

Ordinary Meeting of Council Agenda

to be held on Tuesday 5 March 2019 at 7.00pm Fitzroy Town Hall

Arrangements to ensure our meetings are accessible to the public

Council meetings are held at either the Richmond Town Hall or the Fitzroy Town Hall. The following arrangements are in place to ensure they are accessible to the public:

- Entrance ramps and lifts (off Moor Street at Fitzroy, entry foyer at Richmond).
- Interpreting assistance is available by arrangement (tel. 9205 5110).
- Auslan interpreting is available by arrangement (tel. 9205 5110).
- A hearing loop is available at Richmond only and the receiver accessory is available by arrangement (*tel. 9205 5110*).
- Proposed resolutions are displayed on large screen.
- An electronic sound system amplifies Councillors' debate.
- Disability accessible toilet facilities are available at each venue.

Recording and Publication of Meetings

An audio recording is made of all public Council Meetings and then published on Council's website. By participating in proceedings (including during Public Question Time or in making a submission regarding an item before Council), you agree to this publication. You should be aware that any private information volunteered by you during your participation in a meeting is subject to recording and publication.

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Order of business

- 1. Statement of recognition of Wurundjeri Land
- 2. Attendance, apologies and requests for leave of absence
- 3. Declarations of conflict of interest (Councillors and staff)
- 4. Confidential business reports
- 5. Confirmation of minutes
- 6. Petitions and joint letters
- 7. Public question time
- 8. General business
- 9. Delegates' reports
- **10.** Questions without notice
- 11. Council business reports
- 12. Notices of motion
- 13. Urgent business

1. Statement of Recognition of Wurundjeri Land

"Welcome to the City of Yarra."

"Yarra City Council acknowledges the Wurundjeri as the Traditional Owners of this country, pays tribute to all Aboriginal and Torres Strait Islander people in Yarra and gives respect to the Elders past and present."

1A. Presentation to Officers in Receipt of Recent Awards

2. Attendance, apologies and requests for leave of absence

Anticipated attendees:

Councillors

- Cr Danae Bosler (Mayor)
- Cr Misha Coleman (Deputy Mayor)
- Cr Mi-Lin Chen Yi Mei
- Cr Jackie Fristacky
- Cr Stephen Jolly
- Cr Mike McEvoy
- Cr Daniel Nguyen
- Cr James Searle
- Cr Amanda Stone

Council officers

- Vijaya Vaidyanath (Chief Executive Officer)
- Ivan Gilbert (Group Manager Chief Executive's Office)
- Lucas Gosling (Director Community Wellbeing)
- Gracie Karabinis (Acting Group Manager People, Culture and Community)
- Chris Leivers (Director City Works and Assets)
- Diarmuid McAlary (Director Corporate, Business and Finance)
- Bruce Phillips (Director Planning and Place Making)
- Mel Nikou (Governance Officer)

3. Declarations of conflict of interest (Councillors and staff)

4. Confidential business reports

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- 4.1 Contractual matters; AND Proposed developments; AND Matters relating to legal advice
- 4.2 Contractual matters

Confidential business reports

The following items were deemed by the Chief Executive Officer to be suitable for consideration in closed session in accordance with section 89 (2) of the *Local Government Act* 1989. In accordance with that Act, Council may resolve to consider these issues in open or closed session.

RECOMMENDATION

- That the meeting be closed to members of the public, in accordance with section 89 (2) of the *Local Government Act* 1989, to allow consideration of:
 - (a) contractual matters;
 - (b) proposed developments; and
 - (c) matters relating to legal advice.
- 2. That all information contained within the Confidential Business Reports section of this agenda and reproduced as Council Minutes be treated as being and remaining strictly confidential in accordance with the provisions of sections 77 and 89 of the *Local Government Act* 1989 until Council resolves otherwise.

5. Confirmation of minutes

RECOMMENDATION

That the minutes of the Ordinary Council Meeting held on Tuesday 19 February 2019 be confirmed.

6. Petitions and joint letters

7. Public question time

Yarra City Council welcomes questions from members of the community.

Public question time procedure

Ideally, questions should be submitted to Council in writing by midday on the day of the meeting via the form available on our website. Submitting your question in advance helps us to provide a more comprehensive answer. Questions that have been submitted in advance will be answered first.

Public question time is an opportunity to ask questions about issues for which you have not been able to gain a satisfactory response on a matter. As such, public question time is not:

- a time to make statements or engage in debate with Councillors;
- a forum to be used in relation to planning application matters which are required to be submitted and considered as part of the formal planning submission;
- a forum for initially raising operational matters, which should be directed to the administration in the first instance.

If you wish to raise matters in relation to an item on this meeting agenda, Council will consider submissions on these items in conjunction with and prior to debate on that agenda item.

When you are invited by the meeting chairperson to ask your question, please come forward and take a seat at the microphone and:

- state your name clearly for the record;
- direct your questions to the chairperson;
- ask a maximum of two questions;
- speak for a maximum of five minutes;
- refrain from repeating questions that have been asked previously by yourself or others; and
- remain silent following your question unless called upon by the chairperson to make further comment or to clarify any aspects.

8. General business

9. Delegates' reports

10. Questions without notice

11. Council business reports

Item		Page	Rec. Page	Report Presenter
11.1	Yarra City Council and Neighbourhood Houses Partnership Strategy and Action Plan 2018 - 2021 Annual Progress Report	7	15	Aldo Malavisi - Community Partnerships Unit Manager
11.2	Draft Expression of Interest for the restoration and use of the building at 150- 152 Hoddle Street, Abbotsford	32	39	Michael Ballock – Executive Planner
11.3	Local Area Place Making Study - Scotchmer- North Fitzroy Precinct 3	89	105	Dennis Cheng – Manager Traffic and Engineering
11.4	Trial Mid-Block Road Closure on Gleadell Street	237	245	Dennis Cheng – Manager Traffic and Engineering
11.5	Mid-Year and 2019/20 Budget Review	248	251	Ange Marshall – Chief Financial Officer
11.6	Councillor Attendance at ALGA 2019 National Conference	252	253	Ivan Gilbert – Group Manager Chief Executive's Office

The public submission period is an opportunity to provide information to Council, not to ask questions or engage in debate.

Public submissions procedure

When you are invited by the meeting chairperson to make your submission, please come forward and take a seat at the microphone and:

- state your name clearly for the record;
- direct your submission to the chairperson;
- speak for a maximum of five minutes;
- confine your remarks to the matter under consideration;
- refrain from repeating information already provided by previous submitters; and
- remain silent following your submission unless called upon by the chairperson to make further comment.

12. Notices of motion

Nil

13. Urgent business

Nil

11.1 Yarra City Council and Neighbourhood Houses Partnership Strategy and Action Plan 2018 - 2021 Annual Progress Report

Executive Summary

Purpose

To provide the annual progress report for 2018 of the Yarra City Council and Neighbourhood Houses Partnership Strategy and Action Plan 2018 – 2021.

To seek the endorsement for the second year action plan (2019) of the Partnership Strategy and Action Plan 2018 – 2021 between Yarra City Council and Yarra Neighbourhood Houses.

Key Issues

The Memorandum of Understanding (MOU) and Partnership Strategy 2018 – 2021 define a shared vision for Council and the Houses where collaboration, trust and flexibility can achieve resilient and empowered communities in a rapidly changing environment. The original Memorandum of Understanding was developed following an extensive consultation process in 2007 with all Houses and various Council Units and endorsed by Council in 2008. The Memorandum of Understanding and Partnership Strategy is reviewed every four years in line with the Council Plan. The Memorandum of Understanding was reviewed in 2017 and endorsed by Council in February 2018.

All Houses continue to consolidate their strengths to address the impacts of the changing political, financial and demographic realities by developing new strategies and seeking new opportunities and partnerships to respond to the changes in positive and creative ways.

In moving forward the Yarra Neighbourhood House Network continues to seek collaborative projects, create safe and inclusive spaces, promote sustainability initiatives, provide employment pathways and develop a core message to promote neighbourhood houses, their purpose, value and community benefit to a new demographic.

Financial Implications

There are no financial implications from this annual progress report of the Memorandum of Understanding and the Partnerships Strategy 2018 – 2021.

In 2018/19 Council allocated \$694,535.17 to Yarra's nine Neighbourhood Houses through the Neighbourhood House Funding Program. The funding is based on the model that was adopted by Council in February 2012 and since reviewed in 2015.

PROPOSAL

That Council:

- (a) note the progress report for 2018 of the Neighbourhood Houses Partnership Strategy and Action Plan 2018 2021; and
- (b) endorse the second year action plan for 2019 of the Partnership Strategy and Action Plan 2018 2021.

11.1 Yarra City Council and Neighbourhood Houses Partnership Strategy and Action Plan 2018 - 2021 Annual Progress Report

Trim Record Number: D18/210474 Responsible Officer: Group Manager People, Culture and Community

Purpose

- 1. The purpose of this report is to:
 - (a) provide Council with the annual progress report for 2018 of the Yarra City Council and Neighbourhood Houses Partnership Strategy and Action Plan 2018 2021; and
 - (b) seek Council's endorsement for the second year action plan (2019) of the Yarra City Council and Neighbourhood Houses Partnership Strategy and Action Plan 2018 – 2021.

Background

- 2. Neighbourhood Houses play a vital role in building vibrant local communities. They are important places of inclusion and access and provide valuable social, recreational, educational and training opportunities that enable people to achieve their potential. They connect people in local communities through the vast volunteering opportunities they provide and the wide range of activities on offer.
- 3. The Victoria government provides recurrent funding to Neighbourhood Houses in accordance with Victorian Government priorities through the Neighbourhood House Coordination Program (NHCP) "to support the coordination of a service that provides community development activities consistent with the aims of the Neighbourhood House Coordination Program". (*Neighbourhood House Coordination Program Guidelines 2016–2019 and sector information*). Other sources of income include the Department of Education and Early Childhood Development, Local Government, the Federal Government, Philanthropic Organisations and funds generated by Neighbourhood Houses themselves.
- 4. Neighbourhood Houses are funded through the Department of Health and Human Services to support the provision of community development programs and activities that lead to community strengthening outcomes by:
 - (a) supporting diversity and promoting community participation and inclusion;
 - (b) facilitating community development and capacity building in support of individuals and groups within communities;
 - (c) supporting lifelong learning opportunities for people to improve their access to training and employment pathways; and
 - (d) undertake community development processes to address locally identified priorities and needs.
- 5. Community development practice is based on the premise that communities have strengths and assets.
- 6. Additionally Neighbourhood Houses in Yarra are governed by independent and volunteer Governance Committees and run by paid, staff and volunteers.
- 7. In February 2018, Council formally adopted the Memorandum of Understanding 2018 2021 (MOU). Council also adopted the Partnership Strategy and Action Plan 2018 2021 with Yarra's nine Neighbourhood Houses (the Houses) in February 2018. The Strategy and Action Plan sets out four key areas for action in line with the Council Plan 2017-2021 with actions that support and strengthen the collaborative partnership between Council and the Houses. The nine Houses that are signatories to the MOU Strategy and Action Plan are:
 - (a) Alphington Community Centre;
 - (b) Belgium Avenue Neighbourhood House;

- (c) Carlton Neighbourhood Learning Centre;
- (d) Collingwood Neighbourhood House;
- (e) Finbar Neighbourhood House;
- (f) Fitzroy Learning Network;
- (g) Holden Street Neighbourhood House;
- (h) North Carlton Railway Neighbourhood House; and
- (i) Richmond Community Learning Centre.
- 8. The Strategy and Action Plan confirms and clarifies roles, relationships, partnering opportunities and mutual benefits of a more coordinated, collaborative approach between the Houses and Council. It reflects the strong working relationships of the Houses with a range of Council's branches e.g., Library Services, Open Space Planning and Design Unit, Waste Minimisation, Environment Management Unit, Strategic Transport, Building and Property Management and Maintenance, Communications, Emergency and Risk Management, Leisure Services, Social Policy & Research, Aged and Disability Services and Family, Youth and Children's Services.
- 9. The Strategy and Action Plan is an active document that provides a collaborative environment for the Houses to work in partnership with the various units/branches across Council to deliver appropriate and responsive programs and activities to Yarra residents. The Action Plan is reviewed annually to maintain flexibility.

Achievements of the Partnerships Strategy and Action Plan for 2018

- 10. All Houses delivered on the four key action areas from the Strategy and Action Plan for 2018. (Attachment One). The following are some of the highlights of 2018:
 - (a) Action Area 1: A Healthy Yarra: Community Health, Safety and Well-being:
 - (i) Fitzroy Learning Network (FLN) in partnership with Yarra Libraries and other local agencies delivered the "Community Wellbeing "event at the Atherton Gardens estate to promote awareness and information on mental health and well-being issues and support services. In addition FLN continues to provide a range of youth programs in partnership with Yarra's Youth Services to promote participation and engagement and support young people to reach their full potential;
 - (ii) Belgium Avenue Neighbourhood House (Belgium Avenue) and Collingwood Neighbourhood House (Collingwood) continue to activate the "Gathering Place" at the two estates to provide safe spaces for residents to connect, engage and share experiences and create a sense of belonging. Both Houses offer a range of health and wellbeing programs and activities, such as yoga and relaxation and healthy eating workshops, to engage residents from the Housing estates to participate, connect and improve their health outcomes;
 - (iii) Collingwood continues to successfully provide music performances and family events such as rollerblading in the Collingwood underground carpark for the residents of the estate as well as for the broader community. The underground carpark is an important space for promoting arts and culture to the local community, providing an inclusive environment and engaging with a diverse community to provide a sense of belonging and safety;
 - (iv) North Carlton Railway Neighbourhood House (Railway House) continue to strengthen partnerships with local schools, to engage families, children and young people in a range of skills based and peer support activities. The House in partnership with Yarra Youth Services provides a program where young people have the opportunity to participate and engage in healthy eating, fitness and sporting activities and mental wellbeing in a safe and supported environment;

- (v) Alphington Community Centre (Alphington) in collaboration with the Yarra Libraries provided the Book Bike and Storytime program for young families. In addition Alphington provides ongoing playgroups to support, resource and connect families in the local area. Alphington also provides regular workshops on health and wellbeing for example "improve your gut health". The Alphington Men's shed continues to provide activities to help men maintain connections to community, for skills development and to promote health and wellbeing;
- (vi) Carlton Neighbourhood Learning Centre (CNLC) provides a range of health and wellbeing programs to newly arrived communities to improve their sense of belonging and connection to community through programs such as "Stories of Food" where students learn English by sharing their personal recipes for food and cooking. Participants are able to connect through food, develop cross cultural understanding and improve their physical and mental wellbeing. This project was awarded the "Innovative Community Development Initiative" prize by the North East Neighbourhood House Network region;
- (vii) Richmond Community Learning Centre (RCLC) provides a range of health and wellbeing activities for babies, children and parents such as playgroups, music activities, yoga and relaxation to support healthy and engaged families;
- (viii) Holden St Neighbourhood House (Holden St) is delivering 'After Dark" classes to expand access and inclusion to a broader demographic and to residents who may not be able to attend programs during business hours. Residents have opportunities to access a safe space, participate in programs to improve their social connections and health outcomes;
- (ix) All Neighbourhood Houses are exploring the concept of creating "safe spaces" to welcome and support people experiencing family violence. Managers from several Houses attended training on "Identifying Family Violence" organised through the Northern Integrated Family Violence Services. The information was shared with all Managers via the Neighbourhood House Network; and
- (x) All Houses participated in two presentations (February and April 2018) on Results Based Accountability (RBA) by Council's Coordinator, Community Planning. RBA is an assessment tool for programs and activities provided by Houses. In addition individual Houses organised follow up sessions for their specific staff and committee members to further understand and implement the RBA tool.
- (b) Action Area 2: An inclusive Yarra: Inclusion, diversity and uniqueness are welcomed, respected and celebrated:
 - (i) "From the Heart" a special community event was presented by Yarra's Neighbourhood House Network (the Network) in partnership with Council, Yarra Libraries, Ewing Trust, Neighbourhood Justice Centre, Bendigo Bank and Dimattina to celebrate Sustainability Week 2018. With over 400 residents attending, the event showcased the work of all nine Neighbourhood Houses. Activities on the day included, choirs, arts and crafts, sustainability activities such as composting, children's activities, health and wellbeing activities, information on adult learning programs, and barista services provided by VCAL students. This is an annual event that promotes the community development work and programs of all the Houses to the Yarra residents;
 - (ii) Collingwood and Belgium Ave both delivered festivals and events across the two estates such as the Lunar Festival, Harvest Day Festival, Moon Lantern Soiree and LGTB event- "Love is in the park" and the annual Gala Ball. These popular and inclusive events acknowledge and celebrate the diverse and unique communities living in Yarra;
 - (iii) Fitzroy Learning Network (FLN) in partnership with Council, Yarra Libraries, Ewing Trust, Victorian Multicultural Commission, Cultivating Communities and Fitzroy Legal Service, organised "Bridges2Harmony" in Condell Reserve, Fitzroy. This annual community event promotes cross cultural diversity and celebration,

community connection and engagement for the diverse communities in Fitzroy. Approximately 800 residents attended the event;

- (iv) Belgium Ave, RCLC, and CNLC partnered with Yarra Libraries to once again deliver "Community Connections Day" to cross promote activities and services provided by the Houses and Libraries. The Library Coordinator, Community Learning and Partnerships and Community Programs Librarian continue to meet with individual Houses to promote and support specific House projects and events via the Library newsletter and on social media; and
- (v) Holden Street in collaboration with Yarra Libraries organised the "Movies in the Park" attended by approximately 500 residents. Prior to the movie, participants were able to engage in a range of sustainability activities, such as composting, recycling, art and craft, children's activities and music performances. This annual diverse and cross generational event brings together the local community to engage, participate and contribute;
- (c) Action Area 3: A sustainable Yarra: Council leads on sustainability and protects and enhances its natural environment:
 - (i) Finbar in collaboration with Yarra Libraries delivered its annual Urban Field Day to promote its community garden activities, food and seed sharing project, composting and worm farming, children's activities and health and wellbeing activities to celebrate, connect and engage with the local community. In addition Finbar was the Yarra Sustainability Awards winner for 2018 for the "Community Action: Urban Regeneration Project;
 - (ii) Additionally Finbar in partnership with Yarra's Sustainability Engagement Officer are delivering the "Worm Farming" project to local residents living in a nearby apartment complex to improve knowledge and access to composting and organic gardening. Three large wheelie bin worm farms have been installed on the rooftop of the complex with ongoing support and education on how to operate the worm farms;
 - (iii) RCLC has delivered the popular "Fruit Squad" project via Burnley Backyard to engage the local community in harvesting excess fruit, promote social connections, food sharing, sustainability and reduce food waste and landfill;
 - (iv) RCLC have also partnered with Finbar to develop their knowledge and skills in establishing a compost bucket exchange program based on the Finbar model. The two Houses are also partnering to design and introductory course to "Permaculture and Urban Agriculture" to engage and educate the local residents and improve their capacity for sustainable gardening;
 - (v) Railway House in partnership with the local secondary school has delivered the VCAL (Victorian Certificate of Applied Learning) Gardening Project. The VCAL students have the opportunity to learn about food growing, composting, worm farming and reducing food waste and landfill. The program provides a strong sense of inclusion and belonging where the young people have opportunities to engage positively, learn and participate;
 - (vi) CNLC integrates the Gardening and Food programs with its adult learning programs to support their learning objectives. The kitchen garden provides fresh, free produce to the students and for the community luncheons hosted in partnership with Open Table; and
 - (vii) Holden St in partnership with Council's Urban Agriculture Officer is undertaking fruit tree planting in the adjacent park. This project is an extension of the many sustainability programs offered at the House such as the rain water garden, compost bins, worm farms, food growing, waste minimisation and recycling drop off points. The sustainability programs are integrated with the learning and education programs to increase participants' knowledge skills and awareness. In addition Holden Street provides a "Repair and Share" program to provide

opportunities for developing skills in repairing household items, reducing landfill and connecting with other participants.

- (d) Action Area 4: A prosperous Yarra: Local businesses prosper and creative and knowledge industries thrive:
 - (i) The Network engaged with Council's Economic Development Unit to develop an understanding of the services provided, and explore how the Houses and participants can access these resources and services. The Network and individual houses will continue to explore the potential for partnership opportunities with activities carried out by the Economic Development Unit in 2019.
- 11. Yarra's Neighbourhood Houses are unique in creating spaces and opportunities for people to connect, learn, participate and contribute in their local community through social, educational, recreational and support activities that enable people to achieve their potential, using a unique community development approach.
- 12. All the nine Houses respond to locally identified needs and priorities and each is as diverse as the community it reflects. According to the neighbourhood houses survey for 2017 by the peak body Neighbourhood Houses Victoria (NHVic), approximately 3,800 people visited a Yarra Neighbourhood House in an average week. This included students, volunteers, children in child care or playgroups, people from other organisations participating in training/meetings and "drop-ins". Approximately 85% of the participants were involved in programed activities.
- 13. All nine Houses provide a range of activities that meet the needs of the local community such as accredited vocational education & training, art and craft, children's activities and child care, community choirs, community events and lunches, digital literacy, English language classes, sustainability projects, health and wellbeing classes, men's shed, seniors and youth groups as well as programs for refugee and recently arrived communities.
- 14. In February 2018 several Houses were visited by the Mayor, Cr Daniel Nguyen. The Houses visited were: Fitzroy Learning Network, North Carlton Railway House, Carlton Neighbourhood Learning Centre, Finbar Neighbourhood House, and Holden St Neighbourhood House. This is an annual activity for the new Mayor to become familiar with the programs and activities delivered at the Houses and connect with the participants.

External Consultation

15. The Action Plan is reviewed annually to maintain flexibility. The review process was facilitated by the Manager of the North East Neighbourhood House Network (NENHN) in November 2018 and involved all House Managers, staff and representatives from their respective Governance Committees. The final document has been endorsed by all the Houses and Committees of Governance (Attachment Two).

Internal Consultation (One Yarra)

16. Internal consultation for the review of the Action Plan was undertaken with Council Officers from various Branches including: Community Partnerships, Library Services, Aged and Disability Services, Open Space Planning and Design, Sustainability, Waste Minimisation, Urban Agriculture and Economic Development. The review process was coordinated by Officers from the Community Partnerships Unit who have the major responsibility for managing the relationship with the nine Houses. The implementation of the Action Plan 2019 is co-monitored by the Network and Council's Community Partnerships Unit.

Financial Implications

- 17. There are no financial implications from the annual progress report of the Action Plan 2018 or from the Action Plan 2019.
- 18. In 2018/19 Council allocated \$694,535.17 to Yarra's nine Neighbourhood Houses through the Neighbourhood House Funding Program. The funding is based on the model that was adopted by Council in February 2012 and reviewed in October 2015.

- 19. Further to the Neighbourhood House Funding Program, Council has a capital responsibility as it is the landlord to four of the Houses: Holden Street Neighbourhood House, North Carlton Railway Neighbourhood House, Richmond Community Learning Centre, Burnley Backyard and landlord to the land adjacent to Belgium Avenue Neighbourhood House on which a portable is located and used to provide a range of activities. These financial responsibilities of Council as landlord will not change.
- 20. As a result of a Statewide campaign: "We deliver, Will you?" for additional funding the State Government allocated \$21.8 million to the Neighbourhood House sector over the next four years. This has resulted in RCLC having increased funding from 20 hours to 25 hours per week through the Neighbourhood House Coordination Program (NHCP).

Economic Implications

- 21. The direct and indirect economic implications that Yarra's Houses have on the Yarra community and businesses are primarily around the adult education that occurs within the Houses. This provides opportunities for adults to learn new skills and creates pathways for transition into the workforce, education or further training. This includes the English classes, programs and activities that assist and support newly arrived communities to participate and engage fully in their local community.
- 22. In addition, the Houses provide a variety of full and part time employment for teachers, childcare workers, administration, project workers, community development workers, arts wellbeing practitioners, financial and IT staff that are important sources of local employment.
- 23. Yarra's Houses have a high volunteer participation rate. This contribution to Yarra's economy is significant in terms of economic and social capital.

Sustainability Implications

- 24. The Strategy and Action Plan have an important role in continuing to strengthening the relationship between the Houses and Council's Open Space Planning and Design, Waste Minimisation and Urban Agriculture and Sustainability Units in educating and supporting Houses in their sustainability initiatives.
- 25. Carlton Neighbourhood Learning Centre's (CNLC) multicultural food garden, workshops in the living classroom and Open Table Feasts encourage knowledge on growing food, composting, minimising landfill and provide a gathering place for sharing and learning skills for sustainable living. The "Stories of Food" project that integrates learning English with growing and cooking food was an award winning community development project.
- 26. Finbar continues to expand its Community Garden Project in collaboration with Council's Sustainability Engagement Officer. The House, with the support of Council, is supporting local residents living in a nearby apartment complex to learn about worm farming by helping to install large wheelie bin worm farms. In addition the House has installed three aerobins in the front garden to increase community awareness and access to composting. The liquid fertiliser from the aerobins is shared with the local community. Further to this, the House has set up collection bins for the community to dispose of their 'e-waste', such as mobile phones, batteries and help reduce landfill. The House also offers regular workshops on organic and sustainable gardening to educate and inspire the local community to start their own gardens. Finbar delivers an "Urban Field Day" event promoting a range of sustainability activities to the local community such as composting, worm farming, waste minimisation, recycling and how to grow organic gardens.
- 27. Holden St continues to be a demonstration House for the local community with its many environmental and sustainability activities for reuse, recycling and limiting energy consumption. The House is part of the 'Sustainable Living' Festival with its many environmental features, such as solar panels and lights, water tanks, permaculture and rainwater gardens as well as herb gardens and worm farms. In addition the House provides gardening and preserving workshops and has a produce swap program. The House also has a "Repair and Share" program where participants learn skills in repairing household items and reducing landfill.

- 28. RCLC in partnership with Council's Sustainability Engagement Officer runs the volunteer "Burnley Fruit Squad" to harvest excess fruit from local residents, minimise food waste, promote sustainable food initiatives, share fresh fruit with the community and help improve food security for vulnerable groups in Yarra.
- 29. Alphington continues to strengthen its composting and gardening initiatives with support from local groups to establish a new market garden and an urban orchard in addition to providing sustainability workshops, food preserving and participating in the recently stablished Alphington Farmers Market.
- 30. Railway House is actively promoting recycling practices to its participants and to the local community to improve knowledge on the correct disposal of rubbish and recyclable items. The House also provides workshops with a local artist for children using recyclable items for art activities. In addition the Gardening Group provides workshops on plant selection, care and maintenance for small urban spaces and is engaging with the local VCAL students to teach them about composting, herb growing and tree planting.
- 31. Additionally the Gardening Group is engaged in discussions with Council Officers to redevelop the disused bocce courts pavilion into a shared community garden. In March 2018 Council held a community consultation at Railway House on redevelopment of the bocce courts. Following a report to Council in June 2018, Councillors agreed to develop the disused bocce courts into a shared growing space and retain one lane for bocce. The Gardening Group has developed a draft design for the site and is currently engaged in discussions with Council Officers from Waste Minimisation and Urban Agriculture, Open Space Planning and Design and Community Partnerships. Once agreement has been reached on the draft design, it will then go out for consultation.

Social Implications

- 32. The Houses play a key role in community development through: building knowledge, developing skills, increasing levels of resilience, mutuality and trust within the community. The Houses have a track record in:
 - (a) community building and strengthening through cultural events and festivals celebrating and showcasing Yarra's culturally and linguistically diverse communities (CALD);
 - (b) providing appropriate and responsive community services: for children, young people, the elderly, LGTB communities, people with disabilities, CALD, refugee and newly arrived and disadvantaged communities;
 - (c) providing opportunities to improve community health and well-being and social connectedness, through a range of exercise programs and community gym memberships; and
 - (d) Offering accessible and affordable education, skills development, life-long learning and training opportunities and employment pathways.

Human Rights Implications

33. The Strategy and Action Plan recognises and respects the *Charter of Human Rights and Responsibilities Act* 2006 by actively supporting the Charter's substantive rights through the programs and activities offered by the Houses.

Communications with CALD Communities Implications

- 34. All Houses deliver a range of programs, services and activities to CALD communities across Yarra. There are no implications for the way the Houses communicate, engage with or deliver services to their CALD communities.
- 35. As part of their core business, all Houses, engage with CALD, refugee and newly arrived communities through educational programs, social activities such as community lunches and cultural celebrations such as Refugee Week, Cultural Diversity events and Lunar New Year.

36. Some Houses also have individuals from CALD backgrounds on Committees of Governance, acting as facilitators for specific groups such as playgroups, and leading specific community development projects such as supporting African Women's Groups. Houses also support a wide variety of ethno-specific groups by auspicing their activities.

Council Plan, Strategy and Policy Implications

- 37. The Partnerships Strategy and Action Plan 2018 2021 relates to the new Council Plan 2017 2021 via the following strategic objectives:
 - (a) A healthy Yarra: Community Health, Safety and well-being are a focus in everything we do;
 - An inclusive Yarra: Inclusion Diversity and Uniqueness are welcomed respected and celebrated;
 - (c) A sustainable Yarra: Council leads on sustainability and protects and enhances its natural environment; and
 - (d) A prosperous Yarra: Local businesses prosper and creative and knowledge industries thrive.

Legal Implications

38. The Strategy and Action Plan is not legally binding on either or both parties.

Other Issues

39. Council continues to provide publicity and marketing support to Houses by including articles on programs, services, activities and events delivered by Houses in Yarra News, Yarra website and social media, to improve knowledge and access to more diverse groups.

Conclusion

- 40. The Strategy and Action Plan acknowledges the valuable relationship between Council and the Houses and provides a framework of trust and collaboration to deliver programs and activities that enable people to achieve their full potential and benefit the Yarra community.
- 41. The Strategy and Action Plan defines a shared vision for Council and the Houses, where collaboration, trust and flexibility can achieve healthy, creative, connected and resilient communities in a rapidly changing environment.
- 42. All Houses continue to build on their strengths to address the impacts of the changing political, financial and demographic realities by developing new strategies and seeking new opportunities and partnerships to respond to the changes in positive and creative ways.
- 43. The Network continues to seek out partnership projects, engage with diverse communities, develop a core message to promote neighbourhood houses, their purpose, value and community benefit to a new demographic and increase capacity to accommodate the increasing and changing community needs.
- 44. The Partnership Strategy and Action Plan 2018 2021 is based on the strategic objectives of the Council Plan which demonstrates the level of commitment by the Houses to work in partnership with Council.

RECOMMENDATION

- 1. That Council:
 - (a) note the progress report for 2018 of the Yarra City Council and Neighbourhood Houses Partnership Strategy and Action Plan 2018 – 2021; and
 - (b) endorse the Action Plan 2019 of the Yarra City Council and Neighbourhood Houses Partnership Strategy and Action Plan 2018 – 2021.

CONTACT OFFICER:	Katherine Vrettas
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Attachments

- 1. Yarra City Council and Neighbourhood House Partnership Strategy and Action Plan Year 1 Actions 2018
- 2. Yarra City Council and Neighbourhood House Partnership Strategy and Action Plan Year 2 Actions 2019

Attachment 1 - Yarra City Council and Neighbourhood House Partnership Strategy and Action Plan - Year 1 Actions 2018

Yarra City Council and Neighbourhood Houses Partnership Strategy and Action Plan 2018 – 2021 Year 1 Actions – 2018 - All Houses

Areas for Action - 2018

1. A healthy Yarra: Community Health, Safety and Well-being

Community well-being is the combination of social, economic, environmental, cultural and political conditions that enable people to flourish and fulfil their potential.

Area		Action	Who	Budget	Outcomes/Outputs	Time Frame
NH Programs	1.1	Houses to learn about Results Based Accountability (RBA) and Collective Impact Framework (CIF).	Cheryle Grey	existing	Two workshops were held in March/April on RBA- all Houses attended. Individual Houses organised RBA session for staff & committee members.	February/April 2018
Partnerships	1.2	Houses and Yarra Libraries to work together to build and strengthen relationships and increase collaborative activities.	All Houses Yarra Libraries	existing	Yarra Library staff member attended Yarra Neighbourhood House (NH) Network meetings. One shared activity per year with each House: Finbar – Urban Field Day Fitzroy Learning Network (FLN) – Bridges2Harmony Richmond Community Learning Centre (RCLC) - Community Connections Day at Richmond estate. Holden Street – Movies in the Park. Alphington Community Centre (ACC) – Book bike and story time.	2018

Attachment 1 - Yarra City Council and Neighbourhood House Partnership Strategy and Action Plan - Year 1 Actions 2018

Yarra City Council and Neighbourhood Houses Partnership Strategy and Action Plan 2018 – 2021 Year 1 Actions – 2018 - All Houses

Consultations	1.3	Houses to participate in the consultation process for the development of Council's key strategies e.g. Volunteering, open spaces, urban agriculture	Community Planner to inform Network of Council consultations	existing	Nth Carlton, Carlton Neighbourhood Learning Centre (CNLC), Collinwood & Belgium Ave – From the Heart Community Event. Houses contributed to relevant council consultations for key strategies: Urban Agriculture, Open Space, Access & Inclusion, Multicultural Partnerships Plan and Volunteer	2018 -ongoing
Open spaces	1.4.	Council to work with Houses to activate small scale, under-utilised open spaces to foster opportunities for local connection.	All Houses Council Open space Unit	existing	Strategy. Nth Carlton Gardening Group identified Bocce Courts to develop a community garden. Bridges2Harmony in Condell Reserve, Fitzroy. Suitcase Rummage Sale - Alphington. Planting fruit trees in Holden St Reserve. Herb Garden at Alphington garden space. Compost social enterprise at CNLC. Movies in the Park at Holden St Reserve. Perry Park Gathering Space at Collingwood Neighbourhood House (CNH).	2018 - ongoing

Attachment 1 - Yarra City Council and Neighbourhood House Partnership Strategy and Action Plan - Year 1 Actions 2018

Yarra City Council and Neighbourhood Houses Partnership Strategy and Action Plan 2018 – 2021 Year 1 Actions – 2018 - All Houses

2. An inclusive Yarra: Inclusion, diversity and uniqueness are welcomed, respected and celebrated

Creating a safe place where people of all ages, genders, sexualities, abilities, cultures and backgrounds feel welcome and connected is a priority for Council.

Area		Action	Who	Budget	Outcomes/Outputs	Time Frame
Gathering spaces/ celebration	2.1	Houses and Council to actively collaborate in support, promotion and delivery of on-going celebrations and festivals e.g. Harvest Festival, Harmony Day, Sustainability Week, fetes.	All Houses Council Community Planner Communications Unit	existing	FLN- Bridges2Harmony Festival. CNH - Lunar Festival, Harvest Day Festival. Moon lantern Soiree, Music by Candlelight, Love is in the Park LGBTI event at Collingwood Underground carpark, Roller Disco. Finbar-Urban Field Day. All Houses - From the Heart Festival. Holden St - Movies in the Park. FLN - Community Well Being Day Atherton Gardens. CNLC - Harmony Day, Tower Cinema ACC - Dog Show, Community Fete, Seniors Month.	2018
	2.2	Houses and Council to partner in an annual Neighbourhood House Network event e.g. Music from the Heart and identify opportunities to build on the success of past events.	All Houses Community Planner	existing	From the Heart Community Event that celebrated all the Yarra Houses in	September 2018

Attachment 1 - Yarra City Council and Neighbourhood House Partnership Strategy and Action Plan - Year 1 Actions 2018

Yarra City Council and Neighbourhood Houses Partnership Strategy and Action Plan 2018 – 2021 Year 1 Actions – 2018 - All Houses

					partnership with Council /Yarra Libraries, Ewing Trust, Neighbourhood Justice Centre (NJC), Bendigo Bank and Dimattina.	
Skills Developing/ Skills Harnessing	2.3	Houses to learn about Asset Based Community Development	Cheryle Grey	existing	A workshop was presented in March 2018 - attended by all Houses and committee members.	March 2018
Safe Spaces	2.4	Identify professional development for neighbourhood staff and volunteers on how to listen and provide information about what is available in the community. Explore options for council to support professional development for example: Mental Health First Aid training, Common Risk Assessment Framework (CRAF). e.g. Link in with Council's Skills Training, Women's Health in the North or other networks	All Houses North East Neighbourhood House Network (NENHN)	existing	"Identifying Family Violence" training through Northern Integrated Family Violence Services - Staff from three neighbourhood houses attended.	June and August 2018

Attachment 1 - Yarra City Council and Neighbourhood House Partnership Strategy and Action Plan - Year 1 Actions 2018

Yarra City Council and Neighbourhood Houses Partnership Strategy and Action Plan 2018 – 2021 Year 1 Actions – 2018 - All Houses

3. A sustainable Yarra: Council leads on sustainability and protects and enhances its natural environment

Protecting our natural environment and supporting our community to reduce its environmental footprint will continue to be a priority for Council.

Area		Action	Who	Budget	Outcomes/Outputs	Time Frame
Information and education	3.1	Neighbourhood houses to generate list and description of all sustainability activities and share with sustainability officers at council – include information about demonstration sites e.g. low water use gardens and learning clubs and best practice compost.	All Houses Council Sustainability Unit	existing	Improved sharing of information on sustainability activities in all Houses shared From the Heart Event. Sustainability activities in Houses listed on Council Webpage eg, recycling drop off points.	2018 - ongoing
	3.2	Council's Living Sustainably pages to link to neighbourhood house sustainability initiatives and programs	All Houses Council Communications Unit	existing	Neighbourhood Houses profiled in Council's "Living Sustainably" page.	2018 - ongoing
Reduce waste to landfill	3.3	Council to support neighbourhood houses to be recycling drop-off points (for nonorganic and hard waste items) e.g. mobile phones and batteries	All Houses Council Sustainability Unit Waste Management Unit	existing	Council Sustainability and Waste Management Units support Houses to set up compost bins and worm farms, recycling drop off points, fruit squads, food/seed swaps, urban field days, reducing food waste programs.	2018 - ongoing
	3.4	Develop Neighbourhood Composting Education and engagement (e.g. War on Waste screening)	All Houses Council Waste Management	existing	Education programs offered at Alphington, Finbar, Richmond,	2018 - ongoing

Attachment 1 - Yarra City Council and Neighbourhood House Partnership Strategy and Action Plan - Year 1 Actions 2018

Yarra City Council and Neighbourhood Houses Partnership Strategy and Action Plan 2018 – 2021 Year 1 Actions – 2018 - All Houses

			Unit		Carlton, Nth Carlton, Holden St, to reduce landfill waste. New Compost Hub at Carlton.	
Facilitate Urban Agriculture	3.5	Continue to strengthen the relationship between neighbourhood houses and Urban Agriculture at Council and explore urban agriculture initiatives e.g. community orchard, fruit squad, food swap, seed swap	All Houses Sustainability Unit	existing	Urban Agriculture Unit supports all Houses with:- Fruit Squad activities, food/seed swap, composting/worm farms, gardening/food production, recycling/waste management,	2018 - ongoing

4. A prosperous Yarra: Local businesses prosper and creative and knowledge industries thrive

Supporting local businesses and creative industries not only contributes to Yarra's economy, but also increases employment opportunities, enhances street life and fosters community connectedness.

Area		Action	Who	Budget	Outcomes/Outputs	Time Frame
Employment pathways and micro business and social enterprises for economically disadvantaged.	4.1	Meet with Economic Development Unit from council to understand what services they provide, and how neighbourhood house participants can access these resources and services. Explore the potential for partnership opportunities e.g. Houses to promote or host council's Lunchbox Learning series of seminars.	Economic Development Unit Neighbourhood House Network	existing	Increased understanding by the Houses of the services provided by Council's Economic Development Unit.	2018 - ongoing

Attachment 2 - Yarra City Council and Neighbourhood House Partnership Strategy and Action Plan - Year 2 Actions 2019

Yarra City Council and Neighbourhood Houses Partnership Strategy and Action Plan 2018 – 2021 Year 2 Actions – 2019

Yarra City Council and Neighbourhood Houses Partnerships Strategy & Action Plan 2018 - 2021

Introduction

This partnership strategy was formulated by Yarra Neighbourhood Houses and representatives of Yarra City Council. It provides a partnership approach to addressing identified needs in the community. By working collaboratively, all parties can draw on each other's strengths and leverage resources.

Yarra is home to 89 710 people. About 19% of Yarra residents were born in non-English speaking countries and 23% speak a language other than English at home. The municipality is undergoing rapid change with many new housing developments and the population is expected to grow by 30% over the next 20 years.

The Yarra Neighbourhood Houses and Yarra City Council have a mutual goal to engage with the Yarra community in determining the provision of equitable recreational, social, vocational training environmental and other health and well-being opportunities. In order to work in a truly collaborative manner Yarra City Council and Yarra Neighbourhood Houses signed a Memorandum of Understanding (MOU). A key element of the MOU is the Annual Action Plan. Yarra City Council and Yarra Neighbourhood Houses have developed this Action Plan based on the objectives in the Council Plan 2017 – 2021. Priorities for collaborative work were identified together and actions were developed.

Annual action plan follows this will be reviewed annually to maintain flexibility.

Vision

Council and Neighbourhood Houses collaborating with trust and respect to foster a cohesive, resilient, empowered and engaged community.

Partnership Structure

The following mechanisms are in place to support the partnership to achieve its vision.

- Memorandum of Understanding
- Neighbourhood House Funding
- Annual Planning day with Yarra City Council and Neighbourhood Houses
- Monthly meetings of the Yarra Neighbourhood House Network

Four of the objectives of the Council Plan 2017 – 2021 were chosen as a focus for the Council and Neighbourhood House Strategy. These objectives are:

- Community Health, Safety and well-being are a focus in everything we do
- Inclusion Diversity and Uniqueness are welcomed respected and celebrated
- Council leads on sustainability and protects and enhances its natural environment

Attachment 2 - Yarra City Council and Neighbourhood House Partnership Strategy and Action Plan - Year 2 Actions 2019

Yarra City Council and Neighbourhood Houses Partnership Strategy and Action Plan 2018 – 2021 Year 2 Actions – 2019

- Local businesses prosper and creative and knowledge industries thrive
- 1. Community Health, Safety and Well-being

Community well-being is the combination of social, economic, environmental, cultural and political conditions that enable people to flourish and fulfil their potential.

Neighbourhood Houses provide opportunities to connect people across cultural and socioeconomic divides thus building bridges between diverse groups. However, there is scope to expand the reach of neighbourhood houses. This can be achieved by reviewing activities and events that neighbourhood houses already run and plan ways to increase participation from diverse groups.

2. Inclusion, diversity and uniqueness are welcomed, respected and celebrated

Creating a safe place where people of all ages, genders, sexualities, abilities, cultures and backgrounds feel welcome and connected is a priority for Council.

Neighbourhood houses already make a significant contribution to this objective of the Council Plan through:

- Gathering spaces
- Cultural groups
- Food sharing
- Social enterprise development
- Mentoring
- Accessible education programs
- Family violence/ elder abuse awareness campaigns
- Celebrations and events that foster cultural exchange
- Provision of safe spaces

Attachment 2 - Yarra City Council and Neighbourhood House Partnership Strategy and Action Plan - Year 2 Actions 2019

Yarra City Council and Neighbourhood Houses Partnership Strategy and Action Plan 2018 – 2021 Year 2 Actions – 2019

Areas for Action – 2019

1. A healthy Yarra: Community Health, Safety and Well-being

Community well-being is the combination of social, economic, environmental, cultural and political conditions that enable people to flourish and fulfil their potential.

