily accessible via Melbourne's extensive bicycle network, including Council's Strategic Municipal Corridors as illustrated at Figure 8.

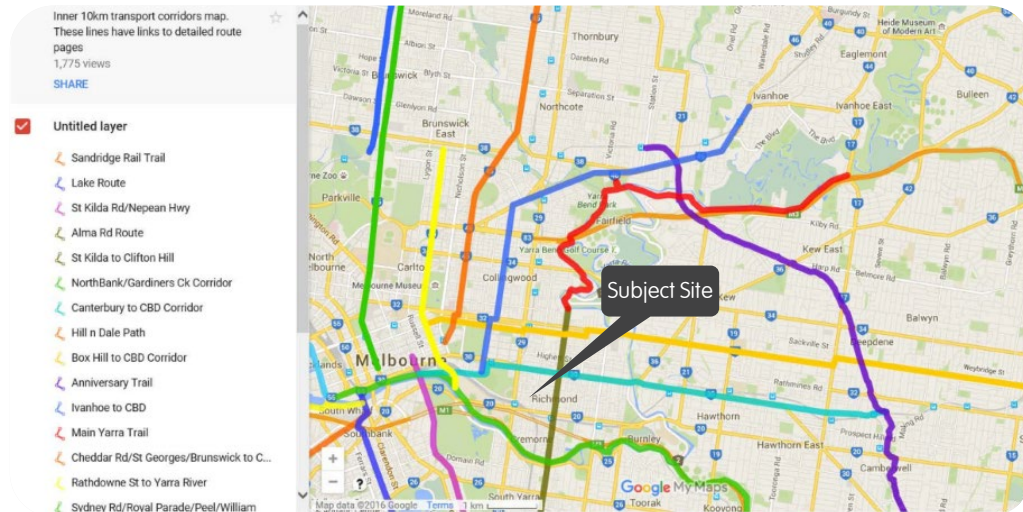


Figure 8 Bicycle Network Plan

These corridors as demonstrated above and in the Strava Heat Map¹ provided at Figure 9 provide cyclists with connections to cycling paths and trails throughout the Metropolitan Melbourne area.



Figure 9 Strava Heatmap

¹ Visualization of public data from users of the Strava application.

3.4.3 Car Share

Car sharing services provide cars on demand, for rent either by the hour or by the day. To use the service, one must register with a car share company, pay a fee and book a car either by phone or on the internet. Cars are picked up and dropped off at designated car share bays.

This service is provided as an ideal choice for people living or working in inner city locations where other modes of transport such as walking, cycling, trams and buses are available. It is also considered a suitable option for people who don't need a car every day or want to get rid of a second car.

The subject site is located within a short walk to car sharing services operated by Flexi Car, GoGet and GreenShare Car as summarised at Table 2.

Table 2 Car Share Availability

Company	Location	Walking Distance
Flexicar	Corner Lennox Street and Gipps Street	170 metres / (2 minutes)
	Corner Lennox Street and Richmond Terrace	350 metres / (5 minutes)
	Corner Swan Street and Church Street	600 metre / (8 minutes)
	Corner Cremorne Street and Gough Street	750 metres / (9 minutes)
GoGet	Corner Lennox Street and Swan Street	210 metres / (3 minutes)
	Corner Lennox Street and Gipps Street	170 metres / (2 minutes)
	Corner Cremorne Street and Jessie Street	300 metres / (4 minutes)
	15 Swan Street (CarePark)	350 metres / (4 minutes)
	Swan Street and Docker Street	450 metres / (6 minutes)
	Tanner Street Car Park	300 metres / (4 minutes)
GreenShareCar	Swan Street and Docker Street	450 metres / (6 minutes)

3.4.4 Bike Share

The cities of Yarra, Melbourne and Port Phillip have joined to trial a dockless electric bike (e-bike) share scheme for one year.

The bike sharer scheme will be operated by Lime, and will comprise bright red and green bikes that are equipped with the latest in GPS technology to ensure Yarra's footpaths remain safe and clear, and bikes are concentrated in areas where they are most in demand

Users will be required to Use the Lime app to find the closest Lime-E Bike.

3.5 Parking Inventory & Demand

IMPACT® commissioned Trans Traffic Survey to undertake 'spot' parking occupancy surveys on **Wednesday 20th March 2019** at 8:00am, 10:00am, 12:00pm and 3:00pm.

The surveyed area has been illustrated in Figure 10 and considered publicly available parking within an approximate 250 metre walking radius of the subject site.

Change of Use:
Retail to Office: 25 - 43 Wangaratta Street, Richmond

Parking Impact Assessment

1 February 2024

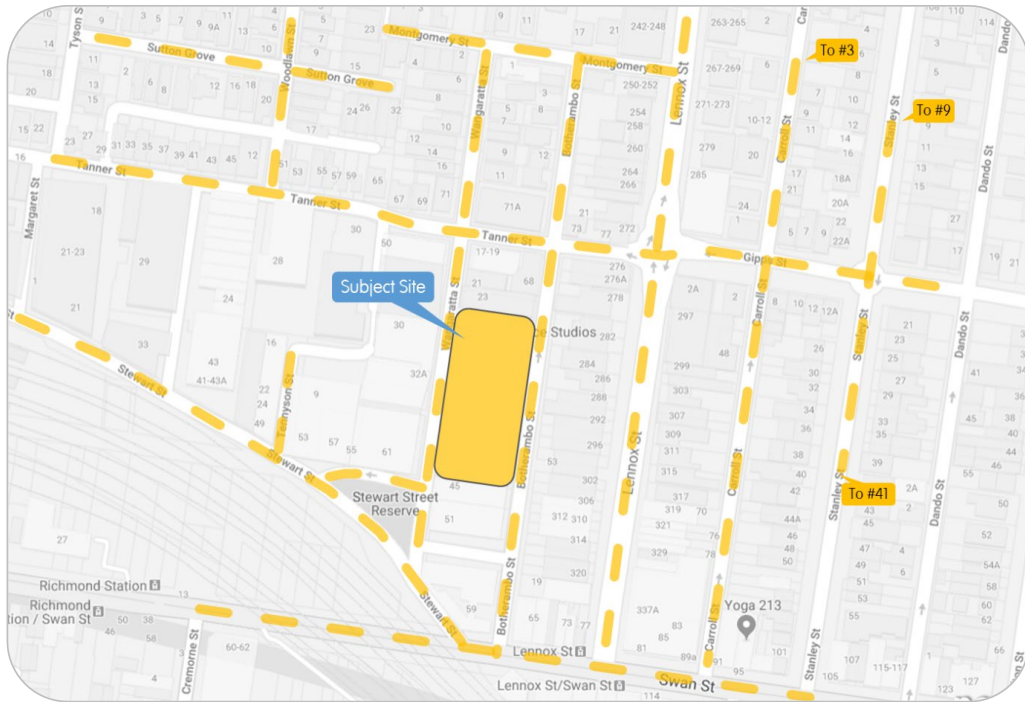


Figure 10 Parking Survey Area

An overall supply of 449 on-street car parking spaces were identified within the study area. Of these spaces, **369** were considered to be available for general public use. It is noted that all of the 369 suitable spaces identified were classified as **short - medium term** parking spaces (being spaces subject to restrictions of 4 hours (inclusive) or less in duration).

The peak parking occupancy was recorded at 12:00pm, when 310 of the 369 publicly available spaces were occupied (representing a ~84% occupancy rate), with 59 spaces remaining vacant at this time.

The car parking occupancy profile across the duration of the survey period is shown in Figure 10.

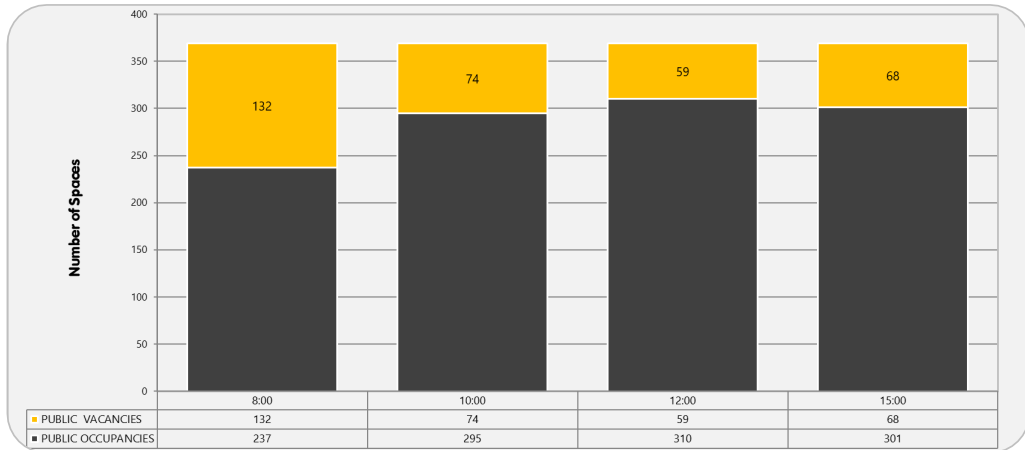


Figure 11 Car Parking Occupancy Profile - Wednesday 20th March 2019

4 Development Proposition

4.1 Use and Yield

The proposal seeks approval to use the existing buildings at 25-29 & Part 31-43 Wangaratta Street, Richmond for the purpose of office use.

These buildings are currently permitted and / or historically been used as warehouse (with ancillary office) and office.

As part of this application, the buildings are to be refurbished which will result in the following development outcome:

Address	Tenancy	Use	Area	Use	Area
		Existing		Proposed	
Building 1: 25-29 Wangaratta Street	Ground Floor - Tenancy 1		413.6		413.6
	Ground Floor - Tenancy 2	Undefined	413.7	Office:	413.7
	First Floor		984		984
Building 2: 31-43 Wangaratta Street	Ground Floor - Tenancy 1	Undefined	356.7		356.7
	Ground Floor - Tenancy 2	Warehouse	444.4	Office:	444.4
	Ground Floor - Tenancy 3	Office	607.3		607.3
	First Floor	Office	1391.5		1391.5
Total Area			4611.2		4611.2

The proposed change in land uses as compared to existing approved uses can be summarised as follows:

Existing office use:	1,998.8 sq.m
Conversion of existing warehouse to office use:	444.4 sq.m
Undefined use to office use:	2,168 sq.m

4.2 Car Parking

Minor reconfiguration of car park bays is planned.

These changes are shown in the plan provided at Appendix B - Plan Number IMP2107010-DG-02-01

These reconfigurations will see that one (1) car parking space is converted to facilitate bicycle parking. Therefore, the site will be serviced by 24 car spaces.

4.3 Bicycle Parking

A total of 31 bicycle spaces are planned, specifically:

- 26 spaces for employees, situated within the building throughout the ground floor, and
- Five (5) spaces for visitors, situated within the building on the ground floor adjacent the northern building entrance from Wangaratta Street.

Bicycle parking spaces are provided in a combination of towel hitches, bicycle hoops and vertical racks.

The location and quantum of bicycle parking is shown in the plan provided at Appendix B - Plan Number IMP2107010-DG-02-01

4.4 Loading Arrangements

Loading and waste arrangements are to be consolidated and provided via the existing Loading Bay Facility within the 31-43 Wangaratta Street Building.

5 Statutory Controls

The relevant traffic and transportation statutory controls are:

Local Policy Framework

- Clause 21.06 - Transport;
- City of Yarra Strategic Transport Statement;
- City of Yarra Council Plan; and
- Swan Street Structure Plan.

Particular Provisions

- Clause 52.06 - Car Parking;
- Clause 52.34 - Bicycle Facilities;
- Clause 65.01 - Approval of an Application or Plan.

5.1 Clause 21.06 - Transport

Clause 21.06 of the Yarra Planning Scheme details the City of Yarra's transport related objectives and strategies. Clause 21.06 acknowledges that the City of Yarra needs to reduce car dependence by promoting walking, cycling and public transport use as viable and preferable alternatives.

Clause 21.06 lists four objectives as follows:

- Objective 30:** To provide safe and convenient pedestrian and bicycle environments;
- Objective 31:** To facilitate public transport usage;
- Objective 32:** To reduce the reliance on the private motor car; and
- Objective 33:** To reduce the impact of traffic.

Notably, Council acknowledges that whilst parking availability is important for many people, in Yarra, unrestricted car use and parking is neither practical nor generally achievable throughout the municipality.

Accordingly, it is Council's intention that car parking be managed to optimise its use and to encourage sustainable transport options.

5.2 City of Yarra Strategic Transport Statement

The Strategic Transport Statement is a document prepared by the City of Yarra in 2006 which addresses the access needs of Yarra's community whilst minimising the impact of cars. The Strategic Transport Statement has a stated vision as follows:

To create a city which is accessible to everyone irrespective of levels of personal mobility and where a fulfilling life can be had without the need for a car

The Strategic Transport Statement lists seven key Strategic Transport Objectives to achieve the vision:

1. Create a city which is a great and safe place to walk and increase the numbers of those walking in Yarra.
2. Create the most bicycle friendly city in Australia and increase the numbers of those cycling in Yarra.
3. Advocate for increased performance of public transport across Melbourne and thereby reduce the number of car trips and through traffic by both Yarra and non-Yarra residents.
4. Ensure that any new road construction is not in conflict with encouraging more sustainable transport use.
5. Ensure Council's response to parking demand is based on Yarra's parking hierarchy and sustainable transport principles.

6. Work to limit freight movement to arterial roads and freeways within Yarra and work to reduce freight movement through Yarra.
7. Encourage Council staff to use more sustainable transport for their travel and increase the capacity of the Council as a whole to respond to and initiate positive actions to further strategic transport objectives 1 to 6.

5.3 City of Yarra Council Plan

The City of Yarra Council Plan (2017 - 2021) proposes several objectives for the municipality, including:

Objective 6: City of Yarra, a place where Connectivity and travel options are environmentally sustainable, integrated and well-designed

To help achieve this objective, Council have listed several strategies that they wish to implement over the next five years, including:

- Investigate and implement effective parking management options
- Develop and promote pedestrian and bicycle infrastructure that encourages alternate modes of transport, improves safety and connectedness.

5.4 Swan Street Structure Plan

The Swan Street Structure Plan was adopted by the City of Yarra in December 2013. The Structure Plan sets out a proposed vision for the future of the activity centre and sets a series of strategic objectives that outline the way the vision will be achieved. With regards to movement and transport, the Swan Street Structure Plan states:

'Fortunately, the compact urban form of the area promotes shorter travel distances and is conducive to active forms of transport. This factor combined with the excellent access to public transport options, strongly positions the study area to achieve more sustainable travel behaviour. However, strengthening the sustainable transport network will require a significant shift from vehicular based travel to sustainable forms of transport.'

5.5 Clause 52.06 - Car Parking

5.5.1 Purpose

The purpose of Clause 52.06 is:

- To ensure that car parking is provided in accordance with the Municipal Planning Strategy and the Planning Policy Framework.
- To ensure the provision of an appropriate number of car parking spaces having regard to the demand likely to be generated, the activities on the land and the nature of the locality.
- To support sustainable transport alternatives to the motor car.
- To promote the efficient use of car parking spaces through the consolidation of car parking facilities.
- To ensure that car parking does not adversely affect the amenity of the locality.
- To ensure that the design and location of car parking is of a high standard, creates a safe environment for users and enables easy and efficient use.

5.5.2 Provision Requirements

To satisfy the above purpose, Clause 52.06 of the Yarra Planning Scheme specifies requirements relating to the provision and design of car parking as follows.

5.5.3 Car Parking Provision Requirements - Clause 52.06-5

Table 1 to Clause 52.06-05 of the Yarra Planning Scheme provides rates for various land uses. The site sits within the PPTN area and hence Column B rates apply.

The following rates are therefore applicable to the various uses contemplated on the site:

Office	3.0 spaces to every 100 sqm leasable floor area
Warehouse	2 spaces to each premises plus 1 space to every 100 sq.m net floor area

The proposal contemplated the following outcome

Existing office use:	1,998.8 sq.m
Conversion of existing warehouse to office use:	444.4 sq.m
Undefined use to office use:	2,168 sq.m

Calculation of the parking implications of the proposal reveals a net parking requirement for 72 spaces.

Use	Area	Statutory Requirement	Use	Area	Statutory Requirement	Net Difference
Existing			Proposal			
Existing Office	1,998.8	59	Office	1,998.8	59	0
Warehouse	444.4	6	Office	444.4	13	7
Undefined	2,168	N/A	Office	2,168	65	65
Total						72

Note:

The above calculation of the parking requirement (net difference) for the undefined areas results in a conservatively high outcome, noting the undefined areas have historically operated for a range of uses including Warehousing, which attracts a statutory requirement

5.5.4 Proposed Provision

A provision of 24 car spaces is contemplated for the proposal.

The proposal therefore seeks approval to reduce the number of parking spaces required under Clause 52.06-5.

5.5.5 Application Requirements and Decision Guidelines to Reduce Car Parking Requirement

An application to reduce (including reduce to zero) the number of car parking spaces required under Clause 52.06-5 must be accompanied by a Car Parking Demand Assessment.

The Car Parking Demand Assessment must assess the car parking demand likely to be generated by the proposal and must also address the following matters, to the satisfaction of the responsible authority.

- The likelihood of multi-purpose trips within the locality which are likely to be combined with a trip to the land in connection with the proposed use.
- The variation of car parking demand likely to be generated by the proposed use over time.
- The short-stay and long-stay car parking demand likely to be generated by the proposed use.
- The availability of public transport in the locality of the land.
- The convenience of pedestrian and cyclist access to the land.
- The provision of bicycle parking and end of trip facilities for cyclists in the locality of the land.
- The anticipated car ownership rates of likely or proposed visitors to or occupants (residents or employees) of the land.
- Any empirical assessment or case study.

Before granting a permit to reduce the number of spaces, the responsible authority must consider the following, as appropriate:

- The Car Parking Demand Assessment.
- Any relevant local planning policy or incorporated plans.
- The availability of alternative car parking in the locality of the land, including:
 - Efficiencies gained from the consolidation of shared car parking spaces.
 - Public car parks intended to serve the land.
 - On-street parking in non-residential zones.
 - Streets in residential zones specifically managed for non-residential parking.
- On-street parking in residential zones in the locality of the land that is intended to be for residential use.
- The practicality of providing car parking on the site, particularly for lots of less than 300 square metres.
- Any adverse economic impact a shortfall of parking may have on the economic viability of any nearby activity centre.
- The future growth and development of any nearby activity centre
- Any car parking deficiency associated with the existing use of the land.
- Any credit that should be allowed for car parking spaces provided on common land or by a Special Charge Scheme or cash-in-lieu payment.
- Local traffic management in the locality of the land.
- The impact of fewer car parking spaces on local amenity, including pedestrian amenity and the amenity of nearby residential areas.
- The need to create safe, functional and attractive parking areas.
- Access to or provision of alternative transport modes to and from the land.
- The equity of reducing the car parking requirement having regard to any historic contributions by existing businesses.

- The character of the surrounding area and whether reducing the car parking provision would result in a quality/positive urban design outcome.
- Any other matter specified in a schedule to the Parking Overlay.
- Any other relevant consideration.

Considering the foregoing, we have undertaken the following car parking demand assessment which outlines our expectations of likely parking demand for the office and retail uses.

5.5.6 Car Park Demand Assessment

5.5.6.1 Preamble

The 'business as usual' approach to parking demand assessments is typically informed by a 'predict and provide' methodology which leverages current behaviour to inform future considerations.

This approach is characterised by mandatory minimum parking requirements, with each development (land use) expected to provide its own parking. The intent being that motorists should nearly always be able to easily find convenient, free parking at every destination.

Under this 'predict and provide' approach, parking planning is based on the premise that the 'parking problem' means 'inadequate supply', and consequently:

1. More parking is better.
2. Every destination should satisfy its own parking needs (minimum ratios).
3. Car parks should never fill.
4. Parking should always be free or subsidised or incorporated into building costs.

However, there is an increasing trend towards more efficient use of existing transport infrastructure as an alternative to expanding roads and parking facilities, incorporated in a technique known as travel demand management (TDM).

TDM emphasises the movement of people and goods, rather than motor vehicles, and gives priority to more efficient travel and communication modes (such as walking, cycling, car sharing, public transport and telecommuting), particularly under congested conditions².

5.5.6.2 Application of TDM in Land Use Applications

It has been set out³ that the 'business as usual' approach needs to be reconsidered especially within Activity Centres and locations that are well served by a range of public transport services, where cycling and walking is a practical alternative to car travel, and where local policy advocates for car parking to be managed to optimise its use and encourage sustainable transport options.

Ronge v Moreland CC [2017] VCAT 550 & Vincent Corporation Pty Ltd v Moreland CC (Includes Summary) (Red Dot) [2015] VCAT 2049 are two VCAT Decision of Significance that signal this change in the consideration of parking provision.

The key highlights of these decisions as they relate to parking are reproduced below:

The Ronge v Moreland CC [2017] VCAT 550

This decision was classified as a 'Red Dot' decision i.e. a decision of interest or significance, on the basis of:

Policy - interpretation or application of policy:

² Austroads Guide to Traffic Management Part 11: Parking

³ Ronge v Moreland CC [2017] VCAT 550) & Vincent Corporation Pty Ltd v Moreland CC (Includes Summary) (Red Dot) [2015] VCAT 2049

- Plan Melbourne 2017-2050 and State and local policy concerning car parking, traffic and shift to other travel modes.

Practice or Procedure - consideration of individual instance or systemic issues

- Necessity to reduce reliance on car based travel

Analysis - exposition of how to assess an issue or matters to consider

- Car parking and traffic

Specifically, the tribunal summarises the key matters as follows:

The recently released Plan Melbourne 2017-2050 is predicting that Melbourne's population will increase from approximately 5 million to 8 million people over the next 33 years. An additional 1.6 million dwellings will be required to house the extra population.

State and local planning policies are already acknowledging the change that is required in the way in which people travel with Plan Melbourne 2017-2050 and State policies referring to 20-minute neighbourhoods and greater reliance on walking and cycling. At the municipal level, Moreland has long been recognised as being at the forefront of encouraging less reliance on car based transport. For example, the Moreland Integrated Transport Strategy 2010 includes a key principle that walking and cycling are the preferred modes of transport.

Our roads are already congested and will be unimaginably so if a 'business-as usual' approach is accepted through until 2050. The stark reality is that the way people move around Melbourne will have to radically change, particularly in suburbs so well served by different modes of public transport and where cycling and walking are practical alternatives to car based travel.

A car parking demand assessment is called for by Clause 52.06-6 when there is an intention to provide less car parking than that required by Clause 52.06-5. However, discussion around existing patterns of car parking is considered to be of marginal value given the strong policy imperatives about relying less on motor vehicles and more on public transport, walking and cycling. Census data from 2011 or 2016 is simply a snapshot in time, a base point, but such data should not be given much weight in determining what number of car spaces should be provided in future, for dwellings with different bedroom numbers.

Policy tells us the future must be different. Oversupplying parking, whether or not to comply with Clause 52.06, has the real potential to undermine the encouragement being given to reduce car based travel in favour of public transport, walking and cycling.

One of the significant benefits of providing less car parking is a lower volume of vehicle movements and hence a reduced increase in traffic movements on the road network.

Vincent Corporation Pty Ltd v Moreland CC (Includes Summary) (Red Dot) [2015] VCAT 2049

This decision was classified as a 'Red Dot' decision i.e. a decision of interest or significance, on the basis of:

Nature of Case:

- A reduction of on-site car parking to zero in major activity centre.

Practice or Procedure - consideration of individual instance or systemic issues

- Considers the relevant tests for reducing on-site car parking in accordance with Clause 52.06 in conjunction with the State and local planning policy framework, particularly for sites in inner city activity centres. Summarises principles applied by the Tribunal in applications under Clause 52.06 to reduce on-site car parking provision

Specifically, the tribunal summarises the key matters as follows:

The decision discusses current policies for car parking and the consideration of both demand and supply management as sought by State policy for integrated transport and the application of this in the assessment of car parking in inner city activity centres such as Brunswick. The decision also reviews a number of other similar recent cases of the Tribunal, deriving some common themes in the consideration of reducing car parking rates in larger, particularly inner city, activity centres. These are:

- a. Parking across an activity centre needs to be addressed in a centre wide manner.

- b. Providing or restricting parking through the planning scheme, only forms one part of the overall management of car parking demand and supply in these centres.
- c. There are a number of alternative means of travel emerging. Most notable of these are share car arrangements and increased bicycle use, including electric bicycles. These have grown extensively in use in the past 10 years. Their impact on empirical demand is not yet known. Policies to minimise car dependency suggest it should lead to less demand.
- d. In areas of existing restricted on-street parking, the supply of on-street spaces for future occupants of a proposal is not particularly relevant to the assessment. In these locations, on-street road management restrictions can ensure future residents of a proposal are not able to rely on any spare capacity in on-street parking.
- e. While a current empirical demand may exist not every site can, or needs to, meet this demand. Some sites may have limited capacity to meet car parking needs, particularly small or constrained sites.
- f. A proposal for a reduced resident parking provision needs to consider:
 - i. Access to public transport, in timing, diversity of routes and frequency.
 - ii. Walkable access from the site to shops and facilities that provide for the daily needs of residents.
 - iii. Access to other transport options including good bicycle infrastructure and walkable access to publicly available shared use cars.
- g. As with any proposal, a decision to reduce parking ultimately must be tested on its own merits with competing issues balanced to achieve net community benefit. Any potential adverse impacts from parking on-street will need to be assessed against the benefits a proposal may bring to the community where car parking forms only one part of a use or development proposal.

These decisions of significance have influenced the assessment of parking provision for various land use proposals and informed decision making by Responsible Authorities and the Tribunal.

5.5.6.3 Empirical Assessment

Office

The statutory parking provision rate of 3.0 spaces per 100 sq.m typically aligns with parking demand in unconstrained scenarios in suburban and outer suburban areas.

In inner city locations, demand rates lower than 3.0 spaces per 100 sq.m are common place. Case studies undertaken by **IMPACT**[®] over a 5 day period at an 11,407 sq.m office development at 40 River Boulevard, Richmond revealed that the office had 372 spaces on site, and was generating a demand for up to 199 spaces, which equates to a rate of 1.74 spaces per 100 sq.m.

We note that whilst the case study site is located in an inner city location and with reasonable access to public transport via light rail services, the case study site does not have direct access to heavy rail.

Accordingly, in a location like the subject site, lower rates would be expected due to higher levels of access and connectivity to high quality public transport services.

5.5.7 Responsible Authority Considerations

The 'business as usual' approach to parking demand assessments is typically informed by a 'predict and provide' methodology which leverages current behaviour to inform future considerations.

Whilst this approach has its merits, especially in suburban locations, it has been argued⁴ that the 'business as usual' approach needs to be reconsidered especially in locations that are well served by a range of heavy and light rail services, where cycling and walking is a practical alternative to car travel, and where local policy advocates for car parking to be managed to optimise its use and encourage sustainable transport options.

In this regard, the proposed development seeks to provide car parking at a rate below the statutory and empirical rate. Specifically, 25 spaces are planned which for the overall development equates to a rate of approximately 0.52 spaces per 100 sq.m.

⁴ Ronge v Moreland CC [2017] VCAT 550

Noting the character of the surrounding area, this provision is considered satisfactory for the following reasons:

- The subject site is easily accessible via sustainable transport options such as heavy rail, light rail, and active transport such as cycling. These diverse modes will provide staff and visitors alternate and viable options to the motor vehicle.
- On street car parking in the locality of the subject site is constrained, specifically, parking is restricted to short to medium term parking. These restrictions preclude the use of on street car parking for office staff, as they typically seek all day (long term) parking opportunities. The constrained on-street parking conditions would therefore serve as a disincentive for employees to drive to work.

Due to the constrained nature of on-street parking in the surrounding area, the limited availability / access to off-street commercial car parking and the accessibility to public transport services, we expect that parking demand will be constrained, with employees taking up sustainable transport modes to travel to and from work.

This outcome would also be assisted by the implementation of a Green Travel Plan, a copy of which is included as Appendix A.

Furthermore, we note that a number of office developments that share similar attributes to the proposed development have been approved by Council and VCAT with parking provision at rates below statutory and empirical rates.

These sites are summarised in Table 3.

Table 3 Recently Approved Office Developments

Development	Yield / Use	Walking Distance to Train	Walking Distance to Tram	Supported Car Parking Rate
484-486 Swan St, Richmond	18,595sq.m office/retail	150 metres	150 metres	0.82 spaces per 100 sq.m
2-16 Northumberland St, Collingwood	15,473sq.m office /shop	700 metres	250 metres	0.88 spaces per 100 sq.m
60-88 Cremorne Street, Cremorne	27,653 sq.m Office / retail	400 metres	300 metres	0.72 spaces per 100 sq.m
51 Langridge St, Collingwood	3,335 sq.m Office	400 metres	300 metres	0.54 spaces per 100 sq.m
480 Swan Street, Richmond	33,678 sq.m Office / retail	50 metres	50 metres	0.69 spaces per 100 sq.m

The proposal provides parking at a rate of approximately 0.52 spaces per 100 sq.m.

