Ordinary Meeting of Council Agenda

to be held on Tuesday 19 December 2017 at 7.00pm
Fitzroy Town Hall

Arrangements to ensure our meetings are accessible to the public

Council meetings are held at either the Richmond Town Hall or the Fitzroy Town Hall. The following arrangements are in place to ensure they are accessible to the public:

- Entrance ramps and lifts (off Moor Street at Fitzroy, entry foyer at Richmond).
- Interpreting assistance is available by arrangement *(tel. 9205 5110).*
- Auslan interpreting is available by arrangement *(tel. 9205 5110).*
- A hearing loop is available at Richmond only and the receiver accessory is available by arrangement *(tel. 9205 5110).*
- Proposed resolutions are displayed on large screen.
- An electronic sound system amplifies Councillors' debate.
- Disability accessible toilet facilities are available at each venue.

Recording and Publication of Meetings

An audio recording is made of all public Council Meetings and then published on Council's website. By participating in proceedings (including during Public Question Time or in making a submission regarding an item before Council), you agree to this publication. You should be aware that any private information volunteered by you during your participation in a meeting is subject to recording and publication.

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Order of business

1. **Statement of recognition of Wurundjeri Land**
2. **Attendance, apologies and requests for leave of absence**
3. **Declarations of conflict of interest (Councillors and staff)**
4. **Confidential business reports**
5. **Confirmation of minutes**
6. **Petitions and joint letters**
7. **Public question time**
8. **General business**
9. **Delegates' reports**
10. **Questions without notice**
11. **Council business reports**
12. **Notices of motion**
13. **Urgent business**

1. Statement of Recognition of Wurundjeri Land

"Welcome to the City of Yarra."

"Yarra City Council acknowledges the Wurundjeri as the Traditional Owners of this country, pays tribute to all Aboriginal and Torres Strait Islander people in Yarra and gives respect to the Elders past and present."

2. Attendance, apologies and requests for leave of absence

Anticipated attendees:

Councillors

- Cr Daniel Nguyen (Mayor)
- Cr Misha Coleman (Deputy Mayor)
- Cr Danae Bosler
- Cr Mi-Lin Chen Yi Mei
- Cr Jackie Fristacky
- Cr Stephen Jolly
- Cr Mike McEvoy
- Cr James Searle
- Cr Amanda Stone

Council officers

- Vijaya Vaidyanath (Chief Executive Officer)
- Andrew Day (Director - Corporate, Business and Finance)
- Ivan Gilbert (Group Manager - CEO's Office)
- Lucas Gosling (Acting Director - Community Wellbeing)
- Chris Leivers (Acting Director – City Works and Assets)
- Bruce Phillips (Director - Planning and Place Making)
- Jane Waldock (Assistant Director - Planning and Place making)
- Fred Warner (Group Manager – People, Culture and Community)
- Mel Nikou (Governance Officer)

3. Declarations of conflict of interest (Councillors and staff)

4. Confidential business reports

Item	
4.1	Matters prejudicial to Council and/or any person
4.2	Industrial matters
4.3	Contractual matters
4.4	Contractual matters

Confidential business reports

The following items were deemed by the Chief Executive Officer to be suitable for consideration in closed session in accordance with section 89 (2) of the *Local Government Act* 1989. In accordance with that Act, Council may resolve to consider these issues in open or closed session.

RECOMMENDATION

1. That the meeting be closed to members of the public, in accordance with section 89 (2) of the *Local Government Act* 1989, to allow consideration of:
   (a) matters prejudicial to Council and/or any person;
   (b) industrial matters; and
   (c) contractual matters.
2. That all information contained within the Confidential Business Reports section of this agenda and reproduced as Council Minutes be treated as being and remaining strictly confidential in accordance with the provisions of sections 77 and 89 of the *Local Government Act* 1989 until Council resolves otherwise.

5. Confirmation of minutes

RECOMMENDATION

That the minutes of the Ordinary Council Meeting held on Tuesday 5 December 2017 be confirmed.

6. Petitions and joint letters

7. Public question time

Yarra City Council welcomes questions from members of the community.

Public question time procedure

Ideally, questions should be submitted to Council in writing by midday on the day of the meeting via the form available on our website. Submitting your question in advance helps us to provide a more comprehensive answer. Questions that have been submitted in advance will be answered first.

Public question time is an opportunity to ask questions about issues for which you have not been able to gain a satisfactory response on a matter. As such, public question time is not:

- a time to make statements or engage in debate with Councillors;
- a forum to be used in relation to planning application matters which are required to be submitted and considered as part of the formal planning submission;
- a forum for initially raising operational matters, which should be directed to the administration in the first instance.

If you wish to raise matters in relation to an item on this meeting agenda, Council will consider submissions on these items in conjunction with and prior to debate on that agenda item.

When you are invited by the meeting chairperson to ask your question, please come forward and take a seat at the microphone and:

- state your name clearly for the record;
- direct your questions to the chairperson;
- ask a maximum of two questions;
- speak for a maximum of five minutes;
- refrain from repeating questions that have been asked previously by yourself or others; and
- remain silent following your question unless called upon by the chairperson to make further comment or to clarify any aspects.

8. General business

9. Delegates' reports

10. Questions without notice

11. Council business reports

Item		Page	Rec. Page	Report Presenter
11.1	Business Advisory Group - appointment of business representatives	8	9	David Walmsley – Manager City Strategy
11.2	Edinburgh Gardens North Precinct and Playground Upgrade	10	16	Justin Hanrahan – Manager Open Space and Recreation
11.3	Victoria Street Precinct and Reimagining Victoria Street update	47	57	Malcolm McCall – Unit Manager Social Policy and Research
11.4	Managing Growth and Change in Yarra	58	65	David Walmsley – Manager City Strategy
11.5	Amendment C223 - Proposed rezoning of 81-95 Burnley Street and 26 Doonside Street, Richmond	66	73	David Walmsley – Manager City Strategy
11.6	Yarra Planning Scheme Amendment C225 - 351 Church Street Richmond - residential aged care facility - height limit exemption	270	278	David Walmsley – Manager City Strategy
11.7	Station Street, Carlton North - Temporary Road Closure	327	336	Ivan Gilbert - Group Manager Chief Executive's Office
11.8	PLN16/0085 - 115 Victoria Parade and 28 - 42 Young Street, Fitzroy - Australian Catholic University: Endorsement of Amended Development	433	440	Mary Osman – Manager Statutory Planning
11.9	Amendment C231 Queens Parade Clifton Hill	577	592	David Walmsley – Manager City Strategy
11.10	Review - Heritage Advisory Committee and the Terms of Reference	919	925	Ivan Gilbert - Group Manager Chief Executive's Office
11.11	Passage of the Marriage Amendment (Definition and Religious Freedoms) Bill	973	976	Ivan Gilbert - Group Manager Chief Executive's Office
11.12	Annual Plan Quarterly Progress Report - September 2017	977	983	Andrew Day – Director Corporate Business and Finance
11.13	Hydrotherapy Pool report	1039	1045	Lucas Gosling – Acting Director Community Wellbeing
11.14	Report on Assemblies of Councillors Held - August to December 2017	1046	1047	Ivan Gilbert - Group Manager Chief Executive's Office

The public submission period is an opportunity to provide information to Council, not to ask questions or engage in debate.

Public submissions procedure

When you are invited by the meeting chairperson to make your submission, please come forward and take a seat at the microphone and:

- state your name clearly for the record;
- direct your submission to the chairperson;
- speak for a maximum of five minutes;
- confine your remarks to the matter under consideration;
- refrain from repeating information already provided by previous submitters; and
- remain silent following your submission unless called upon by the chairperson to make further comment.

12. Notices of motion

Item		Page	Rec. Page	Report Presenter
12.1	Notice of Motion No. 23 of 2017 - Planning Applications in the World Heritage Environs Area	1055	1057	Stephen Jolly - Councillor

13. Urgent business

Nil

11.1 Business Advisory Group - appointment of business representatives

Trim Record Number: D17/195082
Responsible Officer: Manager City Strategy

Purpose

1. For Council to appoint three representatives from the business community to the Business Advisory Group (BAG).

Background

2. There are currently three vacant positions on the BAG.

3. Council's policy on the appointment of community and/or other members to Council Committees states that the *Council will by resolution make the final determination on the selection and appointment of members to any Council Committee*.

External Consultation

4. The nomination period for the three positions on the BAG was held for a period of six weeks. Advertisements calling for nominations for membership of the BAG appeared on Council's website and Facebook and in two separate editions of Council's Business email bulletin.

Internal Consultation (One Yarra)

5. Information on the nomination process was featured in Yarra News from 11 October 2017.

Financial Implications

6. The ongoing costs associated with the BAG are covered by the Economic Development Unit operational budget. There are no significant financial implications that would arise out of appointing additional members.

Economic Implications

7. The objectives of the BAG include:

   (a) capitalising on the knowledge, experience and skill available in Yarra's large and dynamic local business community to assist Council with the implementation of the objectives in the Yarra Economic Development Strategy;

   (b) raising the profile of the business community in Council activities and planning to support the significant contribution made to our local economy;

   (c) ensuring that a key sector of the community is provided with an appropriate communication mechanism with Council; and

   (d) providing strategic advice to Council on issues that affect the business community.

Sustainability Implications

8. There are no apparent sustainability implications.

Social Implications

9. There are no apparent social implications.

Human Rights Implications

10. There are no apparent human rights implications.

Communications with CALD Communities Implications

11. There are no apparent implications relating to CALD communities.

Council Plan, Strategy and Policy Implications

12. The BAG will continue to play a key role in overseeing the implementation and review of Council's Economic Development Strategy.

Legal Implications

13. There are no apparent legal implications.

Nomination to the BAG

14. A total of 7 nominations were received. These have been assessed by the two BAG Councillor delegates, the Mayor Cr Nguyen and Cr Fristacky and the Coordinator Economic Development. Nominations considered the following:

    (a) representation across the municipality;

    (b) representation of different size businesses;

    (c) representation of different business sectors;

    (d) local business proprietor, property owner and/or employee based in the City of Yarra;

    (e) not a member of another Yarra City Council Community Advisory Committee;

    (f) business experience and skills, relevant network connections;

    (g) ability to work collaboratively with other businesses and/or between business and government;

    (h) a strong interest in the ongoing development of a prosperous and unique economy in the City of Yarra;

    (i) willing to commit 2 hours per quarter (8 hours per annum) in attending Business Advisory Group meetings; and

    (j) an ability to complement the skills and attributes of other members of the Group.

Conclusion

15. Three business representatives are required to be appointed to the BAG. Council is requested to now make these formal appointments.

RECOMMENDATION

1. That Council note the report of officers in relation to the Business Advisory Group.
2. The Council, having considered the nominations received for the three business representative positions on the Business Advisory Group, appoint the following applicants to the Business Advisory Group:

    (a) _____

    (b) _____

    (c) _____

3. That the successful and unsuccessful nominees be advised in writing of the outcome of their nomination.

CONTACT OFFICER: **Kim Swinson**
TITLE: **Coordinator Economic Development**
TEL: **9205 5303**

Attachments
There are no attachments for this report.

11.2 Edinburgh Gardens North Precinct and Playground Upgrade

Trim Record Number: D17/186631
Responsible Officer: Manager Open Space and Recreation

Purpose

1. To update Council on the outcomes of consultation on:
   (a) The draft Edinburgh Gardens Northern Precinct Plan; and,
   (b) The draft Edinburgh Gardens Playground Upgrade Concept Plan.
2. To seek Council endorsement for the final Edinburgh Gardens Northern Precinct Plan.

Background

3. The demolition of International House and the associated carpark, as well as the development of the North Fitzroy Community Hub, (Bargoonga Nganjin), have provided an opportunity to review the layout of the northern section of Edinburgh Gardens.
4. Consultation and engagement undertaken in May-June 2016 sought community feedback on various aspects of the Edinburgh Gardens northern precinct, including:
   (a) following the demolition of International House and its associated carpark, what should this land be used for?
   (b) with the existing playground nearing the end of its lifespan, how should this playground be upgraded and improved?
   (c) *given the forthcoming vacancy at the Emely Baker Building, what should this venue be used for?*
5. The key themes that emerged during the 2016 consultation and engagement process can be categorised as follows:
   (a) Park infrastructure:
      (i) additional green space in the park;
      (ii) expansion of the skate park;
      (iii) improved BBQ and picnic area;
      (iv) provision of a basketball half court;
      (v) provision of chess tables; and
      (vi) improvements to cycle/pedestrian paths.
   (b) Playground:
      (i) more nature play opportunities for the playground;
      (ii) a playground that continues to cater to younger age groups; and
      (iii) provision of more shade in the playground.
   (c) Buildings:
      (i) demolish International House building and the carpark (which was viewed as dangerous); and
      (ii) convert the Emely Baker building (old Maternal Child Health centre) to a community space.
6. Feedback and key themes that emerged during the 2016 community consultation, informed the preparation of the:

(a) the draft Northern Precinct Plan, a high-level plan to guide future works in the northern area of the park; and

(b) the draft Playground Upgrade Concept Plan, a more detailed plan for an improved playground within the northern area of the park.

7. In November 2017 further consultation and engagement was undertaken with park users and the local community on the two proposals, (the draft Northern Precinct Plan and the draft Playground Upgrade Concept Plan) (**Attachment 1**).

8. Key features of the draft Edinburgh Gardens Northern Precinct Plan presented to the community include:

(a) relocating the playground so it sits outside of the existing tree protection zone and further away from Alfred Crescent;

(b) returning the existing playground area to open space;

(c) retaining and protecting the existing significant trees on site and planting additional trees;

(d) expanding the skate and BMX park;

(e) relocating and expanding the existing BBQ area so it sits north of the playground and is located closer to the proposed passive recreation area and future Emely Baker Community Space. It is also located further from the proposed skate park expansion so the tables are not inadvertently used as skate infrastructure; and

(f) including and consolidating an 'active precinct' by adding a half-court basketball court and relocating the existing table tennis table to that area.

9. Key features of the draft Playground Upgrade Concept Plan include:

(a) expanding the playground footprint;

(b) adding nature play opportunities to the playground;

(c) increasing the play value of the playground with new and improved equipment; and

(d) moving the playground away from Alfred Crescent and removing the need for a perimeter fence.

10. There is $400,000 allocated in the 2017/18 budget to upgrade the northern playground in Edinburgh Gardens.

External Consultation

11. Consultation and engagement with park users and the local community on the two proposals, (the draft Northern Precinct Plan and the draft Playground Upgrade Concept Plan) ran from 2 to 28 November 2017.

12. The engagement methodology included:

(a) Newsletter mail out to local residents, business' and schools;

(b) Signs placed in the park, in various locations around the northern precinct;

(c) Your Say Yarra project page;

(d) North Fitzroy Primary School and Fitzroy High School (as regular users of this section of the park) were invited to provide feedback;

(e) Participants from the previous consultation in 2016 were contacted directly via email and invited to provide feedback;

(f) Social media posts via Council's Facebook page, to inform Yarra's online community of the consultation; and

(g) A drop-in consultation session held in Edinburgh Gardens between 10am-11:30am on Saturday 18 November to discuss the project with park users and the local community and to gather feedback.

13. Community responses were received in the following ways:
    (a) 65 via Council's Your Say Yarra Page;
    (b) 5 responded directly to Officers via email; and
    (c) Approximately 20 people attended Council's on site meeting with 14 providing written feedback on the day.
14. Participant were asked the following questions in relation to the two proposals:
    (a) What level of support they had for the projects– high, moderate or unsupportive?
    (b) What elements of the plans they supported?
    (c) What elements of the plans they wanted to change and how they would improve it?
15. Feedback received during the consultation period is presented in **Attachment 2**, and summarised below.

Draft North Precinct Plan Analysis

16. The level of support for the draft precinct plan was:
    (a) 43% Highly supportive;
    (b) 45% Moderately Supportive;
    (c) 6% Unsupportive; and
    (d) 6% did not provide a response.
17. The main elements supported from the draft precinct plan were:
    (a) playground relocation and upgrade (29 comments);
    (b) possible skate park expansion (23); and
    (c) BBQ relocation and upgrade (15).
18. The main elements not supported from the draft precinct plan were:
    (a) possible skate park expansion (4 comments);
    (b) moving the table tennis table (3);
    (c) relocating the playground (2); and
    (d) doing any work at all (2).
19. The items suggested to improve the draft precinct plan included:
    (a) additional toilets (12 comments);
    (b) improving the façade and treatment of the Emely Baker building (6); and
    (c) additional drinking fountains (5).

Draft Playground Concept Plan Analysis

20. The level of support for the draft concept plan was:
    (a) 55% highly supportive;
    (b) 27% moderately supportive;
    (c) 12% unsupportive; and
    (d) 6% did not respond.
21. The main elements supported from the draft concept plan were:
    (a) choice and variety of equipment (12 comments);
    (b) like it in general (7); and
    (c) having more seats (3).

22. The main elements not supported from the draft concept plan were:
    (a) proximity to the proposed skate park (5 comments); and
    (b) fence removal from the playground (2).
23. Items suggested to improve the draft concept plan included:
    (a) shade provision (11 comments);
    (b) more nature play features (4); and
    (c) more tree planting (4).
24. The Department of Environment Land Water and Planning (DELWP) as representatives of the Crown and Registered Proprietors of Edinburgh Gardens, have been informed of the proposals.

Internal Consultation (One Yarra)

25. Internal consultation was undertaken with the following areas:
    (a) Advocacy and Engagement to work through a plan for community engagement on this project;
    (b) Council's Heritage Advisor has assessed the proposals and provided a report, included as **Attachment 3**;
    (c) Each of the departments within Recreation and Open Space Branch were involved in the development of the plans; and
    (d) Youth Services were approached to assist in encouraging a younger demographic to have their say on the proposals.

Financial Implications

26. The City of Yarra 2017/18 capital works budget includes $400,000 for works associated with the design and construction of a new playground in the north of Edinburgh Gardens.
27. Projects identified in the final Edinburgh Gardens Northern Precinct Plan would be subject to future annual capital works bid processes.

Economic Implications

28. There are no economic implications associated with this report.

Sustainability Implications

29. The proposal includes new trees be planted and this would provide some carbon sequestration benefits and to that extent contribute to a reduction in the urban heat island effect.
30. Recycled and sustainable materials are proposed to be used for the playground works where possible and the existing playground equipment would be recycled and/or repurposed where possible.
31. Other items associated with the playground are proposed to be removed and recycled where possible or disposed of in an environmentally responsible way.

Social Implications

32. Providing more open space areas and improved facilities will enhance the social and community values by providing more opportunities for people to gather and interact.
33. The proposed infrastructure in the final Edinburgh Gardens Northern Precinct Plan considers multiple stakeholders and user groups and proposes ways in which to cater for them without compromising the existing function or character of the park.

Human Rights Implications

34. There are no known human rights implications associated with this project.

Communications with CALD Communities Implications

35. Consultation materials included a translation panel containing advisory text in eight languages: Vietnamese, Greek, Mandarin, Cantonese, Italian, Turkish, Arabic and Spanish.

Council Plan, Strategy and Policy Implications

36. The draft Edinburgh Gardens North Precinct Plan and the draft Playground Upgrade Concept Plan are supported by the following:

    (a) Council Plan 2017-2021: A Healthy Yarra:

        Strategy 1.1 - *"Maintain and enhance Yarra's open space network to meet the diverse range of community uses*".

    (b) Urban Forest Strategy (2017):

        Target – "*Canopy cover in Yarra will increase by 25% (from 2014 levels) by 2040"*

    (c) Draft Edinburgh Gardens Master Plan (2004):

        Objective 6.2.2 - Activities and Recreation

        Objective 6.2.3 – Infrastructure and Built Fabric

        Objective 6.2.4 - Landscape

    (d) Draft Edinburgh Gardens Conservation Management Plan (2004)

        6.4 - General Conservation Policies, including Use and Public Access; New Buildings and Elements; Skate Park; and, Playgrounds

Legal Implications

37. Edinburgh Gardens is covered by a Heritage Overlay (HO213), accordingly a town planning permit would be required for demolition.