Area		Action	Who	Budget	Outcomes/ Outputs	Time Frame
NH Programs	1.1	Neighbourhood houses to learn about Results Based Accountability (RBA) and Collective Impact Framework(CIF)	Cheryle Grey (Services & Planning Unit)	existing	Training on Collective Impact to be organised for all Houses and Management Committees.	March 2019
			Sue Kent (Belgium Ave)		Belgium Ave to develop a case study of how RBA is applied to programs and to share with the Network.	August 2019
			North East Neigh'hood House Network (NENHN)		Houses to participate in the NENHN Evaluation Community of Practice to develop skills in applying RBA to programs.	May 2019
Partnerships	1.2	Neighbourhood Houses and Yarra Libraries to work together to build and strengthen relationships and increase collaborative activities.	All Houses Yarra Libraries	existing	Share calendar of House events for the year to promote on Council and Yarra Libraries social media. Library staff to meet with Individual Houses to support and promote House program/events.	2019
Consultations	1.3	Neighbourhood Houses to participate in the consultation process for the development of Council's key strategies eg Volunteering, open spaces, urban agriculture	Council All Houses	existing	Council to explore running face to face consultation at Houses to reach particular cohorts that may be under- represented in	2019

Attachment 2 - Yarra City Council and Neighbourhood House Partnership Strategy and Action Plan - Year 2 Actions 2019

Yarra City Council and Neighbourhood Houses Partnership Strategy and Action Plan 2018 – 2021 Year 2 Actions – 2019

					consultations. Houses to continue to make strategies available to community members in hard copy and through on line communication.	2019
Open spaces	1.4	Council to work with Houses to activate small scale, under-utilised open spaces to foster opportunities for local connection.	All Houses Council Open Space Unit	existing	"Greening streets"- Investigate community growing spaces near Houses. Council sustainability Unit to support Houses in navigating the requirements to set up raised beds on nature strips, at the end of Courts and No-Through roads as gathering spaces. Explore how to further develop housing estate open spaces in collaboration with Office of Housing.	2019

Attachment 2 - Yarra City Council and Neighbourhood House Partnership Strategy and Action Plan - Year 2 Actions 2019

Yarra City Council and Neighbourhood Houses Partnership Strategy and Action Plan 2018 – 2021 Year 2 Actions – 2019

2. An inclusive Yarra: Inclusion, diversity and uniqueness are welcomed, respected and celebrated

Creating a safe place where people of all ages, genders, sexualities, abilities, cultures and backgrounds feel welcome and connected is a priority for Council.

Area		Action	Who	Budget	Outcome/ Outputs	Time Frame
Gathering spaces/ celebration	2.1	Houses and Council to actively collaborate in support, promotion and delivery of on-going celebrations and festivals eg Harvest Festival, Harmony Day, Sustainability Week, fetes.	All Houses Council Communication s Unit Yarra Libraries	existing	Develop a calendar of House events to include on Council website and shared with Yarra Libraries. Council to promote Carlton Harmony Day activities Yarra Libraries contribute to Link	2019
					newsletter for housing estates.	
	2.2	NHs and council to partner in an annual NH Network event eg Music from the Heart and identify opportunities to build on the success of past events	All Houses Yarra Libraries	existing	Partner with Libraries to develop written / digital Stories "From the Heart" leading up to From the Heart Event.	2019
					Review the evaluation of "From the Heart" event and determine plan for 2019 event.	
Skills Developing/ Skills Harnessing	2.3	Neighbourhood Houses to learn about Asset Based Community Development (ABCD)	Cheryle Grey (Services & Planning Unit)	existing	Training on ABCD to be organised for all Houses.	2019
			Neighbourhood Houses Victoria (NHVic)		Houses complete online NHVic Micro-credential training program in Community Development Principles.	
Safe Spaces	2.4	Identify professional development for House staff and volunteers on how to listen, refer and provide information about what is available in the community.	All Houses Women's Health in the North (WHIN)	existing	Houses attend & promote "Identifying Family Violence" Training available through WHIN and other relevant agencies.	2019

Attachment 2 - Yarra City Council and Neighbourhood House Partnership Strategy and Action Plan - Year 2 Actions 2019

Explore options for council to support professional development for example:	Community Partnerships	Council to explore opening up training to Houses on relevant	2019
Mental Health First Aid training, Common Risk Assessment Framework (CRAF).	Yarra Libraries	topics; eg Mental Health First Aid, Identifying Children at Risk,	
Eg.Link in with Council's Skills Training, Women's Health in the North or other		Dealing with Challenging Behaviours	
networks	Sue Kent	Belaium Ave to engage with	
	(Belgium Ave)	Melbourne University students to	
		develop a presentation on presentation on place making.	
	Elly Murrell (Community Partnerships)	Grants newsletter to be distributed to all Houses.	
	Michele Watson (Services and Planning Unit)	Houses to offer community training sessions on navigating My AgedCare.	
	Laurice Young (Services and Planning Unit)	Houses to offer Advocacy sessions on National Disability Insurance Scheme (NDIS).	
	Community Partnerships/ Yarra Settlement	Houses to access YSF training eg, Human Rights & Racism, Unconscious Bias, Cultural Competence.	
	Forum(YSF) NENHN	Develop list for support services for House Staff eg. professional debriefing, external supervision/ mentoring.	

Yarra City Council and Neighbourhood Houses Partnership Strategy and Action Plan 2018 – 2021 Year 2 Actions – 2019

Attachment 2 - Yarra City Council and Neighbourhood House Partnership Strategy and Action Plan - Year 2 Actions 2019

Yarra City Council and Neighbourhood Houses Partnership Strategy and Action Plan 2018 – 2021 Year 2 Actions – 2019

3. A sustainable Yarra: Council leads on sustainability and protects and enhances its natural environment

Protecting our natural environment and supporting our community to reduce its environmental footprint will continue to be a priority for Council.

Area		Action	Who	Budget	Outcome/ Outputs	Time Frame
Information and education	3.1	Neighbourhood houses to generate list and description of all sustainability activities and share with Sustainability Officers at Council – include information about demonstration sites eg low water use gardens and learning clubs. Best practice compost.	All Houses Council Sustainability Unit	existing	Raise the profile of Houses as Sustainability Hubs via Council social media, signage, branding etc. eg. Composting demonstrations at Finbar Sustainability Engagement Officer to visit Houses and attend monthly Network meetings. Explore running a Sustainable tour of Houses as part of Sustainable Living Festival. Houses contribute to monthly sustainability eNews (info@Yarra Life).	2019
	3.2	Council's Living Sustainably pages to link to neighbourhood house sustainability initiatives and programs	Sustainability Unit Communication Unit	existing	Council to include ALL Sustainability programs/activities/workshops/ recycling etc provided by Houses on Council website. Council to promote House sustainability activities via networks and eNews.	2019

Attachment 2 - Yarra City Council and Neighbourhood House Partnership Strategy and Action Plan - Year 2 Actions 2019

Yarra City Council and Neighbourhood Houses Partnership Strategy and Action Plan 2018 – 2021 Year 2 Actions – 2019

Reduce waste to landfill	3.3	Council to support neighbourhood houses to be recycling drop-off points (for nonorganic and no hard waste items eg mobile phones and batteries)	All Houses Council Sustainability Unit / Recycling Unit	existing	Houses to access Council's recycled materials for recycled art activities. Council to support Houses to install a recycle station. Council to collect recycling materials from Houses.	2019
			NENHN		Houses to explore Repair workshops eg; Develop a bank of repairers and share with all Houses.	
			Yarra Libraries		Yarra Libraries 'weeded collection" to be donated to Houses "Little libraries".	
	3.4	Develop Neighbourhood Composting Education and engagement	Sustainability Unit Finbar	existing	Expand Composting education and engagement eg, Support apartment dwellers set up worm farms. Council to document case study with Finbar on apartment worm farming and share with other	2019
Facilitate Urban Agriculture	3.5	Continue to strengthen the relationship between neighbourhood houses and Urban Agriculture at Council and explore urban agriculture initiatives eg community orchard, fruit squad, food swap, seed swap	All Houses Sue Kent (Collingwood) Michele Watson	existing	Houses. Houses identify opportunities for more community gardens. Explore role of Houses in Food rescue. Explore Food distribution location for Second Bite in Collingwood.	2019

Attachment 2 - Yarra City Council and Neighbourhood House Partnership Strategy and Action Plan - Year 2 Actions 2019

Yarra City Council and Neighbourhood Houses Partnership Strategy and Action Plan 2018 – 2021 Year 2 Actions – 2019

4. Local businesses prosper and creative and knowledge industries thrive

Supporting local businesses and creative industries not only contributes to Yarra's economy, but also increases employment opportunities, enhances street life and fosters community connectedness.

Area		Action	Who	Budget	Outcomes / Outputs	Time Frame
Employment pathways and	4.1	Meet with Economic Development Unit from council to understand what services they provide, and how neighbourhood house	Economic Development Unit (EDU)	existing	Houses to input into EDU Events Calendar for 2020.	September 2019
micro business and social		participants can access these resources and services.	All Houses		EDU to present at Yarra NH network.	2019
enterprises for economically disadvantaged.		Explore the potential for partnership opportunities eg NHs to promote or host council's Lunchbox Learning series of seminars.			Cross promotion of Economic Development Unit Lunchbox Learning sessions.	
					Houses to contribute to EDU eBulletin – (reach of 3000 people fortnightly).	
					Promote access to Lynda.com for small business training modules available through library membership.	
					Houses to promote Council's young entrepreneur program.	

11.2 Draft Expression of Interest for the restoration and use of the building at 150-152 Hoddle Street, Abbotsford

Trim Record Number: D19/20064 Responsible Officer: Director City Works and Assets

Purpose

1. The purpose of this report is to seek Council endorsement of the draft Expression of Interest for the restoration and use of the building at 150-152 Hoddle Street, Abbotsford, colloquially known interchangeably as the *Sailors and Soldiers Building* or the *Memorial Hall*.

Background

2. On 16 October 2018, Council resolved:

That in the matter of the "Soldiers and Sailors" Building at 150 - 152 Hoddle Street, Collingwood (also known as the Soldiers Memorial Hall - RSL), Council:

- (a) note recent representations made by members of the Tramways and East Melbourne RSL;
- (b) note Council's now adopted Property Strategy and the associated Property Assessment Framework, which is to formally guide the process of reviewing all properties, over a period; and
- (c) request officers to bring forward a comprehensive report to the first Council meeting in November:
 - (i) detailing the history of the ownership and occupancy of that property;
 - (ii) noting that Council has received a number of representations/suggestions concerning the future possible uses of the site, including but not limited to, affordable housing, RSL activities, Arts, Cultural and Social hubs;
 - (iii) referencing Council's Property Strategy and noting the Property Assessment Framework requires Officers to report back to Council outlining the range of potential opportunities for the future management of Council properties and to also include options for Council consideration re future potential development and use of such properties; and
 - (iv) detailing the statutory requirements on the Council should it proceed with any future arrangements concerning development, occupancy and/or use of Council properties.
- 3. A report was presented to the meeting on 13 November 2018 and Council resolved:

That:

- (a) Council note the report in respect of the building at 150-152 Hoddle St, Abbotsford;
- (b) Council acknowledge the keen community interest in this site and note the previous 2013 expression of interest process;
- (c) Council seeks a narrow scope Expressions of Interest (EOI) responses for uses that provide support services and assistance to returned services veterans and noting that the EOI proposal should respond fully to the following requirements:
 - (i) provide satisfactory evidence of a financial capacity to undertake and complete the identified works and restoration and a demonstrated capacity to undertake and supervise the works;

- (ii) undertake all necessary preliminary assessments of the building to restore the integrity of the building and make it fit for purpose, including that the outcome of this step would produce a scope of works costed by a Quantity Surveyor and a conservation management plan to be agreed by Council;
- (iii) have Council approval of the final design;
- (iv) include an assessment of the requirements to upgrade all services to the building;
- (v) have prior approval by Council of the tender to undertake the works;
- (vi) have clearly identified inspection and hold points during the construction of the building to ensure the restoration is undertaken to Council's satisfaction;
- (vii) demand completion of the restoration within a specified time frame;
- (viii) provide for the ongoing maintenance of the building;
- (ix) be consistent with the directions of the Collingwood Town Hall Urban Design Framework;
- (x) include the restoration and preservation of the Honour Roll contained inside 150 Hoddle Street; and
- (xi) include a forward plan to provide for community access and use of the building in the future.
- (d) As a first step in this process, Officers report back to Council with a draft Expression of Interest brief (and proposed criteria) and a proposed time frame for conducting the Expression of Interest, as well as information relating to:
 - (i) any zoning, heritage or cultural obligations/constraints; and
 - (ii) structural constraints on the restoration of the building.

History

- 4. On 22 May 1920, Emma Davies sold the site to The Mayor, Councillors & Citizens of the City of Collingwood for 1,050 pounds; the Certificate of Title shows the owner as the Mayor Councillors and Citizens of the City of Collingwood. No other changes in ownership are recorded on the Title.
- 5. In 1920/21 the property was rated as Returned Soldiers Hall rooms and managed by the Memorial Hall Committee which appears to have effectively acted as a committee of management on behalf of the City of Collingwood.
- 6. In 1923 the foundation stone was laid, which means that the existing building must have been demolished. The building was completed in August 1924 and was a memorial to the *"soldiers of the district who took part in the Great War, is to be at the disposal of citizens for social purposes."* (Herald Sun 24 August 1924)
- 7. On 9 July 1950, the then Collingwood Council entered into an in-perpetuity peppercorn lease with *Returned Soldiers', Sailors' and Airmen's Imperial League of Australia* to use the building subject to a number of conditions.
- 8. RSL Victoria Office have advised that the Collingwood RSL Branch which appears to have occupied the building from 1950 onwards was formally discontinued in 2010, although it appears the building was vacated some time earlier.
- 9. At the November 2018 Council meeting some of this history was challenged, however no further information has been provided to Council to support any changes to the above information.

Uses of the building

10. From 2005 onwards, Council has received a number of documented and verbal proposals to use the site (for both temporary and more permanent uses), including:

- (a) Community, arts and cultural uses (as per the 2013 Expression of Interest process);
- (b) art exhibition space;
- (c) child care facility;
- (d) affordable housing proposal
- (e) church meeting hall;
- (f) community meeting spaces and flexible areas;
- (g) family violence hub;
- (h) use as a facility to meet the needs of Returned Service men and women;
- use of the outdoor area since 2017 for raised garden beds by Collingwood College; and
- (j) 2018 filming application.
- 11. A number of the earlier proposals were not pursued on the basis they were premature due to work being undertaken to prepare the Collingwood Town Hall Urban Design Framework.
- 12. Some of these proposals have not been presented to, or formally been considered, by Council.

Collingwood Town Hall Urban Design Framework

- The Collingwood Town Hall Precinct Urban Design Framework (UDF) was adopted by Council on 17 August 2010. The UDF underwent broad community consultation from 20 April to 15 May 2009 including:
 - (a) Individual discussions with State Government agencies;
 - (b) the draft UDF being placed on Council's website, with an invitation to make submissions;
 - publication of a fact sheet which provided a summary of the key elements of the draft UDF; information regarding public display and information day/s and inviting submissions;
 - (d) availability of the fact sheet in all libraries and the Collingwood and Richmond Town Halls;
 - (e) mail out of fact sheet to the owners and occupiers of land directly affected by the draft UDF and those in the immediate neighbourhood inviting their submissions regarding the draft UDF and inviting them to attend the public information session;
 - (f) notices in the Yarra Leader and Melbourne Times, as well as press releases advising of the exhibition of the draft UDF and the proposed information day;
 - (g) display of material in the Collingwood Town Hall foyer during the consultation period; and
 - (h) an information day on 7 May 2009 in the Town Hall foyer to provide the opportunity to interested persons to ask questions and seek further information. The information day was well attended.
- 14. The purpose of the UDF was stated as:

This urban design framework (UDF) has been prepared to guide development in the Precinct surrounding the Collingwood Town Hall (the Precinct). The City of Yarra has considerable land holdings around the Town Hall. Much of this land is underdeveloped or vacant, offering considerable potential to enliven the Precinct by facilitating appropriate day time and after hour's uses and activities. Specifically the UDF provides a framework to:

(a) Provide a vision for the long term development of the Precinct;

- (b) Realise the opportunity to create a community/civic/employment hub;
- (c) Realise opportunities for creating a very successful people place;
- (d) Provide an alternative opportunity for the City of Yarra to possibly develop a centralised municipal office;
- (e) Explore opportunities for affordable and other forms of housing in the Precinct;
- (f) Guide Council investment in capital works to enhance the public domain within the Precinct; and
- (g) Guide policy development for land use, activities and appropriate built form responses, to be implemented through appropriate statutory planning mechanisms.
- 15. The issue and opportunity identified in the land ownership section is the "...extensive area of public land provides the opportunity for a significant comprehensive development."

Expression of Interest

- 16. The draft Expression of Interest (EOI) requires that respondents address the following matters as a minimum:
 - (a) details of the organisation's capacity and experience in the provision of services to returned service men and women;
 - (b) provision of satisfactory evidence of the financial capacity to undertake and complete the works and restoration and a demonstrated capacity to undertake and supervise the works to completion;
 - (c) provision of a detailed plan to:
 - (i) undertake all necessary preliminary assessments of the building to restore the integrity of the building and make it fit for purpose;
 - (ii) establish a procurement process consistent with Council's Occupational Health and Safety policies and social and environmental procurement policies
 - (iii) produce a scope of works costed by a Quantity Surveyor and a conservation management plan to be agreed by Council and Council approval of the final design;
 - (iv) include an assessment of the requirements to upgrade all services to the building; and
 - (v) have prior approval by Council of the tender and principal contractors to undertake the works;
 - (d) Proposed inspection and hold points during the planning and construction works to ensure the restoration is undertaken to Council's satisfaction including completion of the restoration within a specified time frame to be agreed with Council;
 - (e) provision for the ongoing maintenance of the building;
 - (f) a reconciliation, to Council's satisfaction, of the proposal against directions of the Collingwood Town Hall Urban Design Framework including;
 - (i) delivering on UDF principles;
 - (ii) opportunities for the refurbished facility to contribute to the precinct; and
 - (iii) how the reinstatement and use of the building will contribute to the Collingwood Town Hall precinct;
 - (g) a plan for the restoration and preservation of the Honour Roll contained inside 150-152 Hoddle Street; and
 - (h) a plan to provide for community access and use of the building in the future.

- 17. The draft EOI is contained in <u>Attachment 1</u>.
- 18. Officers have developed draft criteria for the consideration of these matters as part of the review and assessment of any responses to the EOI. These draft criteria are outlined in <u>Attachment 2</u>.
- 19. The draft EOI and the assessment criteria have been peer reviewed by consultants with expertise in this area.

Engineering report

- 20. In January 2019, the Bonacci Group undertook an inspection of the building and have prepared a report (<u>Attachment 3</u>). The Bonacci Group have previously provided Council structural engineering advice on the Memorial Hall building.
- 21. The report found that the building had continued to deteriorate in some cases due to insufficient stiffness within footings on highly reactive clay.
- 22. The report concludes:

The building has a number of fundamental structural issues including foundations that, from the observations made on site as part of the inspection and review process, are undersized and supported on reactive clays. Seasonal movements and lack of footing stiffness has caused reflective cracking in the walls throughout the building. All walls will require underpinning.

23. The report recommends a number of rectification works which it acknowledges will be "*slow* and difficult and ultimately costly".

Planning controls

24. The site at 150-152 Hoddle St, Abbotsford is within a Public Use Zone Schedule 6 (Local Government). Hoddle Street is zoned Road Zone Schedule 1 (RDZ1). Immediately south of the site is a General Residential Zone Schedule 2 (GRZ2) occupied by St Phillips Church. Further south and on the north side of Vere Street, the land is zoned Neighbourhood Residential Zone Schedule 1. The PUZ6 continues at the rear of the Vere Street properties owned by Council and included the Collingwood Town Hall. To the east is the Hurstbridge and South Morang rail lines, Gahan Reserve and a residential area zoned NRZ1. The zones are displayed in the Figure 1 below.

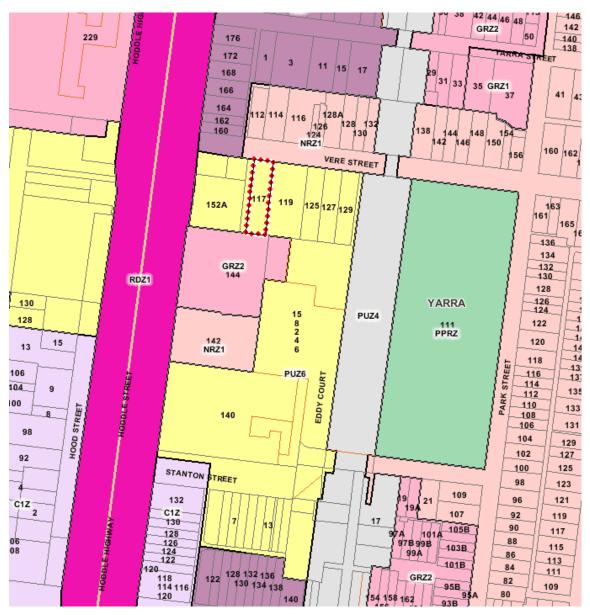


FIGURE 1: ZONING MAP

- 25. The site at 150-152 Hoddle St, Abbotsford is partly also covered by the Design and Development Overlay Schedule 2 (Main Roads and Boulevards) and an area of potential Aboriginal Cultural Heritage Sensitivity.
- 26. The site is also covered by the Heritage Overlay and is graded as a significant building in the Charles Street Precinct (HO313).
- 27. <u>Attachment 4</u> summarises the planning controls that effect the site. In brief, a permit is required for any buildings and works as well as for any use that is not for local government purposes. These requirements come from the zone as well as the Heritage Overlay. Because the building is existing, a Cultural Heritage Management Plan is not required.

External Consultation

28. No external consultation with the broader community has occurred specifically in relation to this report or its recommendations. However, public notification and hearing of submissions will be required should Council determine to enter a long term lease with a service provider.

Internal Consultation (One Yarra)

29. The relevant internal departments have been consulted in preparation of this paper.

Financial Implications

30. The approach adopted in this paper is that the restoration of the building and its use should be at no direct cost to Council.

Economic Implications

31. The Property Strategy formalises assessment and evaluation principles, thus ensuring due consideration of economic implications.

Sustainability Implications

32. Council has the ability to substantially influence the sustainability of new development proposed for this site, and where practical sustainability criteria has been included in the criteria for the EOI.

Social Implications

33. Council has the ability to control the community outcomes for the sites and precinct.

Human Rights Implications

34. There are no human rights implications.

Communications with CALD Communities Implications

35. At this stage there are no communication with CALD community implications.

Council Plan, Strategy and Policy Implications

36. Council's adopted Property Strategy provides a framework for the assessment of these properties. This has been outlined above.

Legal Implications

37. At this stage there are no legal implications, however legal advice may be required in the lease of the property.

Options

38. Council can approve the draft EOI, require modifications or a complete revision of the draft.

Conclusion

- 39. The building at 150-152 Hoddle St, Abbotsford requires substantial investment to restore it to a standard suitable for any occupation and any productive use. Further assessment may be required to ensure that the building can be restored.
- 40. Putting this historic building to a productive use is the best means of ensuring its continued viability and preservation. The development of a conservation management plan is an appropriate means of achieving this outcome. As such, this is a key criteria in the proposed EOI.
- 41. There are a number of potential services that could use the building. An important consideration for Council, is its adopted Collingwood Town Hall Urban Design Framework and how the reuse of the building supports and implements the UDF.

RECOMMENDATION

- 1. That:
 - (a) Council note the officer's report with respect to the Expression of Interest for the restoration and use of the building at 150-152 Hoddle Street, Abbotsford;
 - (b) Endorse the attached draft Expression of Interest and call for responses to the document;
 - (c) Receive a further report on the responses to the Expression of Interest; and
 - (d) The attached structural engineering advice and planning controls review be attached to the Expression of Interest.

CONTACT OFFICER:	Michael Ballock
TITLE:	Executive Planner Strategic Projects
TEL:	9205 5669

Attachments

- 1 Draft EOI Final
- 2. Expression of Interest assessment criteria
- 30 152 Hoddle Street Abbotsford Soldiers and Sailors Memorial Hall Structural Report 2019 - Rev2
- **4**. Planning controls review



REQUEST FOR EXPRESSION OF INTEREST

Draft BRIEF

Yarra City Council 152 Hoddle Street, Abbottsford

Yarra City Council - Ordinary Meeting of Council Agenda - Tuesday 5 March 2019

CONTENTS

1.	Introdu	ction	3
1.1	Purpose		3
2.	Background		
2.1	1 Building assessment		3
2.2	Re-use of the building		4
2.3		g Controls Zones and overlays Collingwood Town Hall Urban Design Framework	4
3.	Scope. . 3.1.1	In Scope	6 6
4.	<i>The EO</i> 4.1.1 4.1.2 4.1.3 4.1.4	I Process Introduction Respondent's Obligations EOI Acceptance Invitation to tender following the EOI	7 7 7

1. INTRODUCTION

The building at 150-152 Hoddle Street, Abbotsford is colloquially known interchangeably as either the *Sailors and Soldiers Building* and *Soldiers Memorial Hall - RSL*. Council is seeking Expressions of Interest (EOI) for the renovation and use of the building from organisations with a proven track record in delivering support services and assistance to returned services veterans.

1.1 Purpose

The aim of this Expression of Interest Brief is to:

- a) communicate to the potential respondents the expectations and requirements for the renovation of the building; and
- b) provide sufficient detail to enable an initial assessment by the Council of the suitability of the respondents.
- c) enable Council to select a respondent to the Expression of Interest process.

2. BACKGROUND

The Collingwood Sailors and Soldiers Memorial Hall was erected to commemorate those from Collingwood who enlisted for service in World War One. The foundation stone was laid by Brigadier General R. Smith in 1923 and the hall was opened by Captain Jacka in 1924. The property is described as the piece of land being Lot 1 on Title Plan 605033A; more particularly described in Certificate of Title Volume 2393 Folio 491. The registered proprietors of 152A Hoddle Street Abbotsford VIC 3067 are The Mayor, Councillors and Citizens of The City of Collingwood.

The land is a level parcel, rectangular in shape, has a north/west orientation, and a frontage of 30.48 metres to Hoddle Street and 30.48 meters to a right of way along the eastern boundary. Vehicular access is available via a right of way at the rear, although the property lacks on site car parking. The land has an area of 919 square metres.

The Soldiers and Sailors building is part of the Charles Street Precinct (HO313) and is not individually listed. The two storey extension at the rear of the building is not part of the original construction

2.1 Building assessment

In June 2013 Westlink Consulting undertook an assessment of the building and observed: Internally, this building provides very poor quality accommodation and is in need of complete internal refurbishment. In our opinion, the building adds little to no value to the land. Given the buildings age (approximately 90 years), and the findings of the above Geotechnical Investigation, along with the fact that there are still areas where the masonry walls, both externally and internally, are showing signs of distress, it may prove more economic to completely demolish the building and construct a purpose facility that is fully code compliant rather than expend a large amount of money on an asset that, having resolved the foundation issue, will continue to require significant expenditure in the short to medium term as other areas deteriorate. A structural assessment of the building was undertaken in BDC Structural Engineers in June 2014. This report found that there was substantial water damage to the building and in parts the ceiling and wall plaster had collapsed. The report concluded:

In this instance the egress of roof water together with inappropriate surface water control around the building is detrimental to the foundation.

The site evidence is conclusive that foundation subsidence of some of the internal walls has taken place due to a combination of the following factors:

- The soil saturation resulting from roof leaks.
- Poor perimeter surface water collection
- The existence of highly reactive clay soil.
- The lack of 'full height' brickwork articulation.
- The existence of numerous large trees too close to the foundation.

It is the writer's opinion that damage to the internal walls of the building has resulted from the combination of highly reactive soil type and a history of saturation of the founding soil. The report made a number of recommendations to make the roof watertight and deal with stormwater from and around the building.

2.2 Re-use of the building

The property is in need of work to secure the fabric of the building and the cost of these works is likely to be substantial. In addition, the building covered by a Heritage Overlay, which limits the changes that can be made to the building.

2.3 Planning Controls

2.3.1 Zones and overlays

The site at 150-152 Hoddle St, Abbotsford is within a Public Use Zone Schedule 6 (Local Government). Hoddle Street is zoned Road Zone Schedule 1 (RDZ1). Immediately south of the site is a General Residential Zone Schedule 2 (GRZ2) occupied by St Phillips Church. Further south and on the north side of Vere Street, the land is zoned Neighbourhood Residential Zone Schedule 1. The PUZ6 continues at the rear of the Vere Street properties owned by Council and included the Collingwood Town Hall. To the east is the Hurstbridge and South Morang rail lines, Gahan Reserve and a residential area zoned NRZ1. The zones are displayed in Figure 1.

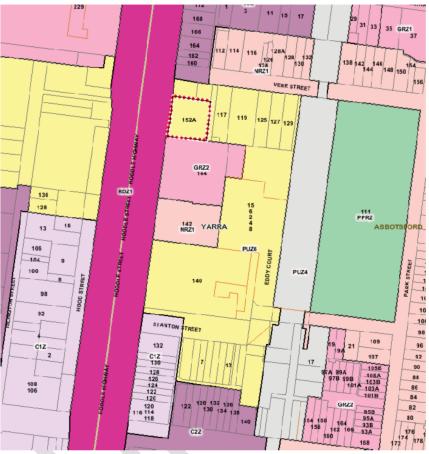


Figure 1: Zoning map

The site at 150-152 Hoddle St, Abbotsford is partly also covered by the Design and Development Overlay Schedule 2 (Main Roads and Boulevards) and an area of potential Aboriginal Cultural Heritage Sensitivity.

The site is also covered by the Heritage Overlay and is graded as a significant building in the Charles Street Precinct (HO313). Refer to Attachment 1 for more details on the planning controls.

2.3.2 Collingwood Town Hall Urban Design Framework

The Collingwood Town Hall Precinct Urban Design Framework (UDF) was adopted by Council on 17 August 2010. The UDF underwent broad community consultation from 20 April to 15 May 2009.

The purpose of the UDF was stated as:

This urban design framework (UDF) has been prepared to guide development in the Precinct surrounding the Collingwood Town Hall (the Precinct). The City of Yarra has considerable land holdings around the Town Hall. Much of this land is underdeveloped or vacant, offering considerable potential to enliven the Precinct by facilitating appropriate day time and after hour's uses and activities. Specifically the UDF provides a framework to:

- Provide a vision for the long term development of the Precinct.
- Realise the opportunity to create a community/civic/employment hub.
- Realise opportunities for creating a very successful people place.
- Provide an alternative opportunity for the City of Yarra to possibly develop a centralised municipal office.
- Explore opportunities for affordable and other forms of housing in the Precinct.
- Guide Council investment in capital works to enhance the public domain within the Precinct.
- Guide policy development for land use, activities and appropriate built form responses, to be implemented through appropriate statutory planning mechanisms.
 - The issue and opportunity identified in the land ownership section is the "...extensive area of public land provides the opportunity for a significant comprehensive development."

The restoration and use of the property must consider the overall intent of the UDF for the precinct and demonstrate that proposal allows and does not prevent the implementation of the UDF principles for the precinct.

3. SCOPE

3.1.1 In Scope

The scope of the project is the reinstatement of the existing building at 150-152 Hoddle Street Abbottsford and its use for the provision of services to returned service men and women. The reinstatement of the building is to be undertaken in accordance with detailed plans and specifications to be developed by the successful Respondent (the Property Manager) to the Council's satisfaction. It is intended that the property shall be maintained to Council's satisfaction for the duration of the occupancy.

In return Council will provide a long term lease of the property to an organisation, at a peppercorn rental, for the use of the building for the provision of support services to returned service men and women. The use will be subject to Council's standard terms and conditions. As a minimum, responses to the EOI must address the following matters:

- a) details of the organisation's capacity and experience in the provision of services to returned service men and women;
- provision of satisfactory evidence of the financial capacity to undertake and complete the works and restoration and a demonstrated capacity to undertake and supervise the works to completion;
- c) provision a Project Management Plan:
 - a. detailing the timing and sequence of all necessary preliminary assessments of the building and property, the production of all necessary reports, plans and specifications to secure Development Approval and Building Permit issue for the Construction Works to restore the integrity of the building and make it fit for purpose;
 - b. establish a procurement process consistent with Council's Occupational Health and Safety policies and social and environmental procurement policies;
 - produce a scope of works costed by a Quantity Surveyor and a Conservation Management Plan to be agreed by Council and Council approval of the final design;
 - d. include an assessment of the requirements to upgrade all services to the building; and

- e. have prior approval by Council of the Construction Works tender and principal contractors to undertake the works;
- Proposed inspection and hold points during the construction works to ensure the restoration is undertaken to Council's satisfaction including completion of the restoration within a specified time frame to be agreed with Council;
- e) provision for the ongoing maintenance of the building;
- a reconciliation, to Council's satisfaction, of the proposal against directions of the Collingwood Town Hall Urban Design Framework including;
 - a. delivering on UDF principles;
 - b. opportunities for the refurbished facility to contribute to the precinct; and
 - c. how the reinstatement and use of the building will contribute to the Collingwood Town Hall precinct.
- g) a plan for the restoration and preservation of the Honour Roll contained inside 150-152 Hoddle Street; and
- h) a plan to provide for community access and use of the building in the future.

4. THE EOI PROCESS

4.1.1 Introduction

The EOI process is the first stage in evaluating potential respondents. Following the evaluation of the EOIs, it is likely that a shortlist of respondents will be invited to submit formal tenders for the renovation of the building and provision of services, although this will depend on various factors, including the responses received to this EOI.

To be eligible for pre-selection and possible appointment, respondents must demonstrate that they possess the necessary qualifications, expertise and management systems that will satisfy the requirements of Council as specified.

Council reserves the right not to proceed with the EOI process or tender process without compensation to respondents.

4.1.2 Respondent's Obligations

Respondents should fully inform themselves of the requirements of this Brief, terms and conditions, closing time and date, EOI lodgement procedures, evaluation process and the evaluation criteria.

Respondents should make all reasonable enquiries they consider necessary to gain an understanding of the scope of works and services to be provided before committing to the preparation and submission of an EOI.

4.1.3 EOI Acceptance

The Council is not bound to accept, consider or assess any EOI. The submission of an EOI will not give rise to any contract between Council and any supplier, or any other party, governing, or in any way concerning, the EOI process, or any other process associated with the services. Council expressly disclaims any intention to enter into any such contract with any respondent or any other such party.

4.1.4 Invitation to tender following the EOI

Subsequent to the evaluation of the EOI, the evaluation panel will determine a shortlist of respondents that may be invited to participate in a restricted tender process.

4.1.5 File Formats

Respondents must ensure that:

- a) Word documents be produced and delivered in the latest version of Microsoft Office;
- b) spreadsheets shall be produced and delivered in the latest version of Microsoft Office Excel;
- c) project programs shall be produced and delivered in the latest version of Microsoft Office Project;
- d) PDF documents (<u>ISO</u>32000-2) are unrestricted and not password protected; and
- e) All images are supplied in a JPEG file format (ISO/IEC 10918).

4.1.6 Conditions of Contract

The conditions of contract have not been provided with this EOI but will be included in any subsequent restricted tender documentation.

4.1.7 Submission of EOIs

Lodgement of EOIs

EOIs close at the date and time stated in this Brief, or as changed by addendum. **EOIs must be submitted only in electronic form using Council's e-tendering portal**. Lodging an EOI in electronic form

To lodge an EOI in electronic form, the respondent must follow the instructions contained in Council's e-tendering portal found by entering <u>https://www.tendersearch.com.au/yarracity/</u>

Lodging by this means must be **COMPLETED** by the closing time.

As the e-tendering portal will not accept an EOI if the upload is in progress at the closing time, the respondent needs to start uploading well before the closing time. If the respondent experiences any difficulty in lodging an EOI in electronic form, the respondent must contact Tender Search on 1800TENDER (1800 836 337). Council will not enter into any discussion with any party at any time regarding the success or failure of any electronic lodging process. An EOI lodged in electronic form must clearly identify the EOS as "Expression of Interest 150-152 Hoddle Street Abbotsford".

Pages of the EOI that are required to be signed by an authorised representative of the respondent must be hand signed, scanned and submitted with the remainder of the EOI. EOIs received late or by any other means than via the e-tendering portal will not be accepted.

EOI Timetable

Date	Action
04 March 2019	EOI issued
01 April 2019	EOI close at 2:00pm
29 April 2019	Shortlisting of respondents finalised
14 May 2019	Report to Council on EOI responses
04 June 2019	Approximate date for Restricted
	tender issued

The EOI timetable is indicative only and subject to change.

Attachment 2: Expression of Interest assessment criteria

Responses to the Expression of Interest document will be evaluated against the criteria listed in Section 4.1.1. These criteria are expanded in more detail hereunder.

1. RESPONDENT'S CAPACITY AND EXPERIENCE IN SERVICE DELIVERY

Details of the organisation's capacity and experience in the provision of services to returned service men and women;

Details of the organisation's past experience in service delivery including:

- a) the nature of services delivered;
- b) over what period;
- c) and the scale of the services delivered (i.e. how many people)

What services are planned for delivery from the Memorial Hall site?

If these are new services, what capacity does the organisation have to deliver them?

How are the services funded?

 PROJECT DELIVERY – FINANCIAL AND PROJECT MANAGEMENT CAPACITY AND EXPERIENCE Provision of satisfactory evidence of the financial capacity to undertake and complete the works and restoration and a demonstrated capacity to undertake and supervise the works to completion;

Evidence of sufficient liquid (cash) reserves to complete the project, likely to be \$4-\$7 million. This could take the form of:

- a) Cash
- b) Bank guarantee
- c) Government guarantee
- d) Mortgage(s)

Unless mortgaged, property would not be satisfactory evidence due to concerns about 'ownership' and the time and process needed to sell property.

3. PROJECT MANAGEMENT PLAN

Provision of a detailed plan to:

- a) Undertake all necessary preliminary assessments of the site and building to restore the integrity of the building and make it fit for purpose;
 - Basic work will require the engagement of professional services to carry out investigations and tests of the site and building elements; preparation of detailed reports, calculations, drawings and cost/time estimates to procure all necessary permits and approvals from Council and other relevant Responsible Authorities in the following areas of activity;
 - i. structural engineering
 - ii. heritage conservation
 - iii. quantity surveyor
 - iv. town planning
 - v. architectural
 - vi. Building Code of Australia compliance

Attachment 2 - Expression of Interest assessment criteria

- b) Establish a procurement process consistent with Council's Governance, Occupational Health and Safety policies and social and environmental procurement policies. This process should
 - a. Show reference to and demonstrate understanding of;
 - i. Council's OH&S policy
 - Demonstrate a clear preference to the procurement of environmentally sustainable goods, services or works;
 - iii. Encourage and promote the use of local businesses where possible;
 - iv. Identify social procurement opportunities with local social enterprises and other relevant parties;
 - v. Satisfy the requirements of Council's Ethical Procurement and Investment Commitment for all relevant procurement activities;
- Include a scope of works costed by a Quantity Surveyor and a Conservation Management Plan for the final design for Council approval;
- d) Include an assessment of the existing services and utilities with detailed requirements for all necessary upgrades to support the proposed future use of the building; and
- e) Recognise the requirement for prior approval by Council of the Construction Works tender process; and
- f) Ensure that time is allocated for Council to endorse the Respondent's preferred principal contractor who would undertake the works.

4. CONSTRUCTION WORKS – PROJECT DURATION AND HOLD-POINTS

Provide for inspection and hold points during the construction works to ensure the restoration is undertaken to Council's satisfaction including provision completion of the restoration within a specified time frame to be agreed with Council.

- a) The Project Plan shall acknowledge Council's role in overseeing the restoration.
- b) Does the project plan contain sufficient hold points for Council to review and sign-off all of the critical elements of the project development and delivery?
- c) As a minimum Council will review for approval:
 - a. Final Architectural Plans;
 - b. Conservation Management Plan;
 - c. Tender documents including Construction Plans and Specifications and the Contract document prior to release;
 - d. Recommended tenderer
 - e. Final works program; and
 - f. The works at Practical Completion prior to a Certificate of Occupancy.

5. ONGOING PROPERTY MANTENANCE

The successful Respondent (Property Manager) shall be responsible for ongoing property maintenance for the duration of the lease to a standard acceptable to Council.

All necessary activities shall be identified in a Property Maintenance Plan, to be submitted for Council approval prior to the issue of the Certificate of Occupancy. This plan should detail the nature, scope and frequency of ongoing routine and cyclic maintenance tasks, including an estimate of the resources required to ensure the sustainable and acceptable presentation and condition of the property. In essence Council will be seeking assurance that;

- a. Adequate provision will be made for the ongoing maintenance of the building; and
- b. That all maintenance responsibilities are clearly defined.

Attachment 2 - Expression of Interest assessment criteria

The approved Property Maintenance Plan will be subject to annual compliance audits by Council and periodic review jointly by the Property Manager and Council.

6. URBAN DESIGN FRAMEWORK

In order to satisfy the requirements of a Development Application for the project, a reconciliation of the proposal against directions of the Collingwood Town Hall Urban Design Framework will be required including;

- a) delivering on UDF principles;
- b) opportunities for the refurbished facility to contribute to the precinct; and
- c) how the reinstatement and use of the building will contribute to the Collingwood Town Hall precinct.

7. THE HONOUR ROLL

The Property Management Plan should include provision for the restoration and preservation of the Honour Roll contained inside the building at 150-152 Hoddle Street

a. Has the preservation of the Honour Roll been considered?

8. COMMUNITY ACCESS AND USE

The Property Management Plan should articulate the nature, scope and scale of opportunities for community access and use of the building which may be accommodated without negative impact on the specific Returned Services personnel/family support service delivery programs (the PRIMARY FUNCTIONS of the building) in the future.

- a. What consideration has been given to community access to the buildings?
- b. What facilities in the renovation would be suitable for community use?

Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall -Structural Report 2019 - Rev2



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CITY OF YARRA

RSL MEMORIAL HALL 152 Hoddle Street, Abbotsford

STRUCTURAL INSPECTION AND REPORT

Author:Callum BarnettChecker:Lou PiovesanReport No:30 11879 02SDate:23rd January 2019

This report has been prepared for City of Yarra in Accordance with the terms and conditions of appointment for the RSL Memorial Hall, Abbotsford, dated 23rd January 2019 and email correspondence from City of Yarra dated 08 2019. Bonacci Group cannot accept responsibility for any use of or reliance on the contents of this report by any third party.

Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall -Structural Report 2019 - Rev2

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Page 2

CONTENTS

- 1 INTRODUCTION
- 2 BUILDING INVESTIGATION & CURRENT BUILDING CONDITION JANUARY 2019
- 3 REGULATORY REQUIREMENTS
- 4 REMEDIAL WORKS & RECOMMENDATIONS
 - 4.1 Foundations
 - 4.2 Ground Floor Framing
 - 4.3 Wall Construction
 - 4.4 Hoddle Street Façade
 - 4.5 Roof Framing
- 5 CONCLUSION

APPENDICES

- Appendix A City of Yarra Drawing 0312 A01/02, A02/03, A03/03
- Appendix B Photographic Record and Photographic Key Plan, 10 January 2019
- Appendix C Foundation Underpinning Works, BDC Group PtyLtd Drawings 0506/7 S1, S2 and S3, dated 7 June 2005



Page 3

1. INTRODUCTION

City of Yarra recently requested that Bonacci Group prepare a structural report reassessing the existing condition (dilapidation) of RSL Memorial Hall located at 152 Hoddle Street, Abbotsford. The structural report outlines, the extent of works required to make the existing building structurally sound.

Bonacci Group conducted a walk-through of the building on Tuesday 8th of January 2019 at 9:00 am. This report has been written based on a visual inspection of the buildings current state and is limited to the parts of the building that were visible and accessible. It incorporates the findings of the previous report to provide a consolidated report of the structural condition of the building.

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Page 4

2. BUILDING INVESTIGATION & CURRENT BUILDING CONDITION - JANUARY 2019

In carrying out our investigation of the existing RSL Memorial hall, there was a high focus in comparing the current state of the building to the previous state of the building encompassed in previous reports.

Since our last report there were evidently more cracks within both perimeter loadbearing and internal walls, with visible cracks both externally and internally within brickwork. Previous remedial works that were conducted on external brickwork of the building were starting to recrack in some cases due to insufficient stiffness within footings on highly reactive clay.

Internal ceilings were falling apart due to water ingress entering via gutters and roof tiling, there were large amounts of rot within ceiling across the building in particular within room 8 which was severely rotted and cracked.