This provision whilst below the statutory and empirical rates, recognises the sustainable transport merits of the site location, and furthermore provides an outcome that maintains the momentum in this locality of prioritising sustainable transport outcomes in lieu of private motor vehicle travel as prescribed in local policy objectives.



5.5.8 Conclusion - Car Parking Provision

Based on the foregoing considerations, we consider that the development proposition satisfies the purpose of Clause 52.06, specifically:

- To ensure that the car parking does not adversely affect the amenity of the locality.
- To ensure the provision of an appropriate number of car parking spaces having regard to the demand likely to be generated, the activities on the land and the nature of the locality.
- To support sustainable transport alternatives to the motor car.

Note:

To support and nature a sustainable transport culture at this site, the applicant has committed to implementing a green travel plan aimed at managing car dependency and encouraging the use of sustainable transport modes, with the objective of permanently changing travel mode behaviour.

The Green Travel Plan is provided at Appendix A.

5.6 Clause 52.34 - Bicycle Facilities

5.6.1 Purpose

The purpose of Clause 52.34 is to encourage cycling as a mode of transport, and provide secure, accessible and convenient bicycle parking spaces and associated shower and change facilities.

5.6.2 Provision Requirements - Clause 52.34.3

To satisfy the above purpose, Clause 52.34-3 of the Yarra Planning Scheme specifies the bicycle parking provision requirements for a variety of different uses within Table 1.

Rates applicable to the proposed uses are:

Office	Employees:	1 space to each 300 sqm floor area
	Visitors:	1 space to each 1,000 sqm floor area

There are no bicycle parking requirements for warehouse uses.

Application of the proposed changes reveals a requirement for 10 additional bicycle parking spaces as following.

Use	Area	Statutory Requirement	Use	Area	Statutory Requirement	Net Difference
Existing			Proposal			
Warehouse	444	0	Office	444	1	1
Undefined	2,168	N/A	Office	2,168	9	9
Total						10

The foregoing assessment reveals that the proposed change of use will result in a requirement for an additional 10 bicycle parking spaces.

Note:

The above calculation of the parking requirement net difference for the undefined areas results in a conservatively high outcome, noting the undefined areas have historically operated for a range of uses, which attract a statutory requirements.

5.6.3 Proposed Provision

A total of 31 bicycle spaces are planned, comprised of 26 spaces for employees and five (5) spaces for visitors.

5.6.4 Adequacy of Proposed Provision

The proposed provision comfortably exceeds the statutory requirement for bicycle parking and will contribute positively to encouraging cycling as a mode of transport to the site.

5.6.5 Design Requirements

Clause 52.34-6 of the Planning Scheme states:

Bicycle spaces should:

- Provide a space for a bicycle of minimum dimensions of 1.7 metres in length, 1.2 metres in height and 0.7 metres in width at the handlebars.

- Be located to allow a bicycle to be ridden to within 30 metres of the bicycle parking space.
- Be located to provide convenient access from surrounding bicycle routes and main building entrances.
- Not interfere with reasonable access to doorways, loading areas, access covers, furniture, services and infrastructure.
- Not cause a hazard.
- Be adequately lit during periods of use.

5.6.5.1 Proposed Design

The bicycle parking design has been assessed against the requirements of Clause 52.34 of the Yarra Planning Scheme, the Australian Standard for Bicycle parking facilities (AS 2890.3:2015) and the manufacturer's specifications.

Details of the nominated product are provided as Appendix C.

5.6.6 Conclusion - Bicycle Parking

We can conclude that bicycle parking provided as part of this development satisfies the purpose of Clause 52.34, specifically:

- To encourage cycling as a mode of transport, and provide secure, accessible and convenient bicycle parking spaces.

5.7 Clause 65.01 - Approval of an Application or Plan

5.7.1 Loading Requirements and Objectives

To address the adequacy of loading for new developments, the Yarra Planning Scheme specifies the following:

- The responsible authority must consider, as appropriate, the adequacy of loading and unloading facilities and any associated amenity, traffic flow and road safety impacts.

5.7.2 Adequacy of Proposed Loading Facilities

In response to the above, we note that:

- Loading for the office use will be undertaken infrequently, typically associated with the occupation and vacation of the tenancies.

The development plans do not show any dedicated loading bay on-site (with the bay shown intended for waste collection purposes only), however we note that as a result of the proposal (i.e. the removal of redundant crossovers) a number of on-street spaces will be created. It is intended to utilise one (1) of these on-street spaces as an informal loading bay for the cafe tenancy (and occasionally the office as required).

5.7.3 Waste Collection

It is proposed to collect waste on-site, by way of private services, using a 6.4 metre waste collection mini rear loader.

Swept paths attached as Appendix B demonstrate the nominated collection vehicle will reverse into the subject site, undertake waste collection and then exit the site in a forward direction.

We are satisfied that there is sufficient room provided to cater for the proposed waste collection vehicle.

Change of Use:
Retail to Office: 25 - 43 Wangaratta Street, Richmond

Parking Impact Assessment

1 February 2024



APPENDIX A
Green Travel Plan

—

37°49'26"S
144°59'34"E

Change of Use: Retail to Office: 25 - 43 Wangaratta Street, Richmond



Green Travel Plan

1 February 2024
Prepared for Cremorne Properties Pty Ltd

IMP2107010GTP01F06

Impact

Change of Use:
Retail to Office: 25 - 43 Wangaratta Street, Richmond

Green Travel Plan

1 February 2024

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Document Information

Client Cremorne Properties Pty Ltd
Report Title Change of Use:
Retail to Office: 25 - 43
Wangaratta Street, Richmond
Report Reference IMP2107010GTP01F06
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Approved By John-Paul Maina

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1 Introduction

1.1 Engagement

IMPACT[®] have been engaged by Cremorne Properties Pty Ltd to prepare a Green Travel Plan (GTP) to supplement a parking impact assessment in support of the proposal to use the existing building at 25-43 Wangaratta Street, Richmond for the purpose of office use

1.2 Scope of Engagement

This GTP has been prepared to accompany a town planning submission. In preparing this GTP we have referenced the following:

- As Built and Proposed Change of Use Plans;
- Australian Bureau of Statistics Census Data (2016); and
- Other web-based sources as referenced in the body of this report.

2 Development Proposition

2.1 Use and Yield

The proposal seeks approval to use the existing buildings at 25-29 & Part 31-43 Wangaratta Street, Richmond for the purpose of office use. These buildings are currently permitted and / or historically been used as warehouse (with ancillary office) and office

As part of this application, the buildings are to be refurbished which will result in the following development outcome:

Address	Tenancy	Use	Area	Use	Area
		Existing			
Building 1: 25-29 Wangaratta Street	Ground Floor - Tenancy 1		413.6		413.6
	Ground Floor - Tenancy 2	Undefined	413.7	Office:	413.7
	First Floor		984		984
Building 2: 31-43 Wangaratta Street	Ground Floor - Tenancy 1	Undefined	356.7		356.7
	Ground Floor - Tenancy 2	Warehouse	444.4	Office:	444.4
	Ground Floor - Tenancy 3	Office	607.3		607.3
	First Floor	Office	1391.5		1391.5
Total Area			4611.2		4611.2

2.2 Car Parking

Minor reconfiguration of car park bays is planned.

These reconfigurations will see that one (1) car space is converted in bicycle parking spaces to service the development. Therefore, a provision of 24 spaces is planned for the development.

2.3 Bicycle Parking

A total of 31 bicycle spaces are planned, specifically:

- 26 spaces for employees, situated within the building throughout the ground floor, and
- Five (5) spaces for visitors, situated within the building on the ground floor accessed via the building access point from Wangaratta Street.

3 Sustainable Transport Planning

3.1 Overview

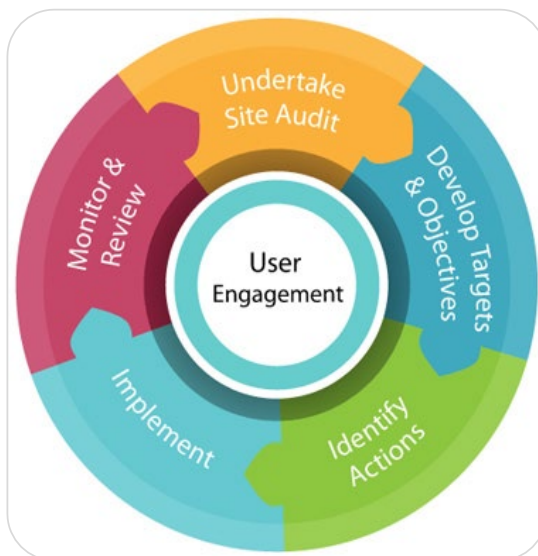
A Green Travel Plan is one of a suite of tools available to assist with Sustainable Transport Planning.

A Green Travel Plan can be defined as a package of site-specific measures implemented to promote and maximise the use of more sustainable modes of travel.

Typically, green travel plans support walking, cycling, public transport and car sharing, which are encouraged via a range of actions, promotional campaigns and incentives, and can deliver a range of benefits to employers, employees, visitors of a development by:

- Reducing the need to provide parking (reducing costs associated with providing parking and helping to create more affordable development outcomes);
- Contribute to corporate social responsibility relating to the triple bottom line, and improve corporate image as an innovative and environmentally-aware organisation;
- Help to appeal to a new generation of professionals who prioritise location and lifestyle over car ownership;
- Help to attract and retain staff (reducing costs associated with staff turnover);
- Contribute to a healthier, happier and more active workplace (reducing costs associated with sick days and an unhealthy, unproductive workforce);
- Create opportunities for healthier lifestyles and more vibrant, cohesive and accessible communities;
- Provide staff and residents with potential travel cost savings;

In preparing a Green Travel Plan, the plan should have regard to the following essential elements¹:



- Site Audit and Data Collection** To understand Base Conditions;
- Objectives and targets** To define the direction and purpose of the travel plan. Targets should be specific, measurable, achievable and time-bound;
- Actions** To help achieve the objectives. Should provide incentives for using sustainable transport modes;
- Promoting and Marketing** Communication strategy to get the message to the target audience;
- Commitment of resources** This would including financial support and human resources to allow for implementation, monitoring, review and continual improvement of the travel plan;
- Monitoring and Review process** Aimed at setting out a systematic approach to measuring the impact of the travel plan;

Governance support Includes appointment of a Travel Plan Coordinator or Committee.

¹ City of Sydney & UK Department for Transport

4 Site-Specific Transport Assessment

4.1 Location

The subject site is located on the eastern side of Wangaratta Street, Richmond as shown in the locality plan and aerial photograph represented in Figure 1. The site falls within the Richmond Station Precinct within The Swan Street Major Activity Centre area.

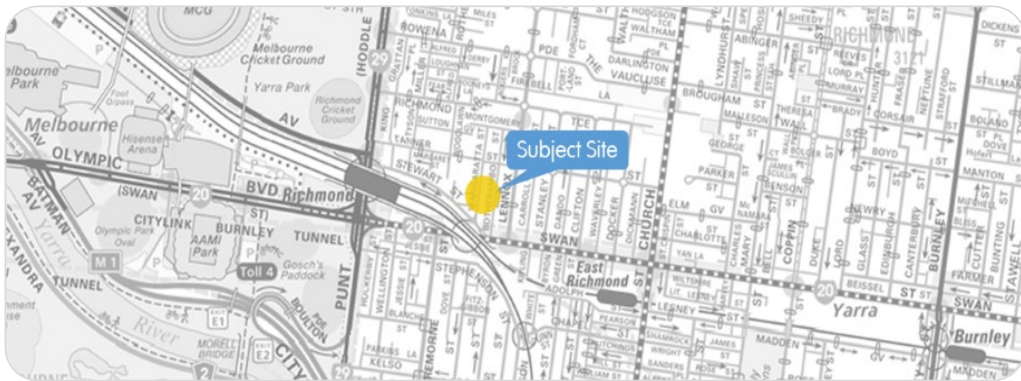


Figure 1 Location of Subject Site

4.2 Public Transport

The site is well connected to public transport services. This is facilitated primarily via the Richmond Railway Station located 300 metres west of the subject site.

Tram route 70 runs along Swan Street, just south of the subject site. The nearest stop associated with this route is located at the corner of Lennox Street and Swan Street, situated approximately 150 metres walking distance southeast of the subject site.

Figure 2 depicts a map of surrounding public transport options, with relevant services described at Table 1.

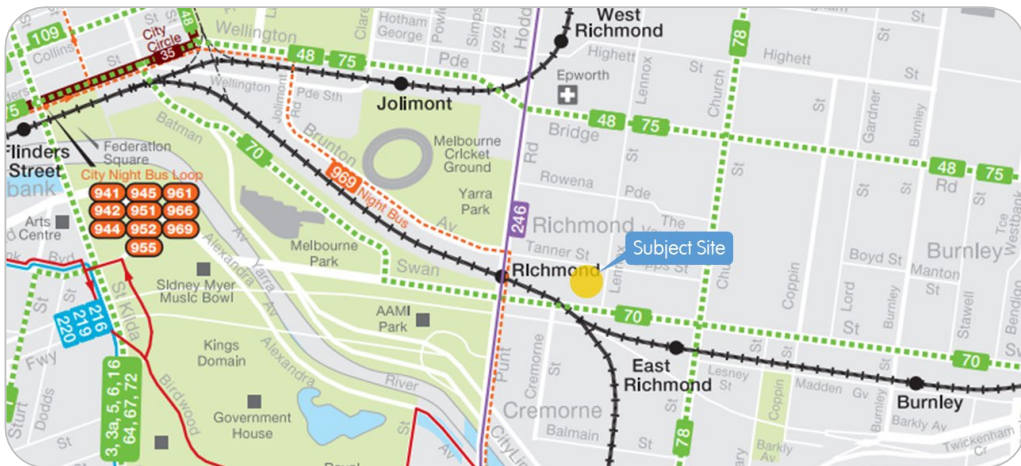


Figure 2 Public Transport Map

Table 1 Public Transport Options

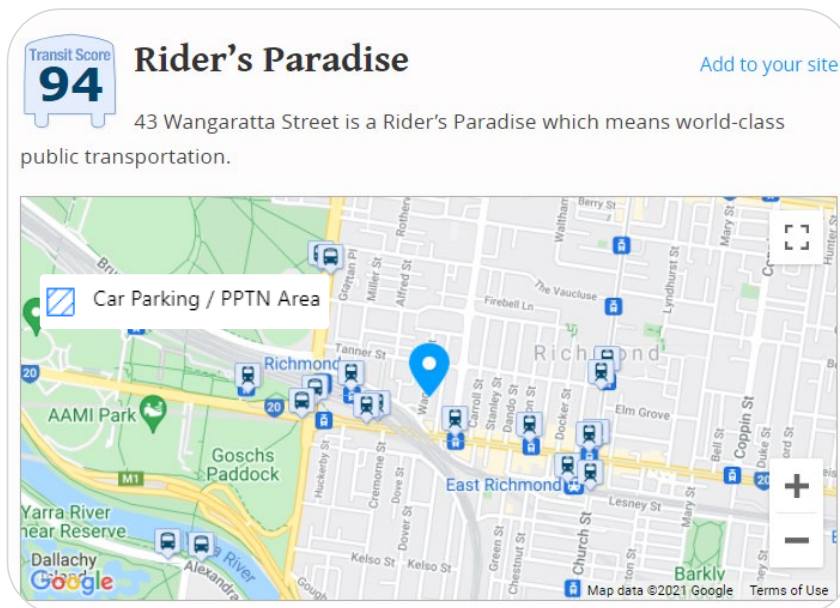
Service	Route	Description	Nearest Stop	Walking Distance
Train		Alamein, Belgrave, Cranbourne, Frankston, Glen Waverley, Lilydale, Pakenham and Sandringham lines	Richmond Railway Station	300 metres (4 minutes)
Tram	48	North Balwyn - Victoria Harbour Docklands	Corner Bridge Road and Punt Road	1 kilometre (13 minutes)
	75	Etihad Stadium Docklands - Vermont South		
	70	Waterfront City Docklands - Wattle Park	Corner Lennox Street and Swan Street	150 metres (2 minutes)
	78	North Richmond - Balaclava via Prahran	Corner Swan Street and Church Street	550 metres (8 minutes)
Bus	246	Elsternwick - Clifton Hill via St Kilda	Corner Rowena Parade and Punt Road	550 metres (8 minutes)

In recognition of the site's accessibility via public transport services, the subject site is included within the State Principal Public Transport Network Area.

The Principal Public Transport Network reflects the routes where high-quality public transport services are or will be provided.

This network is intended to support integrated transport and land use planning by encouraging more diverse and dense development near high-quality public transport to help support public transport usage.

As illustrated in Figure 3, the site has a Transit Score of 94 - Riders Paradise. This means that the site has access to World Class Public Transport².



As demonstrated in the catchment plan at Figure 4, these public transport services can be leveraged to gain access to a significant area of Metropolitan Melbourne (within 30 minutes).

² Walkscore: <https://www.walkscore.com/score/43-wangaratta-st-richmond-vic-australia>

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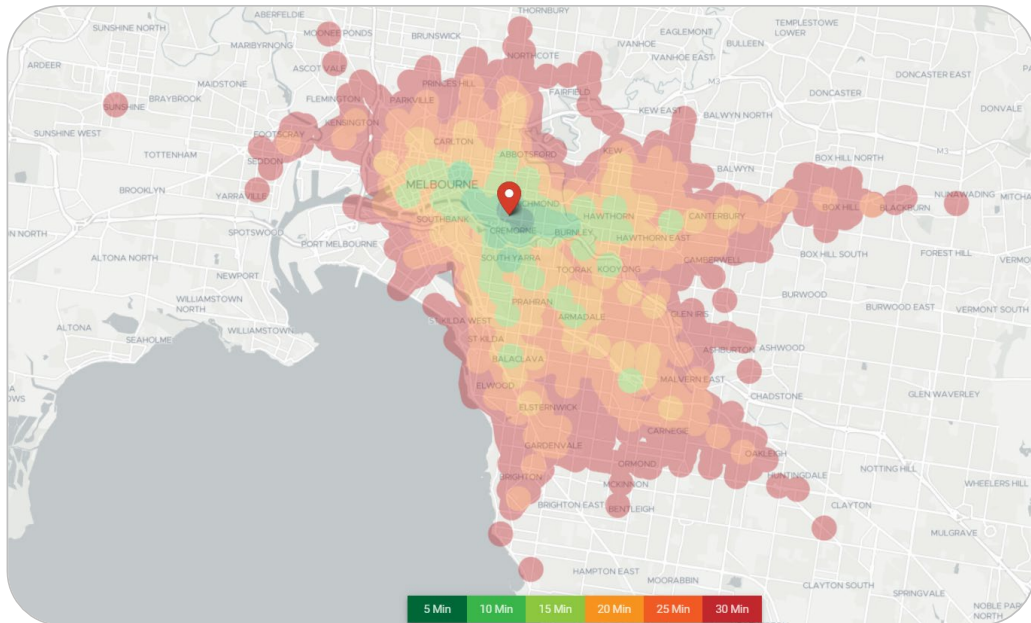


Figure 4 30-Minute Cycling Catchment

The catchment plan³ shows that within 30 minutes, a commuter can travel from the site to:

- North Thornbury / Moonee Ponds
- South Hampton East / Clayton
- West Yarraville / Seddon
- East Nunawading

4.3 Cycling

Cycling Corridors as illustrated at Figure 5.

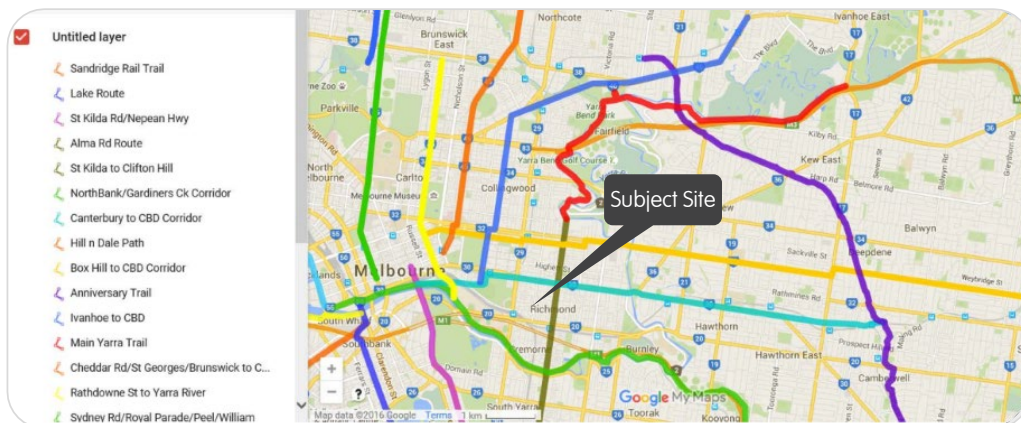


Figure 5 Bicycle Network Plan

³ <https://www.route360.net/>

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These corridors as demonstrated above and in the Strava Heat Map⁴ provided at Figure 6 provide cyclists with connections to cycling paths and trails throughout the Metropolitan Melbourne area.



Figure 6 Strava Heatmap

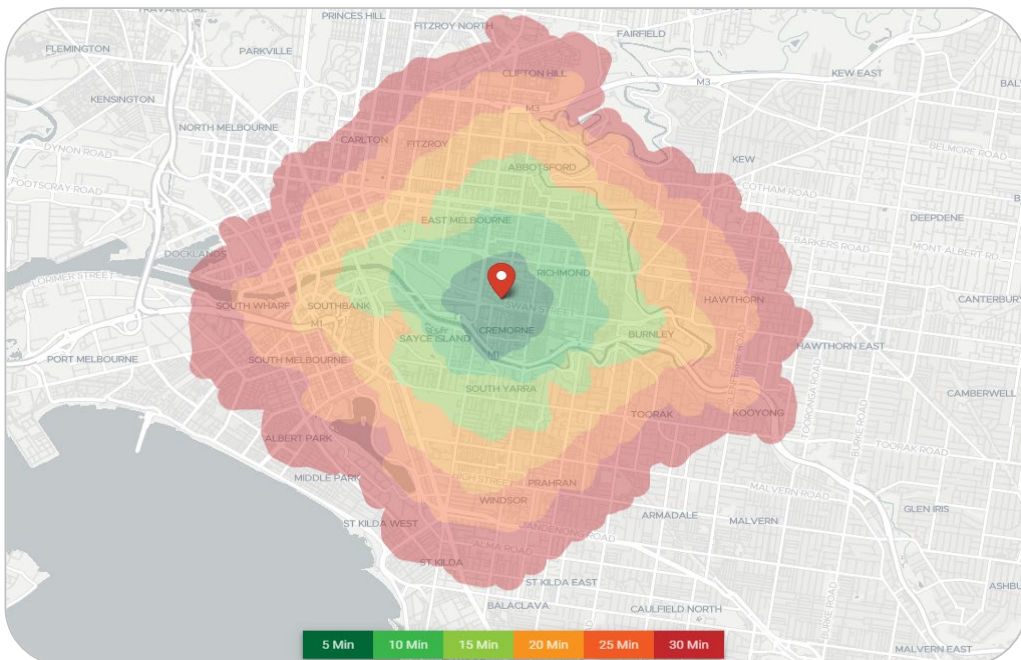


Figure 7 30-Minute Cycling Catchment

The catchment plan⁵ shown in Figure 7 indicates that within 30 minutes, a cyclist can travel from the site to:

- North Fitzroy North
- South St Kilda East
- West Docklands
- East Hawthorn East

⁴ Visualization of public data from users of the Strava application.

⁵ <https://www.route360.net/>

4.4 Walking

Adequate pedestrian access to the site is provided in the form of sealed pedestrian footpaths along Botherambo Street, Wangaratta Street and surrounding roads.

For further context, a Walk Score has been derived that indicates the site is both a 'Walker's Paradise' (i.e. daily errands do not require a car) and 'Rider's Paradise' (i.e. offers world-class public transportation options) with scores of 98 and 96 out of 100 achieved for both categories, respectively.

This is reflective of the site's proximity to everyday needs such as public transport, cafes/restaurants and other day-to-day activities.

This network of footpaths and trails provide an extensive 30-minute walking catchment to the site, as demonstrated in Figure 8.

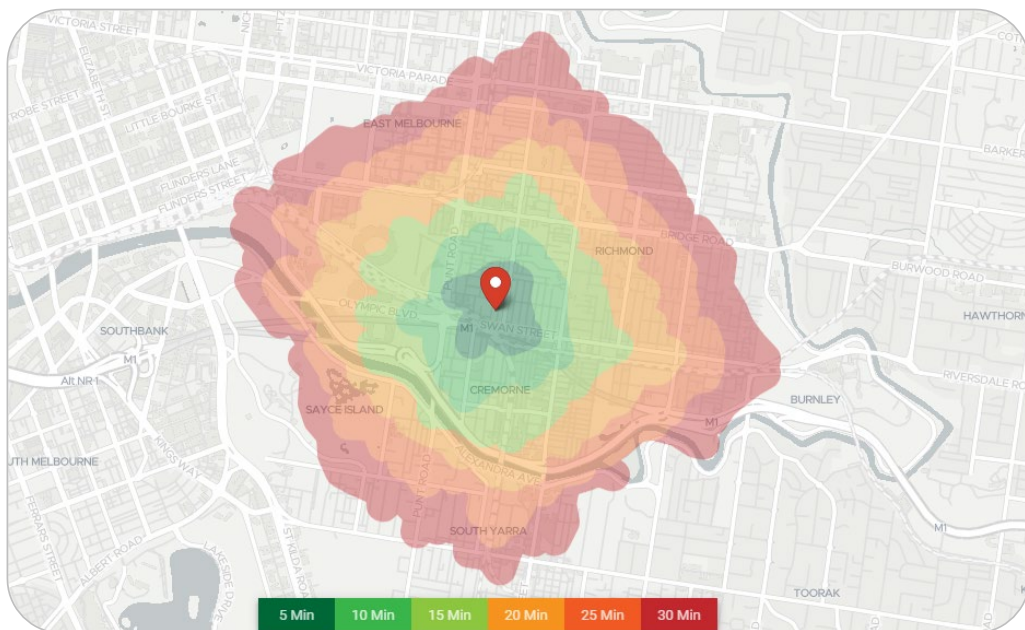


Figure 8 30-Minute Walking Catchment

The catchment plan shown above indicates that within 30 minutes, a pedestrian can travel from the site to:

- North Abbotsford
- South South Yarra
- West Melbourne CBD
- East Burnley

4.5 Car Share Facilities

Car sharing services provide cars on demand, for rent either by the hour or by the day.

To use the service, one must register with a car share company, pay a fee and book a car either by phone or on the internet. Cars are picked up and dropped off at designated car share bays.

This service is provided as an ideal choice for people living or working in inner city locations where other modes of transport such as walking, cycling, trams and buses are available. It is also considered a suitable option for people who don't need a car every day or want to get rid of a second car.

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The subject site is located within a short walk to car sharing services operated by Flexi Car, GoGet and Greenshare Car as summarised at Table 2

Table 2 Car Share Availability

Company	Location	Walking Distance
Flexicar	Corner Lennox Street and Gipps Street	170 metres / (2 minutes)
	Corner Lennox Street and Richmond Terrace	350 metres / (5 minutes)
	Corner Swan Street and Church Street	600 metre / (8 minutes)
	Corner Cremorne Street and Gough Street	750 metres / (9 minutes)
GoGet	Corner Lennox Street and Swan Street	210 metres / (3 minutes)
	Corner Lennox Street and Gipps Street	170 metres / (2 minutes)
	Corner Cremorne Street and Jessie Street	300 metres / (4 minutes)
	15 Swan Street (CarePark)	350 metres / (4 minutes)
	Swan Street and Docker Street	450 metres / (6 minutes)
	Tanner Street Car Park	300 metres / (4 minutes)
GreenShareCar	Swan Street and Docker Street	450 metres / (6 minutes)



5 Green Travel Plan

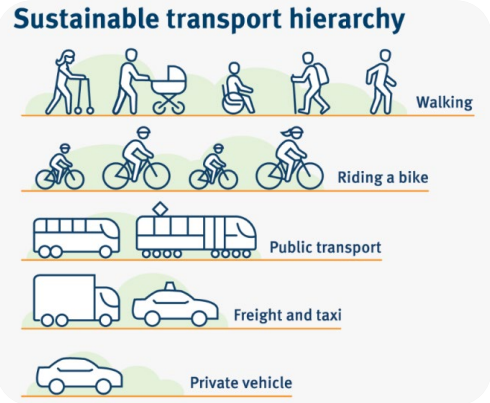
5.1 Objectives

A Green Travel Plan (GTP) sets out measures that aim to manage car dependency and encourage the use of sustainable transport modes, with the objective of permanently changing travel mode behaviour.

Specifically, a GTP seeks to encourage people to adopt a sustainable transport mindset with a preference for:

- Walking;
- Cycling;
- Public transport;
- Carpooling / ride share services

In lieu of single occupant car trips.



5.2 Targets

The goal of this GTP is to achieve a shift towards sustainable transport modes (from private motor vehicle).

To inform considerations relating to targets, reference has been made to the existing travel patterns for people working within the nearby area, as summarised in Table 3.