38. Subject to consideration of the Plans by Council, officers will confirm the need for a planning permit with Statutory Planning and follow the necessary process as required by the Yarra Planning Scheme.

Other Issues

39. Both the draft Edinburgh Gardens North Precinct Plan and the draft Playground Upgrade Concept Plan have been amended in response to feedback received from park users and the local community during the consultation and engagement period.

40. The final Edinburgh Gardens North Precinct Plan (**Attachment 4**) proposes:

    (a) maintaining the general layout proposed for the northern precinct, including relocation of the playground and BBQ, the addition of an 'active precinct' and expanding the existing skate and BMX park;

    (b) stronger separation between the proposed playground and proposed skate/BMX expansion by means of a natural buffer (including mounding and tree planting); and

    (c) additional tree planting in the northern precinct.

41. The final Playground Upgrade Concept Plan (**Attachment 5**) proposes:

    (a) shade sails; and

    (b) increased planting of shade trees.

42. The plan does not propose additional built toilets in the northern precinct. In periods of low use, the Gardens is well serviced for public toilets and until such time as additional toilets are provided in the south of the Gardens, portable toilets are utilised to manage the increased level of demand and use in the Gardens over summer months.

43. The details of other park infrastructure requested (e.g. seats and drinking fountains) will be incorporated during the detailed design phase, when budgets become available to implement future projects in the precinct.

Options

There are two options for consideration by Council:

44. Option 1 - Endorse the final Edinburgh Gardens Northern Precinct Plan and authorise Officers to proceed to the detailed design phase of the playground upgrade project, in the new playground location.

    Pros

    (a) 88% of responses supported the Draft precinct plan;

    (b) The proposed playground in a new location was the feature generating most support from residents, followed by the expanded skate facility;

    (c) Funding is available in the 17/18 Council Budget to replace the existing playground; and

    (d) If supported, the skate facility can be considered as part of future budget allocations.

    Cons

    (a) A small number of respondents did not support the plan or elements contained within the plan.

45. Option 2 – Direct officers to review the final Edinburgh Gardens Northern Precinct Plan and present an updated plan to Council for consideration at a future date.

    Pros

    (a) Addresses the concerns of a small number of respondents that did not support the plan or elements contained within the plan.

    Cons

    (a) Does not respond to the vast majority of responses that supported the Draft precinct plan; and

    (b) Does not utilise the funding allocated for playground renewal in the 2017/18 budget.

Conclusion

46. There was strong support for the proposals incorporated within the Edinburgh Gardens Northern Precinct Plan, in particular, the playground relocation and upgrade, possible skate park expansion and BBQ relocation and upgrade.

47. There was strong support for the proposed playground upgrade, in particular the choice and variety of equipment, and additional seating.

48. The final Edinburgh Gardens Northern Precinct Plan and the final Edinburgh Gardens Playground Concept Plan incorporate minor modifications in response to feedback received during the community consultation and engagement phase.

49. With the modifications outlined it is considered that both the Edinburgh Gardens North Precinct Plan and the Playground Upgrade Concept Plan are appropriate to be endorsed.

50. If endorsed, officers would then prepare detailed design and apply for appropriate permits.

RECOMMENDATION

1. That Council:

   (a) notes the officer report in relation to the Edinburgh Gardens North Precinct and playground update;

   (b) notes and thanks the community for their contribution to the discussion on the proposed developments for both the northern precinct and the playground upgrade;

   (c) endorses the final Edinburgh Gardens Northern Precinct Plan as shown in **Attachment 4**;

   (d) endorses the Edinburgh Gardens Northern Precinct Playground Upgrade Plan as shown in **Attachment 5**; and

   (e) authorises officers to commence detailed design for the playground upgrade works for the playground and apply for any permits required.

CONTACT OFFICER: **Julia Mardjuki**
TITLE: **Open Space Planner**
TEL: **9205 1427**

Attachments

1 Edinburgh Gardens north precinct - community engagement flyer
2 Edinburgh Gardens north precinct - feedback received during consultation period
3 Heritage Assessment Edinburgh Gardens - north precinct proposals
4 Final Edinburgh Gardens Northern Precinct Plan
5 Edinburgh Gardens Northern Precinct Playground Upgrade Plan

Attachment 1 - Edinburgh Gardens north precinct - community engagement flyer

YOUR SAY YARRA

CITY OF YARRA

2016	Preliminary community consultation.
2017	International House demolished, tenants relocated to Bargoonga Nganjin, North Fitzroy Library.
	Emely Baker Building nominated to become a community space.
	Community consultation on draft plans (current).
	Draft plans updated following community feedback.
	Draft Northern Precinct Plan presented to Council for adoption.
2018	Playground Concept Plan finalised.
	Playground construction begins.
2018 and beyond	Future key proposals contained in the Northern Precinct Plan are implemented (subject to future budget and planning processes).

Previous community feedback

In 2016 we sought feedback from park users and other members of the local community on various aspects of the northern precinct.

The draft plans presented in this flyer were informed by the key themes that emerged during this consultation process.

Key community requests included:

Park infrastructure

- Additional green space in the park.
- Expansion of the skate and BMX park.
- Improved BBQ and picnic area.
- Basketball half court.
- Chess tables.
- Improvements to cycling/pedestrian paths.

Playground

- More nature play opportunities.
- Continues to cater to younger age groups.
- More shade.

Buildings

- Demolish International House building and the carpark (now completed).
- Convert the Emely Baker building to a community space (underway).

For information in your language, please call 9280 1940 and quote the **REF** number below. | 欲知粵語版本的資訊，請致電9280 1932並親上下列**REF**號碼。| Για πληροφορίες στα Ελληνικά, παρακαλούμε καλέστε το 9280 1934 και αναφέρετε τον αριθμό **REF** παρακάτω. | Per informazioni in italiano siete pregati di chiamare il numero 9280 1931 e di citare il numero di riferimento (**REF** number) sottoindicato. | 欲知普通话版本的信息，请致电9280 1937并报上下列**REF**号码。| Para información en castellano, llame al 9280 1935 y cite el número de **REF**. De más adelante. | Để biết thông tin bằng tiếng việt, xin hãy gọi số 9280 1939 và nói số **REF** dưới đây. | للمعلومات باللغة العربية، يُرجى الاتصال هاتفيا بالرقم 9280 1930 والاشارة الى رقم المرجع **REF** المذكور ادناه

REF 17336

YOUR SAY YARRA

CITY OF YARRA

Enhancing Edinburgh Gardens: Northern Precinct and Playground

Help us shape North Fitzroy's largest community space.

After consulting with park users in 2016, we have developed two draft plans.

- The Draft Northern Precinct Plan is a high-level plan to guide future works in the northern area of the park.
- The Draft Playground Concept Plan is a more detailed plan for an improved playground within the northern area of the park.

We are now seeking your feedback on the draft plans. Your contributions will help ensure that Edinburgh Gardens remains a great place to meet, play and relax.

The following pages provide an overview of the draft proposals.

HAVE YOUR SAY

Online
Visit **www.yoursayyarra.com.au/eg-north** to learn more about the draft plans and to submit your feedback. Registered users will receive email updates on the project.

In person
Join us on **Saturday 18 November 2017** in the BBQ area of Edinburgh Gardens north. Drop in any time from 10–11.30am to ask questions and provide feedback.

Email
info@yarracity.vic.gov.au
(subject line: *Enhancing Edinburgh Gardens*)

Mail
PO Box 168, Richmond VIC 3121

If you have any questions about the draft plans, contact our Open Space team on **9205 5555.**

Consultation closes 5pm, Tuesday 28 November 2017.

What sort of feedback are we looking for?

- In general, do you support the Draft Northern Precinct Plan?
- In general, do you support the Draft Playground Concept Plan?
- What elements of the two drafts do you like most?
- What elements of the two drafts would like to change and how would you improve them?

NEXT STEPS

Feedback received will inform updates to the draft plans.The Draft Northern Precinct Plan will be presented to Council for adoption and the Draft Playground Concept Plan will then be finalised by officers.

All proposals are subject to future budget and planning processes, with the exception of playground improvements which are funded in the 2017/18 capital works budget.

Attachment 1 - Edinburgh Gardens north precinct - community engagement flyer

YOUR SAY YARRA

CITY OF YARRA

Draft Northern Precinct Plan

This is a high-level plan to guide future works in the northern area of the park.

Key features of the Draft Northern Precinct Plan

- The playground has been moved further south and integrated into the natural setting of the park, clear of the tree protection zones of the existing trees.
- The BBQ area has been relocated to the north of the playground and will provide more tables and incorporate game elements such as chess.
- An activity precinct which will include the relocated table tennis table, a new basketball half-court and expanded skate/BMX facility has been nominated.
- Redundant fencing (no longer required following the removal of the carpark and demolition of International House) will be removed.
- Increased open space for passive recreation opportunities.
- More tree planting.

All proposals are subject to future budget and planning processes, with the exception of playground improvements which are funded in the 2017/18 capital works budget.

Legend

1 Old playground area (to be removed)
2 Proposed new BBQ area
3 Old table tennis table (to be relocated)
4 Proposed new playground area
5 Possible expanded skate and BMX area
6 Existing skate and BMX area
7 Possible new active play area (half court basketball and table tennis)
8 Public toilets
9 Emely Baker building

---- Existing infastruture (to be removed)
···· Tree protection zone
Existing tree

Draft Playground Concept Plan

The Draft Playground Concept Plan is a detailed plan for an upgraded playground within the northern precinct.

The proposed works are funded in the 2017/18 capital works budget.

Key features of the Draft Playground Concept Plan

- New location away from Alfred Crescent and clear of existing mature trees.
- More integration with natural play elements.
- New equipment with more play opportunities.
- Retained focus on equipment for younger age groups (to complement the play opportunities offered in the southern playground).
- New picnic and BBQ area.

A Rockers

B Multi-play see saw

C Activity unity

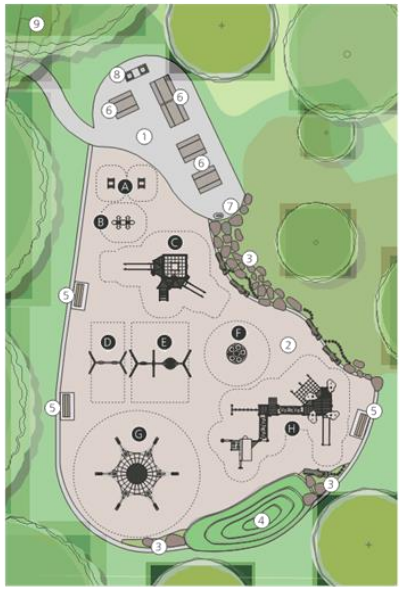
D Toddler swing

E Multi swing with basket swing

F Spinner

G Trampoline

H Activity unit



1 New BBQ area
2 New playground
3 Nature play areas
4 Grassed mound
5 New seats
6 New picnic tables with chess/game designs incorporated
7 Drinking fountain
8 BBQ
9 Bins

Existing tree
New tree
Soft fall mulch
Paving
Grass
Nature play areas
Grass mound
Safety boundary

Attachment 2 - Edinburgh Gardens north precinct - feedback received during consultation period

Attachment 2 - Edinburgh Gardens - northern precinct proposals - feedback received during consultation period

Community engagement November 2017 - verbatim 'Your Say Yarra' responses

No.	Date of contribution	Survey Response								
		postcode?	connection to Edinburgh Gardens?	In general, how much do you support the Draft Northern Precinct Plan?	What elements of the Draft Northern Precinct Plan do you like most?	What elements of the Draft Northern Precinct Plan would you like to change and how would you improve it?	In general, how much do you support the Draft Playground Concept Plan?	What elements of the Draft Playground Concept Plan do you like most?	What elements of the Draft Playground Concept Plan would you like to change and how would you improve it?	Are you responsible for children and, if so, how often do they visit the existing playground?
1	Nov 02 17 05:10:59 pm	3068	I live nearby	Highly supportive	Expansion of the heavily used bmx and skate facility is critical. The existing ramps are significantly over-crowded, and need lighting for evening use, similar to the basketball/tennis/football courts in the park.	Provide lighting for the skate/ Bmx park to be lit until midnight in summer, 10pm in winter	Unsupportive	none	The existing playground is perfectly acceptable. I accompanied my 3 year old nieces there recently and they had a great time. The existing play equipment is aesthetically pleasing and has a retro 1990s appeal. Upgrading this playground is unnecessary and a waste of council rate payers money which could be directed towards expanding the desperately overcrowded skate and bmx ramp.	Don't have children
2	Nov 02 17 08:54:46 pm	3068	I live nearby	Moderately supportive	The extension to the skate park if it includes an area for younger children to scoot (most try in the larger skate bowl which isn't safe) An extra basketball court	Actually really enjoy the current playground and don't see a lot of change with the new one other than an extra set of swings (which is a good idea) - could be cheaper just to install another set of swings though :) Given the findings from the Australian Child Health Poll regarding the safety of trampolines, I'm not sure this is a good addition to the playground - would prefer safety be a top priority of playground planning	Moderately supportive	Nature play areas Extra set of swings 2 activity centres - must have monkey bars (which current plan does) as the local kids are nuts about monkey bars courtesy of a school program run in the local primary across the road.	Australian Child Health Poll suggests trampolines are not safe for children and advises against them. I can't tell from the design of this but I would prefer this not to be in the park as it will be very hard to stop multiple children from jumping on at the same time (the poll found that most injuries occurred when multiple children used trampolines at the same time)	Have children – regularly visit the playground
3	Nov 03 17 10:12:18 am	3054	I live nearby	Moderately supportive	Chess	More tree planting	Highly supportive	Diversity of ages	Remove spinner. Traditional monkey bars would be good.	Have children – regularly visit the playground
4	Nov 04 17 10:09:19 am	3068	I live nearby	Moderately supportive	I like the proposal of a new bbq area, extension of skate park and addition of half basketball courts	I think the playground is not well positioned as it is lacks natural shade	Unsupportive	Not applicable	I think the playground requires some more thought. It appears to be unfenced and has no shade - natural or otherwise. I am a mother of twins and the old playground, being fenced meant that I could take my twins there as babies/toddlers which reduced my risks of isolation and therefore postnatal depression. I think it is important to uphold public spaces which enable mothers (parents) to meet other families, while feeling their children are safely enclosed. I also note the playground does not appear to have any shade, increasing the likelihood of sunburn and heatstroke. The other playground in Edinburgh Gardens also lacks natural and artificial shade and is therefore unused during the hot part of the day in summer-time. It would be better for the playground designers to consider ways to introduce shade if this was the position of the new playground, as the existing playground in the north side of the park has ample natural shade. It would be a great shame for future young families in this area, if this feature was overlooked.	Have children – regularly visit the playground

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5	Nov 04 17 05:56:27 pm	3068	I live nearby	Moderately supportive	Moving and improving the playground, removing the old building next to skate park	I would love to see a cafe! This would generate a great deal of interest I believe and give the park a European feel. If it was within eyesight of the childrens play area it gives parents somewhere to sit and enjoy the park even more. I do believe this would improve the ambiance and also attract extra visitors and locals who may not normally use the park, a lovely place to sit back and enjoy the surrounds, as well as hopefully giving a small business opportunity, for inspiration look no further than the small cafes in Lisbon parks eg. https://www.google.com.au/maps/place/Clara+Clara+Caf%C3%A9/@38.715928,-9.123737,3a,75y,90t/data=!3m8!1e2!3m6!1sAF1QipMwEl9d9gkP7OMZxQEDTuPxfSUEmKfv97yHQxfU!2e10!3e12!6shttps:%2F%2Flh5.googleusercontent.com%2Fp%2FAF1QipMwEl9d9gkP7OMZxQEDTuPxfSUEmKfv97yHQxfU%3Dw129-h86-k-no!7i1280!8i853!4m5!3m4!1s0xd19338aca6ccef7:0xb45e856d383dab2d!8m2!3d38.7159279!4d-9.1237371	Highly supportive	A good chance to improve the older playground and move it away from potentially dangerous mature gum trees	Cafe! Question below needs another category, my children are teenagers, did use to visit playgrounds often when younger	Have children – rarely or never visit the playground
6	Nov 07 17 09:23:30 pm	3068	I live nearby	Moderately supportive	Placement of playground, bbq areas, skate park etc.	Removal of Emely Baker building. Toilets without graffiti on it.	Highly supportive	Variety of activities	Some type of rock climbing activity, more natural play features such as big borders to climb on	Have children – regularly visit the playground
7	Nov 09 17 07:20:00 pm	3068	I live nearby	Moderately supportive	The upgraded playground.	I would like to see some form of fitness equipment put in. They seem to be available in most councils around Brisbane and Sydney and even many Melbourne councils but not Yarra.	Moderately supportive	Upgraded playground.	Too similar to south end. How about something more inspired akin to the sculpture playground in the Hakone Sculpture Park in Japan https://www.tripadvisor.com.au/LocationPhotoDirectLink-g298171-d320696-i42709373-	Have children – regularly visit the playground
8	Nov 10 17 10:36:28 pm	3068	I live nearby, I visit from time-to-time, I commute through the park	Unsupportive	Removal of old playground and old facilities	The current plans are just updates of outdated, conventional ideas on play in public open space. I would scrap virtually all of them and undertake a much more considered, thoughtful approach that actually achieves the objectives of a modern strategy like the recent "Playground Strategies" published by local governments. E.g. http://www.greaterdandenong.com/document/25762/playground-strategy For example: - Unstructured, imaginative play spaces and equipment, not conventional single-purpose eqipment that can be found everywhere else (rockers, see-saws, swings, spinners, etc) - The area should be designed for maximum use in all seasons with overhead cover of at least part of each space (picnic, playground) that is waterproof and also doubles as shade protection. The natural ground should also be designed to be quick drying and interspersed with artificial surfaces that are very fast drying so the area can be used in winter	Unsupportive	None	- Play equipment should be unstructured and imaginative not single-purpose equipment that exists in all other conventional playgrounds. For example, a sandpit should be a central piece of a playground of this budget which can: - be integrated with simple, unstructured water play equipment - form a soft landing area for multi-purpose sitting/balancing/climbing equipment like connected planks (see images below) - connect with nature play including rocks, wooden totem poles, a mound, natural alcoves, native plants, etc - be a place that the community can leave toys for everyone to use (this is common practice in pocket parks and can be extended) - Play equipment should be active (e.g. rope nets to climb) not passive (swings, see saws, spinners) - Most of the play equipment and area should be useful in all seasons not just when the weather is dry - The play equipment should be flexible	Have children – regularly visit the playground
9	Nov 10 17 11:01:11 pm	3054	I live nearby	Highly supportive	Picnic area	Nil	Highly supportive	Trampoline and more natural play	Nil	Have children – rarely or never visit the playground

