



Agenda

Planning Decisions Committee Meeting

6:30 pm, Tuesday 23 September 2025

Fitzroy Town Hall

www.yarracity.vic.gov.au

Planning Decisions Committee

The Planning Decisions Committee is a delegated committee of Council with full authority to make decisions in relation to planning applications and certain heritage referrals.

Addressing the Committee

Planning Decisions Committee meetings are decision making forums and only Councillors have a formal role. However, Council is committed to ensuring that any person whose rights will be directly affected by a decision of Council is entitled to communicate their views and have their interests considered before the decision is made.

There is an opportunity for both applicants and objectors to make a submission to Council in relation to each matter presented for consideration at the meeting.

Before each item is considered, the meeting chair will ask people who have registered to address the committee, to come forward to the microphone, state your name clearly for the record and:

- Speak for a maximum of five minutes;
- Direct your submission to the chair;
- Confine your submission to the planning permit under consideration;
- If possible, explain your preferred decision in relation to a permit application (refusing, granting or granting with conditions) and set out any requested permit conditions and avoid repeating previous submitters;
- Refrain from asking questions or seeking comments from the Councillors, applicants or other submitters;
- If speaking on behalf of a group, explain the nature of the group and how you are able to speak on their behalf.

Once you have made your submission, please remain silent unless called upon by the chair to make further comment or to clarify any aspects.

Councillors will then have an opportunity to ask questions of submitters. Submitters may determine whether or not they wish to take these questions.

Once all submissions have been received, the formal debate may commence. Once the debate has commenced, no further submissions, questions or comments from submitters can be received.

Arrangements to ensure our meetings are accessible to the public

Access to the building is available either by the stairs or lift. Seating is provided to watch the meeting, and the room is wheelchair accessible. Accessible toilet facilities are available. Speakers at the meeting are invited to be seated at the table to address the Council, and all participants are amplified via an audio system. Meetings are conducted in English.

If you are unable to participate in this environment, we can make arrangements to accommodate you if sufficient notice is given. Some examples of adjustments are:

- a translator in your language;
- the presence of an Auslan interpreter;
- loan of a portable hearing loop; and
- reconfiguring the room to facilitate access.

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1. Acknowledgement of Country

“Yarra City Council acknowledges the Wurundjeri Woi Wurrung people as the Traditional Owners and true sovereigns of the land now known as Yarra.

We acknowledge their creator spirit Bunjil, their ancestors and their Elders.

We acknowledge the strength and resilience of the Wurundjeri Woi Wurrung, who have never ceded sovereignty and retain their strong connections to family, clan and country despite the impacts of European invasion.

We also acknowledge the significant contributions made by other Aboriginal and Torres Strait Islander people to life in Yarra.

We pay our respects to Elders from all nations here today—and to their Elders past, present and future.”

2. Attendance, apologies and requests for leave of absence

Attendance

Councillors:

- Cr Stephen Jolly Mayor
- Cr Sarah McKenzie Deputy Mayor
- Cr Evangeline Aston
- Cr Andrew Davies
- Cr Kenneth Gomez
- Cr Sharron Harrison
- Cr Meca Ho
- Cr Sophie Wade

Absent

- Cr Edward Crossland Parental Leave

Council staff:

- Mary Osman General Manager City Sustainability and Strategy
- Danielle Connell Manager Statutory Planning (Acting)
- Joe Byrne Senior Statutory Planner

Governance

- Patrick O’Gorman Manager Governance and Integrity (Acting)
- Mel Nikou Governance Officer
- Milica Latu Coordinator Office of Mayor and Councillors (Acting)

3. Declarations of Conflict of Interest

Any Councillor who has a conflict of interest in a matter being considered at this meeting is required to disclose that interest either by explaining the nature of the conflict of interest to those present or advising that they have disclosed the nature of the interest in writing to the Chief Executive Officer before the meeting commenced.

4. Confirmation of Minutes

RECOMMENDATION

That the minutes of the Planning Decisions Committee held on Tuesday 26 August 2025 be confirmed.

5. Planning Committee Reports

5.1. – PLN25/0181 - 418 Burnley Street, Richmond

Author Joe Byrne – Senior Statutory Planner
Authoriser General Manager City Sustainability and Strategy

Executive Summary

Property	418 Burnley Street, Richmond
Ward	Lennox
Application number	PLN25/0181
Proposal	Construction of a ten storey mixed use building (plus two basement levels), use of the land for Office and Food and Drink Premises, construct and display Business Identification Signage, reduction in the car parking requirements, and create and alter access to a road in Transport Zone 2.
Zoning & Overlay/s	<ul style="list-style-type: none"> - Industrial 3 Zone (IN3Z) - Design and Development Overlay (Schedule 2 and 5) - Development Contributions Plan Overlay (Schedule 1) - Transport Zone (Schedule 2)
Strategic setting	Industrial land
Submissions	22 objections
Parties to the appeal	1 objector party (at time of writing report)
Key reasons for support	<ul style="list-style-type: none"> - Generally complies with built form requirements of the Zone, Overlays, Particular Provisions and local policies and would make a positive contribution to the area; - The car parking reduction is acceptable; - Would not have adverse amenity impacts to surrounding properties

Recommendation	<p>That Council advises the Victorian Civil and Administrative Tribunal (VCAT) that had it been in a position to make a decision, it would have issued a Notice of Decision, with conditions.</p> <p>Key conditions include:</p> <ul style="list-style-type: none"> - Built form changes - Materiality changes - Conditions on uses
Contact Officer	Joe Byrne, Principal Planner (Acting)

Officer Recommendation

That having considered all objections and relevant planning policies, the Committee resolves that had Council been in a position to make a decision, it would have issued a Notice of Decision to Grant Planning Permit PLN25/0181 at 418 Burnley Street, Richmond for:

Control	Clause	Matter for which the permit has been granted
Industrial 3 Zone	33.03-1	To use land for Offices and Restaurant/cafe
Industrial 3 Zone	33.03-4	To construct a building and construct and carry out works
Design and Development Overlay	43.02-2	To construct a building and construct and carry out works
Signs	52.05-12	Construct and display High-wall, Business Identification and internally illuminated Signage
Car Parking	52.06-3	A reduction in the car parking requirements
Land Adjacent to the Principal Road Network	52.29-2	To create and alter access to a road in a Transport 2 Zone

subject to the following conditions:

1. Before the development commences, amended plans to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. When approved, the plans will be endorsed and will then form part of this permit. The plans must be drawn to scale with dimensions, and must be generally in accordance with the decision plans prepared by Studio McCue and dated 22 April 2025 but modified to show:
 - (a) Tenancy 1 at ground level clearly shown as restaurant/café (not F&B);
 - (b) Setback Level 2 northern wall, 2.94m from the northern boundary, consistent with the setback of the north-west corner;
 - (c) Deletion of the gate within the lane;
 - (d) Deletion of the ground floor brick element between the entrance to the food and drink premises entry and the water meter;

- (e) Deletion of the north side ground floor window to the lobby setback 0.665m and replacement wall to abut the boundary;
- (f) The north-facing tower windows to be treated to visually break up the clear glazing using colour, tone, fritting or frosting of the windows;
- (g) CON02 material to the southern boundary wall to include further texture to ensure greater contrast to CON01 (e.g. ribbed concrete);
- (h) The Madden Grove and Burnley Street, street wall brick colour revised to provide variation to the building;
- (i) The street wall brick material, consistent with the Burnley Street and Madden Grove materiality, extended along the western boundary;
- (j) The green wall adjacent to the ground floor entrance shown on the elevations;
- (k) The southern, high wall sign to be shown within title boundaries;
- (l) Clear delineation between the public and private site boundary through the use of alternate paving entirely within the title boundary;
- (m) Footpath levels and finish floor levels at entrances and within setbacks provided to ensure seamless transition;
- (n) Provide a minimum of 48 employee bicycle parking spaces;
- (o) Relocate the two retail visitor bicycle spaces within the north-east corner entrance courtyard.
- (p) Provide all visitor bicycle parking in a publicly accessible area at ground level;
- (q) The width of all access paths to bicycle parking areas dimensioned in accordance with AS2890.3 requirements;
- (r) Notation confirming bicycle storage room will be within a secure lockable compound to provide secure access for employees;
- (s) Spacing between bicycle parking spaces dimensioned in accordance with AS2890.3 requirements;
- (t) The wheel stop distance (measured to point of contact with vehicle tyre) dimensioned in accordance with Section 2.4.5.4 Wheel stops of AS/NZS 2890.1:2004;
- (u) The width of the internal access ramp, including kerbs dimensioned;
- (v) Any changes associated with conditions 55-64 (Head, Transport for Victoria);
- (w) Any requirement of the endorsed Sustainable Management Plan (condition 5) (where relevant to show on plans);
- (x) Any requirement of the endorsed Waste Management Plan (condition 8) (where relevant to show on plans); and
- (y) Any requirement of the Glare Report (condition 24) (where relevant to show on plans).

2. The use and development as shown on the endorsed plans must not be altered (unless the Yarra Planning Scheme specifies that a permit is not required) without the prior written consent of the Responsible Authority.
3. In conjunction with the submission of development plans under Condition 1, a Façade Strategy and Materials and Finishes Plan to the satisfaction of the Responsible Authority must be submitted to and be approved by the Responsible Authority. When approved, the Façade Strategy and Materials and Finishes Plan will be endorsed and will then form part of this permit. This must detail:
 - (a) elevation drawings at a scale of 1:20 illustrating typical podium details, entries and doors, façade services (including service doors) and utilities and typical tower facade details;
 - (b) section drawings to demonstrate façade systems, including fixing details and joints between materials or changes in form;
 - (c) information about how the façade will be maintained, including any vegetation; and
 - (d) coloured drawings outlining colours, materials and finishes.
4. As part of the ongoing consultant team, Studio McCue or an architectural firm to the satisfaction of the Responsible Authority must be engaged to:
 - (a) oversee the design and construction of the development; and
 - (b) ensure the design quality and appearance of the development is realised as shown in the endorsed plans or otherwise to the satisfaction of the Responsible Authority.

Sustainable Management Plan

5. In conjunction with the submission of development plans as required by condition 1, an amended Sustainable Management Plan to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. When approved, the amended Sustainable Management Plan will be endorsed and will form part of this permit. The amended Sustainable Management Plan must be generally in accordance with the Sustainable Management Plan prepared by Environmental Solutions and dated 11 March 2025, but modified to include or show:
 - (a) Deletion of ESD display and metering credit claimed as an Innovation in BESS report;
 - (b) Deletion of App to link building users and facility management credit claim as an Innovation in BESS report;
 - (c) Deletion of Digital noticeboard with various information displayed credit claimed as an Innovation in BESS report; and
 - (d) Clarify the commitment to purchasing 100% Green Power for a minimum 10 year period throughout the energy section of the SMP.
6. The provisions, recommendations and requirements of the endorsed Sustainable Management Plan must be implemented and complied with to the satisfaction of the Responsible Authority.

Acoustic Report

7. The provisions, recommendations and requirements of the endorsed Acoustic Reports must be implemented and complied with to the satisfaction of the Responsible Authority.
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Waste Management Plan

8. In conjunction with the submission of development plans as required by condition 1, an amended Waste Management Plan to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. When approved, the amended Waste Management Plan will be endorsed and will form part of this permit. The amended Waste Management Plan must be generally in accordance with the Waste Management Plan prepared by Traffix Group and dated March 2025, but modified to require:
 - (a) Deletion of reference to residential waste storage area in table 8.
9. The provisions, recommendations and requirements of the endorsed Waste Management Plan must be implemented and complied with to the satisfaction of the Responsible Authority.
10. The collection of waste from the site must be by private collection, unless with the prior written consent of the Responsible Authority.

Landscape Plan

11. Before the building is occupied, or by such later date as approved in writing by the Responsible Authority, the landscaping works shown on the endorsed Landscape Plan must be carried out and completed to the satisfaction of the Responsible Authority. The landscaping shown on the endorsed Landscape Plan must be maintained by:
 - (a) implementing and complying with the provisions, recommendations and requirements of the endorsed Landscape Plan;
 - (b) not using the areas set aside on the endorsed Landscape Plan for landscaping for any other purpose; and
 - (c) replacing any dead, diseased, dying or damaged plants, to the satisfaction of the Responsible Authority.

Use

12. No more than 90 patrons are permitted within the restaurant/café at any one time.
 13. Except with the prior written consent of the Responsible Authority, the restaurant/cafe use authorised by this permit may only operate between the hours of 8.00am and 10.00pm.
 14. Except with the prior written consent of the Responsible Authority, the Office use authorised by this permit may only operate between the hours of 8.00am and 8.00pm.
 15. The communal roof area to be closed between 8.00pm and 8.00am on any day and accessible to office employees only.
 16. The amenity of the area must not be detrimentally affected by the use, including through:
 - (a) the transport of materials, goods or commodities to or from land;
 - (b) the appearance of any buildings, works or materials;
 - (c) the emission of noise, artificial light, vibration, smell, fumes, smoke, vapour, steam, soot, ash, dust, waste water, waste products, grit or oil, or
 - (d) the presence of vermin,to the satisfaction of the Responsible Authority.
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17. The provision of music within the restaurant/cafe must be at a background level only.
18. Speakers external to the building must not be erected or used.
19. The use must at all times comply with the noise limits specified in the Environment Protection Regulations under the Environment Protection Act 2017 and the incorporated Noise Protocol (Publication 1826.4, Environment Protection Authority, May 2021), as may be amended from time to time.
20. Emptying of bottles and cans into bins may only occur between 7am and 10pm on Monday to Saturday or between 9am and 10pm on a Sunday or public holiday.
21. Delivery and collection of goods to and from the land may only occur between 7am and 10pm Monday to Saturday, or after 9am on a Sunday or public holiday except for those allowed under any relevant local law.

Wind Assessment

22. The provisions, recommendations and requirements of the endorsed Wind Assessment Report must be implemented and complied with to the satisfaction of the Responsible Authority.

Green Travel Plan

23. The provisions, recommendations and requirements of the endorsed Green Travel Plan must be implemented and complied with to the satisfaction of the Responsible Authority.

Glare Report

24. Concurrent with the submission of Condition 1 plans, a Reflected Glare Assessment must be submitted to and approved by Responsible Authority. Once approved, the Reflected Glare Assessment will be endorsed and will then form part of the permit. The assessment must show, but not be limited to the following:
 - (a) Details of the reflected sunlight from the tower glazing and any potential interference with train driver visibility, interpretation of rail signals, surrounding roads, and residential properties.
25. Specular light reflectance must be less than 15 per cent for all external building glazing and cladding materials and finishes when measured at an angle of 90 degrees to the surface of the material (normal incidence) except with the written consent of the Responsible Authority.
26. External glazing and cladding materials and finishes must avoid disability glare to public transport operators and road users; and extended periods of discomfort glare for pedestrians and occupants of surrounding buildings caused by glazing and external cladding must be avoided or minimised to the satisfaction of the Responsible Authority.

Public Realm

27. Concurrent with the submission of Condition 1 plans, or by such later date as approved in writing by the Responsible Authority, a Public Realm Plan of all public realm improvements associated with the development must be prepared, submitted and approved by the Responsible Authority. When approved, the plan will be endorsed and will then form part of the permit. The Public Realm Plan must show, but not be limited to, the following:
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- (a) Footpath, kerb & channel and road pavement works along all frontages;
 - (b) Kerb outstands and associated works in accordance with requirements outlined in the Condition 1 plans;
 - (c) Proposed or adjustment of existing utility services;
 - (d) Streetscape/landscape requirements;
 - (e) Any public realm infrastructure required under other permit conditions; and
 - (f) at the permit holder's cost,
- to the satisfaction of the Responsible Authority.

28. Before the development commences (excluding site preparations, demolition and bulk excavation), or by such later date as approved in writing by the Responsible Authority, detailed design drawings of the works approved under the Public Realm Plan, addressing all road infrastructure works (including soft/hard landscaping), must be submitted to and approved by Council's Civil Engineering Department. Once approved, the detailed design drawings will be endorsed and will then form part of the permit.
29. Before the building is occupied, all associated works shown on the endorsed Detailed Design Plans (DDP) for the public realm must be fully constructed and completed to the satisfaction of the Council's Civil Engineering Department and at no cost to the Responsible Authority.

Infrastructure

30. Before the building is occupied, or by such later date as approved in writing by the Responsible Authority, any damage to Council infrastructure resulting from the development must be reinstated:
- (a) at the permit holder's cost; and
 - (b) to the satisfaction of the Responsible Authority.
31. Before the building is occupied, or by such later date as approved in writing by the Responsible Authority, any new vehicle crossing must be constructed:
- (a) in accordance with any requirements or conditions imposed by Council;
 - (b) at the permit holder's cost; and
 - (c) to the satisfaction of the Responsible Authority.
32. Before the building is occupied, or by such later date as approved in writing by the Responsible Authority, any redundant vehicular crossing must be demolished and re-instated as standard footpath and kerb and channel:
- (a) at the permit holder's cost; and
 - (b) to the satisfaction of the Responsible Authority.
33. Concurrent with the submission of Condition 1 plans or by such later date as approved in writing by the Responsible Authority, a vehicle crossing design must be submitted to Council's Civil Engineering Department for approval. The submitted design must demonstrate compliance with City of Yarra's, Vehicle Crossing Information Sheet. Refer to Notes (below) for lodgement details.
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34. Before the building is occupied or by such later date as approved in writing by the Responsible Authority, the footpath along the property's Burnley Street and Madden Grove frontages must be re-sheeted in asphalt:
 - (a) at the permit holder's cost; and
 - (b) to the satisfaction of the Responsible Authority.
 35. Before the building is occupied, or by such later date as approved in writing by the Responsible Authority, the kerb and channel along the property's Burnley Street and Madden Grove frontages must be reconstructed:
 - (a) at the permit holder's cost; and
 - (b) to the satisfaction of the Responsible Authority.
 36. Within 2 months of the completion or by such later date as approved in writing by the Responsible Authority, any damage to Council infrastructure resulting from the development must be reinstated:
 - (a) In accordance with Yarra Standard Drawings | Yarra City Council at the permit holder's cost; and
 - (b) to the satisfaction of the Responsible Authority
 37. Except with the prior written consent of the Responsible Authority, Council assets must not be altered in any way.
 38. All pipes, fixtures, fittings and vents servicing any building on the land must be concealed in service ducts or otherwise hidden from view to the satisfaction of the Responsible Authority.
 39. Before the building is occupied, or by such later date as approved in writing by the Responsible Authority, external lighting capable of illuminating access to the basement car park and front entrance must be provided within the property boundary. Lighting must be:
 - (a) located;
 - (b) directed;
 - (c) shielded; and
 - (d) of limited intensity,to the satisfaction of the Responsible Authority.
 40. Before the building is occupied, or by such later date as approved in writing by the Responsible Authority, the area set aside on the endorsed plans for the car parking spaces, access lanes, driveways and associated works must be:
 - (a) constructed and available for use in accordance with the endorsed plans;
 - (b) formed to such levels and drained so that they can be used in accordance with the endorsed plans;
 - (c) treated with an all-weather seal or some other durable surface; and
 - (d) line-marked or provided with some adequate means of showing the car parking spaces,
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to the satisfaction of the Responsible Authority.

Street Tree Bond

41. Before the development commences, the permit holder must provide an Asset Protection Bond of \$500.00 (ex GST) per tree in Madden Grove adjacent the frontage of the development to the Responsible Authority. The security bond:
 - (a) must be provided in a manner, and on terms, to the satisfaction of the Responsible Authority;
 - (b) may be held by the Responsible Authority until the works are completed to the satisfaction of the Responsible Authority; and
 - (c) in accordance with the requirements of this permit; or
 - (d) otherwise to the satisfaction of the Responsible Authority.

General

42. Before the building is occupied, or by such later date as approved in writing by the Responsible Authority, all new on-boundary walls must be cleaned and finished to the satisfaction of the Responsible Authority.
43. Before the building is occupied, any wall located on a boundary facing public property must be treated with a graffiti proof finish to the satisfaction of the Responsible Authority.
44. Delivery and collection of goods to and from the land may only occur between 7am and 10pm Monday to Saturday, or after 9am on a Sunday or public holiday except for those allowed under any relevant local law.

Construction Management Plan

45. Before the development commences, a Construction Management Plan to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. When approved, the plan will be endorsed and will form part of this permit. The plan must provide for:
 - (a) a pre-conditions survey (dilapidation report) of the land and all adjacent Council roads frontages and nearby road infrastructure;
 - (b) works necessary to protect road and other infrastructure;
 - (c) remediation of any damage to road and other infrastructure;
 - (d) containment of dust, dirt and mud within the land and method and frequency of clean up procedures to prevent the accumulation of dust, dirt and mud outside the land,
 - (e) facilities for vehicle washing, which must be located on the land;
 - (f) the location of loading zones, site sheds, materials, cranes and crane/hoisting zones, gantries and any other construction related items or equipment to be located in any street;
 - (g) site security;
 - (h) management of any environmental hazards including, but not limited to,:
 - (i) contaminated soil;

- (ii) materials and waste;
 - (iii) dust;
 - (iv) stormwater contamination from run-off and wash-waters;
 - (v) sediment from the land on roads;
 - (vi) washing of concrete trucks and other vehicles and machinery; and
 - (vii) spillage from refuelling cranes and other vehicles and machinery;
- (i) the construction program;
 - (j) preferred arrangements for trucks delivering to the land, including delivery and unloading points and expected duration and frequency;
 - (k) parking facilities for construction workers;
 - (l) measures to ensure that all work on the land will be carried out in accordance with the Construction Management Plan;
 - (m) an outline of requests to occupy public footpaths or roads, or anticipated disruptions to local services;
 - (n) an emergency contact that is available for 24 hours per day for residents and the Responsible Authority in the event of relevant queries or problems experienced; and
 - (o) the provision of a traffic management plan to comply with provisions of AS 1742.3-2002 Manual of uniform traffic control devices - Part 3: Traffic control devices for works on roads.
46. If required, the Construction Management Plan may be approved in stages. Construction of each stage must not commence until a Construction Management Plan has been endorsed for that stage, to the satisfaction of the Responsible Authority.
47. The provisions, recommendations and requirements of the endorsed Construction Management Plan must be implemented and complied with to the satisfaction of the Responsible Authority.
48. Except with the prior written consent of the Responsible Authority, demolition or construction works must not be carried out:
- (a) Monday-Friday (excluding public holidays) before 7 am or after 6 pm;
 - (b) Saturdays and public holidays (other than ANZAC Day, Christmas Day and Good Friday) before 9 am or after 3 pm; or
 - (c) Sundays, ANZAC Day, Christmas Day and Good Friday at any time.

Signage

49. The location and details of the signs, including the supporting structure, as shown on the endorsed plans must not be altered (unless the Yarra Planning Scheme specifies that a permit is not required) without the prior written consent of the Responsible Authority.
50. The signs must be constructed, displayed and maintained to the satisfaction of the Responsible Authority.
51. This permit expires 15 years from the date of the permit.
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52. On expiry of this permit, the approved signs and structures built specially to support and illuminate signage must be removed.
53. The signs must not include any flashing or intermittent light, and only the approved northern sign is to be illuminated.
54. This permit will expire if the signs are not erected within 2 years of the date of this permit. The Responsible Authority may extend the period referred to if a request is made in writing before the permit expires or within six months afterwards for commencement or twelve months afterwards for completion.

Head, Transport for Victoria

55. Prior to the occupation of the development hereby approved, the modified crossover must be completed to the satisfaction of the Responsible Authority and at no cost to the Responsible Authority and the Head, Transport for Victoria. The widened crossover must keep minimum distance of 1 metre to the existing electrical pole.
 56. All vehicles must enter and exit the site in a forward direction at all times.
 57. Before the development starts, or such other time agreed to in writing by the Head, Transport for Victoria, amended plans or designers' reports, to the satisfaction of the Head, Transport for Victoria must be submitted to and approved by the Responsible Authority. When approved, the plans and reports will be endorsed and will then form part of the permit. The plans and reports must be generally in accordance with those submitted with the application (dated 22/04/2025, Revision A, prepared by Studio Mccue) but modified to show that:
 - (a) The designs prevent items from being thrown or falling onto railway land from any part of the building development;
 - (b) The development does not cause reflected sunlight to interfere with train driver visibility or interpretation of rail signals;
 - (c) The development does not reflect or refract artificial light such that it interferes with train driver visibility or interpretation of rail signals;
 - (d) The development exterior avoids use of red, green or yellow colour schemes that may interfere with driver operations.
 58. Any windows, doors and balconies that are set back from, and generally facing the railway land title boundary shall:
 - (a) Be designed to prevent items from being thrown or falling onto railway land;
 - (b) Not cause reflected sunlight to interfere with train driver visibility or interpretation of rail signals; and
 - (c) Not reflect or refract artificial light such that it interferes with train driver visibility or interpretation of rail signals.
 59. Prior to the commencement of work on site, detailed construction/ engineering plans and structural computations for any construction in close proximity to railway infrastructure or railway land must be submitted and approved by VicTrack and the Head, Transport for Victoria. The plans must detail all basement excavation design, retention works and controls of the site in close proximity to the railway corridor having any impact on railway land. The design plans must ensure compliance with:
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- (a) Minimum clearances to all electrical assets and procedures for works in close proximity to such assets, including:
 - (i) Energy Safe Victoria (ESV) requirements for minimum clearances to electrical assets and works nearby;
 - (ii) Australian Standards AS2067, AS7000 and Electricity Safety (General Regulations 2019, Part 6) for clearances to electrical assets. Clearances required include for safe working, fire life safety design, electromagnetic interference and earthing, bonding, and electrolysis mitigation design; and
 - (iii) Any other reasonable safety requirements required by the Rail Operator.
 - (b) The required rail collision loadings and collision protection measures for the building supports in close proximity to the rail tracks in accordance with AS5100 Part 1 – ‘Bridge Design, Scope and General Principals’;
 - (c) Earthquake design loadings for structure designated as a minimum Importance Level 2, by AS1170.4 – ‘Structural Design Actions, Earthquake Actions in Australia’; and
 - (d) Demonstration that ground stabilisation devices, such as temporary or permanent ground anchors, soil nails, reinforced earth straps, do not penetrate onto railway land.
60. Prior to the commencement of demolition or construction works, the Rail Operator must be contacted through the email address metrositeaccess@metrotrains.com.au to obtain the Rail Operator’s conditions and safety requirements. Furthermore, the developer is required to contact the Developer Interface Projects team, Department of Transport and Planning to obtain guidance and information regarding preconstruction and construction activities, via developerinterfaceprojects@transport.vic.gov.au.
61. Before the development starts (including demolition and bulk excavation), a Traffic Management Plan must be submitted to, and approved by, the Head of Transport for Victoria. The Traffic Management Plan must provide for:
- (a) How public transport operations, traffic, walking and cycling movements will be managed during the demolition and construction; and
 - (b) How any traffic impact to the railway land and associated infrastructure assets will be mitigated.

The Traffic Management Plan must be implemented and complied with to the satisfaction of the Head, Transport for Victoria.

All costs associated with the preparation and implementation of the Traffic Management Plan will be at no cost to the Head, Transport for Victoria.

The endorsed Traffic Management Plan must not be modified without the prior written consent of the Head, Transport for Victoria.

62. Before the development starts (including demolition and bulk excavation), separate Demolition and/or Construction Management Plan must be submitted to and approved by the Head, Transport for Victoria. When approved, the Demolition and Construction Management Plan will form a part of the permit. The Demolition and Construction Management Plan must include (but not be limited to) details of:
- (a) The buildings, works and other measures necessary to:
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- (i) Protect railway land, track, overhead power and associated infrastructure; and
- (ii) Prevent or minimise disruption to the operation of the railway;
- (b) The remediation of any damage to railway land, track, overhead and underground power and communication assets, and associated infrastructure;
- (c) Details of required access to the railway land during demolition and construction of the development with appropriate durations and schedules;
- (d) Arrangements for:
 - (i) Any hoarding associated with the construction of the development that encroaches onto or overhangs railway land;
 - (ii) Piling, excavation, shoring, stabilising, anchoring, filling, earthworks or construction associated with the development occurring on or next to the boundary of the railway land;
 - (iii) Crane location(s), slew radius and slew locking proposals iv. The deposit or store of waste, fill or other materials associated with the development on the railway land;
 - (iv) Air and dust management;
 - (v) Operating hours;
 - (vi) Noise and vibration controls;
 - (vii) The management of site drainage, effluent and waste; and
 - (viii) The security of the railway land and associated infrastructure.

All demolition and construction works must be carried out in accordance with the approved Demolition and Construction Management Plan unless with the prior written consent of the Head, Transport for Victoria.

The Demolition and Construction Management Plan must be prepared, implemented and monitored at no cost to the Head, Transport for Victoria. The Demolition and Construction Plan must be consistent with any Construction Management Plan required by the Responsible Authority.

- 63. No drainage, effluent, waste soil or other materials must enter or be directed to railway land from the development site or be stored or deposited on railway land by the proponent.
- 64. Access to railway assets by rail staff for the purposes of inspection, cleaning, maintenance and repair shall be maintained at all times. Existing access routes to railway land shall not be closed, diverted or modified without prior agreement with the by the Head, Transport for Victoria and the relevant Rail Transport Operator(s).

Development Contribution

- 65. Prior to the commencement of the development the Development Infrastructure Levy must be paid to Yarra City Council in accordance with the approved Development Contributions Plan.

Permit Expiry

- 66. This permit will expire if:

- (a) the development is not commenced within two years of the date of this permit;
- (b) the development is not completed within four years of the date of this permit; and
- (c) the use is not commenced within five years of the date of this permit.

The Responsible Authority may extend the periods referred to if a request is made in writing before the permit expires or within six months afterwards for commencement or within twelve months afterwards for completion.

Notes:

For the submission of plans pursuant to Condition 34, please submit these to info@yarracity.vic.gov.au marked attention to Engineering Services.

A building permit may be required before development is commenced. Please contact Council's Building Services on 9205 5555 to confirm.

These premises will be required to comply with the Food Act 1984. The use must not commence until registration, or other approval, has been granted by Council's Health Protection Unit.

Provision must be made for drainage of the site to a legal point of discharge. Please contact Council's Building Services on 9205 5555 for further information.

A local law permit (e.g. Asset Protection Permit, Road Occupation Permit) may be required before development is commenced. Please contact Council's Construction Management Branch on Ph. 9205 5555 to confirm.

Legal duties and obligations to protect human health and the environment from pollution and waste are set out in the Environment Protection Act 2017. Please refer to the Environment Protection Authority Victoria website www.epa.vic.gov.au/meeting-your-obligations for more information on identifying potential contamination and complying with your duty to manage contaminated land under the Environment Protection Act 2017.

All future business (whether as owners, lessees/tenants, occupiers) within a development approved under this planning permit, will not be eligible for business parking permits unless the development has heritage controls where parking would adversely affect the integrity of the heritage place, or if there's no increase in separate occupancies and the development does not reduce existing on-site parking. For more information refer to www.yarracity.vic.gov.au/residents/transport/parking/parking-permits.

A local law permit may be required for tree removal. Please contact Council's Compliance Branch on 9205 5555.

Head, Transport for Victoria Notes:

The proposal requires works within the road reserve. Separate approval under the Road Management Act 2004 for this activity is required from the Head, Transport for Victoria prior to commencing any works within the road reserve.

Please contact the Department of Transport and Planning prior to commencing any works. Please refer to the following link for further information:

<https://transport.vic.gov.au/business/road-and-traffic-management/road-permits-andcharges/working-within-the-road-reserve>.

History and Background

1. On 07 August 2025 the applicant lodged a Section 79 appeal to the Victorian Civil Administrative Tribunal seeking review of Yarra City Council's Failure to grant a planning permit. The Compulsory Conference is listed for 3 October 2025, while the hearing is listed for 1, 2, 3 and 4 December 2025.

Site Context

2. The subject site is located on the western side of Burnley Street and the southern side of Madden Grove, approximately 235m to the east of Rooney Street and 250m north of Barkly Avenue in Richmond. The property abuts a narrow right-of-way to the west (approximately 4m wide) which services a number of properties facing Burnley Street and Madden Grove.
3. The site is legally described as Land in Consolidation 103440. The site has a frontage to Burnley Street of 13.94m, a frontage to Madden Grove of 46.27m and an overall area of approximately 856 sqm.
4. The subject site contains a single storey industrial building constructed along the north, south and west boundaries. The building is set back approximately 17m from the eastern boundary, which contains car parking. Vehicle crossovers are located within the north-east corner of the site (with access from Madden Grove) and the east side of the site (with access from Burnley Street).



Figure 1: Image of the subject site to Burnley Street (Planning Officer, August 2025).



Figure 2: Image of the subject site to Madden Grove (Planning Officer, August 2025)

Surrounds

- The site is located within a predominately industrial area, which contains a variety of built form. Land to the north of the subject site, beyond the railway line, is zoned Commercial 1 Zone, while land to the west and south is zoned Industrial 3 Zone. Land to the east, beyond Burnley Street is zoned General Residential Zone 2.

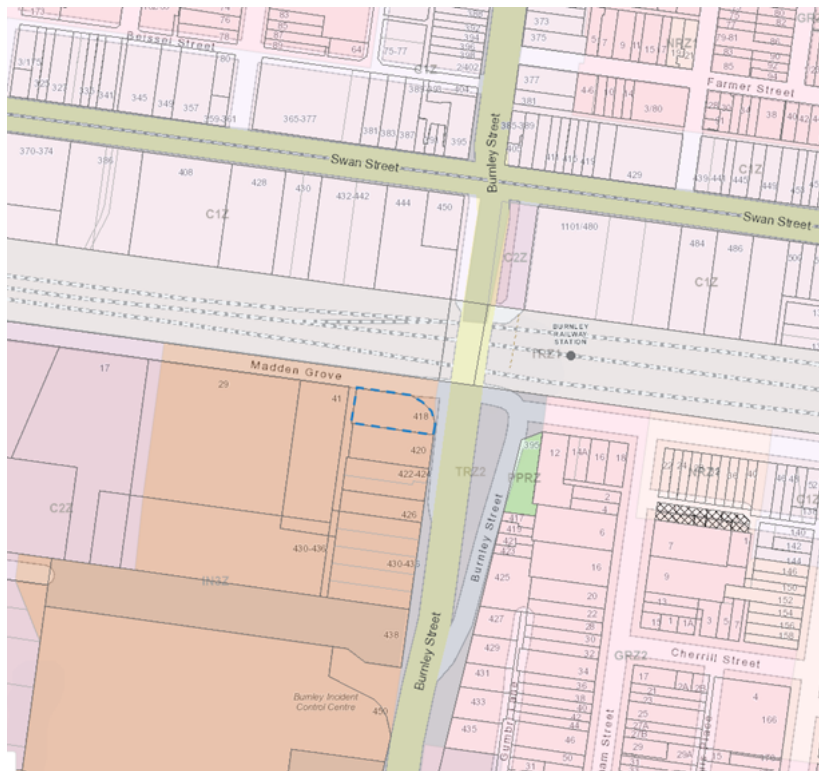


Figure 3: Zoning context of the subject site (VicPlan August 2025)

- The southern section of Burnley Street is characterised by low to mid-rise industrial buildings along the western side, and one to two storey dwellings along the eastern side. This section of Madden Grove is characterised with generally low scale industrial buildings, with pockets of heritage built form.
- The surrounding built form generally consists of brick, concrete or metal sheeted facades with a variety of different sized and proportioned window and door openings.



Figure 4: Aerial image of subject site and surrounding context (NearMap, 3 August 2025)

8. The subject site is located within the southern section of Burnley Street. The inner-suburban locale is well serviced by infrastructure and public transport, with:
 - (a) Swan Street serviced by one tram route (#70 – Docklands to Wattle Park);
 - (b) Train services from Burnley Station (Alamein/Belgrave/Glen Waverley/Lilydale);
 - (c) Bus routes along Burnley Street (603 Bus); and,
 - (d) The CBD within 4km.
9. To the east of the subject site is Burnley Street, which measures approximately 45m in width. The subject site directly faces the raised Burnley Street bridge which extends over the railway line to the north. Car parking is located beneath this bridge, directly opposite the subject site. On the opposite side of Burnley Street is a public park on the corner of Madden Grove. Single to three storey dwellings are located further to the east.
10. To the north of the subject site, beyond Madden Grove is the railway line between Burnley and East Richmond Train Stations. A double storey brick building is located between the railway lines. Further to the north is a row of commercial properties fronting Burnley Street and Swan Street. These properties vary in height between single to triple storey. Constructed on the south-eastern corner of Swan Street and Burnley Street is a 12 storey mixed use building (retail at ground level and office above) approved by the Minister for Planning.
11. Immediately to the south of the subject site is a double storey brick industrial building at No. 420 Burnley Street. The building is currently used for car sales and services. Access to this building is limited to Burnley Street. A vehicle crossover is located along Burnley Street providing vehicle access into the site through a roller door entry. The building has been constructed to all boundaries. There are solar panels positioned along the south

side of the roof form. Non-illuminated business identification signage is located along the Burnley Street facade.

12. To the west of the subject site is a laneway with a width of approximately 4m. This lane is accessible to Madden Grove only and has been gated off. Beyond this lane is a single storey brick and bluestone building at No. 41 Madden Grove. This building has an "Individually significant" heritage grading under HO275 (41 Madden Grove Richmond Precinct). This building has been developed at the rear with a three-storey contemporary extension and used for motor vehicle sales, office and store. Business identification signage is located at the front of the property along Madden Grove.

Proposal

13. The proposal is for construction of a ten storey mixed use building (plus two basement levels), use of the land for Office and Food and Drink Premises, construct and display Business Identification Signage, reduction in the car parking requirements, and create and alter access to a road in Transport Zone 2. A copy of the decision plans and supporting documentation are attached to this report.

Use

14. The building will be used for Offices and a Food and Drink Premises. Further details of the uses are as follows:
 - (a) The office tenancies will be located between ground level and level 8 and will have a combined leasable floor area of 4,797sqm;
 - (b) The food and drinks premise will be located at ground level and will have a leasable floor area of 140sqm;
 - (c) The proposal does not include any details of patron numbers, or hours proposed for either use; and
 - (d) The primary pedestrian entry to the uses will be from the corner of Burnley Street and Madden Grove. Vehicle entry to the basement levels is located off Burnley Street.

Car and Bicycle Parking

15. A total of 28 car parking spaces proposed across both basement levels (11 on Basement Level 1 and 17 on basement level 2), accessed via lifts and a stairwell to the lobby. This includes 2 motorcycle parking spaces.
16. A total of 38 bicycle spaces proposed at ground level (28 employee bicycle parking spaces and 10 visitor bicycle spaces).

Signs

17. Construct and display 3 business identification signs. This includes High wall signs on the southern (2m high x 3.42m wide and non-illuminated) and northern (2.235m high x 3.82m wide and internally illuminated) facades of the development and a ground floor sign along the eastern facade (0.25m high x 2.845m wide and non-illuminated).

Development

18. Further details of the proposed development are as follows:
 - (a) Demolition of all buildings on site (no planning permit required);
-

- (b) 10 storeys (plus two basement levels) with an overall maximum building height of 37.97m (39.57m to the top of the lift overrun);
- (c) Communal roof terrace, including an outdoor fitness area, and a canopy above;
- (d) Two primary frontages along both Burnley Street and Madden Grove, with the primary pedestrian entry on the corner through a recessed courtyard;
- (e) A total net floor area of 5454sqm;
- (f) Contemporary in design with a double storey brick podium to Madden Grove stepping up to three storeys along Burnley Street. The built form has been recessed from the corner of Madden Grove and Burnley Street, creating an open ground floor courtyard to the primary pedestrian entry. The upper levels cantilever over this courtyard;
- (g) Setbacks from the north boundary vary between 0-3.6m, with setbacks from the west varying between 0-1.6m. The building is setback variously from the eastern boundary (given shape of subject site) with 3.82m at the southern-most interface at level 3 and above (with balconies in this setback at levels 3-4);
- (h) Constructed along the southern boundary at all levels; and
- (e) Materials include a combination of grey brick, smooth and exposed aggregate concrete, powdercoated silver finish metal, clear double glazed windows and clear glazed windows with solid black panelling.



Figure 5: Render of the development from the corner of Burnley Street and Madden Grove (Studio McCue March 2025)



Figure 6: Render of the development north from above Madden Grove (Studio McCue March 2025)



Figure 7: Render of the development from west along Madden Grove (Studio McCue March 2025)

Application history

19. The site has the following planning history:

- (a) Planning Permit SP04/0020 was issued 28 April 2004 for subdivision of land into two lots;
- (b) Planning Permit PL04/0731 was issued on 22 October 2004 for use of site as motor vehicle repairs;
- (c) Planning Permit PL06/0804 was issued 22 December 2006 for use of the land for the purpose of warehouse and trade supplies including a reduction in car parking requirements and development of the land through buildings and works to the existing building and display of advertising signage; and

- (d) Planning Permit PL10/0489 was issued 08 December 2010 for construction of an entrance pergola to an existing industrial building.

Statutory controls

- 20. The table contained in the Officer recommendation sets out why a planning permit is required for this matter.
- 21. The following clauses of the Yarra Planning Scheme are also relevant to the proposal:

Clause 33.03 – Industrial 3 Zone (IN3Z)

- 22. An office is a permit required use on the condition the leasable floor area does not exceed the amount specified in the schedule to this zone. Given that there is no amount specified in the schedule to this zone, a planning permit is required for the office.
- 23. A Retail premises (Food and drink premises) is a permit required use.
- 24. A planning permit is required to construct a building or construct or carry out works and therefore a planning permit is required for the proposed works.

Clause 43.02 – Design and Development Overlay (Schedule 2 and 5) [DDO2 and 5]

- 25. A planning permit is required to construct a building or construct or carry out works and therefore a planning permit is required for the proposed works under this overlay.

Clause 45.06 - Development Contributions Plan Overlay (Schedule 1) [DCPO1]

- 26. The DCPO1 sets out mandatory development contributions for particular types of development. As there is an increase in the commercial floor area, the requirements of the DCPO1 apply to this application.

Clause 52.05 – Signs

- 27. A planning permit is not required to construct and display business identification signage if the total display area of all signs does not exceed 8 sqm. Given the total display area of business identification signs on site exceeds 8 sqm (16.9 sqm), a planning permit is required for these signs.
- 28. A planning permit is required to construct and display internally illuminated signage with a total display area exceeding 1.5sqm. Given the total display area of internally illuminated signs on site exceeds 1.5 sqm (15.38 sqm), a planning permit is required.

Clause 52.06 – Car Parking

- 29. Before a new use commences the required car parking spaces must be provided on the land as set out at Clause 52.06-5. The following table outlines the car parking requirements associated with the application:

Proposed Use	Quantity / Size	Statutory Parking Rate*	No. of Spaces Required	No. of Spaces Allocated	Reduction sought
Food and drink premises	140sqm	3.5 spaces to each 100 square	4	1	

		metres of net leasable floor area			
Office	5,291sqm	3 spaces to each 100 square metres of net floor area	158	27	
Total			162	28	134

Clause 52.29 – Land Adjacent to the Principal Road Network

- 30. A planning permit is required to create or alter access to a road in a Transport Zone 2, therefore the proposed new vehicle crossover requires a planning permit.

Clause 52.34 - Bicycle facilities

Proposed Use	Quantity/ Size	Statutory Parking Rate	No. of Spaces Required	No. of Spaces Allocated
Food and drink premises	140sqm	1 to each 300sqm of leasable floor area	0 employee spaces	0 employee spaces
		1 to each 500sqm of leasable floor area	0 Visitor spaces	2 visitor spaces
Office	5,291 sqm	1 to each 300 sq m of net floor area if the net floor area exceeds 1000 sq m	18 employee spaces	28 Employee spaces
		1 to each 1000 sq m of net floor area if the net floor area exceeds 1000 sq m	5 visitor spaces.	10 visitor spaces
Bicycle Parking Spaces Total			18 employee spaces	28 employee spaces
			5 visitor spaces	10 visitor spaces
Showers / Change rooms		1 to the first 5 employee spaces and 1 to each additional 10 employee spaces	3 showers / change rooms	4 showers / change rooms

- o Given the above, no reduction in bicycle parking is required as part of the application.

Clause 53.18 - Stormwater Management in Urban Development

- This clause applies to an application under a provision of a zone to construct a building or construct or carry out works. An application to construct a building or to construct or carry out works:
 - Must meet all of the objectives of Clauses 53.18-5 and 53.18-6.
 - Should meet all of the standards of Clauses 53.18-5 and 53.18-6.

Clause 65 - Decision Guidelines

- These guidelines set out the relevant matters to be considered, as appropriate, before deciding on an application or approval of a plan.

Internal and External Consultation

Advertising

31. The application was advertised under the provisions of Section 52 of the *Planning and Environment Act (1987)* by letters sent to surrounding owners and occupiers and by two signs (displayed on the Burnley Street and Madden Grove frontages).
32. Council received twenty two (22) objections, with the following key grounds:
 - (a) The height, scale and design of the development is inappropriate within the surrounding context;
 - (b) Setting a precedence for excessive building height within the area;
 - (c) Off-site amenity impacts including visual bulk, overlooking, overshadowing and noise impacts to surrounding residential properties;
 - (d) Impacts on the surrounding heritage buildings;
 - (e) Car parking impacts, including increased traffic;
 - (f) Impacts during construction;
 - (g) On-site amenity including lack of services and building sustainability;
 - (h) Extensive use of glass and impacts of glare to surrounding properties;
 - (i) The building not sufficiently adaptable;
 - (j) Wind impacts within the public realm;
 - (k) Misleading and inconsistent documentation submitted;
 - (l) Impacts on existing infrastructure including waste collection and public transportation;
 - (m) Illumination impacts of signs to surrounding dwellings; and
 - (n) Soil contamination impacts.

Internal and external referrals

33. The proposal was referred to the following external agencies and internal business units:

External Agency	Outcome
Head, Transport for Victoria, Department of Transport and Planning	No objection, subject to conditions which have been included in the recommendation
Internal Business Unit/ External consultant	Comments
Heritage Advisor	Supports proposal.
Vipac Wind Consultants	Supports proposal.
Blades Studio (Urban Design – built form)	Support subject to conditions which have been generally included in the recommendation. Key conditions seek to undertake material adjustments.
Strategic Transport	Support subject to conditions which have been generally included in the recommendation. Key conditions seek to increase the number bicycle parking spaces and provide visitor bicycle parking spaces in a publicly accessible area.
City Works – Open Space	Support subject to conditions which have been generally included in the recommendation. Key conditions seek an asset protection bond implemented for each retained street tree.
Strategic Planning	Support subject to conditions. Key recommendations include reducing the height of the building by two storeys and provide upper level setbacks.
ESD	Support subject to conditions which have been generally included in the recommendation. Key recommendations include an amended Sustainable Management Plan to delete numerous claims to items as innovation in BESS.
City Works – Waste Management	Support subject to conditions which have been generally included in the recommendation. Key recommendations include an amended Waste Management Plan to remove reference to residential waste storage area in a table.
Development Engineering	Support subject to conditions, which have been generally included in the recommendation. Key recommendations include accessway widths, wheel stop dimensions and general engineering conditions.

<p>Internal Urban Design (Public Realm)</p>	<p>Support subject to conditions, some of which have been included in the recommendation.</p> <p>Key recommendations include streetscape improvements around the site and rationalise the building line.</p>
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Policy Implications

Planning Policy Framework (PPF)

34. The following policies are of most relevance to this application: Clause 2 - Municipal Planning Strategic, including:

- Clause 02.01 – Context:
 - Clause 02.01-1 – Location
 - Clause 02.01-6 – Built Environment and Heritage
 - Clause 02.01-9 – Transport
- Clause 02.02 – Vision
- Clause 02.03-1 – Settlement:
 - Clause 02.03-3 – Environmental Risks and Amenity
 - Clause 02.03-4 – Built Environment and Heritage
 - Clause 02.03-6 – Economic Development
 - Clause 02.03-7 – Transport
- Clause 02.04 – Strategic Framework Plan
- Clause 11 – Settlement , including:
 - Clause 11.01-S – Settlement
 - Clause 11.02-1S – Supply of Urban Land
- Clause 13 – Environmental risks and amenity:
 - Clause 13.07-1S – Land Use Compatibility
 - Clause 13.07-1L – Interfaces and Amenity
- Clause 15 – Built environment and heritage:
 - Clause 15.01-1S – Urban Design
 - Clause 15.01-1L – Urban Design
 - Clause 15.01-2S – Building Design
 - Clause 15.01-2L – Building Design
 - Clause 15.01-2L-01 – Environmental Sustainable Development
 - Clause 15.01-4S – Healthy Neighbourhoods
 - Clause 15.01-4R – Healthy Neighbourhoods
 - Clause 15.01-5S – Neighbourhood Character
- Clause 17 - Economic Development:
 - Clause 17.01-1S – Diversified Economy
 - Clause 17.01-1L – Employment
 - Cause 17.02-1S – Business
 - Clause 17.02-1L – Retail
- Clause 18 – Transport:
 - Clause 18.01-1S – Land Use and Transport Integration
 - Clause 18.02-1S - Walking
 - Clause 18.02-2S – Cycling
 - Clause 18.02-2L – Cycling
 - Clause 18.02-3S – Public Transport

- Clause 18.02-3R – Principal Public Transport Network
- Clause 18.02-4S – Roads
- Clause 18.02-4L – Road System
- Clause 18.02-4L-01 – Car Parking
- Clause 19 – Development infrastructure:
 - Clause 19.03-3L – Water Sensitive Urban Design

Other relevant documents

35. Urban Design Guidelines for Victoria (DELWP).
36. Yarra Economic Development Strategy 2020 – 2025.

Officer Assessment

37. The primary considerations for this application are as follows:
 - (a) Policy and Strategic support;
 - (b) Land use;
 - (c) Built form and urban design;
 - (d) Environmentally sustainable development and Internal Amenity;
 - (e) Off-site amenity impacts;
 - (f) Car parking, traffic and bicycle facilities;
 - (g) Signage;
 - (h) Other matters; and
 - (i) Other objector concerns.