The condition of the walls varied between fair and poor with most walls predominantly in a poor condition. Most existing cracking within the building appear to be in a similar condition to the previous site investigation report, with many more additional cracks formed within walls over time since the last report. Majority of the cracks were formed around the door and arch openings.

In carrying out our investigation of the existing RSL Memorial hall, there are five (5) areas of interest that have been reviewed and addressed in this report;

- Foundations
- Ground floor framing (and first floor to 2-storey)
- Walls
- Hoddle street façade
- Roof framing

3. REGULATORY REQUIREMENTS

A review of the building should be conducted by a building surveyor, to assess what would be required over and above the structural rectification to bring it in line with the current Building Code of Australia (BCA) requirements. Our report only focussed on structural issues with the building in its current state.



Page 5

4. REMEDIAL WORKS & RECOMMENDATIONS

4.1 Foundations:

- Given the extent of consistent cracking throughout the whole building both internally and externally, strengthening works to all footings involving underpinning a minimum founding depth of 1200 mm below natural ground (200 mm into the underlying silty clay) is required. Jacking of the foundation, may be required where the existing footings have rotated. The southern wall, that has been previously underpinned appears structurally adequate an requires no further remediation work.
- The construction of a 1500 mm wide apron slab to all the building perimeter to mitigate moisture variation and changes in sub-grade adjacent foundations. This would include north, east and along south perimeter walls.
- A root barrier should be installed at the north wall which would be required to substantially mitigate foundation movement and any reflective cracking to the walls over.

NOTE: That all remediation work of other structural elements can commence after underpinning works are completed.

4.2 Ground Floor Framing

- The natural ground under the stage floor framing will need to have a weatherproof and vermin proof barrier, we recommend pouring a 100 mm slab on ground in and around the existing stumps after all loose material and soft ground has been removed.
- Main hall ground floor framing and supporting stumps should be inspected to confirm structural soundness and adequacy. Remediation works may be required to those elements and can only be confirmed after the inspection.
- At the rear of the premises, beneath the two-storey section the existing ground floor is in poor condition and should be replaced or alternatively replaced with a raft slab, that includes stiffening beams at 4.0-meter maximum centres.
- Stair access and landing to the first floor has cracks and the lower section of the pier needs to be demolished and reconstructed. The existing footing should be strengthened as required. The brick balustrading will need to be replaced with a handrail and the locations of cracked brickwork re-pointed. We advise that the staircase be demolished and be reconstructed

4.3 Wall Construction

- The arch from room 7 to entrance corridor (room 8) has failed and this section of the wall will need to be reconstructed.
- The North-west wall has severe signs of cracking throughout the whole part of the wall and through lintels. The entire North wall will need to be re-pointed to restore structural integrity.
- The door way from room 1 to room 3 has failed and this section of the wall will need to be re-pointed.
- The entire internal wall that separates room 1 to room 4,5,6 will have to be re-pointed due to the door way from room 1 to room 6 has failed and door way room 1 to 5 has cracking. There is cracking extended through the entire section of the masonry wall.



Page 6

- The door that connects room 9 to 10 has a large crack, the wall which divides the stage to the hallway will need to be repointed.
- The corner of the building that connects room 9 to 10B will need to will need to be re-pointed due to cracking at lintel support locations over both doors.
- The entire section of the south wall which did not undergo re-construction works will need to be re-pointed due to external patchwork having re-cracked and there are internal cracks running across the whole wall.
- The south west corner of the perimeter wall will need to be repointed due to being heavily cracked both internally and externally.
- South east corner of the first floor within room 12 the upper wall section will need to be repointed due to having failed at window location.
- The entire lintel on the west wall next to the entrance on the South side has failed and will need to be repointed as it has cracked right through at the location where the lintel is bearing on the brickwork.
- Due to the limitation of observation of all cracks within brickwork due to render over brickwork. The render will need to be removed to identify all cracks within the building. This will give you the full extent of any secondary cracking within brickwork. The cracks will need to be repointed with a compatible mortar at all cracked locations, to re-establish the integrity of the wall.
- Places where walls show large amounts of rot will need to be replaced once water ingress has stopped due to roof being fixed and replaced.
- We recommend that vertical control joints be installed after underpinning works completed to allow for future foundation movement in clay. No control joints were evident in the existing building masonry walls.

4.4 Hoddle Street Façade

- The façade is heavily cracked and out of alignment. We recommend that the west
 wall be surveyed to establish its current vertical alignment and then assess the wall
 for its structural stability. Given the extent of cracking to the return wall, temporary
 propping may be required.
- The entire west wall will have to firstly undergo strengthening underpinning works. Then the entire top section of the wall will need to be reconstructed to allow the wall to be realigned or wall to be completely reconstructed in correct vertical alignment.
- The existing gable facades require further lateral stabilization to correct 'out of vertical' alignment. Install supplementary steelwork mullions and whaler to support wall and fix profile or alternatively gable facade to be rebuilt.
- All cracks in the western façade will need to be patched up or repointed as required.
- The roof flashing will need to be replaced as there was clear signs of water ingress observed within the hall area, near the location of the 'gable ridge'.



Page 7

4.4 Roof Framing

Access to the roof was generally not possible during our current inspection and we therefore rely on our previous investigation report for the findings. It is understood that the building has a concrete tile roof supported off battens, with insulation sarking, rafters, under purlins, hip beams, ceiling joints and a number of discrete roof trusses.

In the previous report we note: The roof space was able to be viewed in three (3) discrete locations, namely;

- Above the stage area in the main hall (room 10) where there is no ceiling;
- In the main entrance hall (room 8) where a section of the ceiling has been removed;
- In the north east corner of room 1 where a section of the ceiling has been removed.

From the limited visibility available by viewing into the ceiling space above the stage and hall area (rooms 9 and 10) the timber framing appears in reasonable condition with no obvious signs of distress.

There are roof trusses located along the length of the hall corresponding with the position of the 'engaged piers' on each side wall.

- Roof at front of RSL memorial hall behind turret structure is in poor condition and needs to be replaced.
- North Perimeter and north-east corner where tiles are broken and water is entering will have to be retiled and replaced.
- Strengthening roof works will need to be undertaken to stiffen up the west façade to securely tie it back to the side walls of the hall, unless the wall is rebuilt and properly tied in to the return walls.
- Access through the ceiling above the main entrance confirms that timber trusses are showing signs of rotation and excessive deflection. Bonacci Group believes the trusses require strengthening.
- In many locations internally, there is signs of water ingress at perimeter walls and ceilings. This was evident by the darker staining from water and severe rotting in ceiling plasterboard. Additional investigation of the existing roof truss will be required to establish the structural integrity of the element along with the condition of the existing rafters and battens given the likely damage to these elements by water ingress.
- The entire roof drainage system and down pipes should be redesigned and rebuilt.
- It is recommended that all in-ground storm water drainage to be removed and replaced.
- Any Asbestos roof sheeting to roof above two storey wing (above room 12) to be removed and replaced with insulation and colour bond sheeting and new colour bond gutters.



Page 8

5. CONCLUSION

The building has a number of fundamental structural issues including foundations that, from the observations made on site as part of the inspection and review process, are undersized and supported on reactive clays. Seasonal movements and lack of footing stiffness has caused reflective cracking in the walls throughout the building. All walls will require underpinning.

After underpinning has completed then all cracking of the façade should be identified and appropriately repaired.

Whilst the first-floor framing appeared sound, further investigation should be carried out to ascertain the structural adequacy of these floors and establish if any remedial works are required.

The condition of the existing roof is questionable, and the full extent of remediation work unquantifiable with additional investigation. Given the visible water damage to ceiling and walls we expect that large portions of roof will require remedial work to be carried out. Asbestos should be removed.

Given the buildings age (approximately 90 years), and the findings of our investigation along with the fact that there are still undefined areas where remedial work cannot be quantified. Rectification works that are described in this report will be slow and difficult and ultimately costly.

Having resolved the foundation issue and repairs to the building, there may require further medium to long term remedial work which may still be required as the building ages. Further structural costs may be incurred to bring this facility to code compliant.

We trust that the above report is sufficient to identify the quantum of work required to the existing building to make it structurally sound.

BONACCI GROUP PTY LTD

23rd January 2019

3011 87902S rep 150 - 152 Hoddle St Abbotsford - RSL Memorial Hall 2019



CITY OF YARRA RSL MEMORIAL HALL, 152 HODDLE STREET, ABBOTSFORD STRUCTURAL INSPECTION AND REPORT

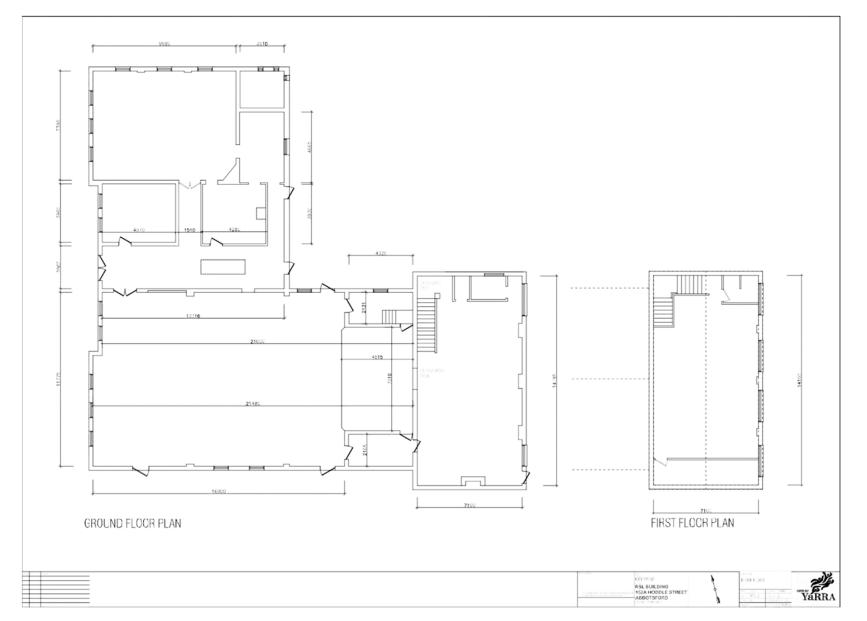
APPENDIX A

CITY OF YARRA DRAWINGS

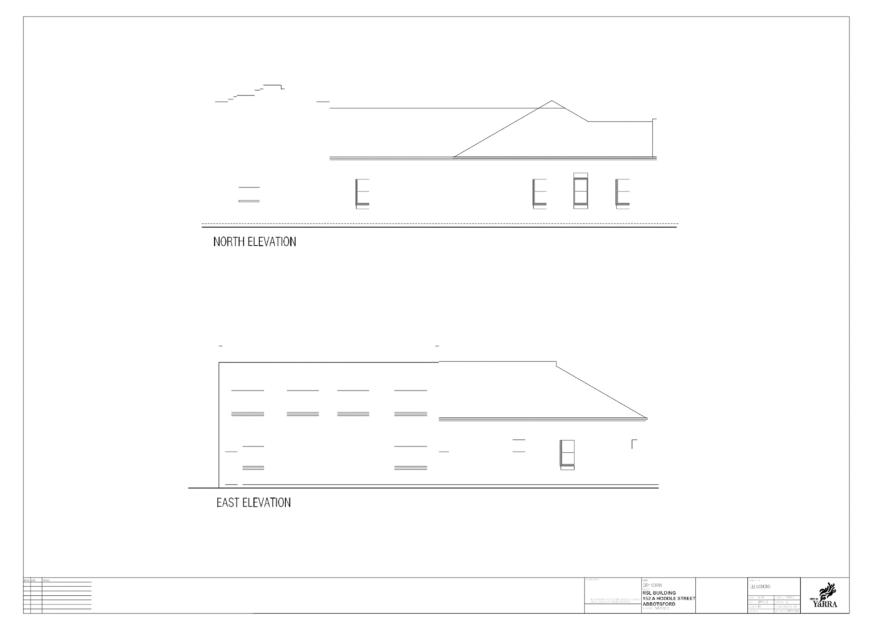
File Drawing No. Yarra 0312 A01/02 Floor Plans

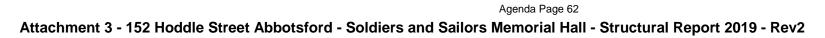
A02/03 Elevations A03/03 Elevations













Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall -Structural Report 2019 - Rev2

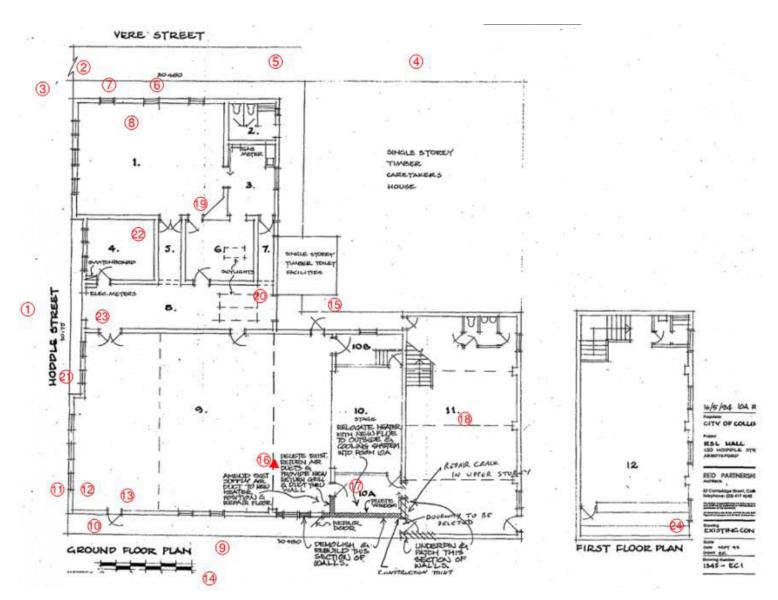
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CITY OF YARRA RSL MEMORIAL HALL, 152 HODDLE STREET, ABBOTSFORD STRUCTURAL INSPECTION AND REPORT

APPENDIX B

Photographic Record and Photographic Key Plan, 10 January 2019





Yarra City Council – Ordinary Meeting of Council Agenda – Tuesday 5 March 2019

Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall -Structural Report 2019 - Rev2

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CITY OF YARRA RSL MEMORIAL HALL, 152 HODDLE STREET, ABBOTSFORD STRUCTURAL INSPECTION AND REPORT

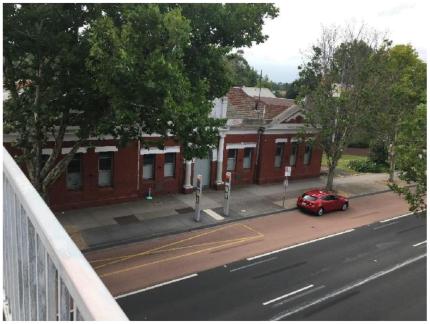


Photo 1: RSL (Collingwood Sailors and Soldiers) Memorial Hall viewed from Hoddle Street pedestrian footbridge.



Photo 2: Turret structure above main entry. North and South gable walls warped and leaning outwards. By observation current state looks similar to previous report photos.

Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall -Structural Report 2019 - Rev2

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CITY OF YARRA RSL MEMORIAL HALL, 152 HODDLE STREET, ABBOTSFORD STRUCTURAL INSPECTION AND REPORT



Photo 3: Main roof viewed from North-West. Remove tiles, re-level battens and re-instate tiles, central part of roof looks bowed which was also stated in previous report.



Photo 4: Existing tiled roof viewed from pedestrian footbridge, Timber caretakers & timber toilets have been demolished since previous report photos.

Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall -Structural Report 2019 - Rev2

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CITY OF YARRA RSL MEMORIAL HALL, 152 HODDLE STREET, ABBOTSFORD STRUCTURAL INSPECTION AND REPORT



Photo 5: Tiled roof at North-East corner. Tiles to be removed, repaired and re-pointed. Eaves, gutters and downpipes to be replaced.

Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall -Structural Report 2019 - Rev2

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CITY OF YARRA RSL MEMORIAL HALL, 152 HODDLE STREET, ABBOTSFORD STRUCTURAL INSPECTION AND REPORT



Photo 6: North Wall looking externally. Re-constructed where ivy had penetrated brick courses, Large crack right through lintel & reflective cracks right along north wall.



Photo 7: North wall has new formation of cracking above lintel, cracks are new and was not observed and documented in previous report.

Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall -Structural Report 2019 - Rev2

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CITY OF YARRA RSL MEMORIAL HALL, 152 HODDLE STREET, ABBOTSFORD STRUCTURAL INSPECTION AND REPORT

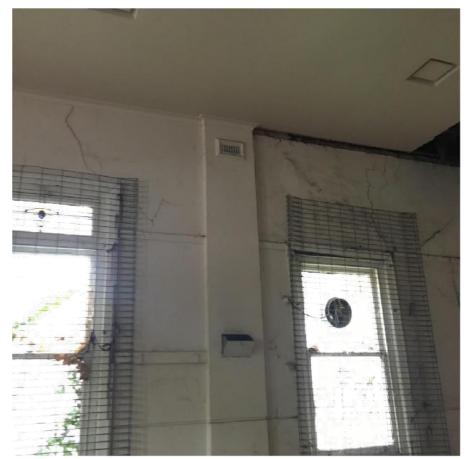


Photo 8: North Wall looking internally both previous photos 6 & 7 are the same cracks viewed externally.

Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall -Structural Report 2019 - Rev2

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CITY OF YARRA RSL MEMORIAL HALL, 152 HODDLE STREET, ABBOTSFORD STRUCTURAL INSPECTION AND REPORT



Photo 9: South wall reconstructed. Tie backs within the brickwork restraining wall.



Photo 10: South-West corner of Hall. Foundation may have dropped. Wall repaired previously, in buildings current observation repair is starting to crack again at top of brick wall.

Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall -Structural Report 2019 - Rev2

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CITY OF YARRA RSL MEMORIAL HALL, 152 HODDLE STREET, ABBOTSFORD STRUCTURAL INSPECTION AND REPORT



Photo 11: South-West corner of Hall - west wall. Cracks to wall repaired externally. Internal crack not repaired.

Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall -Structural Report 2019 - Rev2

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CITY OF YARRA RSL MEMORIAL HALL, 152 HODDLE STREET, ABBOTSFORD STRUCTURAL INSPECTION AND REPORT



Photo 12: South-West corner of Room 9 (Hall). Crack to west wall not repaired, internally. Crack looks to have got larger compared to the photos in previous

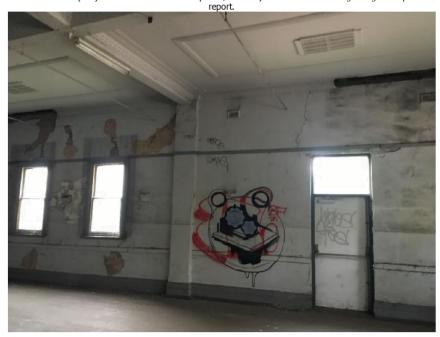


Photo 13: Looking at South Wall internally, cracks have not been repaired internally and run continuously across whole wall. Also brick ties can be seen just below roof level. Patchwork was only done externally, and no work has been done on internal face.

Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall -Structural Report 2019 - Rev2

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CITY OF YARRA RSL MEMORIAL HALL, 152 HODDLE STREET, ABBOTSFORD STRUCTURAL INSPECTION AND REPORT



Photo 14: South wall of hall. Two number tie backs at line of internal engaged piers. Wall at east re-constructed.

Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall -Structural Report 2019 - Rev2

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CITY OF YARRA RSL MEMORIAL HALL, 152 HODDLE STREET, ABBOTSFORD STRUCTURAL INSPECTION AND REPORT



Photo 15: Rear Exit at east end of main entry hall. Disconnected downpipe and heavy water damage to external render. Single story toilet block has been demolished and external brickwork exposed which is heavily cracked.

Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall -Structural Report 2019 - Rev2

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CITY OF YARRA RSL MEMORIAL HALL, 152 HODDLE STREET, ABBOTSFORD STRUCTURAL INSPECTION AND REPORT



Photo 16: Cracking above door (leading from Hall (9) to Room 10A, cracking looks to not have gotten worse since previous report.



Photo 17: Other side of wall shown in photo 16, crack is on both faces of internal brick work wall.

Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall -Structural Report 2019 - Rev2

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CITY OF YARRA RSL MEMORIAL HALL, 152 HODDLE STREET, ABBOTSFORD STRUCTURAL INSPECTION AND REPORT



Photo 18: Room 11 viewed looking North. Concrete stair at left.

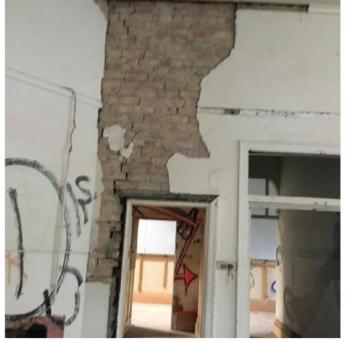


Photo 19: Large cracking at internal wall, room 1/6 interface

Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall -Structural Report 2019 - Rev2

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CITY OF YARRA RSL MEMORIAL HALL, 152 HODDLE STREET, ABBOTSFORD STRUCTURAL INSPECTION AND REPORT



Photo 20: Internal arch leading from room 8 to 7, crack looks to have got worse since previous report.

Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall -Structural Report 2019 - Rev2

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Photo 21: Front Façade West Wall (Hoddle St) has a large crack through lintel at support where lintel is bearing on brickwork. Was not observed and documented in previous report.

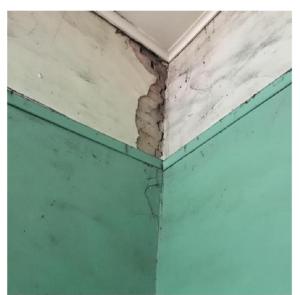


Photo 22: Large cracking in room 4 where brick wall meets each other at corner. Was not overserved and documented in previous report.

Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall -Structural Report 2019 - Rev2

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CITY OF YARRA RSL MEMORIAL HALL, 152 HODDLE STREET, ABBOTSFORD STRUCTURAL INSPECTION AND REPORT



Photo 23: Large cracks & rotting of timber walls from settlement movement and water damage. Crack was observed at entrance of room 8 & 6.



Photo 24: Room 12, first floor at corner of south east wall, cracks starting to form, and roof is starting to fall down from heavy water damage. Walls are heavily water damaged also with large amounts of rot.

Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall -Structural Report 2019 - Rev2

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CITY OF YARRA RSL MEMORIAL HALL, 152 HODDLE STREET, ABBOTSFORD STRUCTURAL INSPECTION AND REPORT





Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall - Structural Report 2019 - Rev2

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CITY OF YARRA RSL MEMORIAL HALL, 152 HODDLE STREET, ABBOTSFORD STRUCTURAL INSPECTION AND REPORT



Tie back from North-West gable into North wall Timber truss above Room 1.



Strengthening beam to roof framing above Hall (viewed looking South).

Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall -Structural Report 2019 - Rev2

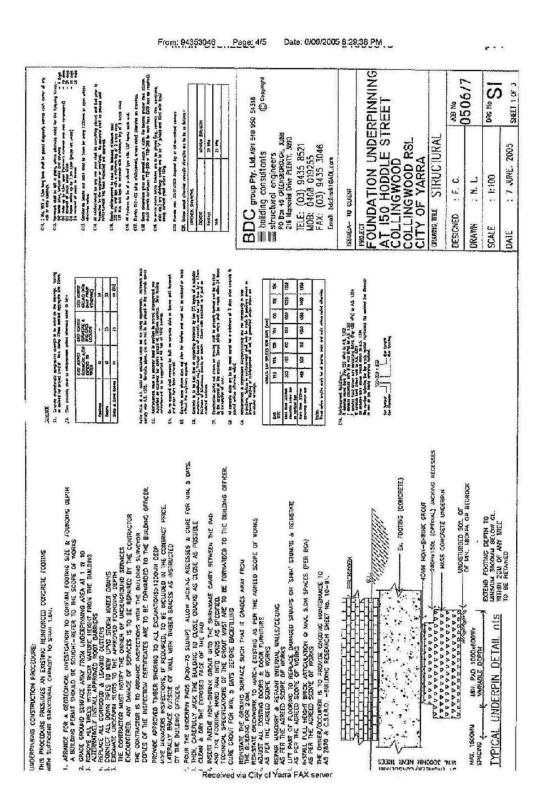
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CITY OF YARRA RSL MEMORIAL HALL, 152 HODDLE STREET, ABBOTSFORD STRUCTURAL INSPECTION AND REPORT

APPENDIX C

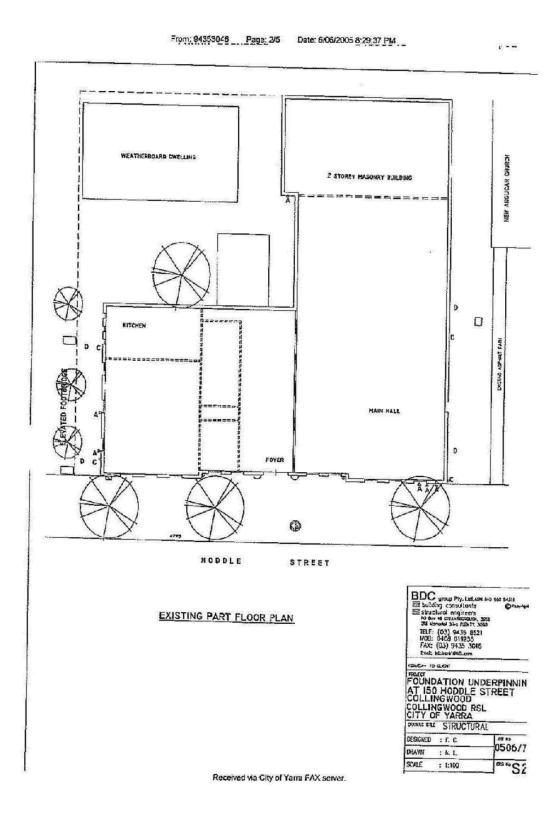
Foundation Underpinning Works, BDC Group Pty Ltd, Drawings 0506/7 S1, S2 and S3, dated 7 June 2005

Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall -Structural Report 2019 - Rev2

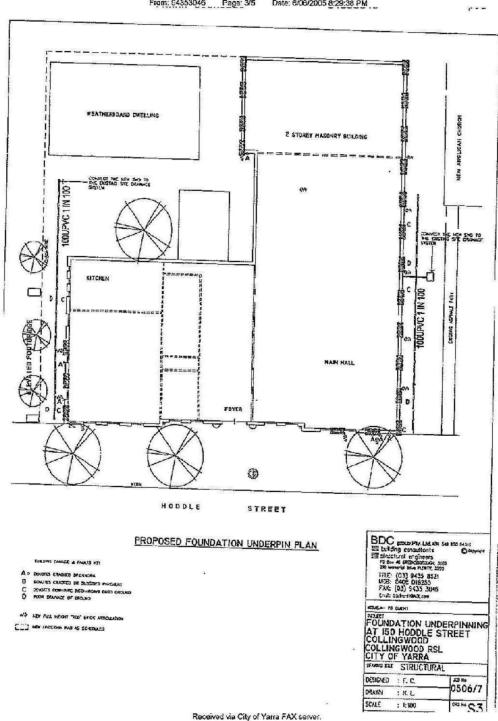


Yarra City Council – Ordinary Meeting of Council Agenda – Tuesday 5 March 2019

Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall - Structural Report 2019 - Rev2



Attachment 3 - 152 Hoddle Street Abbotsford - Soldiers and Sailors Memorial Hall -Structural Report 2019 - Rev2



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Planning controls review 150-152 Hoddle Street Abbotsford

Zone

The land is within a Public Use Zone 6 – Local Government PUZ6

The purpose of the Public Use Zone is

- To implement the Municipal Planning Strategy and the Planning Policy Framework.
- To recognise public land use for public utility and community services and facilities.
- To provide for associated uses that are consistent with the intent of the public land reservation or purpose.

The use of land for a local government purpose does not require a permit. All other uses require a permit. No uses are listed as prohibited within the zone

A permit is required for any buildings and works

Decision Guidelines

Before deciding on an application to use or subdivide land, construct a building or construct or carry out works, in addition to the decision guidelines in Clause 65, the responsible authority must consider, as appropriate:

- The Municipal Planning Strategy and the Planning Policy Framework.
- The comments of any Minister or public land manager having responsibility for the care or management of the land or adjacent land.
- Whether the development is appropriately located and designed, including in accordance with any relevant use, design or siting guidelines.

Overlays

Heritage Overlay

The land is within a Heritage Overlay Schedule 313(Charles Street Precinct Abbotsford)

The HO313 requires that all alterations to the building require a planning permit

The decision guidelines include the following

Before deciding on an application, in addition to the decision guidelines in Clause 65, the responsible authority must consider, as appropriate:

- The Municipal Planning Strategy and the Planning Policy Framework.
- The significance of the heritage place and whether the proposal will adversely affect the natural or cultural significance of the place.
- Any applicable statement of significance (whether or not specified in the schedule to this overlay), heritage study and any applicable conservation policy.
- Any applicable heritage design guideline specified in the schedule to this overlay.
- Whether the location, bulk, form or appearance of the proposed building will adversely affect the significance of the heritage place.
- Whether the location, bulk, form and appearance of the proposed building is in keeping with the character and appearance of adjacent buildings and the heritage place.

- Whether the demolition, removal or external alteration will adversely affect the significance of the heritage place. Whether the proposed works will adversely affect the significance, character or appearance of the heritage place.
- Whether the proposed subdivision will adversely affect the significance of the heritage place.
- Whether the proposed subdivision may result in development which will adversely affect the significance, character or appearance of the heritage place.
- Whether the proposed sign will adversely affect the significance, character or appearance of the heritage place.
- Whether the lopping or development will adversely affect the health, appearance or significance of the tree. Whether the location, style, size, colour and materials of the proposed solar energy

Design and Development Overlay Schedule 2

A permit is required for any buildings or works

The purpose of the DDO2 is:

- To recognise the importance of main roads to the image of the City.
- To retain existing streetscapes and places of cultural heritage significance and encourage retention of historic buildings and features which contribute to their identity.
- To reinforce and enhance the distinctive heritage qualities of main roads and boulevards.
- To recognise and reinforce the pattern of development and the character of the street, including traditional lot width, in building design.
- To encourage high quality contemporary architecture.
- To encourage urban design that provides for a high level of community safety and comfort.
- To limit visual clutter.
- To maintain and where needed, create, a high level of amenity to adjacent residential uses through the design, height and form of proposed development.

Decision guidelines are:

- The contribution of the proposal to the streetscape.
- The design, height and visual bulk of the development in relation to surrounding land uses and developments.
- The design, height and form of the development in relation to the built form character of the street.

Areas of Aboriginal Cultural Heritage Sensitivity

Most of this property is an 'area of cultural heritage sensitivity'. 'Areas of cultural heritage sensitivity' are defined under the Aboriginal Heritage Regulations 2018, and include registered Aboriginal cultural heritage places and land form types that are generally regarded as more likely to contain Aboriginal cultural heritage.

Under the Aboriginal Heritage Regulations 2018, 'areas of cultural heritage sensitivity' are one part of a two part trigger which require a 'cultural heritage management plan' be prepared where a listed 'high impact activity' is proposed. If a significant land use change is proposed (for example, a subdivision into 3 or more lots), a cultural heritage management plan may be triggered. One or two dwellings, works ancillary to a dwelling, services to a dwelling, alteration of buildings and minor works are examples of works exempt from this requirement.

Attachment 4 - Planning controls review

Under the Aboriginal Heritage Act 2006, where a cultural heritage management plan is required, planning permits, licences and work authorities cannot be issued unless the cultural heritage management plan has been approved for the activity.

However alterations to the exterior of a building are an exempt activity under the Regulations (Regulation 14). In addition because the building was lawfully used for the purpose prior to 28 May 2007 the activity is not identified as a High Impact Activity (Regulation 46(3)). As a consequence a Cultural Heritage Management Plan is not required.

11.3 Local Area Place Making Study - Scotchmer-North Fitzroy Precinct 3

Executive Summary

Purpose

To present the recommended traffic management and place making treatments identified in the Local Area Place Making (LAPM) study of Scotchmer-North Fitzroy precinct (LAPM 3).

To seek Council endorsement for referral for budget consideration in 2019/20, 2020/21 and 2021/22.

To seek Council endorsement of the proposal to advocate to VicRoads for implementation of the proposed treatments on the arterial roads.

Key Issues

The LAPM study for the Scotchmer-North Fitzroy precinct (LAPM 3) was undertaken between October 2017 and February 2019.

The first phase of engagement sought community input on precinct issues and ideas via the *Your Say Yarra* online platform and at a face-to-face drop-in session. The predominant concerns raised in the precinct were rat-running and truck traffic on local streets, pedestrian and bike rider safety, and new development.

A local area study group was formed and met in February 2018 consisting of twelve (12) community members, an external consultant, Ward Councillors and Council Officers. This group provided additional insights on local issues and informed the direction of the draft LAPM 3 plan. A further phase of broad community engagement on the draft LAPM 3 plan took place over June and July 2018.

Most of the proposed treatments were well received, with mixed results for entry bans and closures that would restrict vehicle access. The study group met again in September 2018 to discuss the consultation results. The meeting recommended that Council undertake further investigation and consultation on a proposal to close Rae Street and Brunswick Street North at the Capital City Trail to address continued traffic and safety concerns.

Consultation on five options for Rae Street, Brunswick Street North and the Capital City Trail was undertaken in November and December 2018. The majority of the community remained opposed to entry bans and closures.

A recommended LAPM 3 plan has now been prepared (see **Attachment 1**). Council officers are recommending the plan be adopted by Council.

Key recommendations in the plan include bicycle and pedestrian improvements, upgraded traffic calming treatments, intersection improvements and partial road closures. The recommended plan contributes to place making by improving pedestrian and cyclist infrastructure, as well as providing opportunities for landscaping located within kerb extensions and other infrastructure.

Effective traffic management which improves the safety, accessibility and amenity of an area encourages use of the space by the community, which has a place making benefit.

A list of priority projects on the arterial roads has also been formulated for advocacy to VicRoads.

Financial Implications

The estimated cost to deliver all actions in the recommended LAPM 3 plan is in the order of \$1.94M (excl. GST).

With Council endorsement, and allocation of funding in the 2019/20 budget, implementation of the plan can begin in July 2019.

Officers note the draft 2019/20 Budget contains an amount of \$500,000 (excl. GST), and that this would need to be confirmed as part of the budget adoption in June 2019.

The remaining works will be subject to allocation in future budgets, with officers proposing a funding split in the order of \$720,000 (excl. GST) allocated in 2020/2021 and \$720,000 (excl. GST) allocated in 2021/2022. Officers recommend spreading the expenditure over multiple years, primarily to ensure the project is fundable in the context of all other Council priorities, and noting the expenditure already approved for LAPM works in Precincts 13 and 19 over this period.

The proposed Council funding for LAPMs in 2019/2020 enables Council to deliver on its commitment to contribute 50:50 to match the \$1M grant provided by VicRoads for the investigation and delivery of projects that improve road safety on local streets.

Opportunities to fund LAPM treatments through other mechanisms will also be considered.

PROPOSAL

That Council:

- (a) Endorses the recommended traffic management and place making treatments identified in the Local Area Place Making (LAPM) study of Scotchmer-North Fitzroy precinct (LAPM 3).
- (b) Endorses the priority list of traffic management treatments on the arterial roads to advocate to VicRoads for implementation.
- (c) Notes the requirement to allocate Council funding towards LAPMs in 2019/20 in order to acquit the \$1M of funding received from VicRoads towards this program.
- (d) Notes that implementation of the recommended LAPM plan for the Scotchmer-North Fitzroy precinct (LAPM 3) can commence in July 2019, pending confirmation of \$500,000 (excl. GST) currently contained in the draft 2019/20 Budget, which will be determined in June 2019.
- (e) Refers this amount to the 2019/20 Budget process for consideration.
- (f) Notes the remaining works to implement traffic management and place making treatments identified for LAPM 3 will be subject to funding consideration in future budgets, with Officers proposing a funding split in the order of \$720,000 (excl. GST) allocated in 2020/2021 and \$720,000 (excl. GST) allocated in 2021/2022.

11.3 Local Area Place Making Study - Scotchmer-North Fitzroy Precinct 3

Trim Record Number: D19/7601 Responsible Officer: Director City Works and Assets

Purpose

- 1. To present the recommended traffic management and place making treatments identified in the Local Area Place Making (LAPM) study of Scotchmer-North Fitzroy precinct (LAPM 3).
- 2. To seek Council endorsement for referral for budget consideration in 2019/20, 2020/21 and 2021/22.
- 3. To seek Council endorsement of the proposal to advocate to VicRoads for implementation of the proposed treatments on the arterial roads.

Background

Local Area Place Making LAPM

- 4. LAPM is the precinct-based approach to planning and managing Council's local road network. It aims to improve local streets for people by managing vehicle traffic and improving conditions for pedestrians and cyclists.
- 5. The LAPM 3 study has been undertaken in accordance with Council's LAPM Policy 2017 (See **Attachment 2).**
- 6. The LAPM Policy supersedes the Local Area Traffic Management (LATM) Policy first adopted in 2014. The change reflects a multi-disciplinary approach to improving local streets in the City of Yarra by integrating urban design, place making and traffic engineering.
- 7. The LAPM studies are primarily a traffic management study which includes elements of urban design, place making and landscaping where these complement or enhance traffic treatments. The current scope for LAPMs does not seek to deliver place making initiatives which are not explicitly linked to traffic management treatments.
- 8. Effective traffic management which improves the safety, accessibility and amenity of an area encourages use of the space by the community, which has a place making benefit.

Scotchmer-North Fitzroy Precinct 3

- 9. The LAPM 3 study area is bounded by Nicholson Street, May Street, St Georges Road, Brunswick Street and Alexandra Parade in Fitzroy North.
- 10. The study area comprises approximately 3,500 properties and is predominantly residential with the exception of:
 - (a) Retail/commercial uses fronting Nicholson Street and St Georges Road;
 - (b) Retail/commercial uses at the eastern end of Scotchmer Street, including Piedimonte's Supermarket;
 - (c) Merri Creek Primary School, on Miller Street and Ida Street;
 - (d) Deutsche Schule Melbourne, on Barkly Street and Dean Street;
 - (e) Simmonds Catholic College, on Nicholson Street and Alexandra Parade; and
 - (f) The Capital City Trail and adjacent Janet Millman Reserve.
- 11. Engagement with the community is a key component of the LAPM process. The input of the community to identify the issues and needs in their neighbourhood, together with evidence-based analysis, forms the basis for the development of the LAPM plan.

12. Traffic engineering and transport planning consulting firm O'Brien Traffic was engaged to assist Council's Traffic Engineering unit to investigate, develop and consult on a recommended LAPM 3 plan for the Scotchmer-North Fitzroy precinct in December 2017.

External Consultation

- 13. With the assistance of Council's Communications and Engagement unit, a new approach to community engagement was undertaken with the aim of reducing confusion, repetition and the formality of previous studies. A move to online surveys and interactive mapping also reduced costs associated with printing, delivery and data entry of hard copy paper surveys. Community engagement for LAPM 3 was delivered in four stages.
- 14. There has been an extended and involved engagement and investigative process, involving officers, experts and the community to explore and discuss traffic management issues, ideas and possible treatments.

Stage One - Identify issues and ideas (October-November 2017)

- 15. An information postcard titled *Taking it to the streets North Fitzroy* was sent to all properties in the study area, inviting the community to identify the issues and ideas to improve their neighbourhood streets and the safety of pedestrians and cyclists. The study was also advertised through Council's *Facebook* page, *Yarra Life* electronic bulletins and *Yarra News*.
- 16. Your Say Yarra was the primary engagement platform, providing further information on the study, an invitation to nominate as a community volunteer on the Local Area Study Group, and an online survey and interactive mapping tool for feedback.
- 17. During Stage One we received:
 - (a) 430 visitors to *Your Say Yarra*, 94 survey responses and 272 issues and ideas identified on the interactive map;
 - (b) 21 emails and phone calls; and
 - (c) 30 visitors attended a drop-in session at the Holden Street Neighbourhood House on 9 November 2017.
- 18. The predominant concerns raised in the precinct were rat-running and truck traffic on local streets, pedestrian and bike rider safety, and new development.
- 19. The first Local Area Study Group meeting was held on 22 February 2018. The group included residents from all parts of the precinct and representatives of Merri Creek Primary School and Moreland City Council. Ward Councillors, Council Officers and the consultant were also part of the Local Area Study Group. The purpose of the first Local Area Study Group meeting was to discuss local insights on the issues identified in Stage One and inform a draft LAPM 3 plan.

Stage Two - Draft LAPM plan (June and July 2018)

- 20. A second information brochure for *Taking it to the streets North Fitzroy* was sent to all properties in the study area and 242 owner non-occupiers, inviting feedback on the draft LAPM 3 plan. See **Attachment 3**. Further promotion was provided on Council's *Facebook* page, electronic bulletins and an email to previously registered participants.
- 21. Feedback was encouraged through the online interactive mapping platform *Social Pinpoint* which provided further information on the treatments, a 'like' or 'dislike' survey, and discussion forum.
- 22. Residents of neighbouring Moreland City Council raised concerns about a lack of direct consultation and the potential impact from the study. Information brochures were sent to 500 Moreland properties between May Street and Glenlyon Road, Fitzroy North inviting them to participate.
- 23. During Stage Two we received:

- (a) Feedback from 601 users on *Social Pinpoint*, who collectively "liked/disliked" 2,494 times and provided 730 comments on the discussion forum;
- (b) 76 emails and phone calls; and
- (c) 55 visitors attended two drop-in sessions at Holden Street Neighbourhood House on 21 June and 5 July 2018.
- 24. There were mixed levels of support for proposed treatments to restrict vehicle access on some streets by using entry bans and closures. Otherwise most proposed treatments received majority support. Table 1 details the results.