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10	Nov 11 17 10:55:58 am	3068	I live nearby	Highly supportive	The basketball half court and increased landscaping.	It would be great to see Council integrate more water sensitive urban design by virtue of wetlands or rain water gardens to promote biodiversity and make the flora and fauna an attraction. In addition, low scale vegetation along the border of Alfred crescent should be considered to provide a transition into the space. A recent successful example which comprises landscaping on the periphery is in Langdon Reserve, Fitzroy North. The basketball court should be located at the entry of Alfred Crescent as it will activate the streetscape and entice people to the park. Again, Langdon Reserve does this well as well as RMIT University basketball courts. It would be nice to see the pavement treated in different colours or patterns. It is great to see Council proactively improving public space for the community to enjoy.	Highly supportive	Playground	The playground could be constructed completely of recycled materials and also be fully constructed in natural elements such as wood to integrate better with the features of the park such as the trunks of the trees. A larger more expansive activity unit would benefit the younger user groups. Water sensitive design could be integrated to be used in summer, where the playground becomes a water park in itself.	Have children – regularly visit the playground
11	Nov 11 17 03:22:12 pm	3068	I live nearby, I work or study nearby, I commute through the park	Highly supportive	Moving and updating playground and barbecue area	Emely Baker is very unattractive building and closed of that park of gardens. If it must be kept, hopefully refurbishment will open it up. Most importantly, I would LOVE a European style cafe serving basics - coffee, toasties & sparkling wine. This could be leased from Yarra and provide revenue. Maybe it could be incorporated as part of the Emely Baker refurbishment	Highly supportive	New equipment	Nothing	Have children – rarely or never visit the playground
12	Nov 11 17 03:34:05 pm	3068	I live nearby	Highly supportive	Expanding play ground, moving playground away from Alfred Crescent and mature trees	I would like to a shade solution for the playground for both sunny and rainy days (small kids still need to burn off energy in the winter)	Highly supportive	Nature play areas	Shade solution for playground	Have children – regularly visit the playground
13	Nov 12 17 11:13:42 am	3068	I live nearby	Moderately supportive	Removal of building	The park as a whole needs more permanent bins and toilets	Moderately supportive	Simple	Less equipment	Don't have children
14	Nov 13 17 11:53:23 am	3068	I live nearby	Moderately supportive	Seems to be in keeping with current uses of the park.	I wonder if you have considered where to put the parents? At the playground at the southern end of the Gardens, large family groups picnic on the lawn on the periphery of the playground. Presumbly so they can keep an eye on the kids. They picnic in the leash-free dog area, which, as a dog-owner, is mildly annoying because the families get irritated when dogs frolic near their food and children. I assume that when this new playground is built, the parents will also want to picnic on its periphery. So you'll need to think about where these large family groups will go. The family groups are unlikely to want to sit near the swearin', drinkin', rappin' bad boys of the skate bowl. And they also might not want their precious kiddies near the bad language either. So the playground might not be best situated right next to the skate bowl.	Highly supportive	I don't know. I don't have kids	I don't know. I don't have kids	Don't have children
15	Nov 13 17 11:02:20 pm	3068	I live nearby, Other	Moderately supportive	Everything other than the proposed expansion of the skate and BMX area.	Strongly disagree with the proposal to expand skate and BMX area.	Highly supportive	I like it all.	No proposed changes.	Don't have children

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16	Nov 14 17 11:13:55 am	6065	I live nearby, I visit from time-to-time, I commute through the park	Moderately supportive	New trees and more open space	Have seen no plan for improved or altered paths to connect the new elements, add focal points, add a wow factor. Other parks in Melb have a draw card, something that attracts, like water feature, sculpture etc, this current plan adds no visual atttaction other than more open space.	Unsupportive	The grass mound.	Many play areas are now being designed to be visually appealing, mixing in shapes, colours, natural elements. Sadly the proposed play just looks like standard individual elements in a space. Lack of overall visual appeal, especialy for adults. nothing different or unique.	Have children – rarely or never visit the playground
17	Nov 15 17 05:53:45 pm	3068	I live nearby	Moderately supportive	Generally most of it	The plan needs to include more public toilets, particularly at the southern end of the park where there are only single M and F toilets - though the northern end still needs more	Highly supportive	All of it	More toilets!!!!!	Don't have children
18	Nov 15 17 06:00:42 pm	3065	I live nearby	Moderately supportive	disnt read it. i just want more toilets. how cant u see that isnt a priortity?	you desperately need more toilets. the extra 2(!) placed in the newish building nr the cricket/park is not even close to being enough. my 5 year old had to wee on a tree because he couldnt queue that long. disgraceful.	Moderately supportive	again. i am only here to moan about ur lack of toilet facilities	need more toilets! still!	Have children – regularly visit the playground
19	Nov 15 17 11:03:50 pm	3065	I live nearby, I work or study nearby, I commute through the park	Moderately supportive	Updated equipment to play on for the new generation of children	More trees to create shade from the sun	Highly supportive	The safety boundaries	Children love to make up little performances for their parents and friends so have a small stage for them to use would be nice	Have children – occasionally visit the playground
20	Nov 16 17 08:15:32 am	3053	I work or study nearby, I visit from time-to-time	Highly supportive	extending the bmx/ skate park	Give the basket ball courts nets and a fence around it to keep the balls inside the court	Highly supportive	Basket Swing and Trampoline	add another multi swing with basket	Have children – occasionally visit the playground
21	Nov 16 17 08:32:25 am	3068	I live nearby	Moderately supportive	I like that the BBQ area is next to the playground, and away from the basketballs and BMX/skate park. I think if it was next to the skate park, at certain times of the week, it could be dominated by them, which may intimidate or exclude families and others.	I would prefer that the skate park is not next to the playground. We have found drugs in the current skate park. The skaters at times also drink and smoke and play loud music with swear words. I like skaters and I think they should be free to do as they wish. I'm not sure that it is safe to expose children to their non-skating activities from a young age. Likewise, I'm not sure that basketballers want to be downwind of smokers. I like that the current playground is well shaded by large trees and more suitable for younger children. Older children tend to go to the playground at the southern end of the park., which can get busy and unsafe for younger children. In the new plan, the playground doesn't look like it has any shade, which is a shame.	Moderately supportive	Interesting range of equipment	To incorporate a separate area for younger children, who are learning to crawl and walk.	Have children – regularly visit the playground
22	Nov 16 17 08:54:15 am	3067	I work or study nearby	Moderately supportive	idk, make more basket ball courts pls	Um more basketball courts	Moderately supportive	idc	idk	Don't have children
23	Nov 16 17 09:01:49 am	3070	I live nearby	Moderately supportive	The open playground sounds good and more seating will be valuable as Edinburgh gardens is a place where a lot of people go to.	Maybe another basketball court as the basketball court is always stacked with people so another court would make it easier to play.	Highly supportive	Everything	Nothing	Don't have children
24	Nov 16 17 09:08:03 am	3754	I work or study nearby	Moderately supportive	tree protection zone	keep the playground	Highly supportive	the trampoline	change nothing	Don't have children

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25	Nov 16 17 09:35:36 am	3070	I work or study nearby	Highly supportive	I like the abundance of new trees and the effort put towards community sports space.	Make the skate/BMX area more appealing. Decorate the bike and footpaths. Use as much natural play equipment as possible (such as mounds, mini creeks, etc) and add more trees and plants.	Moderately supportive	The new trees and interesting play equipment.	It's a bit tacky. I would like more wholesome materials such as wood and concrete. Wood is always nice and much more appealing than steel and plastic.	Don't have children
26	Nov 16 17 10:33:30 am	3065	I live nearby, I work or study nearby, I commute through the park	Moderately supportive	new playground	i would like more play ground area	Moderately supportive	tramp	It needs a really big slide and a tree house	Have children – regularly visit the playground
27	Nov 16 17 11:23:18 am	3066	I live nearby	Unsupportive	Planting of additional trees, although not much information on this element.	Many playgrounds around Australia are being designed to look good to everyone no just children, even the new Atherton garden play equipment resembles giant metal flowers with solar panels on. Others I've seen use shad cloth to make sculptural designs, or molde the surrounding landscape to creat patterns. The current design sadly just looks like a lot of stuff crammed into a corner.	Unsupportive	The tramploe and raked hill are different, and may draw people, every thing else is very standard.	As mentioned, making the overall plan look better, use something to create an overal design, be it shade cloth, or a design on the ground, sculptures etc. So many other playgrounds look more cohesive and less like a jumble of basic equipment.	Don't have children
28	Nov 16 17 04:54:16 pm	3068	I live nearby	Highly supportive	new facilities bbq area	the new playground area is too large	Unsupportive	none	no comment	Don't have children
29	Nov 18 17 12:18:06 pm	3068	I live nearby	Moderately supportive	It's a good idea to consolidate amenities, but that includes toilets!	Everything is being moved further away from the toilets. As it is, it's not unusual to see skateboarders using trees as urinals. You'll also have young children in the playgrounds for whom this will be a substantial trip. Why not move the toilets further in, or even create a new block? My home fronts onto one of the closest laneways to the gardens. On Cup Day this year, we had substantial numbers of people urinating there. Access to toilets in the Edinburgh Gardens for a variety of reasons seems to be an ongoing problem, and this plan looks set to exacerbate the existing issue.	Moderately supportive	Trampoline	It looks boring and unimaginative!	Have children – occasionally visit the playground
30	Nov 19 17 08:35:00 pm	3068	I live nearby, I visit from time-to-time	Highly supportive	New playground	Nothing really but ensure adequate shade and water drinking fountains (plural)	Highly supportive	Trampoline.	More swings	Have children – rarely or never visit the playground

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31	Nov 19 17 09:48:28 pm	3068	I live nearby	Highly supportive	I like the idea of extending the skatepark and improving the playground facilities. I support moving it further from the road however as a parent of a North Fitzroy school student I hope it's not too far into the middle of the park as I think the proximity to the school and library are great!	While I agree the playgrounds focus on younger children is good I feel that the choice of playground equipment doesn't encourage active and mindful play and feels a little old-fashioned. As a mother of a young child and new to the area I value playgrounds that encourage children to make new friends and play with a wide group of children. In my experience sandpits are great for this as well as water play (I site the playground Kirkdale Reserve on the Merri Creek (close to CERES) as an example of this. I think a trampoline per see is fine but can be exclusive and dangerous. I have seen good examples of inbuilt smaller trampolines into the group which work well for younger kids and are more inclusive. The new Margaret Mahy playground in Christchurch New Zealand has these / it also has water play. Sand play tools (Park Street playground and diggers are also good for kids) basically I love a sandpit and water as I think it just encourages better play and interaction. Small huts also work really well as kids play shops/mummies and daddies etc - a great example of this is a playground in London in Victoria Park in Hackney - surrounding the sandpit were wooden huts so great for	Moderately supportive	Happy with all of it in theory except the trampoline (see my previous page - sorry I filled this out in the wrong order!) except as I have said I don't think the equipment encourages enough active, mindful play that encourages children to be creative and meet and play with kids they don't know.	As I said - sandpits, water play, engineering play (pulleys/diggers etc) Small huts and houses (Princes Park close to Carlton Tennis club next to Ikon park) is another example of these) I can stress enough how they encourage role play and creativity in kids. I used to have a local park in London with a sink and shaped hole and pipe and the hours kids used to spend playing kitchen/restaurants etc I have 2 kids - aged 5 (at Fitzroy Primary) and 2 years and we use this playground practically every day!	Have children – regularly visit the playground
32	Nov 20 17 03:28:08 am	3065	I visit from time-to-time	Moderately supportive	possible expansion of the skate/bmx area and relocating the table tennis table.	not sure, yous do yous	Moderately supportive	i dont mind	nothin	Don't have children
33	Nov 20 17 11:57:20 am	3054	I live nearby, I visit from time-to-time	Highly supportive	The new skate park and basketball court areas. All activities in a similar area.	More public toilets!!!!!!!!!!!!!!!	Highly supportive	The play equipment is modern and fun! Lots of benches for parents to sit on.	The current plan looks great.	Don't have children
34	Nov 20 17 12:03:28 pm	3065	I live nearby	Highly supportive	New play equipment	More public toilets. More BBQ areas	Highly supportive	Large activity units	More trampolines	Have children – regularly visit the playground
35	Nov 20 17 12:08:02 pm	3068	I visit from time-to-time	Moderately supportive	Improved and expanded playground area; facilities for teenagers	location of toilets - too far away from playground area; unclear about location of seats and shade, water fountains -all essential; provision of active fitness facilities; unclear how existing walking and cycling routes are affected - it is important that locals are encouraged to walk or cycle to the park	Moderately supportive	Good range of swing types for toddlers and children; trampoline; 2 different activity units	A water feature would be terrific e.g. like the one on the Merri Trail in East Brunswick - children love it! Ensure there are seats with shade. Shade for the picnic tables. Is there only 1 access point?	Have children – regularly visit the playground
36	Nov 20 17 12:10:42 pm	3073	I visit from time-to-time	Moderately supportive	Mostly I like it	I'm in the park every Sunday for 3-6 hours doing acroyoga. I would like to see slacklining permitted in the park, either as part of the skate park or with its own dedicated area to accommodate public safety.	Highly supportive	The trampoline	Nothing, I like it	Don't have children

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37	Nov 20 17 12:31:14 pm	3068	I live nearby	Highly supportive	The playground and increased green space	The gardens need more public toilets. The eastern side of the Emely Barker building is currently used by patrons to relieve themselves particularly at busy times at the park. Our house faces onto the Emely Barker building and its a little disturbing for me young kids to see people using the building as a toilet from my front yard. Suggestions - build some garden beds / natural structures around the building to prevent people using the corners of the building as a toilet and/or extend the toilet block to put in additional toileting facilities (or build in new toilet facilites at the back of the Emely Barker building that the public can access). Emely Barker building could also do with a significant facelift in line with the rest of the redevelopment of the gardens.	Highly supportive	the trampoline and the inclusion of natural play areas (grass and rocks)	some additional water fountains, and maybe bike / scooter parking for kids	Have children – regularly visit the playground
38	Nov 20 17 12:46:01 pm	3006	I visit from time-to-time	Unsupportive	N/A	Allow slacklining in the park. How can you have a plan which includes so many things, but neglects slacklining? Every weekend there are lines up. People want to be slacklining in the park, and you should consider this.	Moderately supportive	I don't care	Don't care	Don't have children
39	Nov 20 17 12:48:57 pm	3068	I live nearby, I work or study nearby, I visit from time-to-time, I commute through the park	Moderately supportive	Removal of unnecessary fences and new trees	I'm worried about too many trees being removed so I hope heaps more a planted as everyone loves them more than the man made structures of the gardens, it's what generally draws people in as it's not one of those parks with just grass it has some depth to it and shelter from the sun.	Highly supportive	I like the idea of a new BBQ area I think it's great for the community.	I do think the web was originally a good idea in the playground but I worry that having a trampoline in the centre greatly increased major falls despite the soft fall planned to be laid down. Children may jump and then land hard on the web five and under. It concerns me.	Don't have children
40	Nov 20 17 12:50:22 pm	3068	I live nearby	Highly supportive	The nature play areas of the new playground	Including water play into the nature play	Highly supportive	Nature play	Water play	Have children – regularly visit the playground
41	Nov 20 17 01:34:10 pm	3068	I live nearby	Moderately supportive	Chess board	I would like to see outdoor gym equipment added for adults. Many other councils and Yarra doesn't. A vocal minority prevented this in the darling gardens and many residents subsequently see it as a missed opportunity.	Moderately supportive	Variety of equipment	Na	Have children – occasionally visit the playground
42	Nov 20 17 02:10:32 pm	3068	I live nearby, I work or study nearby, I visit from time-to-time, I commute through the park	Moderately supportive	seems thoroughly thought out, a lot of the proposals make a lot of sense.	what is the existing building currently next to the skate park? how old is it? is it protected? wouldn't want to see it go if it had a history behind it	Highly supportive	grass mound as a barrier to the skate park area -	looks good - just amke sure parents can sight kids from all areas so no areas become dead/scary zones	Don't have children

Attachment 2 - Edinburgh Gardens north precinct - feedback received during consultation period

No.	Date of contribution	Survey Response								
		postcode?	connection to Edinburgh Gardens?	In general, how much do you support the Draft Northern Precinct Plan?	What elements of the Draft Northern Precinct Plan do you like most?	What elements of the Draft Northern Precinct Plan would you like to change and how would you improve it?	In general, how much do you support the Draft Playground Concept Plan?	What elements of the Draft Playground Concept Plan do you like most?	What elements of the Draft Playground Concept Plan would you like to change and how would you improve it?	Are you responsible for children and, if so, how often do they visit the existing playground?
43	Nov 20 17 02:28:47 pm	3054	I live nearby, I visit from time-to-time	Highly supportive	More trees, more natural elements, more shade, removal of car park. More passive area.	More creative/imaginative play. The new playground is too formal and high maintenance. Keeping with being more sympathetic to nature and sustainability I would like to see the playground constructed of more natural elements. All the play things proposed create an ongoing burden in the budget far more than natural play elements. Path design that reduces conflict and encourages cooperation between the various user "types" enjoying the park. At the moment long wide paths mean all park users tend to conflict due to off leash dogs, speed, etc	Moderately supportive	It's expanded. It creates a sugnuficant play space. Caters for younger children.	Too structured. Too high maintenance. Too formal. Creative, imaginative, natural surfaces would be preferred and lower maintenance. Not enough thought is given to ongoing maintenance costs for rate payers.	Have children – rarely or never visit the playground
44	Nov 20 17 02:48:22 pm	3068	I live nearby	Highly supportive	Lovely treed areas maintained and extra playgrounds & facilities	Look at using latest permeable paving products in tree protection zones	Highly supportive	New and varied apparatus	Maybe change soft fall area materials to rubber based artificial grass	Have children – occasionally visit the playground
45	Nov 20 17 02:57:07 pm	3068	I live nearby	Moderately supportive	Currently the plan does not excite me too much, either way I'm happy with those aspects of the park.	Highly request a calisthenics set up! There are NONE of n the inner northern suburbs. This would include parallelette bars, various chin up bars etc. there is only ONE in Melbourne (Elwood). It would be a unique and huge advantage to the community including a dedicated physical activity space in Edinburgh Gardens.	Moderately supportive	N/a	N/a	Don't have children
46	Nov 20 17 06:10:15 pm	3068	I live nearby	Moderately supportive	Updated playground	A fenced area to let dogs off in a enclosed area.	Highly supportive	new equipment	none	Don't have children
47	Nov 20 17 07:04:26 pm	3068	I live nearby, I commute through the park	Highly supportive	The ropes with a trampoline in the centre of the roped climbing frame and the large knotted swings. I also like the idea of the more environmental play equipment with natural spaces and plants.	I would improve the Draft Northern Precinct Plan by adding more adventure play equipment and larger spaces to have picnics and climb. I would also introduce some more trees and plants in the dog park for them to discover more smells with. Swanee Mahalia Myles-Malone aged 9.Thank you	Highly supportive	The update and utilising the old maternal heath nurse building	I would love to see a tree house in the plan	Have children – regularly visit the playground
48	Nov 20 17 07:09:52 pm	3068	I live nearby, I visit from time-to-time, I commute through the park	Highly supportive	Proposed new BBQ area, new playground - the space DEFINITELY needs an upgrade to the skate park (expanded also).	It looks fantastic and the proposed plans are highly needed - nothing to change.	Highly supportive	All of it - the existing playground at the northern end of EG is in desperate need of an upgrade / TLC.	no changes / is fantastic as is!	Have children – regularly visit the playground
49	Nov 20 17 08:39:36 pm	3068	I live nearby	Highly supportive	Expanded playground area and BBQ area.	Add in more natural features, such as boulders and logs to create seating and a native planting to create a sort of 'bushland' setting in the park.	Highly supportive	The size of it and the nature-play elements.	It's a minor point, but I'd prefer the playground was in more natural colours- like maybe greens and browns- instead of the yellow panels. I think moe natural colours would blend into the park better.	Have children – regularly visit the playground
50	Nov 20 17 08:46:42 pm	3068	I live nearby	Highly supportive	Expanded BMX and Skate Park. It is one of the most consistently used elements in the park. New BBQ area.	The children's playground seems particularly large. There is already a large and particularly extensive children's playground area. I would reduce this in size.	Unsupportive	The seats. The tables.	I cant see from the attached image but I dont like the idea of the BBQ area being within the children's playground. I have a dog and we use this BBQ area often. If the bbq area is within the fenced playground there wont be a BBQ area that is dog friendly.	Don't have children

Attachment 2 - Edinburgh Gardens north precinct - feedback received during consultation period