Strategic Support

38. In relation to the policies in the Planning Policy Framework (PPF), it is considered that there is strategic support for a multi-storey mixed use building on the subject land. Policies in the PPF encourage intensifying development on sites well connected to public transport.
 39. The site is located within an area of Industrial land in Clause 02.04 (Strategic Framework Plan), which is described at Clause 02-01-8 (Economic Development) as areas that are considered “employment land”, which is defined as follows:

Commercial and industrial land outside of activity centres and major employment precincts, such as CUB in Abbotsford and Botanicca Corporate Park in Richmond as shown in clause 02.04 (Strategic Framework Plan). These areas support employment uses on individual sites or within broader precincts, where employment uses have been maintained through commercial or industrial zoning.
 40. Further to this, Clause 02.03-6 (Economic Development) supports maintaining an adequate supply of employment land to accommodate projected economic and employment growth that meets the diversity of business needs, while encourages preserving and growing Yarra’s employment areas by supporting employment land in the industrial and commercial land shown in clause 02.04 (Strategic Framework Plan).
 41. There will be employment opportunities associated with these uses, while will provide a service to the immediate and wider community, consistent with Clause 17.02-1S –
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Business. The objective of this clause is to encourage development that meets the community's needs for retail, entertainment, office and other commercial services, and the proposal will respond to this.

42. Both State and Planning Policy Framework directives seek to promote the use of sustainable personal transport and increased development close to high-quality transport routes (Clauses 18.01-3L, 18.02-1L, 18.02-2L and 18.02-4L). The site is well connected to public transport opportunities, with tram services along Swan Street (130m north) and Burnley Train Station (90m north-east). This access to public transport encourages the use of alternative transport modes to and from the site; an outcome that is encouraged by Clause 02.03 (Strategic direction) and Clause 18 (Transport).
43. The site is well-positioned to accommodate development, with excellent accessibility to jobs, services and good access to public transport, but this must be balanced against the design response to the surrounding character. Policy support for development must be balanced with built form guidance at Clause 15.01-2L (Building design). This policy calls for development that responds to the surrounding context with regard to urban character. Development must also consider off-site amenity impacts (i.e., overlooking, overshadowing, noise, wind) on surrounding private and public land. These aspects will be discussed in detail later in the report.
44. Having regard to the above, the proposed re-development of the site for a multi-storey, mixed use development is considered to have strategic planning support.

Land use

45. Both the proposed office and the food and drink premises require a planning permit for the use under the zone. As discussed in the previous section, the subject site is considered an appropriate location for a multi-use development given its location being close by to the Swan Street Major Activity Centre (MAC), numerous forms a public transportation and is well serviced by existing infrastructure and services.
46. With respect to land use appropriateness, there is support where it can be demonstrated that use operation would not result in an unreasonable impact to any surrounding residential areas and nearby industrial uses. The purposes of the IN3Z include:
 - (a) *To implement the Municipal Planning Strategy and the Planning Policy Framework;*
 - (b) *To allow limited retail opportunities including convenience shops, small scale supermarkets and associated shops in appropriate locations; and*
 - (c) *To ensure that uses do not affect the safety and amenity of adjacent, more sensitive land uses.*
47. The proposed Food and Drink Premises would be positioned at ground level, with direct access to both Burnley Street and Madden Grove. The Office use is proposed at all levels, with the primary access from the courtyard at ground level. The site is well positioned close to public transportation nodes, including Burnley Train Station, 90m north-east of the site. The development is also positioned close to the Swan Street Major Activity Centre further to the north. And generally the uses are appropriate in this context.
48. The proposal achieves an active commercial frontage to the ground floor courtyard, with the floor plans identifying Tenancy 1 as "F&B" (Food and Beverage premises). This use has not been specifically detailed in the application (i.e. the Yarra Planning Scheme includes "Food and Drink Premises) and no details of the operation of the business provided. Under the definition of a "Food and Drink Premises" this includes uses such as:

Bar, Convenience Restaurant, Hotel, Restaurant and Take away food premises. As no details have been provided as to the operation of Tenancy 1, and given the need for a planning permit for the use is it considered that the plans be updated to reference a restaurant/café as the most appropriate use in this context (this use also reflects similar office developments that provide a food offering). Given the location of residentially zoned land within 60m of the site, this use is acceptable subject to conditions of operation and limitation on patron numbers (to be discussed below).

49. In respect to amenity impacts from the proposed uses, Clause 13.07-1L-01 of the Scheme (Interfaces and Amenity) seeks to ensure that new non-residential uses and development be designed to minimise the impacts of noise, fumes and air emissions, storage and waste, light spill, and overlooking and unsightly views upon nearby existing residential properties, as well on non-residential uses. Amenity impacts relating to built form will be discussed later within the report, with the following discussion limited to the impacts of the use of the Office and the Food and Drink Premises.
50. The closest residential properties to the subject site are across Burnley Street, approximately 60m to the east, separated by the raised Burnley Street bridge and car parking. Hours of operation and patron numbers to the proposed Food and drink premises have been excluded from the submission, as have operation hours to the office.
51. Clause 13.07-1L-01 does not limit hours of non-residential uses within an Industrial Zone, however does limit hours of operation for a use in a residential zone to 8am to 8pm. Although hours outside of these residential limits may be acceptable given the industrial zone and setback from residential properties, these are useful as a guide for the proposed office and restaurant/café. Given the context and the separation of the residential and industrial zones with a major road, it is considered appropriate to allow the restaurant/café to operate between 8.00am and 10.00pm on any day as these hours would allow for the reasonable operation and not intrude past 10.00pm when residential properties would expect less commercial activity. This will be addressed by condition.
52. Further to this, no patron numbers have been identified for the restaurant/café. Based on a floor area of 140sqm, it is considered that a maximum of 90 patrons is appropriate in this instance. While additional patrons may be considered acceptable, without any details or management plans in place a maximum of 90 patrons would provide a safe and amenable operating capacity. This will be addressed by condition.
53. While an office is a less intrusive use, given the potential numbers and various terraces that face or are proximate to the residential zone, it is considered that the office be limited to hours of operation between 8.00am-8.00pm.
54. In terms of access, people visiting the site would typically access the property from Burnley Street and Swan Street to the north, where public transportation is located, rather through the residential area directly to the east.
55. In respect to noise, given the nature of the uses and distance from residential properties, the impacts are expected to be an appropriate balance between the industrial zone (where industrial uses do not require a planning permit and can operate longer) and the residential zone on the east side of Burnley Street. An Acoustic Report was submitted with the application which assessed the potential noise impacts from the site to surrounding residential properties, in particular the potential plant noise emissions and the music noise from the retail tenancy. Regarding the plant noise emissions, the report specifies that it is expected that compliance with EPR 2021 noise limits can comfortably

be achieved using standard acoustic treatments such as acoustically lined ductwork, duct attenuators and acoustic screening as required. Regarding Music Noise Emissions the report recommends the following:

- (a) Any music noise emissions from the retail tenancies shall comply with the Environment Protection Regulations 2021 Part 5.3 Division 4 – Unreasonable and aggravated noise from entertainment venues and outdoor entertainment venues;
 - (b) Any music played at greater than background level as defined in Section 9A of the Liquor Control Reform Amendment (Licensing) Act 2009 shall be played via a limiter set such that music noise levels do not exceed the applicable Environment Protection Regulations 2021 Part 5.3 noise limits for indoor entertainment venues. Consistent with Section 9A of the Liquor Control Reform Amendment (Licensing) Act 2009, background music means a level that enables patrons to conduct a conversation at a distance of 600 millimetres without having to raise their voices to a substantial degree; and
 - (c) Fixed speakers shall not be installed outdoors.
56. These conditions as well as other standard conditions will require noise within the premises to comply with the Environment Protection Regulations, music to be played at background level and no speakers to be installed external to the building.
57. In relation to deliveries to the site, it is expected that these will be small and infrequent, given the nature of the uses. Waste collection will occur within the basement level 1 and will therefore not result in unreasonable amenity impacts. Conditions will be included limiting deliveries and waste collection, in accordance with Council's Local Law guidelines, as well as a general amenity condition.
58. Overall, for the reasons above, the proposed Office and Food and Drink Premises uses are supported and will not result in unreasonable amenity impacts, subject to conditions outlined throughout.

Built form and Urban Design

Design Context

59. The primary built form considerations for the proposed development are the decision guidelines at clause 33.03-4 (Industrial 3 Zone) and policy at clause 15.01-1L (Urban Design) and clause 15.01-2L (Building Design). All of these provisions and guidelines support a development outcome that responds to the existing or preferred neighbourhood character and provides a contextual urban design response reflective of the aspirations for the area. Particular regard must be given to the acceptability of the design in terms of height and massing, street setbacks and relationship to adjoining buildings.
60. As specified previously within the report, the subject site is located within an industrial area, with built form generally low scale, between one and three storeys. Immediate interfaces with the site include:
- (a) North – Railway line between and double storey commercial buildings beyond;
 - (b) West – Part single and part triple storey commercial premises;
 - (c) East – Public park and single to triple storey dwellings beyond; and
 - (d) South – Double storey industrial building.

61. As outlined previously, there is no dispute that strategically the subject site is appropriately located for a higher-density development, being within the IN3Z, and with excellent access to cycling networks, public transport, services and facilities. Based on these attributes, it is a reasonable expectation that this site will experience intensification in use and development
62. In the absence of structure plans or height controls, each application must be considered on its merits. The strategic planning context for the site has been established and is considered to support re-development of the site including for a multi-storey, mixed use building. However, this must be balanced against achieving acceptable design outcomes for the site having regard to relevant built form polices outlined in Clause 15.01-2S (Building design).

Height massing and setbacks

63. Clause 15.01-2L of the Scheme encourages design development to reflect and respond to streetscape elements, including pattern of development and building spacing. As outlined above, the subject site is located within an area of generally low scale development between one and three storeys. There is an emerging character of more intense commercial development within the Swan Street MAC, further north of the subject site. This is evident in the recently constructed building at No. 480 Swan Street Richmond for a 12 storey commercial building (approved under PLN20/0006). This building contributes to the emerging built form character in the area, and is part of the subject site's immediate context, as is the location of railway lines to the north and industrial zoned land to the west and south (where taller development would be expected).



Figure 8 – Artist's impression of No. 480 Swan Street Richmond (Source: Endorsed Plans PLN20/0006)

64. Although the site is not located directly within a MAC or a DDO with clear built form direction, it is within an area in which substantial redevelopment opportunities exist. The proposed development will present as a 10 storey building equating to 37.97m in height. Although the building has been designed to reduce the perception of scale through use of various materials including brick for the street wall/base and glazing above as well as various setbacks from the east boundary, the building will be significantly higher than existing industrial zoned land to the south and west. Council's external Urban Designer's (Blades Studio) were supportive of the overall height and scale of the development

specifying that it will comfortably integrate with the emerging development trajectory of the subject site’s local area.

- 65. Further to this, the subject site has other physical qualities that would support greater built form density including the site’s setback from the residentially zoned land to the east, the positioning of the site close to various public transport options, the site being surrounded by two streets (inclusive of elevated bridge) and a laneway. Further the site faces onto the railway lines to the north (i.e.. another non-sensitive interface). Council’s Urban Designer (Blades Studio) also noted that there is no footpath opposite the subject site and therefore there are no real close range perceptions of the building.
- 66. As identified within the image below, there is larger built form character emerging within the area, particularly within the Swan Street MAC to the north of the site, and the proposed development will fall within this cluster.

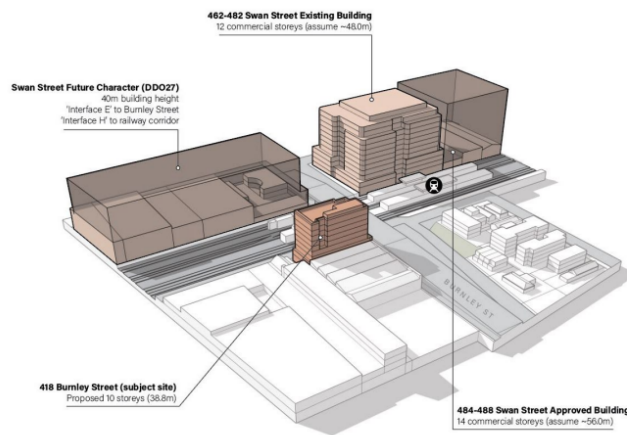


Figure 9: Proposed built form scale (pink) relative to existing approved and potential building scales within close proximity of the site (Source: Blades Studio, 5 August 2025)

- 67. Pursuant to Clause 15.01-1L of the Scheme, development should incorporate setbacks that reflect the general pattern of front side and rear setbacks in the streetscape, as well as limit excessive tiered building profiles on street and laneway frontages and encourage use of contextual materials and finishes at upper levels. In terms of street wall setbacks, the proposed development aligns with the curved property boundary on the north-east corner of the site. Council’s Urban Designer’s (Blade Studio) was supportive of this design noting that this would appropriately respond to the longer-range viewshed and ‘arrival experience’ of pedestrians on approach from Burnley Train Station further east.
- 68. The proposed development incorporates upper level setbacks from the north, east and west boundaries between 1.55m and 3.82m. These setbacks vary in response to the physical context of each abutting boundary property.
- 69. As outlined above, the site abuts a laneway to the west, with a single storey ‘individually significant’ graded building beyond. Given the width of the laneway, the upper level would be setback approximately 5.6m from the heritage building. Although the subject site is not located within a DDO or a heritage overlay and there is an absence of built form policy direction within this area, the building separation requirements consistently used within other DDO’s in Yarra, require a 3m setback from the centre of the laneway. Although not a requirement for this development, this setback provides some guidance for an appropriate built form response. In this instance, the proposal will comply with this requirement (3.6m setback from the centre of the laneway) which will ensure the form

does not dominate the heritage building, while the setback will allow for equitable development. Council's Heritage Advisor was also supportive of the proposal's response to this heritage building, noting that there would be no physical impact to the heritage building given this setback.

70. The setbacks from the north and east boundaries are to Madden Grove and Burnley Street, with the existing rail corridor to the north and the wide Burnley Street corridor and elevated bridge to the east of the site providing an adequate spatial buffer for building prominence. Further, the street widths ensure the development won't result in impacts to close by residential properties.
71. A deeper level 2 recess from the north has been proposed to improve the tower-podium separation. However, the setback does not go far enough to define an adequate separation between the forms (currently minimum 1.55m). An increased setback would create a more effective visual separation. If Council were in a position to support the proposal, a condition would require a setback of 2.94m from the northern boundary of level 2, consistent with the terrace setback within the north-west corner. This would also improve the functionality of this space.
72. Council's Strategic Planning Unit reviewed the application and recommended the following changes in relation to built form and scale:
Reduce the height by two storeys:
 - (a) Provide 3m upper-level setbacks to Madden Grove and Burnley Street;
 - (b) Provide a 3m building separation for upper levels above 20m on the southern boundary; and
 - (c) Extend the ground floor setback to connect to the second tenancy.
73. These changes would go above and beyond what would be reasonable within the site context, given the above assessment and is not supported by any policy. The northern setback is considered appropriate subject to the lobby wall where it is setback 0.665m being pushed to the boundary to ensure there is no area of concealment. As detailed above, the height, scale and setbacks of the proposed development is appropriate subject to conditions, would appropriately respond to the surrounding context and is considered acceptable.

Street Wall

74. The development proposes a three- storey street wall to Burnley Street, which steps down to a two- storey street wall along Madden Grove. The Burnley Street street wall would provide a comfortable response to the street particularly given it's location on a corner (where taller built form is appropriate), while integrating with existing and future street wall heights further north under the DDO27. The lower Madden Grove street wall height has been designed to respond to the adjacent heritage fabric at No. 44 Madden Grove, and although higher than the existing street wall of this property provides an adequate transition. Council's Urban Designer (Blade Studio) was supportive of the proposed street walls.
 75. The building has been designed with the absence of a street wall within the north-east corner of the development and extending the tower glazing down to enhance the lobby entrance. Council's Urban Designer (Blade Studio) was supportive of this approach, noting that the public realm is widened and the relationship between the building height and public realm is improved.
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76. The western boundary street wall opposite the laneway is proposed to be concrete material to the same height as the Madden Grove street wall. Views to this wall remain visible from the public realm and should therefore be of a higher quality material. As such a condition will require the brick material already on the Burnley Street and Maddon Grove street walls to wrap around to this wall as well. This was also recommended by Council's Urban Designer (Blades Studio).

Public realm and pedestrian spaces

77. Clause 15.01-2S encourages the interfaces between buildings and public realm to enhance the function and amenity of the public realm, while support personal safety, perceptions of safety and property security. This can be achieved by designing developments to provide a high level of pedestrian amenity and visual interest, by incorporating well-defined entries at ground level and installing glazed areas which allow permeability into the interior space.
78. At ground level, the proposed development includes a recessed office tenancy along Madden Grove with the primary pedestrian entrance at the north-east corner of the site through a recessed courtyard. There is an absence of a street wall in this corner, with the tower glazing extending down to the entrance, which serves to open up the corner with a more light-weight (i.e.. glazed) tower element. The level of activation to both street frontages is good and the interfaces between buildings and public realm would enhance the function and amenity of the public realm. Council's Urban Design Unit were supportive of the proposed public realm response, although recommended changes to improve the ground floor program with the street. These changes include the following:
- (a) Rationalise setbacks to addresses small poorly defined and underutilised spaces along the building frontages on both Madden Grove and Burnley Street;
 - (b) Ensure clear delineation between the public and private site boundary;
 - (c) Provide footpath levels and FFL at entrances and within setbacks to ensure a seamless transition; and
 - (d) Confirm the substation location and access meets the service authority requirements.
79. If Council were to support this development, conditions would require the ground floor north-facing window to the lobby setback 0.664m to abut the boundary, and deletion of the ground floor brick element between the entrance to the food and drink premises entry and the water meter. This would also assist in utilising space, while eliminating places for concealment and entrapment.
80. Conditions will also require clear delineation between the public and private site boundary through the use of alternate paving and footpath levels and to include finish floor levels at entrances and within setbacks provided to ensure seamless transition. It is inappropriate to require confirmation that the location of the substation would meet the service authority requirements as this is an action the applicant is required to undertake.
81. The extent of glazing to the upper levels and location of balconies along both Madden Grove and Burnley Street would also assist in providing surveillance to the public realm.

Shadows

82. In regard to sunlight access within the public realm, Clause 15.01-2L provides the following guidance:

Unless specified elsewhere in this scheme, ensure development avoids overshadowing of public open space between 10am and 2pm on 22 September.

83. The applicant submitted shadow diagrams hourly between 9am and 3pm. Given the corner allotment, double storey building constructed to the south, and width of Burnley Street to the east the proposed development would not cause an unreasonable increase in shadow impacts on any public open space. This outcome is acceptable, while there will be no unreasonable loss of solar access to the opposing footpaths throughout the day.

Wind

84. Clause 15.01-1L seeks to minimise the potential for development, particularly taller buildings, creating adverse wind conditions in the public realm at street level through building siting and design. Council's Wind Engineers (Vipac) peer reviewed the submitted Wind Assessment and was supportive of the analysis. They agreed that the proposed design is not expected to generate wind levels in excess of the recommended wind comfort criteria at the ground level, pedestrian areas and various terraces and agreed that no modifications to the proposed development are required for the public realm.

Architectural Quality

85. Policy at Clause 15.01-2L encourages design developments to make a positive contribution to the streetscape through high quality architecture and urban design. This policy further supports mid-rise development that contributes to high-quality built form.
86. The material palette of the proposed development includes long brick in grey, concrete and exposed aggregate concrete walls, powdercoated silver finish to metal (to window frames, louvres, canopies and plant screening) and clear glazing, including with solid back panels. This is generally consistent with Clause 15.01-2L which encourages a simple palette of durable materials that respond to the streetscape character through type, colour, finish and contrast. The materials proposed are not uncommon for industrial buildings, and the existing streetscape comprises buildings constructed of brick and concrete, while the use of metal cladding and framing is extensive. Council's Urban Designers (Blade Studio) was generally supportive of the palette of materials, noting that the intended architectural resolution is well-resolved and appropriate and would result in a high-quality design outcome.
87. However the materiality of the street walls lacks refinement and the simplified brickwork should provide enhanced texture, variation, and articulation for an appropriate street-level experience and better materiality response to the adjoining heritage building at No. 41 Madden Grove. Were Council in a position to support the proposal, conditions would be included requiring the materiality of the Madden Grove and Burnley Street street walls to enhance the texture, variation and articulation.
88. The upper levels of the north, east and west elevations use extensive panel glazing. The elevations and sections on the plans show some horizontal shading elements that assist in breaking up the glazing panels. However, there remains a lack of visual interest and articulation to the tower level facades. The elevations show no mention of colour, tone, fritting or frosting, which would assist in improving the articulation of these facades (which would also assist in reducing glare). If Council were to support the development, a
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condition would require the tower windows to be treated to break up the clear glazing using colour, tone, fritting or frosting of the windows.

89. Clause 15.01-2L encourages development to make a positive contribution to the streetscape through high quality architecture and urban design and discourage blank or unarticulated walls. The southern boundary wall has been articulated with precast concrete in a sporadic pattern. Council's Urban Designer (Blade Studio) raised concerns with this patterning recommending that this be further refined by the introduction of a level of banded horizontality that aligns with the expressed 'GSL02' solid backed panelling of glazing to achieve an outcome that reads as more integrated 'in the round', or could pick up better on the verticality of proposed tower fins. However the current randomised arrangement of the southern boundary creates visual interest and breaks up the form, particularly with the setbacks to the open stairwell. The exposed aggregate concrete material however is too similar to the proposed concrete and needs to be better differentiated from the concrete material on this wall. A condition will require a more textured material (i.e. ribbed concrete) to articulate the facade.
90. In addition, a Façade Strategy and Materials and Finishes Plan will be required via condition to ensure the specific details of the facade, materials and finishes are considered acceptable.

Glare impacts

91. Planning Practice Note 96 (issued in December 2022) provides guidance regarding planning considerations for reflected sunlight glare in the built environment. PPN96 states that reflected glare risks can arise when cladding, painted walls or concrete have matt or smooth finishes, although reflection is generally less intense than from glass or polished cladding. The development includes extensive glazing to the upper levels north, east and west facades, and the potential glare impacts from these windows onto neighbouring properties, the public realm, the natural environment, traffic along the street, and in particular the railway line, is unknown. A condition will be included within the recommendation requiring a Glare Report.
92. Further to this, the application was referred to Head, Transport for Victoria and VicTrack were notified of the application. Head, Transport for Victoria did not object to the proposal, although did include a number of conditions including the submission of documents to confirm glare impacts to the railway corridor. VicTrack also did not object to the proposal, although did recommended a condition be included requiring the rail operator be contacted to obtain conditions and safety requirements for works on, over or adjacent to the railway land. This condition was already required as part of Head, Transport for Victoria conditions.

Landmarks, Views and Vistas

93. It is policy at Clause 15.01-2L-02 of the Scheme that important landmarks, views and vistas be protected or enhanced. The impact on long range views and vistas are only relevant where they form part of an identified character of an area (within planning policy) and typically apply to landscapes or natural features.
94. Table 1. (Landmark primary viewpoints and architecture significant elements) of Clause 15.01-2L-02 identifies a number of landmarks within Richmond. However, the closest landmarks to the site (Former Dimmeys store) is over 1km from the site and would not be impacted by the proposal.

Site Coverage

95. The level of site coverage proposed substantially exceeds the maximum of 80% suggested by Clause 15.01-2L. However, as the existing level of site coverage in the surrounding (and immediate) area and within a zone that encourages higher density development (IN3Z) is similar, it is acceptable. Commercial/industrial buildings within this precinct traditionally have high levels of site coverage with this characteristic evident along this part of Burnley Street.

Landscaping

96. Clause 15.01-2L encourages developments to provide landscaping that responds to its site context, enhances the built form, creates safe and attractive spaces and supports cooling and greening of urban areas. The proposal includes landscaping within the ground floor courtyard and the public realm, while planters are proposed to level 2 and level 3 balconies and extensive planting to the roof top terrace. The plant schedule includes a variety of shrubs, grasses and trees. Council's Urban Design Unit confirmed that that the extensive use of the proposed landscaping on site is commendable in respect to the surrounding broader context.
97. A condition will require a street tree/security bond for the protection of the trees in Madden Grove as requested by Council's Open Space team.
98. Overall, the landscaping is considered acceptable and standard conditions will require the landscape plan to ensure ongoing maintenance.

Services and equipment

99. The service equipment has been integrated into the overall design and massing of the building. The service equipment will be contained on the roof top, within the basement levels, while the fire control panel, hydrant, booster, water meter and main switch room will be integrated within the building at ground level. Clause 15.01-2L seeks that service infrastructure is appropriately site and incorporated into the design of new buildings which has limited visibility from the public realm. Given the above, the proposed positioning of the service equipment would be acceptable.

Environmentally Sustainable Design and Internal Amenity

100. Policy at Clauses 02.03 (Strategic Directions), 15.01-2S (Building Design), 15.01-2L (Building Design) 15.01-2L-01 (Environmental Sustainable Development) and 19.03-3L (Water Sensitive Urban Design) of the Scheme encourage environmentally sustainable development (ESD), with regard to water and energy efficiency, building construction and ongoing management. The following are key ESD commitments of the proposed development:
- (a) Good daylight levels throughout the office areas;
 - (b) Fresh air supply to exceed AS1668 by at least 50%;
 - (c) Thermal performance to exceed NCC2019 minimum;
 - (d) Energy efficient heating, cooling, hot water and lighting;
 - (e) A combination of vertical shading fins and horizontal fixed awnings;
 - (f) A minimum 17.7kW solar PV array to the rooftop;

- (g) 103% STORM score relying on a minimum of 518m² of roof connected to a 16,000 litre tank for toilet flushing on Ground and Level 1;
- (h) 40% of coarse aggregate for concrete from recycled sources;
- (i) One electric vehicle charging point (minimum) and electric vehicle readiness to all levels of carpark, including a minimum 10% of car parking spaces; and
- (j) 80% of construction and demolition waste recycled/reused and diverted from landfill.

101. Council's ESD advisor was generally supportive of the proposal, specifying that the submitted SMP largely meets Council's Environmentally Sustainable Design standards. They did however note a number of deficiencies and outstanding information that will be required via condition. This includes deletion of incorrect claims relating to Innovation within the BESS Report, and a green wall adjacent to the ground floor entrance not being shown on the elevations. These are addressed by conditions.

Internal Amenity

102. Clause 15.01-2L of the Scheme seeks high-quality environments for building occupants including internal spaces, access to private open space, daylight and ventilation.
103. The development provides large expanses of glazing at all levels. The SMP submitted indicates that the office areas would be ventilated through mechanical measures. The mechanical ventilation system would be designed to achieve, monitor and maintain an increase in outdoor air of at least 50% above the Australian Standard (1668.2:2012). Council's ESD officer is satisfied with this outcome.
104. The development also achieves good access to daylight for the office levels as well as the restaurant/cafe at ground level. Based on the daylight modelling provided, the office areas achieve daylight factors to at least 45% of the floor area, exceeding the minimum 33% required to achieve best practice. The restaurant/cafe area will achieve daylight to 33% of its nominated area, also complying with the minimum 33% required to meet best practice. Council's ESD officer was satisfied with this outcome.
105. The development includes a recessed entrance area that provides direct access to the office lobby and the restaurant/cafe, as well as the lift core and end-of-trip facilities and service rooms. A communal outdoor area is also proposed at roof level, while the second and third floor offices include external terraces. All of these areas enhance the on-site amenity of the building. Each office level will also be equipped bathrooms and services cupboard for staff use. Separate bathroom facilities are also provided at ground level, as well as bicycle parking and end of trip facilities, including lockers.
106. Furthermore, Clause 15.01-2L specifically identifies that developments should be encouraged to provide:
- (a) *Lighting of common spaces;*
 - (b) *Passive surveillance of common spaces; and*
 - (c) *Landscaping that maximises sightlines.*
-

107. The development adequately responds to the above requirements, noting that this is a proposal for a commercial use with large open plan floor plates with access to daylight direct from windows due to the dual aspect allotment.
108. While the common roof terrace does not receive passive surveillance from the office tenancies, it is considered that this is not warranted. Due to the nature of the use this area would be utilised typically during office hours. To ensure the use of the terrace does not have an unreasonable amenity impact on the surrounding area and as set out in the acoustic report, a condition will ensure that the roof terrace is only accessible to office employees and is not accessible before 8.00am or after 8.00pm (during more sensitive, night-time hours).
109. As outlined previously within the report, the landscaping on site has been maximised within the site.

Off-site amenity impacts

110. Clause 15.01-2L of the Scheme aims to provide building design that minimises the detrimental impact of development on neighbouring properties, the public realm and the natural environment, with potential impacts relating to overshadowing, loss of daylight to windows, visual bulk and overlooking of sensitive areas. The relevant policy framework for amenity considerations is contained within this clause, as well as Clause 13.07-1L-01 (Interface uses policy).
111. In this instance the site is surrounded by land within the IN3Z (to the west and south), C1Z (to the north) and GRZ (to the east side of Burnley Street, with the nearest residential uses occurring within the GRZ approximately 60m to the east.
- Visual bulk
112. In regard to visual bulk impacts to the dwellings to the east, it is considered that within this built form context views from windows to built-form is a reasonable expectation. As outlined above, the subject site is located over 60m from dwellings to the east, and therefore these properties will not be unreasonably impacted.
- Overlooking
113. The closest habitable room windows and SPOS within proximity of the site are associated with dwellings on the opposite side of Burnley Street to the east. Although not applicable in this instance, the overlooking objective of Clause 55 (Standard B4-4) specifies that new development should be designed to avoid direct views into habitable room windows and SPOS of dwellings within a radius of 9 metres and 45 degree arc.
114. Although each office space has a number of windows that face east towards these dwellings, the horizontal distance from the plane of the window well exceeds 9m (more than 60m). Therefore, there would not be any unreasonable overlooking from the site.
- Overshadowing
115. Clause 15.01-2L discourages impacts on existing adjoining development through unreasonable overshadowing of secluded open space and loss of daylight to habitable room windows. The submitted detailed shadow diagrams show that there will be no shadow from the proposed development onto any surrounding areas of POS at any time between 9am and 3pm.
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Daylight to existing windows

116. The separation distance provided by Burnley Street would adequately address matters of daylight to the east. Regarding the north, south and west, the proposed development does not directly face any habitable room windows.

Fumes and air emissions, light spillage

117. The majority of the building is enclosed and the use conducted indoors (with the exception of the outdoor balconies associated with offices and a communal roof top terrace). The proposed uses would not result in unreasonable air emissions, with light spill from the building limited due to the nature of the uses (predominately office) and the location of the commercial ground floor use facing the corner of Burnley Street and Madden Grove. A condition will require the communal roof top terrace to be closed between 8pm and 8am on any day ensuring no impact during accepted sleep disturbance hours, as well as being consistent with the office hours. As stated earlier a condition will also limit the roof terrace to office staff only.

Equitable Development

118. To ensure the 'fair, orderly, economic and sustainable development of land' in accordance with the objective of the Act, matters of equitable development should be considered. To the west side of the laneway is an 'individually significant' graded building, and therefore development potential to this neighbouring site is constrained. Further, there is a lane between the subject site and this property with the upper levels of the proposed building setback, ensuring the development does not unreasonably impact the development potential to that property.
119. To the south, the property has been developed with a double storey industrial building. The development would be mostly constructed along this boundary which allows for equal development opportunities for the site to the south. With any potential development on this adjoining site, it is expected that built form at lower levels would be on the boundary (where this is a common feature of development in the area). The proposed development does not include any south-facing windows or terraces.
120. To the north and east of the site are Madden Grove and Burnley Street respectively, and as such, the sites beyond these are separated by the widths of these carriageways which provide a reasonable buffer (particularly when considering the railway lines to the north and the elevated bridge to the east).

Construction management

121. Policy at Clause 13.07-1L-02 requires all development to ensure that construction times, storage and disposal methods minimize disruption to nearby residential and business uses. A Construction Management Plan will be required to address this.

Car parking, traffic and bicycle facilities*Car parking and traffic*

122. As outlined within the report, the application seeks to provide 28 car parking spaces over the two basement levels. This is less than the requirements of Clause 52.06 (Car Parking) which requires 162 spaces. The variation is acceptable for the following key reasons:
- (a) The provision of no on-site parking for the office would encourage employees to use more sustainable forms of transport since the surrounding road network has a scarcity of available long-stay car parking;
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- (b) The majority of customers to the restaurant would be derived by from office employees as well as foot traffic from residents and employees within the surrounding area;
 - (c) The site is very well positioned in terms of pedestrian access to public transport nodes, shops, businesses and other essential facilities. The site also has good connectivity to the on-road bicycle network; and
 - (d) The following public transport services can be accessed to and from the site by foot: Burnley railway station – 90 metre walk, Swan Street trams – 130 metre walk, Church Street trams – 770 metre walk, East Richmond railway station – 800 metre walk.
123. The proposed development is considered to consistent with the objectives contained in Council's Strategic Transport Statement and is ideally located with regard to sustainable transport alternatives and the reduced provision of on-site car parking.
124. When considering the potential traffic from the development, the proposed peak hour directional split for office and food and drink premises traffic is as follows:
- (a) 0.5 trips per space during peak hours;
 - (b) 14 trips during AM peak hour; and
 - (c) 14 trips during PM peak hour.
125. Council's Development Engineering Unit confirmed that the traffic volumes generated by the proposed development is considered low and should not adversely impact the traffic operation of the surrounding road network.
126. Council's Development Engineering Unit also confirmed that general access arrangements, car parking modules and mechanical parking, gradients and swept path assessment would satisfy all relevant engineering requirements. Additional details are required regarding the width of the internal access ramp, positioning of the proposed gate to the laneway and the wheel stop distance within the basement car park. These requests will be required by conditions including standard general engineering conditions.

Bicycle provision and facilities

127. The development would provide 28 employee spaces 10 visitor spaces and 4 showers / changes rooms, which would exceed the requirements of Clause 52.34 of the Scheme. Council's Strategic Transport Unit were supportive of the proposal, subject to the following details being provided:
- (a) Provide all visitor bicycle parking in a publicly accessible area at ground level;
 - (b) Provide a minimum of 48 employee parking spaces in line with CASBE requirements;
 - (c) Identify on the architectural plans the width of all access paths to bicycle parking areas in accordance with AS2890.3 requirements;
 - (d) Identify spacing between bicycle parking spaces in accordance with AS2890.3 requirements; and
 - (e) The bicycle storage room must be within a secure lockable compound to provide secure access for employees. Identify on the architectural plans the security arrangements to be implemented.
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128. Further to the above, the location of the visitor bicycle spaces within the northern setback to the ground floor office is not a convenient location to the retail use. Conditions will require the above as well as the two retail visitor bicycle spaces relocated within the north-east corner entrance courtyard.

Signage

129. Given the size and illumination of the proposed signs on site a planning permit is required under Clause 52.05 of the Planning Scheme. Clause 52.01-1L-01 of the Scheme encourages signs that are consistent with the character of the area, respect the amenity of the area, are proportioned and designed to complement the host building and site and to preserve active frontages. The proposed sign along the Burnley Street (eastern) facade at ground level is small in scale (0.71m²) and will not impact the proposed frontage. Further, this sign is non-illuminated and will not impact the surrounding amenity.
130. The internally illuminated, high wall sign to the north is appropriate as this faces the railway line and commercial uses on the opposite site. Subject to standard conditions that ensure the lighting is not flashing or intermittent, this sign is acceptable. In relation to the southern high wall sign, this is shown as non-illuminated which is also appropriate as while it faces over industrial zoned land, it would also be visible from the residentially zoned land to the east. However the sign is shown outside of title boundaries, as such a condition will require this to be within title boundaries.
131. The signs appropriately complement the scale of the building and will face Burnley Street to the north and the south (road in a Transport Zone 2), which is supported for high wall signs. These signs would also not directly face residential properties to the east and are well setback from dwellings further the north and south. Given the above, the proposed signs are considered appropriate subject to conditions.

Other matters

132. The following matters are also of relevance to this application:
- (a) **Waste:** a WMP has been submitted and deemed satisfactory subject to conditions that have been included in the recommendation;
 - (b) **Transport Zone 2 (TZ2):** The application was referred to the Department of Transport, given the changes to the access to the road in a Transport Zone 2 (Burnley Street). The proposal was supported, subject to conditions. These conditions have been included in the recommendation;
 - (c) **New gate within the lane:** A gate is proposed within the west side lane. This lane is a public road and construction within this space is not supported. A condition will delete this gate within the recommendation; and
 - (d) **Civil Engineering:** Council's civil engineering unit were generally supportive of the proposal, subject to conditions including public realm details, public lighting etc.

Objector Concerns

133. Key objector concerns have been addressed in the assessment above. Other matters raised are addressed as follows:
- (a) **Setting a precedence for excessive building height within the area:** Future planning permit applications on this site or neighbouring/nearby land would be assessed against relevant planning policy and site conditions at that time. The possibility of setting a precedent cannot be substantiated and is not a relevant planning consideration;
 - (b) **Impacts during construction:** Disruption during the construction phase of development is a temporary and unavoidable consequence of development and not justification to reject development of the site, however a construction management plan would be required by condition;
 - (c) **The building is not sufficiently adaptable.** Council cannot consider potential future uses and how a development accommodates uses that aren't proposed;
 - (d) **Misleading and inconsistent documentation submitted:** Sufficient information to enable an informed view of the application has been made available for viewing as part of the notification process. Comments within the applicant's planning report would not change the overall planning assessment; and
 - (e) **Soil contamination impacts:** The site is not located within an Environmental Audit Overlay and consideration of the potential contamination on the site is assessed under relevant Environment Protection legislation (not planning legislation).

Conclusion

134. The proposed development is considered to demonstrate a high level of compliance with policy objectives contained within the Planning Policy Framework and Municipal Planning Strategy. Notably, the proposal achieves the State Government's urban consolidation objectives.
135. The proposal, subject to the conditions recommended, is an acceptable planning outcome that demonstrates compliance with the relevant Council policies.

Legal and Legislative Obligations

Conflict of interest disclosure

136. Section 130 of the Local Government Act 2020 requires members of Council staff and persons engaged under contract to provide advice to Council to disclose any conflicts of interest in a matter to which the advice relates.
137. The Officer reviewing this report, having made enquiries with relevant members of staff, reports that no disclosable interests have been raised in relation to this report.

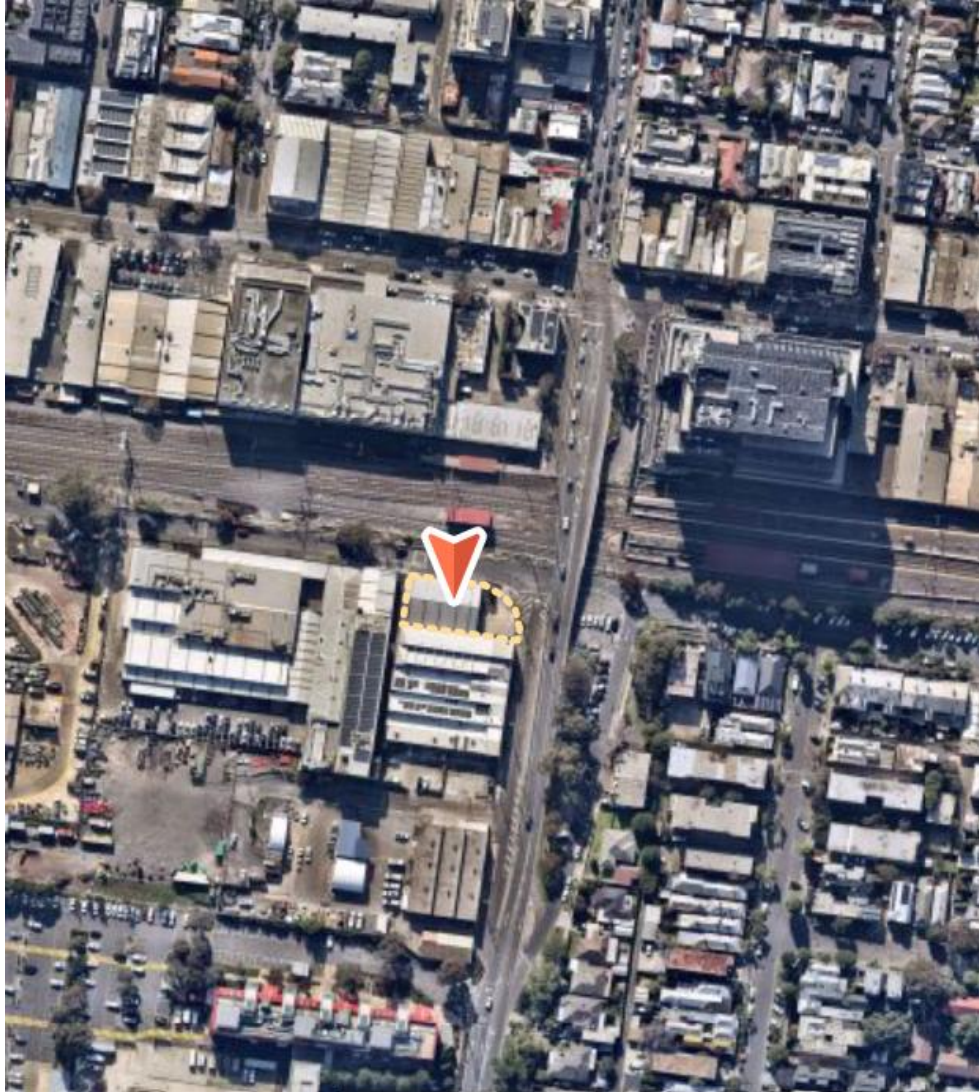
Report Attachments

1. 5.1.1 PLN 25 0181 - 418 Burnley Street Richmond - Site Context Map
2. 5.1.2 PLN 25 0181 - 418 Burnley Street Richmond - Decision Plans
3. 5.1.3 PLN 25 0181 - 418 Burnley Street Richmond - Urban Context Plan
4. 5.1.4 PLN 25 0181 - 418 Burnley Street Richmond - Referral Advice
5. CONFIDENTIAL -5.1.5 PLN 25 0181 - 418 Burnley Street Richmond - Objector List (CONFIDENTIAL)

This attachment is confidential information for the purposes of section 3(1) of the Local Government Act 2020 for the following reason:

- (f) personal information, being information which if released would result in the unreasonable disclosure of information about any person or their personal affairs.

Site Context Map



Source: NeapMap, Aerial imagery dated 3 August 2024



DRAWING LIST

TP-000 - PROJECT INFORMATION
 TP-000 COVER AND REGISTER

TP-010 - SITE PLANS
 TP-011 SITE PLAN - EXISTING AND DEMOLITION
 TP-012 SITE PLAN - PROPOSED

TP-100 - GENERAL ARRANGEMENT PLANS
 TP-101 GA PLAN - BASEMENT LEVEL 2
 TP-102 GA PLAN - BASEMENT LEVEL 1
 TP-103 GA PLAN - GROUND
 TP-104 GA PLAN - LEVEL 1
 TP-105 GA PLAN - LEVEL 2
 TP-106 GA PLAN - LEVEL 3
 TP-107 GA PLAN - LEVEL 4-8
 TP-108 GA PLAN - ROOF TERRACE
 TP-109 GA PLAN - ROOFTOP

TP-200 - BUILDING ELEVATIONS
 TP-201 NORTH ELEVATION
 TP-202 EAST AND WEST ELEVATIONS
 TP-203 SOUTH ELEVATION

TP-300 - BUILDING SECTIONS
 TP-301 BUILDING SECTION A
 TP-302 BUILDING SECTION B
 TP-303 BUILDING SECTION C&D

TP-900 - SHADOW DIAGRAMS
 TP-901 SHADOW DIAGRAM - EQUINOX
 TP-902 SHADOW DIAGRAM - EQUINOX
 TP-903 SHADOW DIAGRAM - EQUINOX
 TP-904 SHADOW DIAGRAM - EQUINOX

PARKING SUMMARY

PARKING TYPE	LEVEL	COUNT
CAR PARK	B1	10
CAR PARK	B2	17
		27
DDA CAR PARK	B1	1
		1
MOTORCYCLE PARK	B2	2
		2
RETAIL VISITOR BICYCLE PARK	GROUND	2
		2
TENANT BICYCLE PARK	GROUND	28
		28
VISITOR BICYCLE PARK	GROUND	8
		8

DEVELOPMENT SUMMARY _ NLA

AREA TYPE	AREA
GROUND	
F&B	140 m²
OFFICE	110 m²
LEVEL 01	
OFFICE	652 m²
LEVEL 02	
OFFICE	586 m²
LEVEL 03	
OFFICE	574 m²
LEVEL 04	
OFFICE	575 m²
LEVEL 05	
OFFICE	575 m²
LEVEL 06	
OFFICE	575 m²
LEVEL 07	
OFFICE	575 m²
LEVEL 08	
OFFICE	575 m²
TOTAL NLA	4937 m²

DEVELOPMENT SUMMARY _ NFA

LEVEL	AREA
GROUND	454 m²
LEVEL 01	689 m²
LEVEL 02	623 m²
LEVEL 03	611 m²
LEVEL 04	611 m²
LEVEL 05	611 m²
LEVEL 06	611 m²
LEVEL 07	611 m²
LEVEL 08	611 m²
LEVEL 09 ROOF	23 m²
TOTAL NFA	5454 m²

DEVELOPMENT SUMMARY _ GFA

LEVEL	AREA
B2	827 m²
B1	824 m²
GROUND	796 m²
LEVEL 01	803 m²
LEVEL 02	733 m²
LEVEL 03	698 m²
LEVEL 04	698 m²
LEVEL 05	698 m²
LEVEL 06	698 m²
LEVEL 07	698 m²
LEVEL 08	698 m²
LEVEL 09 ROOF	89 m²
TOTAL GFA	8260 m²

SITE AREA

856 m²

SITE COVERAGE_GROUND

COVERED AREA	809 m²	95%
UNCOVERED AREA	47 m²	5%

SITE COVERAGE_BASEMENT

COVERED AREA	828 m²	97%
UNCOVERED AREA	28 m²	3%

Builders / Contractors shall verify all dimensions before any work commences. Dimensions shall be checked and verified by a licensed surveyor. Any discrepancies are to be made known to the Architects / Designers studio prior to any works commencing on site. All shop drawings shall be submitted for review and manufacture shall not commence prior to the return of stamped shop drawings.
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GENERAL NOTES

NOTE: ALL AREA CALCULATIONS ARE ADVISORY ONLY AND ALL FIGURES SHOULD BE CHECKED AND VERIFIED BY A LICENSED SURVEYOR.

A 22.04.2025 ISSUE FOR TOWN PLANNING RF1 #1
 12.03.2025 ISSUE FOR TOWN PLANNING

Rev	Date	Chkd	Reason for Issue

Based on Drawings Received:

TOWN PLANNING

Level 4
 1-3 Newton Street
 Cremorne VIC
 3121 Australia
 +61 0458 627 344
 cm@studiomccue.com
 studiomccue.com



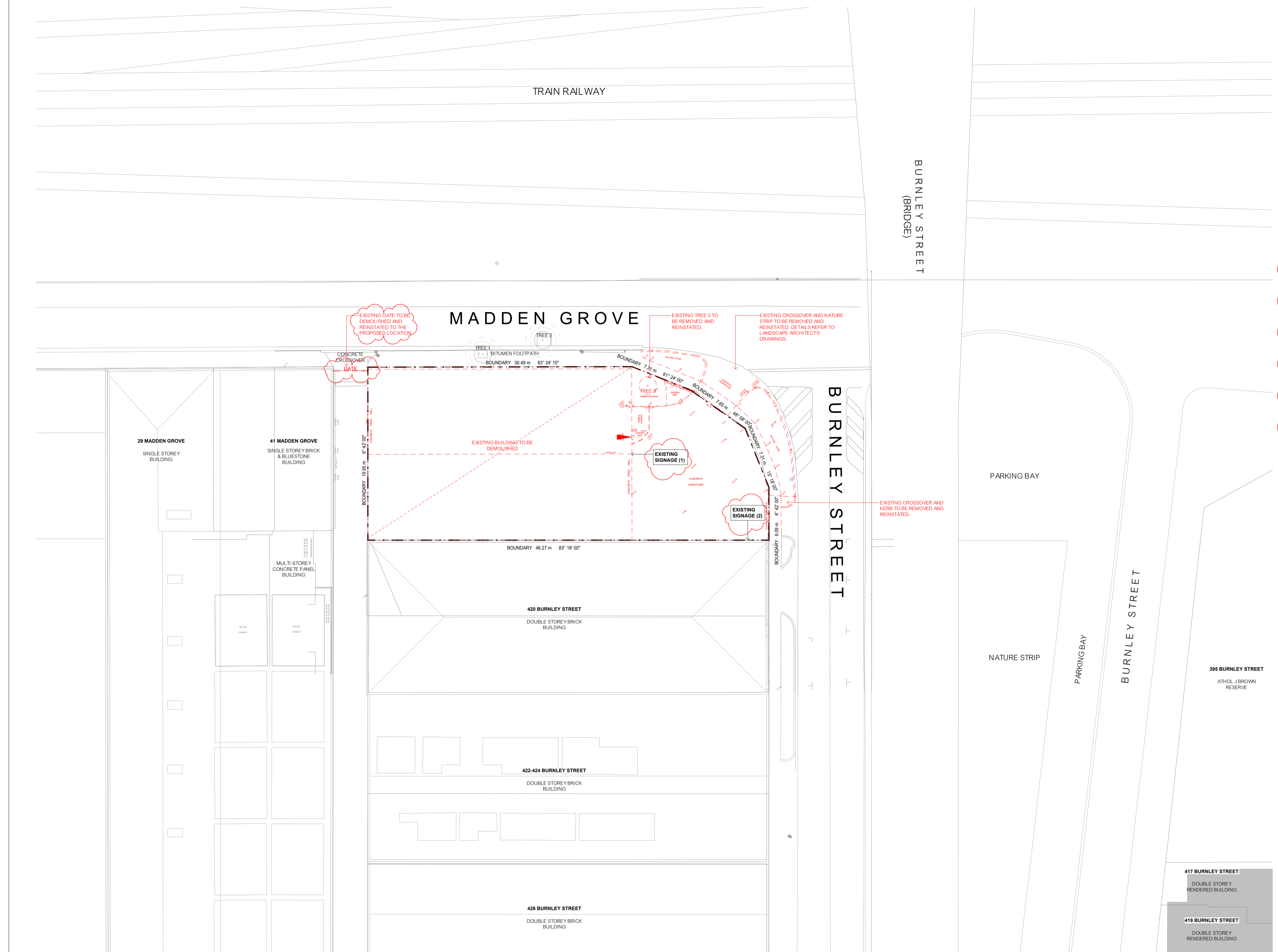
Project 25002 418 Burnley Street
 Office Building

Title COVER AND REGISTER

Date 22.04.2025 Project No 25002

Scale @ A1 Dwg No TP-000

Drawn By VZ Chkd CM Rev A



GENERAL NOTES

SITE AREA	
856 m ²	


LEGEND

- TITLE BOUNDARY
- - - LINE OF TPZ
- - - LINE OF SRZ
- - - TO BE DEMOLISHED


NOTE:
TO BE READ IN CONJUNCTION WITH LAND SURVEY AND THE ARBORIST REPORT.