Table 3 Journey to Work Data (Based on place of work) - 2016 Census Data

% Mode of Travel for 'journey to work' trips	Work within City of Yarra	Work within Inner Melbourne (average)
Car (as driver)	56%	52%
Car (as passenger)	3%	3%
Public Transport	27%	34%
Cycling	5%	3%
Walking	7%	5%
Other Transport	2%	3%

This data indicates that despite its inner-city location, there is still a heavy reliance on private motor vehicle trips to and from work within the City of Yarra.

The buildings at 25-43 Wangaratta Street, Richmond have a total NLA of approximately 4,400 square metres.

Conservatively assuming an occupancy rate of 1 employee per 10 sq.m it is anticipated that there will in the order of 440 employees on-site when development is tenanted and fully operational.

With a total of 24 on-site car parking spaces, this equates to a maximum of 5.5% of employee vehicle trips which can be taken by a car (as a driver).

The subject site is well connected to public transport services (train and bus) as well as Melbourne's extensive bicycle network. It is therefore suggested that the following targets are set for this development:

Staff

- 94.4 of commuter trips (staff) to/from the site be undertaken by sustainable transport modes
 - Mode share of at least 85% for trips by public transport;
 - Mode share of at least 4.4% for trips by bicycle; and

- o Mode share of at least 5% for walking tips.

It is noted that the above targets should be seen as aspirational aims as opposed to strict 'targets', with the various initiatives promoted within this GTP designed to assist in achieving the aims.

To assist with meeting these targets, the following design and operational features / opportunities will be leveraged by the building manager, responsible for coordination and implementation.

5.3 Design Features

5.3.1 Cycling Facilities

The development has a statutory requirement to provide 10 bicycle spaces.

To encourage cycling as a mode of transport, the development contemplates 31 bicycle parking spaces.

The provision of secure, accessible and convenient cycling facilities in excess of the statutory requirements will contribute positively to encouraging cycling as a mode of transport.

5.3.2 Car Parking

To manage car dependency and encourage the use of sustainable transport modes, only 24 spaces are planned on-site.

The under provision of on-site car parking will contribute positively to efforts to support sustainable transport alternatives to the motor car.

5.4 Operational Initiatives

As detailed in the draft action plans provided within Appendix A and in subsequent sections of this report, various operational strategies could be implemented to encourage the use of non-private motor vehicles.

The draft action plan included in this report is an outline of the actions and incentives that could be undertaken / provided and is subject to agreement by the building manager and Council.

The suggested operational strategies are outlined in the following sections of this GTP.

As detailed in the outline action plans provided within Appendix A and in subsequent sections of this report, various operational strategies would be implemented to encourage the use of sustainable transport.

The outline action plan included in this report sets out actions and incentives that should be undertaken / provided.

The operational strategies should include:

5.4.1 Sustainable Transport - Information Pack

To inform staff of alternative travel options and encourage their use, the tenants are to provide a Sustainable Transport Pack to staff that will include:

- Information about the development (design features relating to bicycle facilities, and allocation of car parking facilities);
- City of Yarra TravelSmart Map

This Travel Smart Map (included as Appendix C) provides baseline details relating to:

- o Bicycle and Pedestrian accessibility.
- o Public Transport, including Information on how and where to purchase/top-up Myki, including Mobile Myki;

- Services, including shopping, recreational and educational with suitable non-car based transport options;
- Links to relevant Green Travel, public transport and local services websites.

Note: To ensure staff have access to current, and real time sustainable transport data relating to tram, train and bus timetables, and location of existing and proposed share car schemes, bicycle parking facilities, tram stops, taxi ranks, railway stations, bus stops and bicycle paths, the tenants will be required to install a tablet / ipad or similar in the staff break rooms.

This device would be pre-programmed with sustainable transport applications such as PTV app / Tram Tracker app and other resources and shall serve as the sustainable transport hub for up to date and real time sustainable transport information.

5.4.2 Creating and sustaining an active transport (walking / cycling) culture:

The Building Manager with the assistance of nominated staff will form a committee whose purpose is to create and sustain an active transport culture (cycling and walking).

Given that the site is well connected to the bicycle network, and ample bicycle parking and end of trip facilities are provided, the committee would seek to create and sustain membership to initiatives such as, Bicycle User Groups (BUG's).

These groups should include a Buddy Group where experienced cyclists buddy a novice to ride the best / safest route to the site.

5.4.3 Discouraging Single Occupant Motor Vehicle Travel

1. Provide sustainable transport allowances for staff. This should take the form of:

— **Public Transport:**

- a. Tenants will be required to sign up to the **myki Commuter Club**. This would allow staff members to access a 10% discount in tickets. This can be done independently, or via a salary package scheme.

— **Cycling:**

- a. Staff that live locally or that are amenable to cycling as a mode of travel to and from work will be invited to participate in a salary package scheme, where the employer and or building manager leverages scales of economy and purchases commuter bicycles on their behalf at a reduced cost.

2. Set up a car pooling database. This should take the form of:

- The building manager or respective tenants will maintain a database that actively fosters a culture that allows staff that live in the same locality to connect and car pool. To encourage this outcome, a number of the on-site parking spaces provided for staff should be allocated to the benefit of carpooling staff.

5.4.4 Strategic Marketing

5.4.4.1 New and Existing Staff

Staff should be introduced and immersed into sustainable transport through regular induction programs.

These induction sessions should provide an opportunity where staff who have not yet established sustainable travel habits receive information on efficient and cost-effective transport options.

5.4.4.2 Promotional Functions / Events

The Building Manager could maintain an ongoing focus on promotional functions aimed at raising awareness, motivating people to try alternatives to the car and rewarding people who travel by methods other than the car, and to reach people that may not be receptive to marketing or who are not active information seekers.

These promotions could include but are not limited to walk to work day, ride to work day, or other events specific to the needs of the centre.

It is worth noting however that travel behaviour change programs are an ongoing process and cannot strictly be broken into short term and long term initiatives.

Note:

The coordination, implementation and funding for the Green Travel Plan will be the responsibility of the Building Manager with the intent to achieve:

- Reductions in car usage (particularly single occupancy journeys) and increased use of public transport, walking and cycling;
- Increase awareness of sustainable transport options to new and ongoing staff and visitors;
- Implement travel/behaviour change strategies and use surveys to monitor changes in attitudes and reported behaviour.

To determine if targets are being met (and establish a modal split for the development), a monitoring process will need to be implemented. This process will establish travel patterns, mode splits and distances travelled by occupants of the site.

Where targets are not being met, strategies could be tailored as required to focus on these discrete areas.

6 Monitoring Program

In order to monitor the success of the Green Travel Initiatives, a 3 stage monitoring process should be implemented.

Elected staff representatives would be assigned as Travel Co-ordinators of the Travel Plan. The co-ordinators would be responsible for:

- Organising the monitoring of the performance of the Travel Plan against the targets that have been agreed.
- Reviewing the occupancy / use (and abuse) of the facilities that are provided on site, for example, cycle racks and motorcar parking spaces, clothes lockers, and travel information etc.
- Reviewing the operational success or failure of the plan and produce a review report highlighting successful initiatives and also areas that require improvement. This review report would form the platform of an updated Travel Plan.

The suggested 3 stage monitoring program is outlined below:

6.1 Stage 1

Stage 1, would involve a questionnaire survey of staff at the onset of the initiative. The survey will be useful to collect information on the travel characteristics of the occupants of the site and assist in gauging interest in the various Green Travel initiatives and to seek ideas for other Green Travel initiatives.

A pro forma for the Stage 1 questionnaire is shown attached as Appendix B.

6.2 Stage 2

Stage 2, would involve a questionnaire and feedback form to be filled out by Staff 3 months after occupation, in order to determine what Green Travel initiatives are working and which are not.

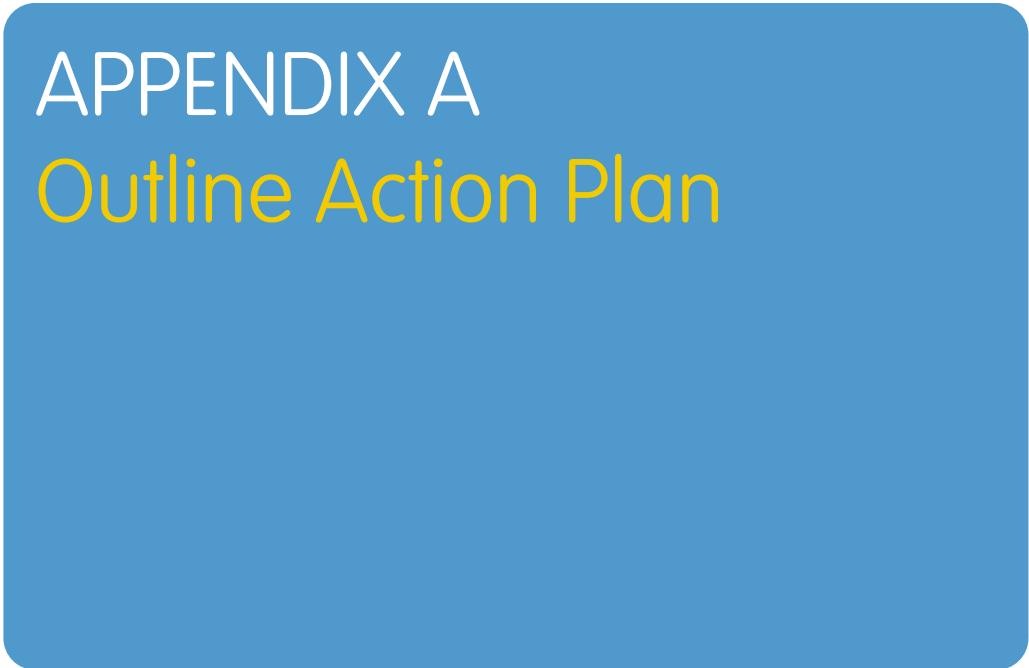
A pro forma for the Stage 2 questionnaire is shown attached as Appendix B

6.3 Stage 3

Stage 3, would be the monitoring component of the plan which would be undertaken 6-12 months after occupation. This questionnaire would test the success rate of the various initiatives and help rework programs to suit the needs of staff, visitors and parents.

A pro forma for the Stage 3 questionnaire is shown attached as Appendix B.

It is expected that the above stages would form the basis of a continuous monitoring program to gauge the effectiveness of the travel plan.



APPENDIX A

Outline Action Plan

- Provided as a Guide to Inform Discussions amongst the Building Manager / Tenants and Appointed Transport Committee
- Nominated Budgets are Indicative Only & Subject to Building Manager / Tenants and Appointed Transport Committee Input.

GENERAL ACTIONS

Operational Initiatives

Promotions including:

- An events calendar: 3-4 events per year. Best coordinated in conjunction with state wide events such as Ride to Work Day, World Environment Day, National Walk to Work Day.
- Plan for lunch, morning teas or breakfasts, guest speakers, demonstrations etc;

Timeline	Estimated Budget	Responsibility
Ongoing	Included in below estimates	Building Manager & Tenants

Cycling

Operational Initiatives

Direct staff to access to up to date route maps of bicycle networks (on road and off road) to assist with journey planning. These could include:

1. Cycling guides provided by Council;
2. Google Maps; or

Propriety applications such as Strava

Direct staff to access to up to date regulations, requirements and cycling etiquette of use to the bicycle network infrastructure. These could include:

1. Rules on riding on shared paths;
2. Rules on riding on the road.

A valuable resource for this information would be www.bicyclenetwork.com.au

For new cyclists, provide the opportunity for staff to participate in a salary package scheme, where the building manger / tenant leverages scales of economy to negotiate and purchase commuter bicycles on their behalf at a reduced cost.

Form a committee whose purpose is to create and sustain an active transport culture. This could take the form of creating and sustaining initiatives such as:

1. Bicycle User Groups (BUG's). These groups could include a Buddy Group where experienced cyclists buddy a novice to ride the best / safest route to the site.
2. Participation in events such as Ride to Workday, World Environment Day etc, including ancillary activities such as morning teas or breakfasts, guest speakers, demonstrations etc;
3. Negotiate a servicing discount for staff at a local bike shop or sports gear store

Offer personalised travel advice to interested staff via journey planners or relevant TravelSmart Guides

Timeline	Estimated Budget	Responsibility
Ongoing	\$1,500 / Year	Building Manager & Tenants

Walking

Operational Initiatives

Encourage walking through participation in events such as Walktober

Set up walking groups that encourage lunchtime walks or walking to or from work or train station

Run a workplace challenge to encourage active travel e.g. staff walking challenge using pedometers so people can keep count of steps walked, graphs or charts of staff progress for healthy competition

Access to up to date route maps of walking networks to assist with journey planning. These could include:

1. Cycling guides provided by Council;
2. Google Maps; or

Propriety applications such as Strava



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Provide umbrellas and wet weather gear for staff that walk to work or in reception for use during day time hours.

Offer personalised travel advice to interested staff via journey planners or relevant TravelSmart Guides

	Timeline	Estimated Budget	Responsibility
	Ongoing	\$1,500 / Year	Building Manager & Tenants
Public Transport			
Operational Initiatives			

Direct Staff to access up to date public transport route & timetable data in the locality via:

1. Application such as PTV's Journey planner.
2. Google Maps;

Provide access to Tram, train and bus timetables in prominent locations within the building and break rooms. This could include leveraging digital platforms via tablet devices that are located in break out rooms that are pre-loaded with applications such as PTV's Journey planner.

For new public transport users, provide the opportunity for staff to secure discounted fares. This could be done by the Building Manager / Tenant signing up to the **myki Commuter Club**. This would allow the centre access to yearly myki passes for their staff at a 10% discount. These discounts could be passed on to their staff for purchase, including via a salary package scheme

Create a committee whose purpose is to create and sustain a public transport culture. This could take the form of creating and sustaining initiatives such as:

- Lunchtime talks on public transport education to promote what is available and service changes where available

Provide information on how to purchase Myki's, associated costs, and latest technology such as Myki Pass - digital myki on Android phones

Offer personalised travel advice to interested staff via journey planners or relevant TravelSmart Guides

	Timeline	Estimated Budget	Responsibility
	Ongoing	\$1,500 / Year	Building Manager & Tenants
Cars			
Operational Features			

Form a committee whose purpose is to create and sustain behaviour change in motorists. This could take the form of creating and sustaining initiatives such as:

1. Organise a car pool postcode morning tea to match up potential car share partners
2. Set up a ride-matching system on a board in the staff room so staff interested in carpooling can link up
3. Provide a car pool guide to staff with hints on how to successfully car pool with colleagues, as well as some basic guidelines to follow.
4. Educate staff of the cost savings & environment benefits of active transport in comparison with car ownership & travel;

Provide recognition for those that use active and sustainable travel at an event or in a newsletter

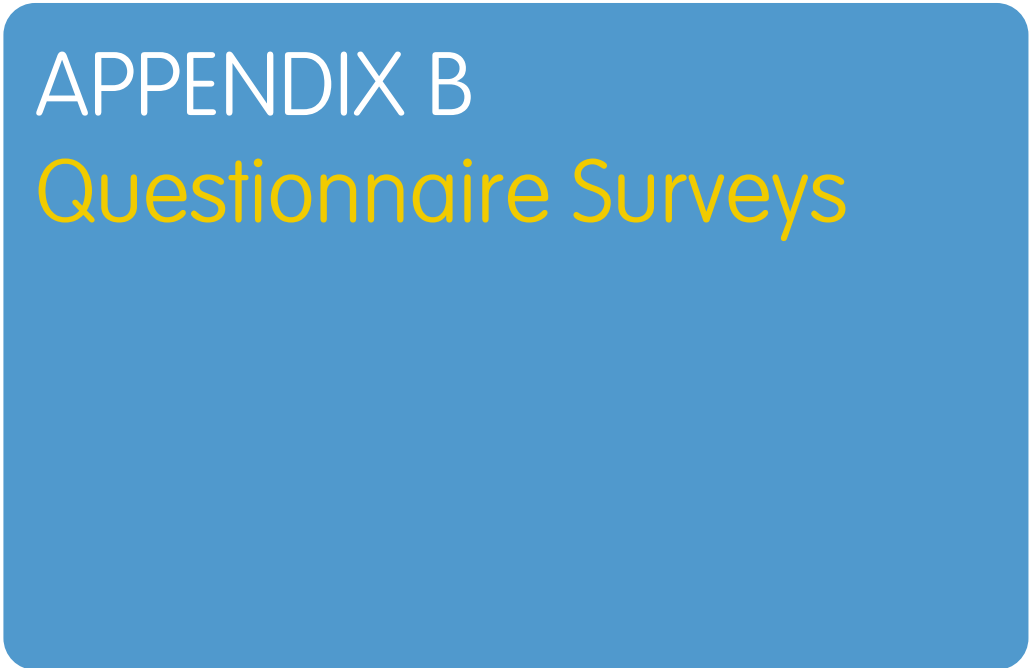
Offer personalised travel advice to interested staff via journey planners or relevant TravelSmart Guides

	Timeline	Estimated Budget	Responsibility
	Ongoing	\$1,500 / Year	Building Manager & Tenants

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APPENDIX B
Questionnaire Surveys

Change of Use:
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Green Travel Plan

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Questionnaire

Green Travel Survey

Stage 2

Date.....

1 Are you a member of Staff, or Customer/Visitor?

Staff	<input type="text"/>
Visitor	<input type="text"/>

2 How do you travel to and from the site?

Bicycle	<input type="text"/>	Walk	<input type="text"/>
Tram	<input type="text"/>	Train	<input type="text"/>
Bus	<input type="text"/>	Motorcycle	<input type="text"/>
Car as Driver	<input type="text"/>	Car as Passenger	<input type="text"/>

3 If you travel by car, either as driver or passenger, have you considered any of the sustainable travel information provided to you regarding alternative travel modes?

4 Has this information been useful and what additional information would you require to allow you to consider alternative transport options?

Thank you for your feedback.



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Green Travel Plan

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Questionnaire

Green Travel Survey

Stage 3

Date.....

1 Are you a member of Staff, or Customer/Visitor?

Staff	<input type="checkbox"/>
Visitor	<input type="checkbox"/>

2 How do you travel to and from the site?

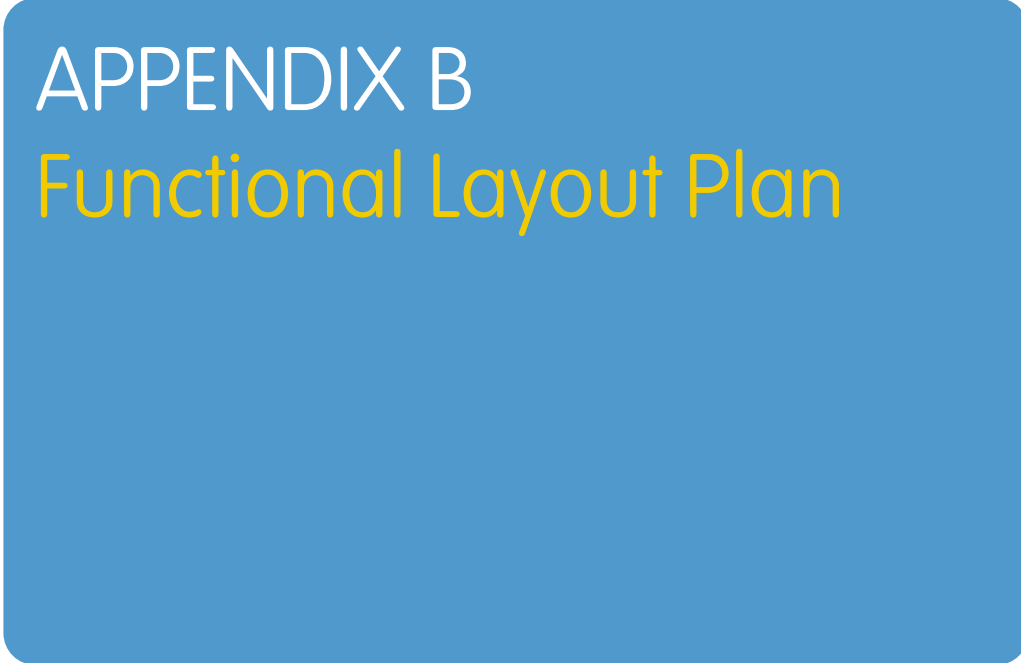
Bicycle	<input type="checkbox"/>	Walk	<input type="checkbox"/>
Tram	<input type="checkbox"/>	Train	<input type="checkbox"/>
Bus	<input type="checkbox"/>	Motorcycle	<input type="checkbox"/>
Car as Driver	<input type="checkbox"/>	Car as Passenger	<input type="checkbox"/>

3 Has this mode of travel changed since you first came to the site, and if so what influenced the change?

4 What reasonable improvements can be made to enhance the appeal of alternative transport options to the site.

Thank you for your feedback.



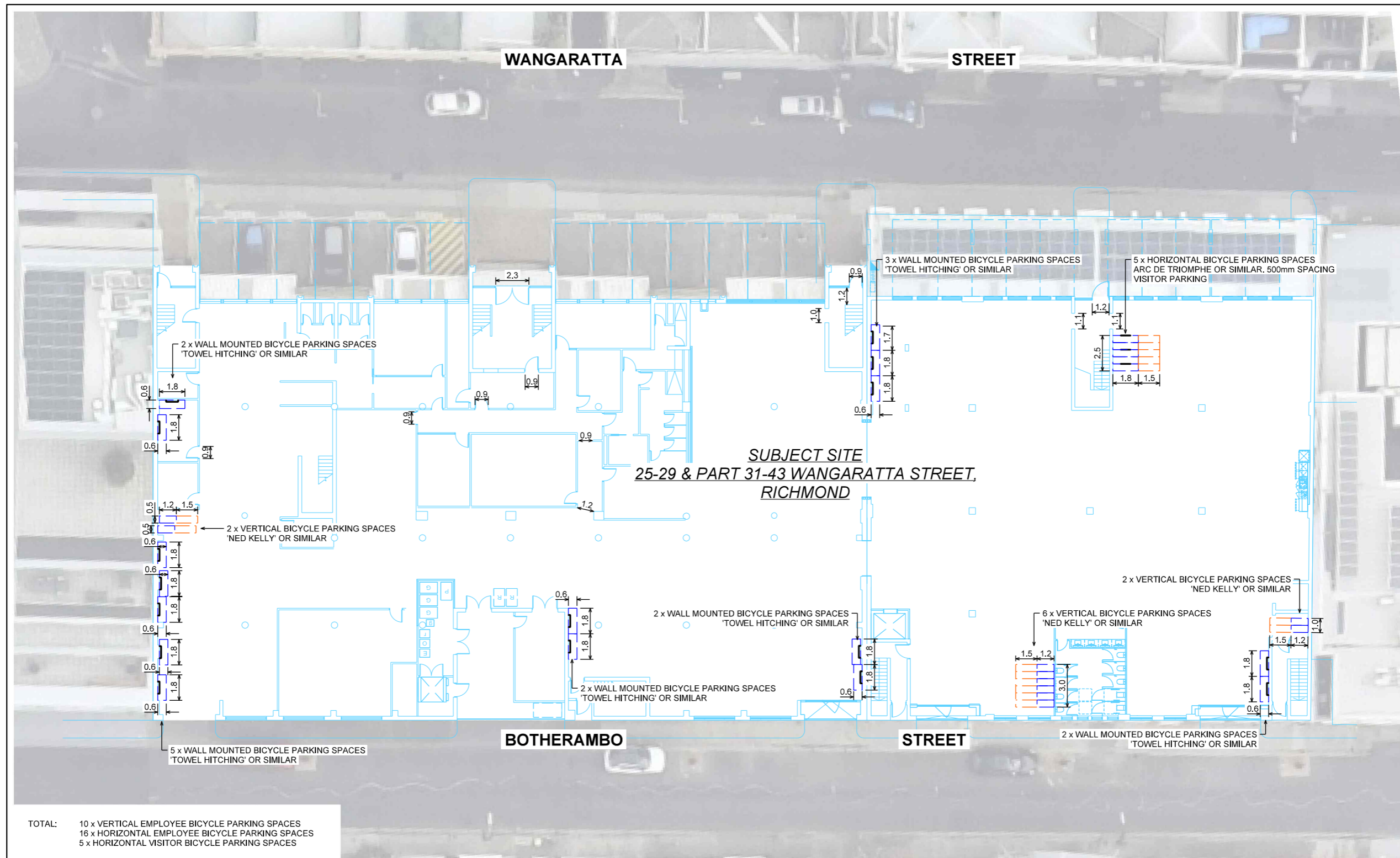


APPENDIX B

Functional Layout Plan



Car Park & Bicycle Parking Design



TOTAL: 10 x VERTICAL EMPLOYEE BICYCLE PARKING SPACES
16 x HORIZONTAL EMPLOYEE BICYCLE PARKING SPACES
5 x HORIZONTAL VISITOR BICYCLE PARKING SPACES

- GENERAL NOTES:
1. ALL DIMENSIONS ARE TO FACE OF KERB AND CHANNEL UNLESS NOTED OTHERWISE.
 2. LOCAL ROADS - WANGARATTA STREET (SPEED ZONE 40 KM/H)
- BOTHERAMBO STREET (SPEED ZONE 40 KM/H)
 3. BASE INFORMATION FROM NEARMAP AERIAL PHOTOGRAPHY DATED 27.07.2021 AND CREMORNE PROPERTIES 2016_25WSR_A1_101_export.dwg DATED 18.04.2023
 4. INSTALL ALL SIGNS AND LINE MARK IN ACCORDANCE WITH AUSTRROADS GUIDE TO TRAFFIC MANAGEMENT VOLUME 10, AS1742 AND RELEVANT VICROADS SUPPLEMENTS AND REMOVE ANY REDUNDANT SIGNS.
 5. BICYCLE PARKING SHEETS DESIGNED IN ACCORDANCE WITH BICYCLE NETWORK SPECIFICATION SHEETS

9/02/2024 9:40:35 AM

CREMORNE PROPERTIES

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Impactaustralia.com.au

MELWAY ONLINE REF: MAP 44 D11

SCALE
1:250 @ A3

Client
CREMORNE PROPERTIES

Project
**PROPOSED CHANGE OF USE TO OFFICE
25-29 & PART 31-43 WANGARATTA STREET, RICHMOND
CITY OF YARRA**

Title
BICYCLE PARKING PLAN

Status
PRELIMINARY

Revision Description
ISSUED FOR INFORMATION

Date
2024-02-09

Drawn / Approved
JT / DH

Drawing Number
IMP2107010 - DG-02-01

Revision
F

APPENDIX C

Bicycle Parking - Specification Sheets

- Bicycle Network - Ned Kelly
- Bicycle Network - Towel Hitching
- Bicycle Network - Arc De Triomphe

Arc de Triomphe™



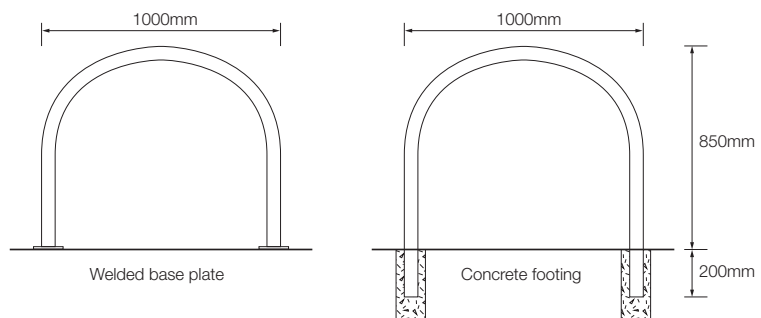
Galvanised finish / Stainless Steel finish

Features



- Each rail supports two adult bikes in an upright position
- Can be either bolted to a concrete slab or concreted in situ
- Available in stainless steel or galvanised steel
- Provides the ability to lock both wheels and frame
- Suitable for foyers and entry areas

Dimensions



Specifications

Material options

- Galvanised (Duragal)
- 316 Marine grade stainless steel

Fixing options

- Welded flange - Bolt on
- In situ

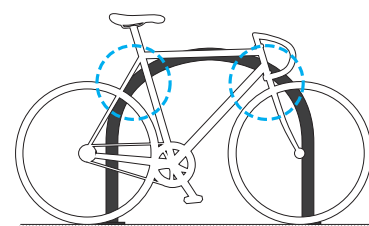
Recommended fasteners

- Galvanised Dynabolts (M10 x 65mm)
- Stainless Dynabolts (M10 x 65mm)
- Shear Nut security fasteners

Dimensions

1000mm [w] x 850mm [h]

Locking Points



V4.1 - 1/05/2017 | Specification may be subject to change without notice. ©Bicycle Network

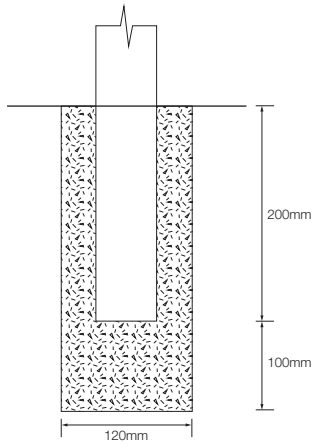


DESIGN. SUPPLY. INSTALL.