No.	Date of contribution	Survey Response								
		postcode?	connection to Edinburgh Gardens?	In general, how much do you support the Draft Northern Precinct Plan?	What elements of the Draft Northern Precinct Plan do you like most?	What elements of the Draft Northern Precinct Plan would you like to change and how would you improve it?	In general, how much do you support the Draft Playground Concept Plan?	What elements of the Draft Playground Concept Plan do you like most?	What elements of the Draft Playground Concept Plan would you like to change and how would you improve it?	Are you responsible for children and, if so, how often do they visit the existing playground?
51	Nov 20 17 09:14:46 pm	3068	I live nearby	Highly supportive	Lovely big park for littler children sounds amazing.	More public toilets - there are always HUGE 30 minute long queues on nice weekends - this is just simply not tolerable for families.	Highly supportive	The trampoline looks cool.	Although this is targeted at the younger kids, I see a lack of play opportunities at ground level for crawlers say aged 8-18 months (i.e. not walking) - like little mounds/bridges to crawl over, things to pull themselves up to standing, activity units that are not too far off the ground, patterned ground (that squishy asphalty stuff)/reflective things on the ground, a sandpit, tunnel etc. etc. My 12 month old wouldn't be able to play independently at this playground.	Have children – regularly visit the playground
52	Nov 20 17 10:04:53 pm	3078	I live nearby	Moderately supportive	Renewal of playground. I like the new bmx area. There is not much for kids 8+, younger kids are well catered for.	The Emily Baker building if being reused needs to face / address the south. Alternatively it should be demolished and returned to public open space. There needs to be a balance between green space and active uses / paved areas. Overall the park needs to be managed well in conjunction with users / citizens. Please allow drumming gatherings in the park.	Moderately supportive	Trampoline	Baby rockers are very rarely used even by babies, they are using a lot of space for these. Please use less plastic material.	Have children – regularly visit the playground
53	Nov 21 17 11:00:15 am	3068	I live nearby	Moderately supportive	I like expanding skate park and getting a new playground, and the new basketball area. Great ideas - but need modifications	There really needs to be a separation between skate area and playground. They draw very different user groups - often skate area has loud music, drinking etc. This is fine as it suits those using the area - but really not great to have that close to a play ground with young kids. Also the BBQ area - good idea to include but again often those using these facilities are drinking, smoking etc - so difficult to deal with when young kids in playgrounds. Just need to be smarter with the layout. Perhaps the basketball court could seperate the skate and playground? Also PLEASE include a netball ring in the basketball area. Netball rings are never included.	Unsupportive	Swings - always highly used.	The main users of current playground are kids under 5, the other playground caters for older kids. This needs to be considered when designing this space. What will ground surface be? Please no bark chips. Will there be shade? This is a must. The elements you have included for playing are not well thought through. A, B, F and G - kids don't use these. They really don't. A trampoline would be great - needs to be in-ground. Investigate other amazing play spaces (look up Malcolm Reserve in Semaphore Park in Adelaide - brilliant play area). If going to build a new play space - how about putting some decent design thought and money into it. This will be such a well used play space - please give us a good one.	Have children – regularly visit the playground
54	Nov 21 17 01:12:15 pm	3066	I live nearby, I visit from time-to-time	Moderately supportive	Active area	Off leash dog area	Moderately supportive	Basket swing	Off leash dog area	Don't have children
55	Nov 21 17 04:53:54 pm	3068	I live nearby	Moderately supportive	A new BBQ area and a new playground area	It would be great if the addition of adult exercise bars/equipment could be added to the park so that adults could use the park for fitness. Why does the table tennis court have to be moved?	Highly supportive	The variety of play equipment is good.	Perhaps the addition of a rope climbing area could be included - something like a web of ropes for children to climb through/over/around.	Don't have children
56	Nov 21 17 06:22:00 pm	3068	I live nearby	Highly supportive	the new playground	consider noise reduction elements especially around the skate & BBQ area. living nearby, these are the areas that seem to get very noisy at night. could they be moved further into the park?	Highly supportive	trampoline	Noise reduction elements, especially around the playground. The children can get noisy, no bad thing but trees of noise reduction screens would be great.	Don't have children

Attachment 2 - Edinburgh Gardens north precinct - feedback received during consultation period

No.	Date of contribution	Survey Response									
		postcode?	connection to Edinburgh Gardens?	In general, how much do you support the Draft Northern Precinct Plan?	What elements of the Draft Northern Precinct Plan do you like most?	What elements of the Draft Northern Precinct Plan would you like to change and how would you improve it?	In general, how much do you support the Draft Playground Concept Plan?	What elements of the Draft Playground Concept Plan do you like most?	What elements of the Draft Playground Concept Plan would you like to change and how would you improve it?	Are you responsible for children and, if so, how often do they visit the existing playground?
57	Nov 21 17 06:38:58 pm	3068	I live nearby, I visit from time-to-time	Moderately supportive	The plan to expand the skate/bmx facility.	Skate/bmx area should be expanded. Currently the bowl is excellent but can only safely accomodate one rider at a time. Often there are queues of 20 people waiting for a turn. It badly needs to be extended to incorporate a 'street' section, which would greatly increase the number of people able to use the facilities at any one time.	Moderately supportive	Not interested in playground plans	Not interested in playground plans	Don't have children
58	Nov 22 17 11:13:21 am	3068	I live nearby, Other	Unsupportive	Expansion of skate park	Seems like a complete waste of money, I like the open space which resulted from removing the building next to the skatepark. I think this proposal will just crowd out that space. I would prefer open space	Unsupportive	None of it.	By not building it.	Don't have children
59	Nov 22 17 11:44:22 am	3058	I visit from time-to-time, I commute through the park	Moderately supportive	Removal of car park.	Keep table tennis table well away from other activities.	Moderately supportive	no comment	no comment	Don't have children
60	Nov 24 17 03:54:10 pm	3068	I live nearby	Highly supportive	Improved playground facilities	Could you include more toddler swings? I have twin granddaughters and have never found a park where there are enough; there are always queues. The multi swing only seems to have one swing - same issue once they get a bit bigger. Toddler swings are the one thing very small children can do. Rockers and see saws never seem popular Can you also get the sort of slides that don't get so hot you can't use them? (I take twins to park every week and consider myself an expert!)	Highly supportive	soft fall mulch is good, better than the rubberised material many new parks use. Also grassed mound sounds good.	Activity units can be worrying, especially once toddlers get over ambitious and climb out of reach. Very stressful, especially with two children. Maybe a few more seats? A lot of grandparents take small children and there are only three seats at the moment. Is the toilet nearby?	Have children – regularly visit the playground
61	Nov 24 17 05:06:09 pm	3068	I live nearby, I visit from time-to-time, I commute through the park, Other	Highly supportive	Expanded BMX & skate area, BBQ area, and.. sigh.. I guess the kids area is good too.	It's okay but if we're expecting more children and families to come, then perhaps more bathrooms would be a good idea. And have them operational until the evening.	Highly supportive	I like that you haven't decided to go with over-designed modern concepts - simply and playful, and safe (of course).	Looks good as long as it doesn't impinge on the BMX/skate area or the fun that is to be had by single, non-children loving people who use the park every weekend, who have chosen not to overpopulate the world and just rock out with mates.	Don't have children
62	Nov 25 17 01:19:41 pm	3068	I live nearby	Moderately supportive	New and larger playground area, new BBQ area and increased open space / tree planting.	Do not proceed with the "possible expanded skate and BMX area" which is proposed directly adjacent to the new playground area. In and around the existing skate park we have often seen often broken glass and people smoking / using offensive language - not appropriate directly adjacent to a young children's playground. We strongly oppose any expansion of the existing skating / BMX area.	Highly supportive	New BBQ's next to playground, more equipment and the nature play areas.	Shade sail over BBQ / table area.	Have children – regularly visit the playground
63	Nov 26 17 09:30:53 pm	3068	I live nearby, I work or study nearby, I visit from time-to-time, I commute through the park	Highly supportive	I like the improvements to the playground, moving it will also create a nice green space to the north along Alfred Cres. It's a good idea to keep it for younger kids (under 5) as the adventure playground to the south is very popular with older kids. I am very supportive of removing the car park. It seems crazy to me that you can drive in and park in the middle of this park. Any expansion to the skate park facility i am very supportive of. It is well used and i enjoy watching the skaters, and my daughter is showing more interest in skating since seeing young girls skate here.	I would reconfigure Emely Baker community building to face inwards (rather than towards Alfred Cres) and have an deck/stage built into the design. This would be attractive to users for hiring the space and would also allow opportunity for formal live music events.	Highly supportive	I like that it is still focused on younger children. I used this a lot when my kid was 1-4, now we mostly go to the adventure playground to the south. I love the idea of the grassed mound.	I've heard that Fitzroy North Primary Schools has recieved funding to create a chess playing area on their grounds. Rather than duplicate across the road in the park, could you have a conversation with them about integration/shared use?	Have children – regularly visit the playground

Attachment 2 - Edinburgh Gardens north precinct - feedback received during consultation period

No.	Date of contribution	Survey Response								
		postcode?	connection to Edinburgh Gardens?	In general, how much do you support the Draft Northern Precinct Plan?	What elements of the Draft Northern Precinct Plan do you like most?	What elements of the Draft Northern Precinct Plan would you like to change and how would you improve it?	In general, how much do you support the Draft Playground Concept Plan?	What elements of the Draft Playground Concept Plan do you like most?	What elements of the Draft Playground Concept Plan would you like to change and how would you improve it?	Are you responsible for children and, if so, how often do they visit the existing playground?
64	Nov 27 17 11:56:40 am	3068	I live nearby	Unsupportive	I would prefer less of our money spent on the park of non residents to enjoy. Better tennis club rooms and football rooms would be great.	We have to many people parking in the streets and littering in the park that we have to pay for. It would be great if the council could focus on traffic management such as speed humps in Scotchmer St, a zebra crossing so our kids are nearly killed by car crossing at the old train line on Scotchmer St. No more BBQ's , the locals don't use. We don't want to have the park as a draw card for day tippers. Please use our rates to support the infrastructure our community for the people who live here.	Moderately supportive	The playground is fine. My kids love the one that's there. The new one is always full of visitors or parties so we use the smaller one	It just needs to basic, I don't need to spend all day at the park. Not everyone had kids at the park. I think we have more desperate areas to spend our money. Such as slowing down the traffics cutting through our suburb so we can walk to school safely	Have children – regularly visit the playground
65	Nov 27 17 09:39:00 pm	3068	I live nearby, I work or study nearby, I visit from time-to-time, I commute through the park, Other	Moderately supportive	Good to see the remnants of old buildings etc removed. Larger playground is good.	The playground is too close to the skateboard area, given the amount of alcohol and dope that is consumed by the skateboarders (I don't have a problem with them doing that, just not closer to young children). There should be more than one drinking fountain in the playground.	Highly supportive	Good mix of different play options	1. More than one drink fountain. 2. Shade cloth areas, especially near the seats.	Have children – regularly visit the playground

Attachment 2 - Edinburgh Gardens north precinct - feedback received during consultation period

Community engagement November 2017 - verbatim pop-up session responses

No.	Date of contribution	Verbal responses recorded at pop-up session by officer (key issues and levels of support verbally confirmed by participants)								
		What is your postcode ?	What is your connection to Edinburgh Gardens?	In general, how much do you support the Draft Northern Precinct Plan?	What elements of the Draft Northern Precinct Plan do you like most?	What elements of the Draft Northern Precinct Plan would you like to change and how would you improve it?	In general, how much do you support the Draft Playground Concept Plan?	What elements of the Draft Playground Concept Plan do you like most?	What elements of the Draft Playground Concept Plan would you like to change and how would you improve it?	Are you responsible for children and, if so, how often do they visit the existing playground?
1	18.11.2017	3068	I live nearby	No response	No response	Activate the old vacant building. Move playground further towards the centre. Placement of BBQs should take wafting of smoke into nearby areas into account.	Highly supportive	No response	No response	Have children - regularly visit the playground
2	18.11.2017	3068	I live nearby	Highly supportive	Skate park	Provide shade for skate park. Restrict car access to entire park.	Highly supportive	No response	More shade for the playground. Make safety a priority.	Don't have children.
3	18.11.2017	3068	I live nearby	Moderately supportive	Basketball halfcourt	Close proximity of playground and skate park may produce tentions - perhaps separate with more landscaping. Do something to discourage long queues at toilets and public urination.	Highly supportive	No response	No response	Have children - oldest uses basketball halfcourt regularly
4	18.11.2017	3065	I live nearby	No response	Happy that skate park will remain easilly observable by surrounding park users. Believes that this will discourage anti-social behaviour.	More toilets. Add wall with painted tennis net to allow people to play tennis against it.	Moderately supportive	Happy to see the large activity units included (H and C) to encourage imaginitve play. Happy to see nearby gum tree retained.	More shade for the playground.	Have children - regularly visit the playground

Attachment 2 - Edinburgh Gardens north precinct - feedback received during consultation period

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5	18.11.2018	3068	I live nearby	Moderately supportive	Expansion of skate park.	Investigate potential for BBQ lids. Retain older elements of the park where possible. Keen for flow/feel of existing skate park to be retained during any expansion (consult with local skaters).	No response	No response	Keep a fence around the playground to keep high speed skate activity separate from young children.	No response
6	18.11.2019	3068	Live nearby and commute through the park	No response	Skate park	Add RC track (remote controlled car track)	No response	No response	No response	No response
7	18.11.2020	3068	Live nearby			Skate park no near playground BBQ area and broken glass away from playground	Moderately supportive	No response	Shade for parts of playground	Have children - regularly visit the playground
8	18.11.2021	3068	I live nearby	Highly supportive	expansion of skate park so younger age groups can use.	Shade for BBQ/picnic	Highly supportive	Trampoline	Double rockers better than smaller. Shade for playground.	Have children - regularly visit the playground
9	18.11.2022	3068	I live nearby I visit commute	Highly supportive	Expanding playground and skate park	No reponse	No response	No resopnse	No response	No response

Attachment 2 - Edinburgh Gardens north precinct - feedback received during consultation period

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10	18.11.2023	3065	I live nearby	Highly supportive	No response	No reponse	Highly supportive	combination of nature and built elements	Fencing or part fence Shade	Have children, occasionally visit playground.
11	18.11.2024	3068	I live nearby I work or study nearby	Highly supportive	Trampoline, spinner, naure play - more please, less fencing	More nature play, natural elements	Highly supportive	All of it		Have children, regularly visit the playground
12	18.11.2025	3068	live, work/study, visit, commute	Highly supportive	Playground upgrade BBQ area and chess board	Incresed BBQ's - 4 burners	Highly supportive	Kids like trampoline and spinner	More drinking taps	Have children - regularly visit the playground
13	18.11.2026	3071	other, skates here	Highly supportive	Extension to skate park	Skate park extension ideas: no street skating area but put in a mini bowl for practicing tricks. (St Kilda has one). 1.2m high, 3m across.	Highly supportive	Make bigger	Nothing	Have children, rarely or never visit the playground (older)
14	18.11.2027	3070	live, visit	Highly supportive	Skate park expansion.	Externd skate park, more bowls (smooth) no street course! Mini ramps, 1.2m high like St Kilda.	Highly supportive	All of it	Bigger kids playground	Have children - occasionally visit the playground

Attachment 2 - Edinburgh Gardens north precinct - feedback received during consultation period

Community engagement November 2017 - verbatim email responses

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Julia,

Thank you for contacting me. In general I think the new design looks good. My only suggestion is that as the skate area frequently attracts a number of spectators there would be merit in either establishing grassy mound where spectators could sit to watch the action, or alternatively a couple of lines of seats along one side.

Regards,

Generally I support the draft northern precinct plan, subject to the comment below.

I don't support the draft playground concept plan, which is boring and unimaginative - children need to explore and develop from finding out, even the swings are anodyne - what is wrong with the adventure playground model, so successful elsewhere (Fitzroy, Atherton Gardens and St Kilda)? At least, introduce some contours, etc.

Two suggestions for the northern precinct plan, based on observations of many years as to use of the Gardens:

1. The grassed expanse along the northern boundary abutting Alfred Crescent is often used for soccer - there is no soccer pitch in the Gardens: this area should be regraded and resurfaced so that it can be used as soccer pitches, not exclusively but at least fit for that use.

2. The area marked 7 for possible half basketball court should have a full sized basketball court - the demand for the court next to the cricket pavilion is often too crowded, impromptu games have to play half a court or players have to wait - given the schools in the area and the popularity of impromptu games there is enough demand to justify a full sized court, plus it is a real local community hallmark.

There is ample space and demand for the suggested improvements, and the additional cost would not be out of proportion to the benefits.

Kind regards

Hi Nicols Ward Councillors

Could you or Yarra Council send me the most recent editions of the Edinburgh Gardens CMP and Master Plan.

Whatever happened to our original cast iron gas lamp standards that have been removed from the Gardens.
I have questioned but received no answer on the following points:-
- how both gas standards were assessed as being in a state of disrepair requiring removal - was this an in-house decision or did a structural engineer certify these heritage cast iron posts were a Public Risk?
- how were they disposed of and is there a paper trail that shows what happened to these heritage items?

Attachment 2 - Edinburgh Gardens north precinct - feedback received during consultation period

- does the Council be replacing them?

With respect to the proposed redevelopment of the northern playground and the area around the skate rink - has Yarra prepared an update of the CMP, the Master Plan and was a Heritage Impact Statement prepared for this proposed playground redevelopment that explains how the proposed works will 'preserve and enhance' the Cultural Significance of the Edinburgh Gardens.

Thanks

I support the draft in general for both the northern precinct and playground plans.

I like the range of activities we are encouraging the community to participate in. The existing skate and BMX area attracts a wide range of ages from toddlers to adults - its great to see this being expanded.
The grassy mound at the southern end of the playground sounds lovely.

I would like to see MORE nature play areas either stretched out around the playground or incorporated in it. It is great to see them included but more space is needed - as they are they will get a lot of wear and tear. Logs to climb, bushes to dodge through, rocks to scramble on. Stretch it out as a garden but make it obvious that it is encouraged for kids to play in the garden beds!!

Kind Regards,

Dear Sir/Madam

Thank you for the opportunity to provide feedback on the proposed upgrades to the northern precinct and playground of Edinburgh Gardens.

Living at the bottom of Fergie Street, we use this area frequently and are directly impacted by the activity in that section of the park.

Whilst we're thrilled about the planned upgrades, we have concerns regarding the proposed extension of the skate park. Our concerns are twofold, being:

- Users of the skate park frequently bring amplified music, and at times temporary lighting fixtures, which result in noise permeating into the surrounding neighbourhood, sometimes late into the night. Our fear is that an increase in the skate facilities will attract more users and more of this behaviour.

- The fundamental purpose of a park is to provide much needed green space to local residents who otherwise lack this in an urban environment. Increasing the skate park will bring more concrete mass to the park at the expense of this beloved green space.

Thank you again for the opportunity to provide feedback on this important upgrade. Please feel free to contact us if you would like any further input or clarification.

Attachment 3 - Heritage Assessment Edinburgh Gardens - north precinct proposals

City of Yarra
Heritage Advice

Application No.: N/A

Address of Property: Edinburgh Gardens

Planner: Carrie Lindsay, Coordinator Open Space Planning and Design

Yarra Planning Scheme References: Clauses 43.01, 21.05 and 22.02.

Heritage Overlay No. HO 213 Brunswick Street and Alfred Crescent North Fitzroy Edinburgh Gardens

Level of significance The Edinburgh Gardens, commenced 1862, is listed as being Individually significant in Appendix 8, *City of Yarra Review of Heritage Overlay Areas 2007* (Revised May, 2017).