EXISTING SIGNAGE

(1)
SIZE: APPROX. 4.5m x 1.2m
AREA: APPROX. 5.4 m²
SIGNAGE BOTTOM HEIGHT FROM NGL: APPROX. 4.8m

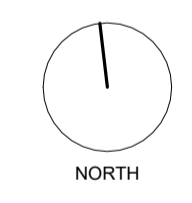


(2)
SIZE: APPROX. 6.8m x 3.1m
AREA: APPROX. 21.08 m²
SIGNAGE BOTTOM HEIGHT FROM NGL: APPROX. 4.6m



A 22.04.2025	ISSUE FOR TOWN PLANNING RFI #1		
12.03.2025	ISSUE FOR TOWN PLANNING		
Rev	Date	Chkd	Reason for Issue

Based on Drawings Received:



TOWN PLANNING

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1-3 Newton Street
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3121 Australia

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STUDIO MCCUE

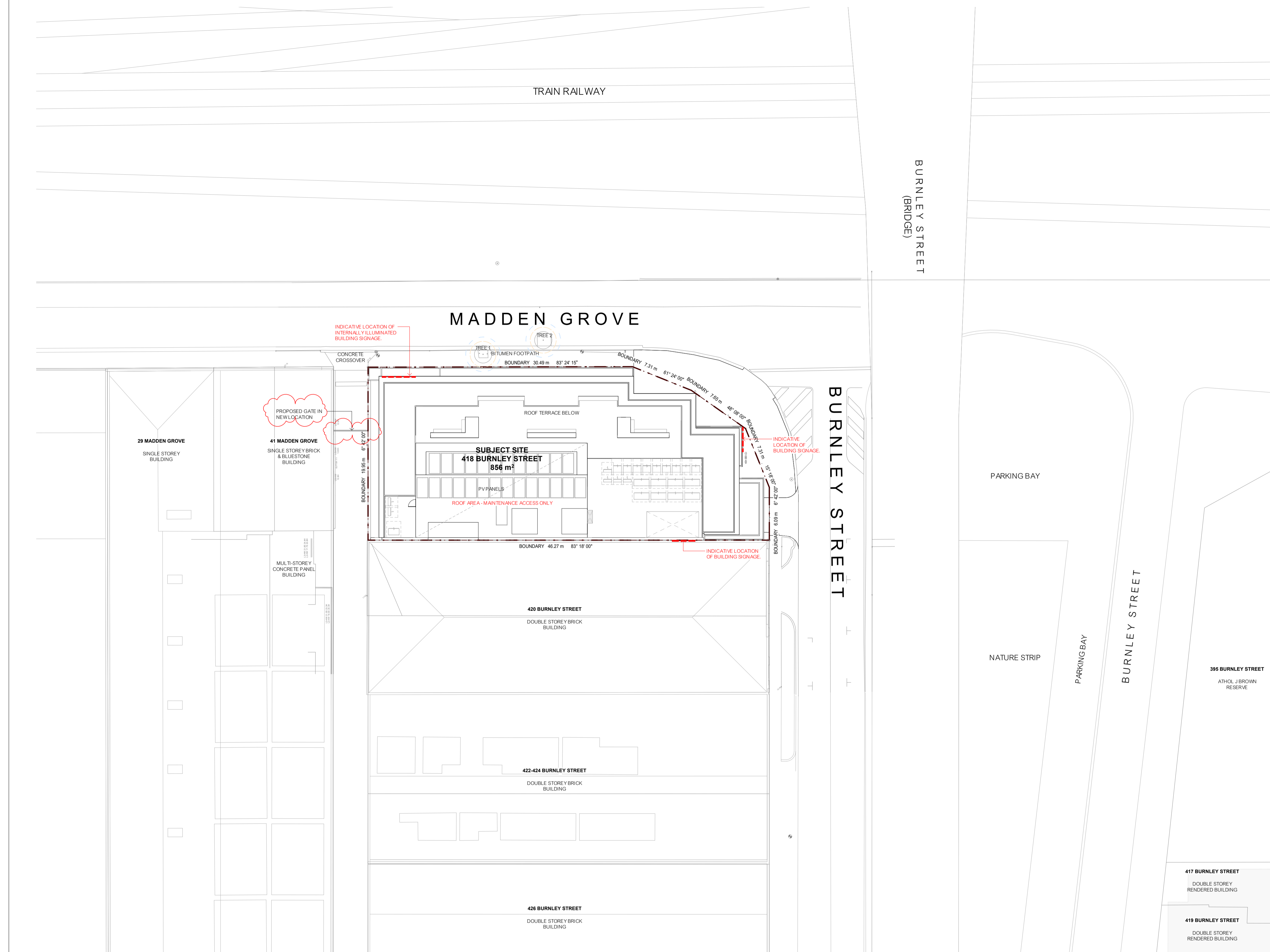
Project 25002 418 Burnley Street
Office Building

Title SITE PLAN - EXISTING AND DEMOLITION

Date 22.04.2025 Project No 25002

Scale @ A1 1:200 Dwg No TP-011

Drawn By VZ Chkd CM Rev A



GENERAL NOTES

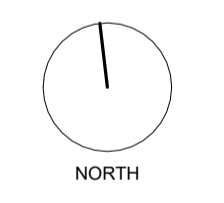
SITE AREA
856 m²

- LEGEND**
- - - - - TITLE BOUNDARY
 - - - - - LINE OF TPZ
 - - - - - LINE OF SRZ
 - - - - - TO BE DEMOLISHED

NOTE:
TO BE READ IN CONJUNCTION WITH LAND SURVEY AND THE ARBORIST REPORT.

A 22.04.2025	ISSUE FOR TOWN PLANNING RFI #1		
12.03.2025	ISSUE FOR TOWN PLANNING		
Rev	Date	Chkd	Reason for Issue

Based on Drawings Received:



TOWN PLANNING

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Project	25002 418 Burnley Street
Title	Office Building SITE PLAN - PROPOSED
Date	22.04.2025
Project No	25002
Scale @ A1	1 : 200
Dwg No	TP-012
Drawn By	VZ
Chkd	CM
Rev	A

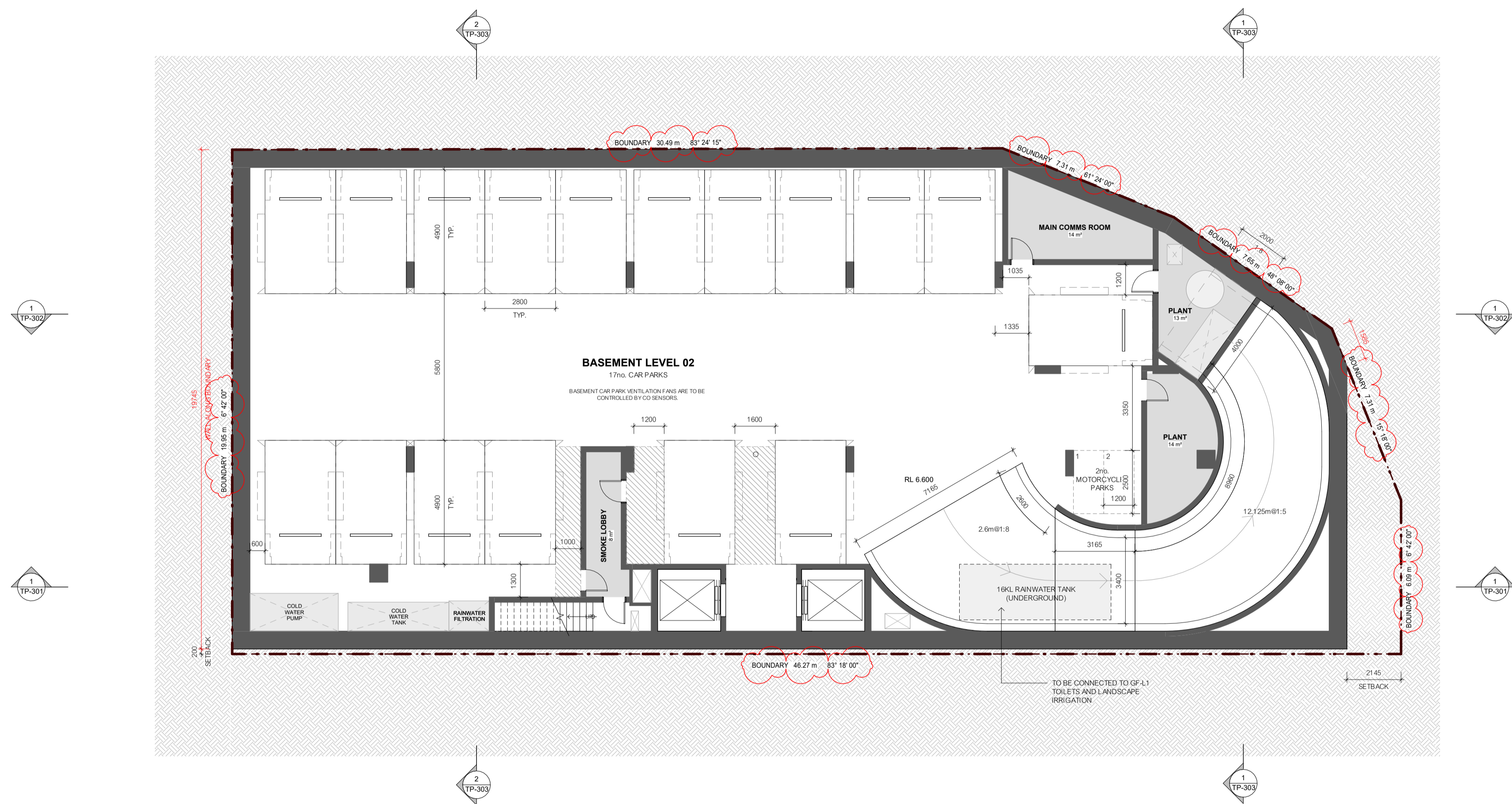
GENERAL NOTES

LEGEND

--- TITLE BOUNDARY

NOTE:

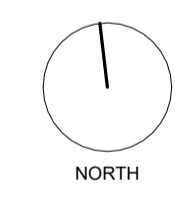
1. FFL, RL AND NGL LEVELS SHOWN ARE IN METER TO AHD.
2. DOUBLE GLAZING OR BETTER TO BE APPLIED TO ALL WINDOWS AND GLAZED DOORS.



A 22.04.2025 ISSUE FOR TOWN PLANNING RFI #1
12.03.2025 ISSUE FOR TOWN PLANNING

Rev	Date	Chkd	Reason for Issue

Based on Drawings Received:



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Project 25002 418 Burnley Street
Office Building

Title GA PLAN - BASEMENT LEVEL 2

Date 22.04.2025 Project No 25002

Scale @ A1 1:100 Dwg No TP-101

Drawn By VZ Chkd CM Rev A

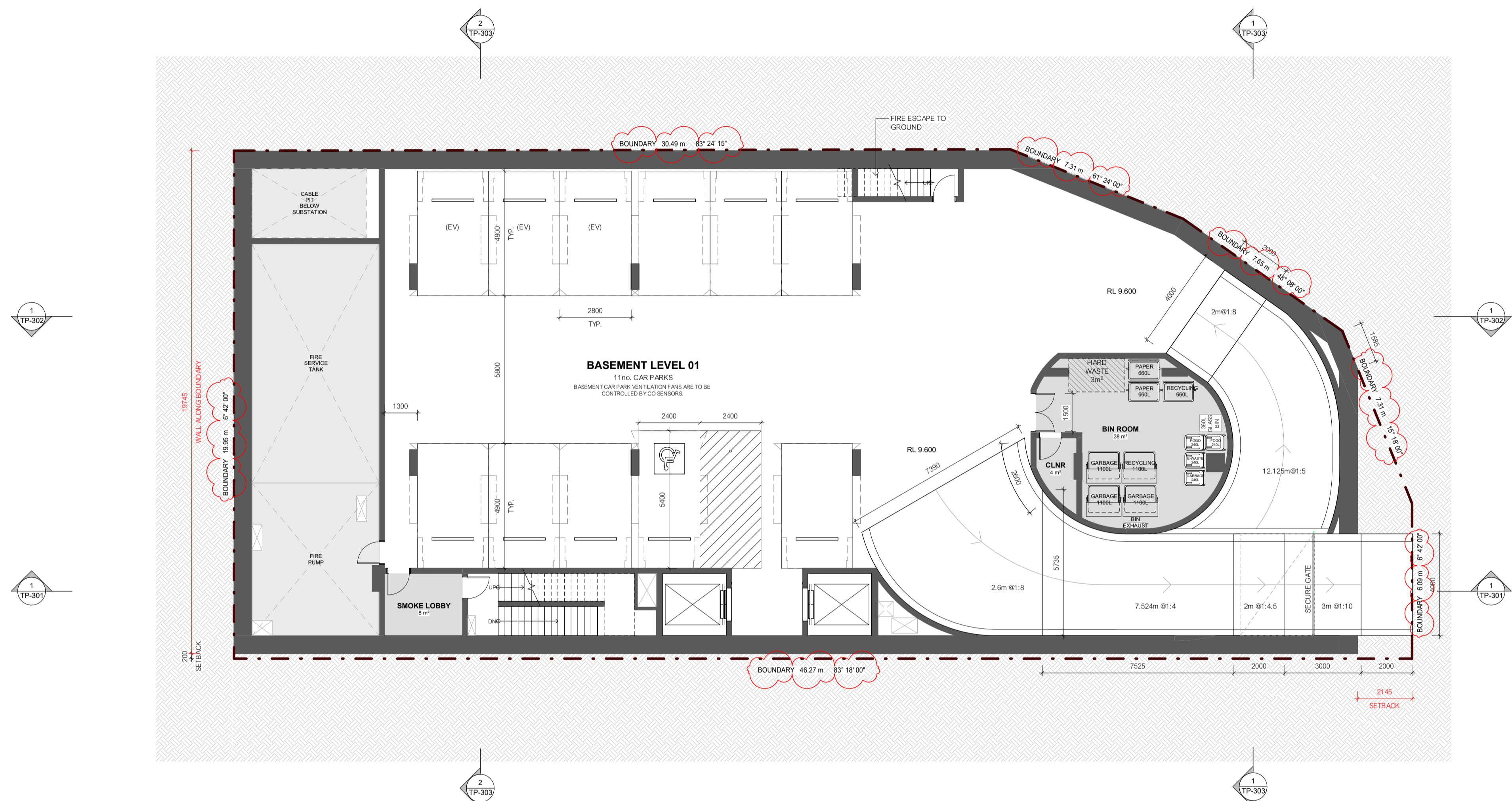
GENERAL NOTES

LEGEND

--- TITLE BOUNDARY

NOTE:

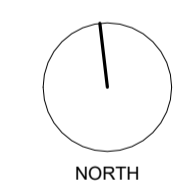
1. FFL, RL AND NGL LEVELS SHOWN ARE IN METER TO AHD.
2. DOUBLE GLAZING OR BETTER TO BE APPLIED TO ALL WINDOWS AND GLAZED DOORS.



A 22.04.2025 ISSUE FOR TOWN PLANNING RFI #1
12.03.2025 ISSUE FOR TOWN PLANNING

Rev Date Chkd Reason for Issue

Based on Drawings Received:



TOWN PLANNING

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Project 25002 418 Burnley Street

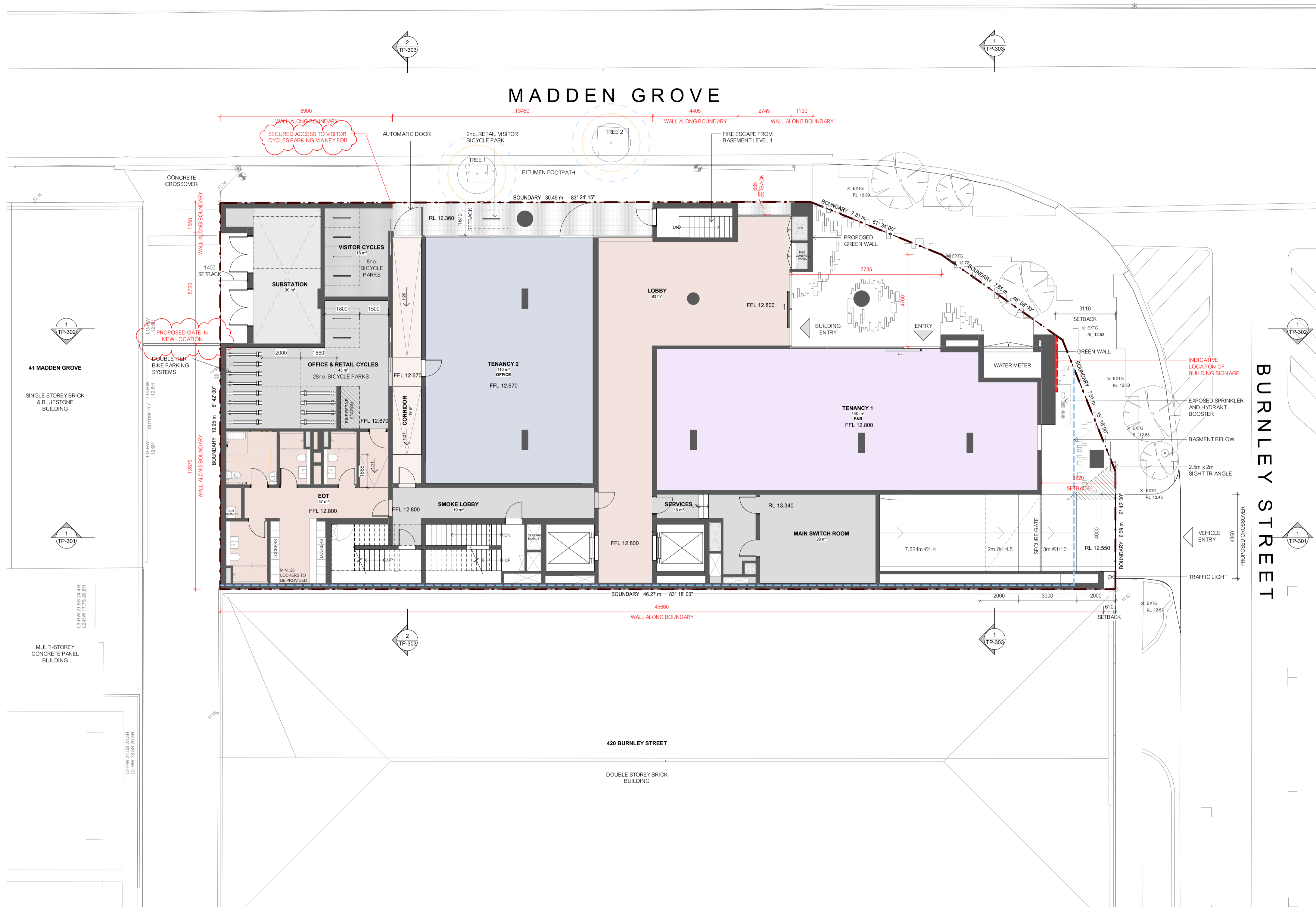
Office Building

Title GA PLAN - BASEMENT LEVEL 1

Date 22.04.2025 Project No 25002

Scale @ A1 1:100 Dwg No TP-102

Drawn By VZ Chkd CM Rev A



GENERAL NOTES

LEGEND

--- TITLE BOUNDARY

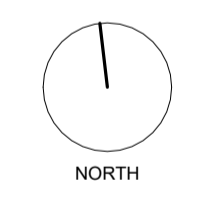
NOTE:

1. FFL, RL AND NGL LEVELS SHOWN ARE IN METERS TO AHD.
2. DOUBLE GLAZING OR BETTER TO BE APPLIED TO ALL WINDOWS AND GLAZED DOORS.

A 22.04.2025 ISSUE FOR TOWN PLANNING RF#1
12.03.2025 ISSUE FOR TOWN PLANNING

Rev	Date	Chkd	Reason for Issue

Based on Drawings Received:



TOWN PLANNING

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Project 25002 418 Burnley Street

Office Building

Title GA PLAN - GROUND

Date 22.04.2025 Project No 25002

Scale @ A1 1:100 Dwg No TP-103

Drawn By VZ Chkd CM Rev A



GENERAL NOTES

LEGEND

--- TITLE BOUNDARY

NOTE:

- FFL, RL AND NGL LEVELS SHOWN ARE IN METERS TO AHD.
- DOUBLE GLAZING OR BETTER TO BE APPLIED TO ALL WINDOWS AND GLAZED DOORS.

A	22.04.2025	ISSUE FOR TOWN PLANNING RFI #1
	12.03.2025	ISSUE FOR TOWN PLANNING
Rev	Date	Chkd Reason for Issue

Based on Drawings Received:

NORTH

TOWN PLANNING

Level 4
1-3 Newton Street
Cremorne VIC
3121 Australia

STUDIO MCCUE

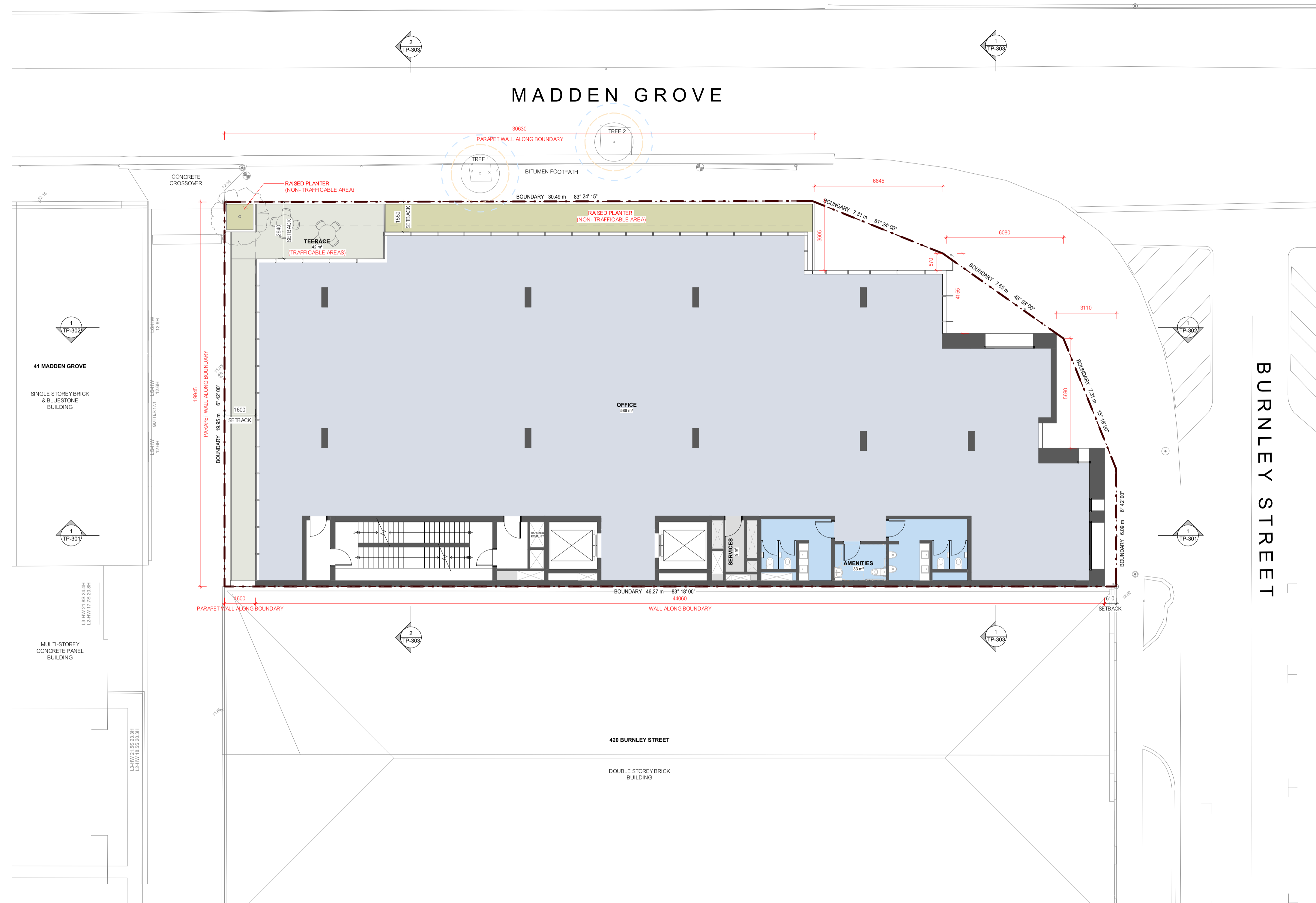
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Project	25002 418 Burnley Street
	Office Building
Title	GAPLAN - LEVEL 1
Date	22.04.2025
Project No	25002
Scale @ A1	1:100
Dwg No	TP-104
Drawn By	VZ Chkd CM Rev A

GENERAL NOTES

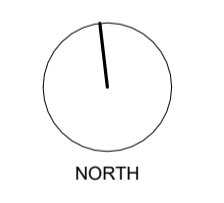
LEGEND
--- TITLE BOUNDARY

NOTE:
1. FFL, RL AND NGL LEVELS SHOWN ARE IN METERS TO AHD.
2. DOUBLE GLAZING OR BETTER TO BE APPLIED TO ALL WINDOWS AND GLAZED DOORS.



Rev	Date	Chkd	Reason for Issue
A	22.04.2025		ISSUE FOR TOWN PLANNING RFI #1
	12.03.2025		ISSUE FOR TOWN PLANNING

Based on Drawings Received:



TOWN PLANNING

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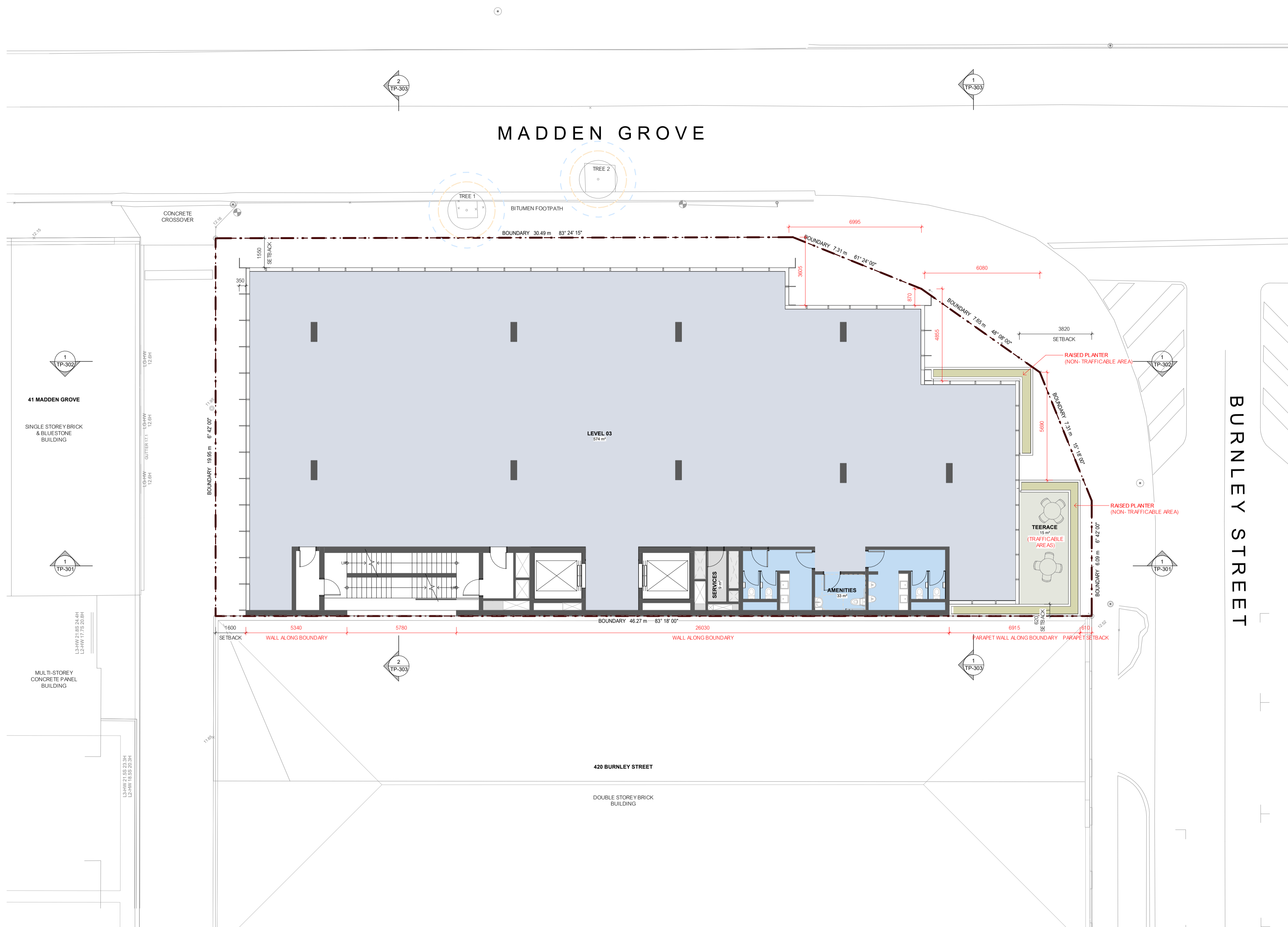
Project 25002 418 Burnley Street
Office Building

Title GA PLAN - LEVEL 2

Date 22.04.2025 Project No 25002

Scale @ A1 1:100 Dwg No TP-105

Drawn By VZ Chkd CM Rev A



GENERAL NOTES

LEGEND

--- TITLE BOUNDARY

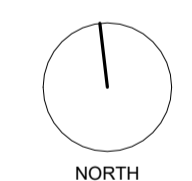
NOTE:

1. FFL, RL AND NGL LEVELS SHOWN ARE IN METERS TO AHD.
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A 22.04.2025 ISSUE FOR TOWN PLANNING RFI #1
12.03.2025 ISSUE FOR TOWN PLANNING

Rev	Date	Chkd	Reason for Issue

Based on Drawings Received:



TOWN PLANNING

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Project 25002 418 Burnley Street
Office Building

Title GA PLAN - LEVEL 3

Date 22.04.2025 Project No 25002

Scale @ A1 1:100 Dwg No TP-106

Drawn By VZ Chkd CM Rev A



GENERAL NOTES

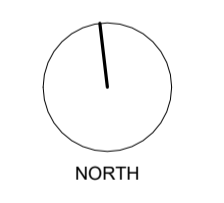
LEGEND
 - - - - - TITLE BOUNDARY

NOTE:
 1. FFL, RL AND NGL LEVELS SHOWN ARE IN METER TO AHD.
 2. DOUBLE GLAZING OR BETTER TO BE APPLIED TO ALL WINDOWS AND GLAZED DOORS.

A 22.04.2025 ISSUE FOR TOWN PLANNING RFI #1
 12.03.2025 ISSUE FOR TOWN PLANNING

Rev	Date	Chkd	Reason for Issue

Based on Drawings Received:



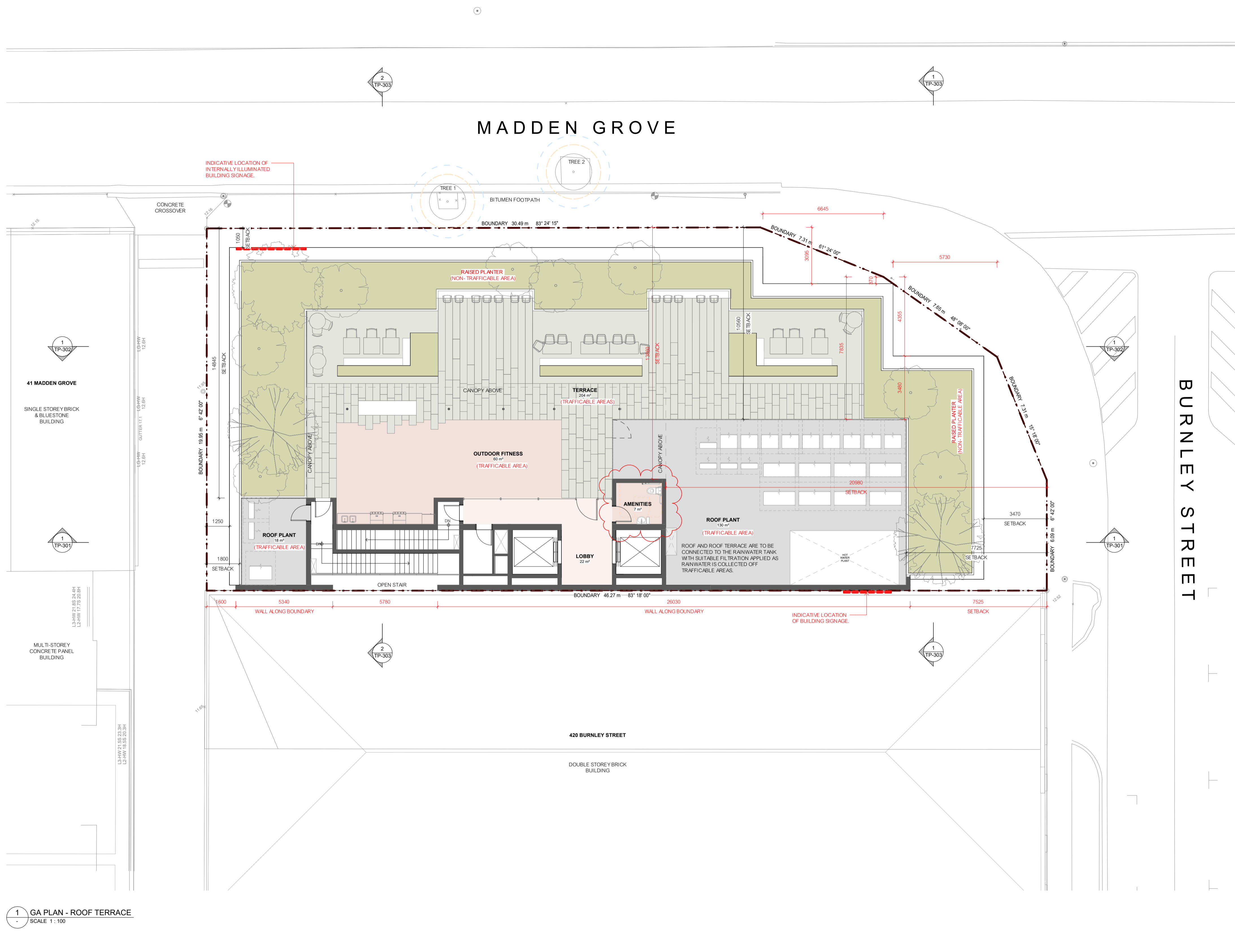
TOWN PLANNING

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 3121 Australia

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Project	25002 418 Burnley Street		
	Office Building		
Title	GA PLAN - LEVEL 4.8		
Date	22.04.2025	Project No	25002
Scale @ A1	1:100	Dwg No	TP-107
Drawn By	VZ	Chkd	CM
		Rev	A



GENERAL NOTES

LEGEND

--- TITLE BOUNDARY

NOTE:

- FFL, RL AND NGL LEVELS SHOWN ARE IN METERS TO AHD.
- DOUBLE GLAZING OR BETTER TO BE APPLIED TO ALL WINDOWS AND GLAZED DOORS.

A	22.04.2025	ISSUE FOR TOWN PLANNING RFI #1
	12.03.2025	ISSUE FOR TOWN PLANNING
Rev	Date	Chkd Reason for Issue

Based on Drawings Received:

NORTH

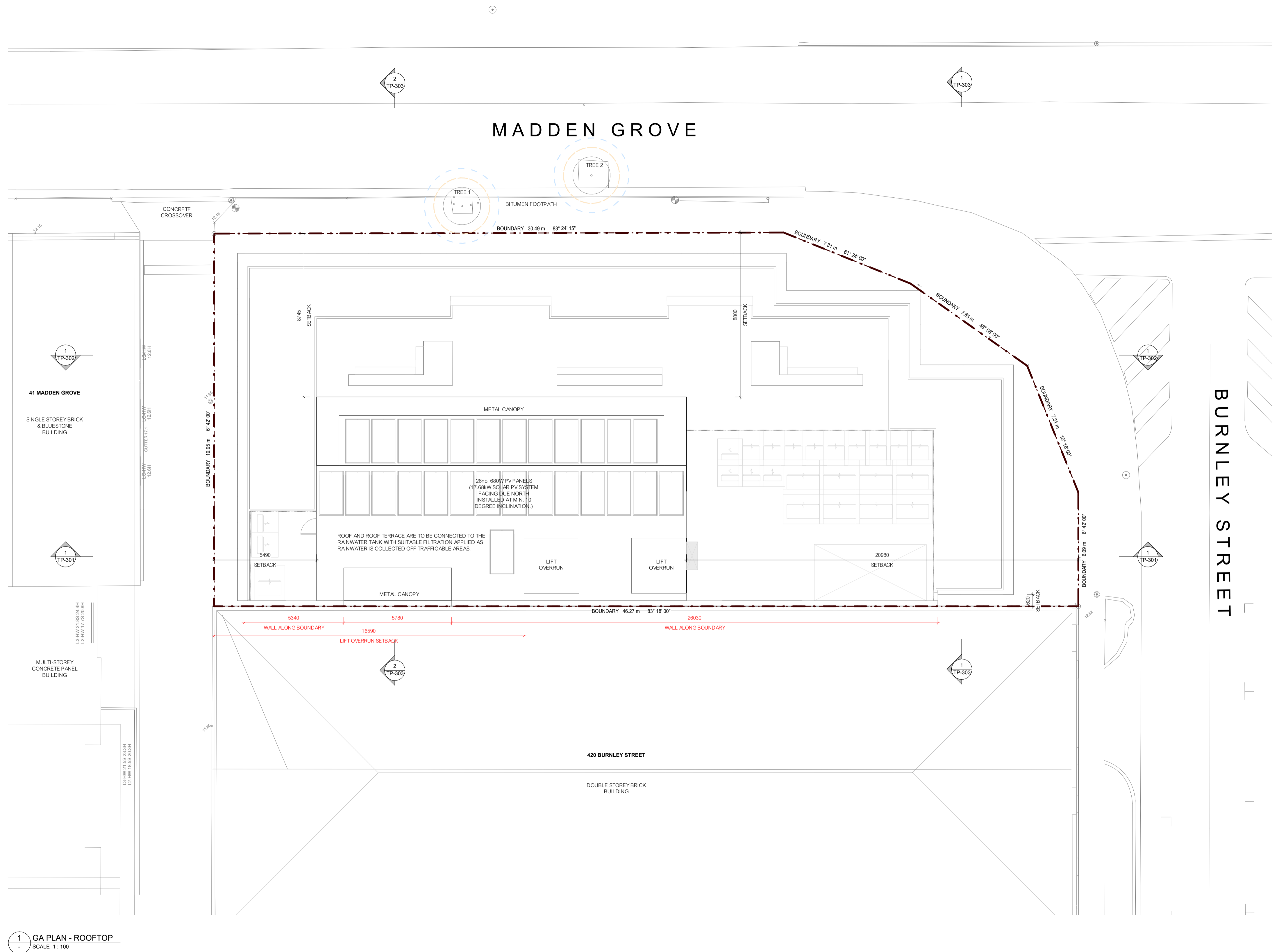
TOWN PLANNING

Level 4
1-3 Newton Street
Cremorne VIC
3121 Australia

STUDIO MCCUE

+61 0458 627 344
cm@studiomccue.com
studiomccue.com

Project	25002 418 Burnley Street
	Office Building
Title	GA PLAN - ROOF TERRACE
Date	22.04.2025
Project No	25002
Scale @ A1	1:100
Dwg No	TP-108
Drawn By	VZ Chkd CM Rev A



1 GA PLAN - ROOFTOP
SCALE 1:100

GENERAL NOTES

LEGEND

--- TITLE BOUNDARY

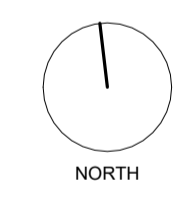
NOTE:

1. FFL, RL AND NGL LEVELS SHOWN ARE IN METERS TO AHD.
2. DOUBLE GLAZING OR BETTER TO BE APPLIED TO ALL WINDOWS AND GLAZED DOORS.

A 22.04.2025 ISSUE FOR TOWN PLANNING RFI #1
12.03.2025 ISSUE FOR TOWN PLANNING

Rev	Date	Chkd	Reason for Issue

Based on Drawings Received:



TOWN PLANNING

Level 4
1-3 Newton Street
Cremorne VIC
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studiomccue.com



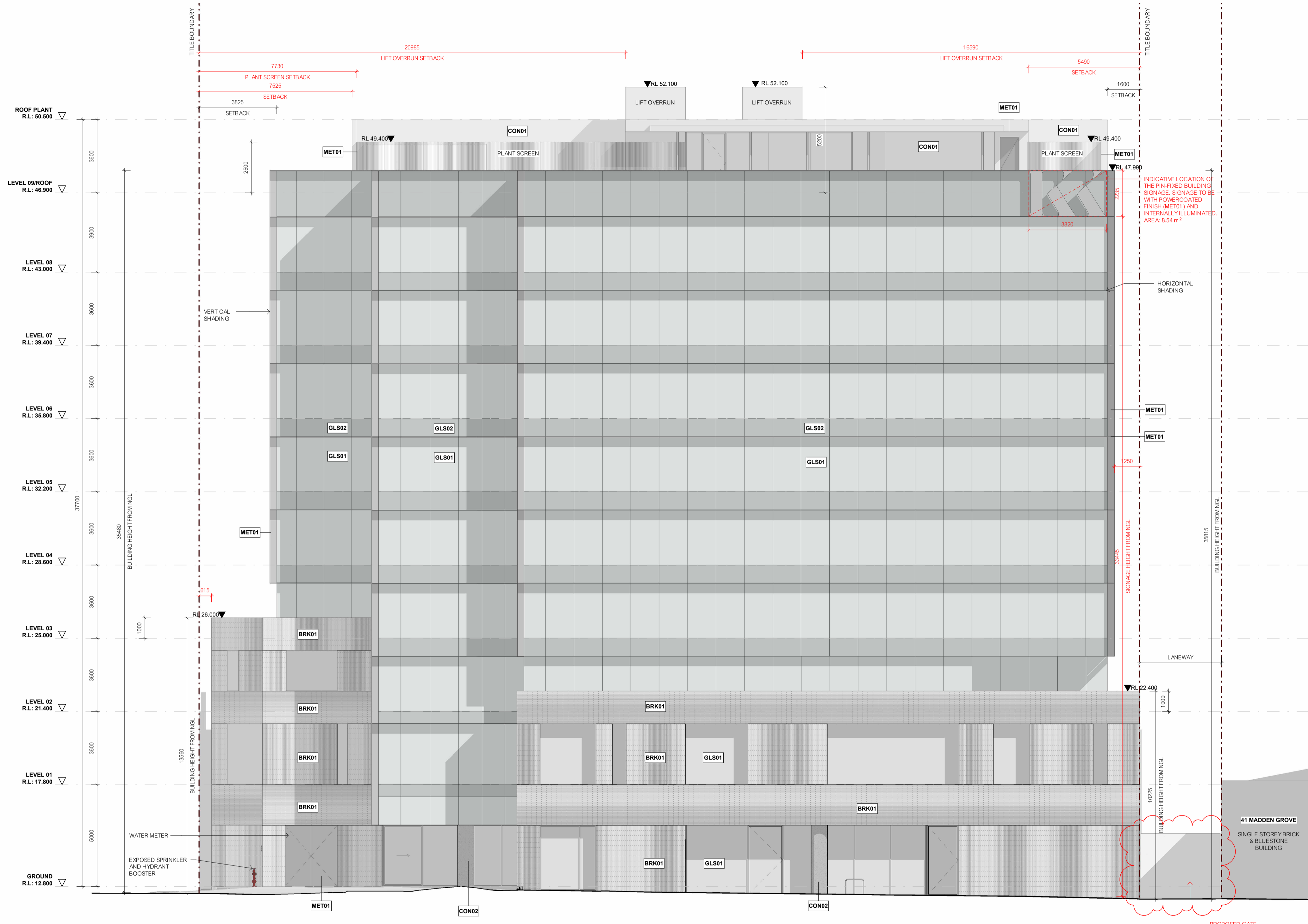
Project 25002 418 Burnley Street
Office Building

Title GA PLAN - ROOFTOP

Date 22.04.2025 Project No 25002

Scale @ A1 1:100 Dwg No TP-109

Drawn By VZ Chkd CM Rev A



1 BUILDING ELEVATION - NORTH
SCALE 1:100

GENERAL NOTES

MATERIAL LEGEND

- BRK01 LONG BRICK - GREY
- CON01 CONCRETE FINISH
- CON02 EXPOSED AGGREGATE CONCRETE
- MET01 POWDERCOATED SILVER FINISH TO METAL (TO WINDOWFRAMES, EXTERNAL LOUVRES, ROOF CANOPY AND PLANT SCREEN)
- GLS01 CLEAR GLAZING - DOUBLE GLAZING OR BETTER TO BE APPLIED TO ALL WINDOWS AND GLAZED DOORS.
- GLS02 CLEAR GLAZING WITH SOLID BACK PANEL

LEGEND

--- TITLE BOUNDARY

NOTE:

1. FFL, RL AND NGL LEVELS SHOWN ARE IN METER TO AHD.
2. DOUBLE GLAZING OR BETTER TO BE APPLIED TO ALL WINDOWS AND GLAZED DOORS.