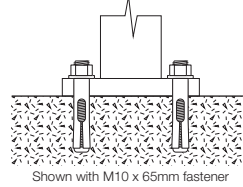
Bicycle Network ABN 41 026 835 903
 p. 1300 727 563 e. parking@bicyclenetwork.com.au bikeparking.com.au
 VIC Level 4, 246 Bourke Street, Melbourne VIC 3000 NSW 234 Crown Street, Darlinghurst NSW 2010
 TAS 210 Collins Street, Hobart TAS 7000 NT Suite 5, 18-20 Cavenagh Street, Darwin 0800

Fixing options

In situ (Concrete footing)

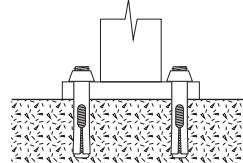


Welded flange (Bolt on) using 4 (total) x fasteners



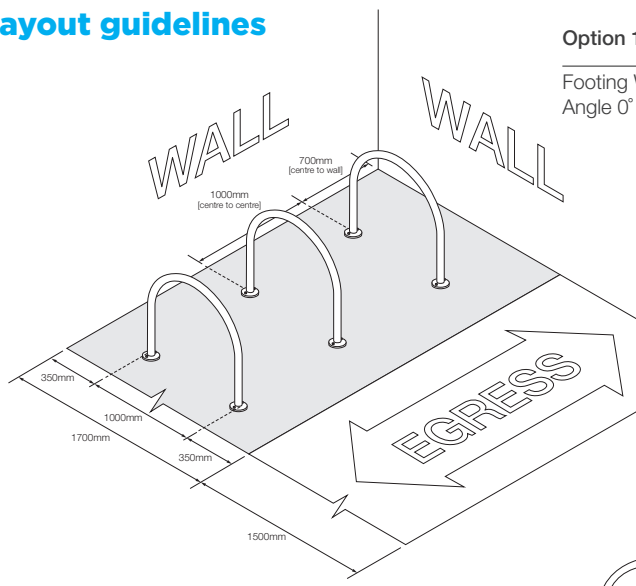
Shown with M10 x 65mm fastener

Welded flange (Security heads) using 4 (total) x fasteners



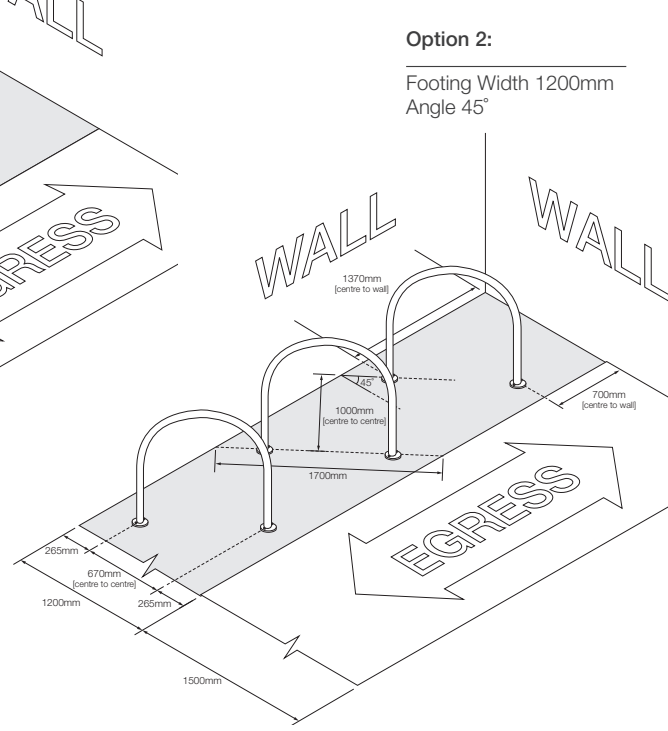
Shown with M10 x 65mm Shear Nuts

Layout guidelines



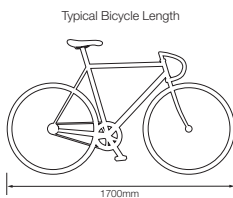
Option 1:

Footing Width 1700mm
Angle 0°



Option 2:

Footing Width 1200mm
Angle 45°



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DESIGN. SUPPLY. INSTALL.

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Towel Hitching™



Zinc finish

Features



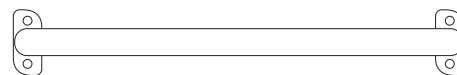
The Towel Rail is a space-efficient solution for parking single bikes against a wall, useful in narrow corridors.

- Space-efficient, can be fitted to narrow corridors
- Supplied in Zinc finish
- Accommodates all types and sizes of bicycle
- Is easy to use with any bike lock
- Requires no lifting

Dimensions



TOP



SIDE

Specifications

Material options

- Zinc finish

Fixing options

- Welded flange - Bolt On

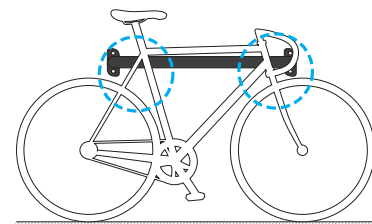
Recommended fasteners

- Zinc plated dynabolts (M8 x 40mm)

Dimensions

900mm [w] x 150mm [d]

Locking Points



V4.1 - 1/05/2017 | Specification may be subject to change without notice. ©Bicycle Network

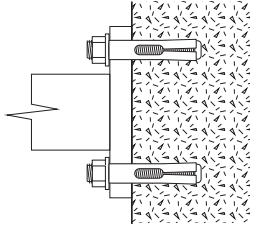


DESIGN. SUPPLY. INSTALL.

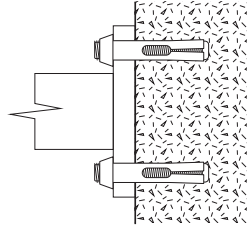
Bicycle Network ABN 41 026 835 903
 p. 1300 727 563 e. parking@bicyclenetwork.com.au bikeparking.com.au
 VIC Level 4, 246 Bourke Street, Melbourne VIC 3000 NSW 234 Crown Street, Darlinghurst NSW 2010
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Fixing options

Fix to a wall using 4x fasteners or Shear Nuts

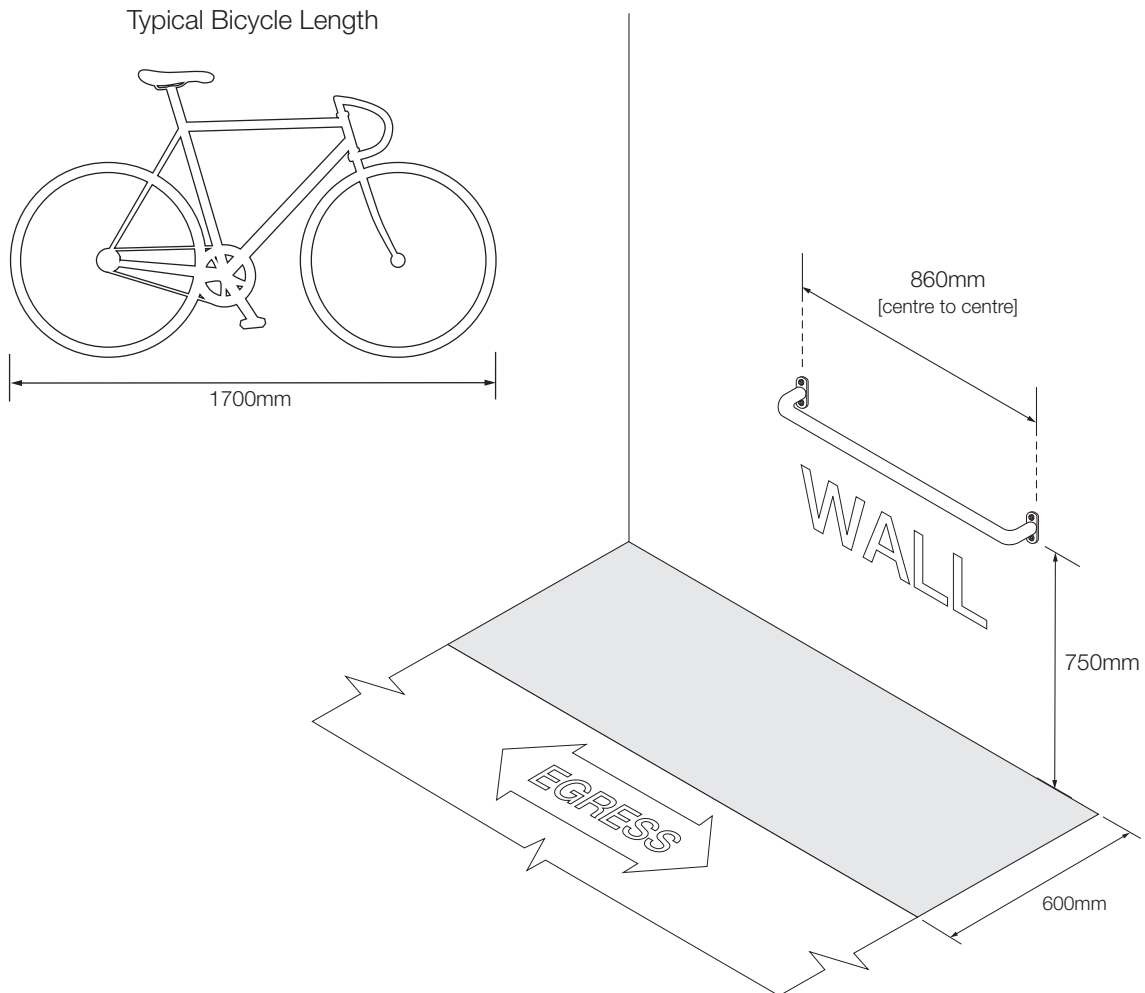


Shown with M8 x 40mm fastener



Shown with M8 x 40mm Shear Nuts

Layout guidelines



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Ned Kelly®



Zinc finish



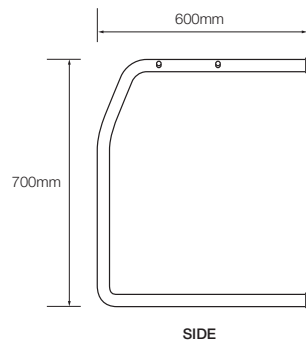
Black powder coat finish

Features



- Each rail provides storage for a single bike
- Suits bikes with full length mud guards
- Available in Zinc finish or Black powder coat over mild steel
- Provides the ability to lock the main frame and one wheel
- Support prongs with protective coating prevent damage to rim
- Can be used with custom framing - no wall needed

Dimensions



Specifications

Material options

- Zinc finish
- Black powder coat over mild steel
- Stainless steel - *Pre-order only*

Fixing options

- Bolt on to wall
- Fixed to support framing

Recommended fasteners - wall

- Dynabolts (M8 x 40mm)
- Shear Nut security fasteners

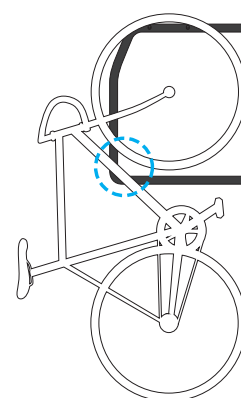
Recommended fasteners - framing

- Bolt and nut (M10 x 60mm)
- Tek screws

Dimensions

125mm [w] x 700mm [h] x 600mm [d]

Locking Points



V41 - 1/05/2017 | Specification may be subject to change without notice. ©Bicycle Network

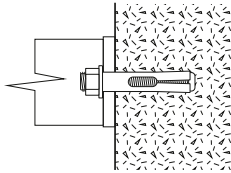


DESIGN. SUPPLY. INSTALL.

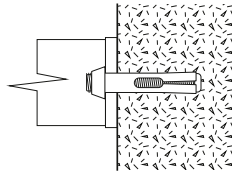
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 p. 1300 727 563 e. parking@bicyclenetwork.com.au bikeparking.com.au
 VIC Level 4, 246 Bourke Street, Melbourne VIC 3000 NSW 234 Crown Street, Darlinghurst NSW 2010
 TAS 210 Collins Street, Hobart TAS 7000 NT Suite 5, 18-20 Cavenagh Street, Darwin 0800

Fixing options

Fix to a wall using 4x fasteners or Shear Nuts

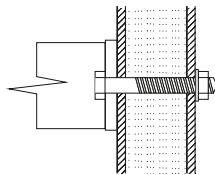


Shown with M8 x 40mm fastener

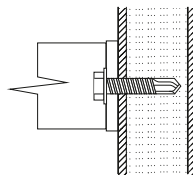


Shown with M8 x 40mm Shear Nuts

Fix to a frame using 4x bolts or Tek Screws

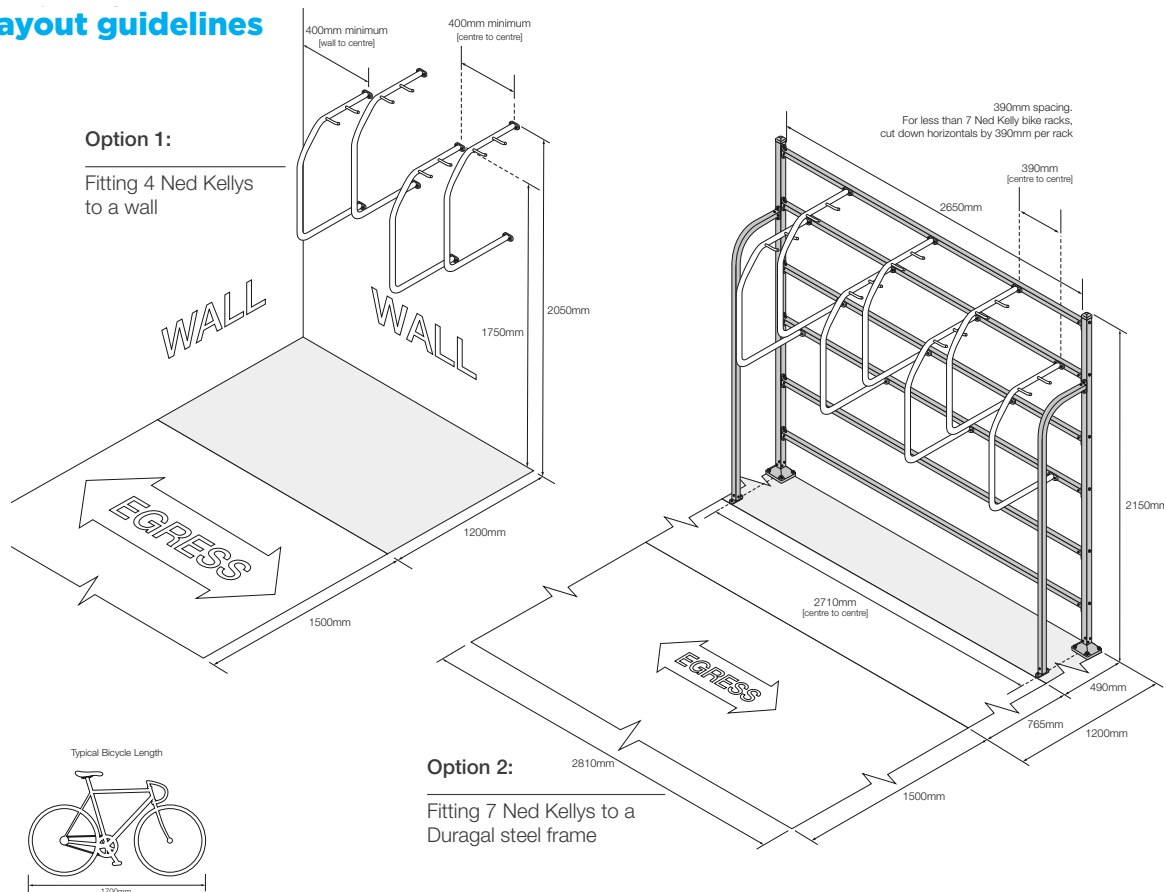


Shown with M10 x 60mm Bolt, Washer & Nut



Shown with Tek Screw

Layout guidelines

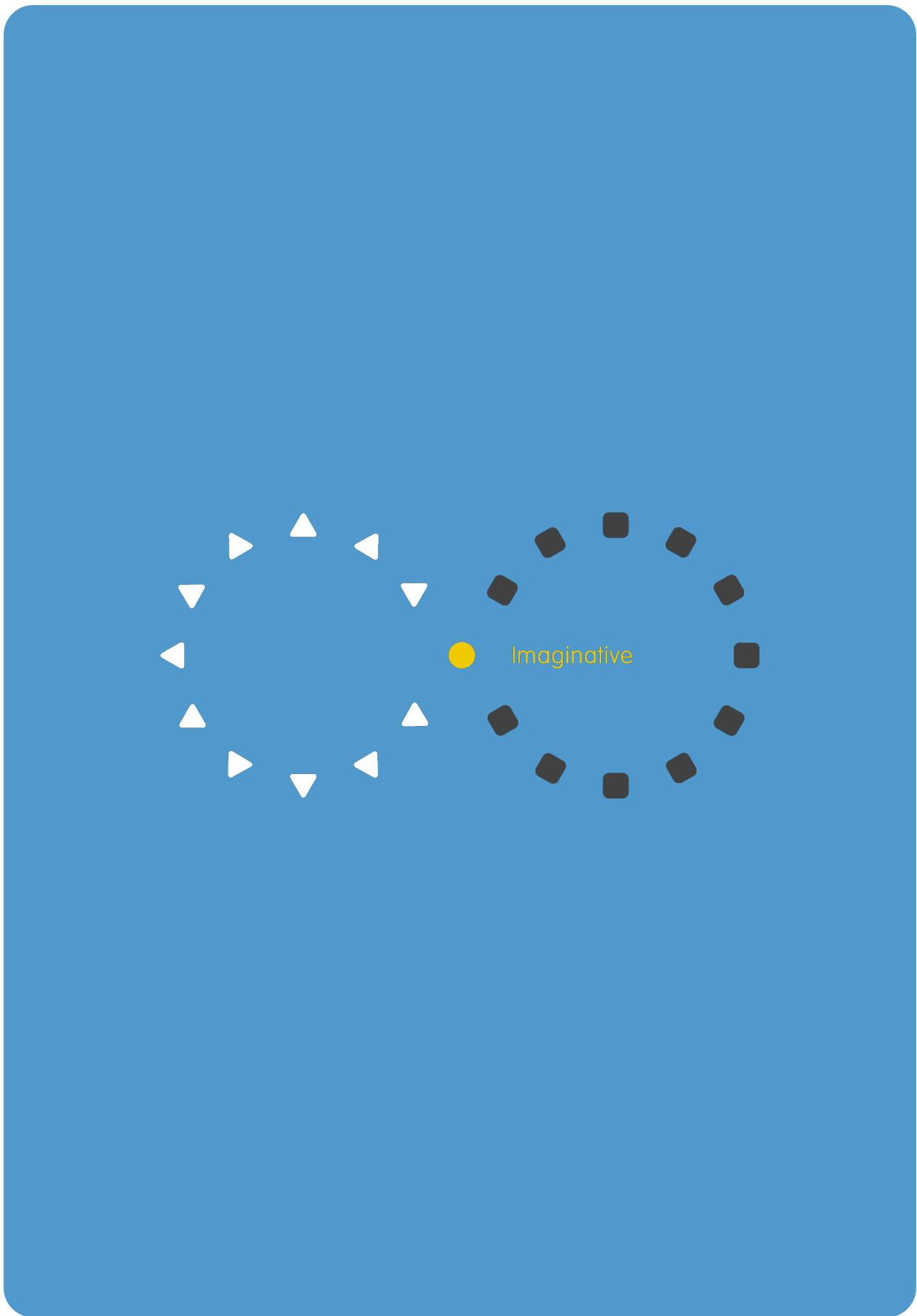


V41 - 1/05/2017 | Specification may be subject to change without notice. ©Bicycle Network

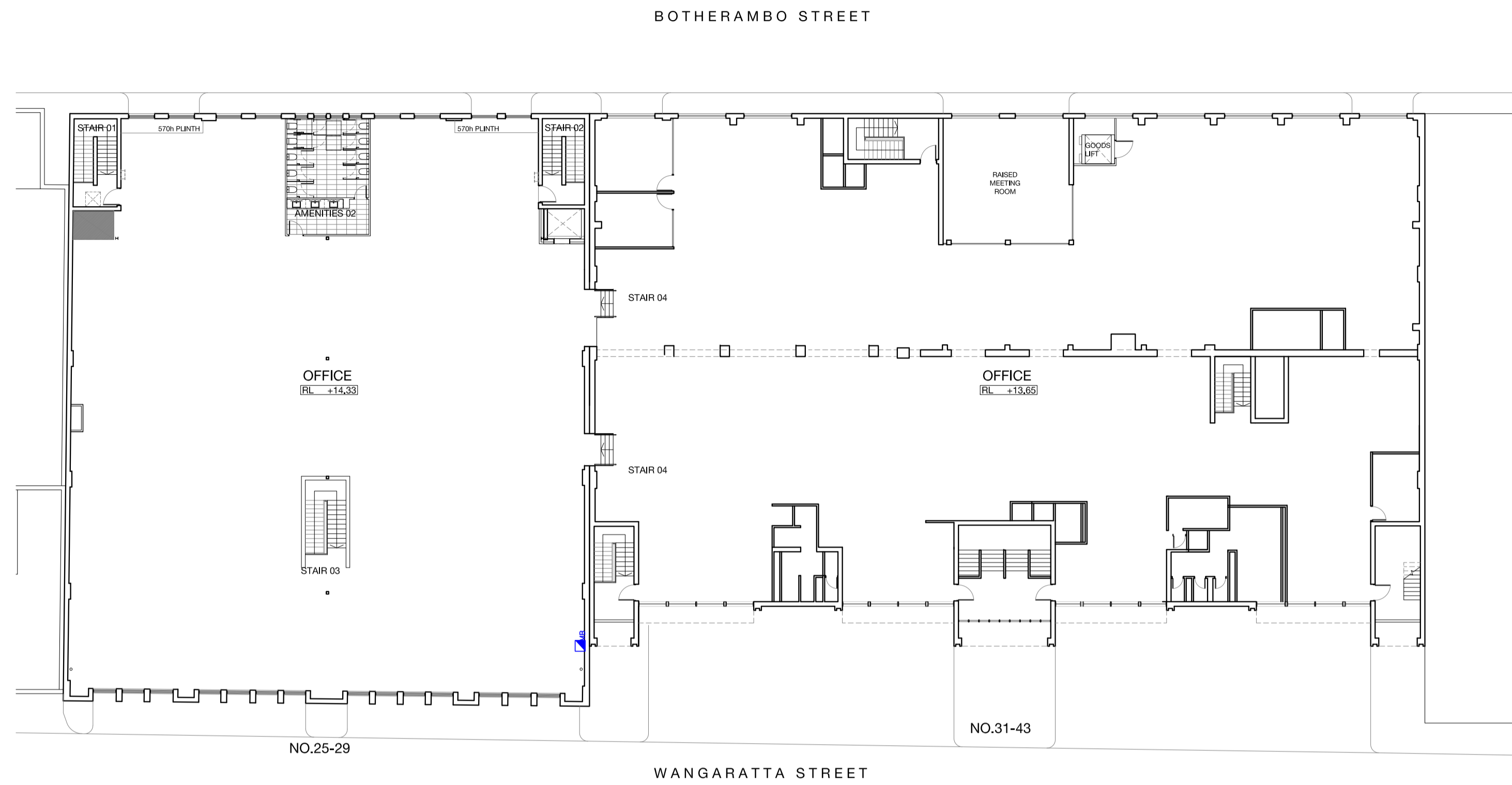


DESIGN. SUPPLY. INSTALL.

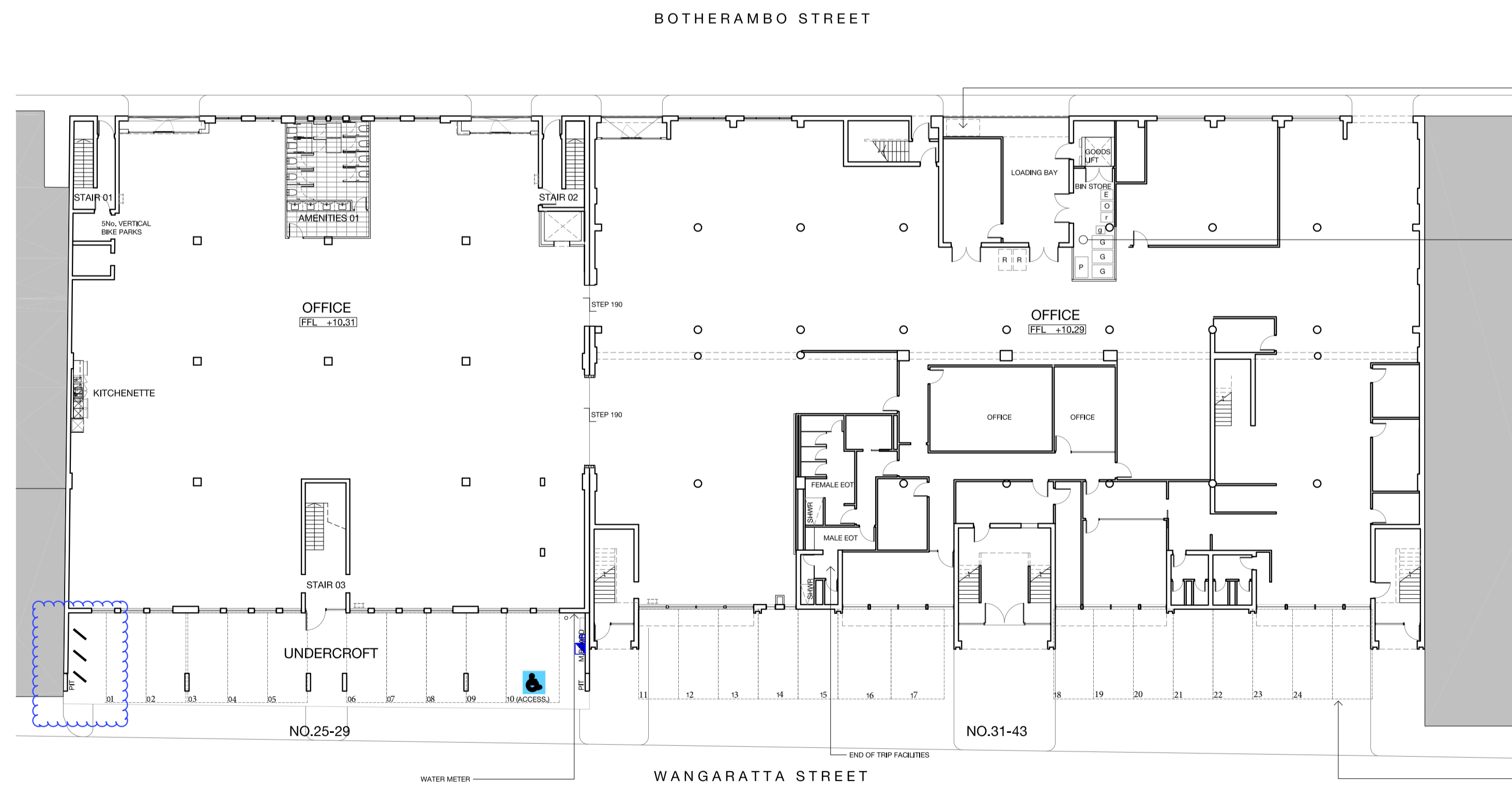
Bicycle Network ABN 41 026 835 903
 p. 1300 727 563 e. parking@bicyclenetwork.com.au bikeparking.com.au
 VIC Level 4, 246 Bourke Street, Melbourne VIC 3000 NSW 234 Crown Street, Darlinghurst NSW 2010
 TAS 210 Collins Street, Hobart TAS 7000 NT Suite 5, 18-20 Cavenagh Street, Darwin 0800



OLAVER



02 PROPOSED FIRST FLOOR PLAN 1:200



01 PROPOSED GROUND FLOOR PLAN 1:200

3x PROPOSED BICYCLE HOOPS LAYOUT IN ACCORDANCE WITH THE BICYCLE PARKING PLAN PREPARED BY IMPACT DATED 09.07.25

29m² BULKY HARD WASTE & WASTE STORE
 BIN STORE TO BE PROVIDED WITH HOT AND COLD WATER AND FLOOR WASTE AND MECHANICALLY VENTED
 BIN NOMINATIONS AS FOLLOWS:
 G 29m³ 600L GARBAGE BIN COLLECTED 3 X WEEKLY
 R 29m³ 600L COMMINGLED RECYCLING BIN COLLECTED 3 X WEEKLY
 C 19m³ 240L COMMINGLED RECYCLING BIN COLLECTED 3 X WEEKLY
 P 19m³ 600L DEDICATED PAPER BIN COLLECTED 3 X WEEKLY
 G 19m³ 120L GLASS ONLY BIN COLLECTED AS REQUIRED TO ENSURE BINS ARE MAINTAINED AND SERVICEABLE
 O 19m³ 240L ORGANIC FOOD WASTE BIN COLLECTED 3 X WEEKLY
 E 19m³ 240L WASTE BIN COLLECTED AS REQUIRED TO ENSURE BINS ARE MAINTAINED AND SERVICEABLE