The citation in the *Victorian Heritage Database* reads

> The following wording is from the John Patrick Landscape Citation, 1998 for the property. Please note that this is a "Landscape Citation", not a "Statement of Significance". For further information refer to the Landscape Citation held by the City of Yarra Landscape Citation, 1998.
>
> Description
>
> The Edinburgh Gardens is a large park surrounded by residential development. The park contains a wide variety of active recreational facilities, as well as extensive passive recreation areas.
>
> The site was temporarily reserved for public use in 1862, and was initially used for cricket. In 1877 a bowling club was established on the site. The site was permanently reserved as a park in two sections in 1881 and 1883. Following this, development started in earnest. Low areas were filled with garbage and manure, as well as for the disposal of night soil. A number of horses were also buried on the site. Avenues within the park were apparently developed along pedestrian wear lines through the site. A creek that ran through the park was eventually converted into an underground drain. Early features of the park included a fountain and a pond, both of which have been removed. A significant development in the history of the park was the construction of the North Fitzroy rail line in 1888, that effectively cut the park into two distinct sections. The rail line was removed in 1981.
>
> Remnant Fabric (Man Made)
>
> The site contains many built structures; most are associated with active recreation, and are of recent origin and low conservation significance. Of note is the prominent Peace Memorial located on the Brunswick Street frontage, erected in 1925. This Classical Revival rotunda appears to be of brick or concrete construction, finished with a cream render, and the bronze domed roof is supported by eight Doric columns. Asphalt paths throughout the park are edged in basalt. A garden bed located centrally within the park features a large pedestal that was mounted by a wooden statue of Queen Victoria. The statue is no longer in place. A path bisecting

the park in a rough north south alignment follows the alignment of the now defunct railway, with a short section of rails extant near the junction of Fergie Street and Alfred Crescent. [Emphasis added.]

Remnant Fabric (Vegetation)

The site is dominated by mature trees, most notably Elms *(Ulmus procera* and *U. x hollandica)* and Common Oak *(Quercus robur), b*oth of which are used extensively as avenue plantings. A notable row of Kurrajong (*Br achy chiton populneus)* s located along the park street frontage. Other notable specimens include Fastigiate Monterey Cypres *(Cupressus macrocarpa* Fastigiata'), Jacaranda *(Jacaranda mimosifolia)* and Bay Tree *(Laurus nobilis).* A mature Holm Oak (*Quercus ilex) n*ear the Peace Memorial has been recorded on the National Trust of Australia's (Victoria) Significant Tree Register. A Privet *(Ligustrum ovalifolium)* hedge in the southern section of the park relates to the defunct railway line.

Statement of Significance

Edinburgh Gardens are locally significant as a major and early recreational facility within the Fitzroy area. The site is intrinsically linked to a number of local sporting groups and the Fitzroy community generally. It contains elements that illustrate the early use of the site, particularly the railway, and contributes significantly to the heritage character and general amenity of the local neighbourhood, particularly as a setting for adjacent significant housing which culminates in a landscape of notable grandeur within the City of Yarra. The site is also significant for the notable numbers of mature trees, one of which is recorded by the National Trust of Australia (Victoria).

End Statement of Significance

Supplementary Information:

Description

less [Sic.]

2 Bay Trees (Laurus nobilis)- see 1998 description: The site contains many built structures; most are associated with active recreation, and are of recent origin and low conservation significance. Of note is the prominent Peace Memorial located on the Brunswick Street frontage, erected in 1925. This Classical Revival rotunda appears to be of brick or concrete construction, finished with a cream render, and the bronze domed roof is supported by eight Doric columns. Asphalt paths throughout the park are edged in basalt. A garden bed located centrally within the park features a large pedestal that was mounted by a wooden statue of Queen Victoria. The statue is no longer in place. A path bisecting the park in a rough north south alignment follows the alignment of the now defunct railway, with a short section of rails extant near the junction of Fergie Street and Alfred Crescent.'

The Edinburgh Gardens is a large park surrounded by residential development. The park contains a wide variety of active recreational facilities, as well as extensive passive recreation areas. Edinburgh Gardens falls within the North Fitzroy Precinct- see City of Yarra Heritage Review: Heritage Overlay Precincts (1998).

Potential Threats

The wide variety of uses of the park and the various structures associated with these uses has led to a disparate layout of the park. Further introduction of inappropriately detailed facilities would erode its heritage character. Coupled with this is the poor level of maintenance of the plant material. Recent plantings, for example around the central garden bed, are inappropriate to the character of the park.

Management Steps

Edinburgh Gardens require the preparation of a detailed conservation study to more fully identify elements of high conservation significance, and outline policies for the gardens on-going conservation. This would effectively update the report prepared by Landform Australia in 1987. A new master plan should then be prepared that would unify the various disparate elements within the park, following the policies set out in the conservation study, to ensure the parks inherent heritage character is preserved. [Emphasis added.]

It would include guidelines for the management of existing plant material, replacement strategies for senescent trees and introduction of new plant material and detailing of hard landscaping features and structures.

Integrity: Fair

Conservation Management Plan

The following more detailed information has been taken from the *Edinburgh Gardens Conservation Management Plan*.

Skate park

History

The skate park was constructed in 1991, continuing the trend towards the provision of both passive and active recreational facilities in the Gardens.31

Description

Located immediately to the south of the former gardener's residence, the skate park comprises a series of an excavated concrete bowls enclosed by a steel mesh fence.

Significance

While the skate park is popular with the youth of the surrounding community, it is of little or no cultural heritage significance. [Emphasis added.] (p. 62)

Playgrounds

History

Approval for the construction of playground in the northern end of the Gardens, opposite the state school, was granted in 1925.32 Playgrounds were built in many of Melbourne's parks and gardens during this period, largely in response to lobbying

by the Guild of Play. Established in Melbourne in 1912, the Guild of Play advocated the need for supervised play as an essential component of a child's development.

Their beliefs stemmed from an international playgrounds movement formed in the United States and England and active in the late nineteenth and early twentieth century. The movement sought to alleviate some of the social problems experienced by families living in crowded inner cities areas.

The existing playgrounds are of recent origin.

Description

The northern playground is located close to Alfred Crescent, opposite the primary school while the southern playground is located at the southern end of the section of the Gardens used as a sports ground. Both are enclosed by recent steel fences and have Soft-Four [Sic. i.e. fall] ground covering. The play equipment is steel framed in the northern playground and of treated pine log construction in the southern playground.

Significance

The playgrounds are popular with the children and family groups who congregate in the Gardens and use these facilities. It is also of some historical interest that a playground is likely to have existed in the general locale of the northern playground since 1925. While the provision of playground facilities is of local historical interest, the existing fabric is of no cultural heritage significance. [Emphasis added.] (pp. 62-63)

Paths

Description

The path network is essentially utilitarian in nature. Entry points correspond to the surrounding street pattern and almost all paths run in a straight line. Most of the secondary diagonal path system, connecting points of interest within the Gardens, has been removed.

Significance

...the main path network through the Edinburgh Gardens remains essentially unchanged since its creation in the 1880s and 1890s. The principal path structure is of primary significance to layout, but not the majority of materials. [Emphasis added.] (pp. 68 – 69)

The curved paths (see next page) in the North area may be part of the system which was there in 1900. Now, instead of being straight as shown on the MMBW Plan, the two remaining short paths (included in the circle) are curved and the diagonal paths below the circle are no longer extant.

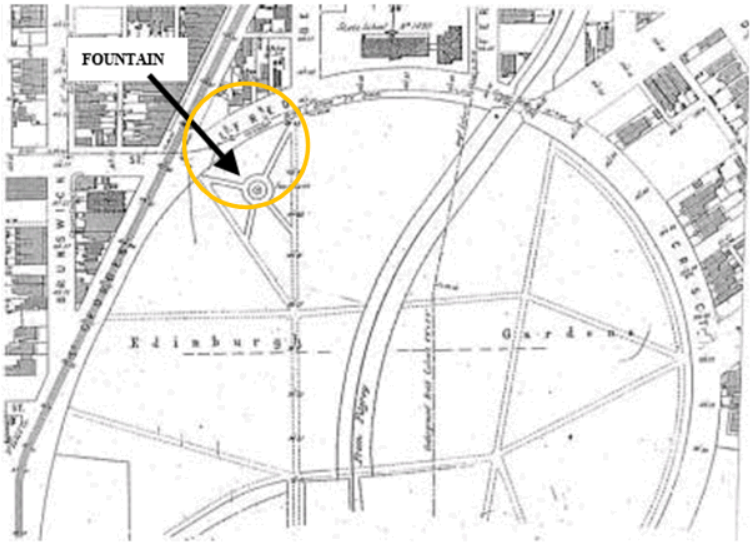


Figure 8 *Portion of MMBW Plan 50 (northern section) c.1900 (Scale: 160 feet to 1 inch)*
Source: State Library Map Collection

Basketball Court

The basketball court is of little or no historic or aesthetic significance. [Emphasis added.] (p. 65)

BBQ

The BBQ is of no significance. . [Emphasis added.] (pp. 74 and 76)

Included in the Statement of Significance is the following

Why is it Significant?

Established for over 140 years, the Edinburgh Gardens are of social significance because of their enduring focus of community use and high regard in which they are held. The Gardens' continuing social importance and popularity is heightened by its accessibility and provision of passive and active recreational facilities within a dense urban setting and provision for community interaction. [Emphasis added.]

Attachment 3 - Heritage Assessment Edinburgh Gardens - north precinct proposals

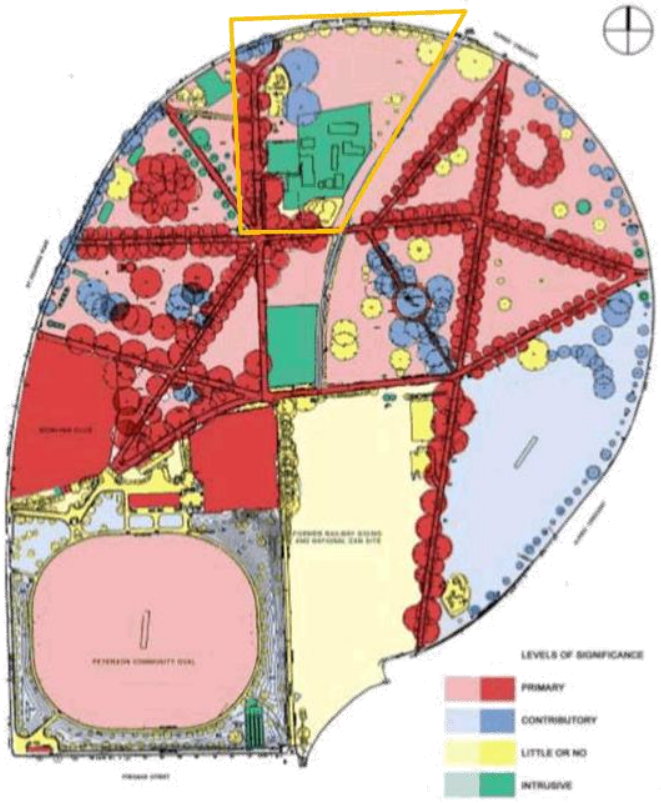


Figure 99 *Hierarchy of significance within the Edinburgh Gardens, hard landscape, buildings and soft landscape*

In summary, the path system, but not the fabric, is of primary significance as are the trees bordering the main path in the relevant area and the grassed area generally. Elements of primary significance should be retained and, if altered, then it should be done with minimal impact on significant fabric. Some other trees are of contributory significance however they will not be affected by the redevelopment. The affected hard landscape and built form elements have no significance. These can be altered, adapted or removed as required.

Apart from historical and aesthetic significance, neither of which will be impacted upon by this proposal, the Edinburgh Gardens has social significance, principally because of its use for sport and recreation and the high regard in which they are held held by the community.

Proposal

To relocate the table tennis table, remove the BBQ and playground and redundant fencing along the north-south path and to demolish the car park. To expand open space abutting Alfred Crescent, to narrow a path from Alfred Crescent and to install new table tennis and half-court basketball area, new BBQ area and playground and expansion of the existing skate park with provision for BMX bikes. New trees to be planted on the existing playground site and re-grassing of the demolished car park site.

Drawing Numbers

3 sheets: Existing conditions Oct 2017, Draft precinct plan Oct 2017, Draft playground concept plan Oct 2017.

Assessment of Proposed Works

Demolition

There are no heritage issues with removing or relocating any of the recreational features or hard landscape elements as proposed.

Specific Policies in the Conservation Management Plan

Sundry Items

3. *Introduce new sporting infrastructure only if it does not adversely impact on heritage values.*

If the sporting facilities are to remain popular and viable, there will be demands from time-to time to introduce new elements, for example a scoreboard, lighting, nets and the like. Generally these types of elements are acceptable depending upon their precise location, scale, bulk and design and the impact that they may have on any particular heritage values. They would need to be assessed on a case-by-case basis and in reference to the conservation policies and strategies and also the masterplan. (p. 143)

Where there are no specific policies for any element the alteration or addition or relocation of any element has been considered within the scope of this policy.

Skate Park

1. *Retain and maintain or remove the skate park as required.*

While the skate park is of recent origins, it is presently one of the most heavily used facilities in the Gardens. Its location is fairly conspicuous and, as such, it should be maintained in good condition if it is to remain in this area. The facility could occupy this, or other areas within the Gardens, such as the former railway siding / National Can site, where there would be greater scope to enlarge the facility and cause less impact on the nineteenth century character of the formal garden environs.

2. *Maintain an appropriate setting for the skate park.*

At present there is considerable damage and vandalism to the perimeter fence of the facility, which requires annual replacement and is often unsightly and in poor condition. The fence does not appear to have any practical purpose. Consideration

> should be given to removing the fence altogether or replacing it with a low grassed mound which would not only serve to partially screen the facility but contain the activities within the immediate area and provide informal amphitheatre seating from which to observe skaters (p. 140.)

While the conservation policy canvassed enlargement of the skate park in another part of the Gardens it also did not consider this to be inappropriate in its present area. In addition, there is a social and aesthetic benefit in keeping this activity in a single location. Last there is also a *quid pro quo* (advantage in return) between removal and re-grassing of the adjacent car park and enlargement of the skate park on an area which is presently grassed. This will result in a nett community benefit without any adverse effects on the significance of this part of the Edinburgh Gardens.

The fence will be removed and a mound will be constructed between the skate park and the playground to create a safety buffer and also to be a more aesthetic element than a fence.

Playgrounds

> *Retain and maintain or remove the playgrounds as required.*
>
> While the fabric of the playgrounds is relatively new and of no historic significance, playground facilities have been part of the Gardens since 1925 and remain popular. If retained, regular maintenance should be undertaken to ensure they remain in good condition. It is recommended that no additional playground areas be introduced to the Gardens as the two facilities are well spaced and located. Equipment can be changed / upgraded as required and in accordance with the appropriate standards. (p. 141.)

The proposal is for an upgrade of the playground and is entirely in accord with this policy. The equipment is contemporary, includes coloured elements and comprises standard playground elements designed in a manner to attract young children. Given the historical reasons for playgrounds in the Gardens and continued use by the community this is important. The proposal is entirely in accord with this policy.

In addition the relocation of the playground further south will allow for increased open space to the north which will provide a better presentation as viewed from Alfred Crescent.

BBQ

The BBQ area south of the existing playground is to be relocated to north of the proposed new playground which is effectively a shuffle around of both elements. This is entirely in accord with the conservation policies.

New trees

There is no specific policy for the planting of trees in this part of the Gardens and specifically where a hard landscape element or building has been removed. However the over-arching general policies provide enough guidance to indicate that new plantings should be in-keeping and should not detract from any aspect of significance. Clearly the former playground site will need replanting and while individual species have not been nominated on the plans yet it is recommended that new tree species should be chosen to complement those nearby.

Basketball Court

> *Retain and maintain or remove the basketball court as required.*
>
> While the basketball court is of no historical significance and in average condition, it has the potential to provide a popular facility within the Gardens. The court is currently under utilised due to its poor surface condition and size. Consideration should be given to either removing the court or upgrading it to include an improved playing surface and hoops. The size could also be enlarged to provide the appropriate size for basketball and two netball courts over the same area. (p. 141)

The existing court, which is outside the northern precinct study area will be retained and a new half court in the the northern precinct study area will be added and the playing surface and hoops will be upgraded. The table tennis area will be relocated to be adjacent to the basketball court. First, this is an improvement as recommended in the conservation policy and secondly relocation of the table tennis area offers better crossover scope for both sports. It is the activity and use which add significance to the Gardens rather than these specific elements in themselves. Redevelopment as proposed is entirely in accord with the policy for the basketball court and the general policy for new sundry elements.

Path

> *Reinstate the removed diagonal paths from the former fountain.*
>
> Should the former fountain in the northern section of the Gardens be reinstated, it is desirable that the removed path system associated with the feature be reconstructed.

It is not proposed to reinstate the fountain (Jubilee Fountain) at this time however the two remaining curved diagonal and straight north-south paths will be retained. The western curved path will be narrowed to exclude the entry of unauthorised vehicles into the Gardens. Noting that the physical fabric of the paths is of no significance this work is acceptable and in accord with the relevant conservation policies.

Parking and car park

> *Parking should generally be confined to the surrounding streets other than for service vehicles engaged in activities associated with events or building works.*
>
> A small amount of parking is presently provided within the Gardens to the north of the former gardener's residence and to the north of the grandstand. The present use of these areas is not limited to official uses and motorists not familiar with the site generally may enter and exit at will. This is at odds with the primarily pedestrian nature of the Gardens and potentially causes a safety issue with the playground and barbeque areas to the north of the site in particular.
>
> ...
>
> It is further recommended that the carpark adjoining the gardener's residence be removed or reduced in size to provide for a limited service area only. It is recommended that they be eliminated if the gardener's residence and depot precinct are removed. .... (p. 133)

The car park has been removed and vehicular access into the Gardens will now be either by Council staff or authorised permit holders on an as needs basis. This is a great improvement to the existing arrangement and is entirely in accord with the relevant policy.

Recommendation / Comments:

Essentially the proposal is a re-ordering of the north area and the replacement of elements of no significance with new upgraded facilities which is important given that the use of the Gardens*, inter alia*, is one reason why the Edinburgh Gardens has cultural heritage significance. Further the Edinburgh Gardens are held in high regard by the community and periodic upgrades will ensure that this community esteem for the Gardens and community use of the Gardens continues.

Overall there is a high, even complete, degree of compliance with the relevant policies set out in the *Edinburgh Gardens Conservation Management Plan*. As a consequence the proposal has a high level of support from a heritage perspective.