Table 1: Stage Two consultation results for draft LAPM 3 plan

Number	Description	Responses	Like	Dislike
1	Install median island on Miller St to stop right turns in and out of Rae St and White St (cyclists excepted)	158	54%	46%
2	Upgrade existing give way slow points with full width speed humps that are bus and bike friendly, and expand garden beds where possible	64	63%	37%
3	No right turn from Nicholson St into Miller St 4pm-6pm Mon-Fri	72	35%	65%
4	No right turn from Nicholson St into Clauscen St 4pm-6pm Mon-Fri	52	38%	62%
5	No right turn from Nicholson St into Barkly St 4pm-6pm Mon-Fri	51	41%	59%
6	Widen median island and install cyclists holding rails on Holden Street at Rae St intersections	98	91%	9%
7A	Close Rae St at Capital City Trail and expand parkland	136	51%	49%
7B	Give pedestrians and bike riders on Capital City Trail priority with a raised crossing at Rae St	87	91%	9%
8A	Close Brunswick St North at Capital City Trail and expand parkland	172	38%	62%
8B	Give pedestrians and bike riders on Capital City Trail priority with a raised crossing at Brunswick St North	102	84%	16%
9	No right turn from Miller St into St Georges Rd 4pm-6pm Mon-Fri	67	67%	33%
10	Zebra crossing and bike sharrows at roundabout of Scotchmer St and Brunswick St North	84	89%	11%
11	Zebra crossing and bike sharrows at roundabout of Scotchmer St and Rae St	61	90%	10%
12	Advocate VicRoads for redesign of intersection of Reid St / Alfred Cres / St Georges Rd, such as traffic signals, relocation of nearby pedestrian crossing and tram stop upgrade	121	87%	13%
13	Zebra crossing and bike sharrows at roundabout of Reid St and Rae St	61	79%	21%
14	Zebra crossing and bike sharrows at roundabout of St Georges Rd South, Rae St and Church St	33	85%	15%
15	Kerb extensions and new traffic islands at Freeman St and St Georges Rd South	30	80%	20%
16	Kerb extensions and new traffic islands at Freeman St and Rae St	31	74%	26%

17	Zebra crossing and bike sharrows at roundabout of Newry St and St Georges Rd South	19	84%	16%
18	Zebra crossing and bike sharrows at roundabout of Newry St and Rae St	37	86%	14%
19	Zebra crossing and bike sharrows at roundabout of York St and Rae St	33	97%	3%
20A	Close laneway west of Rae St (residents excepted)	21	86%	14%
20B	No entry to laneway behind 1-37 Rae St from York St, make northbound only from Alexandra Pde	36	86%	14%
21	Partial road closure on Rae St - No exit to Alexandra Pde	107	60%	40%

- 25. At the second Local Area Study Group meeting held on 20 September 2018, the community's response to the draft LAPM 3 plan was considered and the group worked to develop a recommended LAPM 3 plan. Discussion focussed on treatments where mixed feedback and concerns were raised in the community, as follows:
 - (a) Proposed treatment 1 Install median island on Miller Street to stop right turns in and out of Rae Street and White Street (cyclists accepted). Concerns were raised that the proposed median island treatment would divert traffic to other streets such as Taylor Street and Clauscen Street and that no countermeasures for other streets were being proposed. It was agreed that the treatment be implemented on a trial basis and the traffic impact on surrounding streets monitored and addressed as necessary;
 - (b) Proposed treatment 3, 4, 5 No right turn bans from Nicholson Street into Miller Street, Clauscen Street and Barkly Street 4-6pm Monday to Friday. While the community consultation showed a lack of support for the right turn bans, there was concern that if the turn bans were not implemented, there would be no change in traffic volumes in these streets. The treatments were not recommended to proceed;
 - (c) Proposed treatment 7A and 8A Closure of Rae Street and Brunswick Street North at Capital City Trail and expand parkland. Council Officers advised that a full road closure of Brunswick Street North was not practical at this time due to southbound right turn bans from St Georges Road into Scotchmer Street which prohibit an alternative access into the local area between 7:30am and 9:30am weekdays;
 - (d) VicRoads advised they are not willing to remove these turn bans which were installed to improve tram operations;
 - (e) While the closure of Rae Street received 51% support, implementing this closure would divert traffic to Brunswick Street North. There was concern that Rae Street and Brunswick Street North closures could not be considered in isolation, as traffic would transfer to streets that remain open;
 - (f) There was a strong view that the alternative option of a raised crossing at the Capital City Trail for Brunswick Street North was not sufficient to address the traffic and safety issues. A petition signed by 47 residents of Brunswick Street North was also submitted requesting a closure. See **Attachment 4**. The group requested alternative options be considered. Additional consultation was subsequently undertaken, with five alternative options proposed. These options are outlined in Table 3;
 - (g) Proposed treatment 7B and 8B Raised priority crossing for pedestrians/cyclists of Capital City Trail at the intersections of Rae Street and Brunswick Street North. The proposed treatments received good levels of support from the community. However, as stated above, further options in addition to the raised priority crossings were requested;

- (h) Proposed treatment 9 No right turn from Miller Street into St Georges Road, 4-6pm Monday to Friday. Merri Creek Primary School expressed concerns regarding the impact the proposed turn ban would have on parents collecting children from after school care. It was advised that the preferred alternative route would be to exit via Holden Street and that dedicated right turn signals could be advocated to VicRoads to improve access to St Georges Road. The benefits of the proposed turn ban were agreed and it was recommended to proceed with this treatment;
- (i) Proposed treatment 20A/B Close laneway behind 1-37 Rae Street (residents accepted) or No Entry to laneway from York Street (northbound only from Alexandra Parade). The preferred option, based on the results of the community consultation, was for a No Entry to the laneway at York Street (i.e. northbound only from Alexandra Parade) and this was generally supported by the group; and
- (j) Proposed treatment 21 Partial road closure on Rae Street no exit to Alexandra Parade. There was general agreement from the group to the partial road closure and it was recommended to proceed.
- 26. Some members of the working group expressed a level of frustration with the LAPM process. Feedback from the group included the process being delayed, too lengthy, with a reliance on community consultation instead of technical expertise. There were also concerns raised by the group and through the consultation that underlying transport and planning issues extend beyond the LAPM boundaries and municipality, and consequently need to be addressed by Council.
- 27. Council Officers note the concerns raised. A review of the LAPM Policy (**Attachment 2**) is proposed to be conducted in 2019 to clarify and consider the scope, noting the transition from traffic management to place making, changes to community engagement tools, and feedback from the community and internal stakeholders.
- 28. Some group members were concerned that the results of the Like/Dislike survey process may have been unduly influenced by respondents living outside the LAPM precinct or not directly impacted by the treatments.
- 29. The Like/Dislike survey data is a tool that assists Council Officers to understand the level of community support for proposed treatments. Council Officers' expert assessment of the likely effectiveness of treatments forms the basis for decision making and formulation of the recommended LAPM plan.
- 30. The LAPM Policy (**Attachment 2**) does not specifically define who should be counted in the community consultation, however previous LAPM studies have focussed on the feedback of those living in the LAPM precinct and those directly-impacted in the street.
- 31. Further assessment of the Stage Two consultation results was undertaken to restrict to LAPM 3 respondents only and remove any duplicate email responses. Responses were also limited to one response per property address as another parity check. Table 2 shows the feedback from respondents living in the LAPM precinct, and only in the context of those proposals that impacted vehicle access using entry bans and closures.
- 32. The assessment does not indicate that the survey results were skewed by those respondents living outside the LAPM precinct. The levels of support for treatments by all respondents (Table 1) were similar to the level of support by LAPM 3 respondents only (Table 2) with the exception of the Capital City Trail closure at Rae Street (7A) which was not supported by LAPM 3 respondents only.

 Table 2: Stage Two consultation results for draft LAPM 3 plan by Scotchmer-North Fitzroy respondents only – based on email and property address (in bracket)

Number	Description	Responses	Like	Dislike
1	Install median island on Miller St to stop right turns in and out of Rae St and White St (cyclists excepted)	108 (100)	53% (52%)	47% (48%)

3	No right turn from Nicholson St into Miller St 4pm-6pm Mon-Fri	56 (40)	37% (36%)	63% (64%)
4	No right turn from Nicholson St into Clauscen St 4pm- 6pm Mon-Fri	40 (36)	42% (44%)	58% (56%)
5	No right turn from Nicholson St into Barkly St 4pm-6pm Mon-Fri	40 (38)	45% (45%)	55% (55%)
7A	Close Rae St at Capital City Trail and expand parkland	101 (89)	48% (48%)	52% (52%)
8A	Close Brunswick St North at Capital City Trail and expand parkland	139 (120)	35% (36%)	65% (64%)
9	No right turn from Miller St into St Georges Rd 4pm- 6pm Mon-Fri	40 (36)	73% (75%)	27% (25%)
20A	Close laneway west of Rae St (residents excepted)	20 (18)	85% (83%)	15% (17%)
20B	No entry to laneway behind 1-37 Rae St from York St, make northbound only from Alexandra Pde	32 (28)	84% (82%)	16% (18%)
21	Partial road closure on Rae St - No exit to Alexandra Pde	84 (72)	55% (51%)	45% (49%)

33. Based on the Table 2 results above, the working group generally accepted the recommended treatments, with the exception of the proposed Capital City Trail closures at Rae Street (7A) and Brunswick Street North (8A).

The pros and cons of the proposed Capital City Trail closures were debated by the working group. Issues raised included the traffic impact to surrounding streets if one or both closures occurred, the impact to Fitzroy North local traffic movements, and that safety and traffic issues would remain or be exacerbated by the raised priority crossings.

34. The working group recommended that Council further investigate treatment options for Rae Street and Brunswick Street North in the vicinity of the Capital City Trail, and consult further with the community.

<u>Stage Three – Further consultation on Rae Street and Brunswick Street North at Capital City</u> <u>Trail (November and December 2018)</u>

- 35. O'Brien Traffic undertook a further assessment of various options for Rae Street and Brunswick Street North in the vicinity of the Capital City Trail, to address continued traffic and safety concerns. See **Attachment 5**.
- 36. Council Officers reviewed the report and proposed five options that could be installed in conjunction with the preferred raised priority crossings on the Capital City Trail at Rae Street and Brunswick Street North. See **Attachment 6** for details of each option and a "pros and cons" list.
- 37. An information postcard for *Taking it to the streets North Fitzroy* was sent to all properties in the study area and 500 Moreland properties between May Street and Glenlyon Road, Fitzroy North. Further promotion was provided on Council's *Your Say Yarra* page and an email sent to previously registered LAPM 3 participants.
- 38. During Stage Three we received:
 - (a) Feedback from 679 users on Your Say Yarra;
 - (b) 22 emails and phone calls; and

- (c) 25 visitors attended a drop-in session at Holden Street Neighbourhood House on 10 December 2018.
- 39. As per the further assessment of the Stage Two consultation results, the Stage 3 results were also assessed to ensure that the results were not skewed by respondents living outside the LAPM 3 precinct. Any duplicate email responses were removed and responses were limited to one response per property address.
- 40. Table 3 details the results of the Stage 3 consultation.

Table 3: Stage Three consultation results for alternatives to closures of Rae Street and Brunswick Street North at Capital City Trail by Scotchmer-North Fitzroy respondents only – based on email and property address (in bracket)

Treatment Option	Responses	Like	Dislike	Not Sure
Option 1 - Right turn bans in the morning and afternoon peak from Holden St into Brunswick St North	404 (304)	36% (37%)	55% (53%)	9% (10%)
Option 2 – Install median island on Holden St at Brunswick St North preventing right turns in and out	406 (305)	14% (13%)	81% (81%)	5% (6%)
Option 3 – Install median island on Holden St at Brunswick St North and Dean St preventing right turns in and out	410 (309)	28% (28%)	67% (66%)	5% (6%)
Option 4 - Rae St closure at Capital City Trail and Brunswick St North northbound closure at Park St	403 (303)	11% (13%)	82% (79%)	7% (8%)
Option 5 - Combined full closure of Rae St and Brunswick St North at the Capital City Trail	412 (309)	26% (23%)	72% (75%)	2% (2%)

- 41. Based on the Stage Three feedback it is apparent that the wider community does not support further treatments to restrict vehicle access to Rae Street and Brunswick Street North, with respondents generally opposed to all options. It should be noted that the exception to this was feedback from respondents that live in Brunswick Street North, with 74% of these respondents in support of Option 5, the full closure of Rae Street and Brunswick Street North.
- 42. VicRoads' most recent available crash statistics for the five year period ending January 2019 were obtained for the section of Brunswick Street North between Holden Street and Scotchmer Street. Seven crashes were recorded as follows:
 - (a) 18 May 2015, cyclist travelling south on Brunswick St North was struck by a car turning left into Scotchmer St;
 - (b) 3 July 2015, cyclist travelling eastbound on Capital City Trail was struck by car travelling south on Brunswick St North. Cyclist failed to give way;
 - (c) 16 February 2016, cyclist travelling eastbound on Capital City Trail was struck by car travelling south on Brunswick St North. Cyclist failed to give way;
 - (d) 19 April 2016, cyclist travelling eastbound on Capital City Trail was struck by car travelling south on Brunswick St North. Cyclist failed to give way;
 - (e) 23 June 2016, cyclist travelling westbound on Scotchmer Street was struck by a car turning right into Brunswick St North. Cyclist was riding illegally on footpath;

- (f) 7 December 2016, cyclist travelling westbound on Holden Street was struck by a car turning right into Brunswick St North. Driver failed to give way; and
- (g) 14 March 2018, cyclist travelling eastbound on Scotchmer Street was struck by a car travelling southbound on Brunswick St North. Driver failed to give way.
- 43. These statistics show there is a specific road safety issue at the intersection of the Capital City Trail and Brunswick St North, with three recorded crashes involving cyclists and cars at this location. The remaining four crashes are located either at the Holden Street or Scotchmer Street intersections with Brunswick St North and relate to cars/cyclists failing to give way or in one instance a cyclist riding illegally on the footpath.
- 44. There have been no reported crashes along the length of Brunswick St North between Holden Street and Scotchmer Street (excepting the cluster at the intersection with the Capital City Trail) over the five year period ending January 2019. A road closure of Brunswick St North would not make a significant improvement to cyclist safety at the Holden Street or Scotchmer St intersections.
- 45. Officers therefore recommend that raised priority crossings be installed at the Capital City Trail as these treatments will address the recorded crashes at these locations, and were strongly supported by the Stage Two Consultation results. In addition, officers also recommend the following treatments to address the traffic speed in Brunswick St North:
 - (a) Install kerb extensions to narrow the road at the intersection with Park Street;
 - (b) Upgrade the existing speed humps to Council standard and install kerb extensions at the speed humps to narrow the road between Park Street and Scotchmer Street;
 - (c) Upgrade the existing speed humps to Council standard between Scotchmer Street and St Georges Road; and
 - (d) Install line marking to visually narrow the vehicle lanes.

Stage Four – Recommended LAPM plan (February 2019)

- 46. An information postcard for *Taking it to the streets Fitzroy North* was sent to all properties in the study area and 500 properties in Moreland to view the recommended LAPM 3 plan online, request a hard copy and invited interested parties to attend the Council meeting where the recommended plan for the Scotchmer-North Fitzroy precinct will be considered.
- 47. Further promotion was provided via an email to 457 registered LAPM 3 participants.
- 48. The recommended LAPM 3 plan is summarised in Table 4. The recommended LAPM 3 plan is found as **Attachment 1**. The consultant final report for LAPM 3 is found as **Attachment 7**.

Number	Description
1	Install median island on Miller St to stop right turns in and out of Rae St and White St (cyclists excepted)
2	Upgrade existing give way slow points on Miller St with full width speed humps and bike cut throughs, and expand garden beds where possible
3	No right turn from Miller St into St Georges Rd 4pm-6pm Mon-Fri
4	Change stop priority on Rae St and Barkly St, modify islands
5	Widen median island and install cyclists holding rails on Holden Street at Rae St intersections
6	Give pedestrians and bike riders on Capital City Trail priority with a raised crossing at Rae St
7	Give pedestrians and bike riders on Capital City Trail priority with a raised crossing at Brunswick St North

Table 4: Recommended LAPM 3 plan for Scotchmer-North Fitzroy

8	Zebra crossing and bike sharrows at roundabout of Scotchmer St and Rae St
9	Zebra crossing and bike sharrows at roundabout of Scotchmer St and Brunswick St North
10	Install kerb extensions to narrow the road, upgrade speed humps and line marking in Brunswick Street North
11	Zebra crossing and bike sharrows at roundabout of Reid St and Rae St
12	Advocate VicRoads for redesign of intersection of Reid St / Alfred Cres / St Georges Rd, such as traffic signals, relocation of nearby pedestrian crossing and tram stop upgrade
13	Zebra crossing and bike sharrows at roundabout of St Georges Rd South, Rae St and Church St
14	Kerb extensions and new traffic islands at Freeman St and St Georges Rd South
15	Kerb extensions and new traffic islands at Freeman St and Rae St
16	Zebra crossing and bike sharrows at roundabout of Newry St and St Georges Rd South
17	Zebra crossing and bike sharrows at roundabout of Newry St and Rae St
18	Install "No access to Alexandra Pde, Local Residents Only" sign to deter through traffic using York Place
19	Zebra crossing and bike sharrows at roundabout of York St and Rae St
20	Close laneway west of Rae St (residents excepted)
21	Partial road closure on Rae St - No exit to Alexandra Pde

- 49. It should be noted that Treatment 4 to change the stop priority at the cross-intersection of Rae Street and Barkly Street was not consulted on during Stage Two. It was however identified by community members during the consultation process. Council Officers are satisfied that this treatment, combined with modifications to the existing traffic islands, provides safety benefits for all road users at the intersection and improves cycling along Rae Street. The treatment does not impact local access.
- 50. It should be noted that Treatment 10 to install kerb extensions to narrow the road, upgrade speed humps and line marking in Brunswick St North was not consulted on during Stage 2 or Stage 3. As noted above, due to the fact that all proposed options to restrict vehicle access to Brunswick St North were not supported by the community, officers recommend Treatment 10 to address traffic speed in the street. The treatment dose not impact local access.
- 51. Arterial roads are managed by VicRoads, the state road authority. As such any works on these roads require VicRoads approval. A list of priority projects on the arterial roads has also been formulated through the LAPM 3 study. The recommended priority projects on the arterial roads for advocacy to VicRoads are listed in Table 5.

VicRoads road	Projects to advocate	
	A reduced speed limit	
	Tram stop upgrades	
Brunswick Street and St Georges Road	 Widen crossing point at Capital City Trail to maximise capacity for pedestrians and bike riders crossing at the traffic lights 	
St Georges Road	 Holden St intersection - right turn signals in and out of Holden Street, bike signal head starts 	

Table 5: Priority projects for advocacy to VicRoads in Scotchmer-North Fitzroy LAPM 3

	Scotchmer Street intersection - right turn signals in and out of Scotchmer Street, bike signal head starts
	 Raised threshold treatments at side street intersections for improved pedestrian access
	 Redesign of the intersection of Reid St and Alfred Cres, such as traffic signals, relocation of nearby pedestrian crossing and tram stop upgrade.
	A reduced speed limit
	Tram stop upgrades
Nicholson Street	 Widen crossing point at Capital City Trail to maximise capacity for pedestrians and bike riders crossing at the traffic lights
	 Raised threshold treatments at side street intersections for improved pedestrian access.
Alexandra Parade	More time for pedestrians and trams to cross Alexandra Parade on Nicholson Street and Brunswick Street.

52. A copy of the recommended traffic management plan has been referred to Ambulance Victoria, Metropolitan Fire Brigade and Victoria Police and no objections were received.

Internal Consultation (One Yarra)

- 53. The following Council teams were consulted through the study:
 - (a) Economic Development;
 - (b) Compliance;
 - (c) Construction Management;
 - (d) City Works;
 - (e) Waste Management and Cleansing;
 - (f) Engineering Services;
 - (g) Asset Management;
 - (h) Open Space Planning and Design;
 - (i) Open Space Planning Maintenance;
 - (j) Parking Management;
 - (k) Statutory Planning;
 - (I) Strategic Transport;
 - (m) Social Policy; and
 - (n) Urban Design.
- 54. The following internal feedback was provided:
 - (a) Open Space Planning and Design advised that the Inner Circle Railway Linear Park Reserve Masterplan 2006 refers to the intersection of Brunswick Street North and the Capital City Trail as requiring traffic management and safety improvements in regards to cyclist movements. Traffic Engineering will work closely with Open Space Planning and Design during the detailed design stage for the raised priority crossings at Brunswick Street North and Rae Street;
 - (b) Waste Management and Cleansing identified general issues related to waste service vehicle access. Traffic Engineering will work closely with Waste Management and Cleansing to ensure that designs can accommodate waste service vehicles;
 - (c) Strategic Transport contributed to the discussion regarding bike infrastructure in the precinct, such as bike lanes, conflict points and bike parking opportunities; and

(d) Several suggestions were received during the Stage One consultation for trees and landscaping. These were referred to Council's Urban Design and City Works teams for consideration as part of the annual street tree program for new and replacement trees. Suggestions for trees in the median of St Georges Road South, between Brunswick Street and Nicholson Street, were investigated and ruled out due to underground services.

Financial Implications

- 55. Implementation of the recommended plan has been estimated to cost approx. \$1.94M (excl. GST). The cost estimate is detailed in Table 6, and has been split into two portions:
 - (a) Traffic Management Traffic treatments and lighting costs; and
 - (b) Place Making Infrastructure such as kerb and footpath works, drainage, landscaping.

Number	Description	Traffic Management	Place Making	Cost estimate
1	Install median island on Miller St to stop right turns in and out of Rae St and White St (cyclists excepted)	\$40,000	\$10,000	\$50,000
2	Upgrade existing give way slow points on Miller St with full width speed humps and bike cut throughs, and expand garden beds where possible	\$350,000	\$100,000	\$450,000
3	No right turn from Miller St into St Georges Rd 4pm-6pm Mon-Fri	\$1,000	\$Nil	\$1,000
4	Change stop priority on Rae St and Barkly St, modify islands	\$75,000	\$10,000	\$85,000
5	Widen median island and install cyclists holding rails on Holden Street at Rae St intersections	\$50,000	\$10,000	\$60,000
6	Give pedestrians and bike riders on Capital City Trail priority with a raised crossing at Rae St	\$150,000	\$20,000	\$170,000
7	Give pedestrians and bike riders on Capital City Trail priority with a raised crossing at Brunswick St North	\$160,000	\$30,000	\$190,000
8	Zebra crossing and bike sharrows at roundabout of Scotchmer St and Rae St	\$23,000	\$15,000	\$38,000
9	Zebra crossing and bike sharrows at roundabout of Scotchmer St and Brunswick St North	\$23,000	\$15,000	\$38,000

Table 6: Cost estimates for Recommended LAPM 3 plan

Install kerb extensions to narrow the road, upgrade speed humps and line marking in Brunswick Street North	\$80,000	\$100,000	\$180,000
Zebra crossing and bike sharrows at roundabout of Reid St and Rae St	\$40,000	\$60,000	\$100,000
Advocate VicRoads for redesign of intersection of Reid St / Alfred Cres / St Georges Rd, such as traffic signals, relocation of nearby pedestrian crossing and tram stop upgrade	\$Nil	\$Nil	\$0
Zebra crossing and bike sharrows at roundabout of St Georges Rd South, Rae St and Church St	\$30,000	\$18,000	\$48,000
Kerb extensions and new traffic islands at Freeman St and St Georges Rd South	\$110,000	\$50,000	\$160,000
Kerb extensions and new traffic islands at Freeman St and Rae St	\$110,000	\$30,000	\$140,000
Zebra crossing and bike sharrows at roundabout of Newry St and St Georges Rd South	\$33,000	\$55,000	\$88,000
Zebra crossing and bike sharrows at roundabout of Newry St and Rae St	\$23,000	\$15,000	\$38,000
Install "No access to Alexandra Pde, Local Residents Only" sign to deter through traffic using York Place	\$500	\$Nil	\$500
Zebra crossing and bike sharrows at roundabout of York St and Rae St	\$33,000	\$15,000	\$48,000
Close laneway west of Rae St (residents excepted)	\$1,000	\$Nil	\$1,000
Partial road closure on Rae St - No exit to Alexandra Pde	\$40,000	\$10,000	\$50,000
Total Cost	\$1,372,500	\$563,000	\$1,935,500
	the road, upgrade speed humps and line marking in Brunswick Street North Zebra crossing and bike sharrows at roundabout of Reid St and Rae St Advocate VicRoads for redesign of intersection of Reid St / Alfred Cres / St Georges Rd, such as traffic signals, relocation of nearby pedestrian crossing and tram stop upgrade Zebra crossing and bike sharrows at roundabout of St Georges Rd South, Rae St and Church St Kerb extensions and new traffic islands at Freeman St and St Georges Rd South Kerb extensions and new traffic islands at Freeman St and Rae St Zebra crossing and bike sharrows at roundabout of Newry St and St Georges Rd South Zebra crossing and bike sharrows at roundabout of Newry St and St Georges Rd South Zebra crossing and bike sharrows at roundabout of Newry St and Rae St Install "No access to Alexandra Pde, Local Residents Only" sign to deter through traffic using York Place Zebra crossing and bike sharrows at roundabout of York St and Rae St Close laneway west of Rae St (residents excepted) Partial road closure on Rae St - No exit to Alexandra Pde	the road, upgrade speed humps and line marking in Brunswick Street North\$80,000Zebra crossing and bike sharrows at roundabout of Reid St and Rae St\$40,000Advocate VicRoads for redesign of intersection of Reid St / Alfred Cres / St Georges Rd, such as traffic signals, relocation of nearby pedestrian crossing and tram stop upgrade\$NilZebra crossing and bike sharrows at roundabout of St Georges Rd South, Rae St and Church St\$30,000Kerb extensions and new traffic islands at Freeman St and St Georges Rd South\$110,000Zebra crossing and bike sharrows at roundabout of Newry St and St Georges Rd South\$110,000Zebra crossing and bike sharrows at roundabout of Newry St and St Georges Rd South\$110,000Zebra crossing and bike sharrows at roundabout of Newry St and St Georges Rd South\$110,000Zebra crossing and bike sharrows at roundabout of Newry St and St Georges Rd South\$33,000Zebra crossing and bike sharrows at roundabout of Newry St and St Georges Rd South\$23,000Zebra crossing and bike sharrows at roundabout of Newry St and Rae St\$23,000Install "No access to Alexandra Pde, Local Residents Only" sign to deter through traffic using York Place\$500Zebra crossing and bike sharrows at roundabout of York St and Rae St\$33,000Close laneway west of Rae St (residents excepted)\$1,000Partial road closure on Rae St - No exit to Alexandra Pde\$40,000	the road, upgrade speed humps and line marking in Brunswick Street North\$80,000\$100,000Zebra crossing and bike sharrows at roundabout of Reid St and Rae St\$40,000\$60,000Advocate VicRoads for redesign of intersection of Reid St / Alfred Cres / St Georges Rd, such as traffic signals, relocation of nearby pedestrian crossing and tram stop upgrade\$Nii\$NiiZebra crossing and bike sharrows at roundabout of St Georges Rd South, Rae St and Church St\$30,000\$18,000Kerb extensions and new traffic islands at Freeman St and St Georges Rd South\$110,000\$50,000Kerb extensions and new traffic islands at Freeman St and Rae St\$110,000\$50,000Zebra crossing and bike sharrows at roundabout of Newry St and Georges Rd South\$110,000\$50,000Zebra crossing and bike sharrows at roundabout of Newry St and Rae St\$110,000\$30,000Zebra crossing and bike sharrows at roundabout of Newry St and Rae St\$23,000\$15,000Zebra crossing and bike sharrows at roundabout of Newry St and Rae St\$23,000\$15,000Zebra crossing and bike sharrows at roundabout of York St and Rae St\$33,000\$15,000Close laneway west of Rae St (residents excepted)\$1,000\$NiiPartial road closure on Rae St No exit to Alexandra Pde\$40,000\$10,000

- 56. Many of the additional costs associated with the Place Making portion in Table 6 (approx. \$563,000) can be attributed to correcting existing site issues such as realigning and widening pedestrian pram ramps and median island openings, improving pram ramp grades to be DDA compliant, and upgrading drainage. Historical footpath and road design in the precinct does not meet current engineering and DDA standards in many locations. Whilst the proposed treatments could be implemented at a lower cost (approx. \$1,372,500) by limiting works to line marking and signage, this would not provide the best place making, accessibility and safety outcomes for the community, especially vulnerable road users such as the elderly, children and the mobility impaired.
- 57. Significant costs are also associated with upgrade of lighting for proposed zebra crossings.
- 58. Additional costs are also required in LAPM 3 to amend, supply and install bluestone treatments as required under the Road Material Policy due to the Heritage Overlay in the precinct.
- 59. The recommended plan contributes to place making by improving pedestrian and cyclist infrastructure, as well as providing opportunities for landscaping located within kerb extensions and other infrastructure.
- 60. With any new infrastructure, there are ongoing maintenance and renewal costs. These will be referred to the annual budget process as part of the operational budget to ensure appropriate maintenance of these assets.
- 61. Funding allocation for all LAPM related studies and works has been submitted to Council's Capital bids program 2019/2020 for \$1M. This reflects the commitment Council made in 2016 to match the VicRoads grant allocation of \$1M towards LAPM works in Yarra over 2017/18, 2018/19 and 2019/20.
- 62. The implementation of the recommended LAPM plan for the Scotchmer-North Fitzroy precinct (LAPM 3) can commence in July 2019 with up to \$500,000 (excl. GST) of works in the first year.
- 63. The remaining works to implement LAPM 3 will be subject to the annual Capital Works Program budget process with proposed allocations of an additional \$720,000 (excl. GST) in 2020/2021 and \$720,000 (excl. GST) in 2021/2022.
- 64. Opportunities to fund LAPM treatments through other mechanisms will be considered including:
 - (a) Future capital road works such as road reconstruction;
 - (b) Future utility service road works such as for water mains;
 - (c) Future private development contributions or public realm improvements;
 - (d) Australian Government Black Spot Program; and
 - (e) Other Council or Victorian Government projects.

Economic Implications

65. There are no economic implications associated with the recommended plan.

Sustainability Implications

66. The recommended LAPM 3 plan and priority projects for advocacy to VicRoads improves pedestrian and cyclist safety and connectivity, thus supporting sustainable transport options and usage. Sustainable materials will be utilised where appropriate.

Social Implications

67. LAPMs are designed to consider community needs, and to deliver a safer environment and improved amenity.

Human Rights Implications

68. There are no identified human rights implications associated with this report.

Communications with CALD Communities Implications

69. A language advisory panel was included in all consultation material including contact details and reference number to access Council's interpreter service.

Council Plan, Strategy and Policy Implications

- 70. Objective Six of the Council Plan 20117-2021 refers to A Connected Yarra, a place where connectivity and travel options are environmentally sustainable, integrated and well-designed. The Plan includes Strategy 6.1 Manage traffic movement and promote road safety within local roads specifically identifies the Local Area Place Making program (Initiative 6.1.1).
- 71. The road materials used will be in line with Council's Infrastructure Road Materials Policy.
- 72. Council's Strategic Transport Statement 2012 commits to improve pedestrian crossings, facilities and priority projects.

Legal Implications

- 73. Council has an overall obligation under the *Road Management Act 2004* to manage the local road network in a manner that gives due consideration to community safety.
- 74. Approval for all Major Traffic Control Items will be sought from VicRoads.

Other Issues

- 75. The proposed Council funding for LAPMs in 2019/2020 enables Council to deliver on its commitment to contribute 50:50 to match the \$1M grant provided by VicRoads for the investigation and delivery of projects that improve road safety on local streets.
- 76. Traffic management and place making remain some of the highest priorities for our community. The LAPM process is proven to be a successful model for planning and implementing upgrades in partnership with the community. As precincts continue to evolve, there will be an ongoing need for Council to fund LAPM studies and projects into the future.

Conclusion

- 77. A recommended LAPM plan has been developed for the Scotchmer-North Fitzroy precinct (LAPM 3) in consultation with the community and through an evidence-based analysis.
- 78. A priority list of traffic management treatments for delivery on arterial roads has been formulated to advocate to VicRoads for implementation.
- 79. Implementation of the recommended LAPM plan for the Scotchmer-North Fitzroy precinct (LAPM 3) can commence in July 2019 with up to \$500,000 (excl. GST) of works in the first year, subject to the allocation of funding in the 2019/20 budget.
- 80. The remaining works will be subject to allocation in future budgets, with officers proposing a funding split in the order of \$720,000 (excl. GST) allocated in 2020/2021 and \$720,000 (excl. GST) allocated in 2021/2022. Officers recommend spreading the expenditure over multiple years, primarily to ensure the project is fundable in the context of all other Council priorities, and noting the expenditure already approved for LAPMS works in precincts 13 and 19 over this period.

RECOMMENDATION

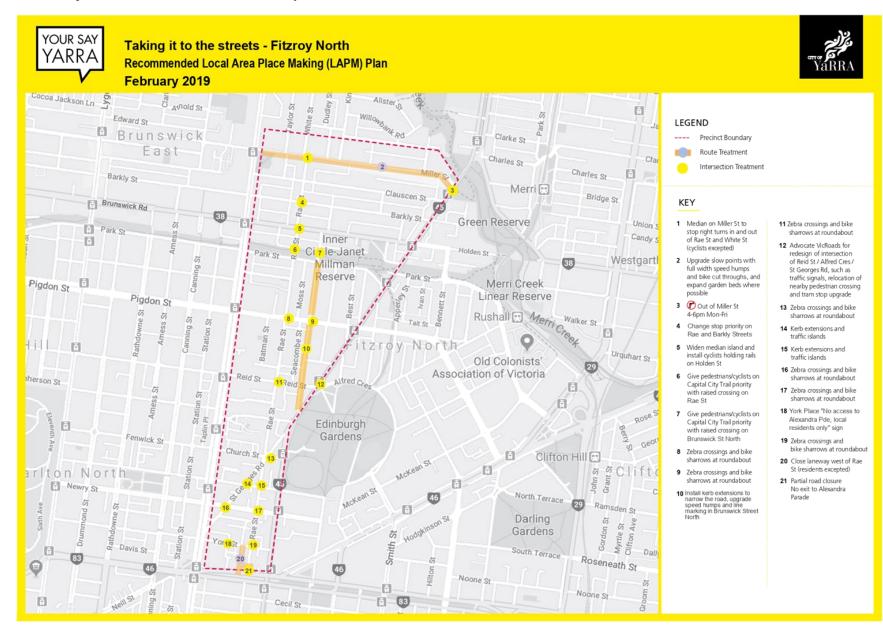
- 1. That Council:
 - endorses the recommended traffic management and place making treatments identified in the Local Area Place Making (LAPM) study of Scotchmer-North Fitzroy precinct (LAPM 3);
 - (b) endorse the priority list of traffic management treatments on the arterial roads to advocate to VicRoads for implementation;
 - (c) notes the requirement to allocate Council funding towards LAPMs in 2019/20 in order to acquit the \$1M of funding received from VicRoads towards this program;
 - (d) notes that implementation of the recommended LAPM plan for the Scotchmer-North Fitzroy precinct (LAPM 3) can commence in July 2019, pending confirmation of \$500,000 (excl. GST) currently contained in the draft 2019/20 Budget, which will be determined in June 2019;
 - (e) refers this amount to the 2019/20 Budget process for consideration; and
 - (f) notes the remaining works to implement traffic management and place making treatments identified for LAPM 3 will be subject to funding consideration in future budgets, with Officers proposing a funding split in the order of \$720,000 (excl. GST) allocated in 2020/2021 and \$720,000 (excl. GST) allocated in 2021/2022.

CONTACT OFFICER:Dennis ChengTITLE:Manager Traffic and Civil Engineering

Attachments

- 1. Fitzroy North recommended LAPM 3 plan Feb 2019
- **2** Local Area Place Making LAPM Policy 2017
- 3. Taking it to the streets North Fitzroy LAPM Draft LAPM plan brochure June 2018
- **4**. Brunswick Street North closure resident petition
- **5**. Alternatives to Rae Street and Brunswick Street closure at Capital City Trail report O'Brien Traffic November 2018
- **6** Alternative options for Rae Street and Brunswick Street North closure at Capital City Trail November 2018
- **7** LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019

Attachment 1 - Fitzroy North recommended LAPM 3 plan Feb 2019



Attachment 1 - Fitzroy North recommended LAPM 3 plan Feb 2019

Taking it to the streets – Fitzroy North Recommended Local Area Place Making (LAPM) Plan

1 Miller St, White St & Rae St

Median on Miller St to stop right turns in and out of Rae St and White St (cyclists excepted)

54% liked this idea

- Proceed on a trial basis, monitor traffic impact on surrounding streets such as Taylor St and provide potential countermeasures should issues arise.
- Right turns are prevented by a median on Miller St, with breaks to allow cyclists to be exempt
- The treatment may address some short-cutting traffic but may also redistribute traffic to other streets
- Local residents and businesses will need to modify travel.

2 Miller St

Upgrade slow points with full width speed humps and bike cut throughs, and expand garden beds where possible

63% liked this idea

- Slow points and full width speed hump to be combined with existing plastic bollards to be removed.
- Bike cut through to be provided behind garden beds to separate bike riders from vehicles.

3 Miller St & St Georges Rd

No right turn from Miller St into St Georges Rd 4pm-6pm Mon-Fri

67% liked this idea

- The right turn ban may reduce delays exiting Miller St in the afternoon and improve bus services.
- The right turn ban may reduce traffic shortcutting through local streets in the afternoon peak.
- Local residents, schools and businesses will not be exempt from the turn ban and will need to modify travel during these times.

 Concern was expressed regarding the impact the proposed turn ban would have on parents collecting children from after school care.

4 Barkly St & Rae St

Change stop priority on Rae St and Barkly St

 This change addresses sight line and design issues with the current arrangement.

5 Rae St and Holden St

Widen median island and install cyclists holding rails on Holden Street at Rae St intersections

91% liked this idea

 Improve median to better accommodate bike riders and pedestrians crossing at this location.

6 Rae St & Capital City Trail

Give pedestrians and bike riders on Capital City Trail priority with a raised crossing at Rae St

91% liked this idea

- A crossing allows pedestrians and bike riders to have priority to cross at all times.
- Raising the crossing lowers travel speed and increases the attention of all road users.
- However safety and traffic concerns still remain to warrant further consultation on other options involving raised crossings.

7 Brunswick St North & Capital City Trail

Give pedestrians and bike riders on Capital City Trail priority with a raised crossing at Brunswick St North

84% liked this idea

- A crossing allows pedestrians and bike riders to have priority to cross at all times.
- Raising the crossing lowers travel speed and increases the attention of all road users.

 However safety and traffic concerns still remain to warrant further consultation on other options involving raised crossings.

8 Scotchmer St & Rae St

Zebra crossing and bike sharrows at roundabout of Scotchmer St and Rae St

90% liked this idea

- Currently pedestrians must give way to all road traffic at roundabouts
- A zebra crossing allows pedestrians to have priority to cross at all times
- Bike sharrows indicate that bike riders share the road space
- Upgrade lighting, drainage and pedestrian accessibility as required.

9 Scotchmer St & Brunswick St North

Zebra crossing and bike sharrows at roundabout of Scotchmer St and Brunswick St North

89% liked this idea

- Currently pedestrians must give way to all road traffic at roundabouts
- A zebra crossing allows pedestrians to have priority to cross at all times
- Bike sharrows indicate that bike riders share the road space
- Upgrade lighting, drainage and pedestrian accessibility as required.

10 Brunswick St North

Install kerb extensions to narrow the road, upgrade speed humps and line marking in Brunswick Street North

- Install kerb extensions to narrow the road, replace existing speed humps and install line marking to visually narrow road width between Park Street and Scotchmer Street.
- Replace existing speed humps and install line marking to visually narrow road width between Scotchmer Street and St Georges Road.

11 Reid St & Rae St

Zebra crossing and bike sharrows at roundabout of Reid St and Rae St

79% liked this idea

- Currently pedestrians must give way to all road traffic at roundabouts
- A zebra crossing allows pedestrians to have priority to cross at all times
- Bike sharrows indicate that bike riders share the road space
- Upgrade lighting, drainage and pedestrian accessibility as required.

12 St Georges Rd, Reid St & Alfred Cr

Advocate VicRoads for redesign of intersection of Reid St / Alfred Cres / St Georges Rd, such as traffic signals, relocation of nearby pedestrian crossing and tram stop upgrade

87% liked this idea

- Introducing traffic signals and other design changes at this busy intersection may reduce confusion and be safer for all road users
- Incorporating the tram stops and a relocated pedestrian crossing at the intersection may improve the operation of the intersection
- Introducing traffic signals at this intersection may however increase traffic to Reid St and Alfred Cr.

13 St Georges Rd, Rae St & Church St

Zebra crossing and bike sharrows at roundabout of St Georges Rd South, Rae St and Church St

85% liked this idea

- Currently pedestrians must give way to all road traffic at roundabouts
- A zebra crossing allows pedestrians to have priority to cross at all times
- Bike sharrows indicate that bike riders share the road space
- Upgrade lighting, drainage and pedestrian accessibility as required.

Attachment 1 - Fitzroy North recommended LAPM 3 plan Feb 2019

Taking it to the streets – Fitzroy North Recommended Local Area Place Making (LAPM) Plan

14 Freeman St & St Georges Rd South Kerb extensions and new traffic islands at Freeman St and St Georges Rd South

80% liked this idea

 A narrower intersection may reduce speed, improve sight lines and reduce pedestrian crossing distance.

15 Freeman St & Rae St

Kerb extensions and new traffic islands at Freeman St and Rae St

74% liked this idea

 A narrower intersection may reduce speed, improve sight lines and reduce pedestrian crossing distance.

16 Newry St & St Georges Rd South

Zebra crossing and bike sharrows at roundabout of Newry St and St Georges Rd South

84% liked this idea

- Currently pedestrians must give way to all road traffic at roundabouts
- A zebra crossing allows pedestrians to have priority to cross at all times
- Bike sharrows indicate that bike riders share the road space
- Upgrade lighting, drainage and pedestrian accessibility as required.

17 Newry St & Rae St

Zebra crossing and bike sharrows at roundabout of Newry St and Rae St

86% liked this idea

- Currently pedestrians must give way to all road traffic at roundabouts
- A zebra crossing allows pedestrians to have priority to cross at all times
- Bike sharrows indicate that bike riders share the road space
- Upgrade lighting, drainage and pedestrian accessibility as required.

18 York Pl & York St

Install "No access to Alexandra Pde, Local Residents Only" sign to deter through traffic using York Place

19 York St & Rae St

Zebra crossing and bike sharrows at roundabout of York St and Rae St

97% liked this idea

- Currently pedestrians must give way to all road traffic at roundabouts
- A zebra crossing allows pedestrians to have priority to cross at all times
- Bike sharrows indicate that bike riders share the road space
- Upgrade lighting, drainage and pedestrian accessibility as required.

20 Laneway behind 1-37 Rae St

Close laneway west of Rae St (residents excepted)

86% liked this idea

- Install a bollard to prohibit through traffic on laneway behind 1-37 Rae Street
- This removes short-cutting traffic using the laneway between York St and Alexandra Pde
- Further consultation with immediate properties recommended to determine best location for bollard
- Warning signs to be installed in advance of closure point.

21 Rae St & Alexandra Pde

Partial road closure on Rae St - No exit to Alexandra Pde

60% liked this idea

- May reduce traffic queues and ratrunning through local streets, especially in the morning
- Local residents and businesses will not be exempt from the bans and will need to modify travel.

Projects to advocate to VicRoads

Nicholson St, Alexandra Pde, Brunswick St and St Georges Rd are managed by VicRoads, the state road authority. Another output of our LAPM study will be to provide a priority list of actions for advocacy to VicRoads. These actions include:

Brunswick St and St Georges Rd

We advocate to VicRoads for:

- A reduced speed limit
- Tram stop upgrades
- Widen crossing point at Capital City Trail to maximise capacity for pedestrians and bike riders crossing at the traffic lights
- Holden St intersection right turn signals in and out of Holden Street, bike signal head starts
- Scotchmer Street Intersection right turn signals in and out of Scotchmer Street, blke signal head starts
- Raised threshold treatments at side street intersections for improved pedestrian access.

Nicholson St

We advocate to VicRoads for

- A reduced speed limit
- Tram stop upgrades
- Widen crossing point at Capital City Trail to maximise capacity for pedestrians and bike riders crossing at the traffic lights
- Raised threshold treatments at side street intersections for improved pedestrian access.

Alexandra Pde

 More time for pedestrians and trams to cross Alexandra Pde on Nicholson St and Brunswick St

Agenda Page 109 Attachment 2 - Local Area Place Making LAPM Policy 2017



Local Area Place Making Policy 2017

Approval Date: XX/XX/2017

Next Review Date: XX/XX/XXXX

1. Introduction

Council's Local Area Place Making (LAPM) Policy 2017 supersedes the Local Area Traffic Management (LATM) Policy first adopted on 20 May 2014.

The change from Traffic Management to Place Making reflects the broader place making approach when undertaking traffic studies in the City of Yarra.

This policy:

- Aims to ensure a consistent, fair and comprehensive approach to the investigation, consultation, designs, implementation and monitoring of Local Area Place Making (LAPM) schemes.
- Establishes a framework to ensure the best use of Council's limited funding and resources.
- Ensures effective collaboration with the community and within Council.
- Demonstrates a commitment to reduce the adverse impact of motor vehicles in the local street network and to improve road safety and community amenity.

2. Background

Place making is a collaborative approach to creating public spaces that best meets the needs and aspirations of the community.

Engagement with the community forms the most important part of place making. The community are best placed to identify the issues and needs in their neighbourhood, and are vital in shaping the final outcome.

Place making also encourages greater collaboration between all areas of Council to ensure the best outcome is achieved.

Council's previous LATM Policy achieved the place making aspirations through its community engagement and collaboration within Council. The previous LATM schemes delivered benefits beyond road safety including, but not limited to, improvements to walking and cycling, landscaping and streetscapes, parking management, and water sensitive urban design.