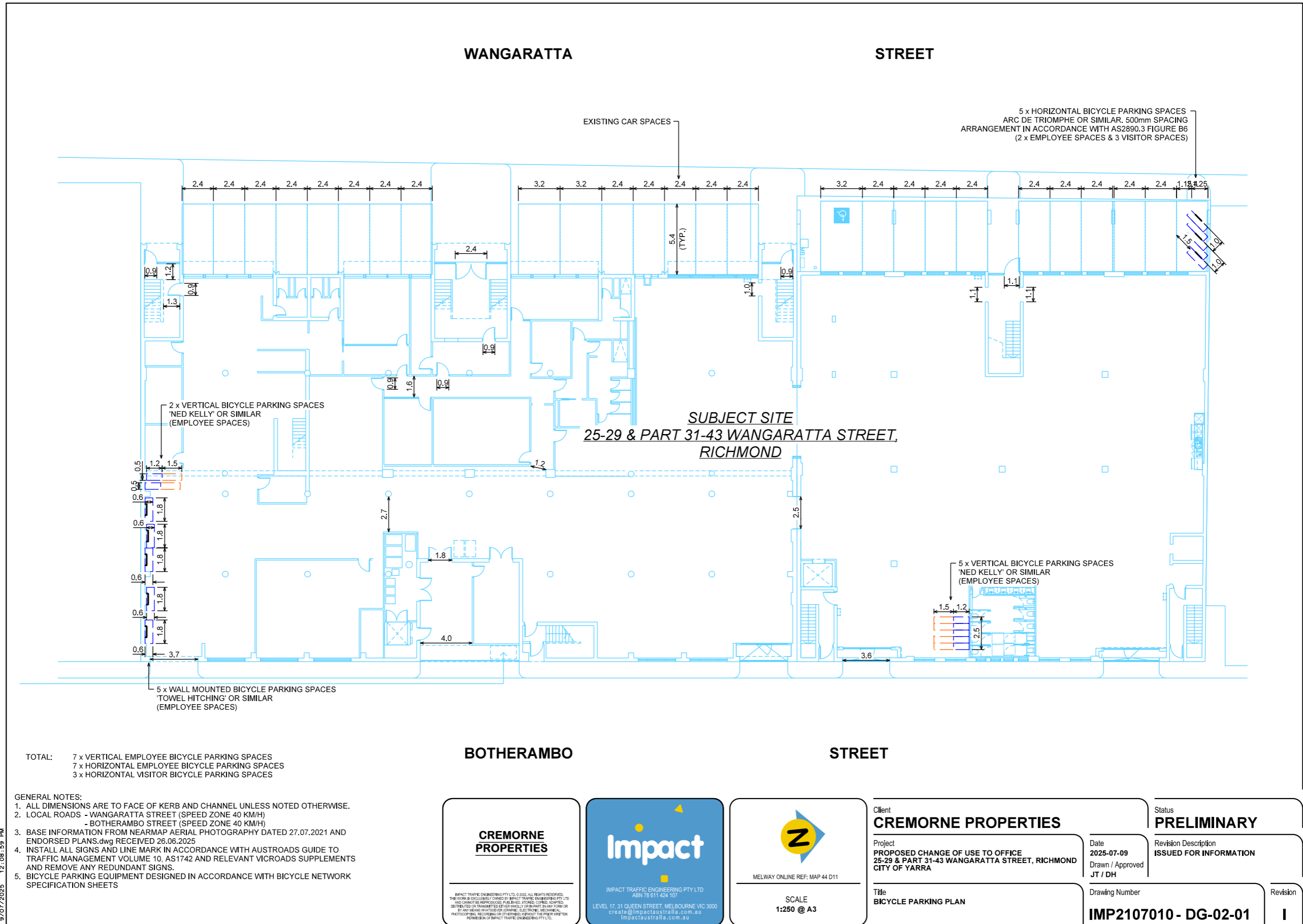
GROUND FLOOR BICYCLE PARKING LAYOUT TO BE IN ACCORDANCE WITH THE BICYCLE PARKING PLAN PREPARED BY IMPACT DATED 09.07.2025

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2016 25WSR
 ADDRESS: 25-29 & 31-43 WANGARATTA STREET RICHMOND, VIC 3121
 CLIENT: WANGARATTA SOUTH PTY LTD
 STAGE: CHANGE OF USE APP

REV	DATE	DRAWN	ISSUE
00	12.08.21	EO	FOR COMMENT
01	14.08.21	EO	FOR COMMENT
02	18.05.22	EO	FOR ENDORSEMENT
03	18.05.22	MJL	FOR ENDORSEMENT
04	03.12.22	TH	FOR ENDORSEMENT
05	23.02.23	TH	FOR ENDORSEMENT
06	17.05.23	TH	FOR ENDORSEMENT
07	18.07.23	TH	FOR ENDORSEMENT
08	27.03.24	EO	FOR ENDORSEMENT

PROPOSED PLANS
 SCALE: 1:1000 MAX
A1-101



37°49'26"S
144°59'34"E

Change of Use: Retail to Office: 25 - 43 Wangaratta Street, Richmond



Parking Impact Assessment

9 July 2025
Prepared for Cremorne Properties Pty Ltd

IMP2107010TTA01F09

Impact

Change of Use:
Retail to Office: 25 - 43 Wangaratta Street, Richmond

Parking Impact Assessment

9 July 2025

Company Information

Impact Traffic Engineering Pty Ltd
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ABN: 78 611 424 107

Email create@impactaustralia.com.au

Website www.impactaustralia.com.au

Document Information

Client Cremorne Properties Pty Ltd
Report Title Change of Use:
Retail to Office: 25 - 43
Wangaratta Street, Richmond
Report Reference IMP2107010TTA01F09
Date of Issue 9 July 2025
Approved By John-Paul Maina

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Document Control

Version	Date	Author
UPDATED FINAL	9 July 2025	Jake Townley



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Change of Use:
Retail to Office: 25 - 43 Wangaratta Street, Richmond

Parking Impact Assessment

9 July 2025

Appendices

- APPENDIX A Green Travel Plan
- APPENDIX B Site Layout Plan
- APPENDIX C Bicycle Parking - Specification Sheets

1 IMPACT[®] Snap Shot

Development Proposition

Location	37°49'26"S 144°59'34"E	25-43 Wangaratta Street, Richmond
Use	Change of use of the existing tenancies within the building at 25-43 Wangaratta Street, Richmond to enable the building to be used for the purpose of office use The proposed change in land uses as compared to existing approved uses can be summarised as follows:	
	Existing office use:	1,998.8 sq.m
	Conversion of existing warehouse to office use:	444.4 sq.m
	Undefined use to office use:	2,168 sq.m
Car Parking	24 car spaces. A reduction of 1 space from existing quantum to accommodate bicycle parking spaces.	
Bicycle Parking	Additional 17 bicycle parking spaces provided (Reduced from previously endorsed 31 spaces)	

Statutory Controls

Particular Provisions

Clause 52.06 - Car Parking

Requirement	The parking provision implications of the proposal reveals a net parking requirement for 72 spaces This calculation of the parking requirement (net difference) results in a conservatively high outcome, noting that the undefined areas have historically operated for a range of uses including Warehousing, which attracts a statutory requirement.
Adequacy of Provision	The site has excellent access to alternative transport modes which provide convenient and sufficient alternatives to private motor vehicle use. The reduced parking provision is consistent with City of Yarra's strategic transport objectives, and there are precedents of similar scale developments approved (by both VCAT and Council) with a similar parking ratios. The proposed provision is considered acceptable in this context. Based on the foregoing considerations, we consider that the development proposition satisfies the purpose of Clause 52.06, specifically: <ul style="list-style-type: none"> — To ensure that the car parking does not adversely affect the amenity of the locality. — To ensure the provision of an appropriate number of car parking spaces having regard to the demand likely to be generated, the activities on the land and the nature of the locality. — To support sustainable transport alternatives to the motor car.



Clause 52.34 - Bicycle Facilities

Requirement	<p>The bicycle parking provision implications of the proposal reveals a requirement for 10 additional spaces.</p> <p>This calculation of the parking requirement net difference results in a conservatively high outcome, noting that the undefined areas have historically operated for a range of uses which attract a statutory requirement.</p>
Adequacy of Provision	<p>A total of 17 bicycle spaces are planned.</p> <p>The proposed provision comfortably exceeds the statutory requirement for bicycle parking and will contribute positively to encouraging cycling as a mode of transport to the site.</p>
Design	<p>Assessed against Clause 52.34 and AS2890.3:2015 and determined to have satisfied the relevant design guidelines.</p>
Conclusion	

— There are no traffic and transport grounds that should prohibit the issue of a permit.

2 Introduction

2.1 Engagement

IMPACT[®] have been engaged by Cremorne Properties Pty Ltd to undertake a Parking Impact assessment of the proposal to use the existing building at 25-43 Wangaratta Street, Richmond for the purpose of office use.

2.2 Scope of Engagement

This Traffic and Transport Assessment report has been prepared to accompany a town planning submission. In preparing this assessment we have referenced the following:

- As Built and Proposed Change of Use Plans;
- City of Yarra Local Policy Framework,
- City of Yarra Planning Scheme, specifically:
 - o Clause 52.06 - Car Parking;
 - o Clause 52.34 - Bicycle Facilities; and
 - o Clause 65.01 - Approval of an Application or Plan.

3 Existing Conditions

3.1 Location

3.1.1 General

The subject site is located on the eastern side of Wangaratta Street, Richmond as shown in the locality plan and aerial photograph represented in Figure 1 and Figure 2 respectively. The site is located in the Richmond Station Precinct within The Swan Street Major Activity Centre.

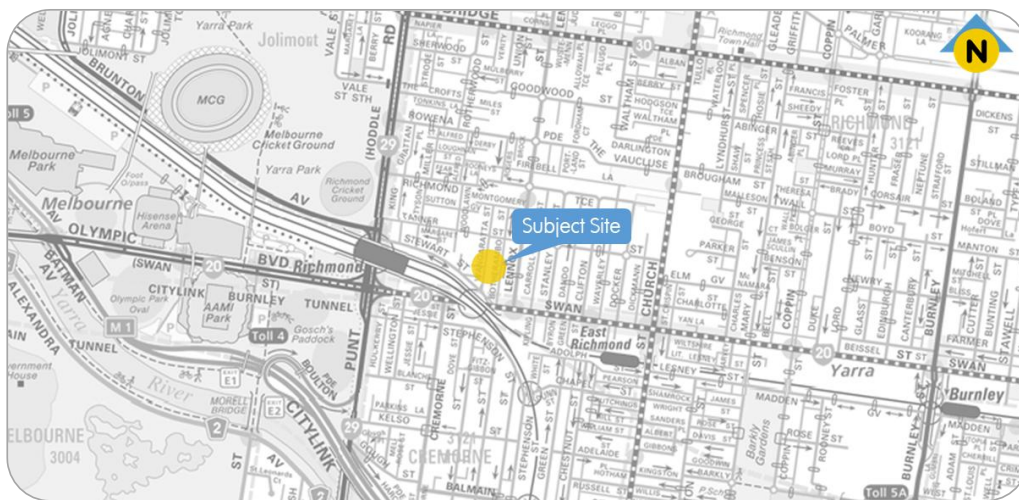


Figure 1 Location of Subject Site





Figure 2 Aerial Photography (Dated 23 February 2019)

The site is rectangular in shape and covers an area of approximately 2,920 square metres. Frontages of approximately 80 metres are provided to both Botherambo Street and Wangaratta Street, forming the eastern and western site boundaries respectively.

Land use in the area is diverse in nature, with mixed-use, neighbourhood residential and commercial uses typically surrounding the subject site.

3.1.2 Swan Street Major Activity Centre

The Swan Street area is an important Major Activity Centre (MAC) in the City of Yarra. It has a unique, strategic position within the metropolitan area offering (amongst other features):

- Unrivalled transport access by all modes of transport;
- Access to major parklands, the city centre, nearby activity centres and the Yarra River; and
- A successful retail core associated with more diverse retail, business and industrial precincts.

3.2 Planning Zone

As illustrated in Figure 3, the subject site is located within the Mixed-use Zone (MUZ).



Figure 3 Land Use Planning Zone

The purpose of the MUZ is, in part, to provide for a range of residential, commercial, industrial and other uses which complement the mixed-use function of the locality.

3.3 Road Network

3.3.1 Wangaratta Street

Wangaratta Street is classified as a local access street. Aligned in a north-south direction, Wangaratta Street connects Montgomery Street to the north and Stewart Street to the south.

Wangaratta Street comprises an approximate 6.7 metre road pavement that provides for two-way traffic and kerbside parallel parking along the western side of the carriageway only (between crossovers). Parking spaces are typically subject to either Loading Zone restrictions between 8:00am-6:00pm on Monday-Friday or 2P restrictions between 8:00am-5:00pm on Monday-Friday.

A default speed limit of 50 kilometres per hour applies to Wangaratta Street along the site frontage. Paved footpaths for pedestrian connectivity are provided along both sides of Wangaratta Street. Its typical cross-section is illustrated at Figure 4.



Figure 4 Views of Wangaratta Street facing South adjacent the Subject Site

3.3.2 Botherambo Street

Botherambo Street is classified as a local access street. Aligned in a north-south direction, Botherambo Street connects Montgomery Street to the north and Swan Street to the south.

Botherambo Street comprises an approximate 6.4 metre road pavement that provides for one way traffic in the northbound direction. Kerbside parallel parking is available along the western side of the carriageway, with spaces subject to 4P restrictions between 7:00am-7:00pm on Monday-Friday and public holidays.

A posted speed limit of 40 kilometres per hour applies to Botherambo Street along the site frontage. Its typical cross-section is illustrated in Figure 5.



Figure 5 Views of Botherambo Street facing North adjacent the Subject Site

3.4 Sustainable Transport

3.4.1 Public Transport

The site is well connected to public transport services. This is facilitated primarily via the Richmond Railway Station located 300 metres west of the subject site.

Tram route 70 runs along Swan Street, just south of the subject site. The nearest stop associated with this route is located at the corner of Lennox Street and Swan Street, situated approximately 150 metres walking distance southeast of the subject site.

Figure 6 depicts a map of surrounding public transport options, with relevant services described at Table 1.

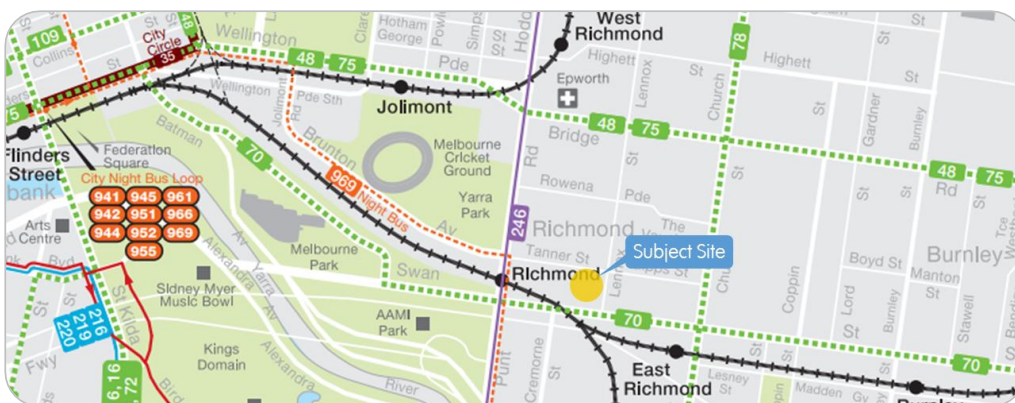


Figure 6 Public Transport Map

Table 1 Public Transport Options

Service	Route	Description	Nearest Stop	Walking Distance
Train		Alamein, Belgrave, Cranbourne, Frankston, Glen Waverley, Lilydale, Pakenham and Sandringham lines	Richmond Railway Station	300 metres (4 minutes)
Tram	48	North Balwyn - Victoria Harbour Docklands	Corner Bridge Road and Punt Road	1 kilometre (13 minutes)
	75	Etihad Stadium Docklands - Vermont South		
	70	Waterfront City Docklands - Wattle Park	Corner Lennox Street and Swan Street	150 metres (2 minutes)
	78	North Richmond - Balaclava via Prahran	Corner Swan Street and Church Street	550 metres (8 minutes)
Bus	246	Elsternwick - Clifton Hill via St Kilda	Corner Rowena Parade and Punt Road	550 metres (8 minutes)

In recognition of the sites accessibility via public transport services, the subject site is included within the State Principal Public Transport Network Area as illustrated at Figure 7.

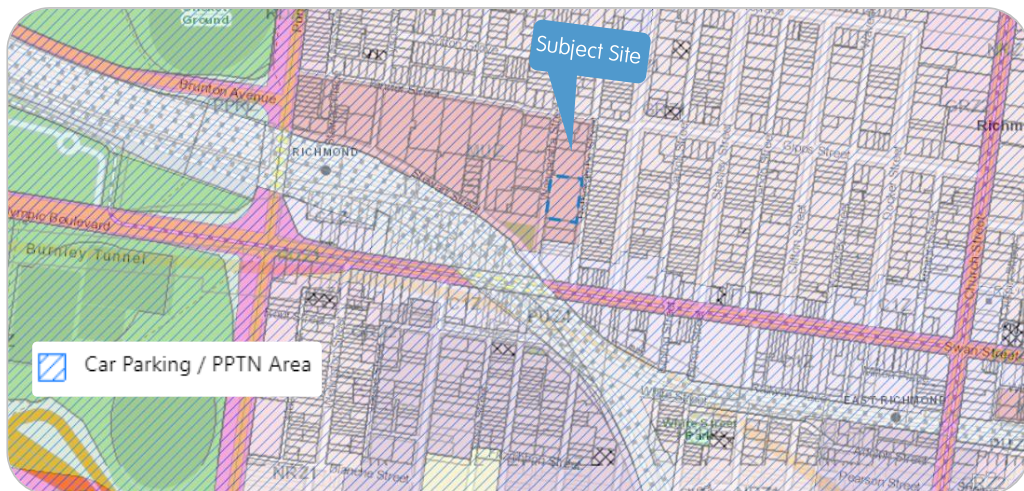


Figure 7 Subject Site location in relation to the surrounding Yarra PPTN

The Principal Public Transport Network reflects the routes where high-quality public transport services are or will be provided.

This network is intended to support integrated transport and land use planning by encouraging more diverse and dense development near high-quality public transport to help support public transport usage.

3.4.2 Cycling

The site is easily accessible via Melbourne's extensive bicycle network, including Council's Strategic Municipal Corridors as illustrated at Figure 8.

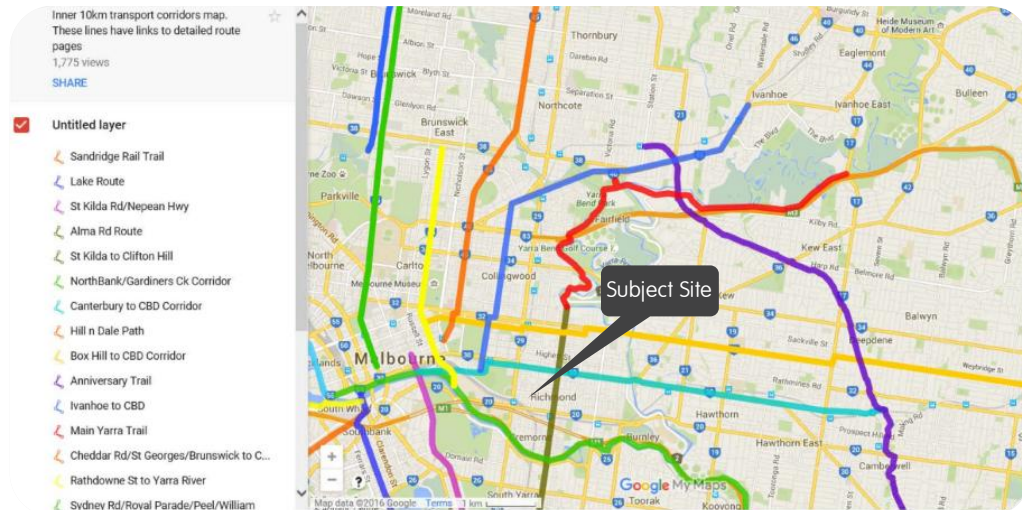


Figure 8 Bicycle Network Plan

These corridors as demonstrated above and in the Strava Heat Map¹ provided at Figure 9 provide cyclists with connections to cycling paths and trails throughout the Metropolitan Melbourne area.



Figure 9 Strava Heatmap

¹ Visualization of public data from users of the Strava application.

3.4.3 Car Share

Car sharing services provide cars on demand, for rent either by the hour or by the day. To use the service, one must register with a car share company, pay a fee and book a car either by phone or on the internet. Cars are picked up and dropped off at designated car share bays.

This service is provided as an ideal choice for people living or working in inner city locations where other modes of transport such as walking, cycling, trams and buses are available. It is also considered a suitable option for people who don't need a car every day or want to get rid of a second car.

The subject site is located within a short walk to car sharing services operated by Flexi Car, GoGet and GreenShare Car as summarised at Table 2.

Table 2 Car Share Availability

Company	Location	Walking Distance
Flexicar	Corner Lennox Street and Gipps Street	170 metres / (2 minutes)
	Corner Lennox Street and Richmond Terrace	350 metres / (5 minutes)
	Corner Swan Street and Church Street	600 metre / (8 minutes)
	Corner Cremorne Street and Gough Street	750 metres / (9 minutes)
GoGet	Corner Lennox Street and Swan Street	210 metres / (3 minutes)
	Corner Lennox Street and Gipps Street	170 metres / (2 minutes)
	Corner Cremorne Street and Jessie Street	300 metres / (4 minutes)
	15 Swan Street (CarePark)	350 metres / (4 minutes)
	Swan Street and Docker Street	450 metres / (6 minutes)
	Tanner Street Car Park	300 metres / (4 minutes)
GreenShareCar	Swan Street and Docker Street	450 metres / (6 minutes)

3.4.4 Bike Share

The cities of Yarra, Melbourne and Port Phillip have joined to trial a dockless electric bike (e-bike) share scheme for one year.

The bike sharer scheme will be operated by Lime, and will comprise bright red and green bikes that are equipped with the latest in GPS technology to ensure Yarra's footpaths remain safe and clear, and bikes are concentrated in areas where they are most in demand

Users will be required to Use the Lime app to find the closest Lime-E Bike.

3.5 Parking Inventory & Demand

IMPACT® commissioned Trans Traffic Survey to undertake 'spot' parking occupancy surveys on **Wednesday 20th March 2019** at 8:00am, 10:00am, 12:00pm and 3:00pm.

The surveyed area has been illustrated in Figure 10 and considered publicly available parking within an approximate 250 metre walking radius of the subject site.

Change of Use:
Retail to Office: 25 - 43 Wangaratta Street, Richmond

Parking Impact Assessment

9 July 2025

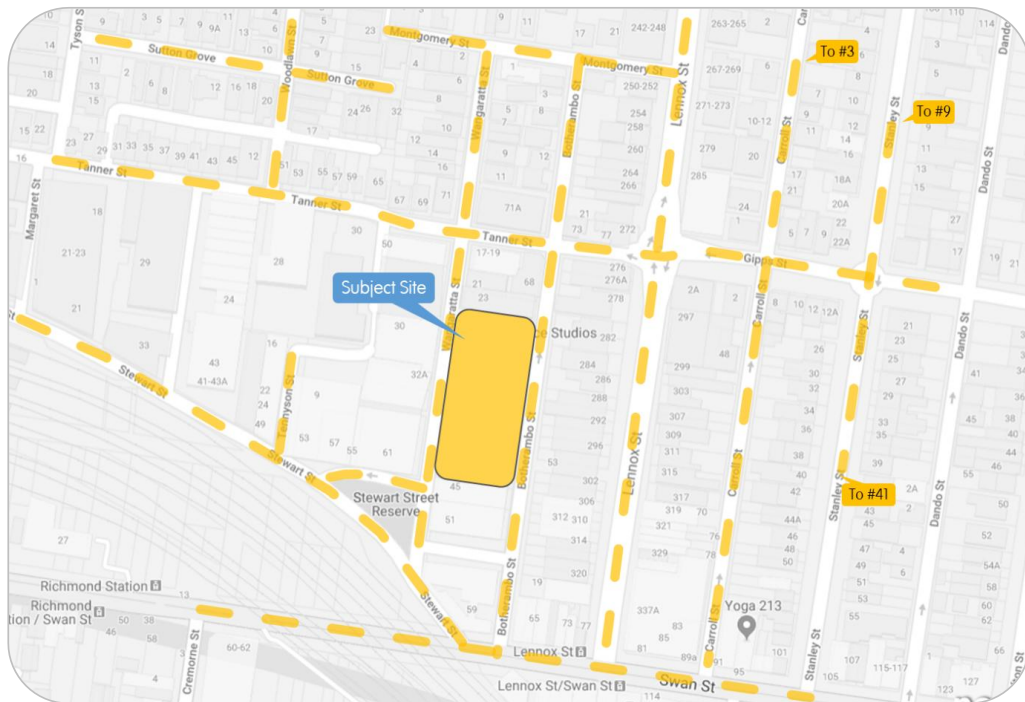


Figure 10 Parking Survey Area

An overall supply of 449 on-street car parking spaces were identified within the study area. Of these spaces, **369** were considered to be available for general public use. It is noted that all of the 369 suitable spaces identified were classified as **short - medium term** parking spaces (being spaces subject to restrictions of 4 hours (inclusive) or less in duration).

The peak parking occupancy was recorded at 12:00pm, when 310 of the 369 publicly available spaces were occupied (representing a ~84% occupancy rate), with 59 spaces remaining vacant at this time.

The car parking occupancy profile across the duration of the survey period is shown in Figure 10.

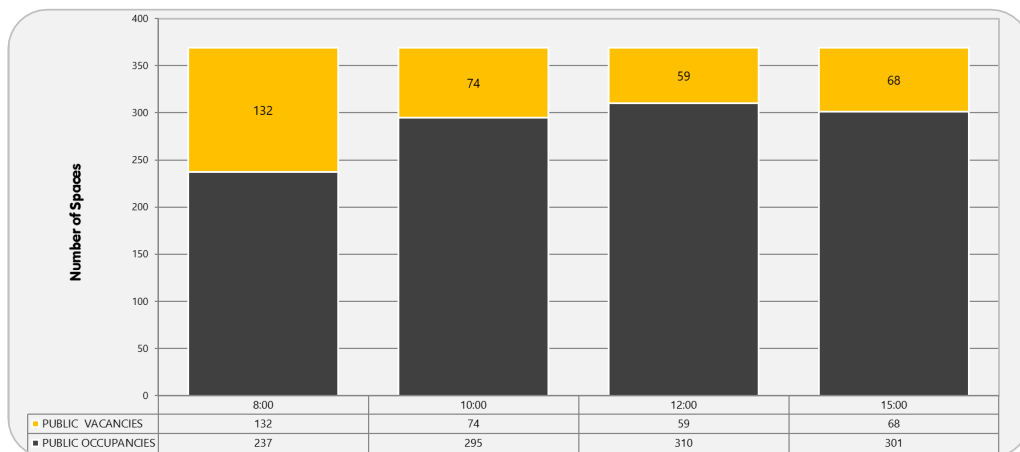


Figure 11 Car Parking Occupancy Profile - Wednesday 20th March 2019

4 Development Proposition

4.1 Use and Yield

The proposal seeks approval to use the existing buildings at 25-29 & Part 31-43 Wangaratta Street, Richmond for the purpose of office use. These buildings are currently permitted and / or historically been used as warehouse (with ancillary office) and office.

As part of this application, the buildings are to be refurbished which will result in the following development outcome:

Address	Tenancy	Use	Area	Use	Area
		Existing		Proposed	
Building 1: 25-29 Wangaratta Street	Ground Floor - Tenancy 1		413.6		413.6
	Ground Floor - Tenancy 2	Undefined	413.7	Office:	413.7
	First Floor		984		984
Building 2: 31-43 Wangaratta Street	Ground Floor - Tenancy 1	Undefined	356.7		356.7
	Ground Floor - Tenancy 2	Warehouse	444.4	Office:	444.4
	Ground Floor - Tenancy 3	Office	607.3		607.3
	First Floor	Office	1391.5		1391.5
Total Area			4611.2		4611.2

The proposed change in land uses as compared to existing approved uses can be summarised as follows:

Existing office use:	1,998.8 sq.m
Conversion of existing warehouse to office use:	444.4 sq.m
Undefined use to office use:	2,168 sq.m

4.2 Car Parking

Minor reconfiguration of car park bays is planned. These changes are shown in the plan in Appendix B.

These reconfigurations will see that one (1) car parking space is converted to facilitate five (5) bicycle parking spaces. Ultimately, the site will be serviced by 24 car spaces.

4.3 Bicycle Parking

A total of 17 bicycle spaces is planned, specifically:

- 14 spaces for employees, comprising of 12 within the building throughout the ground floor and 2 externally within the car parking area on Wangaratta Street, and
- Three (3) spaces for visitors, situated externally within the car parking area on Wangaratta Street.