Signed:

Robyn Riddett

Director – Anthemion Consultancies

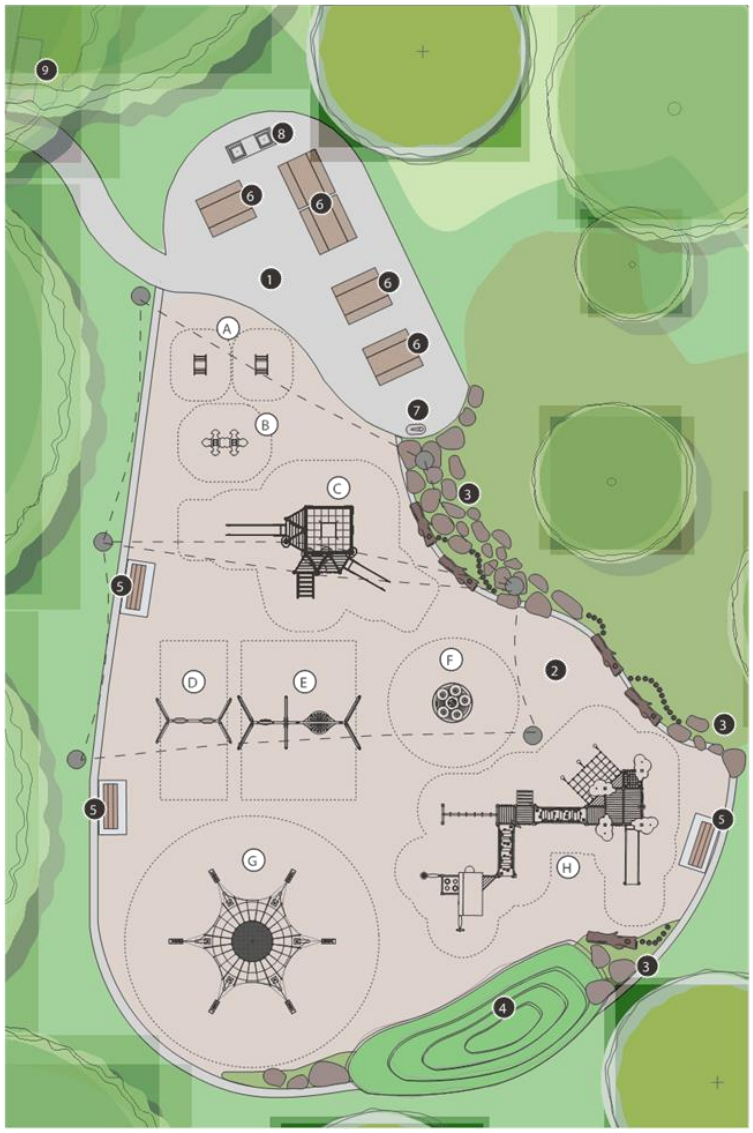
Date: 17 November, 2017

Attachment 4 - Final Edinburgh Gardens Northern Precinct Plan

Attachment 4 - Final Edinburgh Gardens Northern Precinct Plan - for endorsement

Attachment 5 - Edinburgh Gardens Northern Precinct Playground Upgrade Plan

Attachment 5 - Playground Upgrade Concept Plan



LEGEND

1 New BBQ area
2 New playground
3 Nature play areas
4 Grassed mound
5 New seats
6 New picnic tables with chess/game designs incorporated
7 Drinking fountain
8 BBQ
9 Bins

Existing tree
New tree
Soft fall mulch
Paving
Grass
Nature play areas
Grass mound
Safety boundary
Indicative shade sails (to future design)

A Rockers

B Multi-play see saw

C Activity unit

D Toddler swing

E Multi swing with basket swing

F Spinner

G Trampoline

H Activity unit

11.3 Victoria Street Precinct and Reimagining Victoria Street update

Executive Summary

Purpose

To provide an update on several matters relating to the Victoria Street precinct and work relating to the outcomes of the Reimagining Victoria Street engagement.

To acknowledge the significant work that numerous organisations and individuals have undertaken as part of the community campaign to establish a Medically Supervised Injecting Centre (MSIC) in North Richmond.

To ensure continued advocacy and support for evidence based harm reduction measures.

Key Issues

Council undertook the Reimagining Victoria Street community engagement project in 2017. This process identified drug use and community safety as the two biggest concerns for the community.

The issues facing Victoria Street are complex and Council is undertaking a number of initiatives to improve Victoria Street. These include the following:

(a) Works to improve the amenity, safety and public realm in Victoria Street, at the intersection of Lennox and Nicholson Streets;

(b) Initiation of the shopfront improvement program which was informed by a systematic audit of shopfronts and consultation with traders;

(c) Working with community and local agencies to improve the function, vibrancy and amenity of the precinct through the Local Safety Reference Group, Victoria Street Business Association Inc. and Victoria Street Consultative Group;

(d) Ongoing delivery of parking and traffic and cleansing services, and cleansing services including litter and graffiti removal; and,

(e) Continued delivery of Council's Syringe Management Services.

An update on each of these is provided within this report.

Additionally, in 2017 the State Government announced support for a two year trial of a Medically Supervised Injecting Centre (MSIC), two parliamentary inquiries were undertaken, several coroner reports and findings were released, and the State Government installed CCTV cameras.

Financial Implications

Funding has been allocated within the existing budget to undertake the shopfront improvement initiatives outlined. If deemed successful, further funding may be sought in subsequent years to extend the improvements to additional shopfronts. Funding has also been allocated for the urban design works on the corners of Victoria and Lennox/Nicholson Streets.

PROPOSAL

That Council:

(a) notes the updates in this report relating to the Victoria Street Precinct and Reimagining Victoria Street;

(b) authorises the CEO to write to the Department of Health and Human Services to:

(i) express support for the implementation of a trial of a Medically Supervised Injecting Centre (MSIC) in North Richmond;

(ii) accepts the invitation from DHHS to provide a briefing to Council on the intended implementation, community engagement and communications plans; and

(iii) seek clarification on the future of the Needle and Syringe Program and after hours vending machine located at North Richmond Community Health Centre;

(c) recognises the loss of many lives as a result of drug use in the City of Yarra and thanks the numerous organisations and individuals who campaigned tirelessly and acknowledges their significant contribution towards the establishment of trial Medically Supervised Injecting Centre (MSIC) in North Richmond;

(d) continues its advocacy and support for evidence based harm reduction measures that have proven to be effective in reducing the transmission of blood borne viruses (BBVs) and other health impacts associated with injecting drug use; and

(e) seeks a report from officers in 2018 that provides an update on best practice harm reduction strategies for drug use and options for Council in relation to policy positions on service delivery as well as strategic advocacy.

11.3 Victoria Street Precinct and Reimagining Victoria Street update

Trim Record Number: D17/189212
Responsible Officer: Unit Manager Social Policy and Research

Purpose

1. To provide Councillors with an update on several matters relating to the Victoria Street precinct and work relating to the outcomes of the Reimagining Victoria Street engagement.

Background

2. In July 2017 Council resolved to:

   (a) note the outcomes from the Reimagining Victoria Street community engagement process;

   (b) note that the an internal working group will be set up, which will report back to the Executive and Council;

   (c) note the working groups' key priorities, including mapping out which stakeholders play a part in addressing the issues raised, developing an action plan, engaging with key external stakeholders around how to best manage issues that are beyond Council's jurisdiction, and strengthening advocacy around the implementation of a Supervised Injecting Facility and other harm reduction strategies;

   (d) note the community desire for urgent action on the public health, safety and amenity issues in the Victoria St precinct; and

   (e) use the data collected in this project as the basis of an increased advocacy campaign for a medically supervised injecting facility in North Richmond.

3. The Reimagining Victoria Street community engagement process identified drug use and community safety as the two biggest concerns for the community.

4. As a follow on from Reimagining Victoria Street, and as per the resolution above, an internal working group has been formed with membership from across Council. There are a number of current and proposed activities that seek to address the themes and key issues that were identified during the community engagement process. Council is undertaking a number of initiatives to improve Victoria Street. These include the following:

   (a) works to improve the amenity, safety and public realm in Victoria Street, at the intersection of Lennox and Nicholson Streets;

   (b) initiation of the shopfront improvement program which was informed by a systematic audit of shopfronts and consultation with traders;

   (c) working with community and local agencies to improve the function, vibrancy and amenity of the precinct through the Local Safety Reference Group, Victoria Street Business Association Inc. and Victoria Street Consultative Group;

   (d) ongoing delivery of parking and traffic and cleansing services, including litter and graffiti removal; and

   (e) continued delivery of Council's Syringe Management Services.

5. To date in 2017, two parliamentary inquiries have been undertaken, several coroner reports and findings released and in specific relation the precinct, the Victorian Government has installed CCTV cameras and to undertake a two year trial of a Medically Supervised Injecting Centre (MSIC).

Victoria Street precinct - internal and external initiatives

Urban design works forming part of the Public Safety Infrastructure Fund grant

6. Council endorsed the Victoria Street Streetscape Masterplan in September 2015. The Masterplan identifies seven key projects, including the Lennox Street/Nicholson Street intersection upgrade. Council received a $200K grant from the Department of Justice and Regulation as part of the Public Safety Infrastructure Fund grant stream. Council is making a co-contribution of approximately $240K. A condition of the grant is to undertake a shopfront improvement program.

7. At its meeting on 19 September 2017, Council endorsed design concept plans for the Lennox Street/Nicholson Street intersection upgrade and authorised officers to proceed with detailed design documentation, tendering and construction. The works include new seating, paving, lighting, plantings, and improvements for cyclists and pedestrians. The works are to commence in 2018 after the Lunar Festival.

8. A condition of the funding was a formal evaluation of the work's impact upon public amenity and community safety. The evaluation involves pre and post observational studies, street intercept surveys and analysis of pertinent data sets. The following are the defined indicators of success:

   (a) An increase in the number and diversity of people visiting, accessing and utilising the space;

   (b) A reduction in criminal offences recorded across the different crime categories;

   (c) Decreases in drug activity;

   (d) Increases in perceptions of safety; and

   (e) Positive feedback from the community.

9. Council officers are also exploring activation opportunities and potential activities for this space once the works are completed.

10. It is also noted that planning permits have been granted to the adjoining properties to the east and west of the existing Lennox Street seating area that would lead to an increase in building height but also potential street level improvements.

    (a) the site to the east was granted a planning permit for the construction of a three storey building, containing shops at ground floor and dwellings above in July 2016. The permit contained several conditions, and while the applicant has since lodged revised plans for approval, these were deemed unsatisfactory in February 2017; and

    (b) the site to the west was granted a planning permit in May 2006 for an extension to the existing building. Since this period a number of amendments have been received, including one that is under current consideration that proposes to construct a fourth floor and roof top terrace to the existing building.

Audit of existing shopfronts and commencement of a shopfront improvement program

Audit of existing shopfronts and trader intercept surveys

11. A condition forming part of the Public Safety Infrastructure Fund grant agreement is to undertake a shopfront improvement program. The first step of the program involved an audit of the existing shopfronts. This captured information such as type of roller shutter (if present), presence of graffiti and any readily visible security measures such as CCTV. Over one third of shopfronts within the precinct (36%) have a roller shutter, the majority of which are solid metal.

12. The second step involved undertaking intercept surveys of traders to examine their thoughts on improving shopfronts, including any barriers and enablers. Fifty-two businesses completed the face-to-face survey. A majority of respondents (60.8%) had roller shutters installed for safety and security reasons. The most common issues affecting these businesses were graffiti (75%), shoplifting (71.2%), drug dealing (61.5%), drug use (61.5%), and discarded syringes (57.7%).

13. There was very little interest in roller shutter removal with just one business indicating they would support removal and one other saying they may consider it. However, over half (52%) of the interviewed traders were interested in other forms of shopfront improvements including:

    (a) window and shopfront cleaning;

    (b) shopfront and entry repair;

    (c) painting of walls/shutters (solid colour or murals); and

    (d) consultant to assist with window dressing techniques.

Visual merchandising, shopfront cleaning/maintenance

14. Given the strong desire for respondents to retain their roller shutters, several other approaches have been explored to improve shopfronts.

15. The pilot program involves working in close partnership with at least five business/property owners on Victoria Street to improve shopfronts within a short timeframe. The area between Church and Lennox Streets on the southern side of Victoria Street will be targeted to maximise program impact.

16. The project involves one-on-one consultation with participating businesses to develop an achievable shopfront design that fits within the program budget.

17. Participating traders are not expected to contribute any financial amount to this program. However, they are expected to maintain the upkeep of their shopfronts to the standard provided to them at the close of the program. Each participating business will also be provided with a shopfront cleaning/maintenance kit and a shopfront maintenance checklist at the close of the program.

18. It is envisaged that this program will run over a three year period and is planned to commence in January 2018. The program will be evaluated to inform the implementation of a further roll out over an additional two year period.

19. The participants of the pilot will be encouraged to become champions of the program and help promote the benefits to other businesses in the street.

Cleaning blitz on shopfronts / aesthetic improvements to the roller shutters

20. These works will also target the area between Church and Lennox Streets on the southern side of Victoria Street.

21. They will be implemented in conjunction with a number of other initiatives across Council. These include the visual merchandising / shopfront cleaning/maintenance work outlined above, minor improvements to street furniture and signage and some painting of Council infrastructure.

22. A range of options to treat the roller shutters were investigated including cleaning only, block single colour painting, painted murals/artwork, vinyl cut out decals, and full graphic vinyl wrap. Criteria that were taken into account included longevity and sustainability of the treatment, aesthetics, graffiti resistant surfaces, ease of cleaning and past experience of graffiti in Victoria Street.

23. Cleaning of all shopfront facades including roller shutters will be undertaken in the first instance. After this, a graphic vinyl wrap will be applied to the roller shutters on nine shops. The graphics will potentially reflect the merchandising of the shop and visually indicate the types of businesses occurring behind the roller shutters.

24. The vinyl wraps will be graffiti resistant and traders would be issued with appropriate cleaning products and instructions on maintaining the shutters in a graffiti free condition.

Supporting community groups and promoting Victoria Street

25. Council is committed to supporting the Victoria Street Business Association (VSBA) to become an effective and transparent business association and to promote Victoria Street as a visitor destination.

26. Council also provides opportunities for cultural celebrations including the Lunar Festival which is funded through the Celebrate Yarra Triennial Funding Grant.

27. Council has issued several other community grants to organisations that contribute to the local community, including to those who provide services to vulnerable groups.

Local Safety Reference Group

28. The Yarra Local Safety Reference Group was established to coordinate a whole of community partnership response to community safety issues in the City of Yarra.

29. The group meets on a quarterly basis and is co-chaired by Yarra City Council and Victoria Police. The membership also includes cohealth, Department of Health and Human Services, Department of Justice and Regulation, Fitzroy Legal Service, Launch Housing, Neighbourhood Justice Centre, North Richmond Community Health, Women's Health in the North, and Yarra Drug and Health Forum.

30. The group provides a valuable mechanism for key stakeholders to discuss issues and share ideas relating to Victoria Street. Victoria Street is a standing agenda item for the group.

Victoria Street Consultative Group

31. The aim and purpose of the group includes to make Victoria Street a safer, more prosperous and welcoming environment; and to share information and ideas.

32. The group meets on a bi-monthly basis and is chaired by the Neighbourhood Justice Centre and Yarra Drug and Health Forum. The membership also includes Yarra City Council, Victoria Police, North Richmond Community Health, Youth Support and Advocacy Service, Department of Health and Human Services, and residents from the local area.

33. Council officers have presented several items to the group for information and discussion and will continue to attend and contribute.

Parking and traffic

34. The Local Area Place Making study in Abbotsford precinct (LAPM 13 bounded by Johnston Street, Hoddle Street, Victoria Street and Yarra River) has commenced and is seeking community input on ways to improve local streets and the safety of pedestrians and cyclists. The initial consultation concluded in late November and Council has received a significant number of responses to the online survey, and ideas and concerns through the interactive map.

Litter and graffiti

35. Council is working with businesses in Victoria Street to address of the issue of bins being left on the street with enforcement action taken against a number of premises. The area is checked and cleaned every day by the litter control crews. Litterbins are emptied daily.

36. Posters and graffiti are cleaned daily from Council assets on a night shift by graffiti crews; however this does not include shopfronts because they are private property.

Council's syringe management services

37. The impacts of illicit drug use, including unsafely discarded equipment, are a major concern for the community. Council is committed to improving local amenity through reducing the presence of discarded needles, syringes and related waste.

38. Council contracts Innerspace (cohealth) to provide syringe management services. Syringe management services include removal of discarded syringes and related litter from public and private spaces in the City of Yarra via sweeps, and also by requests through the Yarra Syringe Disposal Hotline. Innerspace also regularly clear and monitor syringe disposal units, and collect any discarded syringes and related litter in the immediate area surrounding the units.

39. At its meeting on 17 October 2017 Council considered a report on Council's syringe management services. Council resolved that it:

    (a) notes that although the number of syringes collected via sweeps has almost doubled, this is offset by the decrease in the number of syringes collected via community request, and that the overall increase in syringes collected is around 11%;

    (b) notes the current activity in this space, including Department of Health & Human Services' contract with cohealth and the installation of CCTV along Victoria and Lennox Streets, is impacting or is likely to impact on Council's syringe management services;

    (c) notes that given the current activity in this space, it is inadvisable to make any changes to Council's syringe management services contract at this point in time; and

    (d) notes that the contract manager will continue to monitor the situation and update Council as needed.

Medically Supervised Injecting Centre

40. In October 2017, the State Government announced support for a two year trial of a Medically Supervised Injecting Centre (MSIC) at North Richmond Community Health (NRCH). The Department of Health and Human Services (DHHS) held briefings with professional stakeholders (including Council) in mid-November 2017.

41. The Drugs, Poisons and Controlled Substances Amendment (Medically Supervised Injecting Centre) Bill 2017 has been introduced into parliament. The bill is anticipated to pass parliament before the end of 2017, and the centre is expected to open by July 2018.

42. The bill outlines the licensing regime (2 year trial with option for a 3 year extension and only at NRCH) and exclusions of liability (for drug users and centre staff).

43. The key differences between the proposed MSIC and the one already in Kings Cross Sydney are a longer trial period, deliberate focus on clinician-led service delivery, with the centre director and supervisor required to be registered medical practitioners, and strong integration with other health and support services.

44. Regulations and operating protocols are to be developed and will guide matters such as the type of substances that will be permitted to be used at the centre, what quantity will be deemed personal use for the purpose of injecting at the centre and whether people will be required to provide personal details before they can use the centre. Targeted consultation on the regulations will be undertaken and governance structures, including an advisory panel, are currently being considered. It has already been established that persons under 18 years of age will not be permitted to use the MSIC.

45. An evaluation will be undertaken, including a review after one year.

46. The MSIC forms part of a broader packaged of initiatives, including harsher penalties for heroin dealers.

47. Council has welcomed the decision by the State Government to support a trial of an MSIC in North Richmond. The centre will prevent unnecessary deaths, connect people to health and other services and reduce amenity impacts for the local community.

48. It should be acknowledged that no single intervention can address the breadth of complex and dynamic issues arising from injecting drug use and a suite of harm reduction responses will be required. It is currently unclear as to whether the existing Needle and Syringe Program (NSP) and after hours vending machine at NRCH will continue after the establishment of the MSIC. There appears to be some concerns as to whether such services are compatible with an MSIC. The need for clean injecting equipment for those cohorts who do not use the MSIC will still have to be addressed. There will also be a need for enhanced outreach services in the area to ensure that injecting drug users utilise the MSIC and receive help.

49. On several occasions Council has resolved to support a range of harm reduction measures, in addition to an MSIC. In 2013 Council resolved to endorse officers to engage with the State Government to discuss local implementation of harm reduction strategies. These positions have been articulated both in previous resolutions and strategies, including:

    (a) supporting the peer administered naloxone program;

    (b) extending the hours of needle and syringe programs;

    (c) increasing outreach to people who inject drugs during evenings and weekends;

    (d) enabling peer distributed needles and syringes; and

    (e) installing syringe vending machines.

50. This report proposes that Council continues its advocacy for evidence based harm reduction measures. It also proposes that Council writes to the State Government to seek clarification on whether the NSP and after hours vending machine at NRCH will continue after the establishment of the MSIC and what enhanced outreach services are being proposed.

51. Officers from Council and DHHS have had initial engagement with regards to the establishment of the MSIC. Officers and DHHS have agreed to meet monthly commencing in January 2018.

52. Whilst the DHHS has indicated willingness to further work with Council officers, to date, there has been no formal correspondence between the Victorian Government and the Mayor and Councillors of Yarra City Council. An opportunity exists for better engagement with Councillors to understand DHHS' plans for implementation, community engagement and communications. Officers have made representations to the Department to determine whether DHHS would be able to provide a briefing for Council. DHHS has subsequently extended an invitation to attend a future Council briefing to better inform Councillors of the project.

53. An expert panel of health and community representatives will be established to report on the effectiveness of the medically supervised injecting room. The full membership of this panel has not been decided. Early indications are that a locally based reference group will also be established containing representatives of key local stakeholders.

54. There are also several other evidence based harm reduction strategies and services that could be considered for advocacy which have not been previously considered by Council. For example, strategies to increase the number of pharmacotherapy (methadone and buprenorphine) prescribers and dispensers to help addicted persons transition away from licit and illicit opiates or support for safety testing advisory services for amphetamine-type stimulants. This report proposes that another report is provided to Council in 2018 on these matters.