The LAPM Policy will continue to be administered by Council's Traffic and Special Projects team. Consequently there remains a strong emphasis on traffic calming and improving streetscapes for pedestrians, cyclists and vulnerable road users.

3. Policy Context

The strategic context for decision making on LAPM is provided by the City of Yarra's Safe Travel Strategy 2016, Council Plan 2013-17, the Strategic Transport Strategy (Actions updated 2012), Encouraging and Increasing Walking Strategy, the Parking Strategy, and the Bicycle Strategy.

Responsible Officer: Manager Traffic and Special Projects Document Name: Local Area Place Making Policy 2017 **Page** 1 / 10

Agenda Page 110 Attachment 2 - Local Area Place Making LAPM Policy 2017



4. Scope and Process of Local Area Place Making

The City of Yarra has been divided into 21 local precincts, which are bounded by arterial roads or other physical barriers such as creeks or rivers. (Figure 1)

LAPM is primarily focussed on the planning and management of Council roads within these local precincts. Rather than dealing with specific sites or isolated streets LAPM considers neighbourhood traffic related issues and any proposed solutions in the context of the local precinct.

The LAPM process aims to improve places for people by managing vehicle traffic and improving conditions for pedestrians and cyclists. Methods to achieve this include:

- Reducing the speed of motor vehicles through physical and streetscape treatments
- Diversion treatments - such as entry and turn bans, partial and full road closures
- New pedestrian and cycling facilities
- Signs, line marking and other treatments ٠

Key to the success of LAPM is strong collaboration between the community and all parts of Council.

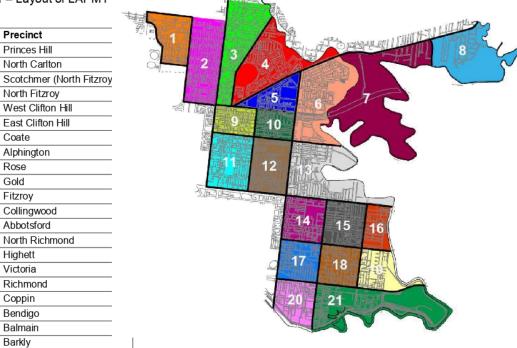


Figure 1 - Layout of LAPM P

Precinct

Coate

Rose

Gold Fitzroy

Highett

Victoria

Coppin

Bendigo

Balmain

Barkly

No.

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Responsible Officer: Manager Traffic and Special Projects Document Name: Local Area Place Making Policy 2017

Page 2 / 10

Agenda Page 111 Attachment 2 - Local Area Place Making LAPM Policy 2017



Local Area Place Making Policy 2017

Approval Date: XX/XX/2017

5. LAPM Prioritisation Process

LAPM typically begins as a result of community requests to reduce traffic volume and speeds in local streets and to improve safety and access for all road users.

In response, an evidence based assessment is made by Council officers that may include:

- Site inspections
- Field tests from the perspective of all road users
- Review of available traffic data and past feedback to Council
- Crash history assessment.

Officers will then refer the request or complaint to the LAPM priority ranking list. However if an immediate safety issue is identified, officers may decide to bypass the LAPM process and consider another appropriate course of action.

A warrant system is used to determine the priorities and rank the 21 local precincts in Yarra. The following criteria are assessed:

- Casualty crashes any reported fatalities, serious injuries and other injuries in the last five years on local streets or intersections
- Traffic speed any local street with an 85th percentile speed generally greater than 44 km/h
- Traffic volume any local street with an average weekday traffic volume generally greater than 1,000 vehicles
- Through traffic any local street with a peak hour to 24 hour volume ratio generally in excess of 14 per cent
- Heavy vehicles any local street with a proportion of commercial vehicles to all traffic generally in excess of five per cent
- Activity land use generators (e.g. hospitals and schools) considered in terms of likely pedestrian and bicycle generation, especially by vulnerable road users
- Complaints expressed by the number of received emails, letters, petitions received by Council.

Other considerations include road hierarchy, land use and public transport. Some local streets serve as higher-order collector roads and therefore carry higher traffic volumes. Some local streets service commercial and industrial areas which therefore experience higher proportions of heavy vehicles. Some local streets cater for trams and buses.

In November officers will review the LAPM priority ranking list to allocate budget for LAPM schemes for the next financial year. Councillors are advised accordingly.

Officers will undertake studies on two LAPM precincts each financial year. This process takes an integrated approach which includes engagement and active participation by the community, resolution of broad traffic issues and development of appropriate treatments. The recommended treatments proposed by a LAPM study, known as a LAPM scheme, will generally be constructed in the following financial year subject to Council adoption and funding availability.

Responsible Officer: Manager Traffic and Special Projects Document Name: Local Area Place Making Policy 2017 **Page** 3 / 10

Next Review Date: XX/XX/XXXX

Agenda Page 112 Attachment 2 - Local Area Place Making LAPM Policy 2017



Local Area Place Making Policy 2017

Approval Date: XX/XX/2017

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Next Review Date: XX/XX/XXXX
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6. Community Engagement

The LAPM study involves extensive engagement with the local community to identify local traffic and parking issues, a review of complaints, and evidence-based assessments undertaken by Council officers and independent traffic consultants.

Parking issues in relation to safety and traffic flow, parking restrictions, parking supply and parking enforcement are considered with any LAPM study.

Council's Community Engagement Policy 2014 oversees the engagement process.

Local residents and businesses are involved with identifying issues and needs for their precinct, and help to shape the final LAPM scheme. This fosters a greater acceptance and approval for a LAPM scheme.

The typical LAPM community engagement process is summarised below and is shown as a flow chart in Figure 2.

Initial Community Circular

A community circular is distributed to all owners and occupiers in the precinct. Community members are invited to complete a survey on traffic, parking and travel in their area and to seek opinion on a speed restraining device, such as a road hump, being located next to their properties. The survey will also invite community volunteers to be involved in the Local Area Study Group (LASG). Input is also sought within Council and its committees such as the Bicycle Advisory Committee, Disability Advisory Committee, Business Advisory Group, Strategic Transport team and Open Space team.

Community Meeting

A forum to outline the LAPM process and to identify and discuss local traffic, parking, travel and streetscape issues. A charter will be made available which outlines the tasks and responsibilities of the community volunteers on the LASG.

Local Area Study Group Meetings

Three LASG meetings are held. These meetings will be attended by Ward Councillors, one of which shall chair the meeting, around 12 community volunteers, Council officers and an independent traffic consultant.

Local Area Study Group Meeting #1

The LASG to identify and prioritise key local area issues based on the initial community survey, review of Council files, data collection and evidence-based assessments.

Local Area Study Group Meeting #2

Traffic consultant presents LAPM treatments to the LASG for consideration. This may include a range of options to address specific issues. The advantages and disadvantages of various treatments will be discussed in accordance with the selection criteria as outlined in the Policy. The objective of this meeting is to gain committee support for a proposed LAPM scheme which is distributed to the community for public comment. This scheme is also distributed throughout Council for comment.

Community Circular #2

A community circular, which details the objectives and specific treatment locations of the proposed LAPM scheme is sent to all owners and occupiers. Community members are invited to complete a survey on LAPM treatments in their area. Input is also sought within Council and its committees.

Responsible Officer: Manager Traffic and Special Projects Document Name: Local Area Place Making Policy 2017 Page 4 / 10

Agenda Page 113 Attachment 2 - Local Area Place Making LAPM Policy 2017



Local Area Place Making Policy 2017

Approval Date: XX/XX/2017

Next Review Date: XX/XX/XXXX

Local Area Study Group Meeting #3

Traffic Consultant outlines the received community responses to the LASG and final recommendations of the LASG will be sought. In its assessment, the LASG will review the three different levels of support for each proposed treatment. This includes:

Overall Support – the percentage support from all responses to the survey for each individual treatment. If a majority of overall support for an individual treatment is received, the LASG will then refer to the 'street' level support.

Street Level Support – the percentage support from the survey response from the street with the proposed treatment. If a majority of street level support for an individual treatment is received, the LASG will then refer to the 'adjacent properties' level support.

Adjacent Properties – the number of properties directly adjacent to the proposed treatment who 'support' and 'do not support' the proposed treatment. If no objections are received by any adjacent property to each proposed treatment, the LASG will include this into the final LAPM scheme. If an objection is received from any adjacent properties, the LASG may consider an alternative proposal in consultation with affected property owner(s) and consideration of the wider support from the street level and/or overall.

Recommended LAPM scheme

Advise all owners and occupiers of the recommended final LAPM scheme and Council Meeting date where LAPM scheme will be considered by Council.

Council Meeting

Council to consider recommendations, make amendments if necessary and endorse the LAPM scheme.

Monitoring and Review

Each LAPM scheme will be reviewed 12 months after implementation to monitor effectiveness. When finalised, the community will be advised that the results of the review are on Council's website.

Councillors and the LASG will also be advised of the results.

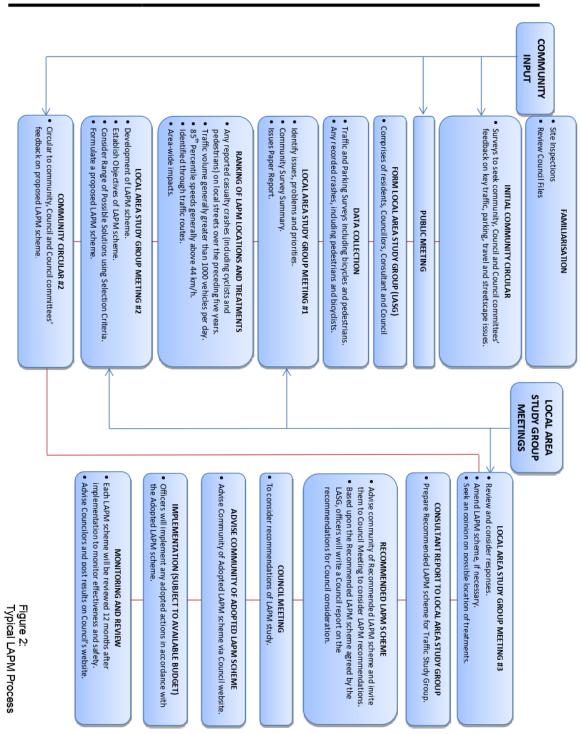




Local Area Place Making Policy 2017

Approval Date: XX/XX/2017

Next Review Date: XX/XX/XXXX







Local Area Place Making Policy 2017

7. Criteria for Selection of Treatments and Examples

Tables 1 to 4 provide a description of accepted treatments and their effectiveness, as summarised from AustRoads Guide to Traffic Management Part 8: Local Area Traffic Management (2008). This information assists the LASG to determine the appropriate treatments based on various criteria.

	Treatments	Reduce speeds	Reduce traffic volume	Reduce crash risk	Increase pedestrian safety	Increase bicycle safety	Loss of parking
	1) Road Cushions (bus routes)	Yes	Yes	Yes	Yes	Yes	No
Vertical deflection	2) Road Humps	Yes	Yes	Yes	Yes	Yes	No
treatments	3) Wombat Crossings	Yes	Yes	Yes	Yes	Yes	Yes
rearments	4) Raised Intersections	Yes	Yes	Yes	Yes	Yes	No

Table 1: Criteria for the selection and use of vertical deflection treatments



Responsible Officer: Manager Engineering Services Document Name: Infrastructure – Local Area Place Making Policy 2017

Agenda Page 116 Attachment 2 - Local Area Place Making LAPM Policy 2017



Local Area Place Making Policy 2017

Approval Date: XX/XX/2017

Next Review Date: XX/XX/XXXX

Table 2: Criteria for the selection and use of horizontal deflection treatments

Treatments		Reduce speeds	Reduce traffic volume	Reduce crash risk	Increase pedestrian safety	Increase bicycle safety	Loss of parking
Horizontal	1) Lane Narrowing/Kerb Extensions	Yes	No	Yes	Yes	Yes	Yes
deflection	2) Slow Points	Yes	Yes	Yes	Yes	No	Yes
	3) Mid-block median treatments	Yes	No	Yes	Yes	Yes	No
treatments	4) Roundabouts	Yes	Yes	Yes	Yes	No	Yes
	5) On-road landscaping	Yes	No	Yes	Yes	Yes	Yes





Agenda Page 117 Attachment 2 - Local Area Place Making LAPM Policy 2017



Local Area Place Making Policy 2017

Approval Date: XX/XX/2017

Next Review Date: XX/XX/XXXX

Table 3: Criteria for the selection and use of diversion treatments

Treatments		Reduce speeds	Reduce traffic volume	Reduce crash risk	Increase pedestrian safety	Increase bicycle safety	Loss of parking
	1) Full Road Closure	No	Yes	Yes	Yes	Yes	Yes
Diversion	2) Half Road Closure	No	Yes	Yes	Yes	Yes	Yes
treatments	3) Modified 'T' Intersection	Yes	No	Yes	Yes	No	Yes
	4) Left-in/Left-out Islands	No	Yes	Yes	Yes	Yes	Yes



Agenda Page 118 Attachment 2 - Local Area Place Making LAPM Policy 2017



Local Area Place Making Policy 2017

Approval Date: XX/XX/2017

Next Review Date: XX/XX/XXXX

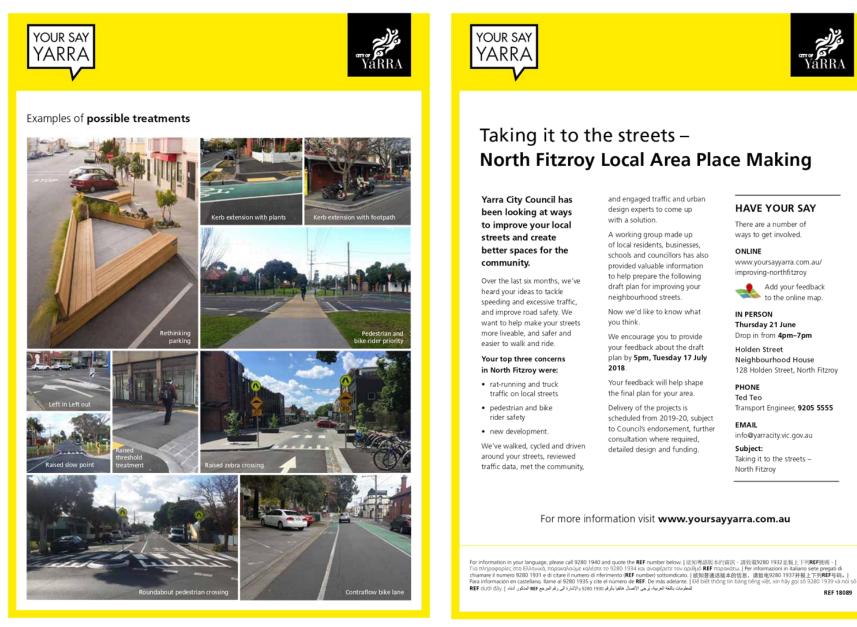
Table 4: Criteria for the selection and use of signs, line marking and other treatments

	Treatments	Reduce speeds	Reduce traffic volume	Reduce crash risk	Increase pedestrian safety	Increase bicycle safety	Loss of parking
	1) Speed Limit Signs and Markings	Yes	No	Yes	Yes	Yes	No
	2) Prohibited Traffic Movement Signs	No	Yes	Yes	Yes	Yes	No
Signs, line	3) One-Way (Street) Signs	No	Yes	Yes	Yes	Yes	No
marking and	4) Give Way Signs	Yes	Yes	Yes	Yes	Yes	No
other	5) Stop Signs	Yes	Yes	Yes	Yes	Yes	No
	6) Marked Pedestrian Crossings	Yes	No	Yes	Yes	Yes	Yes
treatments	7) Shared Zones	Yes	Yes	Yes	Yes	Yes	Yes
	8) Threshold Treatments	Yes	Yes	Yes	Yes	No	No
	9) Bicycle Facilities	No	No	Yes	No	Yes	Yes





Agenda Page 119 Attachment 3 - Taking it to the streets - North Fitzroy LAPM Draft LAPM plan brochure June 2018



Agenda Page 120 Attachment 3 - Taking it to the streets - North Fitzroy LAPM Draft LAPM plan brochure June 2018



Petition to the Yarra City Council

Brunswick Street North street closure

We, the undersigned (*residents, property owners & concerned citizens*) wish to inform the Yarra City Council of our support for the closure of Brunswick Street North as per the current 'North Fitzroy Local Area Place Making' proposal.

We ask that the Yarra City Council close Brunswick Street North north of Park Street to address ongoing and increasing 'rat-running' through our street

	Name (Print)	Address (Print)	Signature
1.	DANID CHALMERS	862 Bronswack St	De
2.	S. HEALLEWOOD	852 BRONSWICK ST	1. My ara
3.	S MARTINO	820 BRUNSLICE ST NTH	The
4.	M. AN Der Son	250 BRUNSWICK ST MM	ma
5.	A orbrien	548 BARSWIK ST DH	stpe.
6.	D GURTNEY	842 Brunswick St NM	Maria
7.	R. JACK	846 BRUNSWICK ST NTH	Khill J
8.	M. Willcax	846 BRUNSWICKST	vala_
9.	F Parcetter	356 Brunsuck St.	an-
10.	E ARRIAGADA	834 BRANSWICKST NTH	- 6.6.8
11.	LISA BURNE	BRY DEVINSVILLE ST NTH	abyrn
12.	Michale Cour	836 Brunswich St With	al-STE 1
13.	A SHUTTER.	\$20 Brunnicket Asta 1	Aluca Lac
14.	GOROBB	828 11 11 11	Elexalit
15.	ATOLES	826 Brunszick St	ACO

Petition to the Yarra City Council

Brunswick Street North street closure

We, the undersigned (*residents, property owners & concerned citizens*) wish to inform the Yarra City Council of our support for the closure of Brunswick Street North as per the current 'North Fitzroy Local Area Place Making' proposal.

We ask that the Yarra City Council close Brunswick Street North north of Park Street to address ongoing and increasing 'rat-running' through our street

	Name (Print)	Address (Print)	Signature
1.	Cameron Hughterson	814 BRUNSWICK St North	A la h
2.	PRIVA JOHAL	80% BINASWICK ST North	papel.
3.	JON WORPOLE	806 BRUNSWICKS ST N	1200
4.	GLENN MORRISON	842 BRUNSWICK ST NORTH	Creat real
5.	CHRIS PERPET	35 stuch me st work	KAU
6.	PLACE PLICE	834 Berntmen 55 MAI AME	Plu -
7.	Annette Teppes	862 Brunswich STN. H. Fit	19 company
8.	Donna Sheridan	1822 Bivnswick st Nh fitz	Hunithe
9.	Sam Zing	· · · · · · · · · · · · · · · · · · ·	Sam
10.	Kate Pleban	BOI BRIMIWICK St North, Fitz	Fallo
11.	TRICIN RANKIN	809 Brunswick St Wh Files	
12.	JULIA SARTESCHI	819 BEUNS WICK ST N NHHAIZ	1 1 12 . /
13.	SUCSIAN PENHA	1 R23 BRUNSWICK ST NIT. WIT	
14.	1.6	851 Krunswich SI Mich Fiter	*
15.		H H	

Petition to the Yarra City Council

Brunswick Street North street closure

We, the undersigned (*residents, property owners & concerned citizens*) wish to inform the Yarra City Council of our support for the closure of Brunswick Street North as per the current 'North Fitzroy Local Area Place Making' proposal.

We ask that the Yarra City Council close Brunswick Street North north of Park Street to address ongoing and increasing 'rat-running' through our street

	Name (Print)	Address (Print)	Signature
1.	MELLINA BARKER	8291 BRUKSWICK ST MOTOTH	sign
2.	nemen BRYANT	829 Bronswick St NHL	K.le
3.	JENNY FU	824 Brunswick St NFL	Jenmy Fu
4.	FRANCESIO FLORID	804 BUNSWUCST NH	Louin
5.	MARG JOHNSTON	\$17 BRUNSWICK ST WT	The for the
6.	DAVID VORENZ	a - a - a -	The
7.	Adde Diviratio	843 Brunswick St North	Han
8.	Amandaluca	827 Brunswich SF MASL	Kell and
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Attachment 4 - Brunswick Street North closure resident petition

Petition to the Yarra City Council

Brunswick Street North street closure

We, the undersigned (*residents, property owners & concerned citizens*) wish to inform the Yarra City Council of our support for the closure of Brunswick Street North as per the current 'North Fitzroy Local Area Place Making' proposal.

We ask that the Yarra City Council close Brunswick Street North north of Park Street to address ongoing and increasing 'rat-running' through our street

	Name (Print)	Address (Print)	Signature
I.	Patrick obeien	848 Brunswich ST. North	Detil:
2.	Penny Tolhost	859 Brownel & North	Pen Tol
	Terrifo Hallwight	835 Aurowek (rubite (Helle
4.	Christlanwigen	835 BRUNSWICK THIZON (anter
5.	WEGAN Micloningun	860 Brunswich St North	Mun
6.	Claire laidlar	840 Brunswick St North	Clew h
7.	Linda Crisp	833 Brunswich St NM	hin
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15.	·	B.L.	

Petition to the Yarra City Council

Brunswick Street North street closure

We, the undersigned (*residents, property owners & concerned citizens*) wish to inform the Yarra City Council of our support for the closure of Brunswick Street North as per the current 'North Fitzroy Local Area Place Making' proposal.

We ask that the Yarra City Council close Brunswick Street North north of Park Street to address ongoing and increasing 'rat-running' through our street

	Name (Print)	Address (Print)	Signature
1.	Tamara Worthin Mecon Osmen	K & B BRUNSWICK ST	que
2.	megen ognen	857 Burnarck St Nth	ergs
3.	Mopps chien	857 Bunanck St NM 857 Bunanck St NM	Taz
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Attachment 5 - Alternatives to Rae Street and Brunswick Street closure at Capital City Trail report - O'Brien Traffic November 2018



Suite 2.03, 789 Toorak Road Hawthorn East Victoria 3123 T: +61 3 9804 3610 W: obrientraffic.com

8 November 2018

Ted Teo City of Yarra PO Box 168 Richmond VIC 3121

Email: ted.teo@yarracity.vic.gov.au

Dear Ted

SCOTCHMER PRECINCT – NORTH FITZROY

I refer to your request for a traffic engineering assessment of the impact of various traffic management treatment options on the surrounding road network, specifically:

- 1. Right turn ban on Holden Street at Brunswick Street North;
- 2. Median island on Holden Street at Brunswick Street North (i.e. left-in/left-out only);
- Median island on Holden Street at Brunswick Street North and Dean Street (i.e. leftin/left out only);
- 4. Partial closures southbound closure on Rae Street and northbound closure on Brunswick Street North at the Capital City Trail; and
- 5. Full closure at Rae Street at the Capital City Trail and median island on Holden Street at Brunswick Street North and Dean Street.

In the course of preparing this letter we have undertaken turning movement counts at key intersections and determined the likely traffic distribution and implications for each treatment option. Our assessment is as follows.

BACKGROUND

Yarra City Council is undertaking a Local Area Place Making (LAPM) Study in the Scotchmer Precinct, North Fitzroy.

The study area is shown in **Figure 1** and is bounded by Nicholson Street, May Street, St Georges Road, Brunswick Street and Alexandra Parade in North Fitzroy.

TRAFFIC ENGINEERING TRAFFIC PLANNING ROAD SAFETY TRAFFIC IMPACT ASSESSMENTS TRANSPORT PLANNING ANDREW O'BRIEN & ASSOCIATES PTY LTD ABN: 55 007 006 037

Attachment 5 - Alternatives to Rae Street and Brunswick Street closure at Capital City Trail report - O'Brien Traffic November 2018



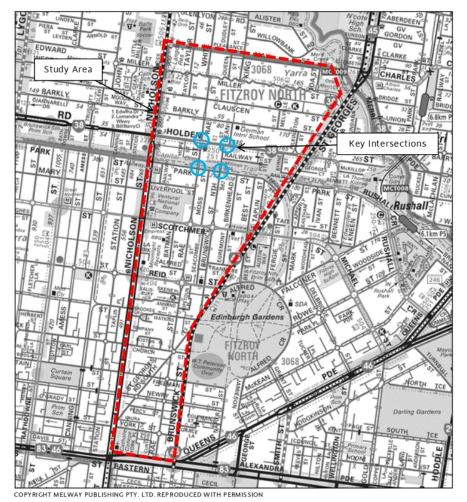


FIGURE 1: LOCATION OF SUBJECT SITE

As part of the LAPM Study, Council is considering various treatment options to reduce traffic volumes and rat running, and to improve pedestrian and cyclist safety, on Brunswick Street North and Rae Street in the vicinity of the Capital City Trail.

EXISTING CONDITIONS

Key Intersections

The Rae Street / Holden Street intersection is a cross intersection with Stop controls on Rae Street and a median island restricting access to left-in/left-out. An aerial photo of the intersection of Rae Street / Holden Street is shown in **Figure 2**.

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Attachment 5 - Alternatives to Rae Street and Brunswick Street closure at Capital City Trail report - O'Brien Traffic November 2018





FIGURE 2: AERIAL PHOTO OF RAE STREET / HOLDEN STREET INTERSECTION

The Rae Street / Park Street intersection is a cross intersection with Stop controls on Rae Street. Park Street permits one-way westbound traffic flow. An aerial photo of the intersection of Rae Street / Park Street is shown in **Figure 3**.



FIGURE 3: AERIAL PHOTO OF RAE STREET / PARK STREET INTERSECTION

The Brunswick Street North / Dean Street / Holden Street intersection is a staggered intersection with Stop controls on Brunswick Street North and Dean Street. An aerial photo of the intersection of Brunswick Street North / Dean Street / Holden Street is shown in **Figure 4**.

Attachment 5 - Alternatives to Rae Street and Brunswick Street closure at Capital City Trail report - O'Brien Traffic November 2018





FIGURE 4: AERIAL PHOTO OF BRUNSWICK STREET NORTH / DEAN STREET / HOLDEN STREET

The Brunswick Street North / Park Street intersection is a cross intersection with Give Way controls on Park Street. Park Street is restricted to one-way westbound to the west of Brunswick Street North and one-way eastbound to the east of Brunswick Street North. An aerial photo of the intersection of Brunswick Street North / Park Street is shown in **Figure 5**.



FIGURE 5: AERIAL PHOTO OF BRUNSWICK STREET NORTH / PARK STREET

Existing Traffic Volumes

O'Brien Traffic commissioned turning movement counts of the 4 key intersections on Thursday 18 October 2018 at the following times:

- 6:30am to 9:30am; and
- 3:30pm to 7:00pm.

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Attachment 5 - Alternatives to Rae Street and Brunswick Street closure at Capital City Trail report - O'Brien Traffic November 2018



These times were selected as they represent typical commuter peak periods.

The existing on-road peak hour volumes are presented in Figures 6 – 7.



FIGURE 6: EXISTING TRAFFIC VOLUMES - AM PEAK HOUR (7AM - 8AM)



FIGURE 6: EXISTING TRAFFIC VOLUMES - PM PEAK HOUR (5:30PM - 6:30PM)

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Attachment 5 - Alternatives to Rae Street and Brunswick Street closure at Capital City Trail report - O'Brien Traffic November 2018



TRAFFIC RE-DISTRIBUTION & IMPACT

For each treatment option, vehicles will be required to find an alternative route to their destination. Where there is a convenient alternative local route, the majority of vehicles will simply transfer to it. However, where the alternative local route is not convenient, a proportion of vehicles will re-route to the arterial road network.

In this case, it is assumed that 40% of traffic will transfer to the arterial network where an obvious alternative route is not available.

This is consistent with Council's findings of the Stage 1 Works Review – LATM 16 Victoria *Precinct (Richmond)* (Stage 1 constructed in 2014). This post construction evaluation found reductions in traffic volumes of 39-65% in streets where traffic movements were restricted (eg. left in/left out treatments) without an increase in traffic volumes on other local streets.

Based on the above, the likely traffic re-distribution of each traffic management option is discussed as follows.

Option 1 – Right turn ban from Holden Street to Brunswick Street North

Option 1 will ban the right turn from Holden Street in Brunswick Street North. This will impact vehicles currently travelling north to south via the Rae Street - Holden Street - Brunswick Street North route. It is likely that some vehicles currently utilising this route will transfer to Dean Street, turn right into Holden Street, then left into Brunswick Street North.

The following assumptions have been made in regard to the traffic re-distribution of Option 1:

- Traffic approaching from Holden Street west (Nicholson Street intersection) to turn right into Brunswick Street North will transfer to/stay on the arterial road network;
- Of the existing traffic turning left from Rae Street (north) into Holden Street, 90% currently turns right into Brunswick Street North and 10% continues eastbound on Holden Street;
- Of the existing traffic turning left from Rae Street (north) into Holden Street, then right into Brunswick Street North:
 - 40% will transfer onto the arterial roads
 - 60% will transfer onto Dean Street, turn right onto Holden Street, then left onto Brunswick Street North.

The traffic redistribution for Option 1, for the AM and PM peak hours, is shown in **Figures 7** and 8 respectively.

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Attachment 5 - Alternatives to Rae Street and Brunswick Street closure at Capital City Trail report - O'Brien Traffic November 2018

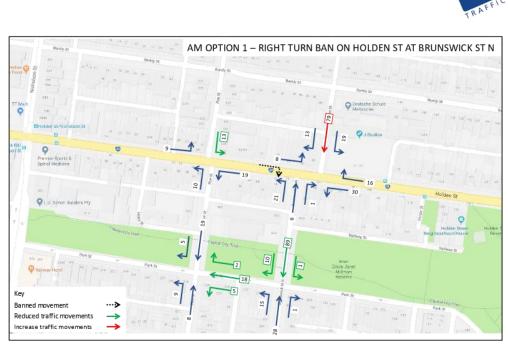


FIGURE 7: OPTION 1 - AM TRAFFIC RE-DISTRIBUTION



FIGURE 8: OPTION 1 - PM TRAFFIC RE-DISTRIBUTION

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Attachment 5 - Alternatives to Rae Street and Brunswick Street closure at Capital City Trail report - O'Brien Traffic November 2018



Option 2 - Median island on Holden Street at Brunswick Street North

Option 2 will install a median island on Holden Street at Brunswick Street North, restricting Brunswick Street North to left-in/left-out. This option will prevent the right turn movement into Brunswick Street North from Holden Street, similar to Option 1. In addition it will prevent vehicles travelling from south to north via the Brunswick Street North – Holden Street - Dean Street route. It is likely that vehicles travelling north will transfer onto Park Street to access St Georges Road / Nicholson Street.

The following assumptions have been made in regard to the traffic re-distribution of Option 2:

- Existing traffic turning right from Holden Street to Brunswick Street North will transfer as per Option 1.
- Of the existing traffic turning right from Brunswick Street North to Holden Street (including traffic continuing into Dean Street):
 - 40% will transfer onto the arterial network
 - 50% will transfer eastbound on Park Street to St Georges Road
 - 10% will transfer westbound on Park Street to Nicholson Street.

The traffic redistribution for Option 2, for the AM and PM peak hours, is shown in **Figures 9** and **10** respectively.

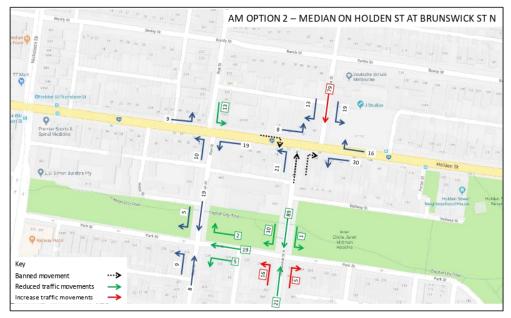


FIGURE 9: OPTION 2 - AM TRAFFIC RE-DISTRIBUTION

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Attachment 5 - Alternatives to Rae Street and Brunswick Street closure at Capital City Trail report - O'Brien Traffic November 2018



FIGURE 10: OPTION 2 - PM TRAFFIC RE-DISTRIBUTION

Option 3 – Median island on Holden Street at Brunswick Street North and Dean Street

Option 3 will install a median island on Holden Street at Brunswick Street North and Dean Street, restricting both streets to left-in/left-out only. This option will restrict vehicles from travelling between the north and south of Holden Street via the local road network. It is likely that vehicles travelling north will transfer onto Park Street and vehicles travelling south will transfer onto Barkley Street to access St Georges Road or Nicholson Street.

The following assumptions have been made in regard to the traffic re-distribution of Option 3:

- Existing traffic turning right from Holden Street to Brunswick Street North will transfer as per Options 1 and 2.
- Existing traffic turning right from Brunswick Street North to Holden Street (including traffic continuing to Dean Street) will transfer as per Option 2.
- Restricting Dean Street to left-in/left-out will transfer traffic currently travelling from the north side to the south side of Holden Street onto the arterial network. It is assumed that of the existing traffic currently travelling north to south:
 - 40% will transfer to (i.e. stay on) the arterial network without travelling through the local area;
 - 50% will transfer to Barkly Street and travel westbound to Nicholson Street;
 - 10% will transfer to Barkly Street and travel eastbound to St Georges Road.

O'BRIEN TRAFFIC 18138 SCOTCHMER LET: 08 NOVEMBER 2018

Attachment 5 - Alternatives to Rae Street and Brunswick Street closure at Capital City Trail report - O'Brien Traffic November 2018



- Of the existing traffic turning right into Dean Street from Holden Street:
 - 40% will transfer to the arterial network;
 - 60% will transfer to Barkly Street (westbound from St Georges Road).

The traffic redistribution for Option 3, for the AM and PM peak hours, is shown in **Figures 11 and 12** respectively.

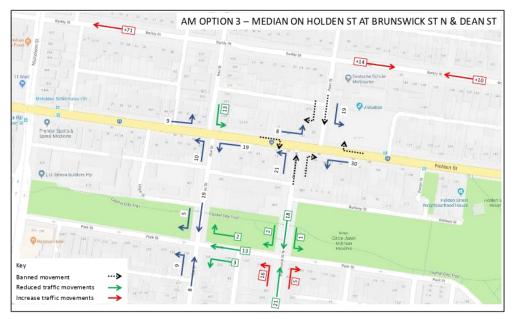


FIGURE 11: OPTION 3 - AM TRAFFIC RE-DISTRIBUTION

Attachment 5 - Alternatives to Rae Street and Brunswick Street closure at Capital City Trail report - O'Brien Traffic November 2018



FIGURE 12: OPTION 3 - PM TRAFFIC RE-DISTRIBUTION

Option 4 – Rae Street southbound closure and Brunswick Street North northbound closure at Capital City Trail

Option 4 will close Rae Street to southbound traffic and close Brunswick Street North to northbound traffic at the Capital City Trail. This option is anticipated to shift existing northbound/southbound traffic between Rae Street and Brunswick Street North based on the new one-way traffic flows.

The following assumptions have been made in regard to the traffic re-distribution of Option 4:

- Existing traffic turning right from Brunswick Street North into Holden Street (including into Dean Street) will transfer as per Option 2.
- Existing traffic turning left from Brunswick Street North into Holden Street will transfer to Rae Street;
- Existing traffic turning left from Holden Street into Rae Street will turn left into Brunswick Street North.

The traffic redistribution for Option 4, for the AM and PM peak hours, is shown in **Figures 13 and 14** respectively.

O'BRIEN TRAFFIC 18138 SCOTCHMER LET: 08 NOVEMBER 2018

Attachment 5 - Alternatives to Rae Street and Brunswick Street closure at Capital City Trail report - O'Brien Traffic November 2018



FIGURE 13: OPTION 4 - AM TRAFFIC RE-DISTRIBUTION

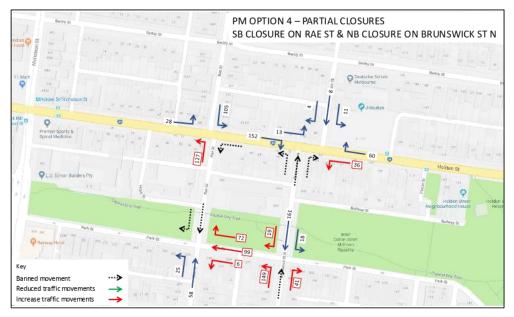


FIGURE 14: OPTION 4 - PM TRAFFIC RE-DISTRIBUTION

Attachment 5 - Alternatives to Rae Street and Brunswick Street closure at Capital City Trail report - O'Brien Traffic November 2018



Option 5 – Rae Street closure at Capital City Trail and median island on Holden Street at Brunswick Street North and Dean Street

Option 5 will close Rae Street at the Capital City Trail and install a median island on Holden Street at Brunswick Street North and Dean Street, restricting traffic movements at both streets to left-in/left-out only. This option will restrict vehicles from travelling between the north and south of Holden Street via the local road network and transfer existing traffic travelling north/south on Rae Street to Brunswick Street North.

The following assumptions have been made in regard to the traffic re-distribution of Option 5:

- Existing traffic turning right from Brunswick Street North into Holden Street (including traffic continuing into Dean Street) will transfer as per Option 2;
- Existing traffic travelling from north of Holden Street (via Rae street or Dean Street) to south of Holden Street will transfer as per Option 3;
- Existing traffic turning left at Rae Street will transfer to Brunswick Street North.

The traffic redistribution for Option 5, for the AM and PM peak hours, is shown in **Figures 15 and 16** respectively.



FIGURE 15: OPTION 5 - AM TRAFFIC RE-DISTRIBUTION

Attachment 5 - Alternatives to Rae Street and Brunswick Street closure at Capital City Trail report - O'Brien Traffic November 2018



FIGURE 16: OPTION 5 - PM TRAFFIC RE-DISTRIBUTION

COMPARISON OF TREATMENT OPTIONS

Pros and Cons

The pros and cons of each treatment option is summarised in Table 1 below.

Attachment 5 - Alternatives to Rae Street and Brunswick Street closure at Capital City Trail report - O'Brien Traffic November 2018



TREATMENT OPTION	PROS	CONS
Option 1: Turn ban on Holden Street at Brunswick Street North Option 2: Median island on Holden Street at Brunswick Street North	 Reduces peak hour traffic volumes on Rae St (north of Holden St) and Brunswick St N (south of Holden St) Maintains accessibility between the local areas north and south of Holden St Reduces peak hour traffic volumes on Rae St (north of Holden St) and Brunswick St N (south of Holden St) Maintains accessibility from the local area north of Holden St to the local area south of Holden St 	 Increases peak hour traffic volumes on Dean St Increases right turn movement from Dean St to Holden St Does not stop the north-south/south- north rat-run Increases peak hour volumes on Dean St and Park St Does not stop the south-north rat-run Increases right turn volumes on Dean St
Option 3: Median island on Holden Street at Brunswick Street North and Dean Street	 Reduces peak hour traffic volumes on Rae St (north of Holden St), Brunswick St N (south of Holden St) and Dean St (north of Holden St) Removes all rat-run routes between the local areas north and south of Holden St 	 Increases peak hour volumes on Barkly St No vehicle accessibility between the local areas north and south of Holden St
Option 4: Rae Street southbound closure and Brunswick Street northbound closure at Capital City Trail	 Simplifies intersections on Capital City Trail (one-way vehicular traffic only) Removes the rat-run route from the local area south of Holden St to the local area north of Holden St 	 Transfers northbound traffic on Brunswick St N to Rae St and southbound traffic on Rae St to Brunswick St N No vehicle accessibility from the local area south of Holden St to the local area north of Holden St
Option 5: Rae Street closure at Capital City Trail and median island on Holden Street at Brunswick Street North and Dean Street	 Removes Rae Street intersection from Capital City Trail Reduces peak hour traffic volumes on Rae St (north and south of Holden St), Brunswick St N (south of Holden St) and Dean St (north of Holden St) Removes all rat-run routes between the local areas north and south of Holden St 	 Increases peak hour volumes on Barkly St No vehicle accessibility between the local areas north and south of Holden St

TABLE 1: OPTION COMPARISON

Impact on Brunswick Street North and Rae Street traffic volumes

The likely change in traffic volumes in Brunswick Street North and Rae Street for each treatment option is summarised in Table 2.

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Attachment 5 - Alternatives to Rae Street and Brunswick Street closure at Capital City Trail report - O'Brien Traffic November 2018



TREATMENT OPTION	LIKELY CHANGE IN TRAFFIC VC	DLUMES (APPROXIMATE)
	BRUNSWICK STREET NORTH	RAE STREET
Option 1: Turn ban on Holden Street at Brunswick Street North	25% decrease	No change
Option 2: Median island on Holden Street at Brunswick Street North	33% decrease	No change
Option 3: Median island on Holden Street at Brunswick St North and Dean St	50% decrease	No Change
Option 4: Rae Street southbound closure and Brunswick Street northbound closure at Capital City Trail	40% decrease north of Park St Minimal change south of Park St	100% increase
Option 5: Rae Street closure at Capital City Trail and median island on Holden Street at Brunswick Street North and Dean Street	30% decrease	All through traffic removed

TABLE 2: IMPACT ON BRUNSWICK STREET NORTH TRAFFIC VOLUMES

CONCLUSION

The above analysis provides an assessment of the traffic impacts of various treatment options for Rae Street and Brunswick Street North in the vicinity of the Capital City Trail for consideration as part of the Local Area Place Making Study.

Should you have any queries, please do not hesitate to contact me on 9804-3610.

Yours sincerely

O'BRIEN TRAFFIC

lacan

Jemima Macaulay Senior Associate

Attachment 6 - Alternative options for Rae Street and Brunswick Street North closure at Capital City Trail November 2018



Attachment 6 - Alternative options for Rae Street and Brunswick Street North closure at Capital City Trail November 2018



Attachment 6 - Alternative options for Rae Street and Brunswick Street North closure at Capital City Trail November 2018



Attachment 6 - Alternative options for Rae Street and Brunswick Street North closure at Capital City Trail November 2018



Attachment 6 - Alternative options for Rae Street and Brunswick Street North closure at Capital City Trail November 2018

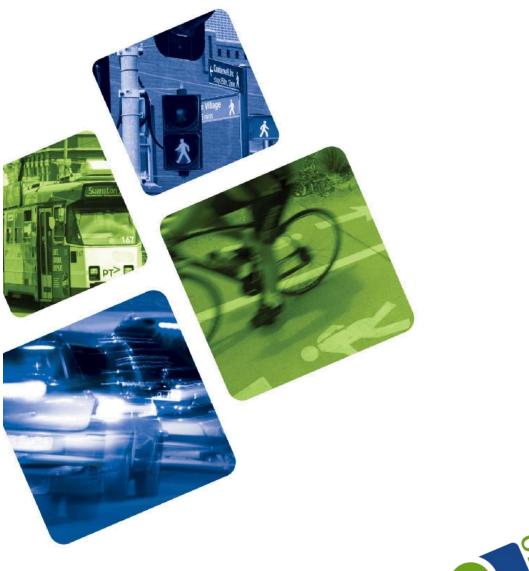


Attachment 6 - Alternative options for Rae Street and Brunswick Street North closure at Capital City Trail November 2018

Summary of pros and cons of alternative options to close Rae St and Brunswick St North at the Capital City Trail

OPTION	DESCRIPTION	PROS	CONS
Option 1	Right turn bans in the morning and afternoon peak from Holden St into Brunswick St North	 Reduces peak hour traffic volumes on Rae St (north of Holden St) and Brunswick St North (south of Holden St) Maintains accessibility between the local areas north and south of Holden St 	 Increases peak hour traffic volumes on Dean St, Barkly St Increases right turn movement from Dean St to Holden St High likelihood of non-compliance by motorists if not regularly enforced by Police
Option 2	Median island on Holden St at Brunswick St North preventing right turns in and out	 Reduces traffic volumes on Rae St (north of Holden St) and Brunswick St North (south of Holden St) Maintains accessibility from the local area north of Holden St to the local area south of Holden St 	 Increases traffic volumes on Dean St, Barkly St and Park St Increases right turn volumes on Dean St
Option 3	Median island on Holden St at Brunswick St North and Dean St preventing right turns in and out	 Reduces peak hour traffic volumes on Rae St (north of Holden St), Brunswick St North (south of Holden St) and Dean St (north of Holden St) Removes all shortcut routes between the local areas north and south of Holden St 	 Increases traffic volumes on Barkly St Changed local access to schools, child care centres and residential properties north of Holden St No vehicle accessibility between the local areas north and south of Holden St
Option 4	Rae St closure at Capital City Trail and Brunswick St North northbound closure at Park St	 Simplifies intersections on Capital City Trail Removes the shortcut route from the local area south of Holden St to the local area north of Holden St 	 Increases traffic volumes on Park St Increases right turn movement from Park St to Nicholson St No vehicle accessibility from the local area south of Holden St to the local area north of Holden St
Option 5	Combined full closure of Rae St and Brunswick St North at the Capital City Trail	 Removes the shortcut route from the local area south of Holden St to the local area north of Holden St Removes conflict between motorists and pedestrians/cyclists at Capital City Trail crossings 	 Increases traffic volumes on Park St May increase traffic volumes on east-west streets such as Holden St and Scotchmer St in the short term No vehicle accessibility from the local area south of Holden St to the local area north of Holden St

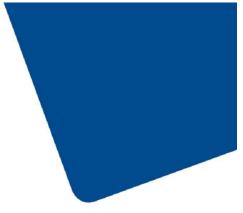
Agenda Page 148 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019





LOCAL AREA PLACE MAKING STUDY

SCOTCHMER LOCAL AREA PLACE MAKING STUDY SCOTCHMER PRECINCT, NORTH FITZROY 17 JANUARY 2019



SCOTCHMER PRECINCT, NORTH FITZROY

CLIENT: Yarra City Council

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Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019

CONTENTS

1	INTRODUCTION	4
2	STUDY AREA	4
3	EXISTING CONDITIONS	5
4	IDENTIFICATION OF ISSUES AND PRIORITIES	11
5	DEVELOPMENT OF POTENTIAL TREATMENTS	15
6	FURTHER CONSULTATION: RAE ST AND BRUNSWICK ST NTH	19
7	DRAFT LAPM SCHEME	20
8	INDICATIVE COST ESTIMATES	22
9	VICROADS ADVOCACY PROJECTS	23
10	CONCLUSION	24
APPE	NDIX A	25
APPE	NDIX B	27
APPENDIX C		29
APPE	NDIX D	31
APPE	NDIX E	33
APPE	NDIX F	39
APPE	NDIX G	43
APPE	NDIX H	52
APPE	NDIX I	69
APPE	L XIDN	76



1 INTRODUCTION

O'Brien Traffic has been engaged by Yarra City Council to undertake a Local Area Place Making Study in the Scotchmer Precinct, North Fitzroy.