Bicycle parking spaces are proposed in a combination of towel hitches, vertical racks and horizontal hoops. Internal bicycle spaces are most conveniently accessed via the doors to Botherambo Street.

The location and quantum of bicycle parking is shown in the plan provided at Appendix B.

4.4 Loading Arrangements

Loading and waste arrangements are to be consolidated and provided via the existing Loading Bay Facility within the 31-43 Wangaratta Street Building.

5 Statutory Controls

The relevant traffic and transportation statutory controls are:

Local Policy Framework

- City of Yarra Strategic Transport Statement;
- City of Yarra Council Plan; and
- Swan Street Structure Plan.

Particular Provisions

- Clause 52.06 - Car Parking;
- Clause 52.34 - Bicycle Facilities;
- Clause 65.01 - Approval of an Application or Plan.

5.1 City of Yarra Strategic Transport Statement

The Strategic Transport Statement is a document prepared by the City of Yarra in 2006 which addresses the access needs of Yarra's community whilst minimising the impact of cars. The Strategic Transport Statement has a stated vision as follows:

To create a city which is accessible to everyone irrespective of levels of personal mobility and where a fulfilling life can be had without the need for a car

The Strategic Transport Statement lists seven key Strategic Transport Objectives to achieve the above vision as follows:

1. Create a city which is a great and safe place to walk and increase the numbers of those walking in Yarra.
2. Create the most bicycle friendly city in Australia and increase the numbers of those cycling in Yarra.
3. Advocate for increased performance of public transport across Melbourne and thereby reduce the number of car trips and through traffic by both Yarra and non-Yarra residents.
4. Ensure that any new road construction is not in conflict with encouraging more sustainable transport use.
5. Ensure Council's response to parking demand is based on Yarra's parking hierarchy and sustainable transport principles.
6. Work to limit freight movement to arterial roads and freeways within Yarra and work to reduce freight movement through Yarra.
7. Encourage Council staff to use more sustainable transport for their travel and increase the capacity of the Council as a whole to respond to and initiate positive actions to further strategic transport objectives 1 to 6.

5.2 City of Yarra Council Plan

The City of Yarra Council Plan (2021 - 2025) proposes several objectives for the municipality, including:

Objective 5: Transport and Movement:

Yarra's transport network is sustainable and recognises streets as important shared public spaces. Transport and movement is accessible, safe and well connected.

To help achieve this objective, Council have listed several strategies that they wish to implement over the next five years, including:

- Lead, promote and facilitate the transition to active transport modes for people living and working in Yarra, as well as people moving through Yarra.
- Advance the transition towards zero-carbon transport by 2030 throughout the municipality.
- Create a safe, well-connected and accessible local transport network including pedestrian and bike routes through Yarra.

5.3 Swan Street Structure Plan

The Swan Street Structure Plan was adopted by the City of Yarra in December 2013. The Structure Plan sets out a proposed vision for the future of the activity centre and sets a series of strategic objectives that outline the way the vision will be achieved. With regards to movement and transport, the Swan Street Structure Plan states:

'Fortunately, the compact urban form of the area promotes shorter travel distances and is conducive to active forms of transport. This factor combined with the excellent access to public transport options, strongly positions the study area to achieve more sustainable travel behaviour. However, strengthening the sustainable transport network will require a significant shift from vehicular based travel to sustainable forms of transport.'

The Swan Street Structure Plan study area and the location of the subject site are shown in Figure 12.



Figure 12 Site Location in the Context of the Surrounding Swan Street Structure Plan Area

5.4 Clause 52.06 - Car Parking

5.4.1 Purpose

The purpose of Clause 52.06 is:

- To ensure that car parking is provided in accordance with the Municipal Planning Strategy and the Planning Policy Framework.
- To ensure the provision of an appropriate number of car parking spaces having regard to the demand likely to be generated, the activities on the land and the nature of the locality.
- To support sustainable transport alternatives to the motor car.
- To promote the efficient use of car parking spaces through the consolidation of car parking facilities.
- To ensure that car parking does not adversely affect the amenity of the locality.
- To ensure that the design and location of car parking is of a high standard, creates a safe environment for users and enables easy and efficient use.

5.4.2 Provision Requirements

To satisfy the above purpose, Clause 52.06 of the Yarra Planning Scheme specifies requirements relating to the provision and design of car parking as follows.

5.4.3 Car Parking Provision Requirements - Clause 52.06-5

Table 1 to Clause 52.06-05 of the Yarra Planning Scheme provides rates for various land uses. The site sits within the PPTN area and hence Column B rates apply.

The following rates are therefore applicable to the various uses contemplated on the site:

Office	3.0 spaces to every 100 sqm leasable floor area
Warehouse	2 spaces to each premises plus 1 space to every 100 sq.m net floor area

The proposal contemplated the following outcome

Existing office use:	1,998.8 sq.m
Conversion of existing warehouse to office use:	444.4 sq.m
Undefined use to office use:	2,168 sq.m

Calculation of the parking implications of the proposal reveals a net parking requirement for 72 spaces.

Use	Area	Statutory Requirement	Use	Area	Statutory Requirement	Net Difference
Existing			Proposal			
Existing Office	1,998.8	59	Office	1,998.8	59	0
Warehouse	444.4	6	Office	444.4	13	7
Undefined	2,168	N/A	Office	2,168	65	65
Total						72

Note:

The above calculation of the parking requirement (net difference) for the undefined areas results in a conservatively high outcome, noting the undefined areas have historically operated for a range of uses including Warehousing, which attracts a statutory requirement

5.4.4 Proposed Provision

A provision of 24 car spaces is contemplated for the proposal.

The proposal therefore seeks approval to reduce the number of parking spaces required under Clause 52.06-5.

5.4.5 Application Requirements and Decision Guidelines to Reduce Car Parking Requirement

An application to reduce (including reduce to zero) the number of car parking spaces required under Clause 52.06-5 must be accompanied by a Car Parking Demand Assessment.

The Car Parking Demand Assessment must assess the car parking demand likely to be generated by the proposal and must also address the following matters, to the satisfaction of the responsible authority.

- The likelihood of multi-purpose trips within the locality which are likely to be combined with a trip to the land in connection with the proposed use.
- The variation of car parking demand likely to be generated by the proposed use over time.
- The short-stay and long-stay car parking demand likely to be generated by the proposed use.
- The availability of public transport in the locality of the land.
- The convenience of pedestrian and cyclist access to the land.
- The provision of bicycle parking and end of trip facilities for cyclists in the locality of the land.
- The anticipated car ownership rates of likely or proposed visitors to or occupants (residents or employees) of the land.
- Any empirical assessment or case study.

Before granting a permit to reduce the number of spaces, the responsible authority must consider the following, as appropriate:

- The Car Parking Demand Assessment.
- Any relevant local planning policy or incorporated plans.
- The availability of alternative car parking in the locality of the land, including:
 - o Efficiencies gained from the consolidation of shared car parking spaces.
 - o Public car parks intended to serve the land.
 - o On-street parking in non-residential zones.
 - o Streets in residential zones specifically managed for non-residential parking.
- On-street parking in residential zones in the locality of the land that is intended to be for residential use.
- The practicality of providing car parking on the site, particularly for lots of less than 300 square metres.
- Any adverse economic impact a shortfall of parking may have on the economic viability of any nearby activity centre.
- The future growth and development of any nearby activity centre
- Any car parking deficiency associated with the existing use of the land.
- Any credit that should be allowed for car parking spaces provided on common land or by a Special Charge Scheme or cash-in-lieu payment.
- Local traffic management in the locality of the land.
- The impact of fewer car parking spaces on local amenity, including pedestrian amenity and the amenity of nearby residential areas.
- The need to create safe, functional and attractive parking areas.
- Access to or provision of alternative transport modes to and from the land.
- The equity of reducing the car parking requirement having regard to any historic contributions by existing businesses.

- The character of the surrounding area and whether reducing the car parking provision would result in a quality/positive urban design outcome.
- Any other matter specified in a schedule to the Parking Overlay.
- Any other relevant consideration.

Considering the foregoing, we have undertaken the following car parking demand assessment which outlines our expectations of likely parking demand for the office and retail uses.

5.4.6 Car Park Demand Assessment

5.4.6.1 Preamble

The 'business as usual' approach to parking demand assessments is typically informed by a 'predict and provide' methodology which leverages current behaviour to inform future considerations.

This approach is characterised by mandatory minimum parking requirements, with each development (land use) expected to provide its own parking. The intent being that motorists should nearly always be able to easily find convenient, free parking at every destination.

Under this 'predict and provide' approach, parking planning is based on the premise that the 'parking problem' means 'inadequate supply', and consequently:

1. More parking is better.
2. Every destination should satisfy its own parking needs (minimum ratios).
3. Car parks should never fill.
4. Parking should always be free or subsidised or incorporated into building costs.

However, there is an increasing trend towards more efficient use of existing transport infrastructure as an alternative to expanding roads and parking facilities, incorporated in a technique known as travel demand management (TDM).

TDM emphasises the movement of people and goods, rather than motor vehicles, and gives priority to more efficient travel and communication modes (such as walking, cycling, car sharing, public transport and telecommuting), particularly under congested conditions².

5.4.6.2 Application of TDM in Land Use Applications

It has been set out³ that the 'business as usual' approach needs to be reconsidered especially within Activity Centres and locations that are well served by a range of public transport services, where cycling and walking is a practical alternative to car travel, and where local policy advocates for car parking to be managed to optimise its use and encourage sustainable transport options.

Ronge v Moreland CC [2017] VCAT 550 & Vincent Corporation Pty Ltd v Moreland CC (Includes Summary) (Red Dot) [2015] VCAT 2049 are two VCAT Decision of Significance that signal this change in the consideration of parking provision.

The key highlights of these decisions as they relate to parking are reproduced below:

The Ronge v Moreland CC [2017] VCAT 550

This decision was classified as a 'Red Dot' decision i.e. a decision of interest or significance, on the basis of:

Policy - interpretation or application of policy:

² Austroads Guide to Traffic Management Part 11: Parking

³ Ronge v Moreland CC [2017] VCAT 550) & Vincent Corporation Pty Ltd v Moreland CC (Includes Summary) (Red Dot) [2015] VCAT 2049

- Plan Melbourne 2017-2050 and State and local policy concerning car parking, traffic and shift to other travel modes.

Practice or Procedure - consideration of individual instance or systemic issues

- Necessity to reduce reliance on car-based travel

Analysis - exposition of how to assess an issue or matters to consider

- Car parking and traffic

Specifically, the tribunal summarises the key matters as follows:

The recently released Plan Melbourne 2017-2050 is predicting that Melbourne's population will increase from approximately 5 million to 8 million people over the next 33 years. An additional 1.6 million dwellings will be required to house the extra population.

State and local planning policies are already acknowledging the change that is required in the way in which people travel with Plan Melbourne 2017-2050 and State policies referring to 20-minute neighbourhoods and greater reliance on walking and cycling. At the municipal level, Moreland has long been recognised as being at the forefront of encouraging less reliance on car based transport. For example, the Moreland Integrated Transport Strategy 2010 includes a key principle that walking and cycling are the preferred modes of transport.

Our roads are already congested and will be unimaginably so if a 'business-as usual' approach is accepted through until 2050. The stark reality is that the way people move around Melbourne will have to radically change, particularly in suburbs so well served by different modes of public transport and where cycling and walking are practical alternatives to car based travel.

A car parking demand assessment is called for by Clause 52.06-6 when there is an intention to provide less car parking than that required by Clause 52.06-5. However, discussion around existing patterns of car parking is considered to be of marginal value given the strong policy imperatives about relying less on motor vehicles and more on public transport, walking and cycling. Census data from 2011 or 2016 is simply a snapshot in time, a base point, but such data should not be given much weight in determining what number of car spaces should be provided in future, for dwellings with different bedroom numbers.

Policy tells us the future must be different. Oversupplying parking, whether or not to comply with Clause 52.06, has the real potential to undermine the encouragement being given to reduce car based travel in favour of public transport, walking and cycling.

One of the significant benefits of providing less car parking is a lower volume of vehicle movements and hence a reduced increase in traffic movements on the road network.

Vincent Corporation Pty Ltd v Moreland CC (Includes Summary) (Red Dot) [2015] VCAT 2049

This decision was classified as a 'Red Dot' decision i.e. a decision of interest or significance, on the basis of:

Nature of Case:

- A reduction of on-site car parking to zero in major activity centre.

Practice or Procedure - consideration of individual instance or systemic issues

- Considers the relevant tests for reducing on-site car parking in accordance with Clause 52.06 in conjunction with the State and local planning policy framework, particularly for sites in inner city activity centres. Summarises principles applied by the Tribunal in applications under Clause 52.06 to reduce on-site car parking provision

Specifically, the tribunal summarises the key matters as follows:

The decision discusses current policies for car parking and the consideration of both demand and supply management as sought by State policy for integrated transport and the application of this in the assessment of car parking in inner city activity centres such as Brunswick. The decision also reviews a number of other similar recent cases of the Tribunal, deriving some common themes in the consideration of reducing car parking rates in larger, particularly inner city, activity centres. These are:

- a. Parking across an activity centre needs to be addressed in a centre wide manner.

- b. Providing or restricting parking through the planning scheme, only forms one part of the overall management of car parking demand and supply in these centres.
- c. There are a number of alternative means of travel emerging. Most notable of these are share car arrangements and increased bicycle use, including electric bicycles. These have grown extensively in use in the past 10 years. Their impact on empirical demand is not yet known. Policies to minimise car dependency suggest it should lead to less demand.
- d. In areas of existing restricted on-street parking, the supply of on-street spaces for future occupants of a proposal is not particularly relevant to the assessment. In these locations, on-street road management restrictions can ensure future residents of a proposal are not able to rely on any spare capacity in on-street parking.
- e. While a current empirical demand may exist not every site can, or needs to, meet this demand. Some sites may have limited capacity to meet car parking needs, particularly small or constrained sites.
- f. A proposal for a reduced resident parking provision needs to consider:
 - i. Access to public transport, in timing, diversity of routes and frequency.
 - ii. Walkable access from the site to shops and facilities that provide for the daily needs of residents.
 - iii. Access to other transport options including good bicycle infrastructure and walkable access to publicly available shared use cars.
- g. As with any proposal, a decision to reduce parking ultimately must be tested on its own merits with competing issues balanced to achieve net community benefit. Any potential adverse impacts from parking on-street will need to be assessed against the benefits a proposal may bring to the community where car parking forms only one part of a use or development proposal.

These decisions of significance have influenced the assessment of parking provision for various land use proposals and informed decision making by Responsible Authorities and the Tribunal.

5.4.6.3 Empirical Assessment

Office

The statutory parking provision rate of 3.0 spaces per 100 sq.m typically aligns with parking demand in unconstrained scenarios in suburban and outer suburban areas.

In inner city locations, demand rates lower than 3.0 spaces per 100 sq.m are common place. Case studies undertaken by **IMPACT**[®] over a 5 day period at an 11,407 sq.m office development at 40 River Boulevard, Richmond revealed that the office had 372 spaces on site, and was generating a demand for up to 199 spaces, which equates to a rate of 1.74 spaces per 100 sq.m.

We note that whilst the case study site is located in an inner city location and with reasonable access to public transport via light rail services, the case study site does not have direct access to heavy rail.

Accordingly, in a location like the subject site, lower rates would be expected due to higher levels of access and connectivity to high quality public transport services.

5.4.7 Responsible Authority Considerations

5.4.7.1 Car Parking Demand Assessment

The 'business as usual' approach to parking demand assessments is typically informed by a 'predict and provide' methodology which leverages current behaviour to inform future considerations.

Whilst this approach has its merits, especially in suburban locations, it has been argued⁴ that the 'business as usual' approach needs to be reconsidered especially in locations that are well served by a range of heavy and light rail services, where cycling and walking is a practical alternative to car travel, and where local policy advocates for car parking to be managed to optimise its use and encourage sustainable transport options.

⁴ Ronge v Moreland CC [2017] VCAT 550

In this regard, the proposed development seeks to provide car parking at a rate below the statutory and empirical rate. Specifically, 25 spaces are planned which for the overall development equates to a rate of approximately 0.52 spaces per 100 sq.m.

Noting the character of the surrounding area, this provision is considered satisfactory for the following reasons:

- The subject site is easily accessible via sustainable transport options such as heavy rail, light rail, and active transport such as cycling. These diverse modes will provide staff and visitors alternate and viable options to the motor vehicle.
- On street car parking in the locality of the subject site is constrained, specifically, parking is restricted to short to medium term parking. These restrictions preclude the use of on street car parking for office staff, as they typically seek all day (long term) parking opportunities. The constrained on-street parking conditions would therefore serve as a disincentive for employees to drive to work.

Due to the constrained nature of on-street parking in the surrounding area, the limited availability / access to off-street commercial car parking and the accessibility to public transport services, we expect that parking demand will be constrained, with employees taking up sustainable transport modes to travel to and from work.

This outcome would also be assisted by the implementation of a Green Travel Plan, a copy of which is included as Appendix A.

Furthermore, we note that a number of office developments that share similar attributes to the proposed development have been approved by Council and VCAT with parking provision at rates below statutory and empirical rates.

These sites are summarised in Table 3.

Table 3 Recently Approved Office Developments

Development	Yield / Use	Walking Distance to Train	Walking Distance to Tram	Supported Car Parking Rate
484-486 Swan St, Richmond	18,595sq.m office/retail	150 metres	150 metres	0.82 spaces per 100 sq.m
2-16 Northumberland St, Collingwood	15,473sq.m office /shop	700 metres	250 metres	0.88 spaces per 100 sq.m
60-88 Cremorne Street, Cremorne	27,653 sq.m Office / retail	400 metres	300 metres	0.72 spaces per 100 sq.m
51 Langridge St, Collingwood	3,335 sq.m Office	400 metres	300 metres	0.54 spaces per 100 sq.m
480 Swan Street, Richmond	33,678 sq.m Office / retail	50 metres	50 metres	0.69 spaces per 100 sq.m

The proposal provides parking at a rate of approximately 0.52 spaces per 100 sq.m.

This provision whilst below the statutory and empirical rates, recognises the sustainable transport merits of the site location, and furthermore provides an outcome that maintains the momentum in this locality of prioritising sustainable transport outcomes in lieu of private motor vehicle travel as prescribed in local policy objectives.



5.4.7.2 Local Policy

Yarra has several local policies and strategies in place for the suburb of Richmond and the Swan Street area that aim to reduce reliance on private motor vehicles and to promote the use of alternative transport modes.

5.4.8 Conclusion - Car Parking Provision

Based on the foregoing considerations, we consider that the development proposition satisfies the purpose of Clause 52.06, specifically:

- To ensure that the car parking does not adversely affect the amenity of the locality.
- To ensure the provision of an appropriate number of car parking spaces having regard to the demand likely to be generated, the activities on the land and the nature of the locality.
- To support sustainable transport alternatives to the motor car.

Note:

To support and nature a sustainable transport culture at this site, the applicant has committed to implementing a green travel plan aimed at managing car dependency and encouraging the use of sustainable transport modes, with the objective of permanently changing travel mode behaviour.

The Green Travel Plan is provided at Appendix A.

5.5 Clause 52.34 - Bicycle Facilities

5.5.1 Purpose

The purpose of Clause 52.34 is to encourage cycling as a mode of transport, and provide secure, accessible and convenient bicycle parking spaces and associated shower and change facilities.

5.5.2 Provision Requirements - Clause 52.34.3

To satisfy the above purpose, Clause 52.34-3 of the Yarra Planning Scheme specifies the bicycle parking provision requirements for a variety of different uses within Table 1.

Rates applicable to the proposed uses are:

Office	Employees:	1 space to each 300 sqm floor area
	Visitors:	1 space to each 1,000 sqm floor area

There are no bicycle parking requirements for warehouse uses.

Application of the proposed changes reveals a requirement for 10 additional bicycle parking spaces as following.

Use	Area	Statutory Requirement	Use	Area	Statutory Requirement	Net Difference
Existing			Proposal			
Warehouse	444	0	Office	444	1	1
Undefined	2,168	N/A	Office	2,168	9	9
Total						10

The foregoing assessment reveals that the proposed change of use will result in a requirement for an additional 10 bicycle parking spaces.

Note:

The above calculation of the parking requirement net difference for the undefined areas results in a conservatively high outcome, noting the undefined areas have historically operated for a range of uses, which attract a statutory requirements.

5.5.3 Proposed Provision

A total of 17 bicycle spaces are planned, comprised of 14 spaces for employees and three (3) spaces for visitors.

5.5.4 Adequacy of Proposed Provision

The proposed provision comfortably exceeds the statutory requirement for bicycle parking and will contribute positively to encouraging cycling as a mode of transport to the site.

5.5.5 Design Requirements

Clause 52.34-6 of the Planning Scheme states:

Bicycle spaces should:

- Provide a space for a bicycle of minimum dimensions of 1.7 metres in length, 1.2 metres in height and 0.7 metres in width at the handlebars.

- Be located to allow a bicycle to be ridden to within 30 metres of the bicycle parking space.
- Be located to provide convenient access from surrounding bicycle routes and main building entrances.
- Not interfere with reasonable access to doorways, loading areas, access covers, furniture, services and infrastructure.
- Not cause a hazard.
- Be adequately lit during periods of use.

5.5.5.1 Proposed Design

The bicycle parking design has been assessed against the requirements of Clause 52.34 of the Yarra Planning Scheme, the Australian Standard for Bicycle parking facilities (AS 2890.3:2015) and the manufacturer's specifications.

Details of the nominated product are provided as Appendix C.

5.5.6 Conclusion - Bicycle Parking

We can conclude that bicycle parking provided as part of this development satisfies the purpose of Clause 52.34, specifically:

- To encourage cycling as a mode of transport, and provide secure, accessible and convenient bicycle parking spaces.

5.6 Clause 65.01 - Approval of an Application or Plan

5.6.1 Loading Requirements and Objectives

To address the adequacy of loading for new developments, the Yarra Planning Scheme specifies the following:

- The responsible authority must consider, as appropriate, the adequacy of loading and unloading facilities and any associated amenity, traffic flow and road safety impacts.

5.6.2 Adequacy of Proposed Loading Facilities

In response to the above, we note that:

- Loading for the office use will be undertaken infrequently, typically associated with the occupation and vacation of the tenancies.

The development plans do not show any dedicated loading bay on-site (with the bay shown intended for waste collection purposes only), however we note that as a result of the proposal (i.e. the removal of redundant crossovers) a number of on-street spaces will be created. It is intended to utilise one (1) of these on-street spaces as an informal loading bay for the cafe tenancy (and occasionally the office as required).

5.6.3 Waste Collection

It is proposed to collect waste on-site, by way of private services, using a 6.4 metre waste collection mini rear loader.

Swept paths attached as Appendix B demonstrate the nominated collection vehicle will reverse into the subject site, undertake waste collection and then exit the site in a forward direction.

We are satisfied that there is sufficient room provided to cater for the proposed waste collection vehicle.

Change of Use:
Retail to Office: 25 - 43 Wangaratta Street, Richmond

Parking Impact Assessment

9 July 2025

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APPENDIX A

Green Travel Plan

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37°49'26"S
144°59'34"E

Change of Use: Retail to Office: 25 - 43 Wangaratta Street, Richmond



Green Travel Plan

23 June 2025
Prepared for Cremorne Properties Pty Ltd

IMP2107010GTP01F06

Impact

Change of Use:
Retail to Office: 25 - 43 Wangaratta Street, Richmond

Green Travel Plan

23 June 2025

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Report Title Change of Use:
Retail to Office: 25 - 43
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Approved By John-Paul Maina

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1 Introduction

1.1 Engagement

IMPACT® have been engaged by Cremorne Properties Pty Ltd to prepare a Green Travel Plan (GTP) to supplement a parking impact assessment in support of the proposal to use the existing building at 25-43 Wangaratta Street, Richmond for the purpose of office use

1.2 Scope of Engagement

This GTP has been prepared to accompany a town planning submission. In preparing this GTP we have referenced the following:

- As Built and Proposed Change of Use Plans;
- Australian Bureau of Statistics Census Data (2016); and
- Other web-based sources as referenced in the body of this report.

2 Development Proposition

2.1 Use and Yield

The proposal seeks approval to use the existing buildings at 25-29 & Part 31-43 Wangaratta Street, Richmond for the purpose of office use. These buildings are currently permitted and / or historically been used as warehouse (with ancillary office) and office

As part of this application, the buildings are to be refurbished which will result in the following development outcome:

Address	Tenancy	Use	Area	Use	Area
		Existing			
Building 1: 25-29 Wangaratta Street	Ground Floor - Tenancy 1		413.6		413.6
	Ground Floor - Tenancy 2	Undefined	413.7	Office:	413.7
	First Floor		984		984
Building 2: 31-43 Wangaratta Street	Ground Floor - Tenancy 1	Undefined	356.7		356.7
	Ground Floor - Tenancy 2	Warehouse	444.4	Office:	444.4
	Ground Floor - Tenancy 3	Office	607.3		607.3
	First Floor	Office	1391.5		1391.5
Total Area			4611.2		4611.2

2.2 Car Parking

Minor reconfiguration of car park bays is planned.

These reconfigurations will see that one (1) car space is converted in bicycle parking spaces to service the development. Therefore, a provision of 24 spaces is planned for the development.

2.3 Bicycle Parking

A total of 17 bicycle spaces are planned, specifically:

- 14 spaces for employees, comprising of 12 within the building throughout the ground floor and 2 externally within the car parking area on Wangaratta Street, and
- Three (3) spaces for visitors, situated externally within the car parking area on Wangaratta Street.

Bicycle parking spaces are proposed in a combination of towel hitches, vertical racks and horizontal hoops.

3 Sustainable Transport Planning

3.1 Overview

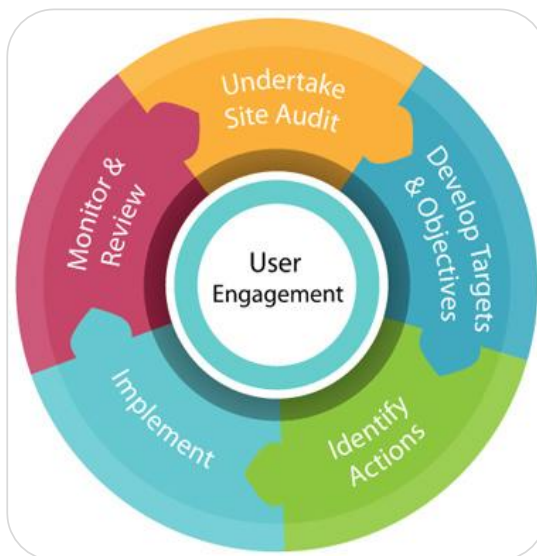
A Green Travel Plan is one of a suite of tools available to assist with Sustainable Transport Planning.

A Green Travel Plan can be defined as a package of site-specific measures implemented to promote and maximise the use of more sustainable modes of travel.

Typically, green travel plans support walking, cycling, public transport and car sharing, which are encouraged via a range of actions, promotional campaigns and incentives, and can deliver a range of benefits to employers, employees, visitors of a development by:

- Reducing the need to provide parking (reducing costs associated with providing parking and helping to create more affordable development outcomes);
- Contribute to corporate social responsibility relating to the triple bottom line, and improve corporate image as an innovative and environmentally-aware organisation;
- Help to appeal to a new generation of professionals who prioritise location and lifestyle over car ownership;
- Help to attract and retain staff (reducing costs associated with staff turnover);
- Contribute to a healthier, happier and more active workplace (reducing costs associated with sick days and an unhealthy, unproductive workforce);
- Create opportunities for healthier lifestyles and more vibrant, cohesive and accessible communities;
- Provide staff and residents with potential travel cost savings;

In preparing a Green Travel Plan, the plan should have regard to the following essential elements¹:



Site Audit and Data Collection To understand Base Conditions;

Objectives and targets To define the direction and purpose of the travel plan. Targets should be specific, measurable, achievable and time-bound;

Actions To help achieve the objectives. Should provide incentives for using sustainable transport modes;

Promoting and Marketing Communication strategy to get the message to the target audience;

Commitment of resources This would including financial support and human resources to allow for implementation, monitoring, review and continual improvement of the travel plan;

Monitoring and Review process Aimed at setting out a systematic approach to measuring the impact of the travel plan;

Governance support Includes appointment of a Travel Plan Coordinator or Committee.

¹ City of Sydney & UK Department for Transport

4 Site-Specific Transport Assessment

4.1 Location

The subject site is located on the eastern side of Wangaratta Street, Richmond as shown in the locality plan and aerial photograph represented in Figure 1. The site falls within the Richmond Station Precinct within The Swan Street Major Activity Centre area.



Figure 1 Location of Subject Site

4.2 Public Transport

The site is well connected to public transport services. This is facilitated primarily via the Richmond Railway Station located 300 metres west of the subject site.

Tram route 70 runs along Swan Street, just south of the subject site. The nearest stop associated with this route is located at the corner of Lennox Street and Swan Street, situated approximately 150 metres walking distance southeast of the subject site.

Figure 2 depicts a map of surrounding public transport options, with relevant services described at Table 1.