A 22.04.2025 ISSUE FOR TOWN PLANNING RFI #1
12.03.2025 ISSUE FOR TOWN PLANNING

Rev	Date	Chkd	Reason for Issue

Based on Drawings Received:

TOWN PLANNING

Level 4
1-3 Newton Street
Cremorne VIC
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Project 25002 418 Burnley Street

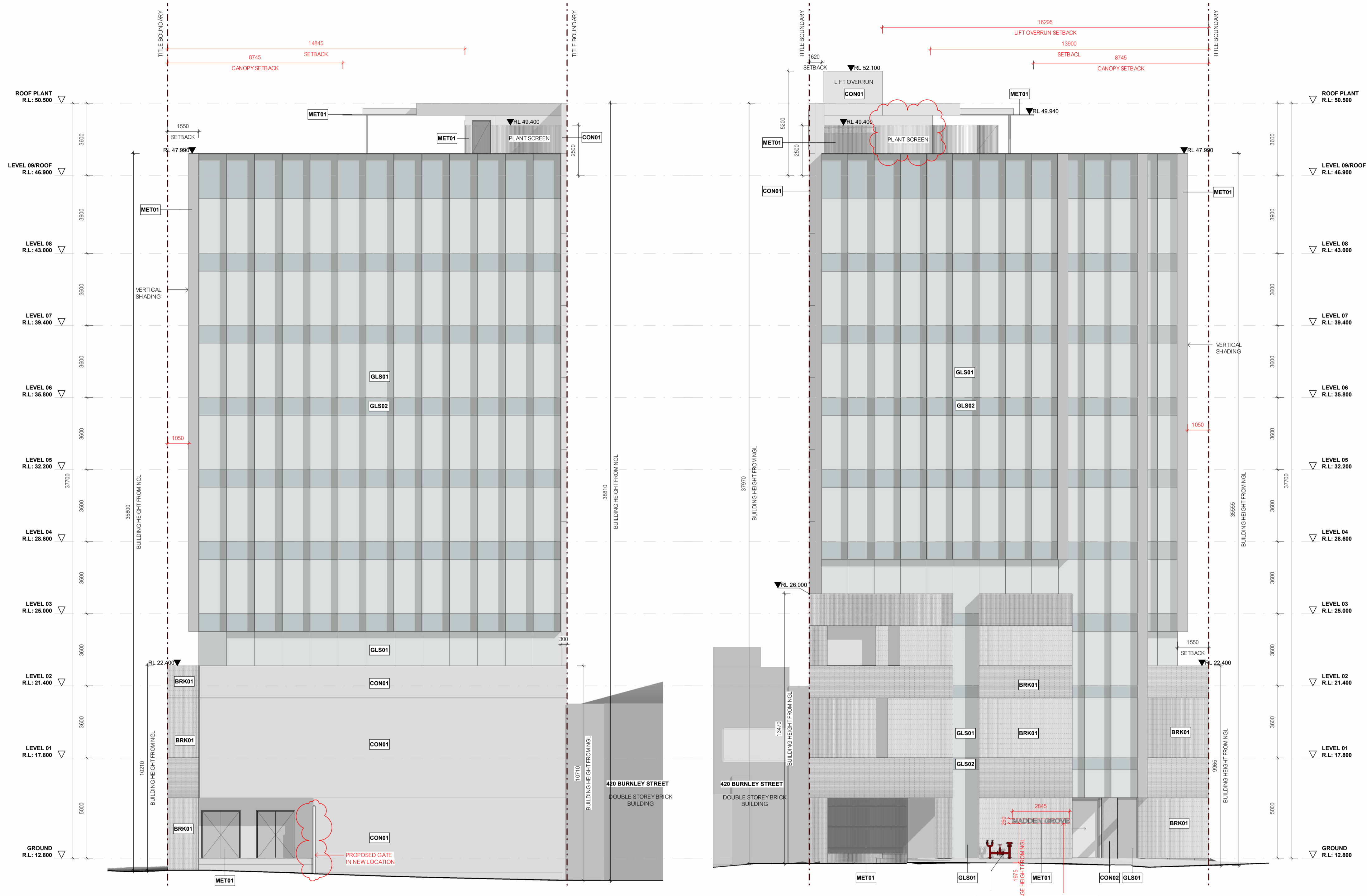
Office Building

Title NORTH ELEVATION

Date 22.04.2025 Project No 25002

Scale @ A1 1:100 Dwg No TP-201

Drawn By VZ Chkd CM Rev A



1 BUILDING ELEVATION - WEST
SCALE 1:100

2 BUILDING ELEVATION - EAST
SCALE 1:100

GENERAL NOTES

MATERIAL LEGEND

- BRK01 LONG BRICK - GREY
- CON01 CONCRETE FINISH
- CON02 EXPOSED AGGREGATE CONCRETE
- MET01 POWDERCOATED SILVER FINISH TO METAL (TO WINDOWFRAMES, EXTERNAL LOUVRES, ROOF CANOPY AND PLANT SCREEN)
- GLS01 CLEAR GLAZING - DOUBLE GLAZING OR BETTER TO BE APPLIED TO ALL WINDOWS AND GLAZED DOORS.
- GLS02 CLEAR GLAZING WITH SOLID BACK PANEL

LEGEND

--- TITLE BOUNDARY

NOTE:

1. FFL, RL AND NGL LEVELS SHOWN ARE IN METERS TO AHD.
2. DOUBLE GLAZING OR BETTER TO BE APPLIED TO ALL WINDOWS AND GLAZED DOORS.

A 22.04.2025 ISSUE FOR TOWN PLANNING RF#1
12.03.2025 ISSUE FOR TOWN PLANNING

Rev	Date	Chkd	Reason for Issue

Based on Drawings Received:

TOWN PLANNING

Level 4
1-3 Newton Street
Cremorne VIC
3121 Australia
cm@studiomccue.com
studiomccue.com



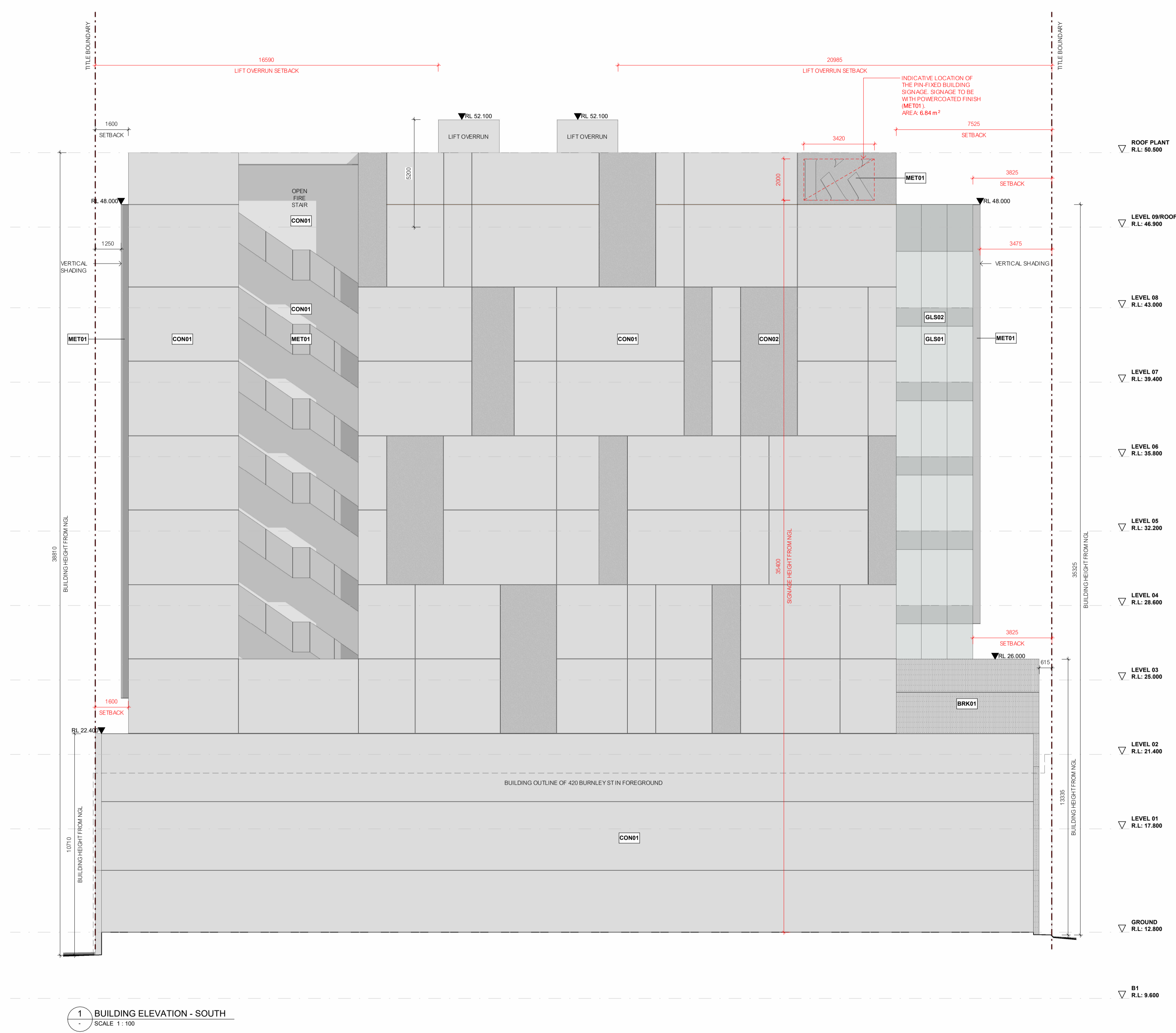
Project 25002 418 Burnley Street
Office Building

Title EAST AND WEST ELEVATIONS

Date 22.04.2025 Project No 25002

Scale @ A1 1:100 Dwg No TP-202

Drawn By VZ Chkd CM Rev A



1 BUILDING ELEVATION - SOUTH
SCALE 1:100

GENERAL NOTES

MATERIAL LEGEND

- BRK01 LONG BRICK - GREY
- CON01 CONCRETE FINISH
- CON02 EXPOSED AGGREGATE CONCRETE
- MET01 POWDERCOATED SILVER FINISH TO METAL (TO WINDOWFRAMES, EXTERNAL LOUVRES, ROOF CANOPY AND PLANT SCREEN)
- GLS01 CLEAR GLAZING - DOUBLE GLAZING OR BETTER TO BE APPLIED TO ALL WINDOWS AND GLAZED DOORS.
- GLS02 CLEAR GLAZING WITH SOLID BACK PANEL

LEGEND

- TITLE BOUNDARY

NOTE:

1. FFL, RL AND NGL LEVELS SHOWN ARE IN METER TO AHD.
2. DOUBLE GLAZING OR BETTER TO BE APPLIED TO ALL WINDOWS AND GLAZED DOORS.

A 22.04.2025 ISSUE FOR TOWN PLANNING RF1 #1
12.03.2025 ISSUE FOR TOWN PLANNING

Rev	Date	Chkd	Reason for Issue

Based on Drawings Received:

TOWN PLANNING

Level 4
1-3 Newton Street
Cremorne VIC
3121 Australia
+61 0458 627 344
cm@studiomccue.com
studiomccue.com



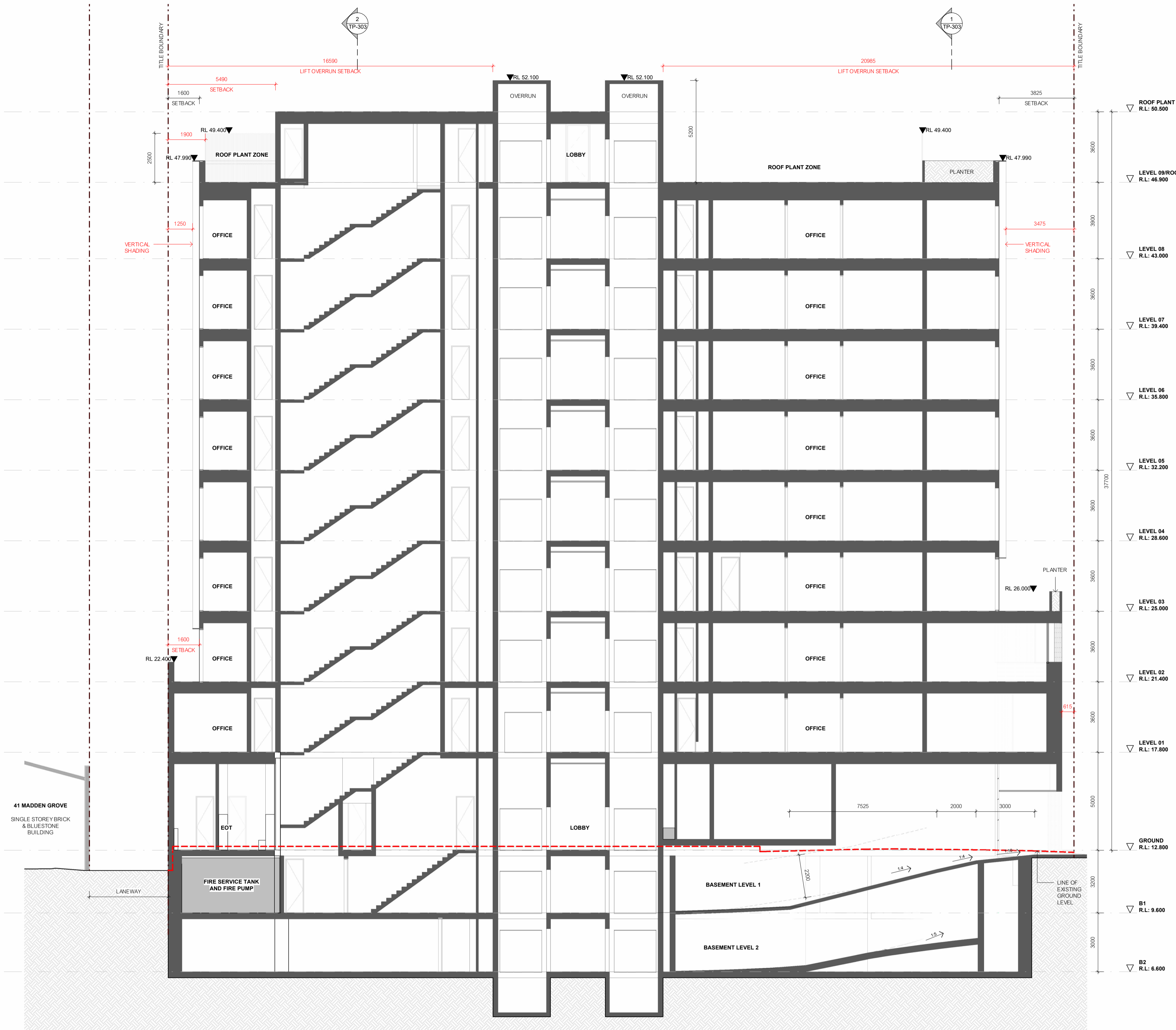
Project 25002 418 Burnley Street
Office Building

Title SOUTH ELEVATION

Date 22.04.2025 Project No 25002

Scale @ A1 1:100 Dwg No TP-203

Drawn By VZ Chkd CM Rev A



Builders / Contractors shall verify all dimensions before any work commences. Dimensions shall be given in meters. Any discrepancies are to be made known to the Architects / Designers studio prior to any works commencing on site. All shop drawings shall be submitted for review and manufacture shall not commence prior to the return of stamped shop drawings.
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GENERAL NOTES

LEGEND

--- TITLE BOUNDARY

NOTE:

- FFL, RL AND NGL LEVELS SHOWN ARE IN METERS TO AHD.
- DOUBLE GLAZING OR BETTER TO BE APPLIED TO ALL WINDOWS AND GLAZED DOORS.

Rev	Date	Chkd	Reason for Issue
A	22.04.2025		ISSUE FOR TOWN PLANNING RF#1
	12.03.2025		ISSUE FOR TOWN PLANNING

Based on Drawings Received:

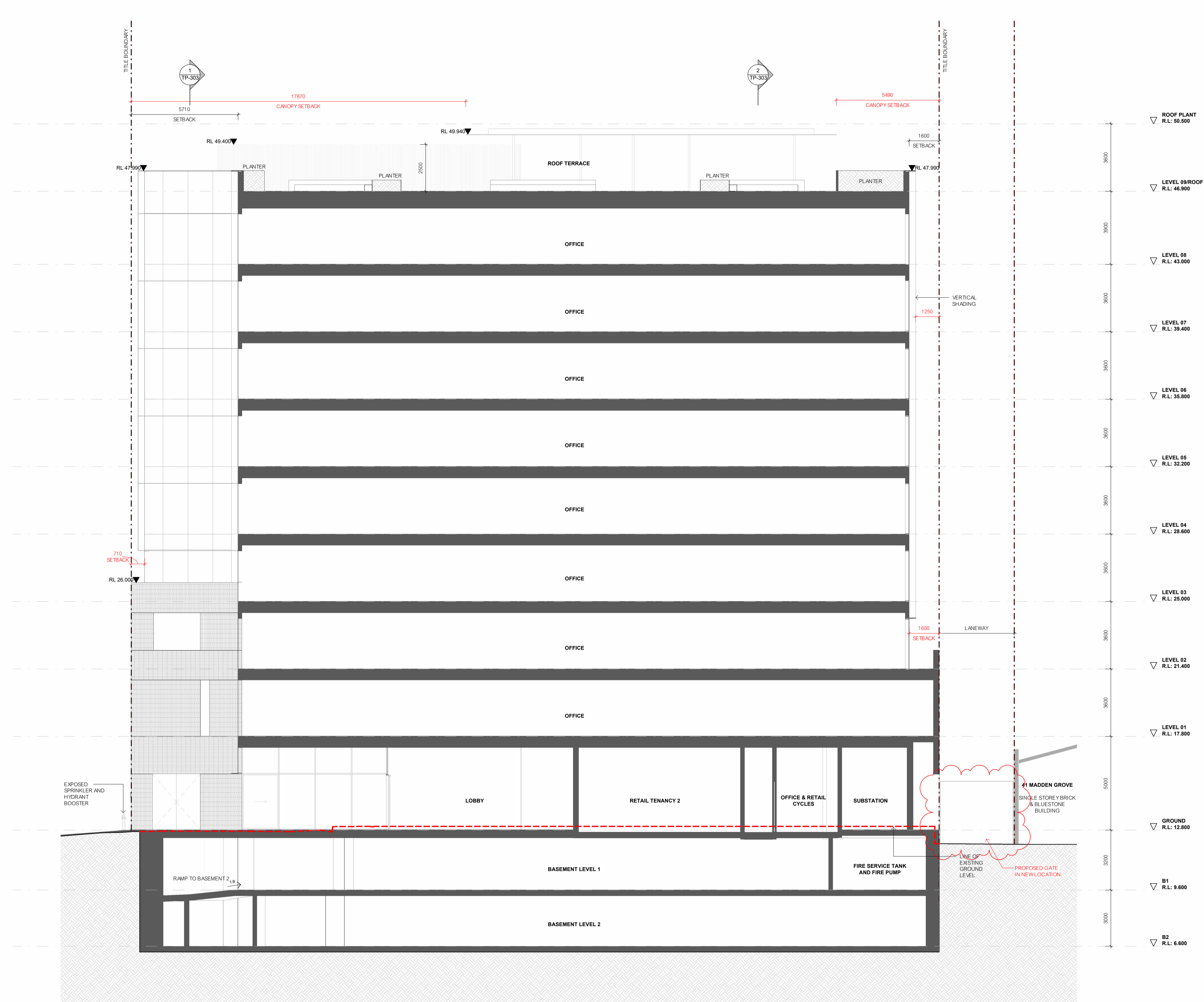
TOWN PLANNING

Level 4
1-3 Newton Street
Cremorne VIC
3121 Australia
+61 0458 627 344
cm@studiomccue.com
studiomccue.com

STUDIO MCCUE

Project 25002 418 Burnley Street
Office Building
Title BUILDING SECTION A

Date 22.04.2025 Project No 25002
Scale @ A1 1:100 Dwg No TP-301
Drawn By VZ Chkd CM Rev A



Builders / Contractors shall verify all dimensions before any work commences. Dimensions shown are in meters to AHD. Any discrepancies are to be made known to the Architects / Designers studio prior to any works commencing on site. All shop drawings shall be submitted for review and manufacture shall not commence prior to the return of stamped shop drawings.
© STUDIO MCCUE ABN 78 679 570 784

GENERAL NOTES

LEGEND
--- TITLE BOUNDARY

NOTE:
1. FFL, RL AND NGL LEVELS SHOWN ARE IN METER TO AHD.
2. DOUBLE GLAZING OR BETTER TO BE APPLIED TO ALL WINDOWS AND GLAZED DOORS.

Rev	Date	Chkd	Reason for Issue
A	22.04.2025		ISSUE FOR TOWN PLANNING RF1 #1
	12.03.2025		ISSUE FOR TOWN PLANNING

Based on Drawings Received:

TOWN PLANNING

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3121 Australia
+61 0458 627 344
cm@studiomccue.com
studiomccue.com

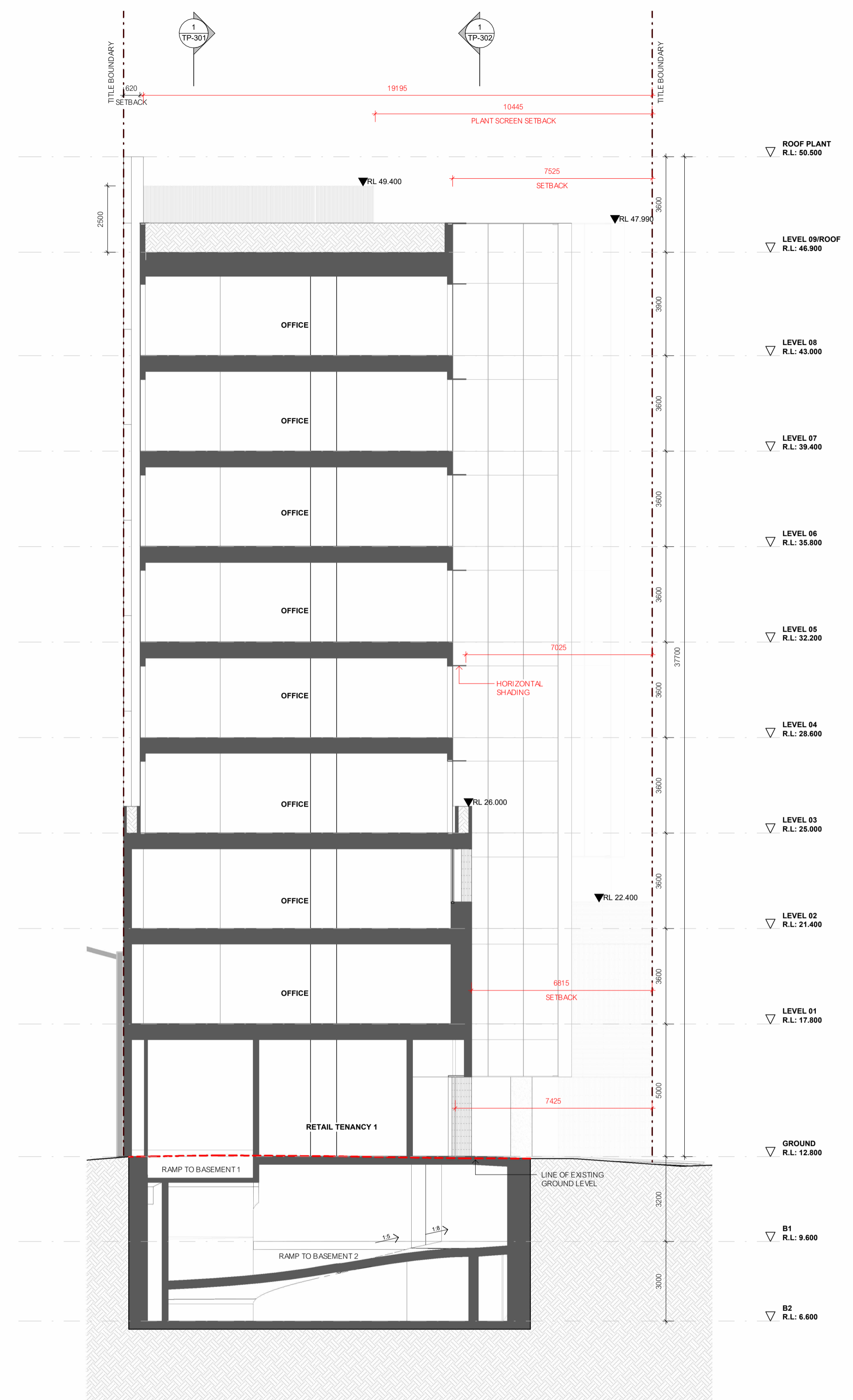
Project 25002 418 Burnley Street
Office Building

Title BUILDING SECTION B

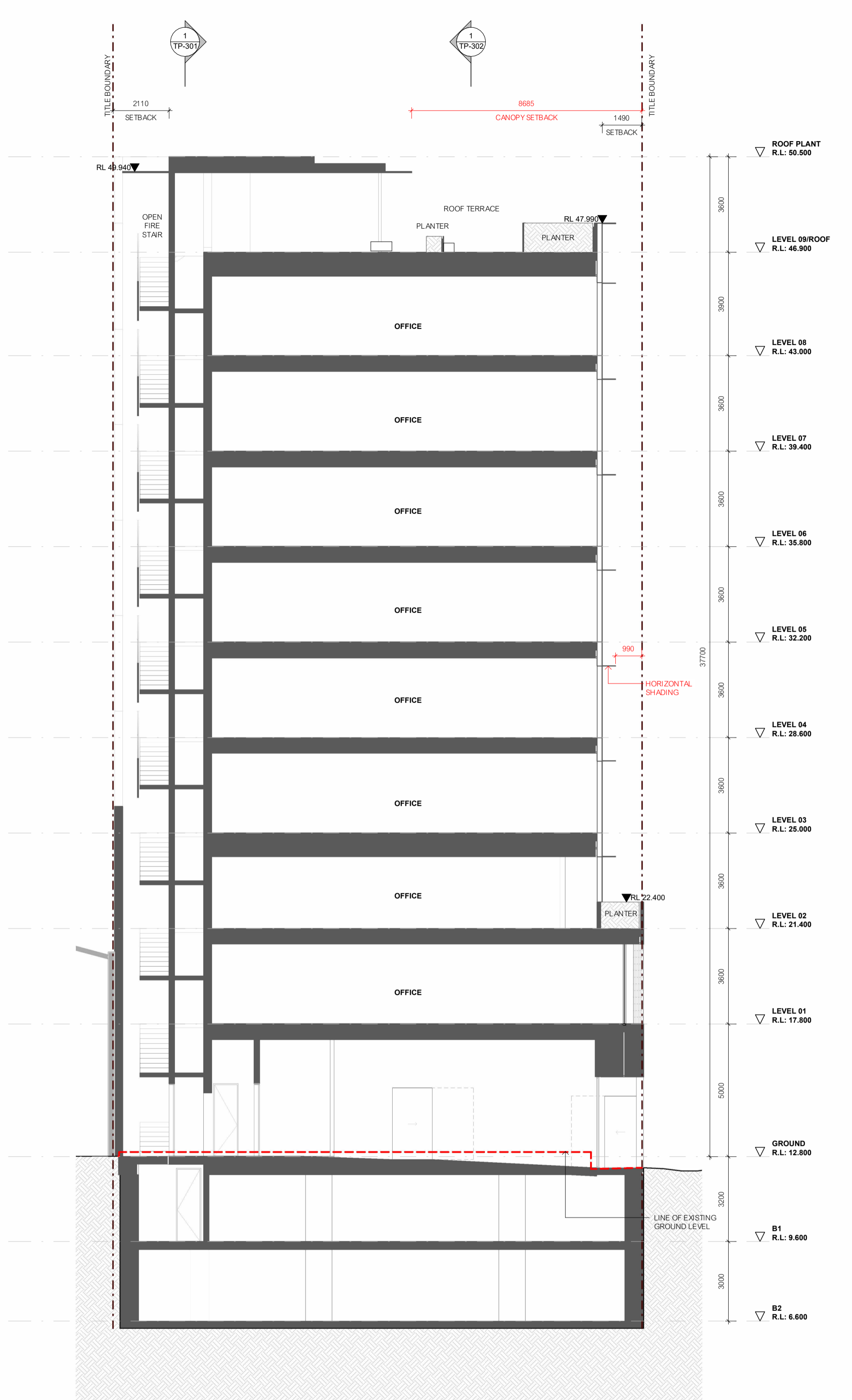
Date 22.04.2025 Project No 25002

Scale @ A1 1:100 Dwg No TP-302

Drawn By VZ Chkd CM Rev A



1 TP SECTION C
TP-101 SCALE 1:100



2 TP SECTION D
TP-101 SCALE 1:100

Builders / Contractors shall verify all dimensions before any work commences. Dimensions shown on drawings are to be used for construction purposes. Any discrepancies are to be made known to the Architects / Designers prior to any works commencing on site. All shop drawings shall be submitted for review and manufacture shall not commence prior to the return of stamped shop drawings.
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GENERAL NOTES

- LEGEND
- TITLE BOUNDARY
- NOTE:
- FFL, RL AND NGL LEVELS SHOWN ARE IN METERS TO AHD.
 - DOUBLE GLAZING OR BETTER TO BE APPLIED TO ALL WINDOWS AND GLAZED DOORS.

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	12.03.2025		ISSUE FOR TOWN PLANNING

Based on Drawings Received:

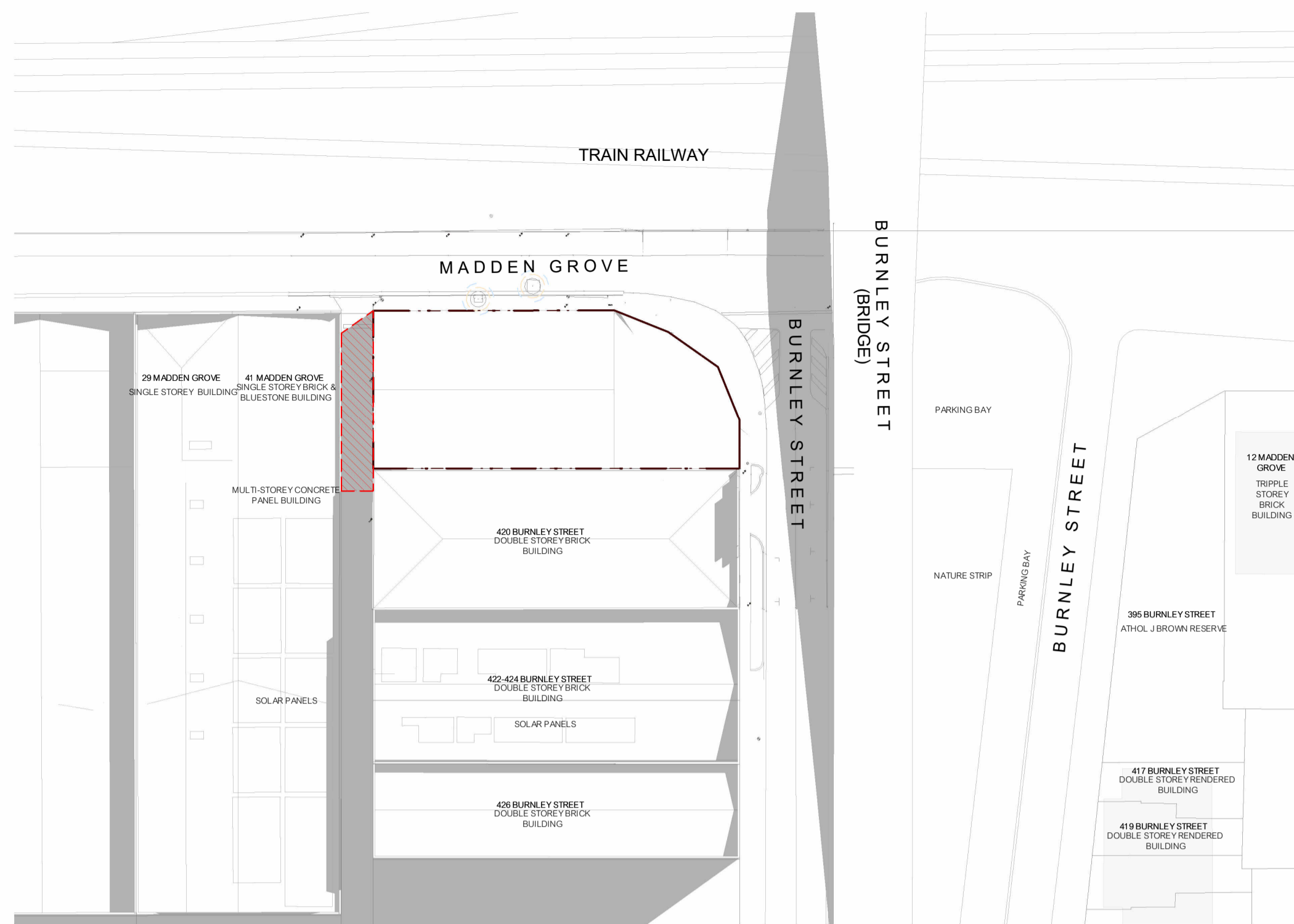
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3121 Australia

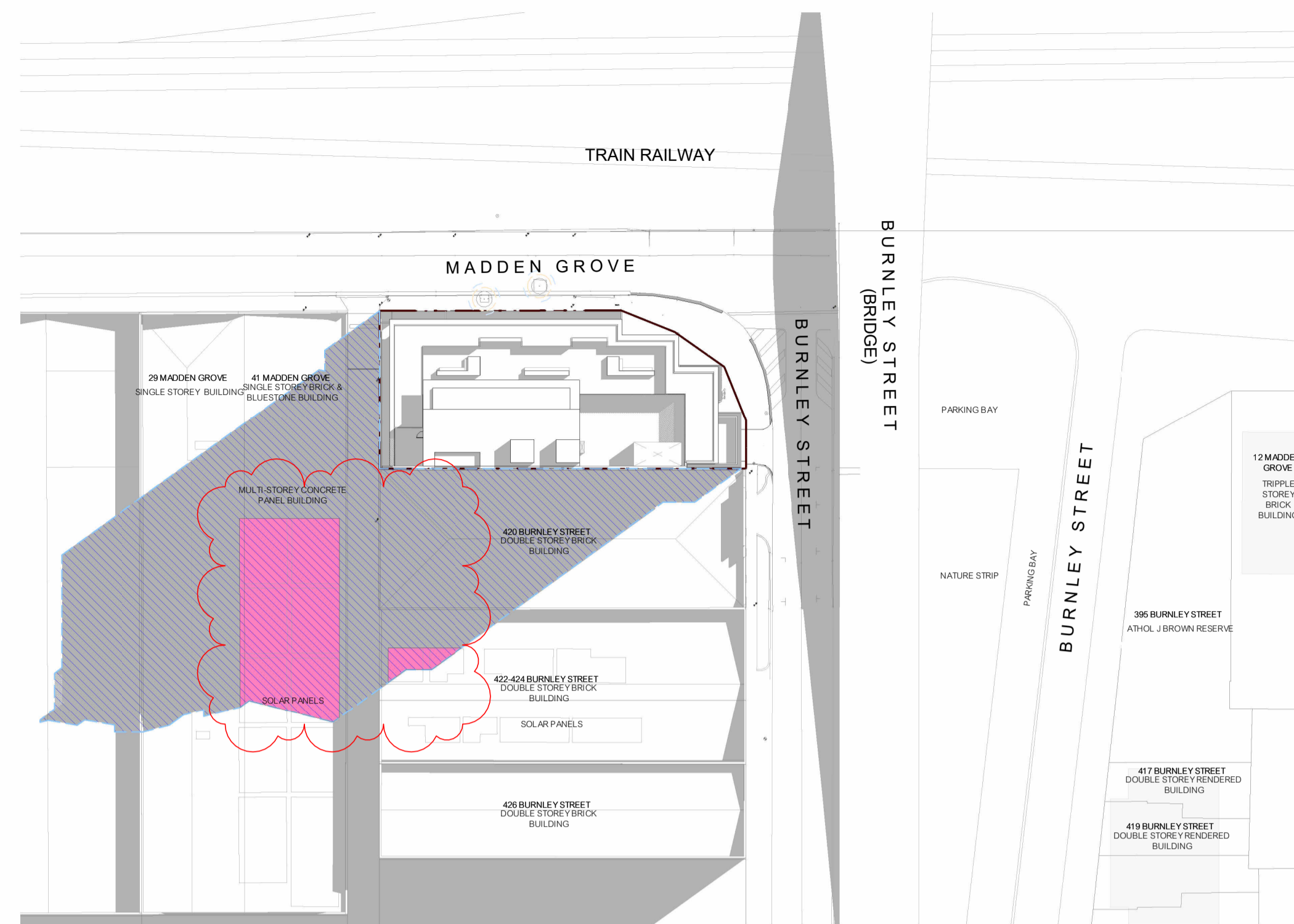
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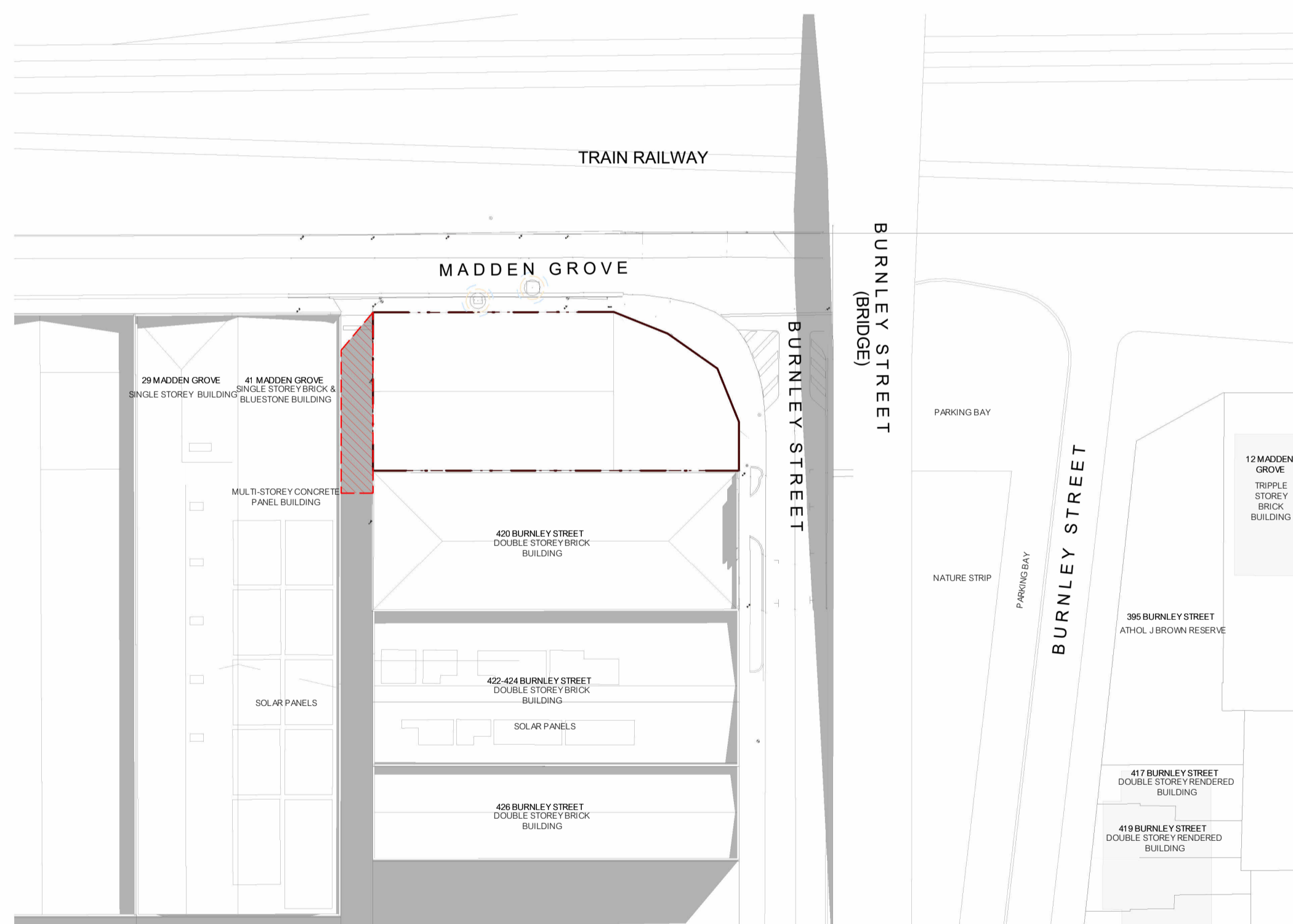
Project	25002 418 Burnley Street
	Office Building
Title	BUILDING SECTION C&D
Date	22.04.2025
Project No	25002
Scale @ A1	1:100
Dwg No	TP-303
Drawn By	VZ
Chkd	CM
Rev	A



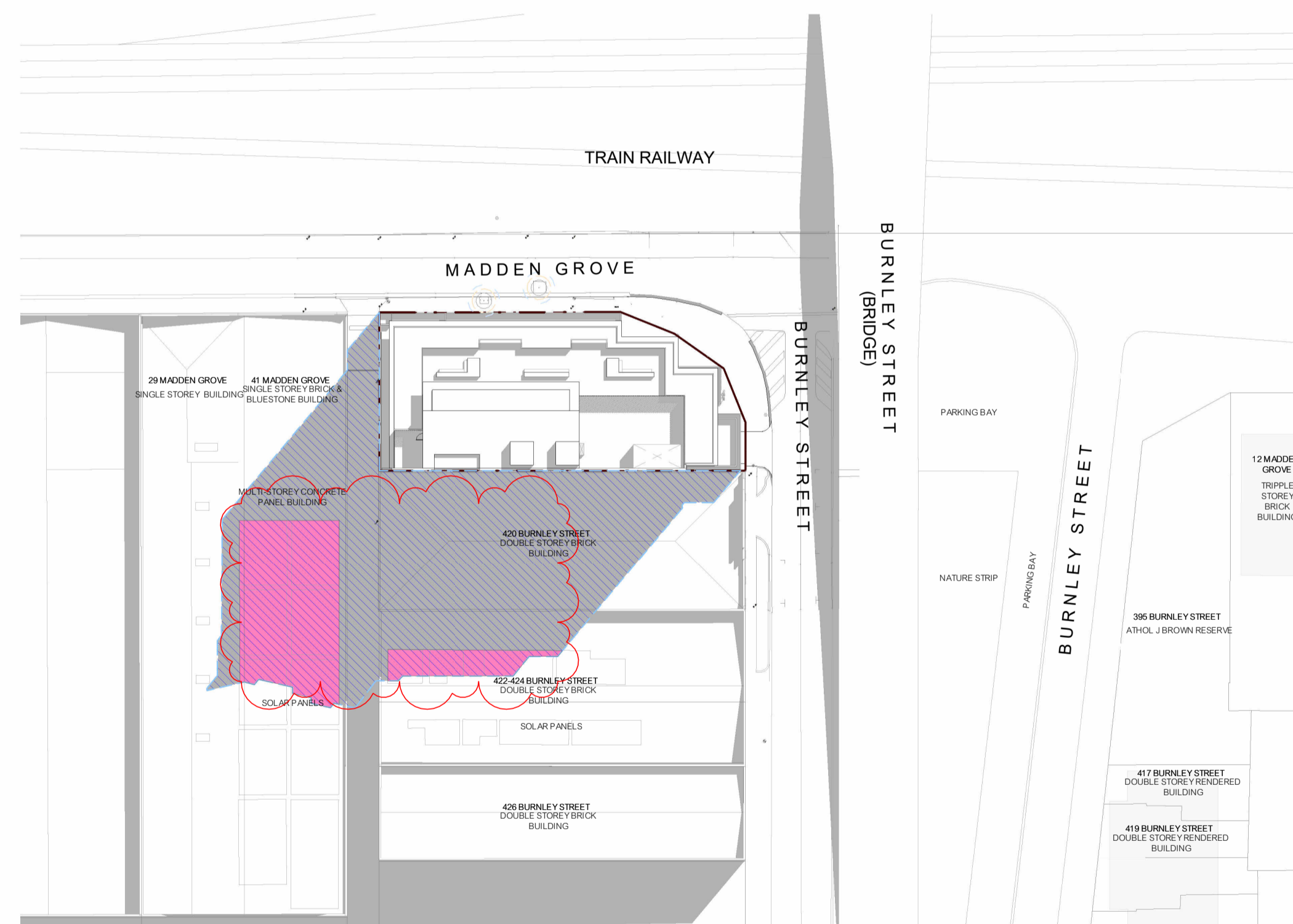
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SCALE 1 : 500



SHADOW DIAGRAM_9AM 22nd SEPTEMBER_PROPOSED
SCALE 1 : 500



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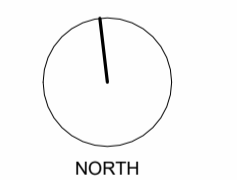
GENERAL NOTES

- LEGEND
- TITLE BOUNDARY
 - EXISTING SHADOW
 - PROPOSED SHADOW
 - PROPOSED SHADOW TO SURROUNDING SOLAR PANELS

A 22/04/2025 ISSUE FOR TOWN PLANNING RF#1
12/03/2025 ISSUE FOR TOWN PLANNING

Rev	Date	Chkd	Reason for Issue

Based on Drawings Received:



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Project 25002 418 Burnley Street