The Study has been undertaken in accordance with Council's Local Area Place Making Policy 2017.

Local Area Place Making (LAPM) is primarily focussed on the planning and management of Council roads. It aims to improve places for people by managing vehicle traffic and improving conditions for pedestrians and cyclists.

Engagement with the community is a key component of the LAPM process. The input of the community in identifying the issues and needs in their neighbourhood, together with analysis of traffic data and crash data, forms the basis for the development of the LAPM scheme.

2 STUDY AREA

The study area is bounded by Nicholson Street, May Street, St Georges Road, Brunswick Street and Alexandra Parade in North Fitzroy, as shown in **Figure 1**.

The study area is predominantly residential, with the exception of:

- retail/commercial uses fronting Nicholson Street and St Georges Road;
- retail/commercial uses at the eastern end of Scotchmer Street, including Piedimonte's Supermarket;
- Merri Creek Primary School, located on the northern side of Miller Street;
- Simmonds Catholic College, on the north-eastern corner of Nicholson Street and Alexandra Parade; and
- the Capital City Trail and adjacent Janet Millman Reserve.

Agenda Page 152 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019

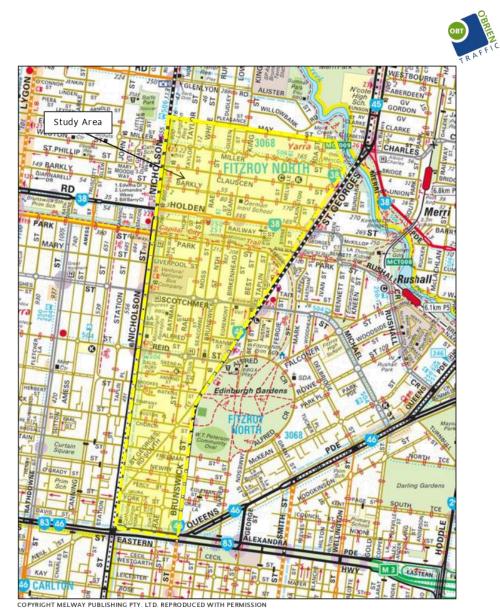


FIGURE 1: LOCATION OF STUDY AREA

3 EXISTING CONDITIONS

3.1 STREET NETWORK

3.1.1 Arterial roads

Nicholson Street, Brunswick Street, St Georges Road and Alexandra Parade are located on the boundary of the study area. These roads are Primary Arterial Roads under the management of VicRoads.

Council typically do not undertake works on VicRoads' roads, however can advocate to

O'BRIEN TRAFFIC 18138 SCOTCHMER REP: SCOTCHMER PRECINCT, NORTH FITZROY : 17 JANUARY 2019

5

Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



VicRoads for works to be undertaken.

The arterial roads are described below.

Nicolson Street, south of Holden Street, provides two traffic lanes and marked kerbside parking bays in each direction, separated by central tram tracks located within a segregated carriageway. North of Holden Street, the carriageway narrows to provide two traffic lanes in each direction with central tram tracks. Kerbside parking is typically permitted outside of clearway times.

St Georges Road, within the study area, provides two traffic lanes and a bicycle lane in each direction with central tram tracks. Kerbside parking is permitted outside of clearway times (which obstructs the bicycle lanes).

Brunswick Street (south of St Georges Road) provides two traffic lanes and a bicycle lane in each direction with central tram tracks. Kerbside parking is permitted outside of clearway times (which obstructs the bicycle lanes).

Alexandra Parade, within the study area, provides four traffic lanes plus kerbside parking in each direction separated by a wide central median.

3.1.2 Major Council Roads and Collector Roads

The main east-west routes through the study area are Holden Street, Scotchmer Street and Reid Street. These streets are described below.

Holden Street is a Major Council Road. It runs in an (approximate) east-west orientation between Nicholson Street and St Georges Road and provides one traffic lane, a bicycle lane and a parking lane in each direction. Traffic signals are provided at its intersections with Nicholson Street and St Georges Road, with pedestrian signals provided approximately mid-way.

Scotchmer Street is a Collector Street under the care and management of Council. It runs in an (approximate) east-west orientation between Nicholson Street and St Georges Road and provides one traffic lane in each direction. Kerbside parking is a mix of angle (60 degree) and parallel parking. Roundabouts are provided on Scotchmer Street at the intersections of Rae Street and Brunswick Street North. Traffic signals are provided at its intersections with Nicholson Street and St Georges Road.

Reid Street is also a Collector Street under the care and management of Council. It runs in an (approximate) east-west orientation between Nicholson Street and St Georges Road and provides one traffic lane, a bicycle lane and a parking lane in each direction. Three speed humps are provided along Reid Street and a roundabout at its intersection with Rae Street. Traffic signals are provided at the intersection of Reid Street and Nicholson Street.

3.1.3 Local streets

The majority of streets within the study area are local streets. These are typically orientated approximately east-west or north-south. While most streets provide for two-way traffic movements, many streets are one-way only. Kerbside parking is typically permitted in most streets.

Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



Various traffic management treatments have been implemented in the local streets. These include speed humps, roundabouts, pedestrian crossings, one way-streets etc.

3.2 PUBLIC TRANSPORT ROUTES

Public transport routes are shown in Figure 2.



FIGURE 2: PUBLIC TRANSPORT ROUTES

3.2.1 Bus routes

Bus routes operate on Miller Street, Holden Street, Reid Street, St Georges Road (Reid Street to Scotchmer Street), and Alexandra Parade.

3.2.2 Tram routes

Nicholson Street, St Georges Road and Brunswick Street are tram routes.



3.3 BICYCLE ROUTES

Bicycle routes are shown in Figure 3.

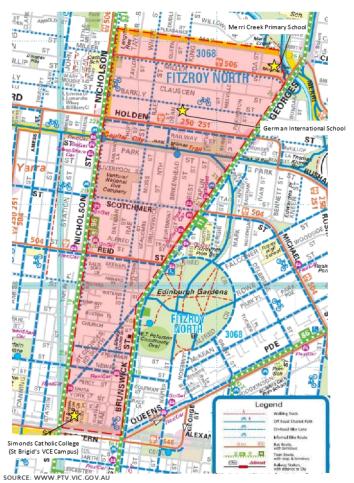


FIGURE 3: BICYCLE ROUTES

3.3.1 Off-road bike paths

The Capital City Trail runs between St Georges Road and Nicholson Street. This is a shared path that is popular with recreational users and commuters.

3.3.2 On-road bicycle routes

On-road bicycle paths are provided on Holden Street, Reid Street, Freeman Street, Newry Street and St Georges Road South.

Miller Street, Barkly Street, Scotchmer Street, Rae Street, White Street, King Street and Nicholson Street (north of Church Street) are informal bicycle routes.



3.4 TRAFFIC VOLUME AND SPEED DATA

Traffic volume and speed data was provided by Council and is presented diagrammatically in **Appendix A**. This includes

- Daily traffic volumes;
- AM and PM peak hour volumes; and
- 85th percentile speed¹ data.

3.4.1 Traffic Volumes

Most local streets within the study area have traffic volumes less than 1,000 vpd, with the exception of:

- Miller Street, Brunswick Street North and Rae Street with weekday traffic volumes in the order of 2-3,000 vpd;
- Barkly Street, Dean Street, Freeman Street, Newry Street and Birkenhead Street with weekday traffic volumes in the order of 1-2,000 vpd.

Holden Street, Scotchmer Street and Reid Street have higher traffic volumes, consistent with their function as Major Council Road/Collector Roads.

3.4.2 Vehicle Speeds

A 40 km/h speed limit applies to the study area.

The majority of streets have 85th percentile speeds less than 40 km/h.

Holden Street (Dean St – Pilkington Street) and Freeman Street (west of St Georges Road) have 85th percentile speeds of 51 km/h and 46 km/h respectively.

3.5 CASUALTY CRASH HISTORY

VicRoads casualty crash data for the period from 1 July 2012 to 30 June 2017 was provided by Council. The crash data is presented diagrammatically in **Appendix B.**

Figure 4 shows the location and number of vehicle, bicycle and pedestrian crashes in the area.

1. The speed at which 85% of all vehicles travel under.

Agenda Page 157 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



FIGURE 4: CASUALTY CRASHES - JANUARY 2012 TO JUNE 2017

Figure 4 shows that there has been a high proportion of bicycle crashes in the study area.

In particular, 3 bicycle crashes have occurred at the intersection of Brunswick Street North and the Capital City Trail. A further 2 bicycle crashes occurred at each of the Brunswick Street North/Scotchmer Street and Rae Street/Reid Street intersections, both of which are roundabouts.

No pedestrian casualty crashes were recorded in the five year period.

3.6 PEDESTRIAN AND CYCLIST COUNTS

Pedestrian and bicycle count data was provided by Council and is presented diagrammatically in **Appendix C**. The data includes pedestrian counts at key intersections and cyclist counts on Rae Street and Brunswick Street North at the Capital City Trail.



4 IDENTIFICATION OF ISSUES AND PRIORITIES

4.1 TRAFFIC VOLUME AND SPEED DATA

The traffic data for the local streets has been analysed against a set of criteria to identify streets where traffic volume, rat-running, vehicle speeds or heavy vehicles may be an issue of concern. The criteria are listed below:

- Traffic volume weekday traffic volume greater than 1,000 vpd;
- Through traffic (rat-running) AM or PM peak hour volumes greater than 14% of the daily volume;
- Vehicle speeds 85th percentile speed greater than 40 km/h; and
- Heavy vehicles proportion of heavy vehicles in excess of 5% of total traffic.

The traffic analysis is summarised in **Figure 5** and discussed further in Sections 4.1.1 to 4.1.4.

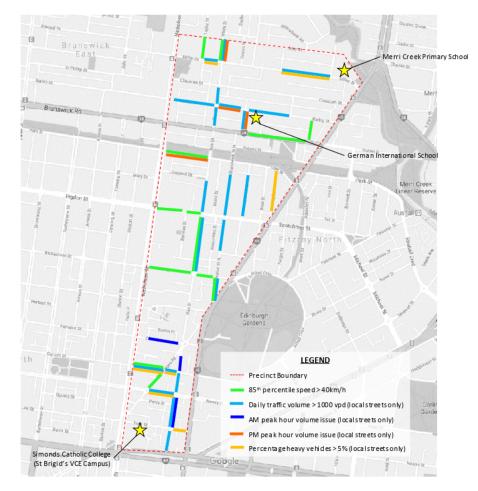


FIGURE 5: POTENTIAL ISSUES OF CONCERN - VEHICLE SPEEDS, TRAFFIC VOLUME, AM/PM PEAK PERIOD RAT-RUNNING, AND HEAVY VEHICLES

O'BRIEN TRAFFIC 18138 SCOTCHMER REP: SCOTCHMER PRECINCT, NORTH FITZROY : 17 JANUARY 2019

11



4.1.1 Daily Traffic Volumes

Typically, local streets with traffic volumes of 1,000 vehicles per day (vpd) or lower is considered acceptable.

The following local streets have weekday traffic volumes greater than 2,000 vpd:

- Miller Street (2,857 vpd);
- Brunswick Street North (1,818-2,728 vpd);
- Rae Street (638-2,480 vpd); and
- Barkly Street (1,401-2,142 vpd).

The following local streets have weekday traffic volumes between 1,000 and 2,000 vpd:

- Dean Street (1,464 vpd); and
- Freeman Street (1, 085 vpd);
- Newry Street (1,053 vpd); and
- Birkenhead Street (1,016 vpd).

Holden Street, Scotchmer Street and Reid Street have higher traffic volumes, consistent with their function as Major Council Road/Collector Roads.

4.1.2 Peak Hour Traffic Volumes

Where the peak hour traffic volume is greater than 14% of the daily traffic volume, through traffic or 'rat-running' is identified as a potential issue.

In the AM peak, this occurs on Church Street and Rae Street (south of St Georges Road).

In the PM peak, this occurs on White Street, Barkly Street, Dean Street and Park Street (west of Rae Street).

4.1.3 Vehicle Speeds

The majority of streets have 85^{th} percentile speeds less than 40 km/h (i.e. the speed limit).

The traffic data indicates 85th percentile speeds around 40-44km/h in the following streets - Taylor Street, White Street, Park Street (west of Rae Street), Pilkington Street, Scotchmer Street (west of Rae Street), Rae Street (Scotchmer Street - Reid Street), Reid Street, Brunswick Street North (south of Reid Street), and St Georges Road (Freeman Street to Newry Street).

Holden Street (Dean St – Pilkington Street) and Freeman Street (west of St Georges Road) have 85^{th} percentile speeds of 51 km/h and 46 km/h respectively.

4.1.4 Heavy Vehicles

Local streets should typically have a low proportion of heavy vehicles – unless they serve industrial areas or are bus routes. In the study area, traffic volumes comprise more than 5% heavy vehicles on the following streets:

Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



- Miller Street bus route;
- Taplin Street predominantly residential street with commercial uses at southern end; and
- Freeman Street, Newry Street and York Street residential streets.

4.2 CRASH DATA

The crash data indicated a high proportion of bicycle crashes in the study area during the five year period analysed. Multiple bicycle crashes occurred at the following locations:

- Brunswick Street North/Capital City Trail 3 bicycle crashes;
- · Brunswick Street North/Scotchmer Street roundabout 2 bicycle crashes; and
- Rae Street/Reid Street roundabout 2 bicycle crashes.

Two motor vehicle crashes occurred at the intersection of Rae Street and Freeman Street, which is a cross intersection controlled by stop signs.

4.3 STAGE ONE CONSULTATION – ISSUES AND IDEAS

Council consulted with the community at the commencement of the project to inform the community of the project and invite their input.

The Stage One Consultation comprised:

- Postcards delivered to residents and businesses;
- Information sessions; and
- On-line survey inviting participants to identify issues of concern and offer suggestions.

The on-line survey identified a range of issues including traffic volumes, rat-running, vehicle speeds, pedestrian and cyclist safety, pedestrian and cyclist infrastructure, and amenity.

4.4 OVERVIEW OF ISSUES

An overview of the issues identified from the traffic data, crash data and community consultation is presented in **Figure 6**.

Agenda Page 161



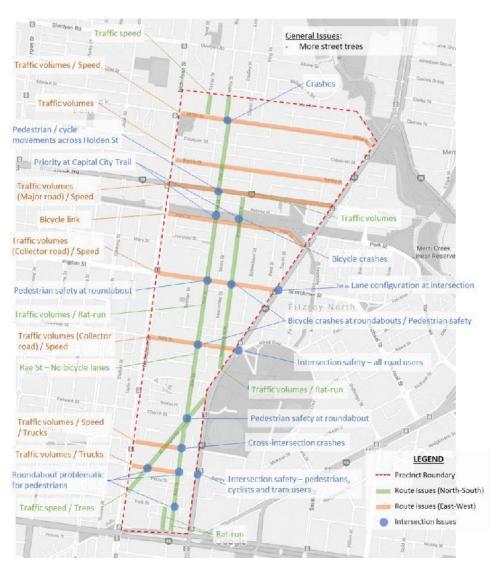


FIGURE 6: OVERVIEW OF TRAFFIC ISSUES IN THE STUDY AREA

4.5 STAGE TWO CONSULTATION – PRIORITIES

Following analysis of the traffic and crash data, and summation of the community input, Council undertook a second on-line survey to rank priorities. The results of the survey are provided in **Appendix D**.

The highest priority Council roads, and the issues of concern, were identified as follows:

 Scotchmer Street – traffic volumes, pedestrian/cyclist safety at roundabouts, issues at Piedemonte's, cycling facilities, trucks.

Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



- Rae Street traffic volumes and rat-running, cycling facilities, Capital City Trail crossing, pedestrian/cyclist safety at roundabouts, pedestrian/cyclist safety at Holden Street, crashes at Miller Street and Freeman Street, trucks, driving on the wrong side of the road.
- Brunswick Street North traffic volumes, rat-running, bicycle crashes, Capital City Trail crossing, pedestrian/cyclist safety at roundabouts, trucks.

Issues identified on the VicRoads managed roads, Brunswick Street, St Georges Road and Nicholson Street, also ranked highly. While issues on these roads are noted, treatments are not included in the LAPM scheme other than in the form of advocacy to VicRoads.

5 DEVELOPMENT OF POTENTIAL TREATMENTS

5.1 LOCAL AREA STUDY GROUP MEETING ONE

The first meeting of the Local Area Study Group was held on Thursday 22nd February 2018 at the Edinburgh Gardens Community Hall.

The aim of the meeting was primarily to develop a draft LAPM Scheme.

A summary of the results of the on-line survey was presented to the Study Group and was generally endorsed by the Group. The Study Group then discussed potential treatment options for the study area.

Minutes of the LASG Meeting are provided in Appendix E.

5.2 POTENTIAL TREATMENTS – LOCAL STREETS

Based on the discussions at the LASG Meeting, potential treatments were developed to address the identified issues. These are discussed below.

5.2.1 Rae Street / White Street / Miller Street

Rat-running along White Street and Rae Street was identified as an issue of concern. To prevent through movements across Miller Street, a central median could be installed on Miller Street to prevent right turns out of Rae Street and White Street and prevent the through traffic movement. Provision should be made to maintain right turning movements for bicycles.

5.2.2 Rae Street / Holden Street

A median treatment is provided on Holden Street at Rae Street to prevent through movements on Rae Street, i.e. Rae Street is restricted to left in-left out (cyclists excepted). The existing treatment could be improved by widening the median and provided holding rails for cyclists crossing Holden Street.

5.2.3 Miller Street

Traffic volume and speed on Miller Street was identified as an issue and concerns were expressed regarding the existing slow point (one lane) treatments. The existing slow point treatments could be upgraded with full width speed humps. However the design



would need to be bus and bicycle friendly. Opportunities to expand the garden beds could also be considered.

To reduce through traffic using Miller Street, a right turn ban could be implemented on Nicholson Street at Miller Street during the afternoon peak period. Desirably, similar bans would be implemented at Clauscen Street and Barkly Street to prevent traffic rerouting to those streets.

Frustration was also expressed with vehicles turning right out of Miller Street to St Georges Road causing delay to other traffic exiting Miller Street, particularly during the afternoon peak period. This could be addressed by implementation of a right turn ban at this location during the afternoon peak period.

5.2.4 Capital City Trail at Rae Street and Brunswick Street North

There were numerous suggestions for priority to be given to the Capital City Trail at Rae Street and Brunswick Street North. Desirably the crossings would be on a raised platform for traffic approaching from Rae Street and Brunswick Street North.

Alternatively, Rae Street and/or Brunswick Street North could be closed to vehicular traffic from north of the Capital City Trail to Park Street and converted to parkland. This would address traffic volume and through traffic concerns on the streets and improve safety for Capital City Trail users. However, the resultant redistribution of traffic would impact the surrounding street network.

5.2.5 Roundabout treatments

Pedestrian and cyclist safety was identified as an issue at several roundabouts in the study area. Zebra crossings and bicycle sharrows could be provided at roundabouts to improve pedestrian crossing opportunities and cyclist safety. Zebra crossings and sharrows are proposed at the following locations:

- Scotchmer Street/Rae Street
- Scotchmer Street/Brunswick Street North
- Rae Street/Reid Street
- Rae Street/Church Street/St Georges Road
- St Georges Road/Newry Street
- Rae Street/Newry Street
- Rae Street/York Street

5.2.6 Freeman Street / Rae Street

Two casualty crashes have occurred at this intersection during the 5-year crash analysis period. Potential improvements would include provision of physical kerb outstands and new traffic islands.

5.2.7 Freeman Street / St Georges Road

Similarly, this intersection could be improved by provision of kerb outstands and traffic islands on the Freeman Street approaches.

O'BRIEN TRAFFIC 18138 SCOTCHMER REP: SCOTCHMER PRECINCT, NORTH FITZROY : 17 JANUARY 2019

16



5.2.8 Laneway between York Street and Alexandra Parade

Through traffic currently uses the laneway west of Rae Street to rat-run between York Street and Alexandra Parade. Two options to address vehicles rat-running through the lane are as follows:

- Close the laneway (residents excepted); or
- Make the laneway northbound only from Alexandra Parade, i.e. no entry to the laneway from York Street.

5.2.9 Rae Street at Alexandra Parade

A partial road closure of Rae Street at Alexandra Parade to prevent vehicles exiting to Alexandra Parade, would eliminate southbound through traffic in this section of Rae Street.

The design would need to take into consideration access to the service station on the eastern corner of Rae Street/Alexandra Parade, in particular the possibility of cars cutting through the service station to bypass the road closure. Consultation should occur with the service station owner.

5.3 POTENTIAL TREATMENTS – ADVOCACY TO VICROADS

The following treatment on the arterial road network could be considered for advocacy to VicRoads.

5.3.1 St Georges Road / Reid Street / Alfred Crescent

Options to improve safety for all road users at the St Georges Road/Reid Street/Alfred Street intersection should be considered. These could include provision of traffic signals, relocation of the nearby pedestrian crossing and a tram stop upgrade.

5.4 CONSULTATION: STAGE THREE – DRAFT LAPM SCHEME

A draft LAPM Scheme comprising the treatments discussed above was put to the community to gauge the level of support for each proposed treatment.

The Stage Three Consultation comprised:

- Brochures delivered to all residents and businesses in the area;
- Information session; and
- On-line survey.

The on-line survey was in the form of an interactive map and invited the community to "like" or "dislike" each treatment and provide a comment if desired.

A copy of the draft LAPM Scheme and the community response is provided in **Appendix F**.

The response rate from the community was approximately 12%. Overall there was strong support for the majority of treatments.

The following treatments were not supported:

O'BRIEN TRAFFIC 18138 SCOTCHMER REP: SCOTCHMER PRECINCT, NORTH FITZROY : 17 JANUARY 2019

17

Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



- Right turn bans from Nicholson Street into Miller Street, Clauscen Street and Barkly Street during the afternoon peak period; and
- Closure of Brunswick Street North at the Capital City Trail

The following treatments had marginal support only:

- Median on Miller Street to stop right turns in and out of Rae Street and White Street (cyclists excepted) - 85 likes, 73 dislikes; and
- Closure of Rae Street at the Capital City Trail 69 likes, 67 dislikes.

At the Capital City Trail intersections with Brunswick Street North and Rae Street, the clear preference was to give priority to Trail users at a raised crossing rather than implement road closures.

The preferred option for the laneway west of Rae Street was to implement No Entry from York Street and make the lane one-way northbound from Alexandra Parade.

5.5 LOCAL AREA STUDY GROUP MEETING TWO

The second meeting of the LASG was held on Thursday 20th September 2018 at the Bargoonga Nganjin North Fitzroy Library.

The aim of the meeting was to consider the results of the community consultation and reach agreement on proposed treatments to form the draft LAPM Scheme.

The LASG was provided with the results of the of the community consultation. Discussion focused around treatments where the community response was not definitive, in particular:

- Median on Miller Street to stop right turns in and out of Rae Street and White Street
 (cyclists excepted) there was concern the proposed median treatment would divert
 traffic to other streets (eg. Taylor Street) and that no countermeasures for other
 streets were being proposed. It was agreed that the treatment be implemented on
 a trial basis and the traffic impact on surrounding streets monitored and addressed
 as necessary.
- Closure of Rae Street and Brunswick Street North at the Capital City Trail or priority for Capital City Trail users at the intersections – the LASG was advised that a full road closure of Brunswick Street North was not practical at this time. Southbound turn bans from St Georges Road into Scotchmer Street prohibit an alternative access into the local area between 7:30am and 9:30am weekdays. Evidence was also provided indicating Council approval for a delivery vehicle route via Brunswick Street North to Piedimonte's supermarket. There was a strong view that the alternative option for Brunswick Street North (i.e. priority for the Capital City Trail) was not sufficient to address the traffic and safety issues. There was concern that if a road closure of Rae Street was implemented, traffic would divert to Brunswick Street North. The LASG requested alternative options be considered. Additional consultation was subsequently undertaken and is presented in Section 6.

Minutes of the LASG Meeting are provided in **Appendix G**, as well as a subsequent email received from a member of the LASG in relation to the Minutes.



6 FURTHER CONSULTATION: RAE ST AND BRUNSWICK ST NTH

6.1 ALTERNATIVE OPTIONS AND TRAFFIC IMPACT ASSESSMENT

Following the second meeting of the LASG, Council determined to further investigate treatment options for Rae Street and Brunswick Street North in the vicinity of the Capital City Trail, and consult further with the community.

A traffic engineering assessment of the impact of various traffic management treatment options on the surrounding road network was undertaken, specifically:

- Right turn ban on Holden Street at Brunswick Street North;
- Median island on Holden Street at Brunswick Street North (i.e. left-in/left-out only);
- Median island on Holden Street at Brunswick Street North and Dean Street (i.e. left-in/left out only);
- Partial closures southbound closure on Rae Street and northbound closure on Brunswick Street North at the Capital City Trail; and
- Full closure at Rae Street at the Capital City Trail and median island on Holden Street at Brunswick Street North and Dean Street.

In order to undertake the assessment, turning movement traffic counts were collected at the following intersections during the AM and PM peak periods:

- Rae Street/Holden Street;
- Rae Street/Park Street
- Brunswick Street North/Holden Street; and
- Brunswick Street North/Park Street.

The traffic engineering assessment is provided in Appendix H.

6.2 CONSULTATION

Following the traffic engineering assessment of the various options, further consultation with the community was undertaken.

The options presented to the community and the community responses is provided in Appendix I.

Overall there was no majority support for any of the treatment options presented. Based on the responses, it appears that the community is split between those wanting to restrict traffic using Brunswick Street North and those wanting to maintain vehicular accessibility.

Given the results of the consultation, it is recommended to not proceed with any of the above options.



However priority could be given to the Capital City Trail at Rae Street and Brunswick Street North at raised crossing points, as this was strongly supported in the earlier consultation.

7 DRAFT LAPM SCHEME

A draft LAPM Scheme has been developed based on the data and consultations and is presented diagrammatically in **Figure 7.**

Concept plans for the proposed treatments are provided in Appendix J.



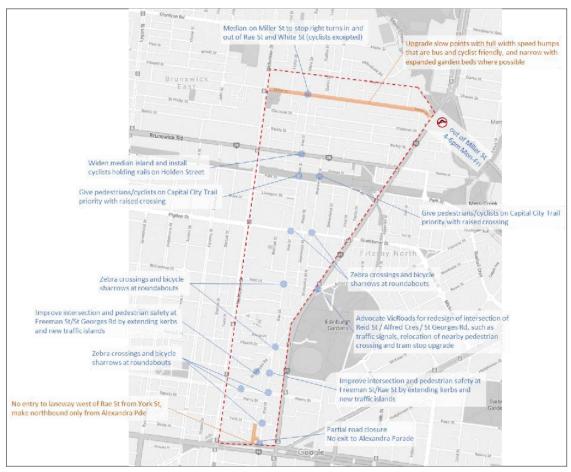


FIGURE 7: DRAFT LAPM SCHEME



8 INDICATIVE COST ESTIMATES

Indicative cost estimates are provided in **Table 1**. The indicative cost estimates do not include street lighting or drainage works.

LOCATION	DESCRIPTION	INDICATIVE COST ESTIMATE*
Miller Street at Rae Street/White Street	Median on Miller St to stop right turns in and out of Rae St and White St (cyclists excepted)	\$30,000
Miller Street	Upgrade slow points with full width speed humps that are bus and bike friendly, and expand garden beds where possible (10 locations)	\$200,000
Holden Street at Rae Street	Widen median island and install cyclist holding rails	\$30,000
Rae Street at Capital City Trail	Give priority to Capital City Trail users with a raised crossing at Rae St	\$50,000
Brunswick Street North at Capital City Trail	Give priority to Capital City Trail users with a raised crossing at Brunswick St North	\$60,000
Miller Street at St Georges Road	No right turn from Miller St into St Georges Rd 4pm-6pm Mon-Fri	\$1,000
Scotchmer Street /Brunswick St North	Zebra crossing and bike sharrows at roundabout	\$8,000
Scotchmer Street/Rae Street	Zebra crossing and bike sharrows at roundabout	\$8,000
Reid St / Alfred Cres / St Georges Rd	Advocate VicRoads for redesign of intersection of Reid St / Alfred Cres / St Georges Rd, such as traffic signals, relocation of nearby pedestrian crossing and tram stop upgrade	Council officer time
Reid Street/Rae Street	Zebra crossing and bike sharrows at roundabout	\$20,000
St Georges Rd South/Rae St/Church St	Zebra crossing and bike sharrows at roundabout	\$8,000
St Georges Rd South/Freeman St	Kerb extensions and new traffic island	\$90,000
Freeman Street/Rae Street	Kerb extensions and new traffic island	\$90,000
St Georges Rd South/Newry Street	Zebra crossing and bike sharrows at roundabout	\$8,000

Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



LOCATION	DESCRIPTION	INDICATIVE COST ESTIMATE*
Newry Street/Rae Street	Zebra crossing and bike sharrows at roundabout	\$8,000
York Street/Rae Street	Zebra crossing and bike sharrows at roundabout	\$8,000
Laneway behind 1-37 Rae Street	No entry to laneway behind 1-37 Rae St from York St, make northbound only from Alexandra Pde	\$1,000
Rae Street at Alexandra Parade	Partial road closure on Rae St - No exit to Alexandra Parade	\$40,000

* Does not include street lighting or drainage works

TABLE 1: INDICATIVE COST ESTIMATES

9 VICROADS ADVOCACY PROJECTS

In addition to the proposed treatments shown on the draft LAPM Scheme, a list of priority projects on the arterial road has been formulated throughout the study.

As discussed earlier, Council typically do not undertake works on the VicRoads managed arterial road network, however can advocate to VicRoads for implementation of treatments.

The priority projects on the arterial road for advocacy to VicRoads are listed below.

Nicholson Street

- Reduce speed limit
- Raised threshold treatments at side street intersections for improved pedestrian access.

Alexandra Parade

 More time for pedestrians and trams to cross along Nicholson Street and Brunswick Street.

Brunswick Street and St Georges Road

- Reduce speed limit
- Tram stop upgrades
- Widen crossing point at Capital City Trail to maximise capacity for pedestrians and bike riders crossing at the traffic lights
- Scotchmer Street intersection filtered right turns in and out of Scotchmer Street, lane arrow markings, bike signal head starts
- Raised threshold treatments at side street intersections for improved pedestrian access.



10 CONCLUSION

A draft LAPM Scheme has been developed in consultation with the community and based on analysis of traffic data and crash data. In addition, a priority list of traffic management treatments on the arterial roads have been formulated.

It is recommended that:

- Council adopt the LAPM Scheme; and
- Council advocate to VicRoads for implementation of the proposed treatments on the arterial roads.



TRAFFIC VOLUME AND SPEED DATA



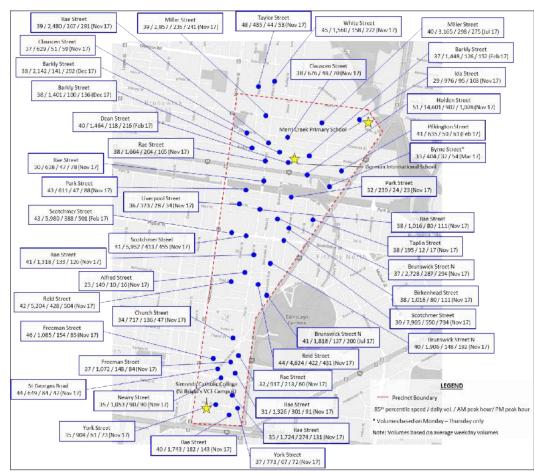


FIGURE A1: TRAFFIC VOLUME AND SPEED DATA



CRASH DATA



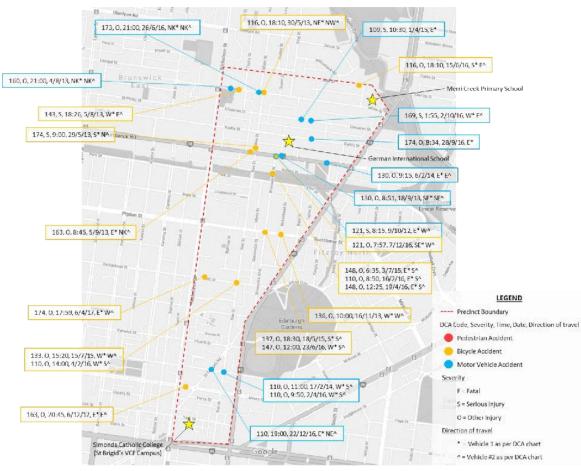


FIGURE B1: CASUALTY CRASH DATA - JULY 2012 TO JUNE 2017





PEDESTRIAN AND CYCLIST COUNT DATA

Agenda Page 177 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019

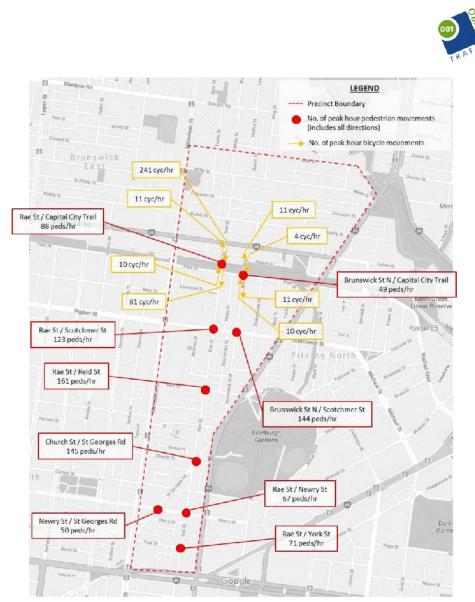


FIGURE C1: PEDESTRIAN AND CYCLIST COUNTS



COMMUNITY CONSULTATION – RANKING OF PRIORITY ISSUES

Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



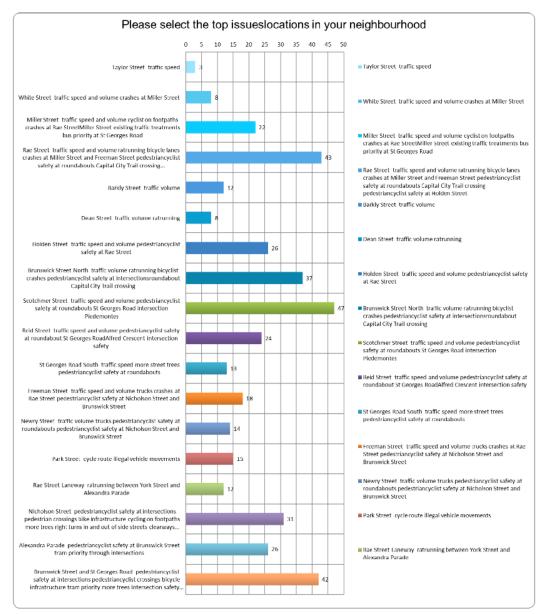


FIGURE C1: RESULTS OF THE ON-LINE SURVEY - RANKING OF PRIORITY ISSUES

O'BRIEN TRAFFIC 18138 SCOTCHMER REP: SCOTCHMER PRECINCT, NORTH FITZROY : 17 JANUARY 2019

32



LOCAL AREA STUDY GROUP MEETING 1:

THURSDAY 22ND FEBRUARY 2018 EDINBURGH GARDENS COMMUNITY HALL, NORTH FITZROY

MINUTES

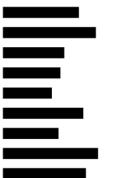


LOCAL AREA PLACE MAKING STUDY: SCOTCHMER - NORTH FITZROY

MINUTES OF LOCAL AREA STUDY GROUP MEETING 1 THURSDAY 22ND FEBRUARY 2018 EDINBURGH GARDENS COMMUNITY HALL

1. ATTENDEES

Cr Jackie Fristacky Cr Mike McEvoy Richard Young, Manager Traffic and Engineering Danny Millican, Coordinator Traffic Ted Teo, Transport Engineer Matt Harridge, Director Jemima Macaulay, Associate



Kerri Gibson, Principal Andrew Tran, Transport Engineer

Apologies:

Cr Misha Coleman

, White Street

City of Yarra O'Brien Traffic O'Brien Traffic Alfred Street Miller Street Rae Street Freeman Street Holden Street Rae Street Barkly Street Rae Street Brunswick Street North Merri Creek Primary School Moreland City Council

2. WELCOME AND INTRODUCTION

The meeting was opened by Richard Young at 6:35pm. Richard then handed over to Matt Harridge to chair the meeting.

Matt invited each person around the table to introduce themselves and briefly state their interest in the study.

O'BRIEN TRAFFIC SCOTCHMER MEETING 1 MINUTES: 08 MARCH 2018



3. BACKGROUND OF LAPMS AND ROLE OF LOCAL AREA STUDY GROUP

Matt referred to a handout provided to the Study Group outlining the discussion points for the meeting.

Matt provided an overview of the background of LAPM, the study process and the role of the Study Group. The cost and timeframe to deliver the LAPM scheme was also discussed.

4. PRIORITISATION OF ISSUES

Matt described the process undertaken to identify the issues in the study area, including the community consultation and data analysis.

It was noted that the crash data used in the study (VicRoads crash data) did not include property damage crashes.

The Study Group raised the following issues/comments:

- Road rage incidents are an issue, for example at Miller Street.
- Tube counters were often located close to speed humps and intersections, therefore the data does not accurately reflect vehicle speeds.
- There is lack of east-west route(s) through study area.
- The study area experiences north-south rat-running.
- Local areas in North Carlton have been successfully treated and Princes Park is also being treated.
- Local streets should be treated and the traffic forced onto the arterial network.
- Historical traffic data could be analysed to determine trends and assist futureproofing.

Matt reported the results of the on-line survey of top issues/locations. The priority locations based on the on-line survey were (in order):

- Scotchmer Street;
- Rae Street;
- Brunswick Street and St Georges Road;
- Brunswick Street North; and
- Nicholson Street.

It was noted that Brunswick Street, St Georges Road and Nicholson Street were VicRoads' managed road. Ted Teo from Council explained that Council could not implement treatments on these roads but could advocate to VicRoads. It was agreed that an output of the study should be a priority list of actions for advocation to VicRoads.

O'BRIEN TRAFFIC SCOTCHMER MEETING 1 MINUTES: 08 MARCH 2018



Matt sought agreement of the priority locations from the Study Group. The Study Group offered the following comments:

- Miller Street and Holden Street should be added to priority list.
- Issues on Miller Street include pedestrians/cyclists, St Georges Road intersection (peak periods), existing one-lane treatments, road rage, school traffic.
- The main issue for Scotchmer Street is at Piedimonte's if that is not to be considered as part of the study then Scotchmer Street is not really an issue (Ted noted that the issues at Piedimonte's would be addressed through the current planning application).
- It was commented that Piedimonte's traffic management proposal was not satisfactory.
- Ted noted that on-road bike facilities on Scotchmer Street had been mentioned during the consultation process, however could not be provided without removing parking.
- There was general agreement with issues identified for Rae Street.
- Queueing in Rae Street at Alexandra Parade was particularly noted, specifically:
 - queues are often up to 200m long;
 - how much is local versus non-local traffic; and
 - cars rat-run to avoid the intersection of Nicholson Street and Alexandra Parade.
- There was general agreement with the issues identified for Brunswick Street North.

5. DRAFT LAPMS SCHEME DEVELOPMENT DISCUSSION

Matt invited the Study Group to break into two groups to discuss potential treatment options for the study area - north of Scotchmer Street and south of Scotchmer Street.

Potential treatments discussed were as follows.

Miller Street

Upgrade existing LATM treatments

Barkly Street

Raise existing speed humps

Barkly Street / Rae Street

Reverse Stop signs

Holden Street / Rae Street

Improve safety for cyclists

O'BRIEN TRAFFIC SCOTCHMER MEETING 1 MINUTES: 08 MARCH 2018



Holden Street / Brunswick Street North

Right turn ban into Brunswick Street North

Rae Street / Capital City Trail

· Road closure, reverse priority (i.e. give way to trail users), or one-way (northbound)

Brunswick Street North / Capital City Trail

• Road closure or reverse priority (i.e. give way to trail users)

Rae Street

- Closure between Scotchmer Street and Reid Street (to enhance cycle route)
- One-way (northbound) between Church Street and Alexandra Parade
- One-way (southbound) between Reid Street and St Georges Road
- Partial/full closure at Alexandra Parade

Lane east of Rae Street

• Install bollard to prevent through traffic (rat-running from York Street)

Freeman Street

- Traffic calming treatment between Nicholson Street and St Georges Road
- One-way (westbound) between Nicholson Street and St Georges Road

St Georges Road intersections (VicRoads)

- Peak hour turn bans at Miller Street and Barkly Street
- Right turn arrow for vehicles exiting Scotchmer Street

Reid Street / St Georges Road (VicRoads)

- Install traffic signals at intersection (remove POS to north)
- Relocate tram stop to south side of intersection
- Zebra crossing on Reid Street

Brunswick Street intersections (VicRoads)

 Crossings to facilitate cyclists crossing Brunswick Street at York Street, Newry Street and Freeman Street

Nicholson Street intersections (VicRoads)

• Peak hour turn bans into Miller Street, Clauscen Street, Barkly Street, Freeman Street and Newry Street

O'BRIEN TRAFFIC SCOTCHMER MEETING 1 MINUTES: 08 MARCH 2018



Nicholson Street / Alexandra Parade (VicRoads)

• Improvements to slip lane (slip lane blocked by through traffic)

General

• Zebra crossings at roundabouts

6. SUMMARY AND NEXT STEPS

Matt indicated that O'Brien Traffic would consider the outputs from the meeting and develop a draft LAPM scheme in consultation with Council.

Ted outlined the next round of community consultation that would take place to ascertain the community's views of the draft scheme. Following the community consultation, the Local Area Study Group will meet again to discuss the community response and amend the draft plan.