Parliamentary inquiries

55. Council made submissions to the two recent inquiries. The Law Reform, Road and Community Safety Committee Inquiry into Drug Law Reform completed hearings in November 2017. The committee will submit their inquiry findings report to the State Government by 9 March 2018, and the State Government has six months to respond.

56. The Legislative Council Inquiry into the Drugs, Poisons and Controlled Substances Amendment (Pilot Medically Supervised Injecting Centre) reported their findings in September 2017. While the report did not directly recommend an MSIC trial, it did note that the issue had 'reached crisis level' and described residents dealing with overdoses in their laneways and discarded syringes in front gardens as intolerable; that MSICs improve the health of IDUs, address visible impacts and reduce demand for ambulances; and MSICs do not have a 'honey pot' effect on crime.

Coroner reports and findings

57. There are four recent coroner reports that specifically address the implementation of harm reduction measures in North Richmond:

    (a) the first by Coroner Jacqui Hawkins (Court Reference COR 2016 2418, February 2017) made recommendations to establish a safe injecting facility trial in North Richmond, to expand the availability of naloxone, and that DHHS undertake a review of DHHS funded services that support the health and wellbeing of injecting drug users and that consultation is undertaken with relevant stakeholders to identify opportunities for improvements. The DHHS review has commended and Council officers were interviewed by consultants in November 2017;

    (b) the second by Coroner Audrey Jamieson (Court Reference COR 2016 2722, February 2017) supported the recommendation by Coroner Hawkins to introduce a trial safe injecting facility; and

    (c) the third by Coroner Gregory McNamara (Court Reference COR 2016 3735, July 2017) supported the recommendations made by Coroner Hawkins.

58. The fourth, again by Coroner Jacqui Hawkins, (Court Reference COR 2017 1152, October 2017) reiterated earlier findings made by Coroner Jacqui Hawkins.

59. Additionally, Coroner Judge Sara Hinchey made a submission into The Inquiry into the Drugs, Poisons and Controlled Substances Amendment (Pilot Medically Supervised Injecting Centre). The transcript of the submission acknowledged that in 2016 the City of Yarra had the highest frequency and rate of overall overdose deaths in Victoria, as well as the highest frequency and rate of heroin involved overdose deaths in Victoria. It discussed how a comparative analysis of heroin involved overdose deaths for 2016 across several Victorian local government areas showed that the City of Yarra had the highest proportion of deaths occurring in public places rather than in private residences, and the highest proportion of heroin involved overdose deaths where the deceased usually resides in another local government area and travelled to inject heroin within the City of Yarra.

CCTV cameras

60. The State Government has now installed 13 cameras in total, affixed to 5 poles in North Richmond, this includes:

    (a) 8 cameras affixed to 3 poles along Lennox Street, located between Victoria Street and Highett Street (east side of Lennox Street and within the boundary of the housing estate); and

    (b) 5 cameras installed on Victoria Street and affixed to 2 poles, one located on the corner of Nicholson Street (on the eastern side) and the other installed opposite Little Lithgow Street (southern side of Victoria Street).

External Consultation

61. Extensive consultation was undertaken with the community as part of the Reimagining Victoria Street process.

Internal Consultation (One Yarra)

62. Updates were sought from relevant staff across the organisation, including from those that form part of the Reimagining Victoria Street internal working group.

Financial Implications

63. Funding has been allocated within the existing budget to undertake the shopfront improvement initiatives outlined. If deemed successful, further funding may be sought in subsequent years to extend the improvements to additional shopfronts. Funding has also been allocated for the urban design works at the intersections of Victoria and Lennox/Nicholson Streets.

Economic Implications

64. Not applicable.

Sustainability Implications

65. Not applicable.

Social Implications

66. Improvements to Victoria Street will contribute to a positive impact on the social environment.

Human Rights Implications

67. Not applicable.

Communications with CALD Communities Implications

68. Not applicable.

Council Plan, Strategy and Policy Implications

69. The Council Plan 2017-2021 identifies higher rates of drug and alcohol abuse, lower perceptions of safety in some parts of Yarra as key issues. It notes that the drug trade and associated health behaviours and outcomes affect the community from both a community safety perspective and a health and wellbeing perspective. Relevant strategies include:

    (a) promote a community that is inclusive, resilient, connected and enjoys strong mental and physical health and wellbeing; and

    (b) assist to reduce the harms from alcohol and drugs on individuals and the community in partnership with state agencies and key service providers.

70. The Council Plan also includes actions to:

    (a) advocate to the state government for a medically supervised injecting facility and other harm minimisation measures in the Victoria Street precinct; and

    (b) work with local partners and agencies to increase health and education, community partnerships and harm minimisation from the abuse of drugs and alcohol.

Legal Implications

71. Not applicable.

Other Issues

72. Not applicable.

Options

73. Not applicable.

Conclusion

74. This report provides an update on several matters relating to the Victoria Street precinct and work relating to the outcomes of the Reimagining Victoria Street engagement. It also acknowledges the significant efforts of numerous organisations and individuals have undertaken as part of the campaign to establish a Medically Supervised Injecting Centre (MSIC) in North Richmond. It recommends that Council continues with advocacy for harm reduction measures and that clarification is sought on several outcomes relating to the MSIC.

RECOMMENDATION

1. That Council:
   (a) notes the updates in this report relating to the Victoria Street Precinct and Reimagining Victoria Street;
   (b) authorises the CEO to write to the Department of Health and Human Services to:
      (i) express support for the implementation of a trial of a Medically Supervised Injecting Centre (MSIC) in North Richmond;
      (ii) accept the invitation that DHHS provide a briefing to Council on the intended implementation, community engagement and communications plans; and
      (iii) provide clarification on the future of the Needle and Syringe Program and after hours vending machine located at North Richmond Community Health Centre; and
   (c) recognises the loss of many lives as a result of drug use in the City of Yarra and thanks the numerous organisations and individuals who campaigned tirelessly and acknowledges their significant contribution towards the establishment of trial Medically Supervised Injecting Centre (MSIC) in North Richmond;
   (d) continues its advocacy and support for evidence based harm reduction measures that have proven to be effective in reducing the transmission of blood borne viruses (BBVs) and other health impacts associated with injecting drug use; and
   (e) seeks a report from officers in 2018 that provides an update on best practice harm reduction strategies for drug use and options for Council in relation to policy positions on service delivery as well as strategic advocacy.

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Attachments
There are no attachments for this report.

11.4 Managing Growth and Change in Yarra

Executive Summary

Purpose

The purpose of this report is to provide an update on work the Strategic Planning Department is undertaking to manage growth and change in Yarra, in particular the Yarra Planning Scheme Rewrite Project.

Financial Implications

The work is being accounted for in Council's Strategic Planning budget.

PROPOSAL

The City of Yarra is undertaking a comprehensive rewrite of the Municipal Strategic Statement (MSS) and Local Policy in the Yarra Planning Scheme (YPS). This is known as the YPS rewrite project.

The YPS rewrite project is supported by a significant work program including Activity Centre planning; preparation of a Spatial Economic and Employment Strategy; preparation of a new Housing Strategy (2017-2032); and heritage investigations.

In addition to the YPS rewrite work, Council is preparing built form analysis, structure plans and planning controls for Yarra's activity centres: Swan, Johnston, Brunswick, Smith and Victoria Streets, Queens Parade and Bridge Road.

This report sets out the process for the YPS rewrite project and the concurrent strategic work for the major Activity Centres in Yarra.

11.4 Managing Growth and Change in Yarra

Trim Record Number: D17/193190
Responsible Officer: Manager City Strategy

Purpose

1. The purpose of this report is to provide an update on work the Strategic Planning Department is undertaking to manage growth and change in Yarra, in particular the Yarra Planning Scheme Rewrite Project.

Background

2. The City of Yarra is undertaking a comprehensive rewrite of the Municipal Strategic Statement (MSS) and Local Policy in the Yarra Planning Scheme (YPS). This is known as the YPS rewrite project.
3. The YPS rewrite project is supported by a significant work program including Activity Centre planning; preparation of a Spatial Economic and Employment Strategy; preparation of a new Housing Strategy (2017-2032); and heritage investigations.
4. New policy is proposed in order to:
   (a) guide and direct growth in employment and housing to appropriate locations; and
   (b) manage the heritage, environment, sustainability initiatives and open space.
5. Council will also need to have regard to the transport network and infrastructure needs as a context to the policy guidelines.
6. The following sections step out the process for re-writing the YPS, including the work underway and programme.

Review of Planning Scheme

7. In 2013-14 Council, as the Planning Authority, reviewed the YPS as required under the provisions of Section 12B of the *Planning and Environment Act 1987*.
8. The review included: a legal review; consultation with Councillors, City of Yarra statutory and strategic planners and urban designers, developers, planning consultants and community groups; and planning peer review. It provided recommendations to improve the effectiveness and efficiency of the YPS, which underpin the current rewrite process. The review was submitted to the Minister for Planning in October 2014.

Liveable Yarra

9. In 2015 Council embarked on the *Liveable Yarra* process, culminating with the deliberative consultation sessions in August and September of that year. These consultations support the YPS rewrite process and allowed an in-depth, two-way conversation with the community about how Yarra can adapt and change to the challenges and opportunities brought about by growth and change.
10. This process sought input from members of the community that do not often have their views heard and also specifically to allow community members to hear each other's perspectives. Council also wanted to allow participants to gain an understanding of the complexity of planning issues and the challenges this presents to Council decision making.
11. This was the first time a deliberative approach has been undertaken for a topic as multifaceted and far-reaching as rewriting the planning scheme.

12. In summary, the *Liveable Yarra* process included a *People's Panel* of 60 members who ultimately provided recommendations to Council on the topics of housing, transport, built form and the local economy. Discussions were also held with Council's Advisory Committees as well as targeted workshops with hard-to-reach communities to ensure the People's Panel were fully informed of the broader community's views. Their recommendations included acknowledgements of the trade-offs and implications of those choices. The Panel's advice is being used to inform the policy development process as part of the YPS rewrite which is currently underway. (See https://www.yarracity.vic.gov.au/the-area/yarras-future/liveable-yarra-project). This Council website provides a library of the material, processes and deliberations of the People's Panel.

Housing Strategy

13. The 2014 YPS review identified a series of policy gaps in the current YPS, including the need for a clear description of how Yarra's housing growth would be accommodated through the preparation of a Housing Strategy. The absence of clear directions would result in uncertainty and resources spent on complex decision making processes.

14. The *Liveable Yarra* Panel provided clear direction to Council on the key issues for consideration when planning for housing growth and change. Seven relevant key themes were considered:

    (a) heritage;

    (b) diversity;

    (c) adaptability;

    (d) communal open space;

    (e) affordable housing;

    (f) transport and infrastructure; and

    (g) sustainability and economic development.

15. A draft Housing Strategy is currently being prepared for Council consideration to set a preferred vision for Yarra over the next 15 years. It aims to provide certainty about the locations and nature of change expected in neighbourhoods as well as direction on key issues such as housing diversity and adaptability, affordable housing and family friendly apartments. It will (once adopted by Council), be included in the draft Municipal Strategic Statement (MSS) and draft local policy as part of the YPS Rewrite process.

16. A draft Housing Strategy is programmed to go on exhibition in early 2018 (March- April), allowing the community to comment on the directions in the strategy. Community input will then be considered in preparing a final strategy that is currently programmed to be reported to Council mid - 2018. The directions of the final strategy will inform new policy as part of the YPS rewrite.

Spatial Economic and Employment Strategy (SEES)

17. In 2015 Council commenced background investigations to provide direction for a spatial strategy that guides growth and change in employment and economic activities. This work provided an analytical evidence base for the draft SEES which is currently being prepared.

18. The draft SEES is intended to assist Council to understand, and capitalise on, Yarra's economic strengths and respond to the key trends and economic drivers over the next 10 to 15 years. It will set out strategies for the spatial distribution of employment and economic activity in Yarra, informing the YPS rewrite and other strategic planning projects.

19. Focussed conversations occurred with local businesses to understand their experience of doing business in Yarra. This input is being considered as part of preparing the draft SEES.

20. A draft SEES is programmed to go on exhibition early 2018 (March- April), allowing the broader community to comment on the directions in the proposed strategy. Community input will be considered in preparing a final strategy that is currently programmed to be reported to Council mid - 2018. The directions of the final strategy (once adopted by Council) will inform new policy as part of the YPS rewrite.

Policy Development

21. Work on a draft revised Heritage Policy has to date included:

    (a) consultation with *Liveable Yarra*, Council's Heritage Advisory Committee (HAC) and Council officers;

    (b) review of Burra Charter and Heritage Council Guidelines;

    (c) policy review to reduce duplication, improve clarity and language; and

    (d) further technical investigations of industrial heritage, residential heritage and heritage in Yarra's activity centres.

22. Technical investigations to support policy is also occurring in relation to: sustainable traffic and parking policy and built form policy.

23. Policy will need to have specific regard to State Government policy and actions and build on work occurring in departments within Council, including for example, Council's recently adopted Urban Forestry Strategy.

24. Focussed conversations on each of the above policy areas, is programmed to occur with the *Liveable Yarra* Reference Group and Council's Advisory Groups early 2018 to allow for draft policy formulation by mid–2018. Policies would then be incorporated into the YPS rewrite (draft MSS and draft local policy) for Council consideration.

YPS Rewrite Statutory Amendment Process

25. When the draft MSS and draft local polices have been prepared, based on the above work and consultation, Council would then commence the formal planning scheme amendment process (summarised broadly below). This is programmed to commence in the second half 2018.

26. *Stage 1*: Planning Authority (Council) seeks *authorisation* from the Minister of Planning to prepare and exhibit a planning scheme amendment. NB. A Planning Scheme proposal cannot be placed on exhibition without this 'authorisation'.

27. *Stage 2*: Exhibition – The amendment is made available to view publicly. During this time the community can make submissions to Council as the planning authority about the draft MSS and draft local policy.

28. *Stage 3*: Council as the planning authority consider submissions and refers them to an independent planning panel (Panels Victoria). An independent planning panel hearing is then conducted and the panel provide a written report to the Council as the Planning Authority.

29. *Stage 4*: Council considers the submissions and the planning panel report, and makes a decision to adopt (with or without changes) or abandon the amendment.

30. *Stage 5*: The amendment is submitted to the Minister for Planning seeking approval and gazettal.

31. *Stage 6:* The Minister for Planning makes a decision. If the Minister approves the Amendment then it is 'gazetted' and forms a legal document.

Activity Centres

32. In addition to the YPS rewrite work, Council is also preparing built form analysis, structure plans and planning controls for Yarra's activity centres: Swan, Johnston, Brunswick, Smith, Victoria Streets, Queens Parade and Bridge Road.

33. The following table sets out an update on the progress of strategic work for the major Activity Centres in Yarra.

Project	Update
Johnston Street **(Amendment C220 + C237)**	This planning scheme amendment is on exhibition until 18 December 2017. Council will then need to consider submissions and mostly likely refer them to an independent planning panel. Following the panel process Council will consider the submissions and the planning panel report, and make a decision to adopt (with or without changes) or abandon the amendment. If supported, the amendment is then submitted to the Minister for Planning for approval. NB. On 5 December, 2017 Council also resolved to request the Minister for Planning for interim Design and Development controls and interim Heritage controls, in accordance with Section 8 (1) (b) and 20 (4) of the *Planning and Environment Act 1987.* This would (if approved by the Minister) allow for controls to come into effect while the permanent provisions undergo the full planning scheme amendment process over the next 12 to 18 months. Council is awaiting approval.
Swan Street **(Amendment C191 + C236)**	Council is awaiting Ministerial 'authorisation' to prepare and exhibit a planning scheme amendment. When this is received the following process occur. Stage 2: Exhibition – The amendment is made available to view publicly. During this time the community can make submissions to Council as the planning authority about the proposal. Stage 3: The planning authority consider submissions and refers them to an independent planning panel. An independent planning panel hearing is then conducted. Stage 4: Council considers the submissions and the planning panel report, and makes a decision to adopt or abandon the amendment. Stage 5: The amendment is submitted to the Minister for Planning seeking approval. NB. Council has also requested the Minister for Planning for interim Design and Development controls and interim Heritage controls, in accordance with Section 8 (1) (b) and 20 (4) of the *Planning and Environment Act 1987.* This would allow for controls to come into effect while the permanent provisions undergo the full planning scheme amendment process over the next 12 to 18 months. Council is awaiting approval by the Minister.
Queens Parade	Following significant effort, Interim Design and Development Overlay (DDO) Controls for Queens Parade (West) Precinct were introduced on 28 March 2017 via Amendment C229. A Built form analysis and a heritage built form review are currently being undertaken to provide **strategic justification and evidence** for future planning controls for the remaining sections of Queens Parade. A planning scheme amendment introducing new planning requirements is scheduled for Council's consideration on 19 December 2017. If Council agree with its content, the amendment would be lodged with the Minister for Planning for 'authorisation'. Assuming this is obtained the amendment would undergo the statutory amendment exhibition process during 2018. Council can also consider seeking interim controls whilst the amendment process occurs similar to its decision

	on 5 December in relation to Johnston Street and Swan Street.
Brunswick, Smith Streets Precinct and Victoria Streets and Bridge Road Precinct - Built form analysis	Substantial work is progressing on two built form analysis projects, covering the Bridge Road and Victoria Street precincts; and also the Brunswick Street and Smith Street precinct (which includes parts of Gertrude Street, Johnston Street and the west side of Wellington Street). This work commenced in April 2017. The purpose of this work is to prepare a 'Built Form Framework' for each precinct, which would recommend principles, guidelines and requirements to guide future development and manage change. This work will provide **strong strategic justification and evidence** for structure plans and future provisions in the Yarra Planning Scheme. The expert urban design consultants, in collaboration with heritage and traffic experts have largely completed analysis work for these projects. This work has been presented and discussed with Councillors in two workshops and also the *Liveable Yarra* Reference Group. The project teams are currently preparing principles and testing different development built form options. Following these being worked through the Planning office will be drafting built form recommendations, and Councillors will receive a special briefing during March 2018 on the draft built form recommendations after which it will be presented for consideration by Council. Once this analysis has been completed and Council has resolved on the Built Form Analysis, Interim Planning Scheme provisions could be sought similar to Johnston Street and Swan Street. Preparation of structure plans, planning scheme provisions and a subsequent planning scheme amendment will follow the recommendations in 2018. Preparation on the structure plans will include community consultation in 2018.

34. It must be recognised that achieving the desired outcomes of the Strategic Planning work plan is affected by a number of externalities. These include:

    (a) policies and actions set out in *Plan Melbourne* (the State Planning Strategic document);

    (b) possible changes to the planning policy framework as a result of the State Government's Smart Planning project and any other reforms;

    (c) consideration of amendments by external decision makers including, Planning Panels Victoria, DELWP and the Minister for Planning; and

    (d) any possible change in metropolitan planning policies or practices should there be a change in State Government in 2018.

External Consultation

35. No consultation has been required for the preparation of this status report.

Internal Consultation (One Yarra)

36. Strategic Planning is working with Statutory Planning, Urban design, other departments and consultants as relevant in undertaking the work.

Financial Implications

37. The costs have been accounted for in the 2017/18 budget for Strategic Planning. The amendment process in the second half of 2018 will be included in the 2018/19 budget for Strategic Planning.

Economic Implications

38. N/A in relation to a status report.

Sustainability Implications

39. N/A in relation to a status report.

Social Implications

40. N/A in relation to a status report.

Human Rights Implications

41. N/A in relation to a status report.

Communications with CALD Communities Implications

42. N/A at this stage. The administration will be providing updated material on the Council website.

Council Plan, Strategy and Policy Implications

43. N/A. This update report relates directly to strategy development of the Council consistent with the intent as outlined in the Council Plan.

Legal Implications

44. N/A

Conclusion

45. The Strategic Planning team is currently undertaking several significant projects which inform the Yarra Planning Scheme Rewrite including: Activity Centre planning; development of a Spatial Economic and Employment Strategy and the development of a new Housing Strategy (2017-2032); and heritage investigations.