Office Building

Title SHADOW DIAGRAM - EQUINOX

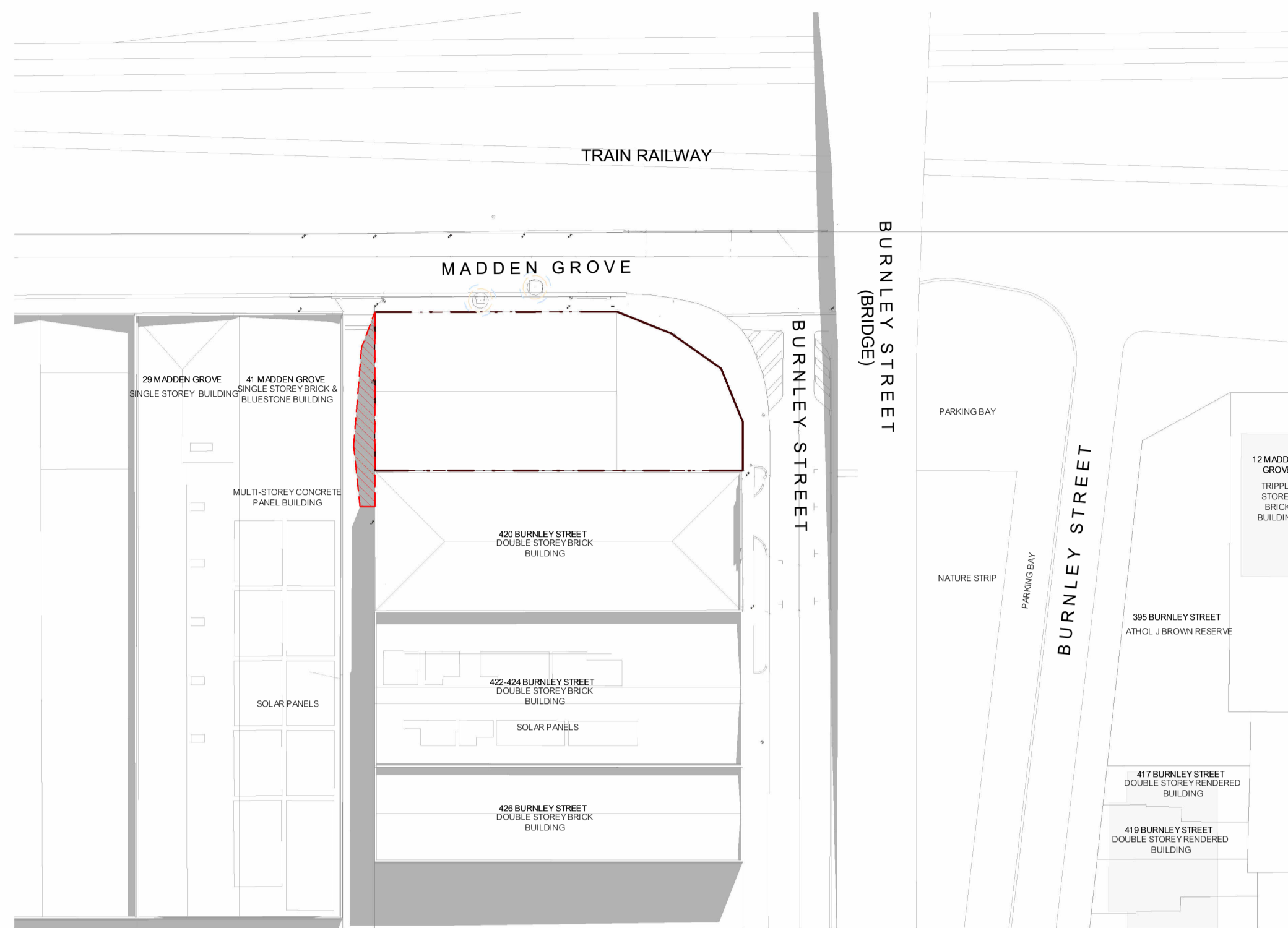
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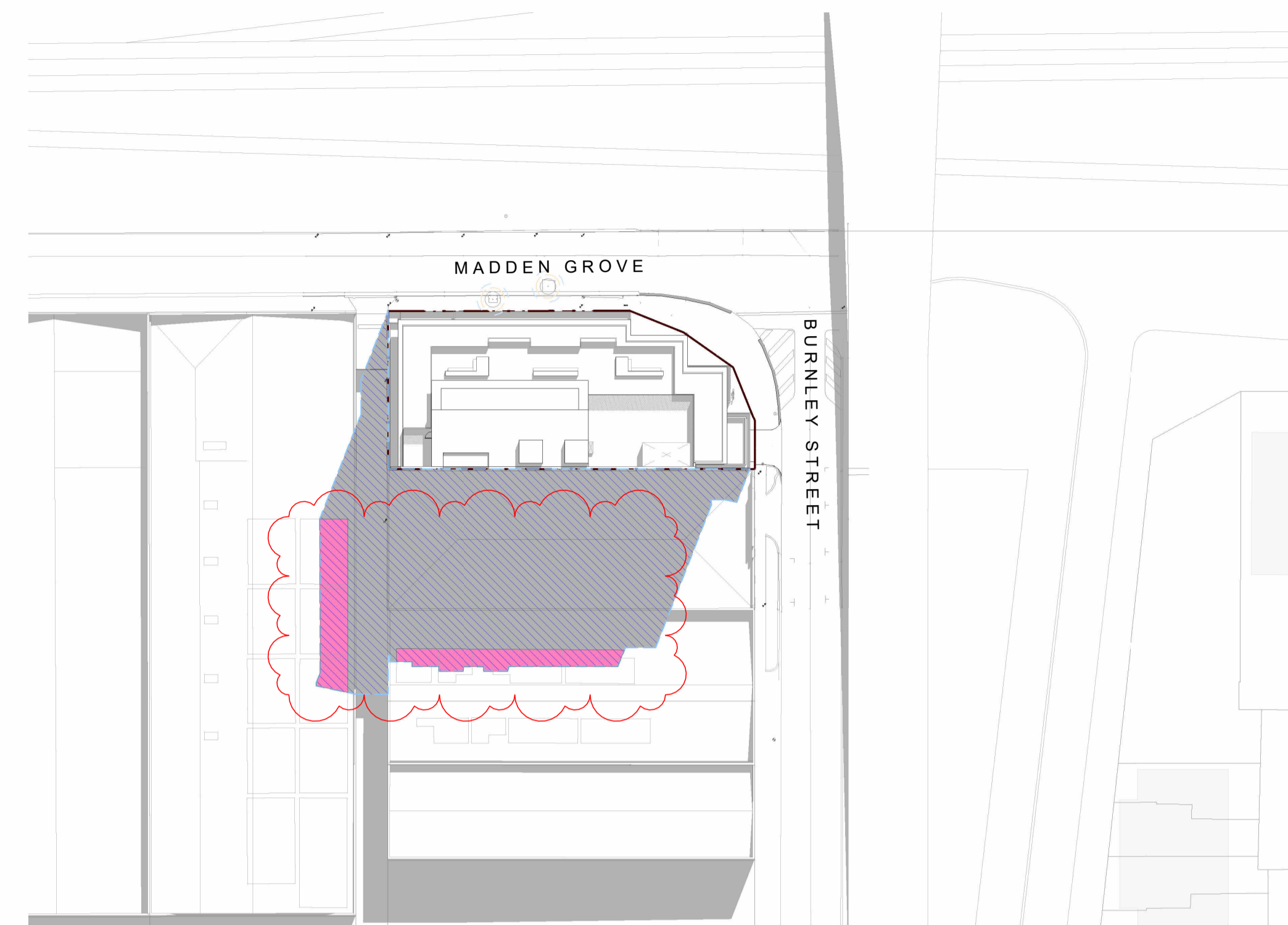
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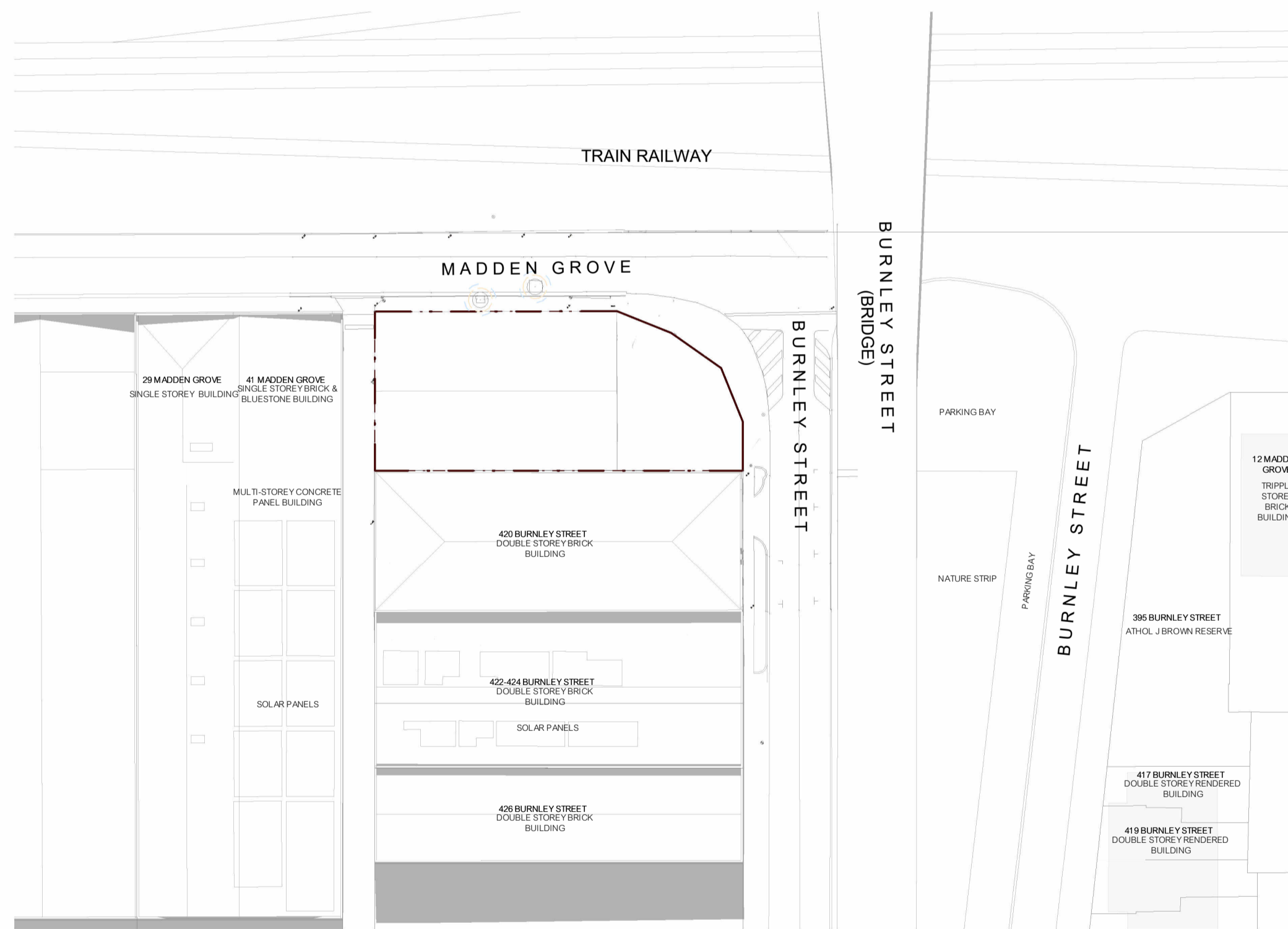
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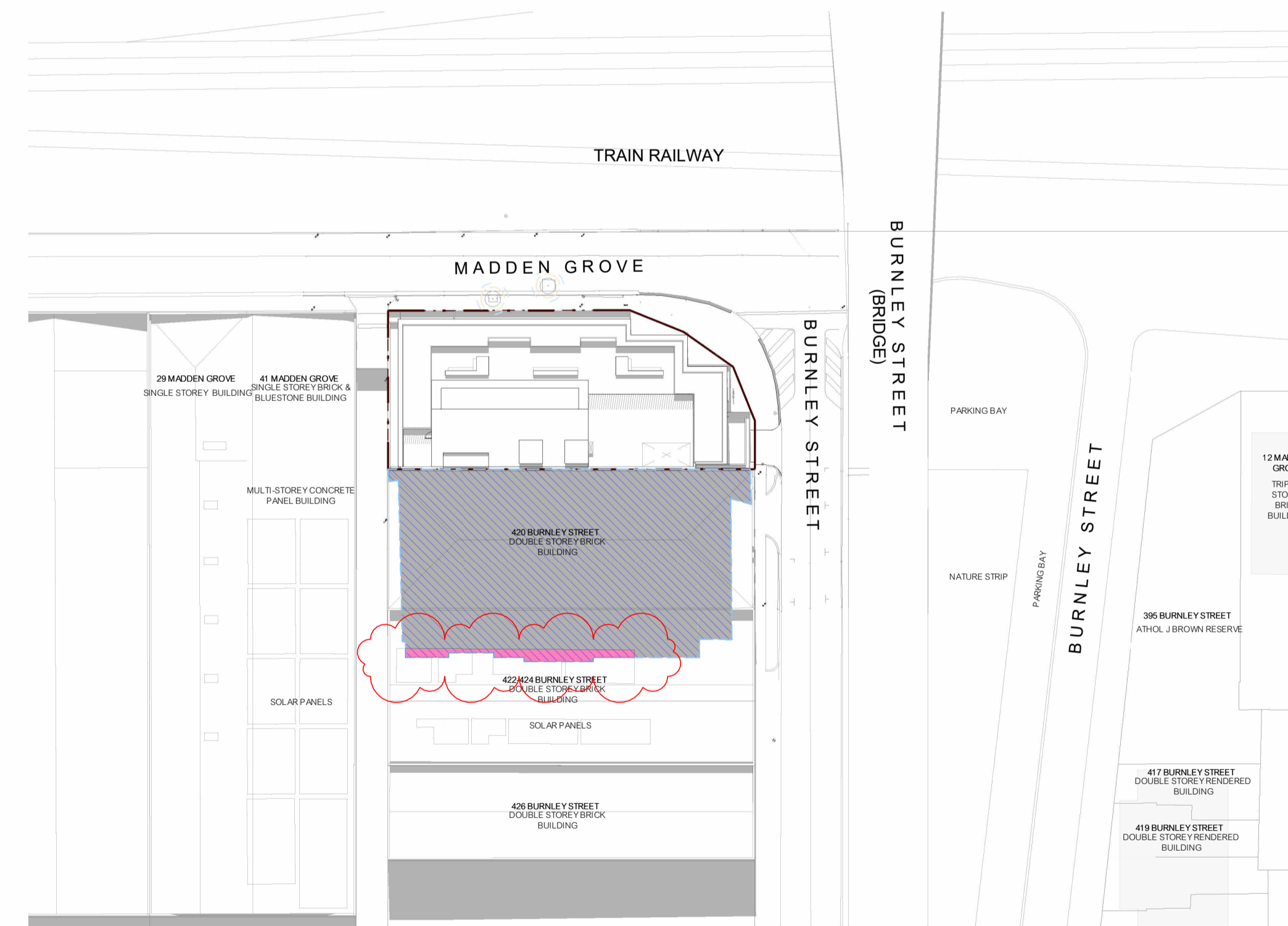
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SCALE 1 : 500



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SCALE 1 : 500



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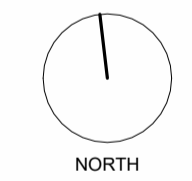
GENERAL NOTES

- LEGEND
- TITLE BOUNDARY
 - EXISTING SHADOW
 - PROPOSED SHADOW
 - PROPOSED SHADOW TO SURROUNDING SOLAR PANELS

A 22/04/2025 ISSUE FOR TOWN PLANNING RFI #1
12/03/2025 ISSUE FOR TOWN PLANNING

Rev	Date	Chkd	Reason for Issue

Based on Drawings Received:



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Project 25002 418 Burnley Street

Office Building

Title SHADOW DIAGRAM - EQUINOX

Date 22.04.2025

Project No 25002

Scale @ A1 1 : 500

Dwg No TP-902

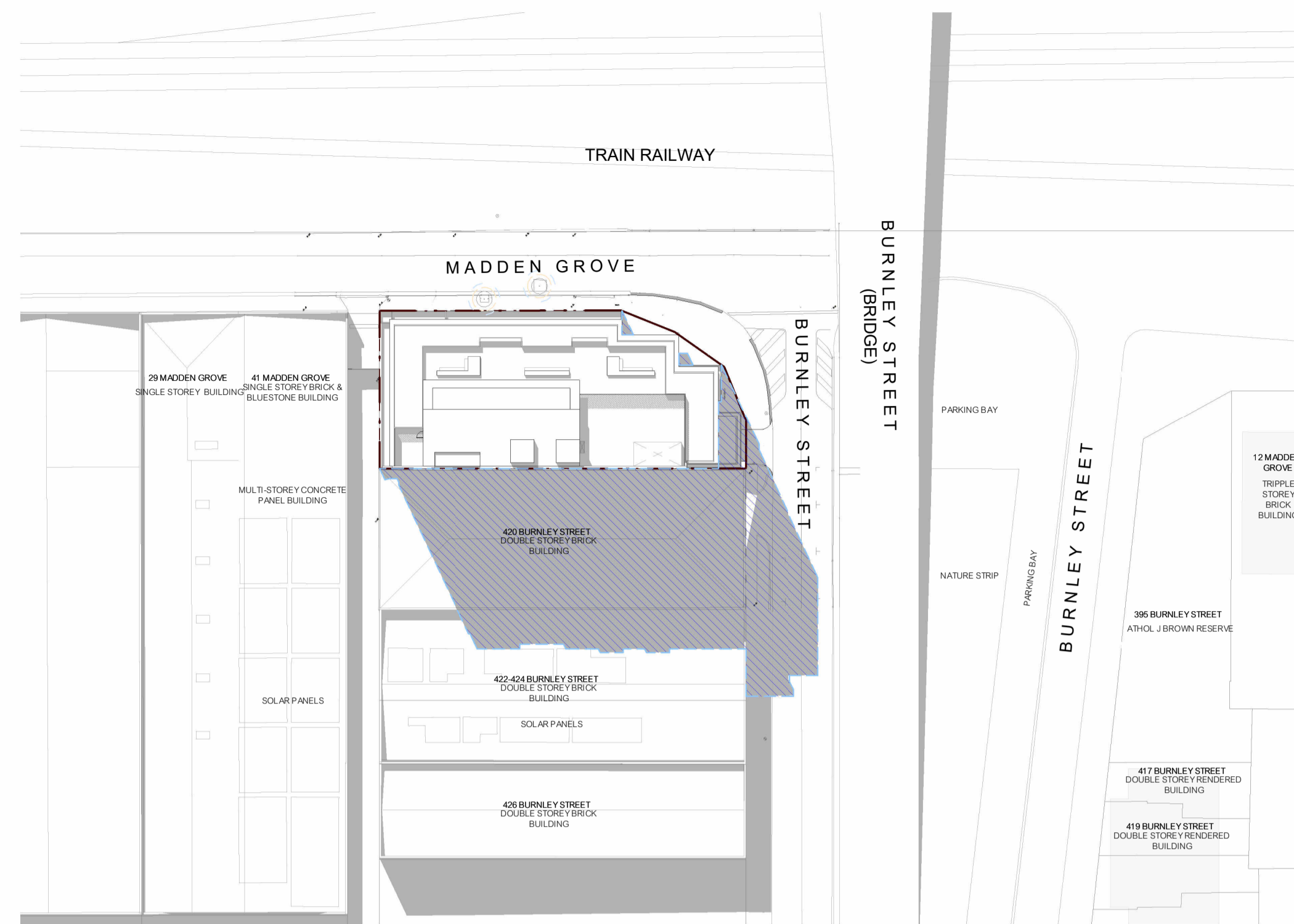
Drawn By VZ

Chkd CM

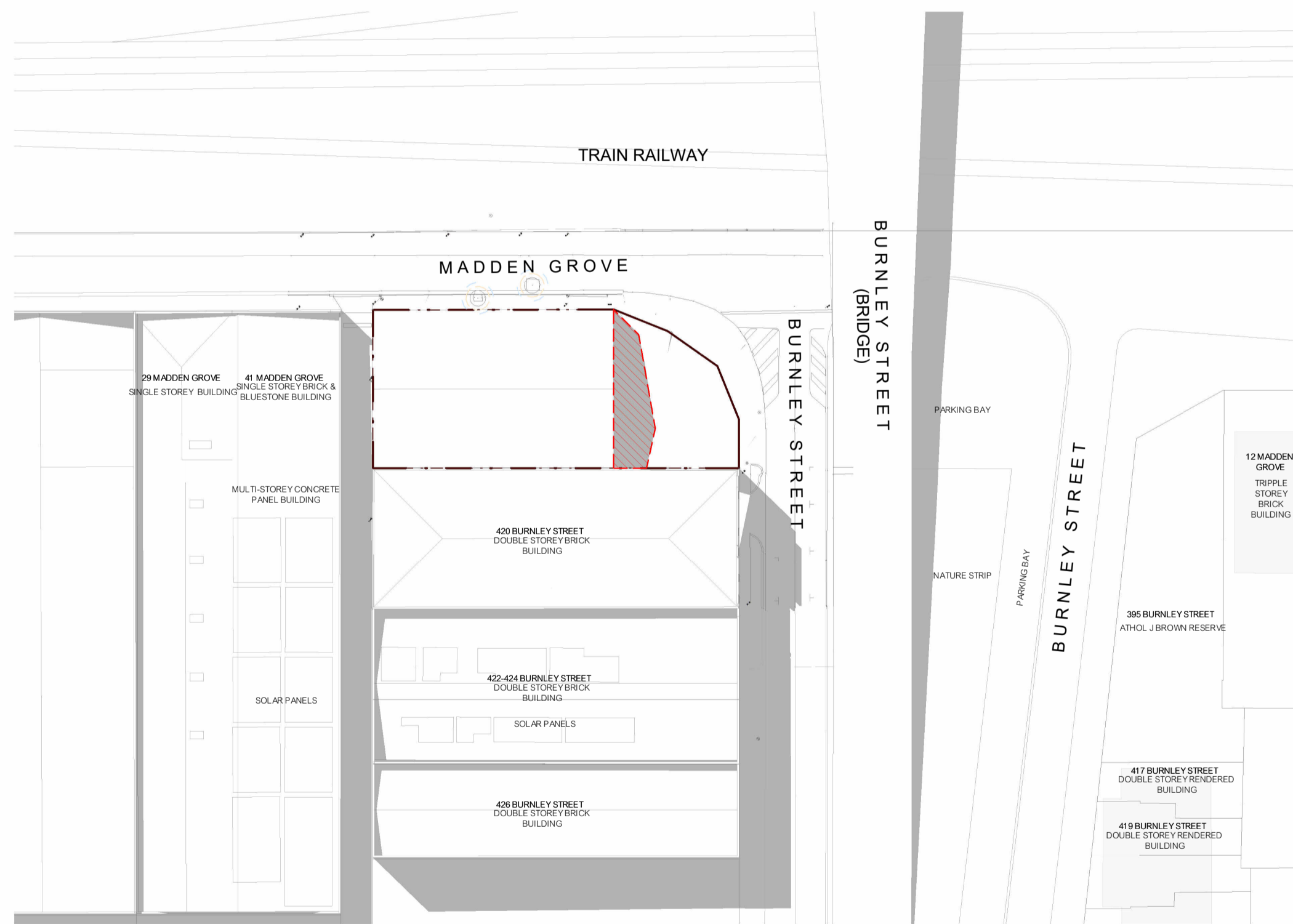
Rev A



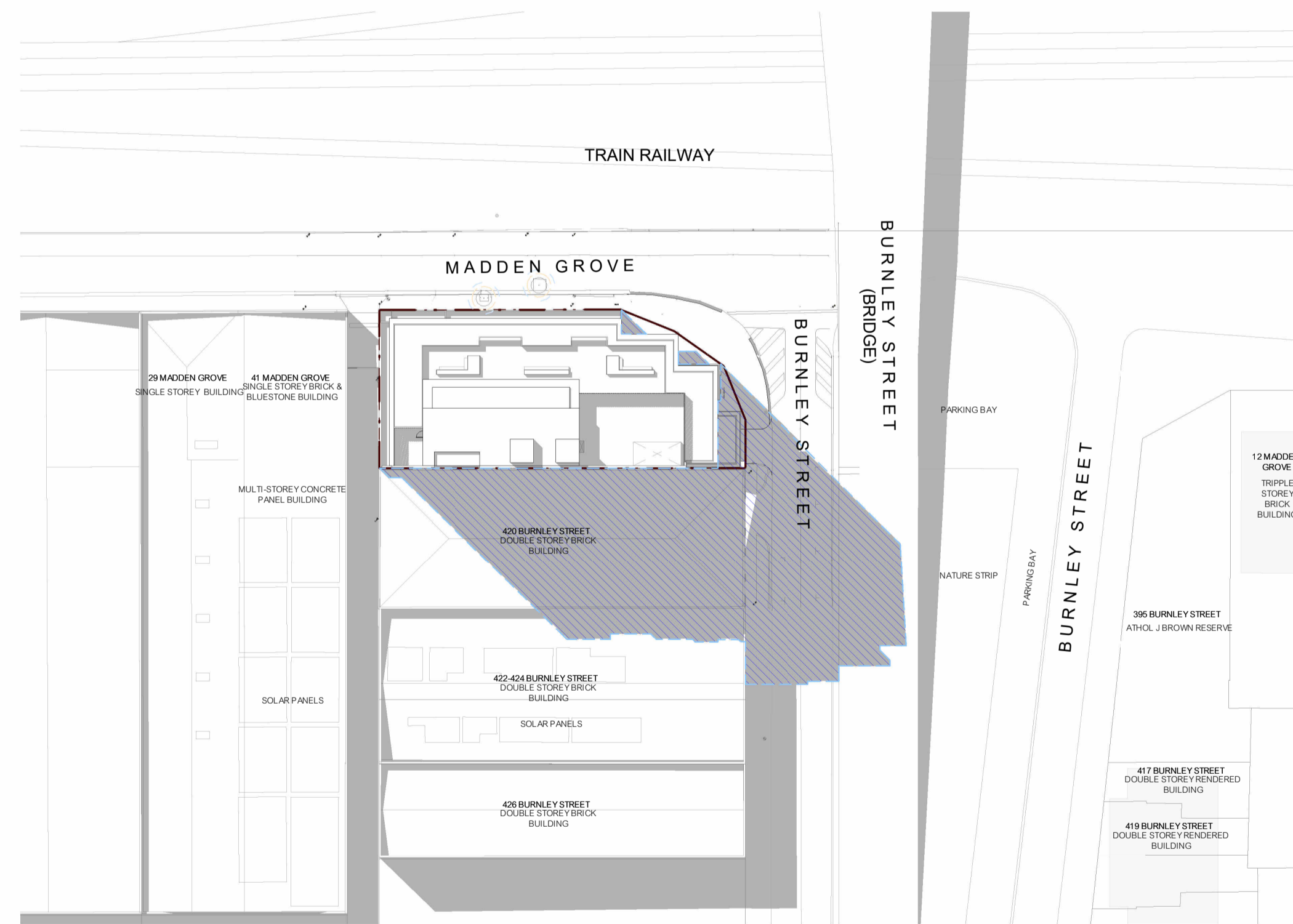
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SCALE 1 : 500



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SCALE 1 : 500



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SCALE 1 : 500



SHADOW DIAGRAM_2PM 21ST JUNE_PROPOSED
SCALE 1 : 500

GENERAL NOTES

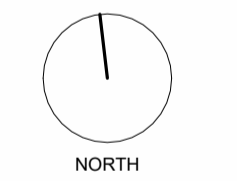
LEGEND

- TITLE BOUNDARY
- EXISTING SHADOW
- PROPOSED SHADOW
- PROPOSED SHADOW TO SURROUNDING SOLAR PANELS

A 22/04/2025 ISSUE FOR TOWN PLANNING RFI #1
12/03/2025 ISSUE FOR TOWN PLANNING

Rev	Date	Chkd	Reason for Issue

Based on Drawings Received:



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3121 Australia

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Project 25002 418 Burnley Street

Office Building

Title SHADOW DIAGRAM - EQUINOX

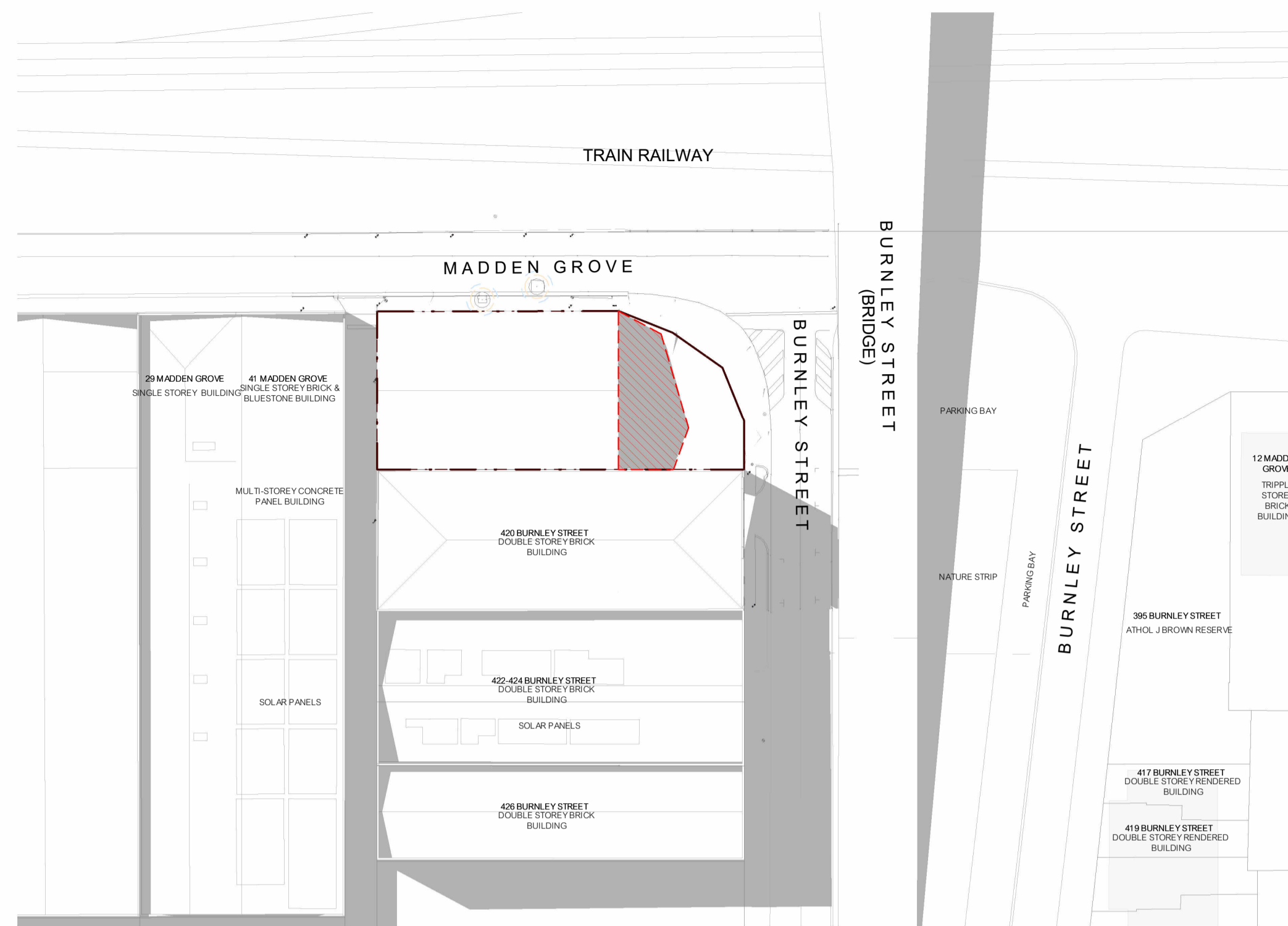
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Project No 25002

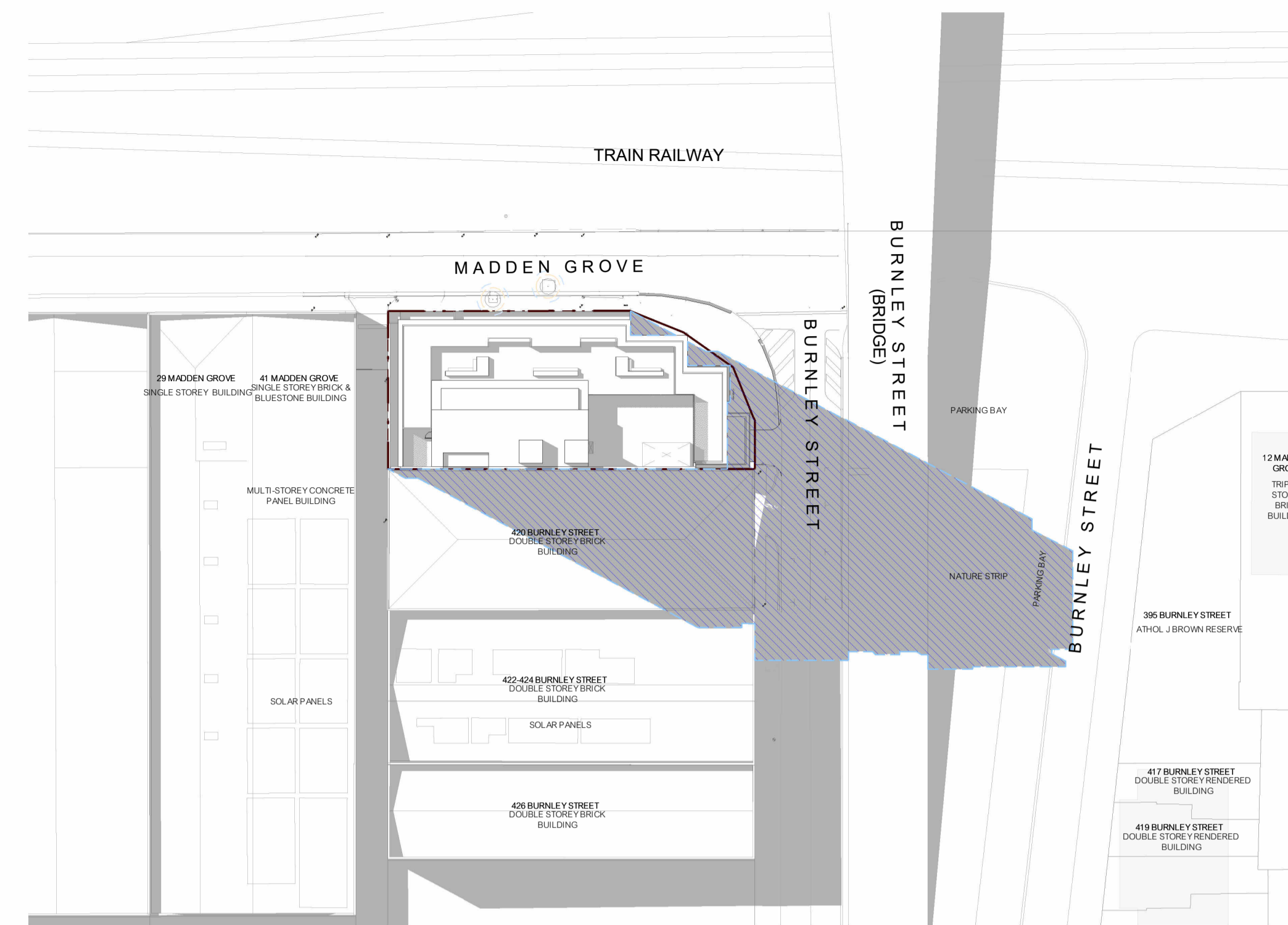
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Drawn By VZ Chkd CM Rev A



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SHADOW DIAGRAM_3PM 22nd SEPTEMBER_PROPOSED
SCALE 1 : 500

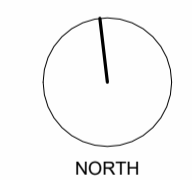
GENERAL NOTES

LEGEND

- TITLE BOUNDARY
- EXISTING SHADOW
- PROPOSED SHADOW
- PROPOSED SHADOW TO SURROUNDING SOLAR PANELS

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Project 25002 418 Burnley Street
Office Building

Title SHADOW DIAGRAM - EQUINOX

Date 22.04.2025 Project No 25002

Scale @ A1 1:500 Dwg No TP-904

Drawn By VZ Chkd CM Rev A

418 BURNLEY STREET, RICHMOND

**STUDIO
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- 1.0 SITE
 - 1.1 Title
 - 1.2 Planning Overlay
 - 1.3 Site Locality
 - 1.4 Site Locality - Surrounding Development
 - 1.5 Site Photos
 - 1.6 Site Photos (continued)
- 2.0 NARRATIVE
 - 2.1 Design Narrative
 - 2.2 Architecture Precedents
 - 2.3 Architecture Precedents
 - 2.4 Landscape Precedents
 - 2.5 Landscape Precedents
 - 2.6 Materiality
- 3.0 CONCEPT DIRECTION
 - 3.1 Street And Corner Activation
 - 3.2 Form and ESD Initiatives
- 4.0 ARCHITECTURE CONCEPT
 - 4.1 Architecture Sketch
 - 4.2 Architecture Sketch
 - 4.3 Architecture Views
 - 4.4 Architecture Views (continued)

PROJECT TEAM

Client:	Kokoda Property
Architect:	Studio McCue
Town Planner:	Contour
Surveyor:	Hellier McFarland
Services Engineer:	IGS
Structural Engineer:	Measure
Traffic:	Traffix Group
Waste:	Traffix Group
ESD:	GIW Environmental Solutions
Acoustic:	Octave Acoustics
Wind:	Mel Consult
Landscape Architect:	John Patrick
Render Artist:	Forge Studios



1.1 TITLE

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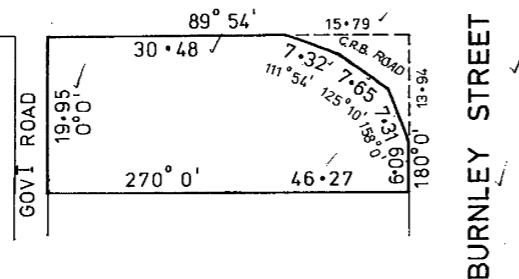
CP 103440
EDITION 2

PLAN OF CONSOLIDATION PARTS OF CROWN ALLOT ^{TS} 21 & 22 SECTION 40 CITY OF RICHMOND PARISH OF JIKA JIKA COUNTY OF BOURKE 0 5 10 20 30 40 LENGTHS ARE IN METRES	ENCUMBRANCES & NOTATIONS
---	--------------------------

APPROVED
 C. P. ALLEN
 Registrar of Titles
 for
 B. H. Grenness
 Assistant Registrar of Titles
 DATE 29-11-74 TIME 8:58

See Vol. 9061 Fol. 728

MADDEN GROVE



*Accept fees \$12.00
 off's made avail
 14.10.74*

LIST OF MODIFICATIONS				
LAND	MODIFICATION	DEALING No.	A.R.T	EDN. No.
	REMOVAL OF EASEMENT	APP1900 SEC73		2

Approved by Council
 8/7/74
 B. L. Shavalter

CONSENT OF COUNCIL Given with the Common Seal of the Mayor and Council of the City of Richmond on the 28 th day of Aug. 1974 J. B. Dougherty Mayor J. B. Langdon This plan has been sealed pursuant to Section 169(1) of the Local Government Act 1958. Town Clerk.	SURVEYORS CERTIFICATION I Certify that this plan has been made by me and accords with title and is mathematically correct and is based on survey. W. J. Walters Licensed Surveyor. Date 18-6-74 F. J. WALTERS & LEA PTY. LTD. CONSULTING ENGINEERS AND SURVEYORS 30 QUEEN STREET MELBOURNE, 3000
---	--

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The Victorian Government acknowledges the Traditional Owners of Victoria and pays respects to their ongoing connection to their Country, History and Culture. The Victorian Government extends this respect to their Elders, past, present and emerging.

REGISTER SEARCH STATEMENT (Title Search) Transfer of Land Act 1958

 VOLUME 09061 FOLIO 728 Security no : 124120249665V
 Produced 29/11/2024 09:56 AM

LAND DESCRIPTION

 Land in Plan of Consolidation 103440.
 PARENT TITLES :
 Volume 08249 Folio 319 Volume 09004 Folio 757
 Created by instrument CP103440 13/12/1974

REGISTERED PROPRIETOR

 Estate Fee Simple
 Sole Proprietor
 BURNLEY STREET INVESTMENTS PTY LTD of 41 MADDEN GROVE RICHMOND VIC 3121
 AL781707D 27/03/2015

ENCUMBRANCES, CAVEATS AND NOTICES

 MORTGAGE AV064631Y 26/11/2021
 COMMONWEALTH BANK OF AUSTRALIA

Any encumbrances created by Section 98 Transfer of Land Act 1958 or Section 24 Subdivision Act 1988 and any other encumbrances shown or entered on the plan or imaged folio set out under DIAGRAM LOCATION below.

DIAGRAM LOCATION

 SEE CP103440 FOR FURTHER DETAILS AND BOUNDARIES

ACTIVITY IN THE LAST 125 DAYS

NIL

-----END OF REGISTER SEARCH STATEMENT-----

Additional information: (not part of the Register Search Statement)

Street Address: 418 BURNLEY STREET RICHMOND VIC 3121

ADMINISTRATIVE NOTICES

 NIL

eCT Control 19531K DENTONS AUSTRALIA
 Effective from 26/11/2021

DOCUMENT END

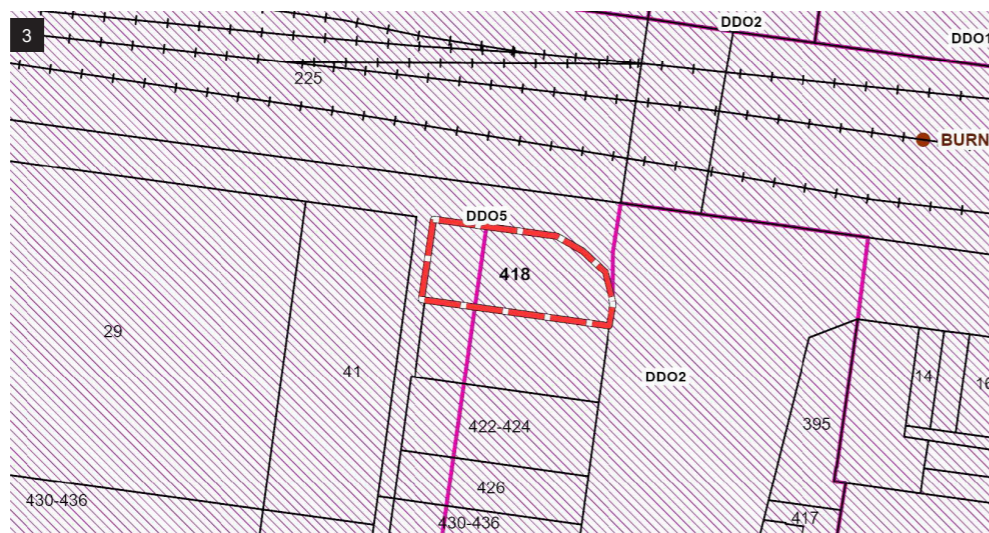
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ARCHITECTURE DIAGRAM

418 BURNLEY STREET RICHMOND

1.2 PLANNING OVERLAY

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- 1 INDUSTRIAL 3 ZONE (IN3Z)
- 2 DEVELOPMENT CONTRIBUTIONS PLAN OVERLAY (DCPO)
- 3 DESIGN AND DEVELOPMENT OVERLAY (DDO)
- 4 SURROUNDING OVERLAYS - HERITAGE OVERLAY (HO)

ARCHITECTURE DIAGRAM

418 BURNLEY STREET RICHMOND

4

1.3 SITE LOCALITY

**STUDIO
MCCUE**



ARCHITECTURE DIAGRAM

418 BURNLEY STREET RICHMOND

1.4 SITE LOCALITY - SURROUNDING DEVELOPMENT



236 Coppin Street
8-storey office complex

358-360 Swan Street
10-storey office complex

331 Swan Street
8-storey office complex

480 Swan Street
12-storey office complex

484-488 Swan Street
14-storey office complex

439-443 Swan Street
7-storey office complex

471-473 Swan Street
8-storey office complex



ARCHITECTURE DIAGRAM

418 BURNLEY STREET RICHMOND

1.5 SITE PHOTOS

**STUDIO
MCCUE**

420 BURNLEY STREET

SITE

MADDEN GROVE



ARCHITECTURE DIAGRAM

418 BURNLEY STREET RICHMOND

7

1.6 SITE PHOTOS (Continued)

**STUDIO
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ARCHITECTURE DIAGRAM

418 BURNLEY STREET RICHMOND

1.7 SITE PHOTOS (Continued)

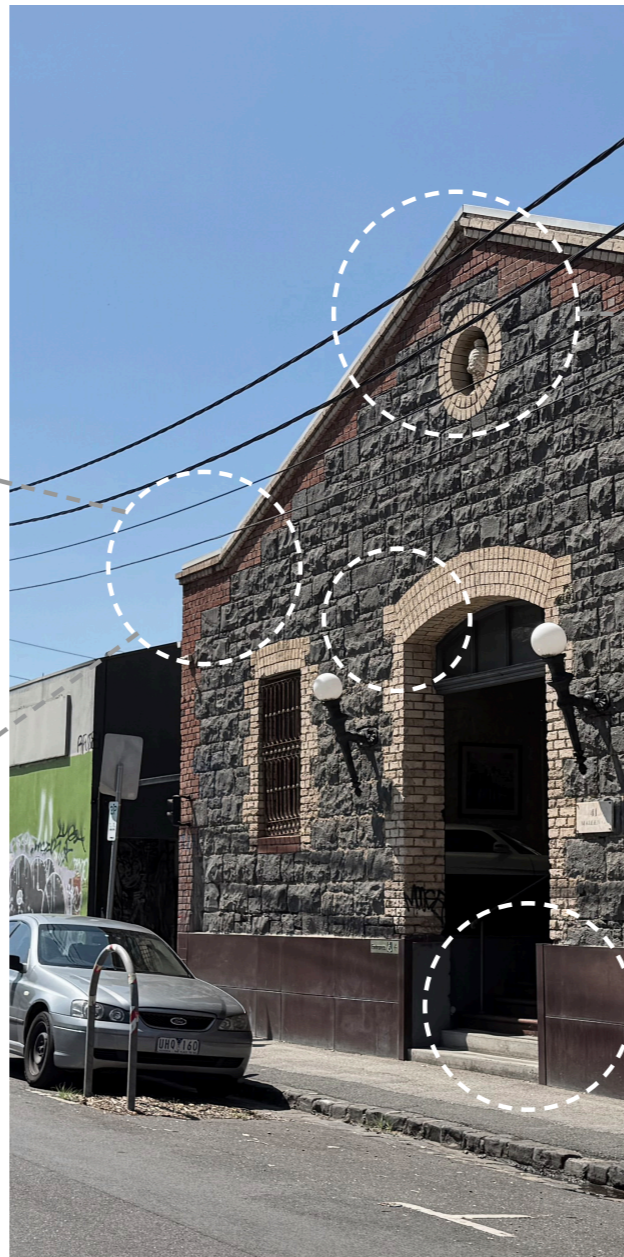
**STUDIO
MCCUE**



DATUM OF PARAPET CAPPING TO THE HERITAGE INFORMS A STRONG PODIUM AND ARTICULATED DETAIL OF THE FACADE



BLUESTONE BLOCK ALTERNATING PATTERN INFORMS THE DETAIL OF THE BRICK PODIUM



EXTENSIVE USE OF MASONRY AT STREET LEVEL INFORMS THE SELECTION OF PUNCHED BRICK OPENINGS TO THE PODIUM FORM



MARKING THE THRESHOLD WITH DEEP RECESSES

ARCHITECTURE DIAGRAM

418 BURNLEY STREET RICHMOND

9

2.0 NARRATIVE

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2.1 DESIGN NARRATIVE

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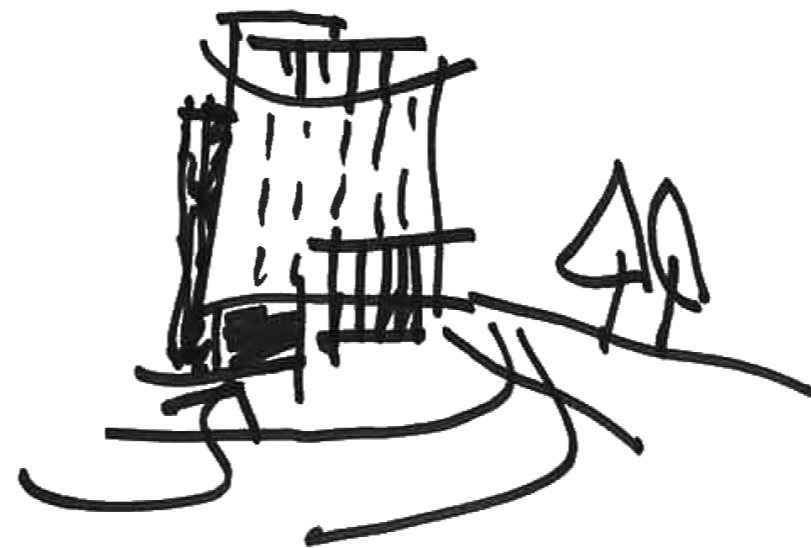
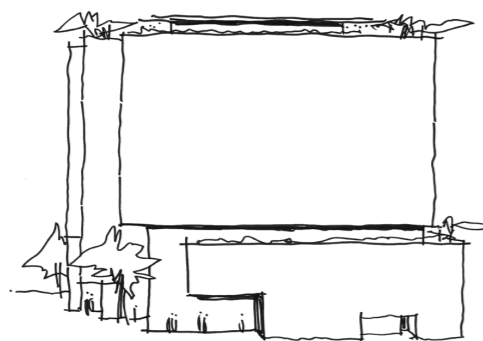
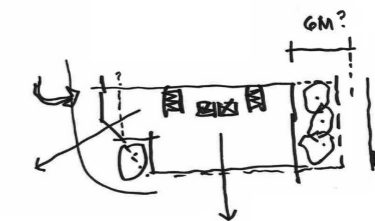
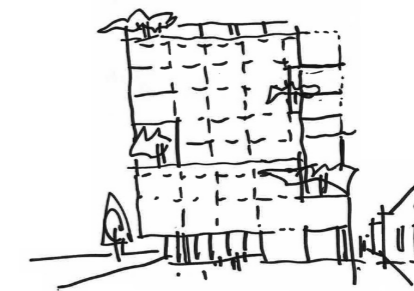
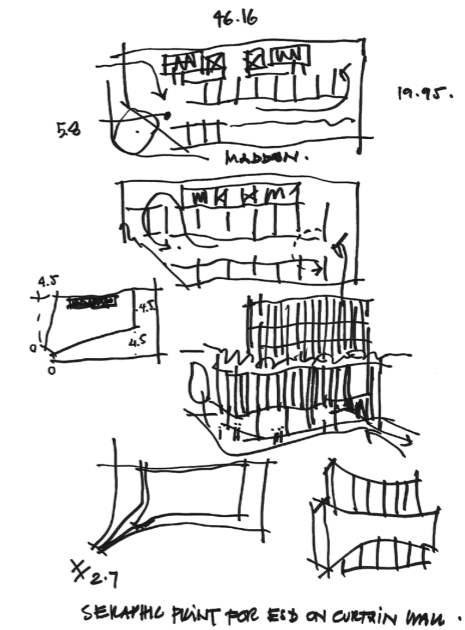
The opportunity to be the first building of scale and heralding a new public corner to Madden Grove the building has an open ground plane for active retail with a simplified building form above.

Viewed from the Burnley overpass and speaking to the opposing corner scale of the Australia Post built form, the language is of a podium form that relates to the height of the adjacent Dutton's heritage bluestone facade.

A deep set recess above podium form and emphasised to the north-west corner respect the gable form of the Dutton's garage building.

The main built form is a consistent floorplate size no smaller than 600sqm NLA where the skin is wrapped in an environmental glass facade with expressed blades responding to orientation to control heat gain.

A recessed top floor pavilion structure offers communal amenity to the building with western terraces to each floor capture city views and provide ample opportunity for greening of outdoor space.