The next meeting of the Study Group will be in May 2018.

9. CLOSE OF MEETING

Councillor McEvoy asked the Study Group to indicate their satisfaction with the format of the meeting.

Matt thanked the Study Group for their participation and closed the meeting at 9:00pm.

O'BRIEN TRAFFIC SCOTCHMER MEETING 1 MINUTES: 08 MARCH 2018



COMMUNITY CONSULTATION - ON-LINE SURVEY/INTERACTIVE MAP: DRAFT LAPM SCHEME AND SURVEY RESPONSES



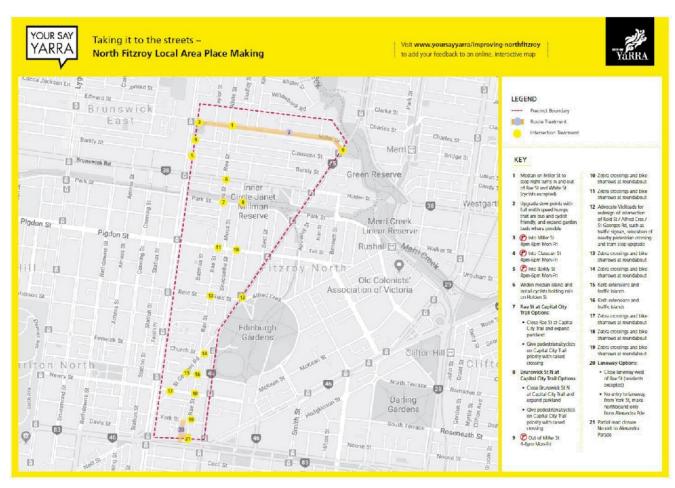


FIGURE F1: DRAFT LAPM SCHEME PRESENTED TO THE COMMUNITY



Agenda Page 188

Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



#	DESCRIPTION	RESP	ONSE
#	DESCRIPTION	LIKE	DISLIKE
1	Median on Miller St to stop right turns in and out of Rae St and White St (cyclists excepted)	85	73
2	Upgrade slow points with full width speed humps that are bus and bike friendly, and expand garden beds where possible	40	24
3	No right turn from Nicholson St into Miller St 4pm-6pm Mon-Fri	25	47
4	No right turn from Nicholson St into Clauscen St 4pm-6pm Mon-Fri	20	32
5	No right turn from Nicholson St into Barkly St 4pm-6pm Mon-Fri	21	30
6	Widen median island and install cyclists holding rails on Holden Street at Rae St intersections	89	9
7A	Close Rae St at Capital City Trail and expand parkland	69	67
7B	Give pedestrians and bike riders on Capital City Trail priority with a raised crossing at Rae St	79	8
8A	Close Brunswick St North at Capital City Trail and expand parkland	66	106
8B	Give pedestrians and bike riders on Capital City Trail priority with a raised crossing at Brunswick St North	86	16
9	No right turn from Miller St into St Georges Rd 4pm-6pm Mon-Fri	45	22
10	Zebra crossing and bike sharrows at roundabout of Scotchmer St and Brunswick St North	75	9
11	Zebra crossing and bike sharrows at roundabout of Scotchmer St and Rae St	55	6
12	Advocate VicRoads for redesign of intersection of Reid St / Alfred Cres / St Georges Rd, such as traffic signals, relocation of nearby pedestrian crossing and tram stop upgrade	105	16
13	Zebra crossing and bike sharrows at roundabout of Reid St and Rae St	48	13
14	Zebra crossing and bike sharrows at roundabout of St Georges Rd South, Rae St and Church St	28	5

Agenda Page 189

Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



#	DESCRIPTION	RESPONSE	
	DESCRIPTION	LIKE	DISLIKE
15	Kerb extensions and new traffic islands at Freeman St and St Georges Rd South	24	6
16	Kerb extensions and new traffic islands at Freeman St and Rae St	23	8
17	Zebra crossing and bike sharrows at roundabout of Newry St and St Georges Rd South	16	3
18	Zebra crossing and bike sharrows at roundabout of Newry St and Rae St	32	5
19	Zebra crossing and bike sharrows at roundabout of York St and Rae St	32	1
20A	Close laneway west of Rae St (residents excepted)	18	3
20B	No entry to laneway behind 1-37 Rae St from York St, make northbound only from Alexandra Pde	31	5
21	Partial road closure on Rae St - No exit to Alexandra Pde	64	43

TABLE F1: ON-LINE SURVEY RESULTS - PROPOSED LAPM TREATMENTS



LOCAL AREA STUDY GROUP MEETING 2:

THURSDAY 20TH SEPTEMBER 2018 BARGOONGA NGANJIN NORTH FITZROY LIBRARY

MINUTES



LOCAL AREA PLACE MAKING STUDY: SCOTCHMER

MINUTES OF LOCAL AREA STUDY GROUP MEETING 2 THURSDAY 20TH SEPTEMBER 2018, BARGOONGA NGANJIN NORTH FITZROY LIBRARY

1. ATTENDEES

Ted Teo, Senior Transport Engineer	Yarra City Council
Danny Millican, Acting Manager Traffic and Civil Eng.	Yarra City Council
Karen Wong, Senior Traffic Engineer	Yarra City Council
Peter Eckersley, Senior Active Transport Project Officer	Yarra City Council
Councillor Mike McEvoy	Yarra City Council
Matt Harridge, Director	O'Brien Traffic
Jemima Macaulay, Senior Associate	O'Brien Traffic
Andrew Tran, Transport Engineer	Moreland City Council
	Brunswick Street North



Councillor Jackie Fristacky Councillor Misha Coleman

Barkly Street Miller Street Yarra City Council Yarra City Council Rae Street

Freeman Street

Merri Creek Primary School

White Street

Rae Street

Rae Street

2. WELCOME AND INTRODUCTION

The meeting was opened by Ted Teo at 6:30pm.

Ted invited each person around the table to introduce themselves and noted apologies from absent volunteers.

O'BRIEN TRAFFIC SCOTCHMER MEETING 2 MINUTES: 20 SEPTEMBER 2018



A submission from David Nicholds was tabled (refer Attachment A).

3. PREVIOUS MINUTES AND CHANGES

No changes to the Minutes of the Local Area Study Group Meeting 1 were recorded.

4. SUMMARY OF COMMUNITY CONSULTATION

Ted presented an overview of the community consultation and noted the response rate was the highest of the three LAPM studies. In particular:

- 3500 brochures were delivered to Yarra residents, 500 brochures were delivered to Moreland residents and 242 brochures were mailed out to non-resident owners;
- 55 people attended the 2 information sessions;
- The response rate was 12% (including Social Pinpoint users, email responses and telephone calls).

5. LAPM PROPOSALS - RESPONSE TO COMMUNITY CONSULTATION

The LASG was provided a document prepared by O'Brien Traffic outlining the results of the community consultation for each proposed treatment.

Where the results of the consultation provided clear direction, O'Brien Traffic provided a recommendation to *proceed* or *not proceed*.

Where the community response was less clear, the LASG was invited to discuss the proposed treatment. Hence the discussion focussed around the proposed treatments as follows.

Proposed treatment 1 – Median on Miller Street to stop right turns in and out of Rae Street and White Street (cyclists excepted)

There was concern that the proposed treatment would divert traffic to other streets (eg. Taylor Street) and no countermeasures for other streets are being proposed. Turn bans to mitigate traffic impact was suggested. Andrew Tran (Moreland City Council) noted Glenlyon Road carries approximately 2,000 vehicle per day.

Recommendation: Proceed on a trial basis, monitor traffic impact on surrounding streets and address issues as they arise. O'Brien Traffic to provide potential countermeasures in the study report to that could be adopted if required.

O'BRIEN TRAFFIC SCOTCHMER MEETING 2 MINUTES: 20 SEPTEMBER 2018



Proposed treatment 2 – Upgrade slow points on Miller Street with speed humps that are bus and bike friendly, and expand garden beds where possible

Ted explained that a cut through would be provided for cyclists behind the garden beds at the slow points.

Recommendation: Proceed

Proposed treatments 3 to 5 – No right turn bans from Nicholson Street into Miller Street, Clauscen Street and Barkly Street 4-6pm Monday to Friday

While the community consultation showed a lack of support for the right turn bans, there was concern that if the turn bans were not implemented, there would be no change in traffic volumes in these streets. The group requested alternative treatments be proposed.

Ted advised that a change of stop priority was being considered at Barkly Street and Rae Street to address sight line and design issues with the current arrangement.

Recommendation: Do not proceed. O'Brien Traffic to provide potential countermeasures in the study report to that could be adopted if required.

Proposed treatments 7A, 7B, 8A and 8B - Closure of Rae Street and Brunswick Street North at Capital City Trail or priority for pedestrians/cyclists of Capital City Trail at the intersections

Ted advised that a full road closure of Brunswick Street North at the Capital City Trail was not practical at this time as VicRoads will not consider the removal of right turn bans into Scotchmer Street from St Georges Road for southbound traffic between 7:30 and 9:30am weekdays. The route is also used by delivery vehicles to access Piedimonte's in a southbound direction. There was some dismay expressed that this treatment was presented to the community for consideration when it wasn't a viable option.

There was a strong view that the alternative option for Brunswick Street North (i.e. priority for Capital City Trail users) was not sufficient to address the traffic and safety issues.

The LASG requested alternative options be considered. Alternative options suggested were a partial (northbound) closure at Capital City Trail and a right turn ban at Holden Street during the AM peak period.

There was concern that if a road closure of Rae Street was implemented, traffic would divert to Brunswick Street North (if that remained opened).

Recommendation: Consider options further and potentially implement options on a trial basis.

O'BRIEN TRAFFIC SCOTCHMER MEETING 2 MINUTES: 20 SEPTEMBER 2018



Proposed treatment 9 – No right turn from Miller Street into St Georges Road, 4-6pm Monday to Friday

While there was concern expressed regarding the impact the proposed turn ban would have on parents collecting children from after school care, the benefits of the proposed turn ban were agreed.

Recommendation: Proceed

Proposed treatment 20A/B – Close laneway west of Rae Street (residents accepted) or No Entry to laneway from York Street (laneway northbound only from Alexandra Parade)

The preferred option, based on the results of the community consultation, was for a No Entry to the laneway at York Street (i.e. northbound only from Alexandra Parade) and this was generally supported by the LASG.

Recommendation: Proceed with No Entry to the laneway west of Rae Street at York Street

Proposed treatment 21 – Partial road closure on Rae Street – no exit to Alexandra Parade

General agreement to the partial road closure.

Recommendation: Proceed

6. PROJECTS TO ADVOCATE TO VICROADS

Jemima Macaulay explained that an outcome of the study was to provide a priority list of projects on arterial roads for advocacy to VicRoads. She listed the following projects:

Nicholson Street

- Reduce speed limit
- Raised threshold treatments at side street intersections for improved pedestrian access.

Alexandra Parade

 More time for pedestrians and trams to cross along Nicholson Street and Brunswick Street.

Brunswick Street and St Georges Road

- Reduce speed limit
- Tram stop upgrades

O'BRIEN TRAFFIC SCOTCHMER MEETING 2 MINUTES: 20 SEPTEMBER 2018



- Widen crossing point at Capital City Trail to maximise capacity for pedestrians and bike riders crossing at the traffic lights
- Scotchmer Street intersection filtered right turns in and out of Scotchmer Street, lane arrow markings, bike signal head starts
- Raised threshold treatments at side street intersections for improved pedestrian access.

Ted noted that an intersection upgrade of St Georges Road/Reid Street was also on Council's list of advocacy projects.

7. OTHER DISCUSSION

There was frustration expressed regarding the extensive consultation over a long period through the LAPM process but lack of action to date.

8. NEXT STEPS

Ted indicated that Council will notify the community of the final draft plan in October 2018.

O'Brien Traffic will develop cost estimates and priorities.

The draft LATM Scheme will go to Council at its meeting in November 2018 and residents will have an opportunity to address Council at that meeting.

9. CLOSE OF MEETING

Ted thanked the Study Group for their participation and closed the meeting at 8:20pm.

Attachment A

Comments from absent volunteers

O'BRIEN TRAFFIC SCOTCHMER MEETING 2 MINUTES: 20 SEPTEMBER 2018

From:

Sent: Monday, 10 September 2018 10:11 PM To: Teo, Ted <Ted.Teo@yarracity.vic.gov.au> Subject: Re: Taking it to the streets North Fitzroy - Local Area Study Group Meeting #2 -Thursday 20 September

Hi Ted,

Unfortunately I will be away on 20 September when the next meeting of the Local Area Study Group is scheduled.

I have reviewed the preliminary results and make the following brief comments, some of which I have expressed before:

- Whatever happens with Rae St and Brunswick St North, priority should be given to bikes over cars at both of the crossings of the Capital City Trail. This will improve the lot of the many cyclists who use this trail. However it will not address the problem of rat-run traffic in the area.
- I like the idea of Rae St being closed at the Capital City Trail but recognise that this could increase the rat-run traffic in Brunswick St North. I believe the proposal I have previously made which includes one way directions for both Rae St and Brunswick St North at the CCT, together with other minor changes, would reduce rat-run traffic while still providing (slightly more circuitous) access to local residents.
- The improvement proposed to the median strip in Holden St opposite Rae St will contribute to improved safety for cyclists and I strongly commend it.
- The partial road closure at the south end of Rae St will greatly improve the quality of life for residents in that immediate area. While it will inconvenience some local residents, including myself, it must be recognised that some changes of habits by local residents may be required to contribute to improved quality of life for their neighbours.
- Advocating to VicRoads for a complete redesign of the dangerous and difficult Richardson St/St Georges Rd/Alfred Crescent intersection is essential.

As a general observation, it is not possible to satisfy the concerns of all residents and I look to the council's professional engineers to propose solutions which are appropriate for the majority of residents and which address real concerns. Having done that, the council must be prepared to advise some residents/merchants that the proposal is for the greater good and that some residents/merchants will have to make minor changes in habits as a result.

While I will be away I will still have email access and I would welcome the opportunity to review anything further that you distribute and I hope that I will be available if any further meetings take place.

Best regards,

O'BRIEN TRAFFIC SCOTCHMER MEETING 2 MINUTES: 20 SEPTEMBER 2018



Email from member of Local Area Study Group Meeting

Hi Ted,

A couple of things. Firstly, thanks for installing the traffic data collectors/counters again. Once you have the data, I would really appreciate an update.

Secondly, thanks for the minutes of the last meeting. I now regret not formally requesting the first meetings minutes to be amended as they have been accepted as true and correct. As the minutes are a record, I wish to state I am not happy to accept the minutes from the second meeting. I will try and keep my responses brief.

Proposed treatments 7A, 7B, 8A and 8B - Closure of Rae Street and Brunswick Street North at Capital City Trail or priority for pedestrians/cyclists of Capital City Trail at the intersections

...... The route is also used by delivery vehicles to access Piedimonte's in a southbound direction.

The response tells half the story. I believe more was stated at the meeting and a truer summary of the points made in relation to the matter would read;

..... Due to the morning turn bans, vehicles (including delivery vehicles) use Brunswick St Nth as a route to access businesses on Scotchmer St (most significantly Piedimonte's). The route is now regularly used by delivery vehicles in both a northbound and southbound direction at all times of the day.

Also;

.....There was a strong view that the alternative option for Brunswick Street North (i.e. priority for Capital City Trail users) was not sufficient to address the traffic and safety issues.

In addition, the question was also raised if consideration had been given to the potential traffic congestion that could occur, particularly on Holden St, if the alternative option for Brunswick St Nth (priority for Capital City Trail users) was implemented, given the high volume of traffic using Brunswick St Nth, meeting with the high volume of cyclists using the Capital City trail (particularly during peak hours), and the close proximity of the Capital City Trail to Holden St.

7. Other business

There was frustration expressed regarding the extensive consultation over a long period through the LAPM process but lack of action to date.

I would also add to the above,

There was frustration expressed with regard to lack of traffic management strategies to address the traffic issues, particularly the north/south "rat-run". There was also concern that greater importance was placed on community responses to traffic management strategies rather than recommendations by Traffic Engineers.

Finally, I have received a response to my FOI to Vic Roads and will be forwarding this on to the team.

Thanks for your assistance.

Kind regards,



TRAFFIC IMPACT ASSESSMENT:

RAE STREET AND BRUNSWICK STREET NORTH TRAFFIC MANAGEMENT OPTIONS

O'BRIEN TRAFFIC 18138 SCOTCHMER REP: SCOTCHMER PRECINCT, NORTH FITZROY : 17 JANUARY 2019



Suite 2.03, 789 Toorak Road Hawthorn East Victoria 3123 T: +61 3 9804 3610 W: obrientraffic.com

8 November 2018

Ted Teo City of Yarra PO Box 168 Richmond VIC 3121

Email: ted.teo@yarracity.vic.gov.au

Dear Ted

SCOTCHMER PRECINCT – NORTH FITZROY

I refer to your request for a traffic engineering assessment of the impact of various traffic management treatment options on the surrounding road network, specifically:

- 1. Right turn ban on Holden Street at Brunswick Street North;
- 2. Median island on Holden Street at Brunswick Street North (i.e. left-in/left-out only);
- Median island on Holden Street at Brunswick Street North and Dean Street (i.e. leftin/left out only);
- 4. Partial closures southbound closure on Rae Street and northbound closure on Brunswick Street North at the Capital City Trail; and
- 5. Full closure at Rae Street at the Capital City Trail and median island on Holden Street at Brunswick Street North and Dean Street.

In the course of preparing this letter we have undertaken turning movement counts at key intersections and determined the likely traffic distribution and implications for each treatment option. Our assessment is as follows.

BACKGROUND

Yarra City Council is undertaking a Local Area Place Making (LAPM) Study in the Scotchmer Precinct, North Fitzroy.

The study area is shown in **Figure 1** and is bounded by Nicholson Street, May Street, St Georges Road, Brunswick Street and Alexandra Parade in North Fitzroy.

TRAFFIC ENGINEERING TRAFFIC PLANNING ROAD SAFETY TRAFFIC IMPACT ASSESSMENTS TRANSPORT PLANNING ANDREW O'BRIEN & ASSOCIATES PTY LTD AEN: 55 007 006 037

Agenda Page 201 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



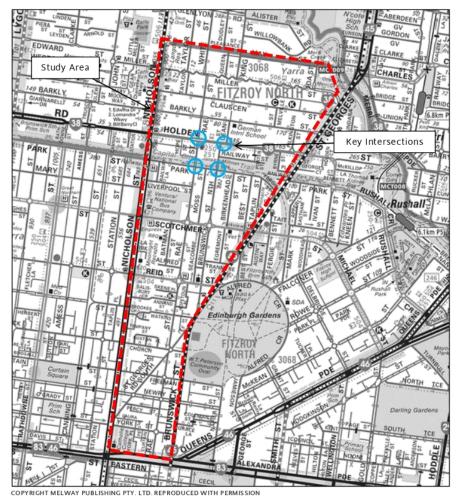


FIGURE 1: LOCATION OF SUBJECT SITE

As part of the LAPM Study, Council is considering various treatment options to reduce traffic volumes and rat running, and to improve pedestrian and cyclist safety, on Brunswick Street North and Rae Street in the vicinity of the Capital City Trail.

EXISTING CONDITIONS

Key Intersections

The Rae Street / Holden Street intersection is a cross intersection with Stop controls on Rae Street and a median island restricting access to left-in/left-out. An aerial photo of the intersection of Rae Street / Holden Street is shown in **Figure 2**.

O'BRIEN TRAFFIC 18138 SCOTCHMER LET: 08 NOVEMBER 2018





FIGURE 2: AERIAL PHOTO OF RAE STREET / HOLDEN STREET INTERSECTION

The Rae Street / Park Street intersection is a cross intersection with Stop controls on Rae Street. Park Street permits one-way westbound traffic flow. An aerial photo of the intersection of Rae Street / Park Street is shown in **Figure 3**.



FIGURE 3: AERIAL PHOTO OF RAE STREET / PARK STREET INTERSECTION

The Brunswick Street North / Dean Street / Holden Street intersection is a staggered intersection with Stop controls on Brunswick Street North and Dean Street. An aerial photo of the intersection of Brunswick Street North / Dean Street / Holden Street is shown in **Figure 4**.





FIGURE 4: AERIAL PHOTO OF BRUNSWICK STREET NORTH / DEAN STREET / HOLDEN STREET

The Brunswick Street North / Park Street intersection is a cross intersection with Give Way controls on Park Street. Park Street is restricted to one-way westbound to the west of Brunswick Street North and one-way eastbound to the east of Brunswick Street North. An aerial photo of the intersection of Brunswick Street North / Park Street is shown in **Figure 5**.



FIGURE 5: AERIAL PHOTO OF BRUNSWICK STREET NORTH / PARK STREET

Existing Traffic Volumes

O'Brien Traffic commissioned turning movement counts of the 4 key intersections on Thursday 18 October 2018 at the following times:

- 6:30am to 9:30am; and
- 3:30pm to 7:00pm.

O'BRIEN TRAFFIC 18138 SCOTCHMER LET: 08 NOVEMBER 2018

Agenda Page 204

Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



These times were selected as they represent typical commuter peak periods.

EXISTING CONDITIONS- AM PEAK HOUR

The existing on-road peak hour volumes are presented in Figures 6 – 7.

FIGURE 6: EXISTING TRAFFIC VOLUMES - AM PEAK HOUR (7AM - 8AM)



FIGURE 6: EXISTING TRAFFIC VOLUMES - PM PEAK HOUR (5:30PM - 6:30PM)

O'BRIEN TRAFFIC 18138 SCOTCHMER LET: 08 NOVEMBER 2018



TRAFFIC RE-DISTRIBUTION & IMPACT

For each treatment option, vehicles will be required to find an alternative route to their destination. Where there is a convenient alternative local route, the majority of vehicles will simply transfer to it. However, where the alternative local route is not convenient, a proportion of vehicles will re-route to the arterial road network.

In this case, it is assumed that 40% of traffic will transfer to the arterial network where an obvious alternative route is not available.

This is consistent with Council's findings of the Stage 1 Works Review – LATM 16 Victoria *Precinct (Richmond)* (Stage 1 constructed in 2014). This post construction evaluation found reductions in traffic volumes of 39-65% in streets where traffic movements were restricted (eg. left in/left out treatments) without an increase in traffic volumes on other local streets.

Based on the above, the likely traffic re-distribution of each traffic management option is discussed as follows.

Option 1 – Right turn ban from Holden Street to Brunswick Street North

Option 1 will ban the right turn from Holden Street in Brunswick Street North. This will impact vehicles currently travelling north to south via the Rae Street - Holden Street - Brunswick Street North route. It is likely that some vehicles currently utilising this route will transfer to Dean Street, turn right into Holden Street, then left into Brunswick Street North.

The following assumptions have been made in regard to the traffic re-distribution of Option 1:

- Traffic approaching from Holden Street west (Nicholson Street intersection) to turn right into Brunswick Street North will transfer to/stay on the arterial road network;
- Of the existing traffic turning left from Rae Street (north) into Holden Street, 90% currently turns right into Brunswick Street North and 10% continues eastbound on Holden Street;
- Of the existing traffic turning left from Rae Street (north) into Holden Street, then right into Brunswick Street North:
 - 40% will transfer onto the arterial roads
 - 60% will transfer onto Dean Street, turn right onto Holden Street, then left onto Brunswick Street North.

The traffic redistribution for Option 1, for the AM and PM peak hours, is shown in **Figures 7** and **8** respectively.

O'BRIEN TRAFFIC 18138 SCOTCHMER LET: 08 NOVEMBER 2018

Agenda Page 206

Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019





FIGURE 7: OPTION 1 - AM TRAFFIC RE-DISTRIBUTION



FIGURE 8: OPTION 1 - PM TRAFFIC RE-DISTRIBUTION



Option 2 - Median island on Holden Street at Brunswick Street North

Option 2 will install a median island on Holden Street at Brunswick Street North, restricting Brunswick Street North to left-in/left-out. This option will prevent the right turn movement into Brunswick Street North from Holden Street, similar to Option 1. In addition it will prevent vehicles travelling from south to north via the Brunswick Street North – Holden Street - Dean Street route. It is likely that vehicles travelling north will transfer onto Park Street to access St Georges Road / Nicholson Street.

The following assumptions have been made in regard to the traffic re-distribution of Option 2:

- Existing traffic turning right from Holden Street to Brunswick Street North will transfer as per Option 1.
- Of the existing traffic turning right from Brunswick Street North to Holden Street (including traffic continuing into Dean Street):
 - 40% will transfer onto the arterial network
 - 50% will transfer eastbound on Park Street to St Georges Road
 - 10% will transfer westbound on Park Street to Nicholson Street.

The traffic redistribution for Option 2, for the AM and PM peak hours, is shown in **Figures 9** and **10** respectively.

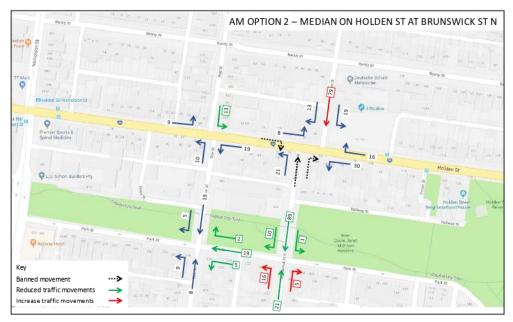


FIGURE 9: OPTION 2 - AM TRAFFIC RE-DISTRIBUTION

O'BRIEN TRAFFIC 18138 SCOTCHMER LET: 08 NOVEMBER 2018



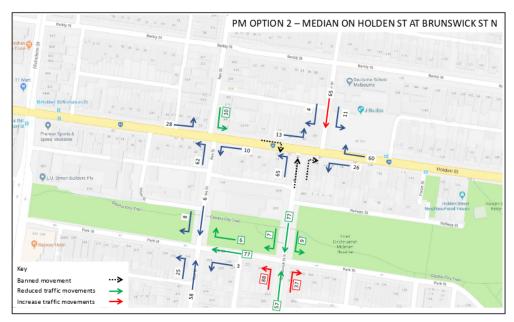


FIGURE 10: OPTION 2 - PM TRAFFIC RE-DISTRIBUTION

Option 3 – Median island on Holden Street at Brunswick Street North and Dean Street

Option 3 will install a median island on Holden Street at Brunswick Street North and Dean Street, restricting both streets to left-in/left-out only. This option will restrict vehicles from travelling between the north and south of Holden Street via the local road network. It is likely that vehicles travelling north will transfer onto Park Street and vehicles travelling south will transfer onto Barkley Street to access St Georges Road or Nicholson Street.

The following assumptions have been made in regard to the traffic re-distribution of Option 3:

- Existing traffic turning right from Holden Street to Brunswick Street North will transfer as per Options 1 and 2.
- Existing traffic turning right from Brunswick Street North to Holden Street (including traffic continuing to Dean Street) will transfer as per Option 2.
- Restricting Dean Street to left-in/left-out will transfer traffic currently travelling from the north side to the south side of Holden Street onto the arterial network. It is assumed that of the existing traffic currently travelling north to south:
 - 40% will transfer to (i.e. stay on) the arterial network without travelling through the local area;
 - 50% will transfer to Barkly Street and travel westbound to Nicholson Street;
 - 10% will transfer to Barkly Street and travel eastbound to St Georges Road.

O'BRIEN TRAFFIC 18138 SCOTCHMER LET: 08 NOVEMBER 2018



- Of the existing traffic turning right into Dean Street from Holden Street:
 - 40% will transfer to the arterial network;
 - 60% will transfer to Barkly Street (westbound from St Georges Road).

The traffic redistribution for Option 3, for the AM and PM peak hours, is shown in **Figures 11 and 12** respectively.



FIGURE 11: OPTION 3 - AM TRAFFIC RE-DISTRIBUTION



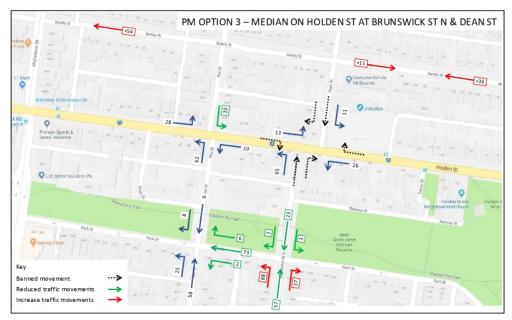


FIGURE 12: OPTION 3 - PM TRAFFIC RE-DISTRIBUTION

Option 4 – Rae Street southbound closure and Brunswick Street North northbound closure at Capital City Trail

Option 4 will close Rae Street to southbound traffic and close Brunswick Street North to northbound traffic at the Capital City Trail. This option is anticipated to shift existing northbound/southbound traffic between Rae Street and Brunswick Street North based on the new one-way traffic flows.

The following assumptions have been made in regard to the traffic re-distribution of Option 4:

- Existing traffic turning right from Brunswick Street North into Holden Street (including into Dean Street) will transfer as per Option 2.
- Existing traffic turning left from Brunswick Street North into Holden Street will transfer to Rae Street;
- Existing traffic turning left from Holden Street into Rae Street will turn left into Brunswick Street North.

The traffic redistribution for Option 4, for the AM and PM peak hours, is shown in **Figures 13 and 14** respectively.

O'BRIEN TRAFFIC 18138 SCOTCHMER LET: 08 NOVEMBER 2018

Agenda Page 211

Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019





FIGURE 13: OPTION 4 - AM TRAFFIC RE-DISTRIBUTION

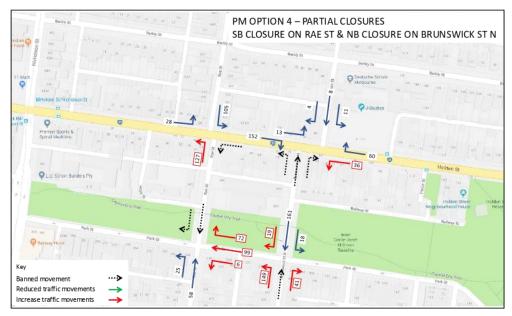


FIGURE 14: OPTION 4 - PM TRAFFIC RE-DISTRIBUTION



Option 5 – Rae Street closure at Capital City Trail and median island on Holden Street at Brunswick Street North and Dean Street

Option 5 will close Rae Street at the Capital City Trail and install a median island on Holden Street at Brunswick Street North and Dean Street, restricting traffic movements at both streets to left-in/left-out only. This option will restrict vehicles from travelling between the north and south of Holden Street via the local road network and transfer existing traffic travelling north/south on Rae Street to Brunswick Street North.

The following assumptions have been made in regard to the traffic re-distribution of Option 5:

- Existing traffic turning right from Brunswick Street North into Holden Street (including traffic continuing into Dean Street) will transfer as per Option 2;
- Existing traffic travelling from north of Holden Street (via Rae street or Dean Street) to south of Holden Street will transfer as per Option 3;
- Existing traffic turning left at Rae Street will transfer to Brunswick Street North.

The traffic redistribution for Option 5, for the AM and PM peak hours, is shown in **Figures 15 and 16** respectively.



FIGURE 15: OPTION 5 - AM TRAFFIC RE-DISTRIBUTION

Agenda Page 213





FIGURE 16: OPTION 5 - PM TRAFFIC RE-DISTRIBUTION

COMPARISON OF TREATMENT OPTIONS

Pros and Cons

The pros and cons of each treatment option is summarised in Table 1 below.

O'BRIEN TRAFFIC 18138 SCOTCHMER LET: 08 NOVEMBER 2018



TREATMENT OPTION	PROS	CONS
Option 1: Turn ban on Holden Street at Brunswick Street North	 Reduces peak hour traffic volumes on Rae St (north of Holden St) and Brunswick St N (south of Holden St) Maintains accessibility between the local areas north and south of Holden St 	 Increases peak hour traffic volumes on Dean St Increases right turn movement from Dean St to Holden St Does not stop the north-south/south- north rat-run
Option 2: Median island on Holden Street at Brunswick Street North	 Reduces peak hour traffic volumes on Rae St (north of Holden St) and Brunswick St N (south of Holden St) Maintains accessibility from the local area north of Holden St to the local area south of Holden St 	 Increases peak hour volumes on Dean St and Park St Does not stop the south-north rat-run Increases right turn volumes on Dean St
Option 3: Median island on Holden Street at Brunswick Street North and Dean Street	 Reduces peak hour traffic volumes on Rae St (north of Holden St), Brunswick St N (south of Holden St) and Dean St (north of Holden St) Removes all rat-run routes between the local areas north and south of Holden St 	 Increases peak hour volumes on Barkly St No vehicle accessibility between the local areas north and south of Holden St
Option 4: Rae Street southbound closure and Brunswick Street northbound closure at Capital City Trail	 Simplifies intersections on Capital City Trail (one-way vehicular traffic only) Removes the rat-run route from the local area south of Holden St to the local area north of Holden St 	 Transfers northbound traffic on Brunswick St N to Rae St and southbound traffic on Rae St to Brunswick St N No vehicle accessibility from the local area south of Holden St to the local area north of Holden St
Option 5: Rae Street closure at Capital City Trail and median island on Holden Street at Brunswick Street North and Dean Street	 Removes Rae Street intersection from Capital City Trail Reduces peak hour traffic volumes on Rae St (north and south of Holden St), Brunswick St N (south of Holden St) and Dean St (north of Holden St) Removes all rat-run routes between the local areas north and south of Holden St 	 Increases peak hour volumes on Barkly St No vehicle accessibility between the local areas north and south of Holden St

TABLE 1: OPTION COMPARISON

Impact on Brunswick Street North and Rae Street traffic volumes

The likely change in traffic volumes in Brunswick Street North and Rae Street for each treatment option is summarised in Table 2.

O'BRIEN TRAFFIC 18138 SCOTCHMER LET: 08 NOVEMBER 2018

Agenda Page 215

Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



TREATMENT OPTION	LIKELY CHANGE IN TRAFFIC VOLUMES (APPROXIMATE)		
	BRUNSWICK STREET NORTH	RAE STREET	
Option 1: Turn ban on Holden Street at Brunswick Street North	25% decrease	No change	
Option 2: Median island on Holden Street at Brunswick Street North	33% decrease	No change	
Option 3: Median island on Holden Street at Brunswick St North and Dean St	50% decrease	No Change	
Option 4: Rae Street southbound closure and Brunswick Street northbound closure at Capital City Trail	40% decrease north of Park St Minimal change south of Park St	100% increase	
Option 5: Rae Street closure at Capital City Trail and median island on Holden Street at Brunswick Street North and Dean Street	30% decrease	All through traffic removed	

TABLE 2: IMPACT ON BRUNSWICK STREET NORTH TRAFFIC VOLUMES

CONCLUSION

The above analysis provides an assessment of the traffic impacts of various treatment options for Rae Street and Brunswick Street North in the vicinity of the Capital City Trail for consideration as part of the Local Area Place Making Study.

Should you have any queries, please do not hesitate to contact me on 9804-3610.

Yours sincerely

O'BRIEN TRAFFIC

ana

Jemima Macaulay Senior Associate



ADDITIONAL CONSULTATION:

RAE STREET AND BRUNSWICK STREET NORTH TRAFFIC MANAGEMENT OPTIONS AND RESULTS OF CONSULTATION

Agenda Page 217 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



Agenda Page 218 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



Agenda Page 219 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



Agenda Page 220 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



Agenda Page 221 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



Agenda Page 222 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



	OPTION 1			OPTION 2			OPTION 3			OPTION 4			OPTION 5		
Email results	Dislike	Like	Not Sure												
Scotchmer LAPM Precinct (City of Yarra)	222	144	38	329	55	22	274	114	22	330	46	27	295	109	8
Moreland Cith Council (Fitzroy North only)	37	14	3	51	1	1	32	20	1	37	8	8	50	3	
North Fitzroy LAPM Precint (City of Yarra)	25	10	9	31	7	6	22	13	9	24	18	3	23	20	2
All Other City of Yarra LAPM Precincts	27	8	5	19	9	12	14	15	11	13	12	15	7	31	3
Moreland City Council (excl. Fitzroy North)	22	6	6	18	6	9	14	12	8	12	15	7	6	26	3
Darebin City Council	30	9	6	21	15	9	20	17	8	12	25	9	7	40	1
All other councils	16	4	5	10	6	9	9	6	9	8	10	6	3	24	
OVERALL	379	195	72	479	99	68	385	197	68	436	134	75	391	253	17

TABLE 11: RESULTS OF CONSULTATION - RAE STREET AND BRUNSWICK STREET NORTH TRAFFIC MANAGEMENT OPTIONS

O'BRIEN TRAFFIC 18138 SCOTCHMER REP: SCOTCHMER PRECINCT, NORTH FITZROY : 17 JANUARY 2019

Agenda Page 223 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



CONCEPT PLANS

O'BRIEN TRAFFIC 18138 SCOTCHMER REP: SCOTCHMER PRECINCT, NORTH FITZROY : 17 JANUARY 2019

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 Traffic Engineering *Road Safety J MACAULAY 18138005.DGN M FERGUSON 18138005 SUITE 2.03, 789 TOORAK ROAD HAWTHORN EAST, VIC, 3123 P: +613 9804 3610 MILLER STREET/ RAE STREET INTERSECTION TREATMENT
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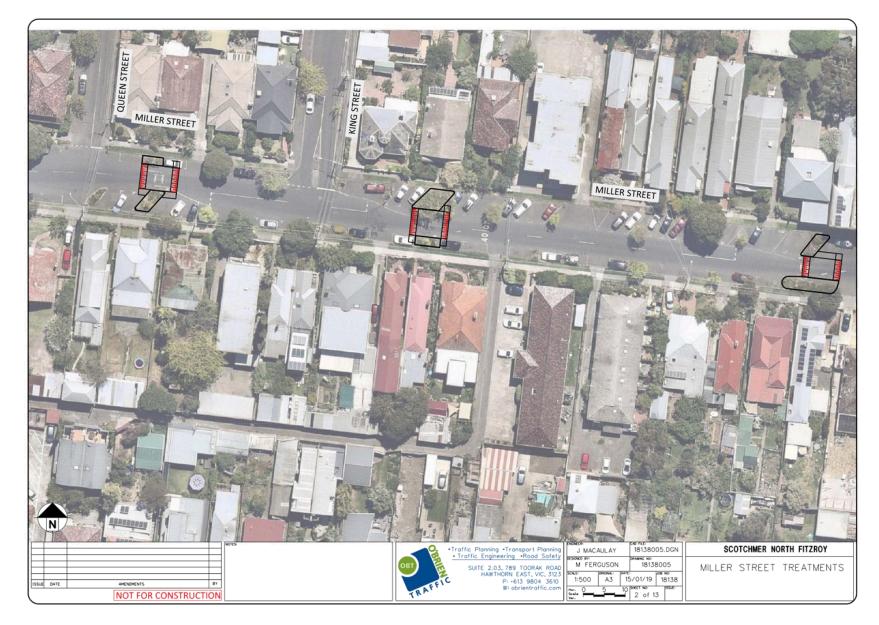
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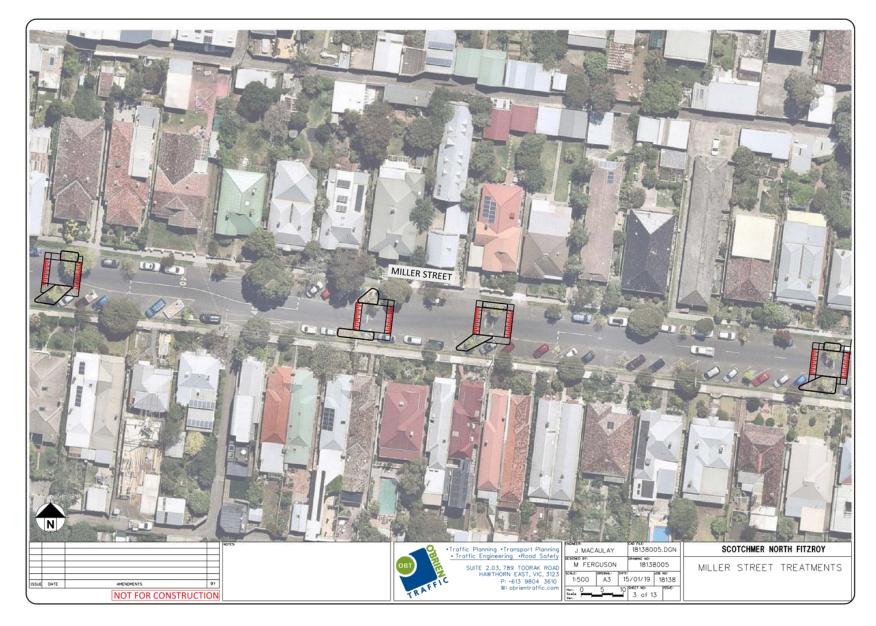
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Agenda Page 224 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019

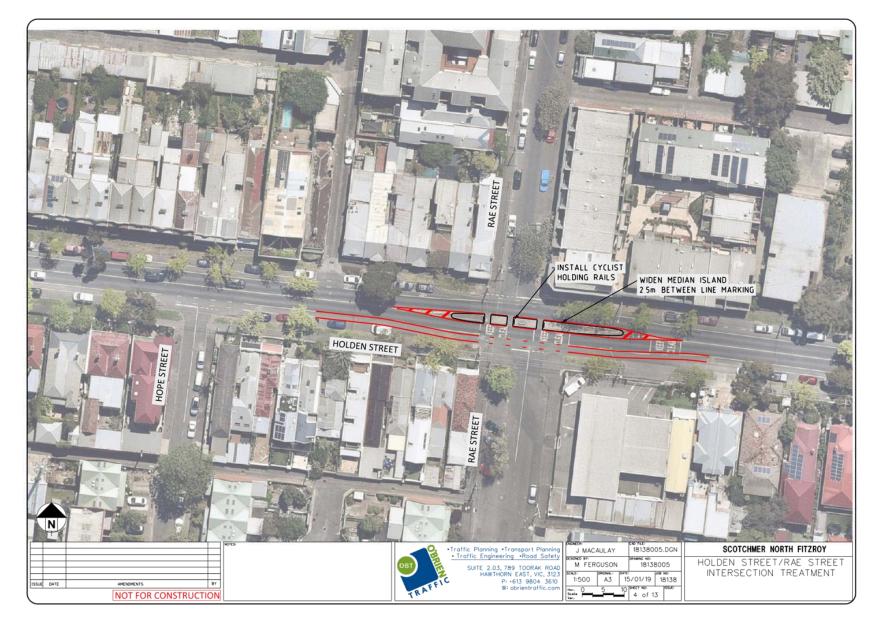
Agenda Page 225 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



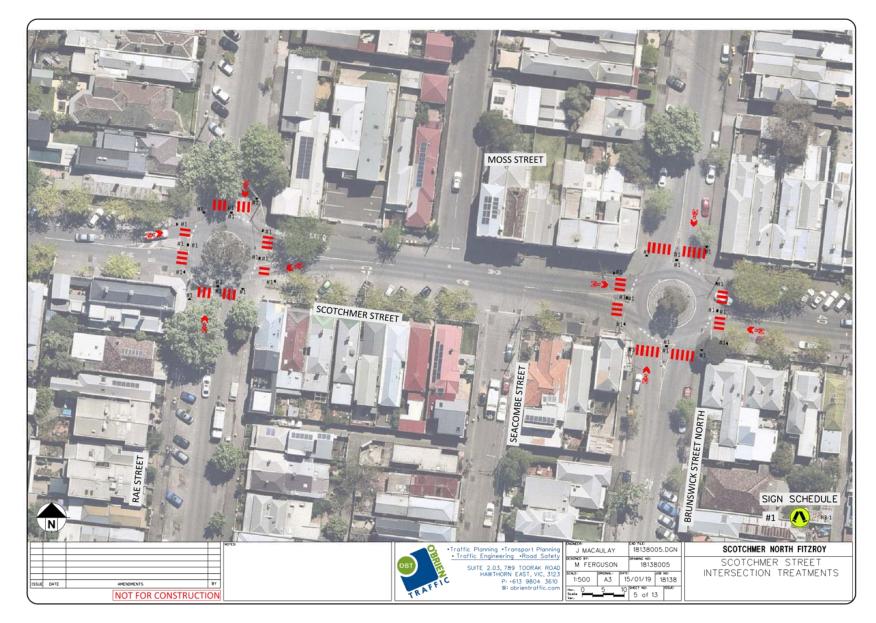
Agenda Page 226 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