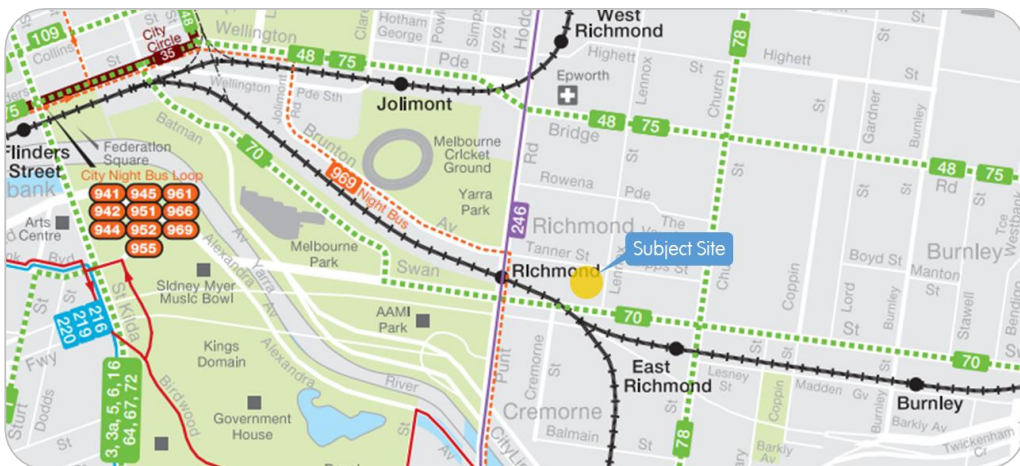


Figure 2 Public Transport Map

Table 1 Public Transport Options

Service	Route	Description	Nearest Stop	Walking Distance
Train		Alamein, Belgrave, Cranbourne, Frankston, Glen Waverley, Lilydale, Pakenham and Sandringham lines	Richmond Railway Station	300 metres (4 minutes)
Tram	48	North Balwyn - Victoria Harbour Docklands	Corner Bridge Road and Punt Road	1 kilometre (13 minutes)
	75	Etiihad Stadium Docklands - Vermont South		
	70	Waterfront City Docklands - Wattle Park	Corner Lennox Street and Swan Street	150 metres (2 minutes)
Tram	78	North Richmond - Balaclava via Prahran	Corner Swan Street and Church Street	550 metres (8 minutes)
	246	Elsternwick - Clifton Hill via St Kilda	Corner Rowena Parade and Punt Road	550 metres (8 minutes)

In recognition of the site's accessibility via public transport services, the subject site is included within the State Principal Public Transport Network Area.

The Principal Public Transport Network reflects the routes where high-quality public transport services are or will be provided.

This network is intended to support integrated transport and land use planning by encouraging more diverse and dense development near high-quality public transport to help support public transport usage.

As illustrated in Figure 3, the site has a Transit Score of 94 - Riders Paradise. This means that the site has access to World Class Public Transport².

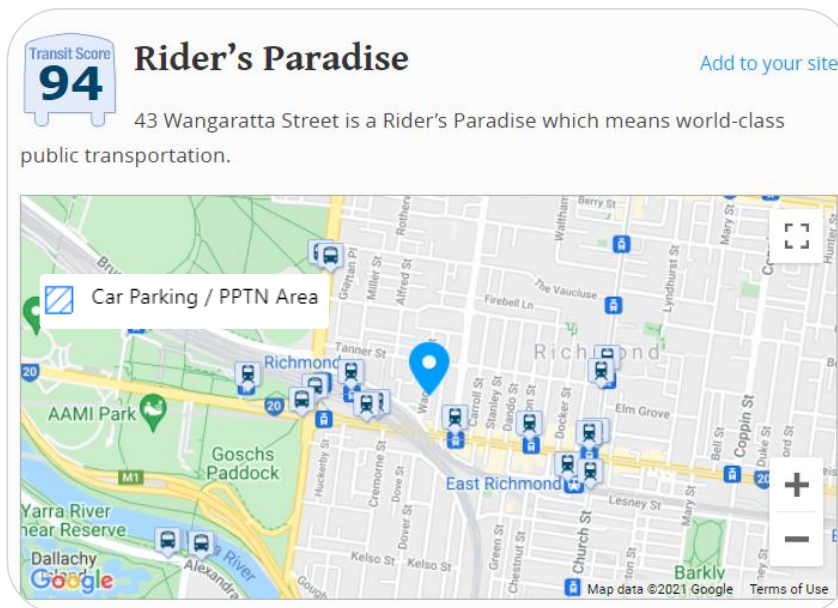


Figure 3 Transit Score Results

As demonstrated in the catchment plan at Figure 4, these public transport services can be leveraged to gain access to a significant area of Metropolitan Melbourne (within 30 minutes).

² Walkscore: <https://www.walkscore.com/score/43-wangaratta-st-richmond-vic-australia>

Change of Use:
Retail to Office: 25 - 43 Wangaratta Street, Richmond

Green Travel Plan

23 June 2025

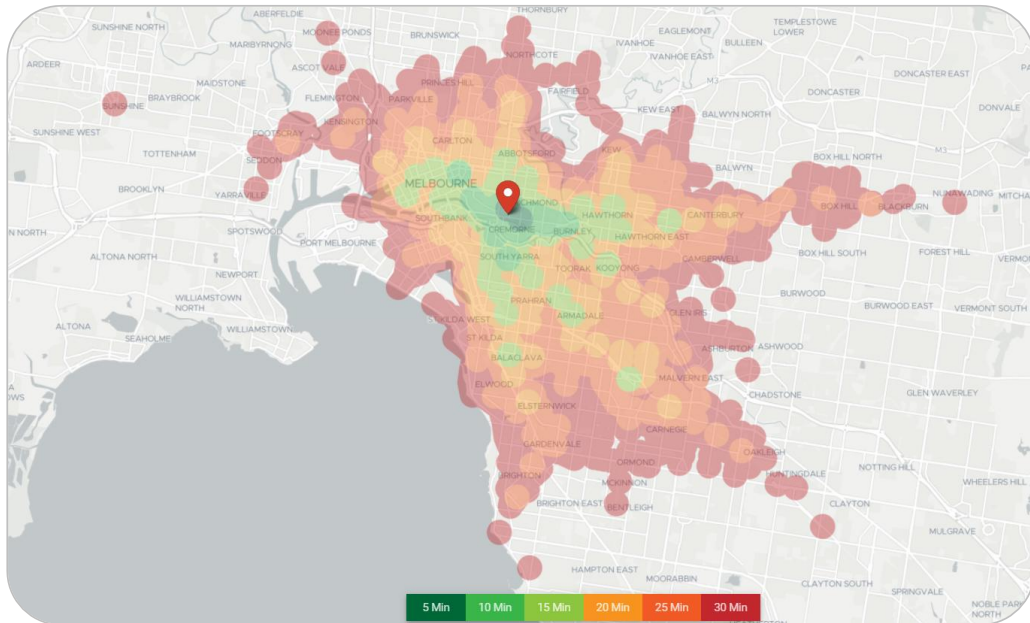


Figure 4 30-Minute Cycling Catchment

The catchment plan³ shows that within 30 minutes, a commuter can travel from the site to:

- North Thornbury / Moonee Ponds
- South Hampton East / Clayton
- West Yarraville / Seddon
- East Nunawading

4.3 Cycling

Cycling Corridors as illustrated at Figure 5.

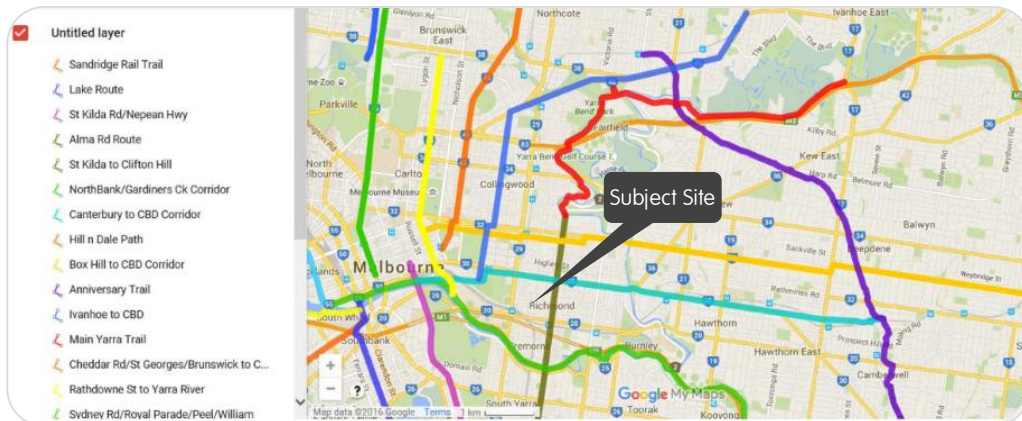


Figure 5 Bicycle Network Plan

[3 https://www.route360.net/](https://www.route360.net/)

These corridors as demonstrated above and in the Strava Heat Map⁴ provided at Figure 6 provide cyclists with connections to cycling paths and trails throughout the Metropolitan Melbourne area.



Figure 6 Strava Heatmap

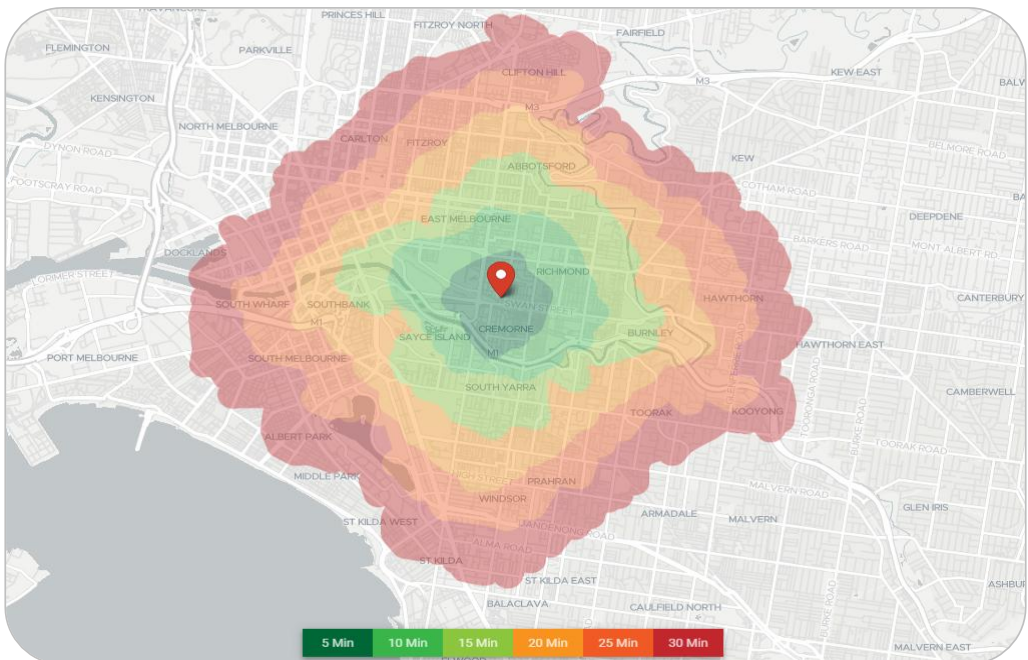


Figure 7 30-Minute Cycling Catchment

The catchment plan⁵ shown in Figure 7 indicates that within 30 minutes, a cyclist can travel from the site to:

- North Fitzroy North
- South St Kilda East
- West Docklands
- East Hawthorn East

⁴ Visualization of public data from users of the Strava application.

⁵ <https://www.route360.net/>

4.4 Walking

Adequate pedestrian access to the site is provided in the form of sealed pedestrian footpaths along Botherambo Street, Wangaratta Street and surrounding roads.

For further context, a Walk Score has been derived that indicates the site is both a 'Walker's Paradise' (i.e. daily errands do not require a car) and 'Rider's Paradise' (i.e. offers world-class public transportation options) with scores of 98 and 96 out of 100 achieved for both categories, respectively.

This is reflective of the site's proximity to everyday needs such as public transport, cafes/restaurants and other day-to-day activities.

This network of footpaths and trails provide an extensive 30-minute walking catchment to the site, as demonstrated in Figure 8.

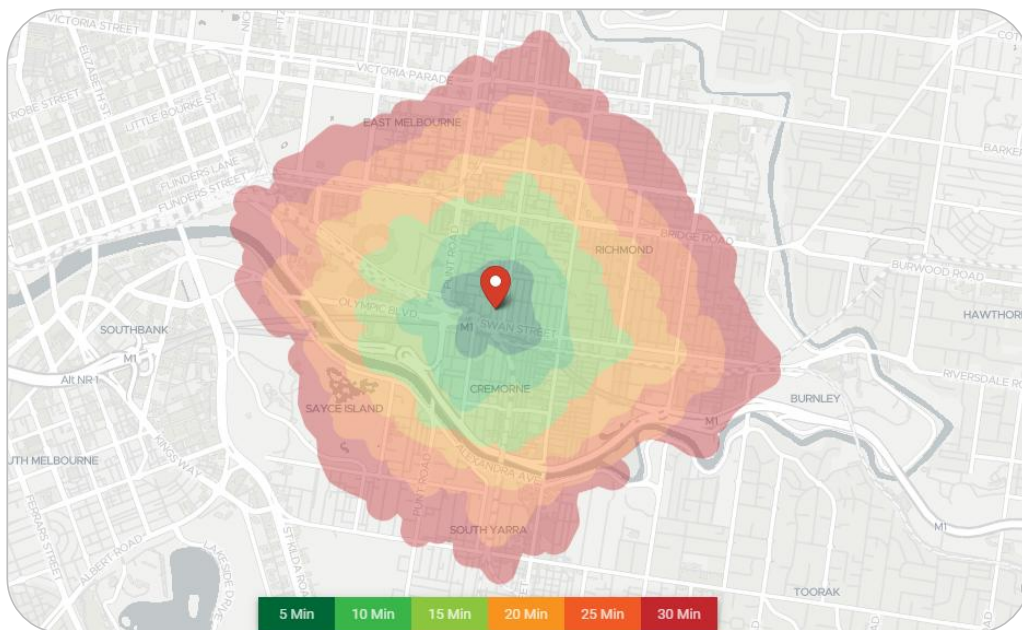


Figure 8 30-Minute Walking Catchment

The catchment plan shown above indicates that within 30 minutes, a pedestrian can travel from the site to:

- North Abbotsford
- South South Yarra
- West Melbourne CBD
- East Burnley

4.5 Car Share Facilities

Car sharing services provide cars on demand, for rent either by the hour or by the day.

To use the service, one must register with a car share company, pay a fee and book a car either by phone or on the internet. Cars are picked up and dropped off at designated car share bays.

This service is provided as an ideal choice for people living or working in inner city locations where other modes of transport such as walking, cycling, trams and buses are available. It is also considered a suitable option for people who don't need a car every day or want to get rid of a second car.

The subject site is located within a short walk to car sharing services operated by Flexi Car, GoGet and Greenshare Car as summarised at Table 2

Table 2 Car Share Availability

Company	Location	Walking Distance
Flexicar	Corner Lennox Street and Gipps Street	170 metres / (2 minutes)
	Corner Lennox Street and Richmond Terrace	350 metres / (5 minutes)
	Corner Swan Street and Church Street	600 metre / (8 minutes)
	Corner Cremorne Street and Gough Street	750 metres / (9 minutes)
GoGet	Corner Lennox Street and Swan Street	210 metres / (3 minutes)
	Corner Lennox Street and Gipps Street	170 metres / (2 minutes)
	Corner Cremorne Street and Jessie Street	300 metres / (4 minutes)
	15 Swan Street (CarePark)	350 metres / (4 minutes)
	Swan Street and Docker Street	450 metres / (6 minutes)
	Tanner Street Car Park	300 metres / (4 minutes)
GreenShareCar	Swan Street and Docker Street	450 metres / (6 minutes)

5 Green Travel Plan

5.1 Objectives

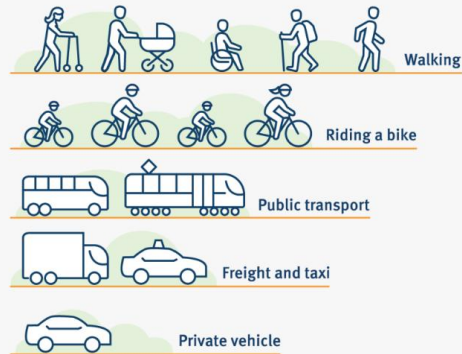
A Green Travel Plan (GTP) sets out measures that aim to manage car dependency and encourage the use of sustainable transport modes, with the objective of permanently changing travel mode behaviour.

Specifically, a GTP seeks to encourage people to adopt a sustainable transport mindset with a preference for:

- Walking;
- Cycling;
- Public transport;
- Carpooling / ride share services

In lieu of single occupant car trips.

Sustainable transport hierarchy



5.2 Targets

The goal of this GTP is to achieve a shift towards sustainable transport modes (from private motor vehicle).

To inform considerations relating to targets, reference has been made to the existing travel patterns for people working within the nearby area, as summarised in Table 3.

Table 3 Journey to Work Data (Based on place of work) - 2016 Census Data

% Mode of Travel for 'journey to work' trips	Work within City of Yarra	Work within Inner Melbourne (average)
Car (as driver)	56%	52%
Car (as passenger)	3%	3%
Public Transport	27%	34%
Cycling	5%	3%
Walking	7%	5%
Other Transport	2%	3%

This data indicates that despite its inner-city location, there is still a heavy reliance on private motor vehicle trips to and from work within the City of Yarra.

The buildings at 25-43 Wangaratta Street, Richmond have a total NLA of approximately 4,400 square metres.

Conservatively assuming an occupancy rate of 1 employee per 10 sq.m it is anticipated that there will in the order of 440 employees on-site when development is tenanted and fully operational.

With a total of 24 on-site car parking spaces, this equates to a maximum of 5.5% of employee vehicle trips which can be taken by a car (as a driver).

The subject site is well connected to public transport services (train and bus) as well as Melbourne's extensive bicycle network. It is therefore suggested that the following targets are set for this development:

Staff

- 94.4 of commuter trips (staff) to/from the site be undertaken by sustainable transport modes
 - Mode share of at least 85% for trips by public transport;
 - Mode share of at least 4.4% for trips by bicycle; and

- Mode share of at least 5% for walking tips.

It is noted that the above targets should be seen as aspirational aims as opposed to strict 'targets', with the various initiatives promoted within this GTP designed to assist in achieving the aims.

To assist with meeting these targets, the following design and operational features / opportunities will be leveraged by the building manager, responsible for coordination and implementation.

5.3 Design Features

5.3.1 Cycling Facilities

The development has a statutory requirement to provide 10 bicycle spaces.

To encourage cycling as a mode of transport, the development contemplates 17 bicycle parking spaces.

The provision of secure, accessible and convenient cycling facilities in excess of the statutory requirements will contribute positively to encouraging cycling as a mode of transport.

5.3.2 Car Parking

To manage car dependency and encourage the use of sustainable transport modes, only 24 spaces are planned on-site.

The under provision of on-site car parking will contribute positively to efforts to support sustainable transport alternatives to the motor car.

5.4 Operational Initiatives

As detailed in the draft action plans provided within Appendix A and in subsequent sections of this report, various operational strategies could be implemented to encourage the use of non-private motor vehicles.

The draft action plan included in this report is an outline of the actions and incentives that could be undertaken / provided and is subject to agreement by the building manager and Council.

The suggested operational strategies are outlined in the following sections of this GTP.

As detailed in the outline action plans provided within Appendix A and in subsequent sections of this report, various operational strategies would be implemented to encourage the use of sustainable transport.

The outline action plan included in this report sets out actions and incentives that should be undertaken / provided.

The operational strategies should include:

5.4.1 Sustainable Transport - Information Pack

To inform staff of alternative travel options and encourage their use, the tenants are to provide a Sustainable Transport Pack to staff that will include:

- Information about the development (design features relating to bicycle facilities, and allocation of car parking facilities);
- City of Yarra TravelSmart Map

This Travel Smart Map (included as Appendix C) provides baseline details relating to:

- Bicycle and Pedestrian accessibility.
- Public Transport, including Information on how and where to purchase/top-up Myki, including Mobile Myki;

- o Services, including shopping, recreational and educational with suitable non-car based transport options;
- o Links to relevant Green Travel, public transport and local services websites.

Note: To ensure staff have access to current, and real time sustainable transport data relating to tram, train and bus timetables, and location of existing and proposed share car schemes, bicycle parking facilities, tram stops, taxi ranks, railway stations, bus stops and bicycle paths, the tenants will be required to install a tablet / ipad or similar in the staff break rooms.

This device would be pre-programmed with sustainable transport applications such as PTV app / Tram Tracker app and other resources and shall serve as the sustainable transport hub for up to date and real time sustainable transport information.

5.4.2 Creating and sustaining an active transport (walking / cycling) culture:

The Building Manager with the assistance of nominated staff will form a committee whose purpose is to create and sustain an active transport culture (cycling and walking).

Given that the site is well connected to the bicycle network, and ample bicycle parking and end of trip facilities are provided, the committee would seek to create and sustain membership to initiatives such as, Bicycle User Groups (BUG's).

These groups should include a Buddy Group where experienced cyclists buddy a novice to ride the best / safest route to the site.

5.4.3 Discouraging Single Occupant Motor Vehicle Travel

1. Provide sustainable transport allowances for staff. This should take the form of:

— **Public Transport:**

- a. Tenants will be required to sign up to the **myki Commuter Club**. This would allow staff members to access a 10% discount in tickets. This can be done independently, or via a salary package scheme.

— **Cycling:**

- a. Staff that live locally or that are amenable to cycling as a mode of travel to and from work will be invited to participate in a salary package scheme, where the employer and or building manager leverages scales of economy and purchases commuter bicycles on their behalf at a reduced cost.

2. Set up a car pooling database. This should take the form of:

- The building manager or respective tenants will maintain a database that actively fosters a culture that allows staff that live in the same locality to connect and car pool. To encourage this outcome, a number of the on-site parking spaces provided for staff should be allocated to the benefit of carpooling staff.

5.4.4 Strategic Marketing

5.4.4.1 New and Existing Staff

Staff should be introduced and immersed into sustainable transport through regular induction programs.

These induction sessions should provide an opportunity where staff who have not yet established sustainable travel habits receive information on efficient and cost-effective transport options.

5.4.4.2 Promotional Functions / Events

The Building Manager could maintain an ongoing focus on promotional functions aimed at raising awareness, motivating people to try alternatives to the car and rewarding people who travel by methods other than the car, and to reach people that may not be receptive to marketing or who are not active information seekers.

These promotions could include but are not limited to walk to work day, ride to work day, or other events specific to the needs of the centre.

It is worth noting however that travel behaviour change programs are an ongoing process and cannot strictly be broken into short term and long term initiatives.

Note:

The coordination, implementation and funding for the Green Travel Plan will be the responsibility of the Building Manager with the intent to achieve:

- Reductions in car usage (particularly single occupancy journeys) and increased use of public transport, walking and cycling;
- Increase awareness of sustainable transport options to new and ongoing staff and visitors;
- Implement travel/behaviour change strategies and use surveys to monitor changes in attitudes and reported behaviour.

To determine if targets are being met (and establish a modal split for the development), a monitoring process will need to be implemented. This process will establish travel patterns, mode splits and distances travelled by occupants of the site.

Where targets are not being met, strategies could be tailored as required to focus on these discrete areas.

6 Monitoring Program

In order to monitor the success of the Green Travel Initiatives, a 3 stage monitoring process should be implemented.

Elected staff representatives would be assigned as Travel Co-ordinators of the Travel Plan. The co-ordinators would be responsible for:

- Organising the monitoring of the performance of the Travel Plan against the targets that have been agreed.
- Reviewing the occupancy / use (and abuse) of the facilities that are provided on site, for example, cycle racks and motorcar parking spaces, clothes lockers, and travel information etc.
- Reviewing the operational success or failure of the plan and produce a review report highlighting successful initiatives and also areas that require improvement. This review report would form the platform of an updated Travel Plan.

The suggested 3 stage monitoring program is outlined below:

6.1 Stage 1

Stage 1, would involve a questionnaire survey of staff at the onset of the initiative. The survey will be useful to collect information on the travel characteristics of the occupants of the site and assist in gauging interest in the various Green Travel initiatives and to seek ideas for other Green Travel initiatives.

A pro forma for the Stage 1 questionnaire is shown attached as Appendix B.

6.2 Stage 2

Stage 2, would involve a questionnaire and feedback form to be filled out by Staff 3 months after occupation, in order to determine what Green Travel initiatives are working and which are not.

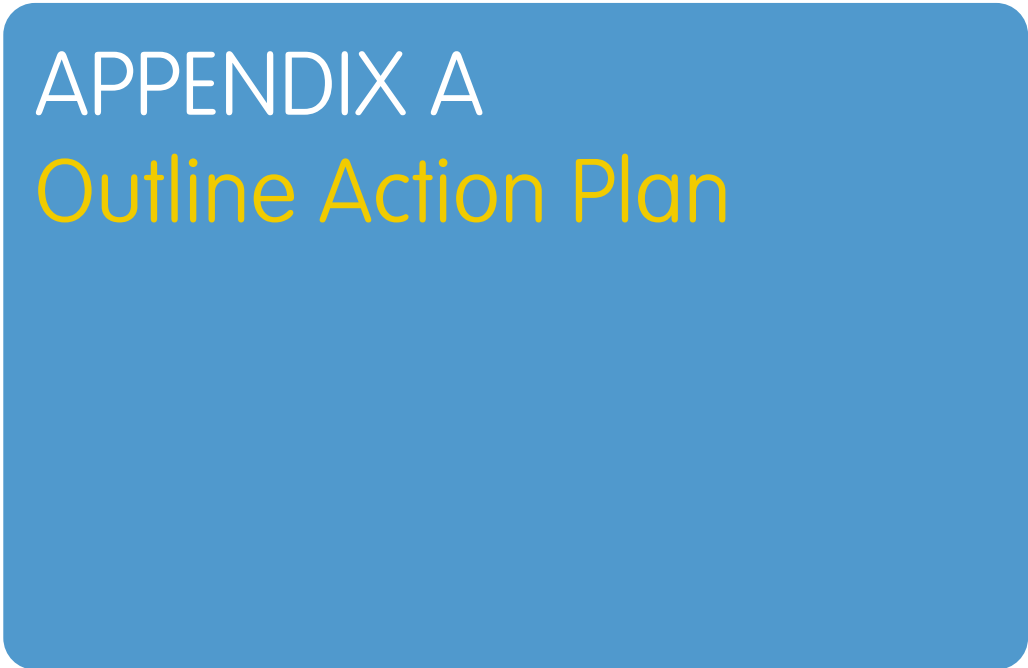
A pro forma for the Stage 2 questionnaire is shown attached as Appendix B

6.3 Stage 3

Stage 3, would be the monitoring component of the plan which would be undertaken 6-12 months after occupation. This questionnaire would test the success rate of the various initiatives and help rework programs to suit the needs of staff, visitors and parents.

A pro forma for the Stage 3 questionnaire is shown attached as Appendix B.

It is expected that the above stages would form the basis of a continuous monitoring program to gauge the effectiveness of the travel plan.



APPENDIX A

Outline Action Plan

- Provided as a Guide to Inform Discussions amongst the Building Manager / Tenants and Appointed Transport Committee
- Nominated Budgets are Indicative Only & Subject to Building Manager / Tenants and Appointed Transport Committee Input.

GENERAL ACTIONS

Operational Initiatives

Promotions including:

- An events calendar: 3-4 events per year. Best coordinated in conjunction with state wide events such as Ride to Work Day, World Environment Day, National Walk to Work Day.
- Plan for lunch, morning teas or breakfasts, guest speakers, demonstrations etc;

Timeline	Estimated Budget	Responsibility
Ongoing	Included in below estimates	Building Manager & Tenants

Cycling

Operational Initiatives

Direct staff to access to up to date route maps of bicycle networks (on road and off road) to assist with journey planning. These could include:

1. Cycling guides provided by Council;
2. Google Maps; or

Propriety applications such as Strava

Direct staff to access to up to date regulations, requirements and cycling etiquette of use to the bicycle network infrastructure. These could include:

1. Rules on riding on shared paths;
2. Rules on riding on the road.

A valuable resource for this information would be www.bicyclenetwork.com.au

For new cyclists, provide the opportunity for staff to participate in a salary package scheme, where the building manger / tenant leverages scales of economy to negotiate and purchase commuter bicycles on their behalf at a reduced cost.