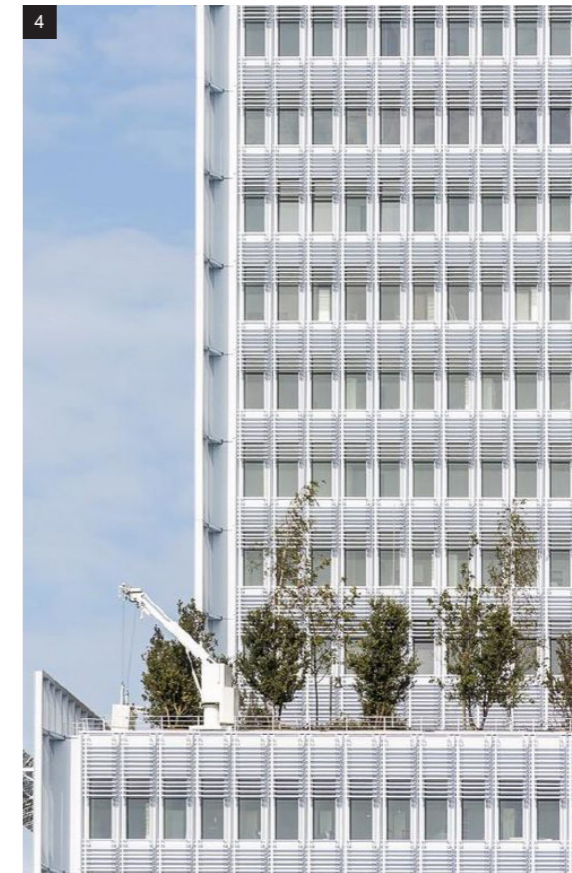
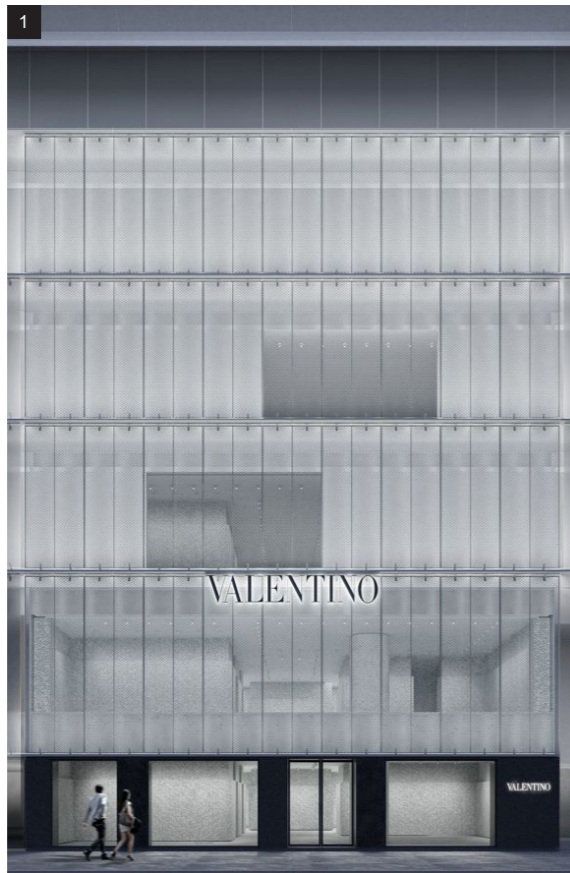


ARCHITECTURE DIAGRAM

418 BURNLEY STREET RICHMOND

2.2 ARCHITECTURE PRECEDENTS

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- 1 Simple form with a translucent skin (visible when looking out from floorspace) to control heat load, select areas of clear glazing
- 2 The big idea of a curving facade boldly articulated
- 3 Active ground plane and recessed shopfront glazing to base
- 4 Articulated recesses in form express planar architecture

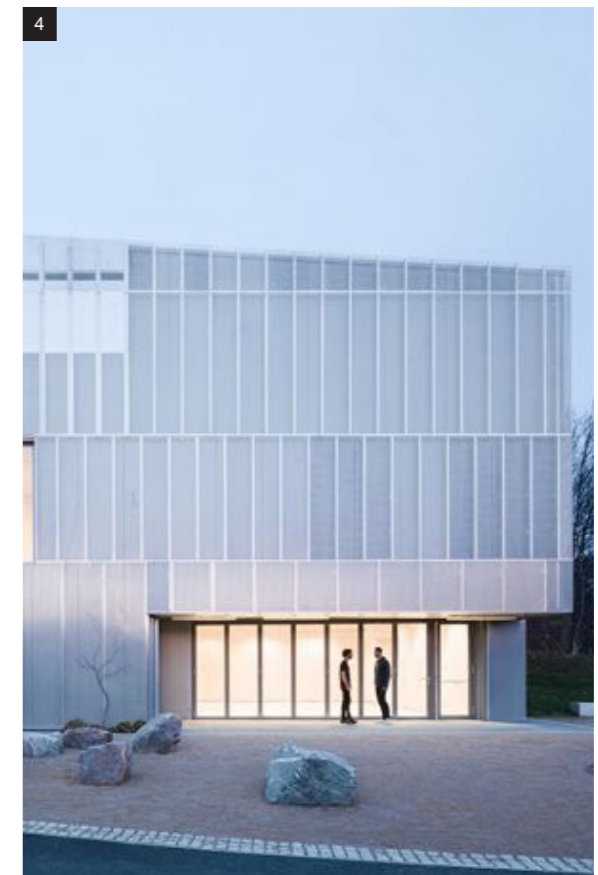
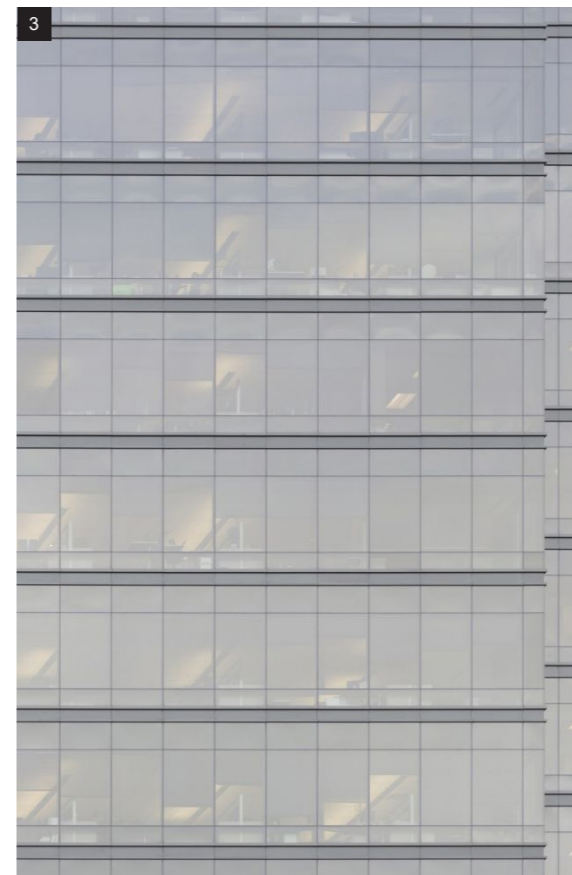
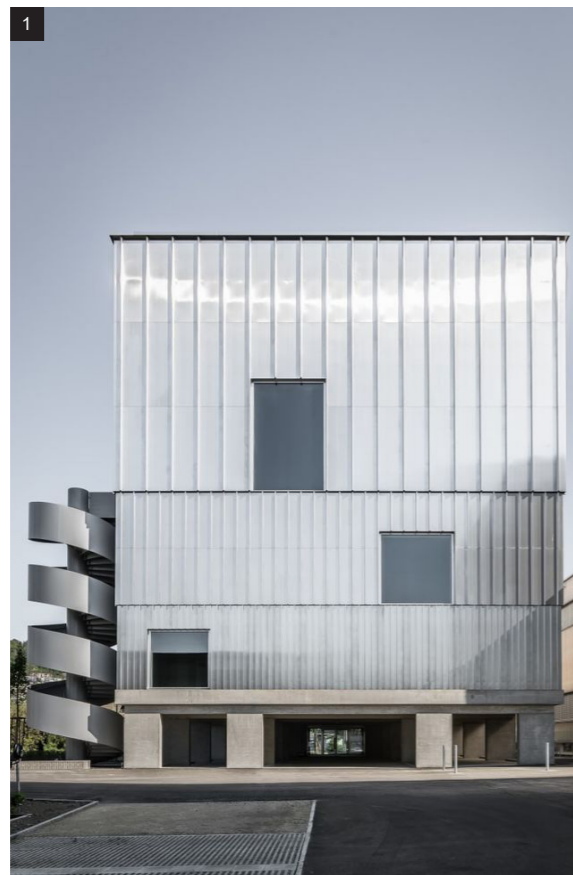
PRECEDENT IMAGERY

418 BURNLEY STREET RICHMOND

12

2.3 ARCHITECTURE PRECEDENTS

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- 1 Simplified form to maximise NLA with focus areas of detail
- 2 External brickwork skin of alternating pattern making up the podium expression
- 3 Softly tinted glass to upper storeys enabled by external louvers and blades
- 4 Cutouts within the form to provide terraces area to and above the podium whilst also articulating the rising form

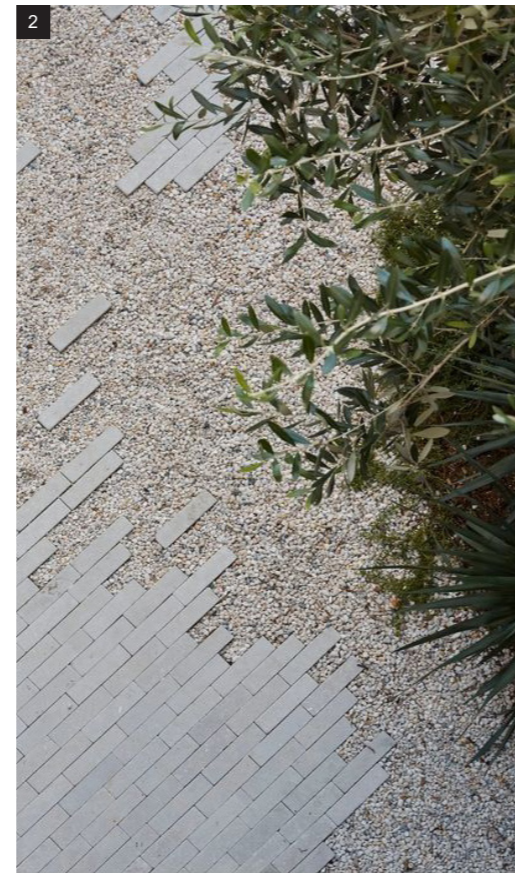
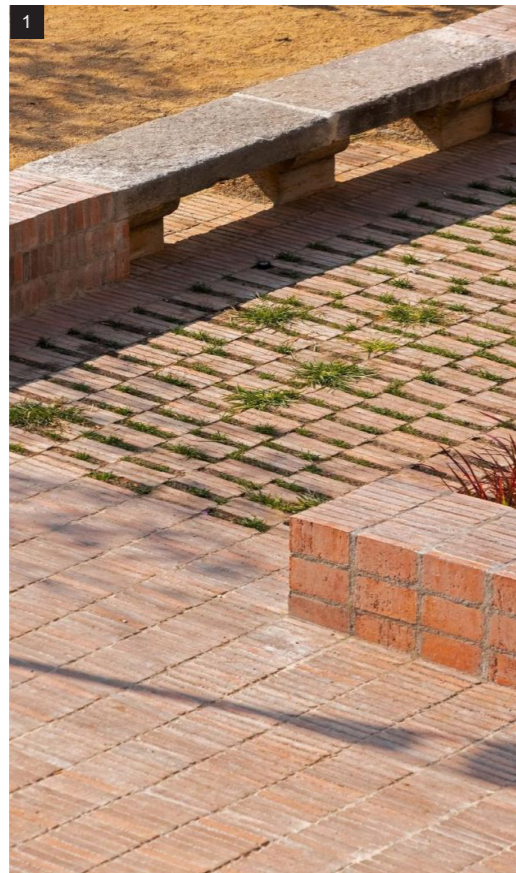
PRECEDENT IMAGERY

418 BURNLEY STREET RICHMOND

13

2.4 LANDSCAPE PRECEDENTS

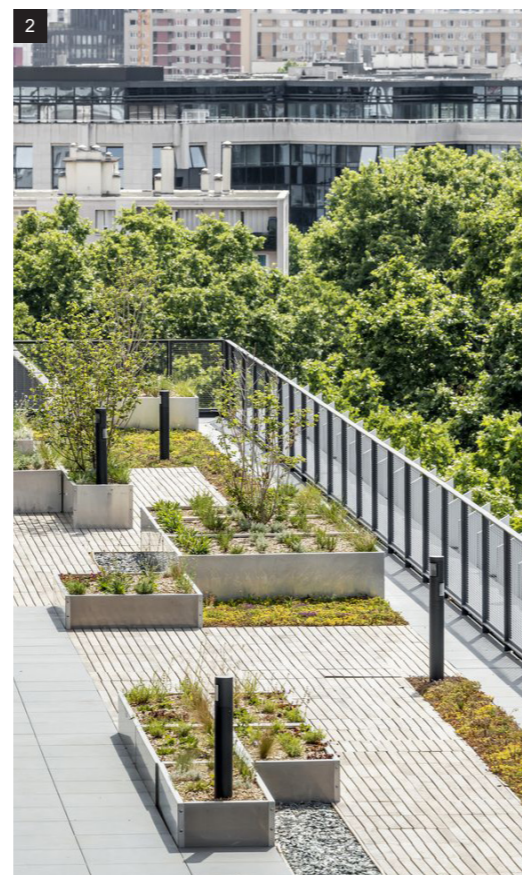
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- 1 Repeating rhythm of masonry formatted to align with the podium architecture are repeated in landscape detail
- 2 Batten rhythm of brickwork tiles with planter areas at the arrival to the building
- 3 Localised areas for tree planting at street level, enlivens the nature strip and main pedestrian arrival to the building
- 4 Evergreen creepers on the podium form help integrate and give a more residential feel to the commercial building

2.5 LANDSCAPE PRECEDENTS

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- 1 Casual seating areas on the rooftop for breakout and lunch areas with perimeter planting
- 2 Raised planters for deeper soil zones allowed near or over structural support
- 3 External pergola structure for under cover working and relaxation areas
- 4 Tree planting to key vistas to create areas of intimacy on the roof

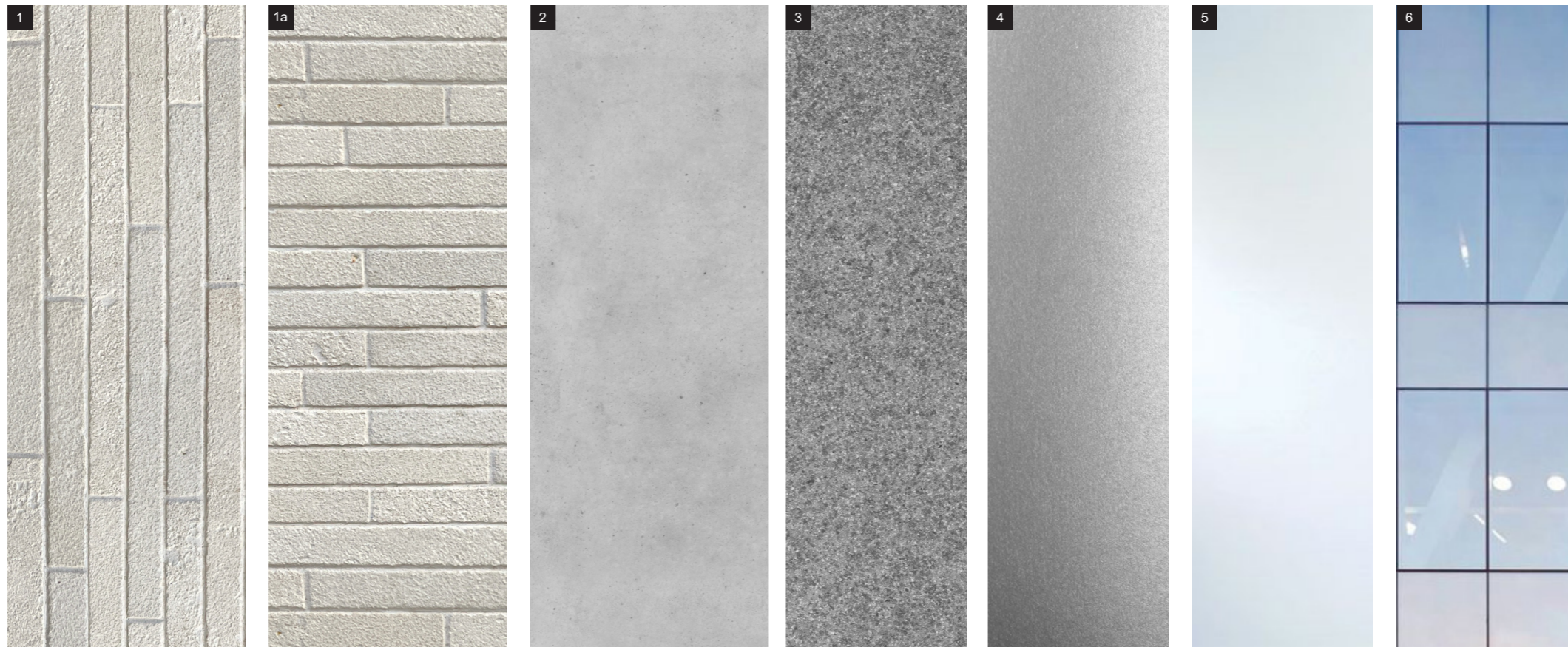
PRECEDENT IMAGERY - ROOF GARDEN

418 BURNLEY STREET RICHMOND

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2.6 MATERIALITY

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- 1 Long Brick - laid vertically to the podium
- 1a Long Brick - laid horizontally on the podium cross matched to the brick area below
- 2 Concrete finish to expressed columns at ground floor and the southern precast walling
- 3 Exposed Aggregate Concrete banding detail inlaid in precast walling to the south
- 4 Powdercoated Finish - Silver, to window frame detail and external fins for sun shading
- 5 Clear glass to comply with SMP
- 6 Clear Glass with spandrel panel to comply with SMP

MATERIALITY

418 BURNLEY STREET RICHMOND

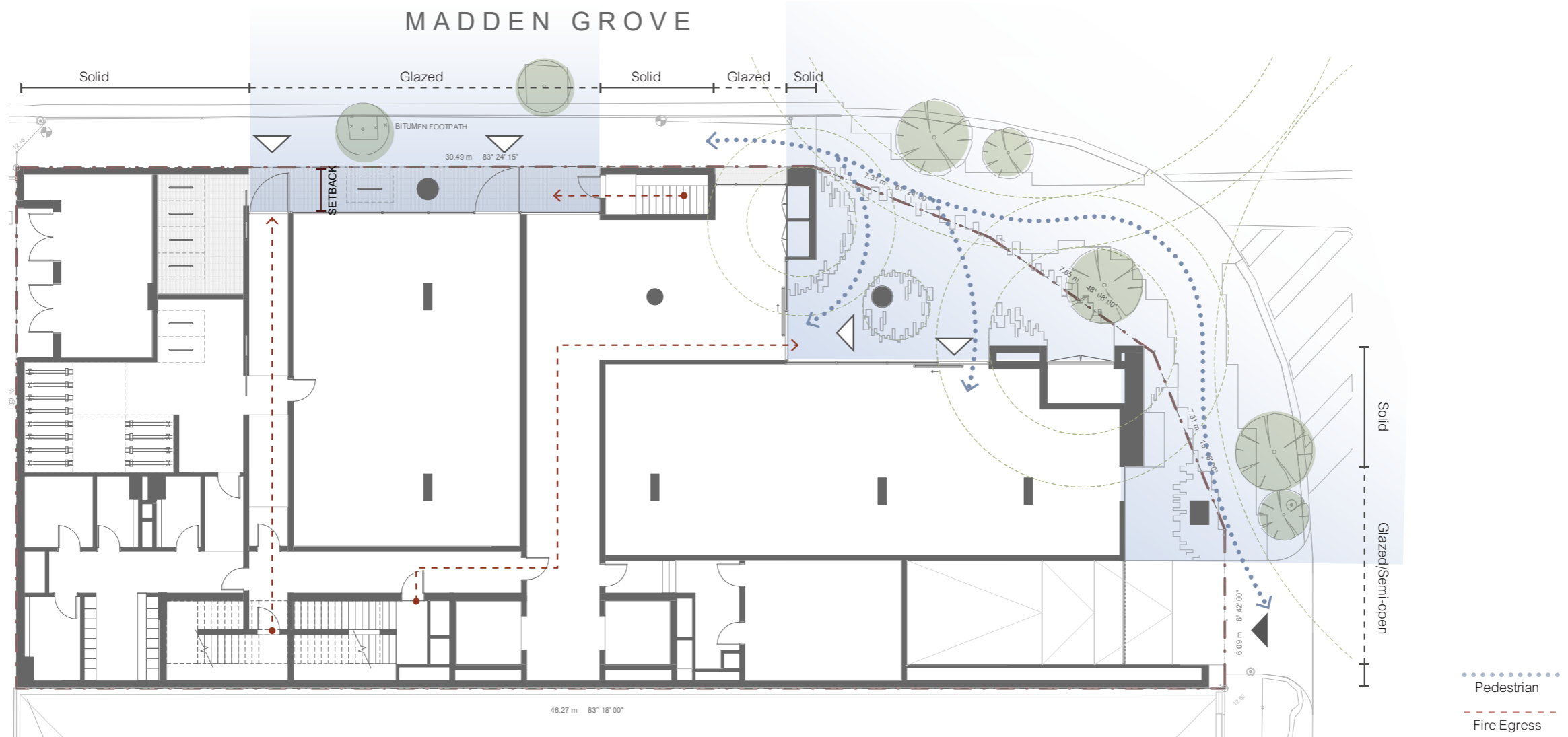
16

3.0 CONCEPT DIRECTION_PLANNING

**STUDIO
MCCUE**

3.1 STREET AND CORNER ACTIVATION

**STUDIO
MCCUE**



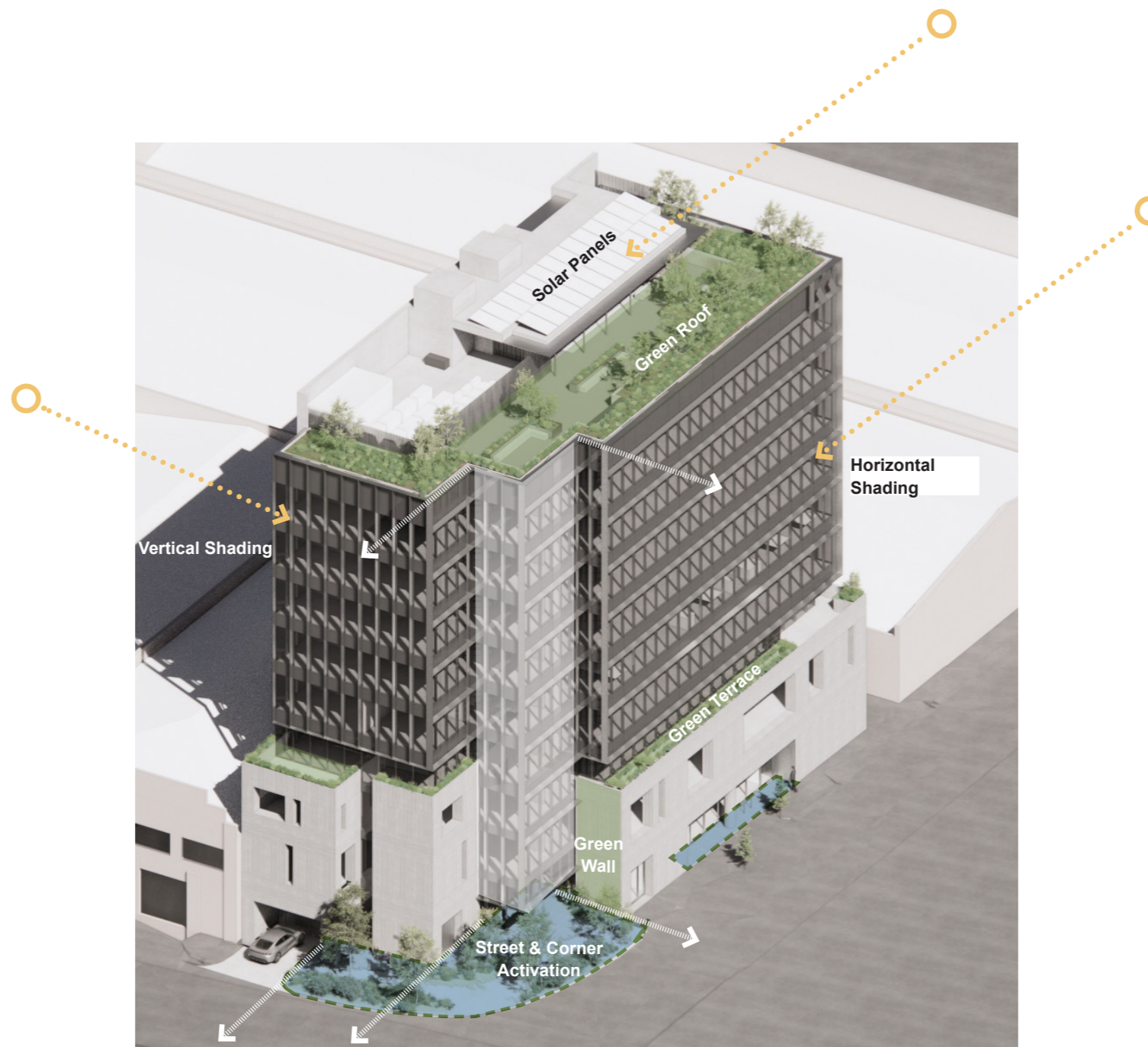
The ground floor plane provides a number of entry points for street activation. The primary indent in the form is to the commercial and Retail 1 entry with arrival forecourt providing opportunity for paving and planting. A coffee window to the Burnley Street frontage enables retail tenants to trade on to two aspects. Large extent of glazed openings on the northern extent of the commercial entry lobby provide strong connection to the street-scape and beyond. The secondary retail space with its glazed frontage inset from footpath level provides an opportunity for visitor cycle space at street and undercover weather protection of the entry. Adjacent are the entry points to the end of trip access for both visitor cycle parking for the building and cycle parking for the tenants.

FLOOR PLAN - GROUND ACTIVATION

418 BURNLEY STREET RICHMOND

3.8 Form and Mass

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Targeting BESS Excellence there are a number of sustainability initiatives at play across the proposal. The open corner at street level enables significant tree and creeper planting to green the masonry podium form on arrival. Upper storeys of curtain wall glass with fixed horizontal and vertical louvres respond to site orientation to provide as clear a glazed strategy as possible to meet the objectives of BESS. The active rooftop space with landscaped areas for tenant dining and breakout are a welcome amenity addition to extensive end of trip facilities at ground floor. Ample cycle spaces are provided at ground for occupant and visitors alike whilst 28 car spaces and two motor cycle spaces meet tenant objectives with respect to transport access. Sited directly adjacent the Burnley Station the site is well served by public transport.

3D AXONOMETRIC VIEW

418 BURNLEY STREET RICHMOND

4.0 ARCHITECTURE DIRECTION

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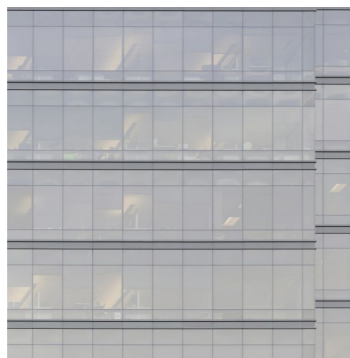
4.1 Concept Direction_Architecture Sketch

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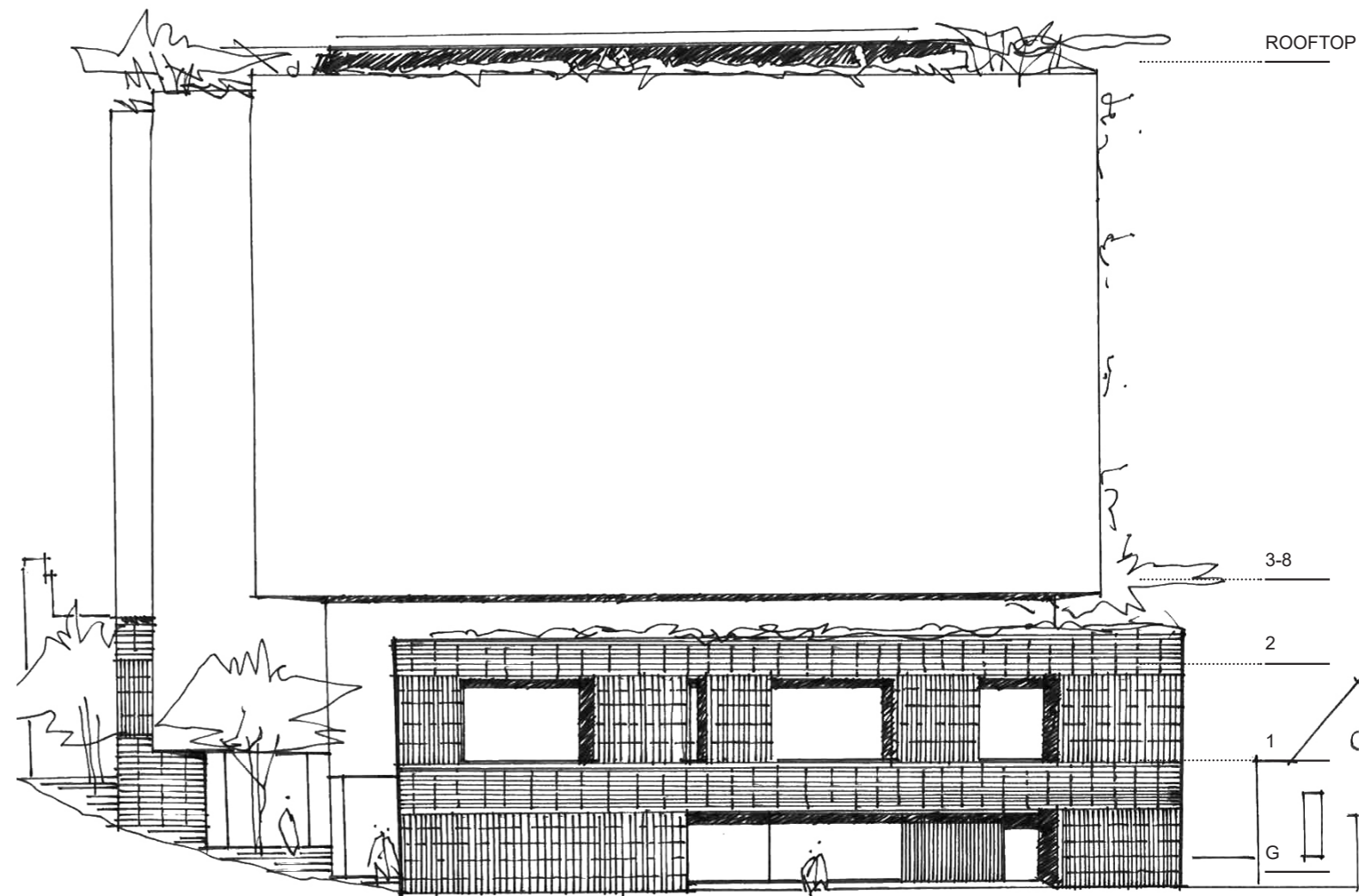
- 1 Strongly articulated podium with expressed brickwork to reflect adjacent heritage building
- 2 Curtain wall upper with light tint to meet ESD rating
- 3 Equitable development and visual interest to south boundary



1



2

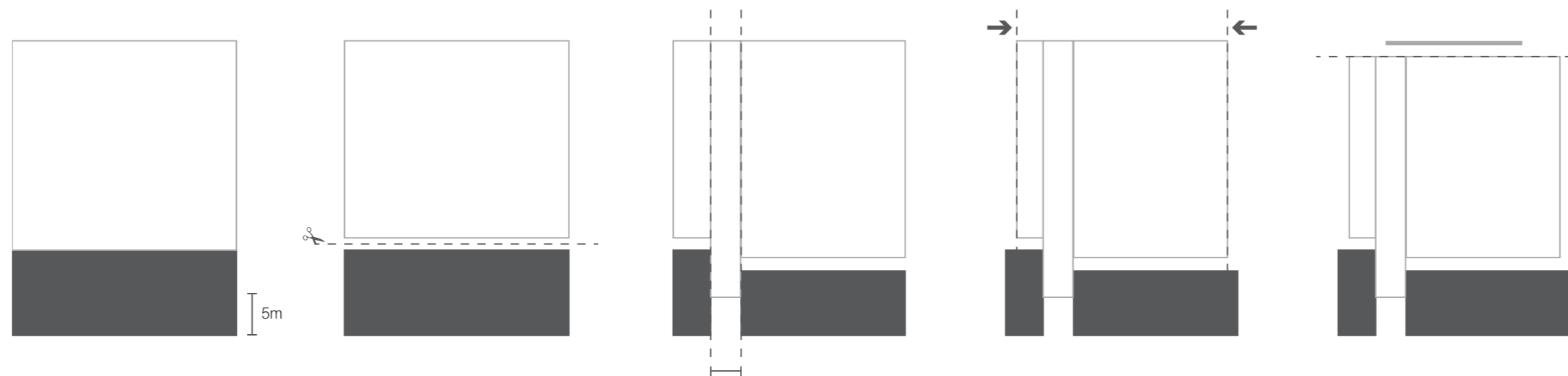


SKETCH CONCEPT

418 BURNLEY STREET RICHMOND

4.2 Concept Direction_Architecture Sketch

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1 Retail glazing with ground floor at 5m floor to floor

2 Recess at level two as an expressed break between podium and upper form

3 Open and glazing corner to Burnley St and Madden Grove

4 Double step setback to Burnley Street aspect; Podium setback at Level 2 respecting heritage of Dutton's

5 Setback pavilion roof element as communal amenity

SKETCH CONCEPT

418 BURNLEY STREET RICHMOND

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4.3 Concept Direction_Architecture Views

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Brick base with articulated curtain wall facade with recessed break between forms. The podium form will transition from a tall 3 storeys to Burnley Street whilst it steps down progressively to two storeys on Madden Grove, adjacent Dutton's heritage facade.

CORNER VIEW FROM BURNLEY STREET AND MADDEN GROVE

ARTIST'S IMPRESSION

418 BURNLEY STREET RICHMOND

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4.4 Concept Direction_Architecture Views

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The indented corner to the north-western podium form respects the setback to the Dutton's Garage building with a strong datum of the brick podium contextually relevant to its neighbours.

MADDEN GROVE AERIAL VIEW

ARTIST'S IMPRESSION

418 BURNLEY STREET RICHMOND

4.5 Concept Direction_Architecture Views

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The southern aerial view on approach to the site and that of the Burnley overpass shows the open staircase and detail pattern to the precast panels. Opportunity for building signage to the top right panels are shown only for artist impression purposes. The Eastern end of each floorplate above the podium is glazed to provide a more active edge and elevation to this view.

SOUTHERN AERIAL VIEW FROM BURNLEY STREET

ARTIST'S IMPRESSION

418 BURNLEY STREET RICHMOND

4.6 Concept Direction_Architecture Views

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The north-western view of the building with its deep recess in form above podium level and detailed brick podium that relates to the variegated nature of bluestone ornamentation of the Dutton's Garage building. The external fins both horizontal and vertical articulate the curtain wall facade above the brick base with planting spilling over any transition in form.

VIEW ACROSS MADDEN GROVE

ARTIST'S IMPRESSION

418 BURNLEY STREET RICHMOND

**STUDIO
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THANKYOU

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418 Burnley Street, Richmond Urban Design Advice

Date	5 th August 2025
Council	Yarra City Council
Reference	PLN25/0181

1.0 Background

Site Description	<ul style="list-style-type: none"> The subject site is a curved rectangular allotment located on the southwestern corner of Burnley Street (to the east) and Madden Grove (to the north), Richmond. It abuts a government road to the west, has a total site area of 865sqm (approx.) and is generally flat. It is occupied by single storey warehouse.
Site Interfaces	<ul style="list-style-type: none"> North – Madden Grove lies north of the site, which is approximately 10.0m wide. It has no footpath on the northern side. Further beyond is the railway corridor serving the Belgrave, Alamein and Glen Waverley lines. East – Burnley Street lies directly east of the subject site, which is broad road reserve of approximately 50.0m in width that features numerous at-grade and elevated road connections. Further beyond is the Athol J. Brown public open space reserve, Burnley Train Station and a low-scale residential pocket. South – 420 Burnley Street, Richmond lies directly south of the subject site, which is a relatively low-scale red-brick warehouse. It forms a number of similar built form conditions with properties further south. West – A narrow laneway exists directly west of the subject site, with the individually-significant bluestone heritage fabric of 41 Madden Grove, Richmond located further beyond (HO275). The balance of Madden Grove further west generally features nondescript commercial built form through to Rooney Street.
Proposal	<ul style="list-style-type: none"> A ten-storey predominantly office development with basement car parking is proposed, which rises to approximately 38.8m high (excluding plant).
Zone/s	<ul style="list-style-type: none"> IN3Z – Industrial 3 Zone (Clause 33.03).
Design Relevant Overlay/s	<ul style="list-style-type: none"> DDO2 – Main Roads and Boulevards (Clause 43.02). HO275– 41 Madden Grove, Richmond (Clause 43.01) (external to site). DDO27 – Swan Street Activity Centre Precinct 3 (Clause 43.02) (external to site).
Design Relevant Policy	<ul style="list-style-type: none"> Clause 02.03 – Strategic Directions. Clause 02.04 – Strategic Framework Plans. Clause 13.07-1L-01 – Interfaces and Amenity Clause 15.01-1L – Urban Design. Clause 15.01-1L-01 – Signs. Clause 15.01-2L – Building Design. Clause 17.01-1L – Employment. The Victorian Urban Design Guidelines ('VUDG'). Relevant State and Regional design provisions.
Documents Reviewed	<ul style="list-style-type: none"> The architectural plans prepared by Studio McCue (Revision A, dated 22.04.2025). The urban context and design response report prepared by Studio McCue (dated 12.03.2025). The town planning report prepared by Contour Consultants (Version 2, dated 05.25). The landscape concept plan prepared by John Patrick Landscape Architects (dated 03.25).

2.0 Urban Design Assessment

2.1 Design Context

Although the subject site arguably does not enjoy the same type of express policy support for intensive redevelopment as other parts of the Yarra municipality (including the 'Swan Street Major Activity Centre' toward the north of the subject site), it clearly has multiple physical qualities that support greater built form intensity. These include (but are not limited to):

- The complete **absence of any abutting or proximate residential sensitivity** that might constrain development potential through scale and amenity impacts, with the closest residentially-zoned property located some 60.0m (approx.) east of the subject site.
- The **robust public realm condition** in abuttal to the subject site, which consists of a wide and elevated arterial road (to the east of the subject site) and an at-grade railway corridor (to the north, beyond Madden Grove). Interestingly, there is effectively no opposing footpath condition to each of the subject site's abutting streets, which distinguishes the subject site from the vast majority of properties within Melbourne and which has design implications on close-range perceptions of built form and potential design responses. Refer below.



Figure 1 - Various site photos highlighting the existing robustness of abutting streetscape interfaces and general absence of a meaningful opposing footpath condition, including Burnley Street (left) and Madden Grove (right) (Source: own photos, July 2025).

- The **extreme proximity to multiple fixed public transportation options**, particularly the Burnley Train Station (approximately 75.0m northeast) and tram services on Swan Street (further north). Both position the subject site as highly accessible and suitable for redevelopment and stand in addition to other proximate transportation options and modes (including the Main Yarra Trail cycling corridor).
- The **emerging built form character** of the subject site's immediate context, which is evident within the existing 12 storey office development at 462-482 Swan Street, Richmond (ie. the 'Australia Post' building) and will be further reinforced by future realisation of the 14 storey office approval at 484-490 Swan Street, Richmond. Both are contributing to the emergence of a clustering of higher-scale forms in close proximity to the Burnley Train Station that are readily visible from - and influential on - the subject site's immediate context. Both will be supplemented by future similarly-sized developments as other Swan Street redevelop in accordance with DDO27 over time.
- The precedent set by other pockets of Yarra that are similarly zoned IN3Z that have approval to host taller commercial developments as **'buffering uses' and developments** (in accordance with the purpose of the zone). In this sense we find the findings of Tribunal decisions such as *'Pertile v Yarra CC [2019] VCAT 851'* relevant insofar as it confirms the general suitability of taller office development for IN3Z-zoned locations.

- The **absence of any particularly valued aspect of the subject site's existing condition**, at least as highlighted by the absence of a HO or VPO or similar. This distinguishes the subject site from other properties in close proximity that do have valued existing fabric, particularly that which exists directly west of the subject site at 41 Madden Grove, Richmond (HO275).

As a consequence, we see the subject site as an ideal candidate for taller, more intensive redevelopment provided it responds appropriately to local context (as reflected by multiple Planning Scheme provisions, particularly Clause 15.01-2L and DDO2). Our assessment of this is as follows.

2.2 Design Assessment

Building Height and Massing

- The proposal appears to adopt a site-responsive approach to **street wall heights** that responds to the nuances of the subject site's physical location. To Burnley Street, the proposed three storey street wall height (approximately 13.5m high) will provide a comfortable level of spatial definition to the adjoining abutting Burnley Street 'service road' and will comfortably integrate with existing and future street wall heights further north under DDO27 'Interface E' and its 14.0m high street wall aspirations (as per the desire of DDO2 to do so). To Madden Grove, the intention to pursue a comparatively lower-scale street wall height will visibly gesture to the presence of adjoining heritage fabric at 41 Madden Grove, Richmond through a tangible scale reduction. This is consistent with the urban design provisions of Clause 15.01-1L that specifically seek it.
- Proposed **street wall setbacks** are also considered and appear to be driven by the subject site's unusual curving property alignment (within the northeast) as well as local viewsheds. For example, the design intention to largely build street walls on or close to the public realm will provide the type of clear public/private delineation expected by Clause 15 and its underpinning Victorian Urban Design Guidelines, and the intention to effectively widen the public realm within the northeast of the subject site will respond appropriately to the longer-range viewshed and 'arrival experience' of pedestrians on approach from Burnley Train Station further east.
- Going further, we see the **technical absence of a street wall within the northeast** of the subject site as a particularly high-quality urban design response. By eschewing a traditional 'street wall/upper form' massing typology within this aspect of the subject site, the proposal instead achieves a 'tower touchdown' type outcome in which the legibility of the proposed 'front door'/lobby is enhanced, the width of the public realm is widened and the relationship between the building height and the public realm is improved (ie. taller building heights are not setback and 'detached' from the public realm behind an intervening street wall).
- **Upper-level setbacks** similarly appear to vary in response to the physical realities of each abutting streetscape condition. To Burnley Street, a prevailing upper-level setback of 3.8m (approx.) will comfortably define a street wall/upper form building profile, reduce scale and bulk in longer-range views from GRZ-zoned properties further afield, set an appropriate equitable development precedent for future development of similarly IN3Z-zoned land further south, and respond to the provisions of DDO2 that seek broader streetscape integration along Yarra's main roads and boulevards (when allied with existing built form and DDO27 further north).

To Madden Grove, we think the absence of an opposing footpath supports a comparatively lesser upper-level setback that better-defines the breadth of the opposing railway corridor, especially when allied with the unlikelihood that 41 Madden Grove will redevelop further (due to HO275 and its recent rear redevelopment) and that opposing private land to the north will 'rise sheer' to a height of 40m under DDO27 's 'Interface H' setback provisions. This allows greater urban design flexibility for the subject site to provide a slightly 'robuster' standalone built form outcome, and the intended 1.5m upper level setback to Madden Grove achieves this.

- Although the **maximum height** of the proposal incorporates a number of mechanisms that reduce perceptions of scale (including highly recessive public realm setbacks for the uppermost storey and predominant office floor-to-floor heights of 3.6m), the scale of the proposal will be markedly taller than that which currently exists on site or has traditionally existed within the subject site's broader IN3Z pocket. However, we also note that the Planning Scheme actively seeks a far more intensive *future character* in close proximity to the subject site (as outlined in the diagram below), and consider that the general height and scale of the proposal will comfortably integrate with the emerging development trajectory of the subject site's local area. It will also contribute to a 'clustering' of similarly-scaled developments on non residential-zoned land in close proximity to Burnley Train Station. When allied with the demonstrated absence of unreasonable amenity impact (including Equinox shadow impact upon the Athol J. Brown Reserve or GRZ-zoned land further east), we support the proposed height.

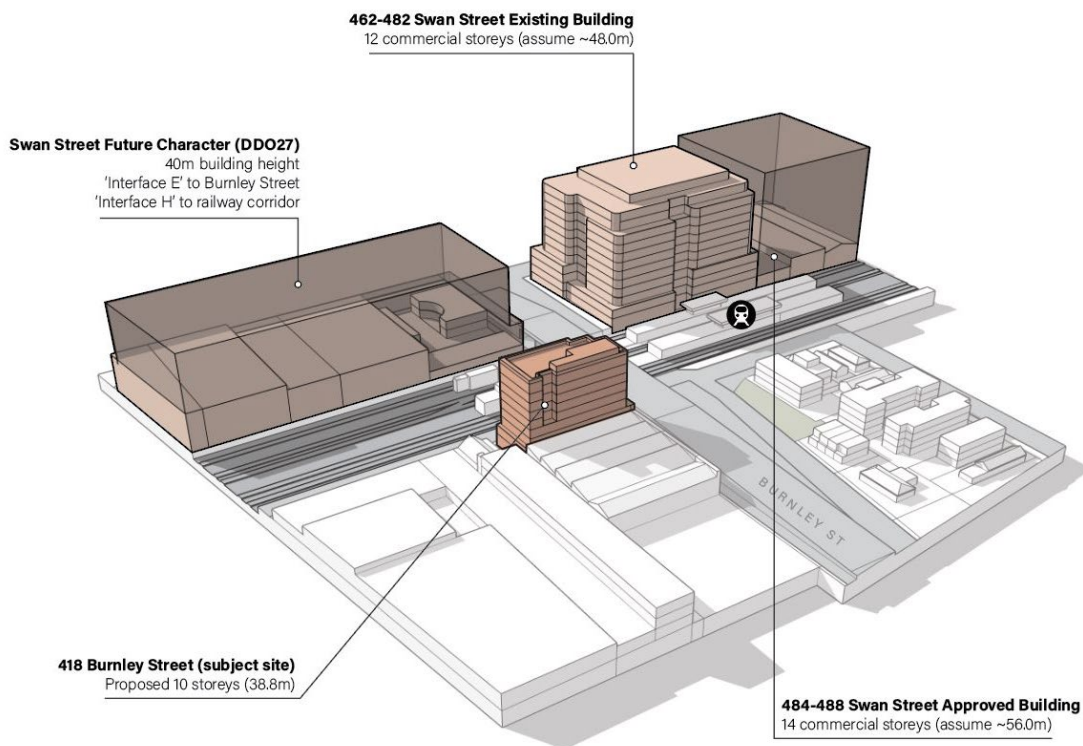


Figure 2 - Proposed built form scale (pink) relative to existing, approved and future building scales on non-residential land in close proximity. The model in this image is to scale.

Architecture and Signage

- The general **palette of materials and intended architectural resolution** is well-resolved and appropriate and will result in a high-quality design outcome. The fundamental intention to pursue a 'heavyweight' long-brick finish for all proposed street walls (BRK01) will 'ground' the proposal to its local context and respond to the prevalence of masonry on existing adjoining IN3Z-zoned conditions. Above this, the pursuit of a juxtaposing 'lighterweight' glazed office tower finish is typical of office building typologies, and will reduce perceptions of scale where most visible in longer-range views whilst referencing the emerging pattern of materiality in other proximate recent office developments (ie. the 'Australia Post' building at 462-482 Swan Street, Richmond).
- We note the presence of existing established vegetation within the subject site's existing condition and consider the the extensive use of **proposed landscaping** throughout the proposal to be commendable,

including within the Ground Floor (widened public realm), atop podiums and within proposed rooftop amenities. This is particularly so within the broader context of the subject site's IN3Z zoning where high levels of landscaping are arguably uncharacteristic.

- All proposed internally-illuminated business identification **signs** (atop the northern and southern elevations) are architecturally-integrated with the host building and well-resolved, and will avoid creating visual clutter. This is consistent with the signage provisions Clause 15.-01-1L-01, and consider the absence of similarly-located signage within the eastern and western elevations to have benefits relative to potential residential or heritage sensitivities.
- The use of patterned pre-cast concrete panelling within the proposed **southern boundary wall** is acceptable and typical of boundary wall conditions, although could be further refined as discussed below. We also note the pursuit of an inset external metal staircase within this elevation, which will provide for a level of visible built form resolution until such time as future development occurs immediately south at 420 Burnley Street, Richmond.
- **Minor design amendments** would further improve the proposed architectural response, and we suggest the following:
 - Substitution of all CON01 concrete panelling within the western street wall elevation for a BRK01 brick finish instead. This is because it is highly unlikely that land further west of the subject site will redevelop further, with views toward this aspect of the proposal therefore remaining available in perpetuity. Doing so would better-respond to the tactile bluestone expression of 41 Madden Grove and potentially any heritage values associated with it.
 - Refinement of the southern boundary precast concrete patternation to better-integrate with the expression of the balance of propped facades, as it is currently expressed as a sporadic pattern of 'hit and miss' aggregate concrete patternation. This could be further refined by the introducing of a level of banded horizontality that aligns with the expressed 'GSL02' solid backed panelling of glazing to achieve an outcome that reads as more integrated 'in the round', or could pick up better on the verticality of proposed tower fins. We say this even in acknowledgement and anticipation of reduced boundary wall visibility over the longer term when 420 Burnley Street, Richmond redevelops in the future.
 - Contemplating the manner in which street wall materiality might better-respond to the nuances of each adjoining streetscape. This is because the consistent use of BRK01 grey brickwork for all aspects of the proposed street walls arguably fails to have the same level of regard to local streetscape conditions as other aspects of the proposal (ie. street wall height, upper level setbacks etc.). This may lend itself to a darker finish of brickwork to Madden Grove to further respond to the bluestone materiality of adjoining heritage fabric immediately west, and a 'warmer tone' of brickwork to Burnley Street that better-integrated with the prevailing pattern of red brickwork immediately south.

Recommendation 01 - Undertake minor materiality amendments.

Equitable Development

- The subject site's **western opposing condition** is not a future development site (41 Madden Grove, Richmond) due to the combination of site-specific heritage fabric and recent rear redevelopment. The proposal's western setback profile (including podium height, upper-level setbacks and western internal aspect) is appropriate and will not result in equitable development issues.
- **Land immediately south** of the subject site is similarly zoned IN3Z and included in DDO2 and benefits from many of the same physical attributes that support redevelopment of the subject site. They are future development sites, and the image overleaf outlines our view of how they are likely to develop.

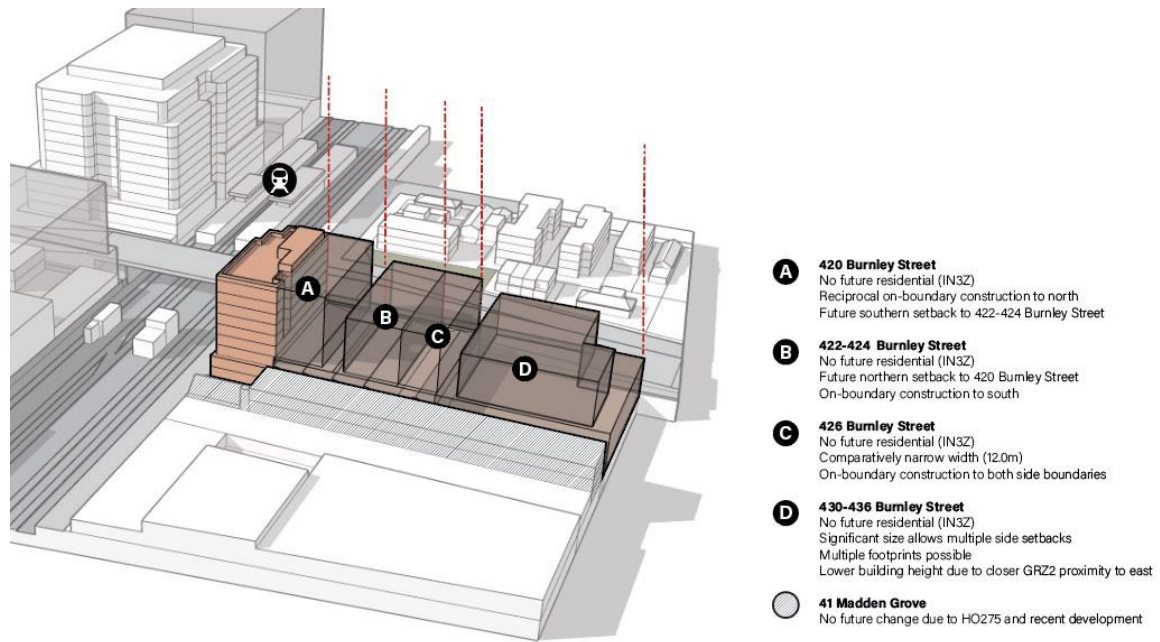


Figure 3 - Likely pattern of future development of IN3Z-zoned land surrounding the subject site.