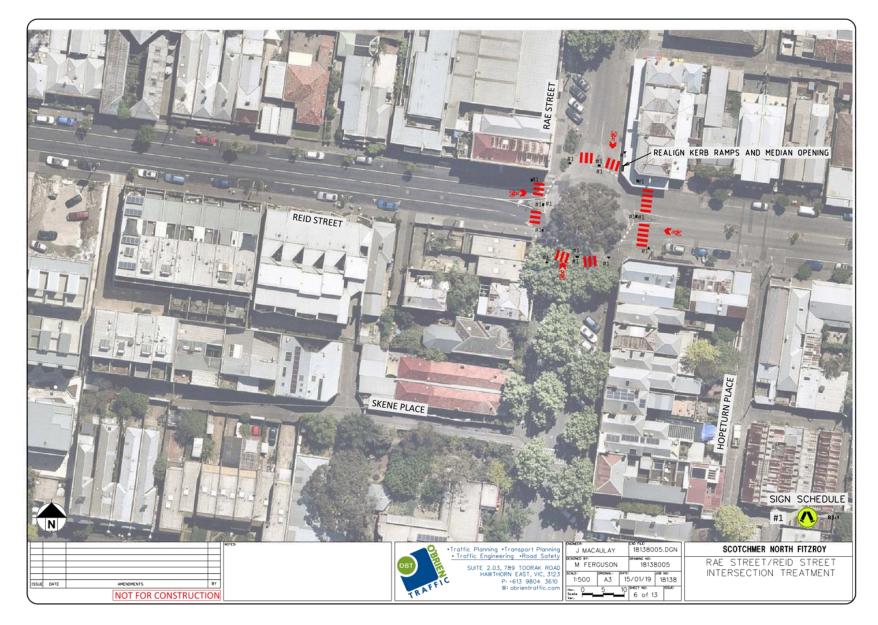
Agenda Page 227 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



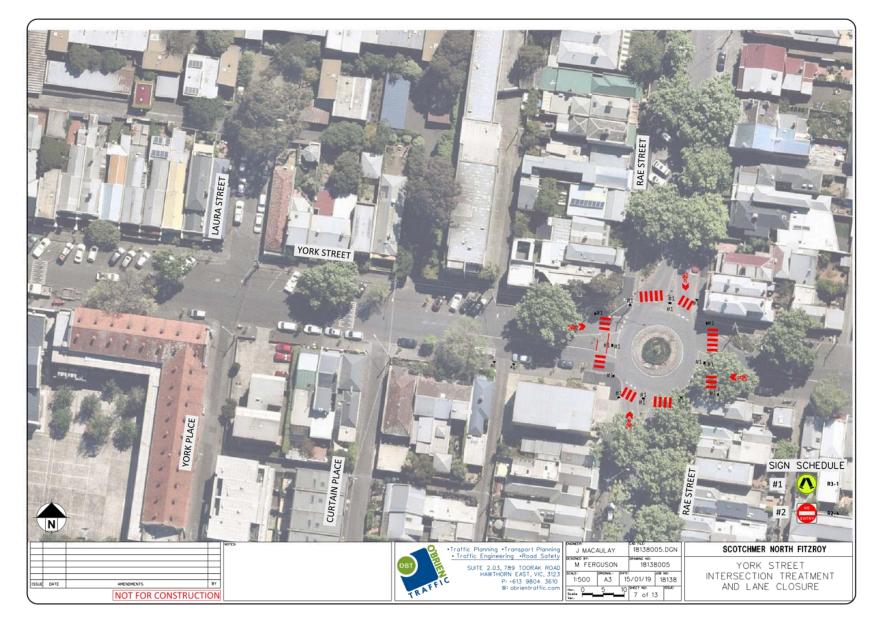
Agenda Page 228 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



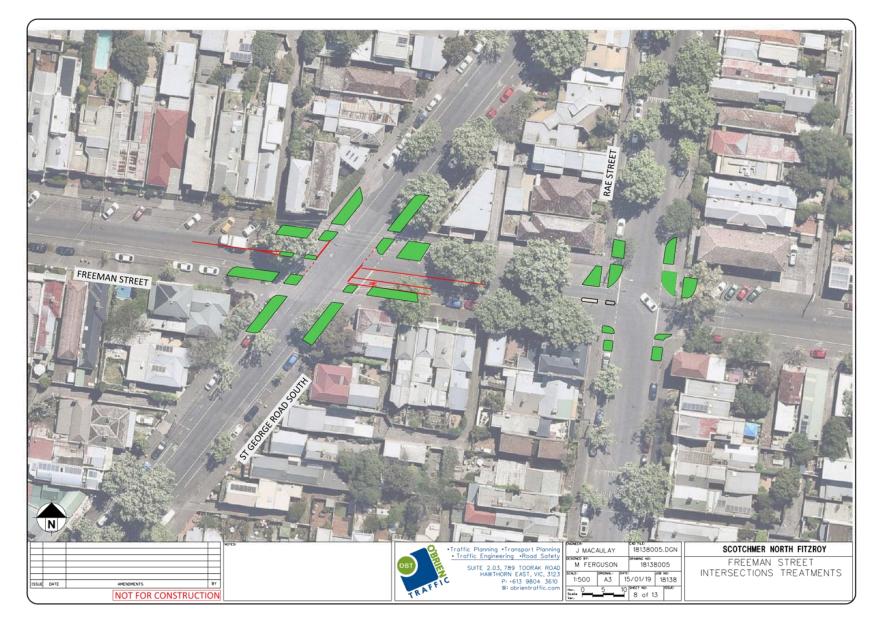
Agenda Page 229 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



Agenda Page 230 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



Agenda Page 231 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



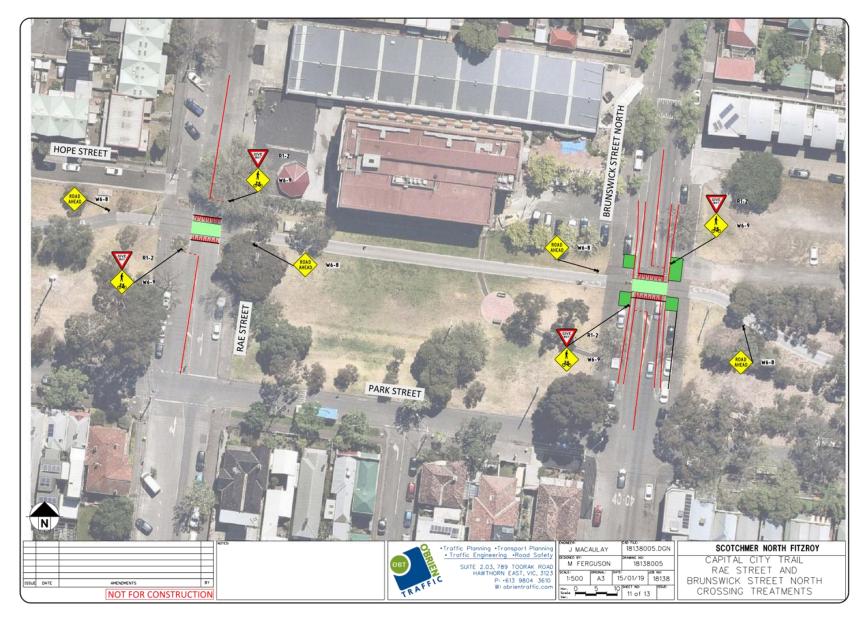
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Agenda Page 232 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019

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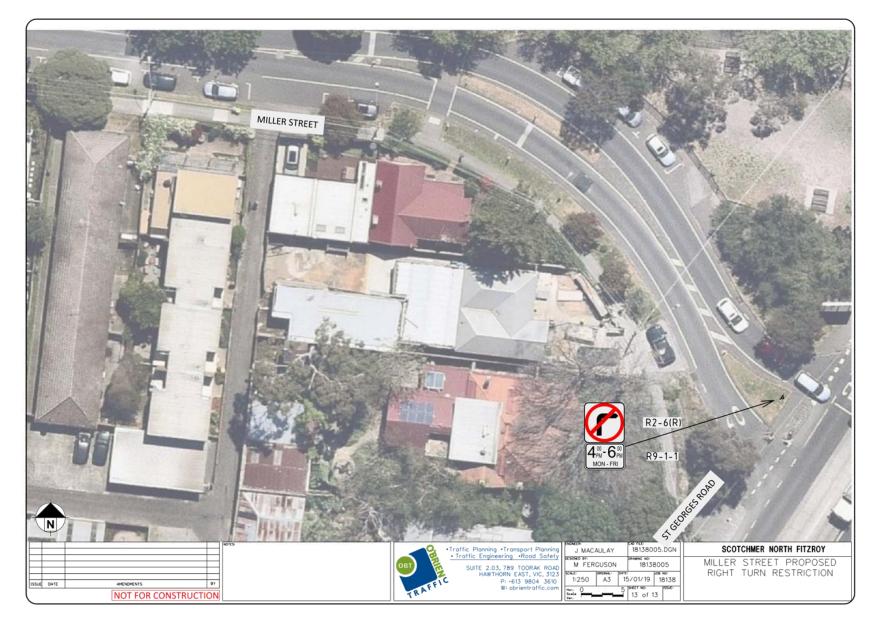
Agenda Page 234 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



Agenda Page 235 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



Agenda Page 236 Attachment 7 - LAPM 3 Scotchmer North Fitzroy final report O'Brien Traffic January 2019



11.4 Trial Mid-Block Road Closure on Gleadell Street

Executive Summary

Purpose

To present to Council a report as requested on the option of trialling a mid-block closure of Gleadell Street to through traffic and recommendations to improve pedestrian and cyclist safety on Gleadell Street.

Key Issues

The process for trialling a mid-block closure of Gleadell Street is covered by the *Local Government Act* 1989 which includes provisions for the closure of roads. Council would be required to submit a detailed traffic engineering assessment of any road closure proposal to VicRoads, as well as undertake a community engagement on the proposal.

Any trial mid-block closure of Gleadell Street would be expected to take nine months for the assessment, consultation and approval process to be completed.

The current preferred approach to considering major changes to local streets and giving full consideration to local needs, is through a wider area study such as a Local Area Place Making (LAPM) process. This precinct requires considerable analysis due to the many different land uses and the significant stakeholder usages of these spaces. An overall traffic, access and movement study should be undertaken before any significant decisions are made.

The Highett Precinct (LAPM 15), which includes Gleadell Street, is third highest in priority for study and is nominally planned to commence in three years' time, subject to future priority assessment and funding allocation. Officers believe this is good timing for consideration of traffic treatments in this precinct given the changes currently occurring in the precinct, and expect the potential impact of more students moving between both campuses of Richmond High School will be better understood by then.

However, if Council resolved to progress with a trial more immediately, three short term options to improve pedestrian and cyclist safety on Gleadell Street have been identified by Council officers. These include installation of a raised threshold treatment at the intersection of Gleadell Street and Highett Street, possible changes to the car parking arrangements to allow installation of a cycle lane on the west side of Gleadell Street, and consideration of making the street one-way for vehicular traffic.

Financial Implications

A traffic, access and movement study would require consultants to undertake an analysis which would require funding. Other options are costed below for Council's information.

Option 1: Temporary mid-block closure of Gleadell Street

The assessment required by VicRoads pursuant to the Local Government Act to determine the potential impacts and undertake the necessary community consultation is estimated to cost in the order of \$40,000. There is currently no budget allocation for the required assessment and consultation.

The lowest cost infrastructure option for a trial mid-block road closure would be in the order of \$3,000, with post-implementation survey cost of \$5,000.

Option 2: Raised threshold treatment on Gleadell Street at Highett Street

The cost to design and build a raised threshold treatment on Gleadell Street at Highett Street intersection is estimated at approximately \$150,000.

Option 3: Cycle lane on the west side on Gleadell Street

The cost for changing to parallel parking and installing a cycle lane is estimated at approximately \$20,000. The loss of approximately 30 car parking spaces would have a financial impact to Council of approximately \$30,000 per annum.

Option 4: Convert Gleadell Street to one-way

The assessment required by VicRoads pursuant to the Local Government Act to determine the potential impacts and undertake the necessary community consultation is estimated to cost in the order of \$40,000. There is currently no budget allocation for the required assessment and consultation.

The minimum capital cost for implementing one-way starts at approximately \$100,000, with post-implementation survey cost of \$5,000.

Officers note that Options 1, 3 and 4 may result in the reallocation of traffic movements into nearby streets. The current preferred approach to considering major changes to local streets and giving full consideration to local needs, is through the Local Area Place Making (LAPM) process.

PROPOSAL

- 1. That Council:
 - (a) notes the process pursuant to Schedule 11, 10(1)(c) of the *Local Government Act* 1989 required to trial a road closure on Gleadell Street;
 - (b) refers the consideration of a road closure and other possible traffic management treatments on Gleadell Street to the LAPM 15 (Highett Precinct) study which will consider traffic needs and impacts across the whole neighbourhood bound by Bridge Road, Burnley Street, Church Street and Victoria Street, Richmond;
 - (c) consider implementing Option 2; a raised threshold treatment on Gleadell St at its intersection with Highett Street, noting that this would require a budget allocation in the order of \$150,000, which has not been budgeted for at this stage; and
 - (d) considers an allocation of \$150,000 for these works as part of the 2019/20 budget deliberations.

11.4 Trial Mid-Block Road Closure on Gleadell Street

Trim Record Number: D19/12947 Responsible Officer: Director City Works and Assets

Purpose

- 1. To present an update to Council on the option of trialling a mid-block closure of Gleadell Street to through traffic and recommendations to improve pedestrian and cyclist safety on Gleadell Street.
- 2. This report was requested by Council at its meeting on 16 October 2018.

Background

3. At its meeting on 16 October 2018, Council adopted the following Resolution;

"That Council:

- (a) receives a further report in February 2019 which includes:
 - (i) a process for undertaking a trial mid-block road closure on Gleadell Street; and
 - (ii) recommendations for measures to immediately improve pedestrian and cycling safety on Gleadell St, including at the intersections of Bridge Rd and Highett St;"

Precinct approach

- 4. It is important that this Council request is considered in the context of the broader precinct.
- 5. The new Richmond High School land use is a further important aspect to the precinct.
- 6. The following section however addresses the specific Council resolution from the Council Meeting on 16 October 2018.

Process for trialling a mid-block road closure

- 7. The *Local Government Act* 1989 (the Act) includes provisions for the closure of roads. Permanent and temporary road closures are treated the same under the Act. Under the provisions, any person or organisation has the right to make a submission on a road closure proposal and VicRoads is required to review the proposal and prepare a report for Council to consider.
- 8. Council would be required to submit a detailed traffic engineering assessment of any road closure proposal to VicRoads. The traffic assessment would involve data collection (e.g. traffic volume and speed counts, origin/destination surveys and intersection and queuing surveys), analysis and modelling. The assessment must identify potential impacts of the road closure and note how those impacts can be mitigated. In addition to this, VicRoads requires a summary of public submissions relating to the road closure proposal in the information provided by Council.
- 9. An independent traffic consultant would need to be engaged to undertake the traffic assessment for the proposal as Council does not have the required computer software to perform the traffic modelling work.
- 10. VicRoads would require a minimum of one month following receipt of the traffic assessment to consider its position and prepare a report to Council.
- 11. Following receipt of the VicRoads report to Council, the community can be consulted on the trial closure based on the key findings of the traffic assessment and VicRoads report.

- 12. In line with Section 223 of the Local Government Act, the consultation period will be a minimum of 28 days following the date on which the public notice is published. There would also be a requirement to cater for any person who wishes to make their submission in person at a meeting of the Council.
- 13. A final report which considers feedback from VicRoads and any further community submissions would then be provided to Council for Council consideration and a decision.
- 14. Should Council decide to pursue the trial closure of Gleadell Street, it is expected to take nine months for the assessment, consultation and approval process to be completed. (See **Attachment 1**).
- 15. Whilst officers strongly support undertaking trials and view road closures as an effective measure to reduce through traffic and improve safety for road users, it should be noted that the installation of temporary traffic signage and bollards alone, whilst providing a low-cost solution, may impact the visual amenity of the street if implemented without strategic consideration to urban design.
- 16. The current preferred approach to considering major changes to local streets and giving full consideration to local needs, is through a precinct wide approach such as the Local Area Place Making (LAPM) process.
- 17. Decision making in the LAPM process is determined on an evidence based approach with treatments identified and developed based on the input of the wider community. The strategic approach adopted in LAPMs ensures that the key traffic issues are addressed by selecting treatments which benefit the broader precinct rather than considering individual streets in isolation. The process also enables improvements to the public realm through integrating urban design and place making into decisions about designing streets.
- 18. The collaborative approach of the LAPM process also enables the community to work from first principles in the consideration of traffic treatments based on a range of options and an informed position. This approach ensures that traffic treatments are not considered in isolation, as well as limiting some of the risks associated with locking in the outcome or proposal before the impacts and potential trade-offs are identified and tested with the community.

Status on Council's LAPM program

- Council has committed to a three year LAPM program. LAPM studies will be undertaken in Scotchmer (LAPM 3) and North Carlton (LAPM 2) in 2018/19, followed by delivery in subsequent years. Completed LAPM studies in Princes Hill (LAPM 1), Rose (LAPM 9), Gold (LAPM 10), Abbotsford (LAPM 13) and Bendigo (LAPM 19) are planned to be delivered over the next two to three years.
- 20. Each year, officers identify which precincts are the highest priority for a LAPM study (see **Attachment 2**). Officers have completed the annual prioritisation ranking assessment and identified that East Clifton Hill (LAPM 6) is the next highest priority for study, followed by West Clifton Hill (LAPM 5).
- 21. The Highett Precinct (LAPM 15), which includes Gleadell Street, is third highest in priority for study and is nominally planned to commence in three years' time, subject to future priority assessment and funding allocation. Officers will continue to monitor changes in the precinct including any road safety issues within the vicinity of the Richmond High School as it expands over the coming years and adjust the priority ranking as appropriate.

Short-term options to improve pedestrian and cyclist safety on Gleadell Street

- 22. VicRoads' most recent available crash statistics for the five year period ending January 2019 were obtained for Gleadell Street. Three crashes were recorded as follows:
 - (a) 11 January 2019, cyclist travelling westbound on Highett Street was struck by a car turning right into Gleadell Street;

- (b) 19 January 2017, cyclist travelling westbound on Highett Street was struck by a car turning left out of Gleadell Street; and
- (c) 6 October 2016, cyclist travelling eastbound on Bridge Road was struck by a car turning left into Gleadell Street.
- 23. These statistics show there is an over representation of crashes involving vehicles turning into or out of Gleadell Street and into the path of cyclists travelling along Highett Street and Bridge Road. There have been no reported crashes along Gleadell Street over the five year period ending January 2019. A mid-block closure of Gleadell Street would not make a significant improvement to cyclist safety at the intersections.
- 24. A cyclist volume survey undertaken in October 2018 indicated that in the AM peak period (7am-9am) there were 23 cyclists using Gleadell Street. In the PM peak period (5pm-7pm) 21 cyclists were recorded using Gleadell Street. These volumes are considered to be consistent with Gleadell Street's status as a local road, and do not indicate that Gleadell Street is currently used as a cyclist commuter route.
- 25. In September 2018 a raised pedestrian crossing was built on Gleadell Street in front of Richmond High School to improve pedestrian connectivity and safety. Further detailed investigation and surveys are required to assess the effectiveness of this pedestrian crossing.
- 26. Introducing raised threshold treatments on Gleadell Street at its intersections with Bridge Road and Highett Street would be the most effective treatment to reduce the likelihood and severity of crashes at these locations.
- 27. Raised threshold treatments are considered an effective measure to improve the street environment by providing visual cues to car drivers that vulnerable road users are in the area. The physical level difference introduced by raised threshold treatments also lowers the speed of vehicles.
- 28. Raised threshold treatments were installed along Wellington Street between Alexandra Parade and Johnston Street as part of the Victorian Government Blackspot Program. These treatments have reduced both the frequency and severity of incidents involving vulnerable road users travelling on Wellington Street and cars turning into or out of side streets.
- 29. VicRoads is in the process of altering the intersections along Bridge Road to extend the kerbs and raise the pavement as part of their Bridge Road Pedestrian Improvement Project. This project includes the installation of a raised threshold treatment on Gleadell Street at its intersection with Bridge Road and is expected to be delivered by the end of FY 2018/19.
- 30. The process for raising the pavement on Gleadell Street at its intersection with Highett Street would require investigation of pedestrian access, all traffic modes using the street, drainage and underground services. A detailed design would be required to ensure all road users can be safely and conveniently accommodated. To undertake the design work for a raised threshold treatment would take approximately two months at a cost of \$10,000.
- 31. There would be no requirement to seek VicRoads consent to consider a raised threshold treatment at Gleadell/Highett Streets.
- 32. A report was presented to Council on 16 October 2018, outlining other options that could be considered to improve the safety of pedestrians and cyclists on Gleadell Street. These included possible changes to the car parking arrangements to allow installation of a cycle lane on the west side of Gleadell Street, and consideration of making the street one way for vehicular traffic.
- 33. The inclusion of a bicycle lane on the west side of Gleadell Street would require a change from the existing 90 degree parking arrangement to parallel parking. This change would remove the risk of cars reversing into the proposed cycle lane and improve safety for both cyclists and crossing pedestrians.

- 34. The change of parking on the west side of Gleadell Street from 90 degree parking to parallel parking requires the loss of approximately 30 car parking spaces, and may have an impact on key stakeholders such as the Richmond Recreation Centre, Richmond Union Bowling Club, Gleadell Street Market and others. The process for changing to parallel parking requires community engagement with key stakeholders, and would take up to three months to complete.
- 35. There is no evidence of reported crashes involving cyclists and vehicles along the length of Gleadell Street, and this option is not recommended at this time.
- 36. Converting Gleadell Street into a one-way street, either northbound or southbound, would reduce the number of vehicle movements in the street and could improve safety for cyclists and pedestrians. Similarly to the mid-block road closure option, making Gleadell Street one-way may result in the reallocation of traffic movements into nearby streets. Officers would need to consider the possible impacts that either a northbound or southbound one-way system would have on local access and amenity, as well as road safety in the broader area.
- 37. The process for implementing one-way traffic on Gleadell Street is similar to a road closure and is treated in the same way under the Local Government Act. Should Council decide to pursue the one-way option, it would be expected to take nine months for the assessment, consultation and approval process to be completed.
- 38. There is no evidence of reported crashes involving cyclists and vehicles along the length of Gleadell Street, and this option is not recommended at this time.

External Consultation

- 39. Should Council proceed with the option to close Gleadell Street to through traffic, a number of key stakeholders would need to be consulted. These include Richmond Union Bowling Club, Richmond High School, Victorian School Building Authority (VSBA), Richmond Multicultural Children's Centre, Lynall Hall, Leo Berry Gym, Bridge Church, building owners and tenants in the precinct and Gleadell Street Market stall holders.
- 40. The Richmond High School principal has recently been consulted on the option to close Gleadell Street. It was agreed that further usage data should be collected once movement patterns associated with the school have been established. This data will assist in determining the appropriate option(s) for the street in consultation with the community and other stakeholders.
- 41. Gleadell Street Market stall holders have previously indicated concern with the removal of parking in the street associated with the recently installed raised pedestrian crossing at the Richmond High School. Additional consultation with stall holders on a trial mid-block closure would be required.

Internal Consultation (One Yarra)

- 42. Council's Urban Design Unit has been consulted and advised that the Bridge Road Streetscape Masterplan recognises that Gleadell Street be established as a civic hub to support community and institutional uses in the Town Hall and Gleadell Street Precinct. The Masterplan identifies traffic measures and a shared zone to focus on pedestrian priority and walking connections, as well as improved bicycle priority and access. A mid-block road closure may impact on the Urban Design intent for the Town Hall and Gleadell Street Precinct.
- 43. Council's Compliance and Parking Services team has been consulted on the option to close Gleadell Street. They advise undertaking further consultation with key stakeholders including Gleadell Street Market stall holders.
- 44. Richmond Recreation Centre has been consulted on the option to close Gleadell Street. Officers have advised that the Richmond Recreation Centre receives regular bus movements related to school swimming classes and other activities. A mid-block closure would impact bus movements as buses would not be able to turn around safely at the closure point.

Reversing or three point turn movements represent significant safety issues for buses and cyclists/pedestrians.

45. Removal of parking and infrastructure works to create a turning area may be possible, but would have impacts on parking availability to local traders, the Richmond Recreation Centre and Gleadell Street Market stall holders, and would also add significantly to the capital cost of the project.

Financial Implications

Option 1: Temporary mid-block closure of Gleadell Street

- 46. Should Council decide to pursue the temporary road closure of Gleadell Street, the level of assessment required by VicRoads pursuant to the Local Government Act to determine the potential impacts and undertake the necessary community consultation is estimated to cost in the order of \$40,000. There is currently no budget allocation for the required assessment and consultation.
- 47. The lowest cost infrastructure option for a trial mid-block road closure would be the installation of signage and bollards, and temporary traffic management to provide a safe worksite. The cost of this infrastructure option is in the order of \$3,000 and would need to be allocated within existing budgets. There would be limited maintenance costs associated with this option.
- 48. There would be a need for Council to undertake traffic data collection on Gleadell Street and other nearby streets in order to monitor the impact of the road closure. The cost of these surveys would be in the order of \$5,000.

Option 2: Raised threshold treatment on Gleadell Street at Highett Street

- 49. The cost to design and build a raised threshold treatment on Gleadell Street at Highett Street intersection is estimated at approximately \$150,000.
- 50. As noted in paragraph 25 above, the raised threshold treatment option at Gleadell Street and Bridge Road is being delivered as part of the VicRoads Bridge Road Pedestrian Improvement Project.

Option 3: Cycle lane on the west side on Gleadell Street

51. The cost for changing to parallel parking and installing a cycle lane is estimated at approximately \$20,000. The loss of approximately 30 car parking spaces would have a financial impact to Council of approximately \$30,000 per annum.

Option 4: Convert Gleadell Street to one-way

- 52. Should Council decide to pursue the option making Gleadell Street one-way, the level of assessment required by VicRoads pursuant to the Local Government Act to determine the potential impacts and undertake the necessary community consultation is estimated to cost in the order of \$40,000. There is currently no budget allocation for the required assessment and consultation.
- 53. The capital cost for implementing one-way starts at approximately \$100,000. This would involve line marking, kerb extensions and signage as a minimum infrastructure cost.
- 54. There would be a need for Council to undertake traffic data collection on Gleadell Street and other nearby streets in order to monitor the impact of the conversion to one-way. The cost of these surveys would be in the order of \$5,000.

Economic Implications

55. The permanent loss of on-street parking as a result of any road closure on Gleadell Street may have an adverse impact on the Gleadell Street Market, Richmond Recreation Centre, Bridge Road/Church Street shops and eating establishments.

Sustainability Implications

56. There are no sustainability issues arising from the recommendations contained in this report.

Social Implications

57. Implementation of a raised threshold treatment on Gleadell Street at its intersection with Highett Street could provide social benefits such as public seating and gathering spaces, as well as improvements to road safety.

Human Rights Implications

58. There are no human rights issues arising from the recommendations contained in this report.

Communications with CALD Communities Implications

59. There is no communication with CALD communities required for this report.

Council Plan, Strategy and Policy Implications

60. Implementation of a raised threshold treatment on Gleadell Street at its intersection with Highett Street to provide a safer environment for road users is in line with Council's Safe Travel Strategy.

Legal Implications

61. There are no legal implications arising from the recommendations contained in this report.

Other Issues

- 62. Council officers regularly receive requests from members of the community for Council to close roads across the municipality to address issues such as traffic using local roads as opposed to using arterial roads. In each case, members of the community are advised that road closures will only be considered as part of the LAPM process for the respective precincts, as this is the most appropriate mechanism in which to study, identify and communicate potential traffic impacts associated with a road closure and consider submissions from the public on the proposal.
- 63. The consideration of a road closure on Gleadell Street, outside of the LAPM process, is not consistent with officers' previous responses to road closure requests in the municipality. Before a decision can be made about closing a road, analysis of the traffic impacts to this street and roads within the precinct need to be undertaken, and this is ideally done as part of a LAPM process.

Conclusion

- 64. A trial mid-block road closure is possible in Gleadell Street and officers acknowledge that such treatment would reduce through traffic in the street. It is also recognised that road closures could provide opportunities to further enhance the streetscape character on Gleadell Street.
- 65. There is evidence of reported crashes involving cyclists and vehicles at the intersections of Gleadell Street with Bridge Road and Highett Street, but not along the mid-section of Gleadell Street.
- 66. A raised threshold treatment at Gleadell/Highett Streets is the most effective way of improving cyclist and pedestrian safety on Gleadell Street. Providing a raised threshold treatment is considered to be a higher priority than providing a mid-block road closure or making Gleadell Street one-way at this time.
- 67. Officers note that Griffiths Street and Palmer Street also represent opportunities for similar raised threshold treatments to improve road safety, especially given the location of Richmond High School. The VSBA have been urged to assist in funding treatments of this type as part of the Richmond High School development, but have not agreed to this to date. The investigation and any proposed implementation of these treatments should be addressed through the LAPM 15 (Highett Precinct) study.

RECOMMENDATION

- 1. That Council:
 - (a) notes the process pursuant to Schedule 11, 10(1)(c) of the *Local Government Act* 1989 required to trial a road closure on Gleadell Street;
 - (b) refers the consideration of a road closure and other possible traffic management treatments on Gleadell Street to the LAPM 15 (Highett Precinct) study which will consider traffic needs and impacts across the whole neighbourhood bound by Bridge Road, Burnley Street, Church Street and Victoria Street, Richmond;
 - (c) consider implementing a raised threshold treatment on Gleadell Street at its intersection with Highett Street, noting that this would require a budget allocation in the order of \$150,000, which has not been budgeted for at this stage; and
 - (d) considers an allocation of \$150,000 for these works as part of the 2019/20 budget deliberations.

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Attachments

- **1**. Attachment 1 Expected timescale for trial mid-block road closure process
- 2. Attachment 2 LAPM Priority List February 2019

Attachment 1 - Attachment 1 - Expected timescale for trial mid-block road closure process

Step 1 – Preparation of information to allow Vic	Roads to prepare its report to Council			
Prepare scope, tender and appoint consultant	Late March to mid April 2019			
to undertake traffic analysis				
Data collection and preparation of background	Late March to mid April 2019			
information				
Traffic analysis performed by consultant	Mid April to late May 2019			
Consultation with community and stakeholders	Early June to early July 2019			
following receipt of external traffic analysis				
Finalise traffic report and provide information	Early July to late July 2019			
to VicRoads				
Step 2 – VicRoads consider information and pro	vide report to Council			
VicRoads prepare response to Council	Late July to early September 2019			
Step 3 – Community consultation as per require	ments of Section 223 of LGA 1989			
Prepare consultation material based on	Early September to mid September 2019			
VicRoads report				
28-day consultation period as per LGA	Mid September to mid October 2019			
requirements				
Review of consultation and preparation of	Mid October to mid November 2019			
Council report				
Step 4 – Report to Council				
Report to Council meeting	First cycle of Council meetings in December			
-	2019			

Attachment 1 – Expected timescale for trial mid-block road closure process

Agenda Page 247 Attachment 2 - Attachment 2 - LAPM Priority List February 2019

RANKING LIST 2019				
Rank LAPM Area		Comment		
Rank	LAPM Area	Comment		
1	East Clifton Hill (6)	Last Studied 2012/13		
2	West Clifton Hill (5)	Last Studied 2011/12		
3	Highett (15)	Last Studied 2007/08		
4	Alphington (8)	Last Studied 2012/13		
5	Coppin (18)	Last Studied 2012/13		
6	Richmond (17)	Last Studied 2013/14		
7	North Richmond (14)	Last Studied 2011/12		
8	Coate (7)	Western area completed 2008/09		
9	Victoria (16)	Last reviewed 2015		
-	North Carlton (2)	Being Studied 2018/19		
-	Fitzroy (11)	Impacted by M41 Water Main Renewal		
-	Collingwood (12)	Impacted by CMP sites & Last reviewed 2015		
-	North Fitzroy (4)	Impacted by M41 Water Main Renewal		
-	Barkly (21)	Western area included in LATMS 20, reviewed November 2014		
-	Balmain (20)	Impacted by CMP sites & Last reviewed November 2014		
-	Gold (10)	Stage 3 construction 2017/18		
-	Rose (9)	Stage 2 construction 2018/19 and 2019/20 - 30km/h Trial Area		
-	Princes Hill (1)	Stage 1-3 construction 2018/19 and 2019/20		
-	Scotchmer (3)	Study 2017/18		
-	Abbotsford (13)	Study 2017/18		
-	Bendigo (19)	Study 2017/18		

Attachment 2 - LAPM Priority List February 2019

11.5 Mid-Year and 2019/20 Budget Review

Trim Record Number: D19/21569 Responsible Officer: Director Corporate, Business and Finance

Purpose

- 1. For Council to consider funding the following projects referred to the mid-year budget review:
 - (a) Creative Yarra additional grant funding; \$60k for 2019/20 and \$60k for 2020/21;
 - (b) Celebrate Yarra additional grant funding; \$80k for 2019/20, \$80k for 2020/21, and \$80k for 2021/22;
 - (c) Replacement of Climate Emergency banners at Collingwood & Fitzroy Town Halls; \$5k;
 - (d) Bicycle Network Infrastructure; \$200k;
 - (e) Victoria St Litter & Oil Interceptor; \$150k;
 - (f) Building Energy Infrastructure; \$400k; and
 - (g) Activity Centre Design; \$160k.

Background

- 2. At its meeting on 19 February 2019, Council received its 2018/19 mid-year financial report. The report indicated a modest above budget net income surplus of \$332k (excluding \$3m grant associated with depot relocation).
- 3. A number of projects had previously been referred to the mid-year financial report for funding consideration. Councillors requested further information on these projects and for these to be considered again at its next meeting, which is the purpose of this report.

External Consultation

4. External consultation was not required.

Internal Consultation (One Yarra)

5. Internal consultation with the responsible officers for these projects has occurred.

Financial Implications

6. The financial implications will be for Council to utilise its modest forecast above budget surplus of \$332k to fund a selection of the projects referred to the mid-year budget review. This then reduces the amount of cash available going in to the 2019/20 budget year by the same amount.

Economic Implications

7. There are no economic implications.

Sustainability Implications

8. There are no sustainability implications.

Social Implications

9. There are no social implications.

Human Rights Implications

10. There are no Human Rights implications.

Communications with CALD Communities Implications

11. Communications with CALD Communities was not required.

Council Plan, Strategy and Policy Implications

12. There are no Council Plan, Strategy, or Policy implications.

Legal Implications

13. There are no legal implications.

Projects Referred to Mid-Year Budget Review

<u>Creative Yarra & Celebrate Yarra Grants – \$140k in 2019/20, \$140k in 2020/21, \$80k in 2021/22</u>

14. Some clarity was requested around the requirements of the Creative Yarra and Celebrate Yarra grants, as they are for future years' budgets. The reason that funding for these programs is being sought through the mid-year budget review process is because the applications for those grants are called for early on in the calendar year, and the organisation needs to be clear on how much funding is available to offer. If these bids were subject to the Annual Budget process, the timing would be too late. For example, applications for the 2019/20 grants are being called for now, whereas Council won't resolve on its 2019/20 Budget until late June 2019. If Council only funds the 2019/20 part of the request, the same situation will occur for the 2020/21 and 2021/22 grants, again due to the timing of when the applications go out and when Council resolves on its Budget.

Bicycle Network Infrastructure - \$200k

- 15. The proposed new initiative bids for 2019/20 include an allocation of funding for Stage 2 of the Wellington Street separated bike lane project which has a planning permit to proceed. This is currently in tender evaluation and a report will be presented to Council shortly.
- 16. Some external funding is likely for the Wellington Street project which would then enable some relocation to other cycling projects for consideration. That will become clearer over the next couple of months.
- 17. Projects as possible candidates for that reallocation of funding are being arranged for Councillors to consider should the external funding eventuate.
- NB. Some Councillors have sought information on signage on bike lanes to seek better and safer traffic flow between vehicles and bikes – some allocation could be provided to this should Councillors see that as a priority (say \$30 to \$40k)

Victoria St Litter & Oil Interceptor - \$150k

19. The approximate cost of this project is \$150k. It consists of detailed design and construction of a Gross Pollutant Trap oil interceptor to capture and remove litter and pollutants from the Victoria street drainage line. This project is deliverable in 2018/19 with an appropriate budget allocation.

Building Energy Infrastructure - \$400k

- 20. Since the budget considerations in June 2018, Council received a report on 18 December, 2018 entitled *Buildings and Street Lighting Energy Efficiency Opportunities*. This was to report against the Annual Plan Action milestone (3.2) to *'provide a public report to Council with a prioritised list of building energy projects on Council buildings'*
- 21. This report outlined the trend analysis of Council's gross greenhouse emissions and related actions.
- 22. The report highlighted that a key priority opportunity for Council is to continue to reduce Council building energy use by optimising the substantial existing sustainable plant that Council has already invested in, and actively manage the operation of buildings and equipment with energy efficiency in mind.
- 23. The current new initiative bids under consideration for 2019/20 include a 'Building Optimisation Officer' which would be required to achieve these performance outcomes of the existing plant and in refurbishments and new builds.

- 24. The December report also indicated that a further major opportunity for emission reduction is to commence a *stage 2 of street lighting efficiency retrofits*.
- 25. The submission to the 2019/20 budget allocation is for an allocation of \$200,000 in the renewal budget as a start-up program to commence this work for a renewal of some 200 lights on major roads. This would commence a program over some 7 years to replace lights on the major roads saving about \$285,000 and between 700 to 1,000 tCO2e per annum. The full costings over the 7 years would be approximately \$2m. Potential Grants and rebates may exist for this program over the 7 years.
- 26. NB. Some other possible projects were outlined to Councillors for consideration as part of the December 2018 report to Council (provided in a Board Books Memo dated 18 December under the Planning and Transport tab entitled *'Prioritised List of Buildings and Street Light Energy Efficiency Opportunities'*)

Activity Centre Design - \$160k

27. The previous 2018/19 bid comprised of 4 components being:

	Total	\$160k
(d)	Cremorne Public realm	\$25k
(c)	Burnley Station	\$60k
(b)	Cremorne Street Intersection	\$50k
(a)	Judd Street carpark feasibility	\$25k

- 28. The Judd Street carpark analysis has been prepared internally during the second quarter of 2018/19. This provides a basis for some consideration of this carpark for future conversations.
- 29. The Cremorne Street intersection maybe able to be funded by the State as part of the Cremorne investigations this is yet to be determined.
- 30. The Burnley / Swan intersection precinct is an important emerging precinct and some design money would be ideal. As part of the 'Inner Metropolitan Partnership' there is a possibility of this area being funded, in part, by the State for an 'access, place and movement' study. Council would need to fund part of this study. An allocation of say \$10K in 2019/20 to match a contribution by the State would be ideal and enable the study to proceed.
- 31. The Cremorne public realm has 2 current new initiative bids for 2019/20 for consideration by Council. Further work in Cremorne is ideal but requires analysis and considerations with the program with the Victorian Planning Authority.

Options

- 32. Council has the following options:
 - (a) Accepting the officer's recommendation; or
 - (b) Holding all of the forecast surplus for carry over into the 2019/20 Budget; or
 - (c) Changing the officer's recommendation and choosing different projects from the ones referred to the mid-year review to fund.

Conclusion

- 33. Council has a small amount of forecast above budget surplus available \$332k to potentially allocate to projects referred to the mid-year budget review.
- 34. Officers have confirmed that sufficient funding is available to allocate this \$332k in the 2018/19 financial year.
- 35. The recommendation below is based on deliverability of projects in the remainder of the 2018/19 financial year.

RECOMMENDATION

- 1. That Council, having noted the 2018/19 December Financial report:
 - (a) allocate the following amounts from the mid-year budget review forecast above budget surplus:
 - \$140k to the Creative Yarra and Celebrate Yarra grants and in the future years of Council's long term financial strategy, build this amount into the base operating budget for 2020/21 onwards, with an additional amount of \$80k also added in from 2021/22 onwards;
 - (ii) \$42k to the Bicycle Network Infrastructure; and
 - (iii) \$150k to the Victoria Street Litter & Oil Interceptor;
 - (b) allocate \$5k for the replacement of the Climate Emergency banners at Collingwood & Fitzroy Town Halls, from forecast savings from Council's 2018-19 Capital Works program; and
 - (c) refer for further budget considerations to the 2020/21 budget process:
 - (i) the Building Energy Infrastructure; and
 - (ii) the Activity Centre Design.

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Attachments

There are no attachments for this report.

11.6 Councillor Attendance at ALGA 2019 National Conference

Trim Record Number: D19/21342 Responsible Officer: Group Manager Chief Executive's Office

Purpose

1. To approve attendance by Councillors, at the principal national local government convention, the National General Assembly of the Australian Local Government Association (ALGA), to be held from 16 – 19 June 2019 at the National Convention Centre, Canberra.

Background

- 2. Council has in recent years successfully submitted a number of motions to the ALGA National General Assembly.
- 3. Council has generally been represented at both the National General Assembly and the State Council by one or two Councillors, in order to present the Council's motions and also meet other Councillors from around the nation/state and learn of issues being addressed by local governments and importantly, methods of dealing with same.
- 4. Council's Expense Entitlement Policy provides:
 - (a) "Councillors' attendance at interstate and overseas conferences and Council's payment of airfares is subject to the approval of the Council;" and
 - (b) "Subject to the availability of funds, Council shall pay for the cost of registration fees, accommodation and travelling expenses, meals and other incidental expenses associated with authorised attendance at conferences and seminars."
- 5. Expressions of interest have been sought from Councillors, with the Mayor (Cr Danae Bosler) and Deputy Mayor (Cr Misha Coleman) expressing a desire to attend.

Consultation

6. Not relevant to this report.

Financial Implications

- 7. The costs associated with attendance by the Mayor and Deputy Mayor at the Assembly are estimated at \$4,130, broken down as follows:
 - (a) Conference registration 2 attendees at \$900 each = \$1,800;
 - (b) Airfare 2 attendees at 550 each = 1,100;
 - Accommodation 1 Councillor at \$730 (Cr Bosler does not require accommodation); and
 - (d) Meals, taxis and other incidentals Total for two Councillors \$500.
- 8. Provision is made in the Council budget for Councillors to attend approved conferences.

Economic Implications

9. There are no economic implications.

Sustainability Implications

10. There are no sustainability implications.

Social Implications

11. Not applicable to this report.

Human Rights Implications

12. There are no Human Rights implications associated with this report.

Communications with CALD Communities Implications

13. There are no communications with CALD communities implications associated with this report.

Council Plan, Strategy and Policy Implications

14. The attendance at peak body conferences enables discussion with Councillors across the nation to compare issues, processes, services standards which assist Council in formulating its own plans, strategies and work procedures.

Legal Implications

15. There are no legal issues concerned with attendance by Councillors at the National General Assembly or the State Council, save that details of interstate travel (dates, attendees and costs) must be recorded in the interstate travel register, in accordance with the *Local Government (General) Regulations*, in respect of the National General Assembly.

Other Issues

16. None applicable.

Options

17. None applicable.

Conclusion

18. That Council authorise the attendance of the Mayor and Deputy Mayor at the National General Assembly 2019.

RECOMMENDATION

 That, pursuant to Council's Expense Entitlement Policy, Council approve attendance by the Mayor (Cr Danae Bosler) and Deputy Mayor (Cr Misha Coleman) at the Australian Local Government Association (ALGA) National General Assembly in Canberra from 16 – 19 June 2019.

CONTACT OFFICER:Rhys ThomasTITLE:Senior Governance AdvisorTEL:9205 5302

Attachments

There are no attachments for this report.