Form a committee whose purpose is to create and sustain an active transport culture. This could take the form of creating and sustaining initiatives such as:

1. Bicycle User Groups (BUG's). These groups could include a Buddy Group where experienced cyclists buddy a novice to ride the best / safest route to the site.
2. Participation in events such as Ride to Workday, World Environment Day etc, including ancillary activities such as morning teas or breakfasts, guest speakers, demonstrations etc;
3. Negotiate a servicing discount for staff at a local bike shop or sports gear store

Offer personalised travel advice to interested staff via journey planners or relevant TravelSmart Guides

Timeline	Estimated Budget	Responsibility
Ongoing	\$1,500 / Year	Building Manager & Tenants

Walking

Operational Initiatives

Encourage walking through participation in events such as Walktober

Set up walking groups that encourage lunchtime walks or walking to or from work or train station

Run a workplace challenge to encourage active travel e.g. staff walking challenge using pedometers so people can keep count of steps walked, graphs or charts of staff progress for healthy competition

Access to up to date route maps of walking networks to assist with journey planning. These could include:

1. Cycling guides provided by Council;
2. Google Maps; or

Propriety applications such as Strava



Change of Use:
Retail to Office: 25 - 43 Wangaratta Street, Richmond

Green Travel Plan

23 June 2025

Provide umbrellas and wet weather gear for staff that walk to work or in reception for use during day time hours.

Offer personalised travel advice to interested staff via journey planners or relevant TravelSmart Guides

	Timeline	Estimated Budget	Responsibility
	Ongoing	\$1,500 / Year	Building Manager & Tenants
Public Transport			
Operational Initiatives			

Direct Staff to access up to date public transport route & timetable data in the locality via:

1. Application such as PTV's Journey planner.
2. Google Maps;

Provide access to Tram, train and bus timetables in prominent locations within the building and break rooms. This could include leveraging digital platforms via tablet devices that are located in break out rooms that are pre-loaded with applications such as PTV's Journey planner.

For new public transport users, provide the opportunity for staff to secure discounted fares. This could be done by the Building Manager / Tenant signing up to the **myki Commuter Club**. This would allow the centre access to yearly myki passes for their staff at a 10% discount. These discounts could be passed on to their staff for purchase, including via a salary package scheme

Create a committee whose purpose is to create and sustain a public transport culture. This could take the form of creating and sustaining initiatives such as:

- Lunchtime talks on public transport education to promote what is available and service changes where available

Provide information on how to purchase Myki's, associated costs, and latest technology such as Myki Pass - digital myki on Android phones

Offer personalised travel advice to interested staff via journey planners or relevant TravelSmart Guides

	Timeline	Estimated Budget	Responsibility
	Ongoing	\$1,500 / Year	Building Manager & Tenants
Cars			
Operational Features			

Form a committee whose purpose is to create and sustain behaviour change in motorists. This could take the form of creating and sustaining initiatives such as:

1. Organise a car pool postcode morning tea to match up potential car share partners
2. Set up a ride-matching system on a board in the staff room so staff interested in carpooling can link up
3. Provide a car pool guide to staff with hints on how to successfully car pool with colleagues, as well as some basic guidelines to follow.
4. Educate staff of the cost savings & environment benefits of active transport in comparison with car ownership & travel;

Provide recognition for those that use active and sustainable travel at an event or in a newsletter

Offer personalised travel advice to interested staff via journey planners or relevant TravelSmart Guides

	Timeline	Estimated Budget	Responsibility
	Ongoing	\$1,500 / Year	Building Manager & Tenants

Change of Use:
Retail to Office: 25 - 43 Wangaratta Street, Richmond

Green Travel Plan

23 June 2025

APPENDIX B

Questionnaire Surveys

Change of Use:
Retail to Office: 25 - 43 Wangaratta Street, Richmond

Green Travel Plan

23 June 2025

Questionnaire

Green Travel Survey

Stage 2

Date.....

1 Are you a member of Staff, or Customer/Visitor?

Staff
Visitor

2 How do you travel to and from the site?

Bicycle
Tram
Bus
Car as Driver

Walk
Train
Motorcycle
Car as Passenger

3 If you travel by car, either as driver or passenger, have you considered any of the sustainable travel information provided to you regarding alternative travel modes?

4 Has this information been useful and what additional information would you require to allow you to consider alternative transport options?

Thank you for your feedback.



Change of Use:
Retail to Office: 25 - 43 Wangaratta Street, Richmond

Green Travel Plan

23 June 2025

Questionnaire

Green Travel Survey

Stage 3

Date.....

1 Are you a member of Staff, or Customer/Visitor?

Staff	<input type="checkbox"/>
Visitor	<input type="checkbox"/>

2 How do you travel to and from the site?

Bicycle	<input type="checkbox"/>	Walk	<input type="checkbox"/>
Tram	<input type="checkbox"/>	Train	<input type="checkbox"/>
Bus	<input type="checkbox"/>	Motorcycle	<input type="checkbox"/>
Car as Driver	<input type="checkbox"/>	Car as Passenger	<input type="checkbox"/>

3 Has this mode of travel changed since you first came to the site, and if so what influenced the change?

4 What reasonable improvements can be made to enhance the appeal of alternative transport options to the site.

Thank you for your feedback.



—

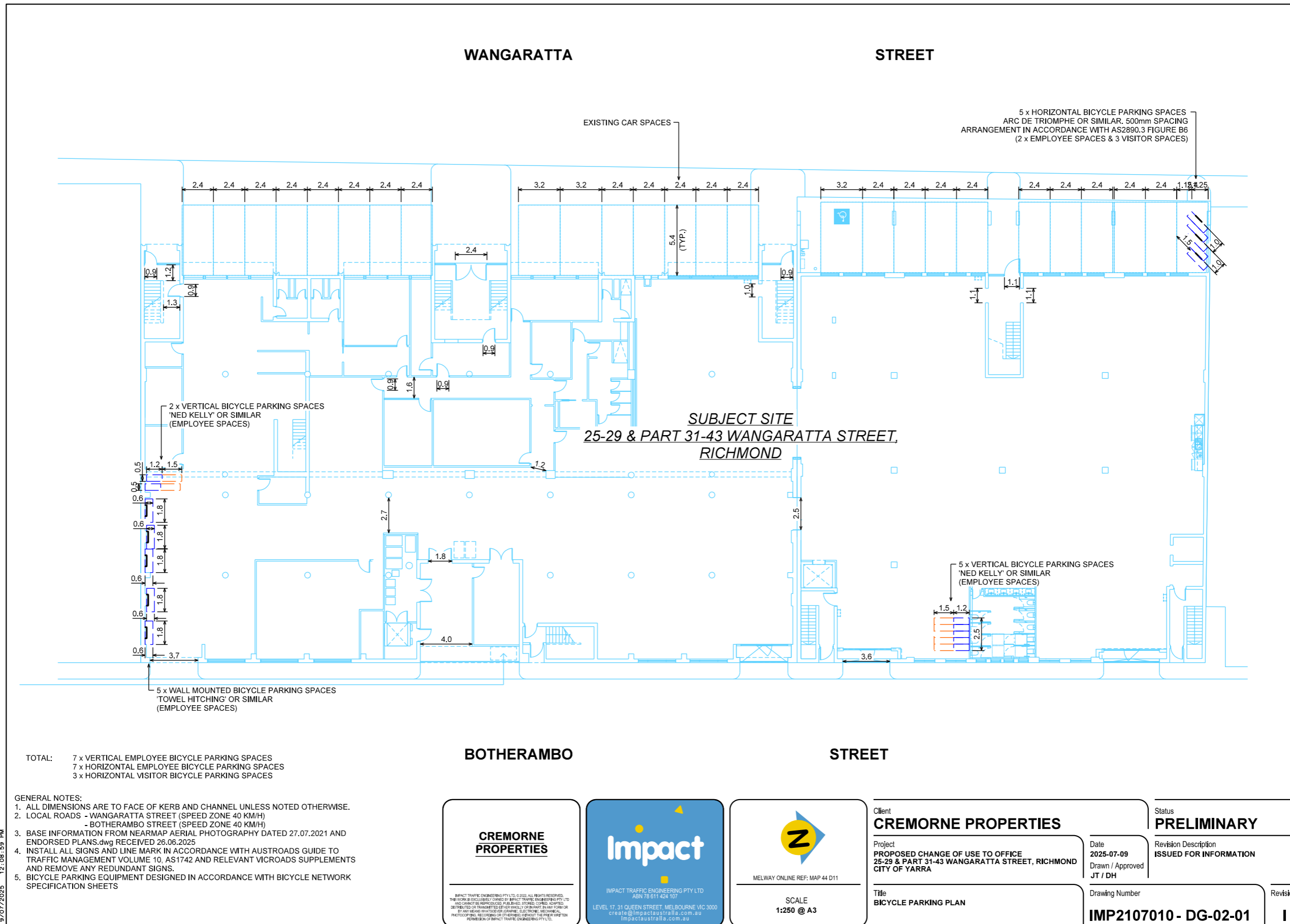
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APPENDIX B

Site Layout Plan

—

Car Park & Bicycle Parking Design



—

—

APPENDIX C

Bicycle Parking - Specification Sheets

-
- Bicycle Network - Ned Kelly
 - Bicycle Network - Towel Hitching
 - Bicycle Network - Arc De Triomphe

Towel Hitching™



Zinc finish

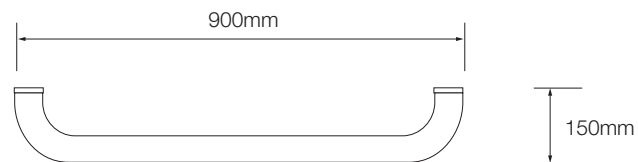
Features



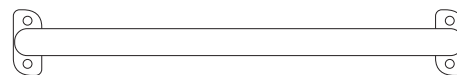
The Towel Rail is a space-efficient solution for parking single bikes against a wall, useful in narrow corridors.

- Space-efficient, can be fitted to narrow corridors
- Supplied in Zinc finish
- Accommodates all types and sizes of bicycle
- Is easy to use with any bike lock
- Requires no lifting

Dimensions



TOP



SIDE

Specifications

Material options

- Zinc finish

Fixing options

- Welded flange - Bolt On

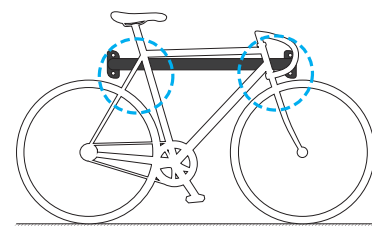
Recommended fasteners

- Zinc plated dynabolts (M8 x 40mm)

Dimensions

900mm [w] x 150mm [d]

Locking Points



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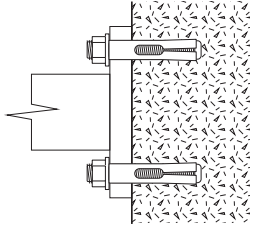


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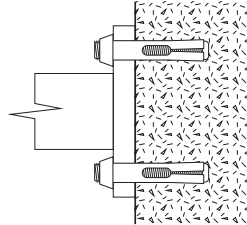
Bicycle Network ABN 41 026 835 903
 p. 1300 727 563 e. parking@bicyclenetwork.com.au bikeparking.com.au
 VIC Level 4, 246 Bourke Street, Melbourne VIC 3000 NSW 234 Crown Street, Darlinghurst NSW 2010
 TAS 210 Collins Street, Hobart TAS 7000 NT Suite 5, 18-20 Cavenagh Street, Darwin 0800

Fixing options

Fix to a wall using 4x fasteners or Shear Nuts

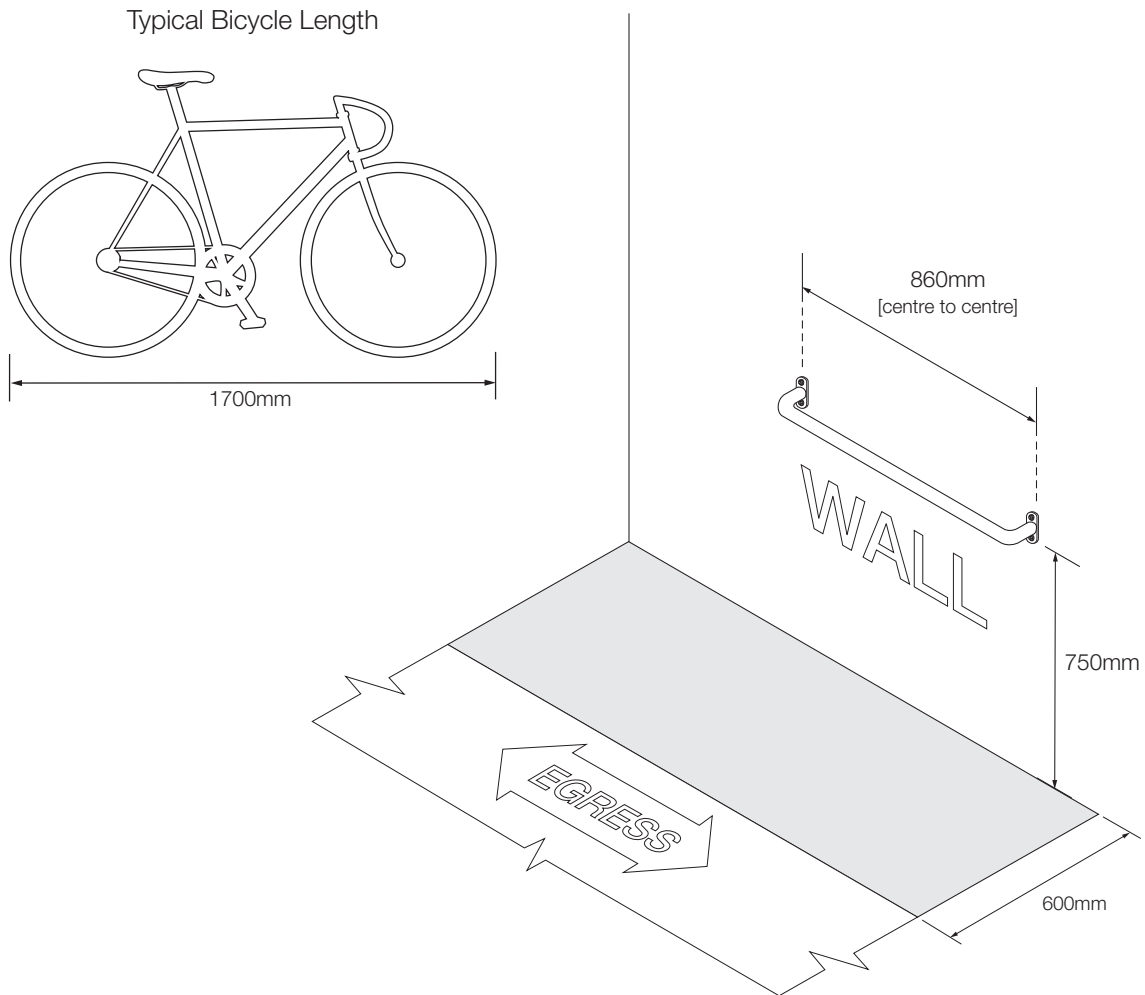


Shown with M8 x 40mm fastener



Shown with M8 x 40mm Shear Nuts

Layout guidelines



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Towel Hitching™



Zinc finish

Features



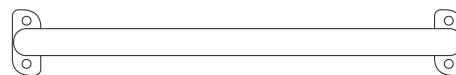
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- Space-efficient, can be fitted to narrow corridors
- Supplied in Zinc finish
- Accommodates all types and sizes of bicycle
- Is easy to use with any bike lock
- Requires no lifting

Dimensions



TOP



SIDE

Specifications

Material options

- Zinc finish

Fixing options

- Welded flange - Bolt On

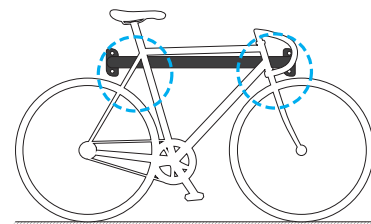
Recommended fasteners

- Zinc plated dynabolts (M8 x 40mm)

Dimensions

900mm [w] x 150mm [d]

Locking Points



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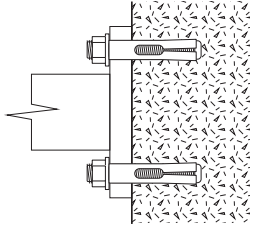


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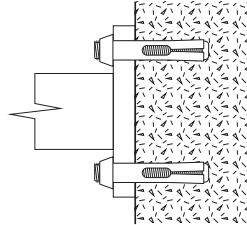
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Fixing options

Fix to a wall using 4x fasteners or Shear Nuts

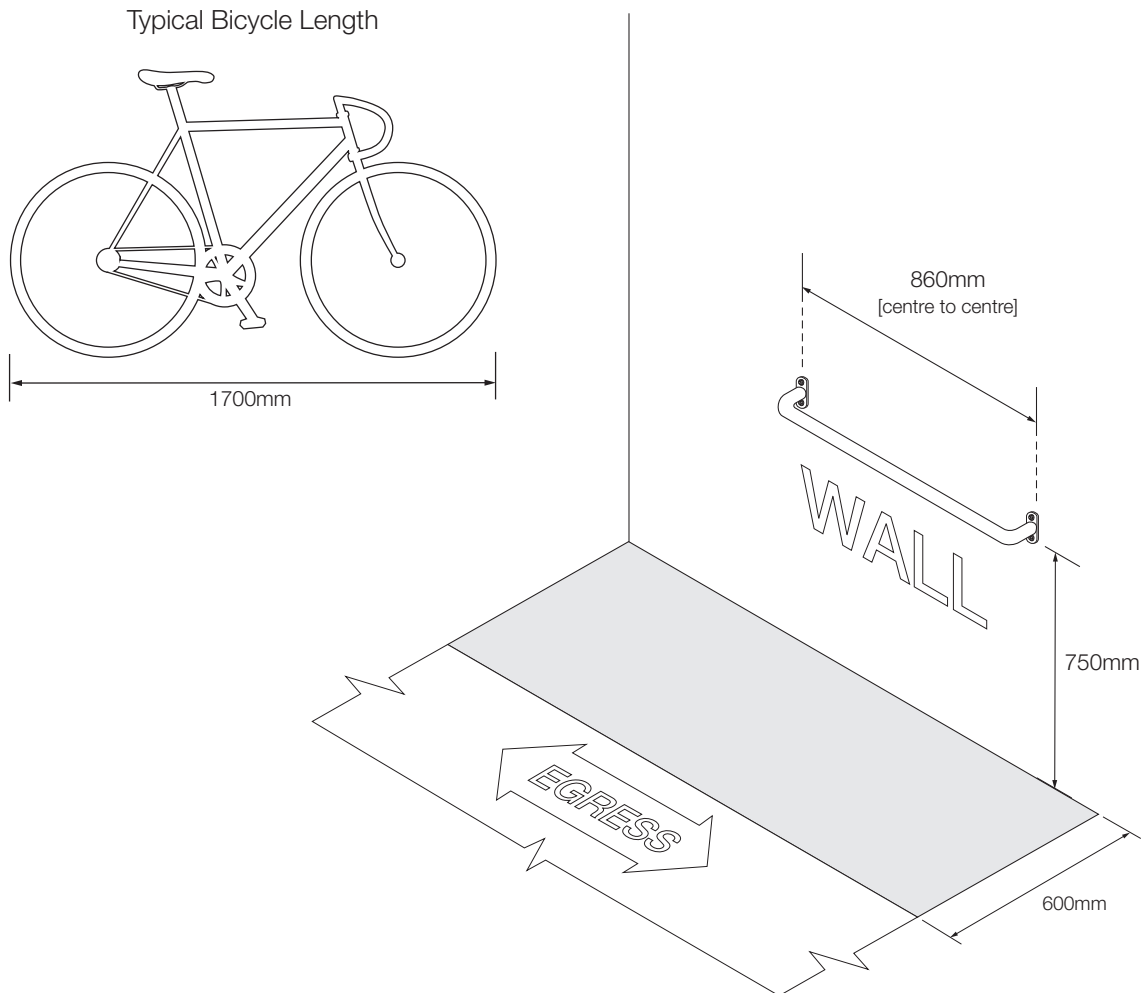


Shown with M8 x 40mm fastener



Shown with M8 x 40mm Shear Nuts

Layout guidelines



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Arc de Triomphe™



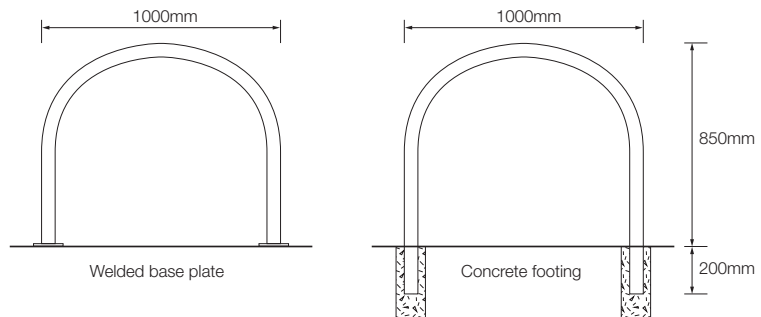
Galvanised finish / Stainless Steel finish

Features



- Each rail supports two adult bikes in an upright position
- Can be either bolted to a concrete slab or concreted in situ
- Available in stainless steel or galvanised steel
- Provides the ability to lock both wheels and frame
- Suitable for foyers and entry areas

Dimensions



Specifications

Material options

- Galvanised (Duragal)
- 316 Marine grade stainless steel

Fixing options

- Welded flange - Bolt on
- In situ

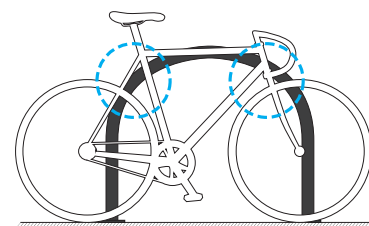
Recommended fasteners

- Galvanised Dynabolts (M10 x 65mm)
- Stainless Dynabolts (M10 x 65mm)
- Shear Nut security fasteners

Dimensions

1000mm [w] x 850mm [h]

Locking Points



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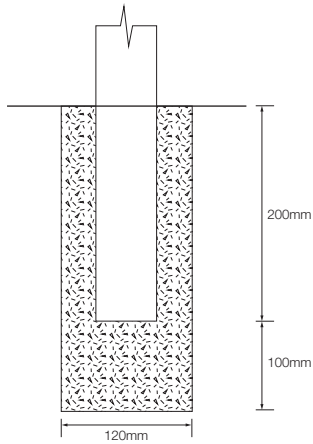


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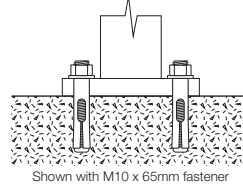
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Fixing options

In situ (Concrete footing)

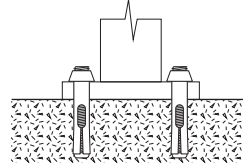


Welded flange (Bolt on) using 4 (total) x fasteners



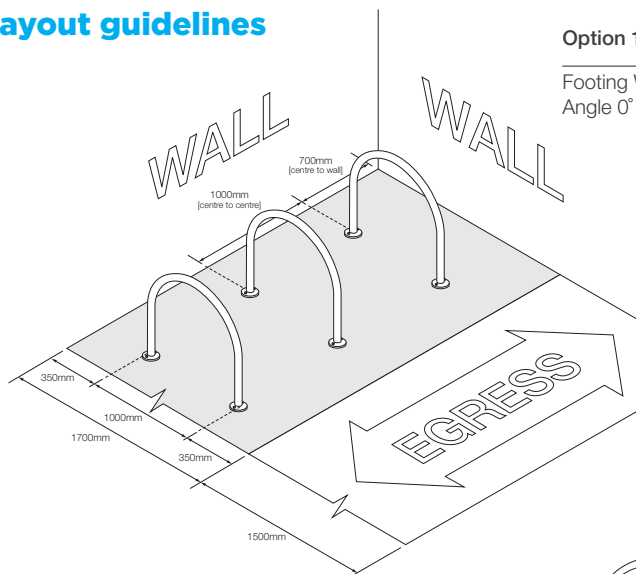
Shown with M10 x 65mm fastener

Welded flange (Security heads) using 4 (total) x fasteners



Shown with M10 x 65mm Shear Nuts

Layout guidelines

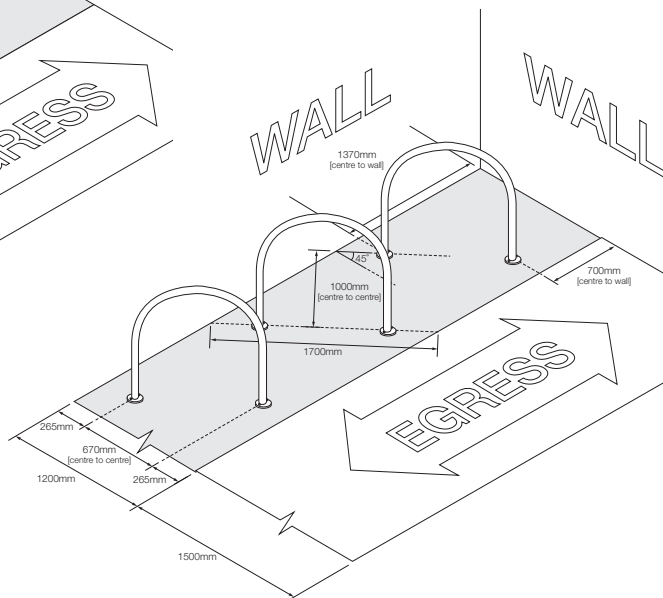


Option 1:

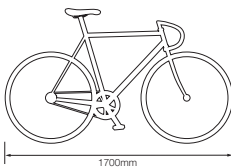
Footing Width 1700mm
Angle 0°

Option 2:

Footing Width 1200mm
Angle 45°



Typical Bicycle Length

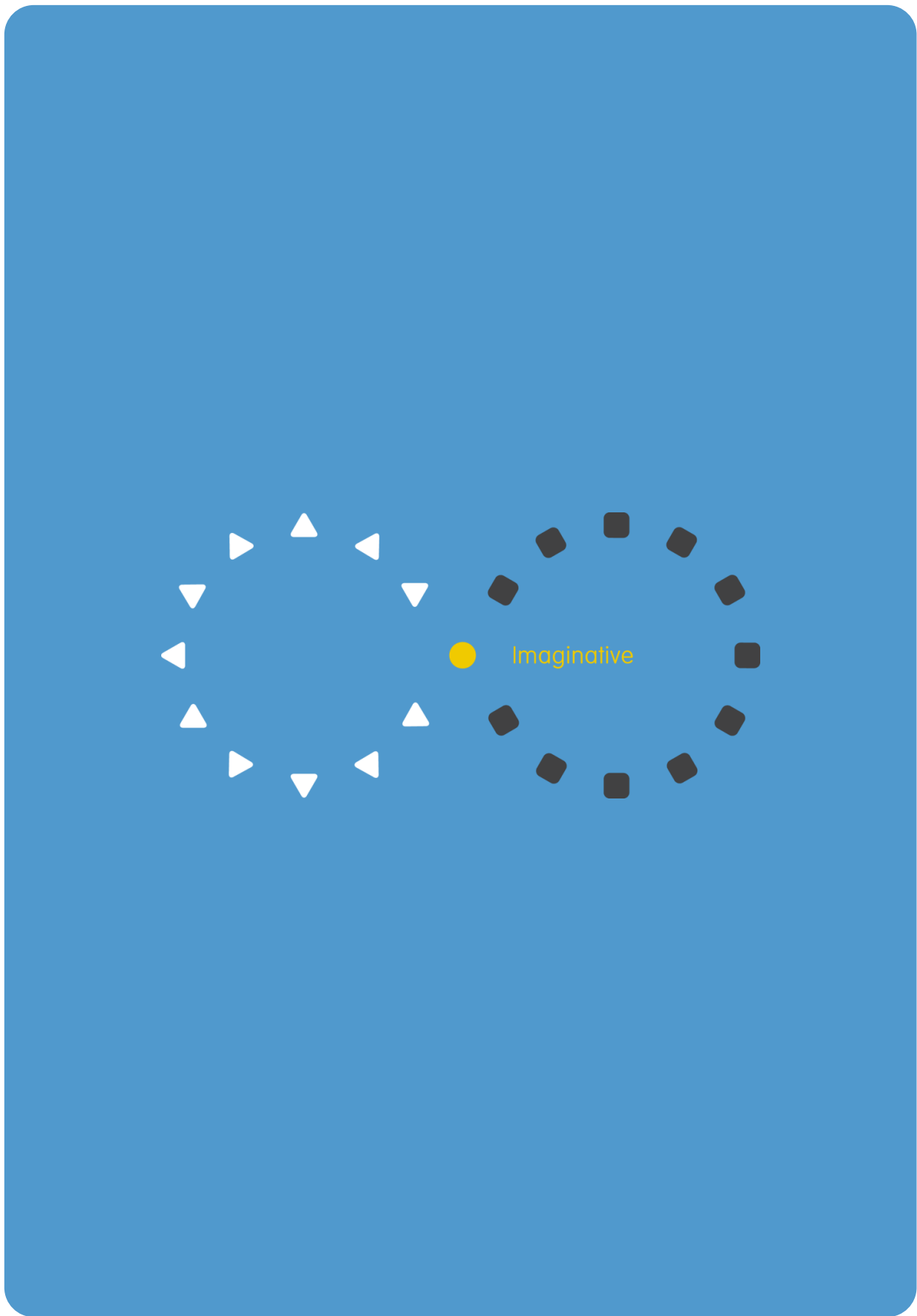


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6. DTP Applications

Nil.

7. Confidential Planning Decisions Committee Reports

Nil.