- We support the proposed intention to rise sheer to the subject site's southern boundary with on-boundary construction, which will be reciprocated in the likely future redevelopment of 420 Burnley Street, Richmond immediately south. Future on-boundary construction adjacent to the proposal's inset open-air stairwell will simply result in an enclosed stairwell (similar to the vast majority of commercial office developments). If necessary, a permit condition can necessitate relocation of upper-level south-facing signage in the event that Council issues a future permit for similarly-scaled redevelopment of 420 Burnley Street.

Other

- We understand that Council is not seeking external urban design comment on matters of public realm amenity and safety. However, we note that:
 - Some aspects of the Ground Floor plane seek approval to create new places of concealment and entrapment, including the intended relocation of laneway gating for substation access (to the west of the subject site) and publicly-accessible Ground Plane insets to both streetscapes (particularly Madden Grove). After-hours safety is particularly important for the proposal because the suite of intended uses are not '24/7 uses' able to provide activation at all hours of the day (like residential uses).
 - More could be done to activate the public realm within the design of the proposed street walls, which have relatively narrow and confined patterns of fenestration (Levels 1 and 2) and could include more glazing at the Ground Plane (particularly for bicycle storage areas and for sightlines).

Recommendation 02 – Further refine the Ground Plane and proposed street walls to maximise public safety.

3.0 Summary

In summary, we support the proposal from an urban design perspective and believe it will generally make a

high-quality and contextually-responsive contribution to the subject site's local area.

We recommend the following:

- **Recommendation 01 - Undertake minor materiality amendments.**
- **Recommendation 02 - Further refine the Ground Plane and proposed street walls to maximise public safety.**

We trust the above assists and would welcome the opportunity to review further iterations of the proposal as the application progresses.

Please do not hesitate to contact Blades directly on 0431 326 507 or at info@blades.studio should you have any queries regarding the above.

Blades



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City of Yarra

16 July 2025

Ref: 30N-25-0246-TRP-110884-0

333 Bridge Road, Richmond VIC 3121

Dear Byrne, Joe,

418 Burnley St, Richmond

This peer review of MEL Consultants "Desktop Environmental Wind Assessment" (Report 25021A-DE-EWA01) is based on Vipac's experience as a wind engineering consultancy. No wind tunnel studies have been undertaken to support this review.

Vipac has reviewed the assessment as well as the drawings provided (see References attached) and has the following comments:

- i. The MEL Consultants Environmental Wind Assessment has been prepared based on drawings dated 11th March 2025 and consultancy experience. No wind tunnel testing has been carried out to support their assessment. We have no issues with this method for a desktop study as this is a common approach to provide architects, developers and responsible authorities advice on the wind impact of the proposed design.
- ii. We have no issues with the analysis approach or description of the wind environment. MEL Consultants have clearly described the process for a desktop assessment, and this is consistent with the approach that Vipac would take.
- iii. The report has used the assessment criteria from the City of Yarra Planning Scheme Clause 58.04-4 (Standard D17) Vipac has no issues with this
- iv. The report analysed the wind effects, and its findings can be summarised as follows:
 - a) Increased wind conditions along Burnley Street and Madden Grove due to flow induced to ground level by the development.
 - b) A minimum of Walking comfort criterion in the streetscapes of Burnley Street and Madden Grove, with areas away from the site having conditions similar to existing conditions and likely to achieve the Standing Comfort Criterion.
 - c) Standing comfort criterion at the main building entry and both tenancy entries.
 - d) The Safety criterion would be expected to be achieved at all ground level areas.
 - e) The Walking Comfort Criterion on the north-west Level 02 terrace corner with conditions achieving the Standing criterion away from corner; Standing comfort on the east Level 03 terrace; Standing/Sitting comfort on the rooftop communal terrace; and the Safety criterion at all terraces.
- v. Vipac agrees with the report's conclusion that the proposed design is not expected to generate wind levels in excess of the recommended wind comfort criteria at the ground level pedestrian areas and various terraces and agrees that no modifications to the proposed development are required for the public realm.



City of Yarra
418 Burnley St, Richmond
Review of a wind assessment report

In conclusion, the MEL Consultants Environmental Wind Assessment report uses the proper analysis and methodology to analyse the wind effects on the pedestrian level surrounding the proposed development. The report found that the proposed design would be expected to generate winds within the recommended wind comfort criterion. Vipac generally agrees with the assessment conclusions.

Vipac has reviewed the updated design dated 22 April 2025 and can confirm that the assessment conclusions are still valid for the updated design.

Vipac makes no further comments or recommendations.

Yours sincerely,

Vipac Engineers & Scientists Ltd

A handwritten signature in blue ink, appearing to read "Zu Shuyun".

Zhuyun Xu
Principal Engineer

Peyman Mayeli
Senior Engineer

16/07/2025

30N-25-0246-TRP-110884-0

Commercial-In-Confidence

Page 2 of 3



City of Yarra
418 Burnley St, Richmond
Review of a wind assessment report

References:

1. Studio Mccue, Town Planning, 25002 418 Burnley Street, *Rev A*, 22nd April 2025
2. *J. Kostas*, MEL Consultants, 418 BURNLEY STREET, RICHMOND, Desktop Environmental Wind Assessment (Report 25021A-DE-EWA01) 7 March 2025

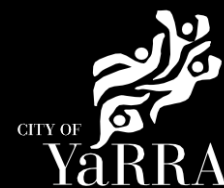
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Commercial-In-Confidence

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Strategic Transport Formal Referral Response



Application Information:

Referral Officer:	Philip Mallis
Officer:	Joe Byrne
Council Reference:	PLN25/0181
Address:	418 Burnley St, Richmond VIC 3121
Proposal:	Construction of a ten storey mixed use building (plus two basement levels), use of the land for Office and Food and Drink Premises, construct and display Business Identification Signage, reduction in the car parking requirements, and create and alter access to a road in Transport Zone 2.
Comments Sought:	<p>Comments on the application from a Strategic Transport perspective are requested on the following:</p> <ul style="list-style-type: none"> • Access and Safety • Bicycle Parking Provision • Adequacy of car parking / bicycle parking spaces provided • Design and location of car parking / bicycle parking spaces and facilities • Green Travel Plan
Disclaimer:	Council's Strategic Transport unit provides the following information which is based on the information provided in the referral request memo referenced above.
Prev. Responses:	No - First Referral

Comments

Access and Safety

No issues identified.

Bicycle Parking Provision

Statutory Requirement

Under the provisions of Clause 52.34-3 of the Yarra Planning Scheme, the development's bicycle parking requirements are as follows:

Proposed Use	Quantity/ Size	Statutory Parking Rate	No. of Spaces Required	No. of Spaces Allocated
Office (other than specified in the table)	5,454 sqm	1 employee space to each 300 sqm of net floor area if the net floor area exceeds 1000 sqm	18 employee spaces	28 employee spaces
		1 visitor space to each 1000 sqm of net floor area if the net floor area exceeds 1000 sqm	5 visitor spaces	8 visitor spaces
Retail premises (other than specified in the table)	140 sqm	1 employee space to each 300 sqm of leasable floor area	0 employee spaces	0 employee spaces
		1 visitor space to each 500 sqm of leasable floor area	0 visitor spaces	2 visitor spaces
Bicycle Parking Spaces Total			18 employee spaces	28 employee spaces
			5 visitor spaces	10 visitor spaces
Showers / Change rooms		1 to the first 5 employee spaces and 1 to each additional 10 employee spaces	2 showers / change rooms	4 showers / change rooms

The development provides a total of 10 additional employee spaces and 5 additional visitor spaces above the requirements of the Scheme.

[Adequacy of visitor spaces](#)

Number of Spaces

The provision of 10 visitor bicycle spaces is adequate for the following reasons:

- The proposed development requires 5 visitor bicycle parking spaces, as per the City of Yarra Planning Scheme requirements. The provision of 10 visitor spaces exceeds this requirement by 5 spaces. **(adequate)**
- Supply meets CASBE requirements, which require 1 visitor bicycle space per 500 sqm of net leasable floor area for an office development. The given development has a net leasable office floor area of 4797 sqm, requiring a total of 10 visitor spaces. The provision of 10 visitor spaces therefore meets this requirement. **(adequate)**

Design and Location of Visitor Spaces

The design and location of the visitor bicycle parking spaces is inadequate for the following reasons:

- Pursuant to Clause 52.34-5, all visitor spaces are to be provided at a bicycle rail. The visitor parking facilities are provided as horizontal facilities aligning with this requirement. **(adequate)**

- Architectural plans indicate that 2 of the total 10 visitor parking spaces are provided in a public area, with adequate passive surveillance and activity, providing a safe and accessible bicycle parking space. However, the remaining 8 spaces do not currently meet these standards and must be reviewed in accordance with the above. **(inadequate)**

Adequacy of employee spaces

Number of spaces

The provision of 28 employee spaces is inadequate for the following reasons:

- The proposed development requires 18 employee bicycle parking spaces, as per the City of Yarra Planning Scheme requirements. The provision of 28 employee spaces exceeds this requirement by 10 spaces. **(adequate)**
- In accordance with CASBE requirements, the development must provide 1 employee bicycle space per 100 sqm of net leasable area of office. The given development has a net leasable office area of 4797 sqm, requiring a total of 48 employee spaces. The provision of 28 employee spaces therefore does not meet this requirement. **(inadequate)**

Design and location of employee spaces and facilities

Employee spaces are inadequately located and designed for the following reasons:

- The width of the access ramp to the employee bicycle parking storage area is not identified on Architectural plans. Pursuant to AS2890.3 the minimum width of an access path is 1,500mm. **(inadequate)**
 - Scaled measurement of the ramp suggests it is of suitable width, but this must be shown on architectural plans.
- Pursuant to AS2890.3, more than 20% of the employee bicycle parking must be floor level, horizontal spaces. In the given development, 12 two-tiered spaces (total 24 spaces) and 2 bicycle rails (total 4 spaces) are provided. Given that the lower tier of every two-tiered space is at ground level, this results in a total provision of 16 ground-level horizontal spaces, equating to 67% of the total bicycle parking provision. This exceeds minimum requirements and is therefore compliant. **(adequate)**
- Dimensions identifying the spacing of bicycle parking spaces are not provided on the architectural plans. Plans must indicate all relevant dimensions, in accordance with AS2890.3 and Clause 52.34 of the Planning Scheme. **(inadequate)**
- Pursuant to Clause 52.34-3 & AS2890.3, bicycle spaces and related facilities for employees must be provided in a bicycle locker, or a lockable compound. While the architectural plans indicate secure access via a key FOB locking mechanism for the visitor parking area, the plans do not indicate locking mechanisms for the staff bicycle storage room or end-of-trip facilities, therefore not satisfying minimum requirements. **(inadequate)**
- Adequate end-of-trip facilities are provided at ground level, adjacent to and directly accessible from the bicycle storage room. **(adequate)**

Green Travel Plan

The application includes a Green Travel Plan (GTP). The GTP provides all the required information and can be endorsed.

Recommendations

The following should be shown on the plans before endorsement:

1. Provide all visitor bicycle parking in a publicly accessible area at ground level.
2. Provide a minimum of 48 employee parking spaces in line with CASBE requirements.
3. Identify on the architectural plans the width of all access paths to bicycle parking areas in accordance with AS2890.3 requirements.
4. Identify spacing between bicycle parking spaces in accordance with AS2890.3 requirements.
5. The bicycle storage room must be within a secure lockable compound to provide secure access for employees. Identify on the architectural plans the security arrangements to be implemented.

Sustainable Transport Planner (Strategic Transport Unit): Philip Mallis

Date: 02/07/25



C&R Ryder Consulting P/L
12/8 Sigma Drive
Croydon South, VIC, 3136
ABN: 47 376 684 521
E: cameron@cryder.com.au
P: 0434 351 567

Planning Referral Response

Date: Wednesday, 9 July 2025 Reference No: **PLN25/0181**

Address: 418 Burnley St, Richmond VIC 3121 Site visit completed: No

Nature of Referral: Construction of a ten storey mixed use building

Overlays

None affecting trees

Scope

This referral has reviewed the supplied arborist report and plans in relation to the proposed works as detailed above. There are no overlays affecting the removal, destruction or lopping of trees and all trees within private property are smaller than the City of Yarra Local Law threshold for a permit.

Documents Reviewed

- Arboricultural Report, 418 Burnley St, Richmond, prepared by M. Rogers, John Patrick Landscape Architects Pty. Ltd., dated 13 February 2025
- Architectural Drawings, prepared by Studio McCue, project: 25002 418 Burnley Street, date 22 April 2025.

Findings

I have reviewed the Arboricultural Report and largely agree with its findings and contents.

1. Tree 3 can be removed, there are no permit triggers.
2. Protection measures as per the Arboricultural Report should be implemented.

Recommendations

It is recommended that:

1. Trees 1 & 2 are retained.
2. An asset protection bond is implemented of \$500.00 per tree for Trees 1 & 2
3. Trees 1 & 2 are retained and protected in line with the Tree Protection Measures (Section 6) as detailed in the Arboricultural Report (John Patrick Landscape Architects Pty. Ltd., dated 13 February 2025) with the following modifications:
 - 3.1 Hoarding is to be a maximum of 1.5m in height
 - 3.2 The trees must be inspected at least every 6 weeks by a project arborist.
 - 3.3 No rubbish is allowed to collect within the hoarding box that surrounds the tree.

Ref: PLN25-0181 - 418 Burnley St, Richmond.docx

-
4. Approval is given for the removal of Tree 3.
 5. Before works start (including demolition and excavation), notice in writing must be provided to the responsible authority from a Project Arborist confirming their engagement to oversee all relevant tree protection works. The Project Arborist must be an appropriately experienced and qualified professional (minimum Diploma of Arboriculture or equivalent in experience).

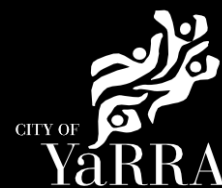
If you have any further queries, please don't hesitate to contact me on 0434 351 567.

Regards



Cameron Ryder
C&R Ryder Consulting Pty Ltd

Strategic Planning Formal Referral Response



Application Information:

Referral Officer: Kyle Everett

Officer: Joe Byrne

Council Reference: PLN25/0181

Address: 418 Burnley St, Richmond VIC 3121

Proposal: Construction of a ten storey mixed use building (plus two basement levels), use of the land for Office and Food and Drink Premises, construct and display Business Identification Signage, reduction in the car parking requirements, and create and alter access to a road in Transport Zone 2.

Comments Sought: Click here to view referral memo:
[IREF25/00775 - Referral Request](#)

Disclaimer: Council's Strategic Planning Unit provides the following information which is based on the information provided in the Statutory Planning referral request memo referenced above.

The Strategic Planning comments in this assessment focus on how the application would compare with:

- proposed Planning Controls recommended in the Cremorne Urban Design Framework (UDF); and
- the role/function of the Burnley South Industrial Precinct outlined in the Strategic Economic and Employment Strategy (SEES).

They do not provide commentary on other sections of the Planning Scheme or fully assess the internal amenity of the application.

Prev. Responses:

Strategic Planning full comments

Land use intensity and building height

Heights proposed in Cremorne are typically:

- 28m for most areas within Cremorne
- 32m along Cremorne Street
- 36m along the northern end of Stephenson Street, reflecting existing development.

The development proposes a height of 35.4m which is at the taller end of spectrum for building heights.

The Burnley South Industrial Precinct is largely centred around the Citi Power depot, City of Yarra Depot and Fire Rescue/Emergency Services offices.

The Yarra Spatial Economic and Employment Strategy (SEES) outlines a strategy to retain existing industrial precincts for manufacturing and urban services.

The SEES notes that the direction of these precincts will be underpinned by major land uses. In the case of Burnley South, the Citi Power Depot.

Citi Power in their "Regulatory proposal 2026-31" have indicated they plan significant investment in the depot for both training and industrial purposes.

The scale of the development may be greater than actual demand for offices in this location.

Street wall setback

The revised Cremorne UDF typically requires buildings to be set back at ground level as a minimum. This is to improve the public realm of streets that were previously designed for a commercial/industrial business. The proposal of the corner forecourt is supported.

Considering that the design is proposing an entry to tenancy 2 from Madden Grove, a continuation of the ground floor setback would be ideal (See Figures 1 and 2).

Otherwise, the proposed corner setback would be a welcome addition to the development.

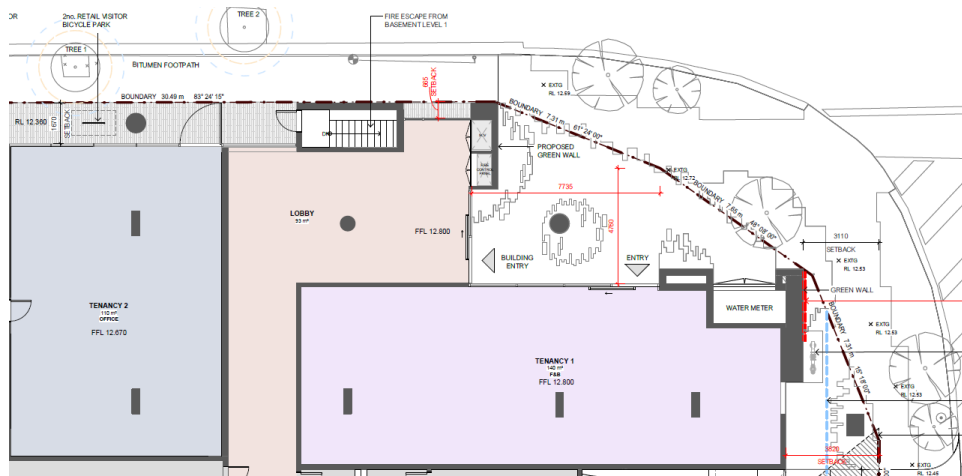


Figure 1 Advertised ground floor plans

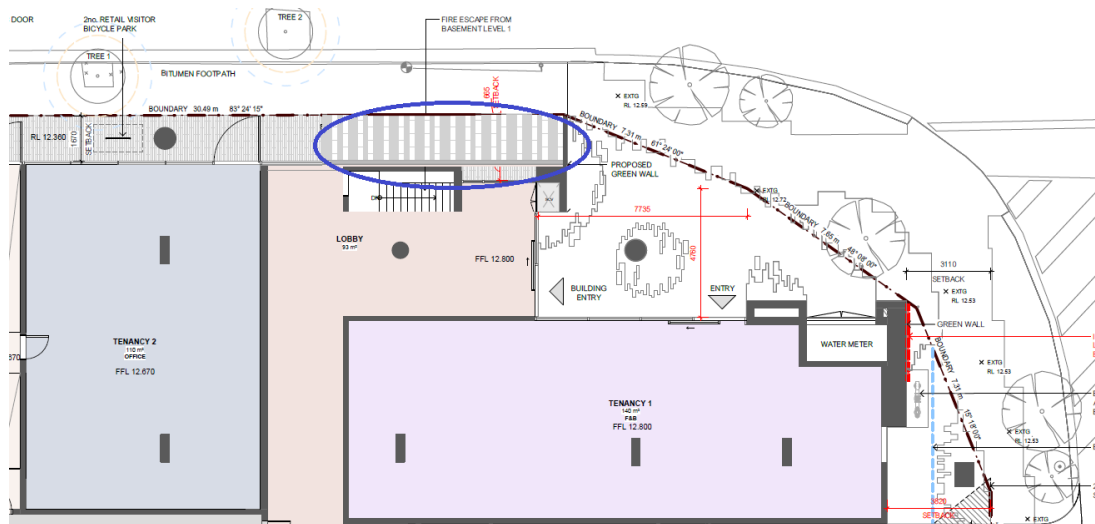


Figure 2 Ground floor plans - blue outline showing extension of ground setback

Street wall height Requirements

The Cremorne UDF generally proposes a 12m street wall height with key streets with greater widths (i.e. Church Street) a 16m street wall height.

The proposed street wall height is 13.5m is slightly taller than most areas within Cremorne however, this variation is minimal.

Upper-level setback Requirements

The Cremorne UDF generally proposes an upper-level setback of 3m.

The upper-level setbacks are minimal to Madden Grove and Burnley Street. The application proposes approximately 1.5m and 1m respectively. The design proposes a zigzag setback on the corner.

The upper-level setbacks to the corner provide an adequate design response and articulates the upper-levels of the building.

The other upper-level setbacks are inadequate for the height and scale of the development and do not provide adequate distinction between the street wall and upper levels.

Greater Upper-level setbacks should be required, possibly 3m.

Building separation

The Cremorne UDF proposes 3m building separation above 20m on all common boundaries.

The application proposes a sheer wall to its southern boundary. This outcome is not desirable considering the height and the potential for the wall to be visible for some quite time.

Figure 3 shows the building outline on the east elevation if the application were to be designed using the building separation provisions within the Cremorne UDF.

A full building setback may not be required if the core of the building needed to remain in proposed location. An example where either side of the building is setback is shown below in Figures 4 and 5.

The level of growth in office developments is not expected for this precinct and a more resolved design treatment is needed.

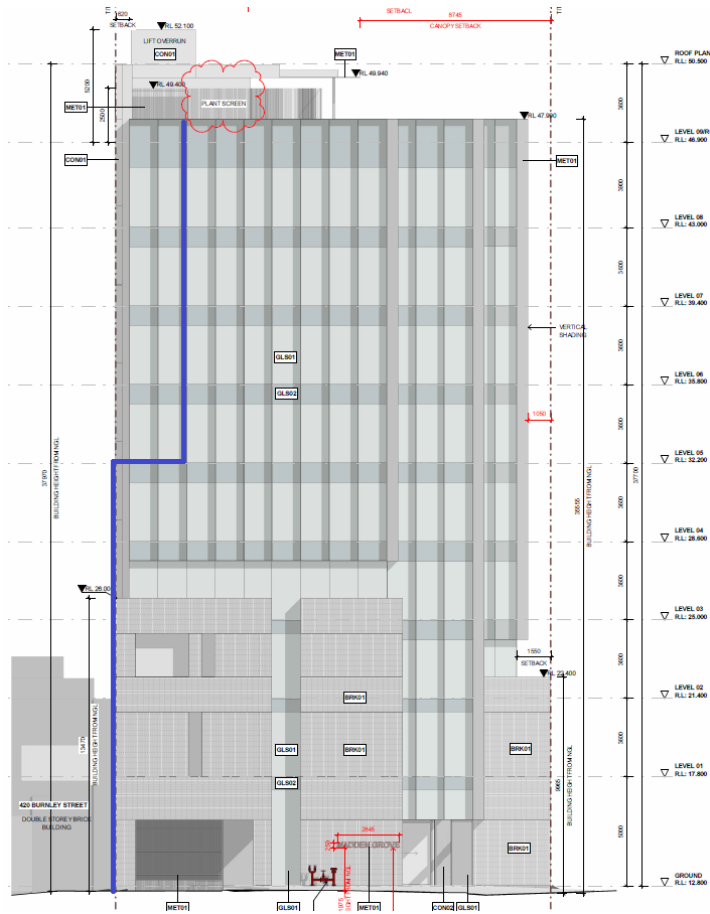


Figure 3 Cremorne building separation (blue) provisions against eastern elevation

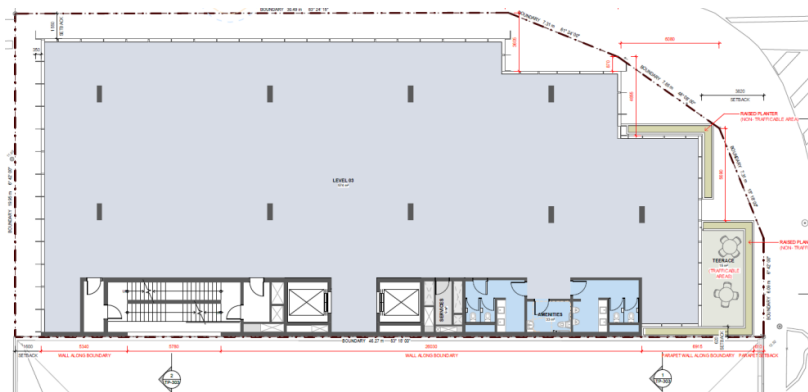


Figure 4 Advertised plans without building separation

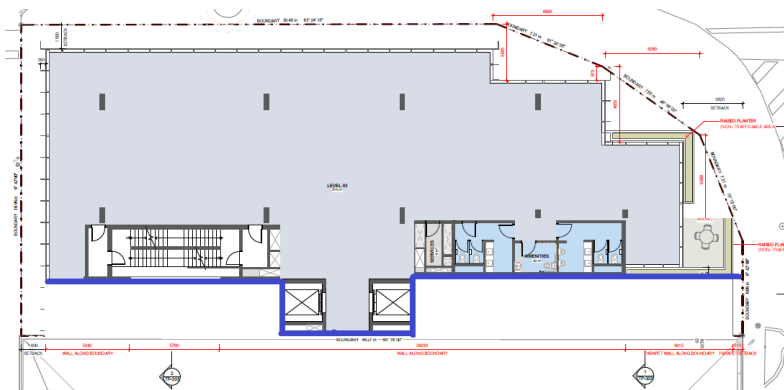


Figure 5 Potential building separation without moving building core (blue)

Summary and conclusion of proposal

The role of the Burnley South Industrial Precinct is to provide for manufacturing and urban services to the City of Yarra. There is a strong indication that key businesses will remain in the area for light industrial purposes. There are questions around the appropriateness of an office development and real demand for space in this location.

Considering this, the scale and mass of the proposal requires some further reconsideration when compared to the planning controls proposed for the Cremorne Major Employment Precinct. This includes:

- Overall height is at the taller end of what is being proposed in Cremorne.
- Upper-level setbacks are not enough to provide distinction between upper-levels and the street.
- Building design and separation to the south needs reconsidered.
- Minor adjustments to the ground level setbacks should be made.

The overall height should be reduced considering the size of the land the site is located on, the role of the precinct and that the application has not provided design excellence to offset the scale. Two levels would reduce the building to a total height of 27.8m which is much more aligned with the general height proposed in Cremorne.

Upper levels should be setback to provide distinction between upper-levels and the street.

The application proposes a blank wall on its southern boundary. As mentioned above, its unlikely that we will see a development in the near future. The application has not considered its surrounds and how it will develop. Building separation similar to that proposed in Cremorne could be used as a guide.

Recommendations

It is recommended that if the application were to proceed that it provides the following:

1. Reduce the height by two storeys;
2. Provide 3m upper-level setbacks to Madden Grove and Burnley Street.
3. Provide a 3m building separation for upper levels above 20m on the southern boundary.
4. Extend the ground floor setback to connect to the second tenancy.

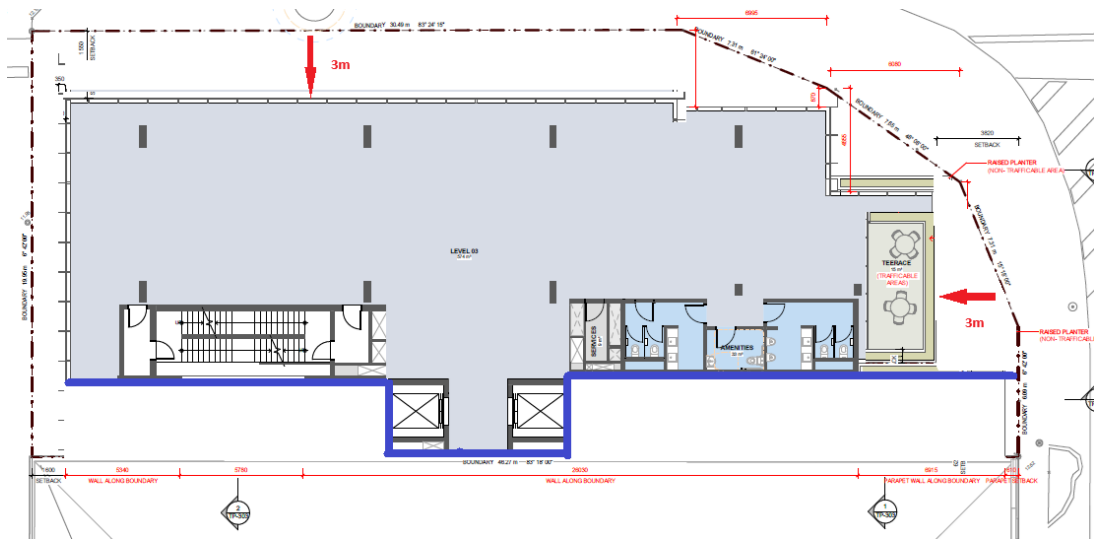


Figure 6 Aerial view of recommended 3m upper-level setback and building separation.

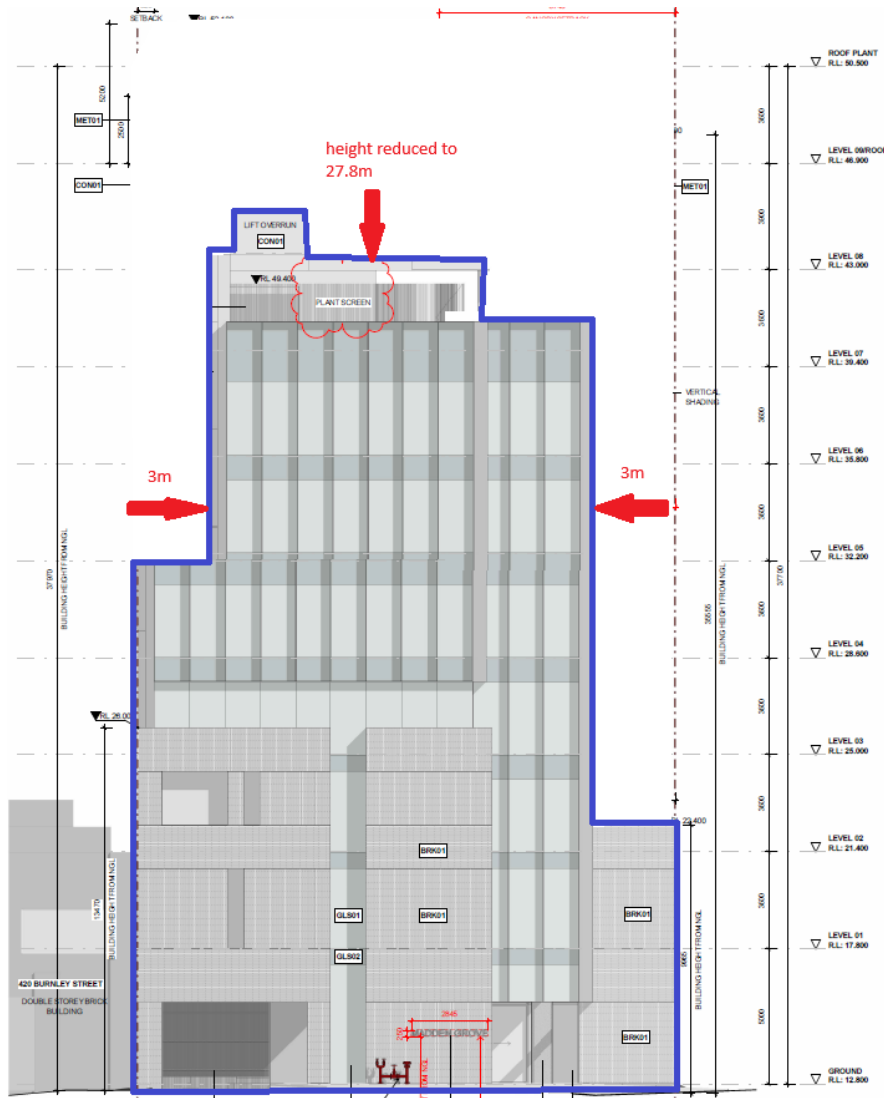
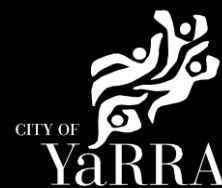


Figure 7 East elevation view of recommended 3m building separation, reduction in height of two storeys and 3m upper-level setback

Strategic Planner: Kyle Everett
 Date: 09 July 2025

ESD Formal Referral Response



Application Information:

Referral Officer:	Euan Williamson
Officer:	Joe Byrne
Council Reference:	PLN25/0181
Address:	418 Burnley St, Richmond VIC 3121
Proposal:	Construction of a ten storey mixed use building (plus two basement levels), use of the land for Office and Food and Drink Premises, construct and display Business Identification Signage, reduction in the car parking requirements, and create and alter access to a road in Transport Zone 2.
Comments Sought:	Click here to view referral memo: Record D25/223552: IREF25/00774 - Internal Referral Request
Disclaimer:	Council's ESD Officer provides the following information which is based on the information provided in the referral request memo referenced above.
Prev. Responses:	

ESD comments were requested on the following:

- New referral

In assessing this application, the following documents were reviewed:

- SMP prepared by GIW dated 11 March 2025
- Architectural drawings prepared by Studio McCue dated 22 April 2025

Comments

The standard of the submitted ESD largely meets Council's Environmentally Sustainable Design (ESD) standards.

Furthermore, it is recommended that all ESD commitments (1), deficiencies (2) and the outstanding information (3) are addressed in an updated SMP report and are clearly shown on Condition 1 drawings. ESD improvement opportunities (4) have been summarised as a recommendation to the applicant.

Should a permit be issued, the following ESD commitments (1) and deficiencies (2) should be conditioned as part of a planning permit to ensure Council's ESD standards are fully met:

1. Applicant ESD Commitments

- Good daylight levels throughout the office areas.
- Fresh air supply to exceed AS1668 by at least 50%.
- Low VOC carpets, paints, sealants and adhesives.
- Low formaldehyde timber products.
- Thermal performance to exceed NCC2019 minimum.

- Energy efficient heating, cooling, hot water and lighting.
- A combination of vertical shading fins and horizontal fixed awnings.
- A minimum 17.7kW solar PV array to the rooftop.
- Water efficient taps, fixtures and fittings.
- 103% STORM score relying on a minimum of 518m² of roof connected to a 16,000 litre tank for toilet flushing on Ground and Level 1.
- 40% of coarse aggregate for concrete from recycled sources.
- All timbers to be certified sustainable by FSC or PEFC.
- Formwork, pipes, flooring, blinds and cables to meet Green Building Council Aust PVC guidelines.
- 28 bicycle parking spaces for staff, plus an additional 10 spaces for visitors.
- End of trip facilities include 4 showers and 28 lockers for staff.
- One electric vehicle charging point (minimum) and electric vehicle readiness to all levels of carpark, including a minimum 10% of car parking spaces.
- 80% of construction and demolition waste recycled/reused and diverted from landfill.
- Waste management streams for paper, recycling, FOGO, landfill and hardwaste.
- Additional vegetated landscaping to roof, terraces and green wall to improve the ecological value of the site.
- Vegetation and pale materials to reduce urban heat impact.
- ESD checkpoints during construction phase.
- Sensors to assist air quality.

2. Application ESD Deficiencies

- ESD display and metering claimed as an Innovation in BESS but this is not considered innovative. Please remove Innovation credit claim from the BESS report.
- App to link building users and facility management claimed as an Innovation in BESS but this is not considered innovative. Please remove Innovation credit claim from the BESS report.
- Digital noticeboard with various information displayed claimed as an Innovation in BESS but this is not considered innovative. Please remove Innovation credit claim from the BESS report.

3. Outstanding Information

- The Innovation section of the BESS report includes a commitment to 10 year Green Power purchasing, but this is not included in the body of the SMP. Clarify the commitment to purchasing 100% Green Power for a minimum 10 year period throughout the energy section of the SMP report.
- A green wall adjacent to the ground floor entrance is proposed. Please note the green wall on the elevations (currently labelled as long grey brick).

4. ESD Improvement Opportunities

- Consider increasing vegetation landscaping.
- Consider battery energy storage system.
- Consider heat recovery ventilation system.
- Consider additional solar PV to rooftop.
- Consider building integrated solar PV to glazing of façade.
- Consider comprehensive commissioning and tuning to all major building services
- Recommend lead construction contractor to have valid ISO14001 or similar EMS.

- Recommend comprehensive CMP to developed and implemented.

If you or the applicant would like to discuss this further, please contact me.

ESD Advisor: Euan Williamson
Date: 03 July 2025

Sustainable Management Plan (SMP)

for planning applications being considered by Yarra Council



ESD in the Planning Permit Application Process

Yarra City Council's planning permit application process includes Environmentally Sustainable Development (ESD) considerations. This is now supported by the ESD Local Policy Clause 22.17 of the Yarra Planning Scheme, entitled *Environmentally Sustainable Development*.

The Clause 22.17 requires all eligible applications to demonstrate best practice in ESD, supported by the Built Environment Sustainability Scorecard (BESS) web-based application tool, which is based on the Sustainable Design Assessment in the Planning Process (SDAPP) program.

As detailed in Clause 22.17, this application is a 'large' planning application as it meets the category *Non-residential 1. 1,000m² or greater*.

What is a Sustainable Management Plan (SMP)?

An SMP is a detailed sustainability assessment of a proposed design at the planning stage. An SMP demonstrates best practice in the 10 Key Sustainable Building Categories and;

- Provides a detailed assessment of the development. It may use relevant tools such as BESS and STORM or an alternative assessment approach to the satisfaction of the responsible authority; and
- Identifies achievable environmental performance outcomes having regard to the objectives of Clause 22.17 (as appropriate); and
- Demonstrates that the building has the design potential to achieve the relevant environmental performance outcomes, having regard to the site's opportunities and constraints; and
- Documents the means by which the performance outcomes can be achieved.

An SMP identifies beneficial, easy to implement, best practice initiatives. The nature of larger developments provides the opportunity for increased environmental benefits and the opportunity for major resource savings. Hence, greater rigour in investigation is justified. It may be necessary to engage a sustainability consultant to prepare an SMP.

Assessment Process:

The applicant's town planning drawings provide the basis for Council's ESD assessment. Through the provided drawings and the SMP, Council requires the applicant to demonstrate best practice.

1. Indoor Environment Quality (IEQ)

Objectives:

- to achieve a healthy indoor environment quality for the wellbeing of building occupants
- to provide a naturally comfortable indoor environment will lower the need for building services, such as artificial lighting, mechanical ventilation and cooling and heating devices

Issues	Applicant's Design Responses	Council Comments	CAR*
Natural Ventilation and Night Purging	Mechanical fresh air supply rates 50% above the AS1668:2012 rates.	Satisfactory	1
Daylight & Solar Access	Good daylight to office areas.	Satisfactory	1
External Views	External views provided.	Satisfactory	1
Hazardous Materials and VOC	Low VOC carpets, paints, adhesives and sealants. Low formaldehyde engineered timber also.	Satisfactory	1
Thermal Comfort	Good thermal comfort levels.	Satisfactory.	1

* Council Assessment Ratings:

- 1 – Design Response is **SATISFACTORY**
- 2 – Design Response is **NOT SATISFACTORY**
- 3 – **MORE INFORMATION** is required
- 4 – **ESD IMPROVEMENT OPPORTUNITIES**

References and useful information:

- SDAPP Fact Sheet: [1. Indoor Environment Quality](#)
- Good Environmental Choice Australia Standards www.geca.org.au
- Australian Green Procurement www.greenprocurement.org
- Residential Flat Design Code www.planning.nsw.gov.au
- Your Home www.yourhome.gov.au

2. Energy Efficiency

Objectives:

- to ensure the efficient use of energy
- to reduce total operating greenhouse emissions
- to reduce energy peak demand
- to minimize associated energy costs

Issues	Applicant's Design Responses	Council Comments	CAR*
NCC Energy Efficiency Requirements	NCC energy efficiency requirements have been exceeded.	Satisfactory	1
Thermal Performance	NCC thermal performance requirements have been exceeded.	Satisfactory.	1
Greenhouse Gas Emissions	The Innovation section of the BESS report includes a commitment to 10 year Green Power purchasing, but this is not included in the body of the SMP	Clarify the commitment to purchasing 100% Green Power for a minimum 10 year period throughout the energy section of the SMP report.	3
Hot Water System	Efficient electric heat pump	Satisfactory	1
Peak Energy Demand	Various initiatives will reduce peak demand.	Satisfactory	1
Effective Shading	A combination of vertical shading fins and horizontal fixed awnings.	Satisfactory	1
Efficient HVAC system	VRV/VFV energy efficient HVAC with a COP of at least 3.4	Satisfactory	1
Car Park Ventilation	CO sensor controlled VSD on mechanical ventilation system to carpark.	Satisfactory	1
Efficient Lighting	Energy efficiency lighting no great than 4.5W/m ² for office and 14W/m ² for retail.	Satisfactory	1
Electricity Generation	A minimum 17.7kW solar PV array to the rooftop.	Satisfactory	1
Other	-	-	-

* Council Assessment Ratings:

- 1 – Design Response is **SATISFACTORY**
- 2 – Design Response is **NOT SATISFACTORY**

- 3 – MORE INFORMATION is required
- 4 – ESD IMPROVEMENT OPPORTUNITIES

References and useful information:

- SDAPP Fact Sheet: [2. Energy Efficiency](#)
- House Energy Rating www.makeyourhomegreen.vic.gov.au
- Building Code Australia www.abcb.gov.au
- Window Efficiency Rating Scheme (WERS) www.wers.net
- Minimum Energy Performance Standards (MEPS) www.energyrating.gov.au
- Energy Efficiency www.resourcesmart.vic.gov.au

3. Water Efficiency

Objectives:

- to ensure the efficient use of water
- to reduce total operating potable water use
- to encourage the collection and reuse of rainwater and stormwater
- to encourage the appropriate use of alternative water sources (e.g. grey water)
- to minimise associated water costs

Issues	Applicant’s Design Responses	Council Comments	CAR*
Minimising Amenity Water Demand	WELS rated water efficient taps, fixtures and fittings as follows: <ul style="list-style-type: none"> - 4 Star toilets - 4 Star showerheads - 5 Star taps - 5 Star dishwashers 	Satisfactory	1
Water for Toilet Flushing	16,000 litre rainwater tank connected to toilet flushing on Ground and Level 1.	Satisfactory	1
Water Meter	Water meters to each tenancy	Satisfactory	1
Landscape Irrigation	Rainwater to supply irrigation needs	Satisfactory	1
Other	-	-	-

*** Council Assessment Ratings:**

- 1 – Design Response is **SATISFACTORY**
- 2 – Design Response is **NOT SATISFACTORY**
- 3 – **MORE INFORMATION** is required
- 4 – **ESD IMPROVEMENT OPPORTUNITIES**

References and useful information:

- SDAPP Fact Sheet: [3. Water Efficiency](#)
- Water Efficient Labelling Scheme (WELS) www.waterrating.gov.au
- Water Services Association of Australia www.wsaa.asn.au
- Water Tank Requirement www.makeyourhomegreen.vic.gov.au
- Melbourne Water STORM calculator www.storm.melbournewater.com.au
- Sustainable Landscaping www.ourwater.vic.gov.au

4. Stormwater Management

Objectives:

- to reduce the impact of stormwater runoff
- to improve the water quality of stormwater runoff
- to achieve best practice stormwater quality outcomes
- to incorporate Water Sensitive Urban Design principles

Issues	Applicant’s Design Responses	Council Comments	CAR*
STORM Rating	103% STORM score relying on a minimum of 518m ² of roof connected to a 16,000 litre tank for toilet flushing on Ground and Level1.	Satisfactory	1
Discharge to sewer	-	-	-
Stormwater Diversion	-	-	-
Stormwater Detention	-	-	-
Stormwater Treatment	-	-	-
Others	-	-	-

* Council Assessment Ratings:

- 1 – Design Response is **SATISFACTORY**
- 2 – Design Response is **NOT SATISFACTORY**
- 3 – **MORE INFORMATION** is required
- 4 – **ESD IMPROVEMENT OPPORTUNITIES**

References and useful information:

- SDAPP Fact Sheet: [4. Stormwater Management](#)
- Melbourne Water STORM calculator www.storm.melbournewater.com.au
- Water Sensitive Urban Design Principles www.melbournewater.com.au
- Environmental Protection Authority Victoria www.epa.vic.gov.au
- Water Services Association of Australia www.wsaa.asn.au
- Sustainable Landscaping www.ourwater.vic.gov.au

5. Building Materials

Objectives:

- to minimise the environmental impact of materials used by encouraging the use of materials with a favourable lifecycle assessment

Issues	Applicant's Design Responses	Council Comments	CAR*
Reuse of Recycled Materials	No specific information provided.	Satisfactory	1
Embodied Energy of Concrete and Steel	40% of coarse aggregate for concrete from recycled sources.	Satisfactory	1
Sustainable Timber	All timbers to be certified sustainable by FSC or PEFC	Satisfactory	1
Design for Disassembly	No specific information provided.	Satisfactory	1
PVC	Formwork, pipes, flooring, blinds and cables to meet Green Building Council Aust PVC guidelines.	Satisfactory	1

*Council Assessment Ratings:

- 1 – Design Response is **SATISFACTORY**
- 2 – Design Response is **NOT SATISFACTORY**
- 3 – **MORE INFORMATION** is required
- 4 – **ESD IMPROVEMENT OPPORTUNITIES**

References and useful information:

- SDAPP Fact Sheet: [5. Building Materials](#)
- Building Materials, Technical Manuals www.yourhome.gov.au
- Embodied Energy Technical Manual www.yourhome.gov.au
- Good Environmental Choice Australia Standards www.geca.org.au
- Forest Stewardship Council Certification Scheme www.fsc.org
- Australian Green Procurement www.greenprocurement.org

6. Transport

Objectives:

- to minimise car dependency
- to ensure that the built environment is designed to promote the use of public transport, walking and cycling

Issues	Applicant’s Design Responses	Council Comments	CAR*
Minimising the Provision of Car Parks	Reduced car parking rates	Satisfactory	1
Bike Parking Spaces	28 bicycle parking spaces for staff, plus an additional 10 spaces for visitors.	Satisfactory	1
End of Trip Facilities	4 showers and 28 lockers for staff	Satisfactory	1
Car Share Facilities	None	-	-
Electric vehicle charging	One electric vehicle charging point and electric vehicle readiness to all levels of carpark, including a minimum 10% of car parking spaces.	Satisfactory	
Green Travel Plan	-	-	-

*Council Assessment Ratings:

- 1 – Design Response is **SATISFACTORY**
- 2 – Design Response is **NOT SATISFACTORY**
- 3 – **MORE INFORMATION** is required
- 4 – **ESD IMPROVEMENT OPPORTUNITIES**

References and useful information:

- SDAPP Fact Sheet: [6. Transport](#)
- Off-setting Car Emissions Options www.greenfleet.com.au
- Sustainable Transport www.transport.vic.gov.au/doi/internet/icy.nsf
- Car share options www.yarracity.vic.gov.au/Parking-roads-and-transport/Transport-Services/Carsharing/
- Bicycle Victoria www.bv.com.au

7. Waste Management

Objectives:

- to ensure waste avoidance, reuse and recycling during the design, construction and operation stages of development
- to ensure long term reusability of building materials.
- to meet Councils' requirement that all multi-unit developments must provide a Waste Management Plan in accordance with the *Guide to Best Practice for Waste Management in Multi-unit Developments 2010*, published by Sustainability Victoria

Issues	Applicant's Design Responses	Council Comments	CAR*
Construction Waste Management	80% of construction and demolition waste recycled/reused and diverted from landfill.	Satisfactory	1
Operational Waste Management	Waste management streams for paper, recycling, FOGO, landfill and hardwaste.	Satisfactory	1
Storage Spaces for Recycling and Green Waste	Space for these waste streams can be identified in the basement level waste room.	Satisfactory	1
Others	-	-	-

*Council Assessment Ratings:

- 1 – Design Response is **SATISFACTORY**
- 2 – Design Response is **NOT SATISFACTORY**
- 3 – **MORE INFORMATION** is required
- 4 – **ESD IMPROVEMENT OPPORTUNITIES**

References and useful information:

- SDAPP Fact Sheet: [7. Waste Management](#)
- Construction and Waste Management www.sustainability.vic.gov.au
- Preparing a WMP www.epa.vic.gov.au
- Waste and Recycling www.resourcesmart.vic.gov.au
- Better Practice Guide for Waste Management in Multi-Unit Dwellings (2002) www.environment.nsw.gov.au
- Waste reduction in office buildings (2002) www.environment.nsw.gov.au

8. Urban Ecology

Objectives:

- to protect and enhance biodiversity
- to provide sustainable landscaping
- to protect and manage all remnant indigenous plant communities
- to encourage the planting of indigenous vegetation

Issues	Applicant's Design Responses	Council Comments	CAR*
On Site Topsoil Retention	There is no topsoil currently onsite.	Satisfactory	1
Maintaining / Enhancing Ecological Value	Additional vegetated landscaping to roof, terraces and green wall to improve the ecological value of the site.	Satisfactory	1
Heat Island Effect	Vegetation and pale materials to reduce urban heat impact.	Satisfactory	1
Green wall, roofs, facades	A green wall adjacent to the ground floor entrance is proposed.	Please note the green wall on the elevations (currently labelled as long grey brick).	3
Other	-	-	-

*Council Assessment Ratings:

- 1 – Design Response is **SATISFACTORY**
- 2 – Design Response is **NOT SATISFACTORY**
- 3 – **MORE INFORMATION** is required
- 4 – **ESD IMPROVEMENT OPPORTUNITIES**

References and useful information:

- SDAPP Fact Sheet: [8. Urban Ecology](#)
- Department of Sustainability and Environment www.dse.vic.gov.au
- Australian Research Centre for Urban Ecology www.arcue.botany.unimelb.edu.au
- Greening Australia www.greeningaustralia.org.au
- Green Roof Technical Manual www.yourhome.gov.au

9. Innovation

Objectives:

- to encourage innovative technology, design and processes in all development, which positively influence the sustainability of buildings

Issues	Applicant's Design Responses	Council Comments	CAR*
Carbon neutral power	A minimum 10 year agreement for Green Power.	Satisfactory	1
Global Sustainability	ESD checkpoints during construction phase.	Satisfactory	1
IEQ sensors	Sensors to assist air quality	Satisfactory	1
Building ESD display	Display and metering	Not considered innovative. Please remove credit claim from the BESS report.	2
Building Smart Phone app	App to link building users and facility management.	Not considered innovative. Please remove credit claim from the BESS report.	2
Digital notice board	Digital noticeboard with various information displayed.	Sounds similar to the Building ESD display. Not considered innovative. Please remove credit claim from the BESS report.	2

*Council Assessment Ratings:

- 1 – Design Response is **SATISFACTORY**
- 2 – Design Response is **NOT SATISFACTORY**
- 3 – **MORE INFORMATION** is required
- 4 – **ESD IMPROVEMENT OPPORTUNITIES**

References and useful information:

- SDAPP Fact Sheet: [9. Innovation](#)
- Green Building Council Australia www.gbca.org.au
- Victorian Eco Innovation lab www.ecoinnovationlab.com
- Business Victoria www.business.vic.gov.au
- Environment Design Guide www.environmentdesignguide.com.au

10. Construction and Building Management

Objectives:

- to encourage a holistic and integrated design and construction process and ongoing high performance

Issues	Applicant's Design Responses	Council Comments	CAR*
Building Tuning	No specific information provided.	Recommend comprehensive commissioning and tuning of all major services.	4
Building Users Guide	No specific information provided.	Satisfactory	1
Contractor has Valid ISO14001 Accreditation	No specific information provided.	Recommend lead construction contractor to have valid ISO14001 or similar EMS.	4
Construction Management Plan	No specific information provided.	Recommend comprehensive CMP to developed and implemented.	4
Others	-	-	-

*Council Assessment Ratings:

- 1 – Design Response is **SATISFACTORY**
- 2 – Design Response is **NOT SATISFACTORY**
- 3 – **MORE INFORMATION** is required
- 4 – **ESD IMPROVEMENT OPPORTUNITIES**

References and useful information:

- SDAPP Fact Sheet: [10. Construction and Building Management](#)
- ASHRAE and CIBSE Commissioning handbooks
- International Organization for standardization – ISO14001 – Environmental Management Systems
- Keeping Our Stormwater Clean – A Builder's Guide www.melbournewater.com.au

Sustainable Management Plan (SMP) for planning applications being considered by Yarra Council



Applicant Response Guidelines

Project Information:

Applicants should state the property address and the proposed development's use and extent. They should describe neighbouring buildings that impact on or may be impacted by the development. It is required to outline relevant areas, such as site permeability, water capture areas and gross floor area of different building uses. Applicants should describe the development's sustainable design approach and summarise the project's key ESD objectives.

Environmental Categories:

Each criterion is one of the 10 Key Sustainable Building Categories. The applicant is required to address each criterion and demonstrate how the design meets its objectives.

Objectives:

Within this section the general intent, the aims and the purposes of the category are explained.

Issues:

This section comprises a list of topics that might be relevant within the environmental category. As each application responds to different opportunities and constraints, it is not required to address all issues. The list is non-exhaustive and topics can be added to tailor to specific application needs.

Assessment Method Description:

Where applicable, the Applicant needs to explain what standards have been used to assess the applicable issues.

Benchmarks Description:

The applicant is required to briefly explain the benchmark applied as outlined within the chosen standard. A benchmark description is required for each environmental issue that has been identified as relevant.

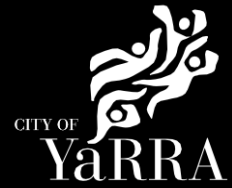
How does the proposal comply with the benchmarks?

The applicant should show how the proposed design meets the benchmarks of the chosen standard through making references to the design brief, drawings, specifications, consultant reports or other evidence that proves compliance with the chosen benchmark.

ESD Matters on Architectural Drawings:

Architectural drawings should reflect all relevant ESD matters where feasible. As an example, window attributes, sun shading and materials should be noted on elevations and finishes schedules, water tanks and renewable energy devices should be shown on plans. The site's permeability should be clearly noted. It is also recommended to indicate water catchment areas on roof- or site plans to confirm water re-use calculations.

City Works Formal Referral Response



Application Information:

Referral Officer:	Atha Athanasi
Officer:	Joe Byrne
Council Reference:	PLN25/0181
Address:	418 Burnley St, Richmond VIC 3121
Proposal:	Construction of a ten storey mixed use building (plus two basement levels), use of the land for Office and Food and Drink Premises, construct and display Business Identification Signage, reduction in the car parking requirements, and create and alter access to a road in Transport Zone 2.
Comments Sought:	Click here to view referral memo: Record D25/223551: IREF25/00773 - Internal Referral Request
Disclaimer:	Council's City Works Unit provides the following information which is based on the information provided in the referral request memo referenced above.
Prev. Responses:	

Comments:

The waste management plan for 418 Burnley St, Richmond by Traffix Group and dated 12/3/2025 is not satisfactory from a City Works Branch's perspective.

Issues to be rectified include, but may not be limited to the following:

1. Please remove reference to residential waste storage area in table 8.

Waste Management Officer: Atha Athanasi

Signature:

Date: 26/06/2025

Development Engineering Formal Referral Response



Application Information	
Referral Officer	Artemis Bacani
Officer	Joe Byrne
Council Reference	IREF25/00772
Address	418 Burnley Street, Richmond
Application No.	PLN25/0181
Proposal	Construction of a ten storey mixed use building (plus two basement levels), use of the land for Office and Food and Drink Premises, construct and display Business Identification Signage, reduction in the car parking requirements, and create and alter access to a road in Transport Zone 2.
Comments Sought	<ul style="list-style-type: none"> • Crossover width and location • Basement arrangements • Reduction in car parking • Traffic issues • Road infrastructure • Streetscape improvements
Disclaimer	Council's Development Engineering unit, provides the following advice based on information provided in the referral request memo referenced above.

Engineering referral details

Council's Engineering Referral team has reviewed the drawings and documents provided by the Statutory Planning department, as outlined in **Table 1** below.

A list of requirements for the applicant and proposed conditions to be included in the Planning Permit have been outlined in the **Section 1 – Engineering requirements** below. Specific details of the assessment are provided in **Section 3** - and have informed the requirements and conditions.

Note: the engineering related matters highlighted in the Planning referral have been assessed and included in the response.

Table 1 - Drawings and documents reviewed by Engineering

Consultant	Drawing No. or Document	Revision	Dated
Studio McCue	TP-012 <i>Site Plan - Proposed</i>	A	22 April 2025
	TP-101 <i>GA Plan – Basement Level 2</i>	A	22 April 2025
	TP-102 <i>GA Plan – Basement Level 1</i>	A	22 April 2025
	TP-103 <i>GA Plan - Ground</i>	A	22 April 2025
	TP-301 <i>Building Section A</i>	A	22 April 2025
Traffix Group	<i>Traffic Engineering Assessment</i>	C	13 March 2025
	<i>Waste Management Plan</i>	B	12 March 2025
Contour	<i>Town Planning and Urban Context Report (RFI Updates)</i>	V2	May 2025

SECTION 1: Engineering requirements – Issue to the applicant

The applicant must satisfy the engineering items outlined in **Table 2** below. A written response must be provided for each requirement, and the action is to be completed prior to resubmission.

Any amendments to plans/drawings or updates to reports/documents must be highlighted using a **red cloud** around the relevant section. In the written response, indicate the relevant sheet/pages of each document which have been amended.

Should any engineering comments / direction not be adopted, the relevant planner is **REQUIRED** to refer back to the Civil Engineering team for further discussion / comments / conditions prior to finalising reports.

Table 2 - Engineering requirements for applicant

Item	Engineering requirement	Action for applicant
1	The width of the internal access ramp and kerbs to be dimensioned on the drawings.	Update the relevant drawings accordingly
2	It is recommended for the proposed gate to be positioned south of the existing sewerage vent pole.	Update the relevant drawings accordingly
3	Dimension the wheel stop distance.	Update the relevant drawings accordingly

SECTION 2: Engineering conditions – Planning Permit

The conditions outlined in **Table 3** below must be included in the Planning Permit to ensure the specific engineering requirements are complied with.

Note: further conditions may be required if any of the items in Table 2 are not fulfilled prior to the issuing of the Planning Permit.

Table 3 - Engineering conditions to be included in the Planning Permit

Condition related to...	Engineering condition
VC: <i>Reinstatement</i>	<p><u>Before the building/s is/are occupied</u>, or by such later date as approved in writing by the Responsible Authority, any redundant vehicular crossing must be demolished and re-instated as footpath, verge (if applicable), and kerb and channel:</p> <p>(a) at the permit holder's cost; <i>and</i> (b) to the satisfaction of the Responsible Authority.</p> <p>A note should be placed on the plan indicating redundant driveways are to be removed and the road pavement, kerb and channel and footpath reinstated to Council's satisfaction.</p>
VC: <i>Design</i>	<p><u>Concurrent with the submission of Condition 1 plans</u> or by such later date as approved in writing by the Responsible Authority, a vehicle crossing design must be submitted to Council's Civil Engineering Department for approval.</p> <p>The submitted design must demonstrate compliance with City of Yarra's, <i>Vehicle Crossing Information Sheet</i>.</p>
VC: <i>Construction</i>	<p><u>Before the building/s is/are occupied</u>, or by such later date as approved in writing by the Responsible Authority, any new vehicle crossing must be constructed:</p> <p>(a) at the permit holder's cost; <i>and</i> (b) to the satisfaction of the Responsible Authority.</p>
Public Realm: <i>Footpath specific</i>	<p><u>Before the building/s is/are occupied</u> or by such later date as approved in writing by the Responsible Authority, the footpath along the property's Burnley Street and Madden Grove frontages must be re-sheeted in asphalt:</p> <p>(a) at the permit holder's cost; <i>and</i> (b) to the satisfaction of the Responsible Authority(s).</p>
Public Realm: <i>Kerb & channel specific</i>	<p><u>Before the building/s is/are occupied</u>, or by such later date as approved in writing by the Responsible Authority, the kerb and channel along the property's Burnley Street and Madden Grove frontages must be reconstructed:</p> <p>(a) at the permit holder's cost; <i>and</i> (b) to the satisfaction of the Responsible Authority(s).</p>

Condition related to...	Engineering condition
Public Realm: Road pavement specific	<p><u>Before the building/s is/are occupied</u>, or by such later date as approved in writing by the Responsible Authority, the road pavement along the property's Burnley Street and Madden Grove frontages must be reconstructed:</p> <ul style="list-style-type: none"> (a) at the permit holder's cost; <i>and</i> (b) to the satisfaction of the Responsible Authority(s).
Reinstatement of damages caused during development works	<p>Within 2 months of the completion or by such later date as approved in writing by the Responsible Authority, any damage to Council infrastructure resulting from the development must be reinstated:</p> <ul style="list-style-type: none"> (a) In accordance with Yarra Standard Drawings Yarra City Council (b) at the permit holder's cost; and (c) To the satisfaction of the Responsible Authority.
Note related to...	Engineering note
Service infrastructure adjustment to suit finished grades, alignments, etc.	<p>Any service poles, structures or pits located within the public realm areas that interfere with the proposal, must be adjusted accordingly:</p> <ul style="list-style-type: none"> (a) at the permit holder's cost; and (b) to the satisfaction of the Responsible Authority.
Existing parking infrastructure	<p>No parking restriction signs, or line-marked on-street parking bays are to be removed, adjusted, changed or relocated without approval or authorisation from Council's Parking Management unit and Construction Management branch.</p> <p>Any on-street parking reinstated as a result of development works must be approved by Council's Parking Management unit.</p>
Private utility assets	<p>Areas must be provided inside the property line and adjacent to the footpath to accommodate pits and meters. No private pits, boundary traps, valves or meters on Council property will be accepted.</p>
Adjusting utility infrastructure	<p>Any services poles, structures or pits that interfere with the proposal must be adjusted, removed or relocated at the owner's expense after seeking approval from the relevant authority.</p>
Discharge of Water from Development	<p>Only roof runoff, surface water and clean groundwater seepage from above the water table can be discharged into Council drains.</p>
Discharge of Water from Development	<p>Council will not permit clean groundwater from below the groundwater table to be discharged into Council's drainage system. Basements that extend into the groundwater table must be waterproofed/tanked.</p>
Parking sensors	<p>The removal of any kerbside parking sensors and any reinstatement of parking sensors will require the Permit Holder to pay Council the cost of each parking sensor taken out from the kerb/footpath/roadway. Any costs associated with the reinstatement of road infrastructure due to the removal of the parking sensors must also be borne by the Permit Holder.</p>

Condition related to...	Engineering condition
<p>Advice Note</p>	<p>The applicant must apply for a Legal Point of Discharge under Regulation 133 – Stormwater Drainage of the Building Regulations 2018 from Yarra Building Services unit. Any storm water drainage area within the property must be provided for and be connected to the nearest Council pit of adequate depth and capacity (legal point of discharge), or to Council’s satisfaction under Section 200 of the Local Government Act 1989 and Regulation 133.</p>

SECTION 3: Engineering detailed assessment

CAR PARKING PROVISION

Proposed Development

Under the provisions of Clause 52.06-5 of the Yarra Planning Scheme, the development's parking requirements are as follows:

Proposed Use	Quantity/ Size	Statutory Parking Rate*	No. of Spaces Required	No. of Spaces Allocated
Office	5,291 m ²	3 spaces to each 100 m ² of net floor area	158	27
Food & Drink Premises (Café)	163 m ²	3.5 spaces to each 100 m ² of leasable floor area	4	1
Total			162 spaces	28 spaces

* Since the site is located within the Principal Public Transport Network Area, the parking rates in Column B of Clause 52.06-5 now apply.

The proposed development will require a car parking reduction of 134 car spaces (consisting of 131 office spaces and 3 spaces for the food and drink premises) in the car parking requirements of *Clause 52.06*.

To reduce the number of car parking spaces required under Clause 52.06-5 (including to reduce to zero spaces), the application for the car parking reduction must be accompanied by a Car Parking Demand Assessment.

Car Parking Demand Assessment

In reducing the number of parking spaces required for the proposed development, the Car Parking Demand Assessment would assess the following:

Parking Demand Consideration	Details
<i>Parking Demand for Office Use</i>	<p>The office use will be provided with 27 car spaces resulting in a car parking rate of 0.51 spaces per 100 square metres of net floor area.</p> <p>Throughout the municipality, a number of developments have been approved with no car spaces or a reduced office rate, as shown below:</p> <ul style="list-style-type: none"> ▪ 71-93 Gipps Street, Collingwood – 0.96 spaces/100 m² ▪ 506 & 508-510 Church Street, Cremorne – 1.09 spaces/100m² ▪ 2-16 Northumberland Street, Collingwood – 0.89 spaces/100m² <p>The proposed office parking rate of 0.51 spaces per 100 square metres of floor area is considerably lower compared with the rates shown above.</p>

Parking Demand Consideration	Details
	The lower parking rate is offset by the site’s access to public transport and seeks to encourage the use of more sustainable forms of transportation.
<i>Parking Demand for Food and Drink (Café) Use</i>	<p>The statutory car parking requirement for the food and drink use is 4 spaces.</p> <p>The 1 car space allocated to the café would be provided for staff use.</p> <p>All other parking generated by the use would be accommodated on-street in the surrounding road network.</p> <p>The majority of customers to the food and drink use would be derived by foot traffic from the above offices, and residents and employees of nearby residences and businesses.</p>
<i>Availability of Public Transport in the Locality of the Land</i>	<p>The following public transport services can be accessed to and from the site by foot:</p> <ul style="list-style-type: none"> ▪ Burnley railway station – 90 metre walk ▪ Swan Street trams – 130 metre walk ▪ Church Street trams – 770 metre walk ▪ East Richmond railway station – 800 metre walk
<i>Multi-purpose Trips within the Area</i>	Visitors and customers to the development could combine their visit by engaging in other activities or business whilst in the area.
<i>Convenience of Pedestrian and Cyclist Access</i>	The site is very well positioned in terms of pedestrian access to public transport nodes, shops, businesses and other essential facilities. The site also has good connectivity to the on-road bicycle network.

Appropriateness of Providing Fewer Spaces than the Likely Parking Demand

Clause 52.06 lists a number of considerations for deciding whether the required number of spaces should be reduced. For the subject site, the following considerations are as follows:

Consideration	Details
<i>Availability of Car Parking</i>	<p>Traffix Group have referenced the <i>Journey to Work Data</i> from the 2016 Census.</p> <p>The data shows the majority of employees working in Richmond travel to work by alternative transportation modes and not by private vehicle.</p> <p>In addition to the above, Traffix Group had commissioned a car parking occupancy survey on Tuesday 28 January 2025 at 3 pm.</p> <p>The survey area includes sections of Madden Grove, Burnley Street and Adam Street.</p>

Consideration	Details
	<p>A parking supply of between 126 parking spaces were identified.</p> <p>The survey results an occupancy of 94 % or 118 spaces occupied.</p> <p>The surveys confirm that parking demand in this part of Richmond is very high and would be a disincentive for visitors and staff to commute to and from the site by private vehicle.</p>
<i>Relevant Local Policy or Incorporated Document</i>	<p>The proposed development is in line with the objectives contained in Council's <i>Strategic Transport Statement</i>. The site is ideally located with regard to sustainable transport alternatives and the reduced provision of on-site car parking would potentially discourage private motor vehicle ownership and use.</p>

Adequacy of Car Parking

From a traffic engineering perspective, the reduction of parking for the proposed development is considered appropriate in the context of the development and the surrounding area.

The operation of the development should not adversely impact on the existing on-street parking conditions in the area.

The Engineering Referral team has no objection to the reduction in the car parking requirement for this site.

TRAFFIC IMPACT ASSESSMENT

Trip Generation

The trip generation for the site adopted by Traffix Group is as follows:

Proposed Use	Adopted Traffic Generation Rate	AM Peak Hour	PM Peak Hour
Office (27 car spaces)	- 0.5 trips per space during peak hours	14 trips	14 trips
Food and drinks (1 car space)			
Total		14 Trips	14 Trips

It is agreed the traffic generated by the proposed development is considered low and should not adversely impact the traffic operation of the surrounding road network.

DEVELOPMENT LAYOUT DESIGN

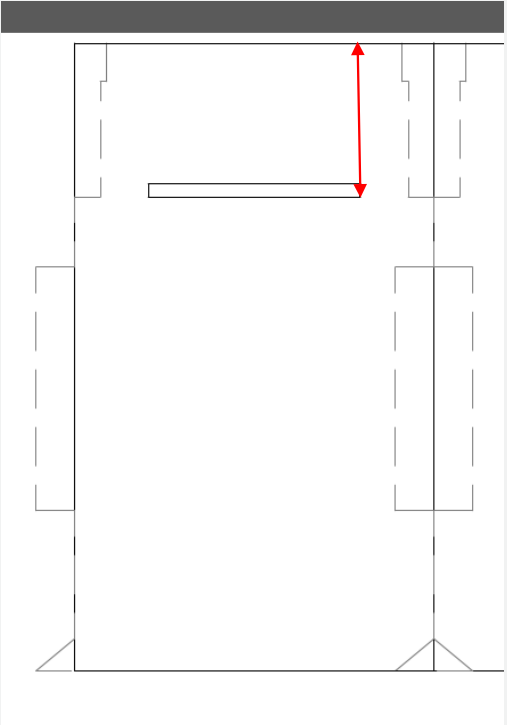
Layout Design Assessment

Item	Assessment
Access Arrangements	
Width of Development's Entrance	The development's entrance is 4 metres in width.
Width of Internal Access Ramp	The width of the internal access ramp is not dimensioned on the drawings.
Visibility	A visibility triangle measuring 2.5 metres by 2 metres is provided on the exit lane of the car park entrance to satisfy <i>Design standard 1 – Accessway</i> .
Headroom Clearance	A minimum headroom clearance of 2.2 metres is provided under the critical structures of the basement car park access ramp to satisfy <i>Design standard 1 - Accessways</i> .
Car Parking Modules	
Car Spaces	The dimensions of the car spaces of 2.8 metres by 4.9 metres satisfies <i>Design standard 2 – Car parking spaces</i> .
Accessible Car Space	The dimensions of the accessible car space and shared area of 2.4 metres by 5.4 metres satisfies <i>AS/NZS 2890.6:2009</i> .
Aisles	A minimum aisle of 5.8 metres is provided to satisfy <i>Design standard 2 – Car parking spaces</i> .
Column Depths and Setbacks	The column depths and satisfy <i>Figure 5.2 of AS/NZS 2890.1:2004</i> .
Clearances to Walls	Car spaces adjacent to a wall have been provided with a minimum clearance of 300 millimetres to satisfy <i>AS/NZS 2890.1:2004</i> .
Gradients	
Ramp Grade for the first 5.0 metres inside the Property	The ramp grade for the first 5.0 metres inside the development comprises of a flat section followed by a 1 in 10 section to satisfy <i>Design standard 3 – Gradients</i> .
Transition Grades at Bases of 1 in 4 and 1 in 4.1 Ramp Sections	The length of the 1 in 8 transition grade at the base of the 1 in 4 ramp sections is 2.6 metres and is considered satisfactory.
Swept Path Assessment	
Swept Path Diagrams – 36145-01B, Sheet No.01, Rev A	The swept path diagrams for the B99 design vehicle template demonstrates the circulation and passing movements on the access ramps and within the basement levels are adequate.
Swept Path Diagrams – 36145-01B, Sheet No.02, Rev A	The swept path diagrams for the B85 design vehicle template demonstrates adequate entry and exit movements into and out of the end car spaces on all basement levels.
Swept Path Diagrams – 36145-01B, Sheet No.03, Rev A	The swept path diagrams for the 6.34-metre-long Waste Wise Mini demonstrates adequate entry and exit movements into and out of the site via Madden Grove.

Item	Assessment
Swept Path Diagrams – 36145-01B, Sheet No.04, Rev A	The swept path diagrams for the 6.34-metre-long Waste Wise Mini demonstrates adequate entry and exit movements into and out of Basement Level 1.
Other Items	
Vehicle Crossing Vertical Ground Clearance Check	The vertical ground clearance assessment using the B99 design vehicle template demonstrates adequate clearance along the vehicle crossing and the development’s access ramp.
Motorcycle Spaces	The dimensions of the motorcycle spaces of 1.2 metres by 2.5 metres satisfy <i>AS/NZS 2890.1:2004</i> .
Existing Gate - Laneway	According to Council records, the existing gate obstructing the laneway was approved by Council as part of the planning application for 41 Madden Grove. If the gate had not been previously approved, the Engineering unit would have requested for the gate to be removed.


Engineering Advice for Design Items to be Addressed by the Applicant

Item	Details
Width of Internal Access Ramp	The applicant to dimension on the drawings the width of the internal access ramp, including kerbs.
Proposed Gate - Laneway	It is recommended for the applicant to position the proposed gate south of the sewerage vent pole to provide unobstructed access to the pole for maintenance. The position of the gate must not obstruct or diminish any part of the windows or glazing at the adjacent property.
Wheel Stop – Basement Car Park	The applicant to dimension on the drawings the wheel stop distance (measured to point of contact with vehicle tyre) in accordance with <i>Section 2.4.5.4 Wheel stops</i> of <i>AS/NZS 2890.1:2004</i> .

Item	Details
	 <p>The diagram shows a floor plan of a rectangular room. A red double-headed vertical arrow is positioned in the upper right quadrant, indicating a specific vertical measurement. A horizontal line is drawn across the room, intersecting the arrow. The room features a door on the left wall and a window on the right wall. The walls are represented by solid lines, and the floor is a solid line at the bottom. There are also dashed lines indicating internal divisions or furniture placement.</p>

SECTION 4: Acknowledgement

Engineer: Artemis Bacani

Signature: 

Date: 4 July 2025

SECTION 2: Engineering Requirements for Applicant

Item	Engineering Requirements	Action for Applicant
General		
1	Stormwater from the development must not discharge into the lane on the west side of the site	All stormwater discharge must be directed to Council drainage in Madden grove or Burnley Street.
2	Substation access from lane may be restrictive and obstruct vehicle access to other properties.	Obtain power authority confirmation that access is Satisfactory Confirm substation maintenance will not impede access to other abutting property owners
3	If modifications to the existing kerb and channel alignment are proposed, the concept designs must provide sufficient surface levels to demonstrate that surface stormwater will drain to a Council stormwater pit.	Kerb and channel modification concepts must demonstrate surface rainwater will drain to a Council pit.
	The footpaths adjoining the development must be reconstructed with DDA compliant cross-falls	The footpaths adjoining the development in Madden Grove and Burnley Street must be reconstructed to the satisfaction of Council. The applicant must provide construction plans in accordance with Council requirements and engineering for Council approval before any works are undertaken within the road reserve.
4	All redundant property stormwater drains in the road reserve must be removed and the footpath and kerb and channel reinstated in accordance with Council standards and requirements.	Redundant property stormwater drains within the road reserve must be identified and notations added to plans indicating that they will be removed and council infrastructure reinstated to Council satisfaction.

5	The ground floor plan does not show any existing service authority infrastructure (pits, valves, hydrants, cabinets etc) in the footpath. These should be shown on the ground floor plans with a notation that they are to be adjusted to match surface level changes.	Show service authority assets (pits, valve covers, hydrants, cabinets, etc) on the plans and add notation that service authority assets are to be adjusted to match any surface level changes
6	Existing parking signs should be shown on the ground floor plan.	Show existing parking signs on the ground floor plan.
7	The existing raised pavement in the road just east of the lane is not shown on the ground floor plan.	Show the existing raised pavement on the ground floor plan.
8	The existing Council stormwater pit in the Burnley Street channel adjacent to the south boundary of the development is not shown on the ground floor plan.	Show the existing stormwater pit on the ground floor plan.
9	The ground floor plan shows a kerb layback 5m long where the existing vehicle crossing is to be removed. The vehicle crossing layback at the corner of Burnley St and Madden Grove must be removed and a pram crossing constructed. The kerb and channel on each side of the pram crossing must be reinstated to the satisfaction of Council.	Show a pram crossing and adjoining kerb and channel reinstatement on the ground floor plan where the existing vehicle crossing is to be removed. Add notation to the ground floor plan to construct a pram crossing and to reinstate the kerb and channel and footpath to the satisfaction of Council.

SECTION 2: Engineering Conditions/Notes – Planning Permit

The conditions & notes outlined in **Table 3** below must be included in the Planning Permit to ensure the specific engineering requirements are complied with.

Note: further conditions may be required if any of the items in Table 2 are not fulfilled prior to the issuing of the Planning Permit.

Table 1 – Engineering conditions and notes to be included in the Planning Permit

Condition related to...	Planning permit CONDITIONS
Public lighting: <i>Construction</i>	The provisions, recommendations and requirements of the endorsed Public Lighting Plan must be implemented and complied with at no cost to Council and to the satisfaction of the Responsible Authority.
Public Realm: <i>Footpath specific</i>	<p><u>Before the building/s is/are occupied</u> or by such later date as approved in writing by the Responsible Authority, the footpath along the property’s Madden Grove and Burnley St frontage/s must be reconstructed in asphalt:</p> <ul style="list-style-type: none"> (a) at the permit holder’s cost; <i>and</i> (b) to the satisfaction of the Responsible Authority(s).
Public Realm: <i>Kerb & channel specific</i>	<p><u>Before the building/s is/are occupied</u>, or by such later date as approved in writing by the Responsible Authority, the kerb and channel along the property’s Madden Grove and Burnley St frontage/s must be reconstructed:</p> <ul style="list-style-type: none"> (a) at the permit holder’s cost; <i>and</i> (b) to the satisfaction of the Responsible Authority(s).
Public Realm: <i>Road pavement specific</i>	<p><u>Before the building/s is/are occupied</u>, or by such later date as approved in writing by the Responsible Authority, the road pavement along the property’s Madden Grove and Burnly St frontage/s must be re-sheeted:</p> <ul style="list-style-type: none"> (a) at the permit holder’s cost; <i>and</i>

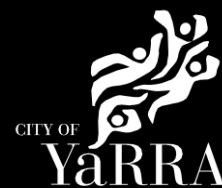
	to the satisfaction of the Responsible Authority(s).
Public Realm: <i>Functional layout plan</i> <i>[High level details]</i>	<p><u>Concurrent with the submission of Condition 1 plans</u>, or by such later date as approved in writing by the Responsible Authority, a Public Realm Plan of all public realm improvements associated with the development must be prepared, submitted and approved by the Responsible Authority. When approved, the plan will be endorsed and will then form part of the permit.</p> <p>The Public Realm Plan must show, but not be limited to, the following:</p> <ul style="list-style-type: none"> (a) Footpath, kerb & channel and road pavement works along all frontages (b) Kerb outstands and associated works in accordance with requirements outlined in the Condition 1 plans (c) Proposed or adjustment of existing utility services (d) Streetscape/landscape requirements (e) Any public realm infrastructure required under other permit conditions. (f) at the permit holder's cost; <i>and</i> (g) to the satisfaction of the Responsible Authority.
Public Realm: <i>Detailed design plans</i>	<p><u>Before the development commences (excluding site preparations, demolition and bulk excavation)</u>, or by such later date as approved in writing by the Responsible Authority, detailed design drawings of the works approved under the Public Realm Plan, addressing all road infrastructure works (including soft/hard landscaping), must be submitted to and approved by Council's Civil Engineering Department. Once approved, the detailed design drawings will be endorsed and will then form part of the permit.</p>
Public Realm: <i>Construction & handover</i>	<p>Before the building is occupied, all associated works shown on the endorsed Detailed Design Plans (DDP) for the public realm must be fully constructed and completed to the satisfaction of the Council's Civil Engineering Department and at no cost to the Responsible Authority.</p>
Reinstatement of damages caused during development works	<p><u>Within 2 months of the completion</u> or by such later date as approved in writing by the Responsible Authority, any damage to Council infrastructure resulting from the development must be reinstated:</p> <ul style="list-style-type: none"> (a) In accordance with Yarra Standard Drawings Yarra City Council (b) at the permit holder's cost; <i>and</i> (c) To the satisfaction of the Responsible Authority.

Note related to...	Planning permit NOTES
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Stormwater: <i>Site discharge</i>	The site stormwater must be directed to the nominated legal point of discharge (LPD) and shall be limited to equivalent pre-development levels or 70% impervious coverage, whichever is lowest, for a 20% AEP rainfall event.
Stormwater: <i>Onsite detention</i> Stormwater: <i>Onsite detention</i>	The development must detain onsite, at a minimum, the 10% AEP storm event. For cases where a safe overland flow path cannot be provided or where flows exceeding pipe capacity may impact the development or adjacent, upstream, or downstream properties, the requirement will be to detain the 1% AEP storm event.
Stormwater: <i>Redundant outlets</i>	All redundant property drain outlets are to be demolished, removed and reinstated to Council’s satisfaction and at the Permit Holder’s cost.
Stormwater: <i>Groundwater management</i>	Only roof runoff, surface water and clean groundwater seepage from above the water table can be discharged into Council drains. Council will not permit clean groundwater from below the groundwater table to be discharged into Council’s drainage system. Basements that extend into the groundwater table must be waterproofed/tanked.
Service infrastructure adjustment to suit finished grades, alignments, etc	Any service poles, structures or pits located within the public realm areas that interfere with the proposal, must be adjusted accordingly: (a) at the permit holder’s cost; and (a) to the satisfaction of the Responsible Authority.
Existing parking infrastructure	No parking restriction signs, or line-marked on-street parking bays are to be removed, adjusted, changed or relocated without approval or authorisation from Council’s Parking Management unit and Construction Management branch.
Private utility assets	Areas must be provided inside the property line and adjacent to the footpath to accommodate pits and meters. No private pits, boundary traps, valves or meters on Council property will be accepted.
Adjusting utility infrastructure	Any services poles, structures or pits that interfere with the proposal must be adjusted, removed or relocated at the owner’s expense after seeking approval from the relevant authority.
Existing parking infrastructure	Any on-street parking reinstated as a result of development works must be approved by Council’s Parking Management unit.
Parking sensors	The removal of any kerbside parking sensors and any reinstatement of parking sensors will require the Permit Holder to pay Council the cost of each parking sensor taken out from the kerb/footpath/roadway. Any costs associated with the reinstatement

	of road infrastructure due to the removal of the parking sensors must also be borne by the Permit Holder.

Urban Design Formal Referral Response



Application Information:

Referral Officer:	Lucy Stratton
Officer:	Joe Byrne
Council Reference:	PLN25/0181
Address:	418 Burnley St, Richmond VIC 3121
Proposal:	Construction of a ten-storey mixed use building (plus two basement levels), use of the land for Office and Food and Drink Premises, construct and display Business Identification Signage, reduction in the car parking requirements, and create and alter access to a road in Transport Zone 2.
Comments Sought:	Click here to view referral memo: D25/225343 - IREF25/00483 - Referral Request
Disclaimer:	Council's Urban Designer provides the following information which is based on the information provided in the referral request memo referenced above.
Prev. Responses:	Not applicable.

Recommendation

Changes are required to make the proposal acceptable from an urban design perspective:

- Rationalise the building line to avoid the creation of small recesses and improve visibility and functionality.
- Design streetscape improvements around the site, using the recommended marked up plan to guide the extent of works.
- Confirm the substation location and access meets service authority requirements.

Comment Summary

Public Realm Interface

- The site has frontages to Madden Grove (north) and the Burnley Street service lane (east). Vehicular entry is proposed from Burnley Street along the southern boundary, utilising an existing crossover.
- Two ground floor tenancies are proposed (food and beverage and office). The central lobby and food and beverage are accessed from the inset forecourt area. Generally the street frontages provide a good level of activation.
- The following recommendations are made to improve the ground floor program and interface with the street:
 - Rationalise setbacks to addresses small poorly defined and underutilised spaces along the building frontages on both Madden Grove and Burnley Street.
 - Ensure clear delineation between the public and private site boundary.

- Provide footpath levels and FFL at entrances and within setbacks to ensure a seamless transition.
- Confirm the substation location and access meets the service authority requirements.

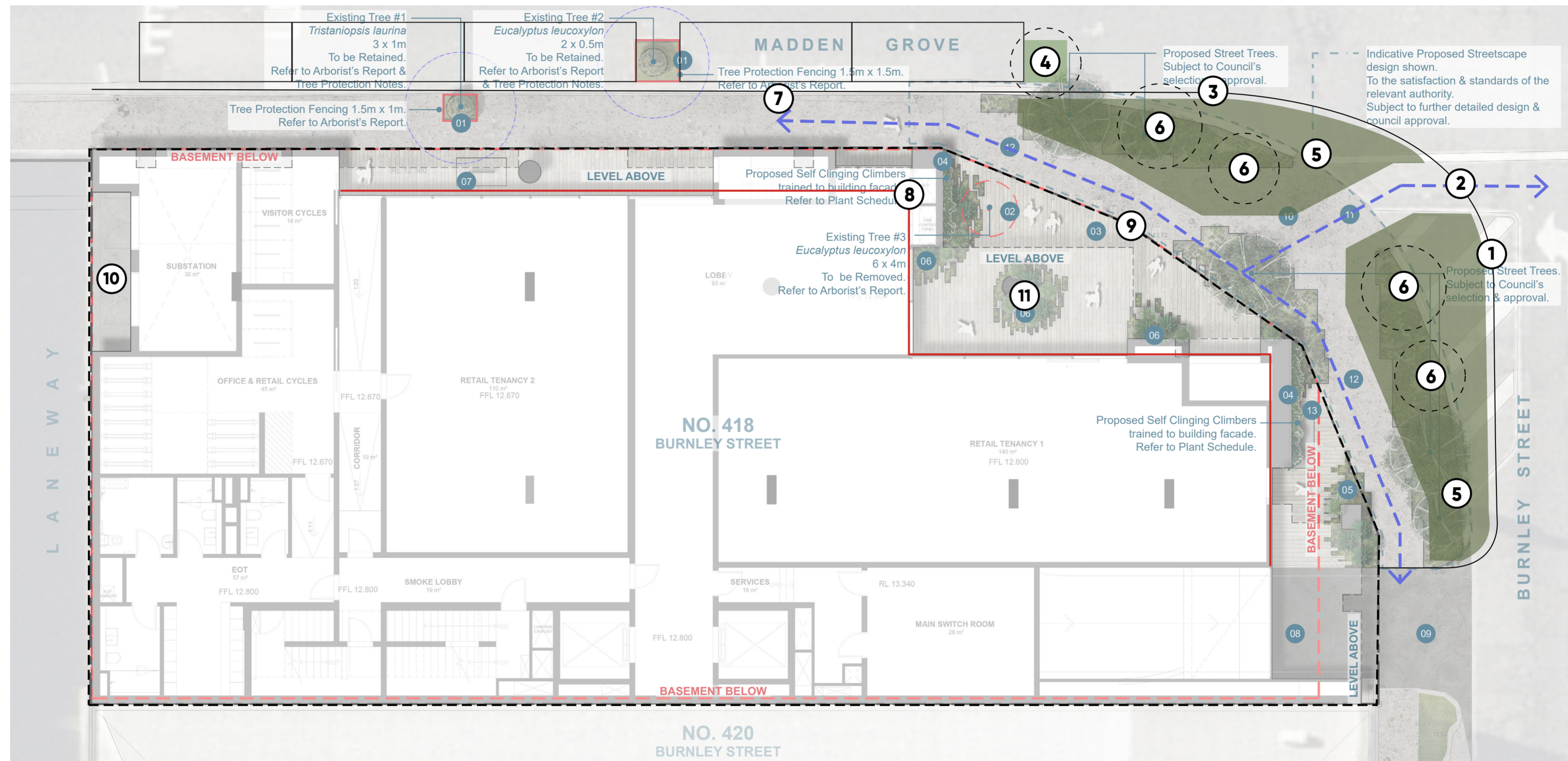
Streetscape and Capital Works

- There is significant opportunity for streetscape works along Madden Grove and Burnley Street. Providing a generous kerb outstand will improve pedestrian crossing safety and allow for generous garden beds and planting.
- The markup provided should be investigated and detailed in a Public Realm and Streetscape Plan to the satisfaction of Urban Design.
- There are no known planned / approved capital works around the site being led by the Urban Design Team.

Urban Designer: Lucy Stratton
Date: 17 July 2025

Recommended Streetscape Improvements

Urban Design Markup Only 15/07/2025



1. Kerb extension on Burnley Street to narrow crossing distance. Tighten up intersection as much as possible, subject to detailed swept path analysis.
2. New pram crossing to align with crossing point on Burnley Street service lane.
3. Reinstated footpath, kerb and channel along site frontages, with asphalt footpaths and sawn bluestone kerb. Refer to any reinstatement requirements from the Civil Engineering team. Footpath cross-falls must be DDA compliant.
4. Opportunity for 2 x on street car parking bays (must be a minimum of 6 metres in length per bay) and 1 x road tree cutout (1.5m x 1.5m).

5. New garden beds on Madden Grove and Burnley Street. Garden beds to accommodate all existing services and pits. Maintain minimum footpath width of 1.8m.
6. New garden bed trees to meet required intersection offset requirements.
7. Investigate removing terminal power pole.
8. Rationalise building line to improve the public realm outcome.
9. Site boundary clear of gardens beds. Asphalt footpath only. Clear delineation between public and private through materiality.

10. Confirm siting of substation and access with relevant authorities. Noting the gradient of the laneway and level change required to access the substation. Potential to relocate the gate south of the sewer vent pole to provide unobstructed access for maintenance by the responsible Service Authority
11. All on basement podium garden beds require allowance for irrigation and adequate drainage. All pipework must be concealed and integrated in adjacent pavement construction.

Notes: Subject to swept path and drainage assessment. As well as site survey / detailed dimensions and service locations. Garden beds to be a minimum of 5sqm in area.

Informal Referral Meeting

Heritage



Application Information for Referral IREF25/01243

Officer:	Joe Byrne
Council Reference:	P691/2025
Address:	418 Burnley St, Richmond VIC 3121
Proposal:	Planning VCAT Child Process - VCAT Appeal
Zone:	IN3Z - Industrial 3 Zone
Overlay(s):	DDO2 - Design and Development Overlay - Schedule 2 (Main Roads and Boulevards) DDO5 - Design and Development Overlay - Schedule 5 DCPO1 – Development Contributions Plan Overlay
	Heritage Grade: Not Applicable

Meeting Details

Date:	25 August 2025
Referral Officer	Diahnn McIntosh
Relevant Information:	Click here to view the relevant documents in Content Manager: <ul style="list-style-type: none"> • PLN25/0181 - 418 Burnley Street Richmond - S52 Advertising - Plans

Advice Provided at Meeting

The following comments were made:

1. There is no physical impact to the heritage building due to the setback from the east (from the lane). The only potential concerns would be from existing view lines and these seem to be reserved to the street façade fronting Madden Grove. The only distant view lines of the building would be from beyond the railway line and beyond, which cannot be considered as significant as this is an industrial area. The context of the heritage building is not original and has been completely redeveloped in recent years.

Planner Name: Joe Byrne

Date: 25 August 2025



Department of Transport and Planning

GPO Box 2392
Melbourne, VIC 3001 Australia
www.transport.vic.gov.au

Ref: PPR 49130/25

Joe Byrne
Yarra City Council
PO BOX 168
RICHMOND VIC 3121

Dear Joe,

PLANNING APPLICATION NO.: PLN25/0181
DEPARTMENT REFERENCE NO: PPR 49130/25
PROPERTY ADDRESS: 418 BURNLEY STREET, RICHMOND VIC 3121

Section 55 – No objection subject to conditions

Thank you for your referral of the above application to the Head, Transport for Victoria, Department of Transport and Planning under Section 55 of the *Planning and Environment Act 1987*.

The Head, Transport for Victoria has considered this application and does not object if the permit is subject to the following conditions:

1. Prior to the occupation of the development hereby approved, the modified crossover must be completed to the satisfaction of the Responsible Authority and at no cost to the Responsible Authority and the Head, Transport for Victoria. The widened crossover must keep minimum distance of 1 metre to the existing electrical pole.
2. All vehicles must enter and exit the site in a forward direction at all times.
3. Before the development starts, or such other time agreed to in writing by the Head, Transport for Victoria, amended plans or designers' reports, to the satisfaction of the Head, Transport for Victoria must be submitted to and approved by the Responsible Authority. When approved, the plans and reports will be endorsed and will then form part of the permit. The plans and reports must be generally in accordance with those submitted with the application (dated 22/04/2025, Revision A, prepared by Studio Mccue) but modified to show that:
 - a. The designs prevent items from being thrown or falling onto railway land from any part of the building development.
 - b. The development does not cause reflected sunlight to interfere with train driver visibility or interpretation of rail signals.
 - c. The development does not reflect or refract artificial light such that it interferes with train driver visibility or interpretation of rail signals.
 - d. The development exterior avoids use of red, green or yellow colour schemes that may interfere with driver operations.
4. Any windows, doors and balconies that are set back from, and generally facing the railway land title boundary shall:
 - a. Be designed to prevent items from being thrown or falling onto railway land.
 - b. Not cause reflected sunlight to interfere with train driver visibility or interpretation of rail signals.



- c. Not reflect or refract artificial light such that it interferes with train driver visibility or interpretation of rail signals.
5. Prior to the commencement of work on site, detailed construction/ engineering plans and structural computations for any construction in close proximity to railway infrastructure or railway land must be submitted and approved by VicTrack and the Head, Transport for Victoria. The plans must detail all basement excavation design, retention works and controls of the site in close proximity to the railway corridor having any impact on railway land. The design plans must ensure compliance with:
 - a. Minimum clearances to all electrical assets and procedures for works in close proximity to such assets, including:
 - i. Energy Safe Victoria (ESV) requirements for minimum clearances to electrical assets and works nearby.
 - ii. Australian Standards AS2067, AS7000 and Electricity Safety (General Regulations 2019, Part 6) for clearances to electrical assets. Clearances required include for safe working, fire life safety design, electromagnetic interference and earthing, bonding, and electrolysis mitigation design.
 - iii. Any other reasonable safety requirements required by the Rail Operator.
 - b. The required rail collision loadings and collision protection measures for the building supports in close proximity to the rail tracks in accordance with AS5100 Part 1 – ‘Bridge Design, Scope and General Principals’.
 - c. Earthquake design loadings for structure designated as a minimum Importance Level 2, by AS1170.4 – ‘Structural Design Actions, Earthquake Actions in Australia’.
 - d. Demonstration that ground stabilisation devices, such as temporary or permanent ground anchors, soil nails, reinforced earth straps, do not penetrate onto railway land.
 6. Prior to the commencement of demolition or construction works, the Rail Operator must be contacted through the email address metrositeaccess@metrotrains.com.au to obtain the Rail Operator’s conditions and safety requirements. Furthermore, the developer is required to contact the Developer Interface Projects team, Department of Transport and Planning to obtain guidance and information regarding preconstruction and construction activities, via developerinterfaceprojects@transport.vic.gov.au.
 7. Before the development starts (including demolition and bulk excavation), a Traffic Management Plan must be submitted to, and approved by, the Head of Transport for Victoria. The Traffic Management Plan must provide for:
 - a. How public transport operations, traffic, walking and cycling movements will be managed during the demolition and construction; and
 - b. How any traffic impact to the railway land and associated infrastructure assets will be mitigated.

The Traffic Management Plan must be implemented and complied with to the satisfaction of the Head, Transport for Victoria.

All costs associated with the preparation and implementation of the Traffic Management Plan will be at no cost to the Head, Transport for Victoria.

The endorsed Traffic Management Plan must not be modified without the prior written consent of the Head, Transport for Victoria.
 8. Before the development starts (including demolition and bulk excavation), separate Demolition and/or Construction Management Plan must be submitted to and approved by the Head, Transport for Victoria. When approved, the Demolition and Construction Management Plan will form a part of the permit. The Demolition and Construction Management Plan must include (but not be limited to) details of:
 - a. The buildings, works and other measures necessary to:

- i. Protect railway land, track, overhead power and associated infrastructure.
- ii. Prevent or minimise disruption to the operation of the railway.
- b. The remediation of any damage to railway land, track, overhead and underground power and communication assets, and associated infrastructure.
- c. Details of required access to the railway land during demolition and construction of the development with appropriate durations and schedules.
- d. Arrangements for:
 - i. Any hoarding associated with the construction of the development that encroaches onto or overhangs railway land.
 - ii. Piling, excavation, shoring, stabilising, anchoring, filling, earthworks or construction associated with the development occurring on or next to the boundary of the railway land.
 - iii. Crane location(s), slew radius and slew locking proposals
 - iv. The deposit or store of waste, fill or other materials associated with the development on the railway land.
 - v. Air and dust management.
 - vi. Operating hours.
 - vii. Noise and vibration controls.
 - viii. The management of site drainage, effluent and waste.
 - ix. The security of the railway land and associated infrastructure.

All demolition and construction works must be carried out in accordance with the approved Demolition and Construction Management Plan unless with the prior written consent of the Head, Transport for Victoria. The Demolition and Construction Management Plan must be prepared, implemented and monitored at no cost to the Head, Transport for Victoria.

The Demolition and Construction Plan must be consistent with any Construction Management Plan required by the Responsible Authority.

- 9. No drainage, effluent, waste soil or other materials must enter or be directed to railway land from the development site or be stored or deposited on railway land by the proponent.
- 10. Access to railway assets by rail staff for the purposes of inspection, cleaning, maintenance and repair shall be maintained at all times. Existing access routes to railway land shall not be closed, diverted or modified without prior agreement with the by the Head, Transport for Victoria and the relevant Rail Transport Operator(s).

The Head, Transport for Victoria also requests the following Notes be included on the permit:

The proposal requires works within the road reserve. Separate approval under the Road Management Act 2004 for this activity is required from the Head, Transport for Victoria prior to commencing any works within the road reserve. Please contact the Department of Transport and Planning prior to commencing any works.

Please refer to the following link for further information:

<https://transport.vic.gov.au/business/road-and-traffic-management/road-permits-and-charges/working-within-the-road-reserve>

It is noted to Council's consideration that there is no pedestrian visibility triangle provided at the southern side of the proposed access because of the adjacent building being built to boundary. It is required under Clause 52.06-9 – Design Standard for Car Parking. Alternative traffic solution should be considered to provide view of pedestrians on the footpath or alert pedestrians of outgoing vehicles.

Please forward a copy of any decision to this office as required under the *Planning and Environment Act 1987*.

Should you have any enquiries regarding this matter, please contact Nhi Nguyen on 03 98818057 or statutory.planning@transport.vic.gov.au

Yours sincerely,

A handwritten signature in black ink, appearing to read 'D. Singleton', with a long horizontal flourish extending to the right.

Dwayne Singleton

Team Leader Statutory Planning, Inner Metropolitan Region

Under delegation from the Head, Transport for Victoria, Department of Transport and Planning

15/07/2025

Cc: Permit applicant

6. DTP Applications

Nil.

7. Confidential Planning Decisions Committee Reports

Nil.