46. This work is programmed to meet important milestones and to position the Council to be able to better manage growth and development pressures.

47. The report outlines the key steps and stages of these studies and the intent towards policy formulation.

48. The outcome being pursued is the successful gazettal of a new Planning Scheme for Yarra City Council that assists further in managing growth and development pressure. To achieve this outcome solid strategic analysis is required on some key matters to underpin the basis of the proposed Planning Scheme so that it is robust and can successfully navigate the rigour of a Panel enquiry process and the Minister for Planning's scrutiny.

49. It must also have due regard to the State Planning policies; the reconciliation amongst State policy drivers and local aspirations is one of the key challenges to achieving this successful outcome.

RECOMMENDATION

1. That Council note:

   (a) the report on the significant Strategic Planning work being undertaken to manage growth and change in Yarra, in particular the Yarra Planning Scheme (YPS) Rewrite Project, which is supported by a program of Activity Centre planning; preparation of a Spatial Economic and Employment Strategy (SEES); preparation of a new Housing Strategy (2017-2032) and evidence based investigations;

   (b) the draft policy is being prepared based on strategic evidence based investigations and focussed discussions with the Liveable Yarra Reference Group and Council's Advisory Committees; and engagement with the broader community on the SEES and Housing Strategy;

   (c) the draft Yarra Planning Scheme planning policy is programmed to the considered by Council in mid-2018;

   (d) that in parallel to the Yarra Planning Scheme rewrite project built form analysis and planning scheme amendments are progressing for six of Yarra's activity centres – Brunswick, Smith, Johnston, Swan and Victoria Streets, Bridge Road, and Queens Parade; and

   (e) that the significant Strategic Planning work is programmed throughout 2018 and then the formal Amendment processes under the Planning and Environmental Act.

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Attachments
There are no attachments for this report.

11.5 Amendment C223 - Proposed rezoning of 81-95 Burnley Street and 26 Doonside Street, Richmond

Trim Record Number: D17/197248
Responsible Officer: Director Planning and Place Making

Purpose

1. The purpose of this report is for Council to consider seeking authorisation to exhibit Amendment C225 in accordance with the *Planning and Environment Act 1987,* which seeks to rezone land at 81-95 Burnley and 26 Doonside Streets, Richmond from Industrial 3 to Mixed Use Zone and apply a Development Plan Overlay and an Environmental Audit Overlay.

Background

Request for Amendment

2. In 2009 Council prepared Amendment C99 which rezoned the land in the Doonside precinct from Industrial 3 to Mixed Use Zone. The owner of 81-95 Burnley and 26 Doonside Streets, Richmond expressed concern about the impact the rezoning may have on the operation of the business and asked to be excluded from the amendment (refer to Figure 1). The panel report for Amendment C99 documented that "*Council advised the Panel that it excluded these properties as it was of the opinion that this area of land could be included in a future rezoning proposal*".

Figure 1: Aerial photo, subject site marked in red

3. Changing business conditions in recent years has meant that the business has needed to shift the warehouse component to Brooklyn, and consolidate and upgrade the showroom and administration components in Richmond.

4. In August 2016, consultants representing the land owner lodged a request for Council to amend the Yarra Planning Scheme to:

   (a) rezone the land from Industrial 3 to Mixed Use;

   (b) apply the existing Design and Development Overlay Schedule 9 (DDO9 – Doonside Precinct) to the site; and

   (c) apply an Environmental Audit Overlay to the site.

5. The proposed application of DDO9 limits controls to delivering built form outcomes only. Council officers assessed the initial request and worked with the proponent to propose a different planning tool, the Development Plan Overlay, in the amendment to facilitate the delivery of:
   (a) built form that responds to its context and heritage items on site;
   (b) public open space and a pedestrian link;
   (c) affordable housing;
   (d) intersection upgrade (Doonside and Burnley Streets); and
   (e) commercial floor space to maintain employment generating uses on site.
6. The amendment proposes to:
   (a) rezone the land from Industrial 3 to Mixed Use Zone;
   (b) introduce the Environmental Audit Overlay (which addresses soil contamination); and
   (c) introduce a Development Plan Overlay (DPO) that allows development which accords with an approved development plan.
7. The Development Plan Overlay is included as Attachment 1 to this report.
8. The proposal facilitates a development of this 1.3 hectares site for residential, commercial and open space, with:
   (a) approximately 506 dwellings built in six towers ranging in height between 5 and 7 storeys fronting Appleton Street up to 12 storeys along Doonside Street;
   (b) 576 square metres of public open space facing Doonside Street;
   (c) a 9 metre wide pedestrian link which runs through the site adjoining the public open space;
   (d) 5% of dwellings to be for affordable housing;
   (e) retention of heritage buildings including the entire building at 26 Doonside Street; and
   (f) at least 7,000 square metres of commercial / retail floor space (part of which will see the “Harry the Hirer” administrative headquarters remain on site).
9. Attachments 2-4 to this report show artistic representations of how the development could look in terms of scale and height. The final architectural detail would be dealt with at planning permit stage. Attachment 5 shows a landscape concept.

Issues

Strategic justification of the amendment

10. There is strong policy support for a rezoning within the Municipal Strategic Statement of the Yarra Planning scheme (YPS):
    (a) in Clause 21.03 the site is identified as a strategic redevelopment site; and
    (b) figure 21, Neighbourhood Map: North Richmond specifically identifies the site to be rezoned to mixed use zone.
11. The proposed amendment completed the rezoning of the ‘missing part’ of the Doonside Precinct that was rezoned in 2009.
12. The proposed amendment is consistent with the Victoria Street Structure Plan, in which the land is included in Precinct 11a *Doonside Precinct – potential housing mixed with retail and business*.

DDO9 vs proposed DPO

13. A *Development Plan Overlay* is a planning tool that is appropriate for areas which require the form and conditions of future use and development to be shown on a development plan before a permit can be granted to use or develop the land. Council has applied this provision to a number of sites previously: DPO5 Channel 9 Site, Bendigo St, East Richmond; DPO 11 Amcor Site, Alphington.

14. The DPO has been used as it allows for controls to deliver public benefits set out in paragraph 5. DDO9 only provides built form controls.

15. The proposed Development Plan vision reflects the design objectives in DDO9 and provides for additional heritage outcomes.

16. The proposed DPO includes, among other things:

    (a) height and setback requirements that reflect those in DDO9;

    (b) over-shadowing restrictions to protect the existing amenity of residents along Appleton Street;

    (c) protection of the future amenity of the proposed pedestrian link;

    (d) a requirement for high quality architecture; and

    (e) a heritage impact statement.

Built Form

17. Council officers have sought urban design and heritage advice in assessing the proposed built form outcomes. Heritage is addressed in a section below. The composition of elements has regard to the existing context, future residential amenity, heritage, amenity of open space and avoiding amenity impacts on neighbouring properties.

18. The proposed street wall heights respond to the heritage street wall along Burnley Street and wrap around into Doonside and Appleton Streets. The buildings above street wall height provide a 9 metre separation between each building, allowing for good amenity for future residents. Importantly they also scale down from the taller buildings along Doonside and Burnley Streets to the existing low scale residential development along Appleton Street. The new buildings fronting Appleton Street have a preferred height limit of 5 – 7 storeys. Their upper levels along Appleton Street are set back at least 13 metres which is consistent with the approved development on the adjoining lot. This substantial setback also mitigates overshadowing impacts and reduces their visibility from residential properties on Appleton Street. The buildings along Doonside Street are 8, 11 and 12 storeys.

19. The heights are consistent with approved development on surrounding properties. Land to the north in Doonside Street is zoned mixed use and Victoria Gardens shopping centre is located to the north east so there are no amenity concerns in Doonside Street.

19. Additional provisions in the proposed DPO to reduce impacts from the development include minimising vehicle movements in Appleton Street, screening of car parking so that it is not visible from the street, minimising the impacts of wind effects from the development and ensuring that there are active street frontages on Burnley Street and facing the pedestrian link.

Heritage

20. Part of the site (land adjacent to Burnley Street) is covered by HO 375 (81-95 Burnley Street). This was the Russell Manufacturing Company Pty Ltd, and later Repco. HO252 covers 26 Doonside Street Richmond (Former Repco offices).

21. The heritage facade (former Repco headquarters) on Burnley Street would be retained as would the heritage building in its entirety at 26 Doonside Street.

22. Council officers obtained expert heritage advice from GJM Consulting on the proposal. The recommendations from GJM were incorporated into the proposed amendment, particularly in relation to:

    (a) siting of the buildings from the heritage building at 26 Doonside Street;

    (b) proposed heights and upper level setbacks at 81-95 Burnley Street; and

    (c) requirements in the Development Plan Overlay, including the requirement for a Heritage Impact Statement and built form guidelines.

Traffic

23. Primary access to the site is proposed from Doonside Street. Appleton Street will not be used for primary traffic movements. A single, secondary vehicle entrance/exit is proposed along Appleton Street at the furthest point from the Burnley / Appleton Streets intersection.

24. The proponent provided a traffic report which has been reviewed in detail by Council's traffic consultants and by Vic Roads.

25. Initial assessment from VicRoads indicated that the intersection of Doonside and Burnley Streets would need to be signalised as a result of the cumulative traffic impacts of developments in area. VicRoads indicated that signalisation of the intersection should include Buckingham Street.

26. Council's traffic consultants advised that the intersection of Doonside and Burnley Streets would need to be signalised as a result of the cumulative traffic impacts of developments in area. Further investigation of the design of this intersection is required. The proposed DPO *includes the need for a Traffic Impact Assessment Report (TIAR) to include consideration of any development stages and approved/current development applications within the immediate area surrounding the site. The TIAR is to:*

    (a) identify mitigating works required for each development stage in the Development Plan;

    (b) assess whether a two way or a four way signalised intersection between Burnley Street/Doonside Street/Buckingham Street is required and the trigger for providing the signalised intersection; and

    (c) identify a new intersection layout and operation, if required, approved by VicRoads in consultation with the Responsible Authority.

Open space and landscaping

27. The proposal includes public open space along Doonside Street, communal open space for residents at first floor level and provision for private open space. Attachment 4 provides a landscape concept plan for the public and communal open space as well as off site, streetscape landscaping.

28. The site is included in a location where land contribution is preferred to a cash equivalent in Clause 22.12 Public open space contribution.

29. Clause 52.01 of the Yarra Planning Scheme obliges the proponent to make a contribution of open space of either 4.5% of the land area or 4.5% of the value of the land where the land or buildings are intended to be used for residential purposes. The proposed DPO proposes 576 square metres of public open space on Doonside Street which meets the open space requirement.

30. In addition to the 576 square metres of public open space, the proposal also offers a 9 metre wide pedestrian link that runs through the site and would allow pedestrian access from Doonside to Appleton Street. The link would receive sunlight between 10am and 2pm at the equinox.

Affordable housing/ housing diversity

31. The proposed Development Plan requires that the proponent provides 5% of the overall number of dwellings as affordable housing. This would be secured by way of a Section 173 Agreement being included on the titles which must be entered into prior to the issue of the planning permit being issued in accordance with the Development Plan. All costs of the agreement, including Council's costs, would be met by the proponent.

32. The proposed 5% is consistent with Council's adopted Policy Guidance Note for Affordable Housing in Significant Redevelopments (November 2017). The Policy Note outlines Council's expectations for developers in the municipality to dedicate at least 5% of significant new developments to affordable housing. The Policy applies to rezoning sites of 50 + dwellings.

33. The Development Plan requires the preparation of a housing diversity report, the purpose of which is to undertake a demographic analysis of the types of people and households anticipated to live within the development and demonstrate how the development plan responds to the particular housing needs of the occupants across their lifetime - proposed dwelling design and bedroom mix.

Retaining land for employment

34. Ongoing employment would be generated as a result of the retention of "Harry the Hirer", inclusion of small scale retail / commercial opportunities and home office housing product.

35. The proposed Development Plan Overlay mandates at least 7,000 square metres of gross floor area will be for employment. The administrative headquarters of the "Harry the Hirer" business will remain on the site. There is also provision made for home offices to be built fronting the pedestrian link.

36. The total net employment increase arising from the development is estimated at around +135 jobs based on retention of around 100 "Harry the Hirer" staff on site; new small scale retail / commercial opportunities and an estimated 3% of residents employed at home.

Environmental Audit Overlay

37. Where land is rezoned from an industrial use to a residential use, the Environmental Audit Overlay (EAO) is applied so that there is rigorous assessment of any soil contamination at the site and any contamination can be suitably addressed.

38. An *Environmental Site Assessment*, prepared by JBS&G, was submitted with the amendment request. The Preliminary and Detailed Site Investigations were undertaken and identified that impacted materials could be managed through removal or suitable capping of the site.

39. Ministerial Direction No. 1 (Potentially Contaminated Land) states:

    (a) In preparing an amendment which would have the effect of allowing (whether or not subject to the grant of a permit) potentially contaminated land to be used for a sensitive use, agriculture or public open space, a planning authority must satisfy itself that the environmental conditions of that land are or will be suitable for that use.

40. This use of the EAO satisfies the requirements of the Ministerial Direction.

External Consultation

41. Officers sought the view of Vic Roads in relation to traffic.

42. If Council resolves to seek '*authorisation'* for the amendment and it is approved by DELWP for formal exhibition, the amendment documentation would be exhibited for a period of one month in accordance with the requirements of the *Planning and Environment Act 1987*. This includes giving notice to those considered to be materially affected and would include all owners and occupiers of land within and immediately adjoining the amendment area. This would provide the community an opportunity to submit on the proposed controls.

43. In addition to the statutory notification, the proponent proposes to undertake targeted consultation with residents immediately adjoining the site, prior to the formal exhibition of the amendment. The proponent has advised that their engagement program may include:

(a) circulation of information;

(b) community drop in days;

(c) community stakeholder meetings;

(d) project updates via a newsletter, website or social media; and

(e) creating an engagement report.

44. If the amendment is approved, a Development Plan needs to be approved by the Responsible Authority. The proposed DPO includes a provision that: *The Development Plan shall be available for public inspection and submission for 28 days prior to its consideration by the responsible authority. Any submissions must be considered by the responsible authority in its decision*. NB. Once the Development Plan is approved, a planning permit process is not subject to third party notification and appeal.

Internal Consultation (One Yarra)

45. Officers have sought internal comments from urban design, engineering – drainage and traffic, sustainable transport (cycling) and statutory planning. These comments have informed the requirements of the proposed Development Plan Overlay.

Financial Implications

46. The costs associated with the exhibition of the amendment, statutory fees, and panel fees would be met by the proponent. Other aspects would be met by the strategic planning budget.

Economic Implications

47. The proposed Development Plan Overlay mandates approximately 5,500 square metres of gross floor area would be for employment generating uses.

Sustainability Implications

48. The proposed DPO provides for a mix of additional housing in proximity to employment generating uses on site and public transport.

Social Implications

49. There are no anticipated social implications.

Human Rights Implications

50. There are no anticipated human rights implications.

Communications with CALD Communities Implications

51. If Council agrees to request the Minister for Planning to authorise the amendment, its public exhibition would occur around Easter next year. Notification and consultation about the amendment would include advice about the use of the interpreter service by residents. This would be available to help affected parties to understand the proposal and associated processes. The Amendment process also involved steps outlined in the Council engagement strategy to assist CALD communities.

Council Plan, Strategy and Policy Implications

52. The amendment is consistent with the Council plan objective 3: *Making Yarra More Liveable*.

<u>Business and Industrial Land Strategy</u>

53. In 2009, Amendment C99 rezoned all industrial land in the vicinity - except this site. Following the amendment, the Business and Industrial Land Strategy (BILS) was prepared in 2011 and adopted in 2012. As the land at 81-85 Burnley Street had been excluded from Amendment C99, the BILS recommended that the site be rezoned to Business 3 Zone which was converted to Commercial 2 Zone when the commercial zones were reformed.

54. This is at odds with the Municipal Strategic Statement (MSS) at Clause 21 of the planning scheme which clearly states that this land should be rezoned to Mixed Use.

55. The BILS is not part of the planning scheme – it is not an incorporated document, nor is it a reference document and it has not been translated into any part of the planning scheme. The MSS has statutory weight.

56. Importantly, the proposed amendment facilitates a development that has at least 7,000 square metres of gross floor area for employment generating uses.

Victoria Street Structure Plan, adopted 2010

57. As outlined in paragraph 11 the proposal is in Precinct 11 of the Structure Plan, which is identified as a substantial change area and having the potential for mixed use development.

Legal Implications

58. The amendment would be processed and considered in accordance with the provisions of the *Planning and Environment Act 1987*.

Other Issues

59. There are no other issues.

Options

60. There are two options for deciding on this proposed amendment:

    (a) option 1: seek authorisation from the Minister for Planning for the amendment to go on public exhibition. As outlined in this report there is strong strategic direction within the Yarra Planning Scheme to support a rezoning of the land; or

    (b) option 2: not support the rezoning and abandon the amendment. This is contrary to the strategic direction in the Planning Scheme.

Conclusion

61. Proposed Amendment C223 seeks to rezone land at 81-95 Burnley and 26 Doonside Streets, Richmond (Harry the Hirer) from Industrial 3 to Mixed Use Zone and apply a Development Plan Overlay and an Environmental Audit Overlay.

62. This is a proponent led amendment that facilitates the redevelopment of an industrial site to a mid-rise, mixed use development that provides the following benefits:

    (a) commercial floor space to maintain employment generating uses on site;

    (b) open space with a pedestrian link;

    (c) intersection upgrades (Doonside and Burnley Streets);

    (d) retain the former Repco heritage façade on Burnley Street and retain the heritage building at 26 Doonside Street; and

    (e) provision of 5% affordable housing.

63. There is strong strategic support in the planning scheme to see this land rezoned from Industrial 3 to Mixed Use Zone.

RECOMMENDATION

1. That Council:
   (a) note the officer report on Amendment C223 which seeks to rezone land at 81-95 Burnley and 26 Doonside Streets, Richmond from Industrial 3 to Mixed Use Zone and apply a Development Plan Overlay and an Environmental Audit Overlay;
   (b) note the analysis in the officer report on various aspects including the matter of affordable housing provision as a component of the proposal;
   (c) pursue a section 173 agreement requiring 5% of the overall number of dwellings in the approved Development Plan as affordable housing in association with an accredited Housing Association; and
   (d) seek authorisation from the Minister for Planning to prepare Amendment C223 to the Yarra Planning Scheme in accordance with section 8A of the *Planning and Environment Act 1987*.
2. If authorisation is granted officers arrange for the exhibition of Amendment C223 in accordance with Section 19 the requirements of the *Planning and Environment Act 1987.*

CONTACT OFFICER: **Amanda Haycox**
TITLE: **Strategic Planner**
TEL: **9205 5322**

Attachments
1 Draft DPO Am C223 for Council meeting 19 Dec 2017 seeking authorisation
2 Planning report Tract Amendment C223
3 Heritage Report Bryce Raworth Amendment C223
4 Traffic Report Ratio Amendment C223
5 Explanatory Report Am C223 for Council 19 Dec 2017 seeking authorisation
6 Appleton Street, Burnley Street and Doonside Render Am C223

--/--/2017
Proposed C223

SCHEDULE 15 TO THE DEVELOPMENT PLAN OVERLAY

Shown on the planning scheme map as **DPO15.**

81-95 BURNLEY STREET & 26-34 DOONSIDE STREET, RICHMOND

1.0 Requirements before a permit is granted

--/--/2017
Proposed C223

A permit may be granted for the following before a development plan has been approved:

- Buildings or works necessary for existing businesses or uses to continue.
- Consolidation or subdivision.
- Removal or creation of easements or restrictions.
- Demolition or removal of buildings.
- The construction or carrying out of minor buildings or works, including site preparation.
- Buildings and works associated with or for the purpose of obtaining a certificate or statement of environmental audit under the Environment Protection Act 1970, where these works do not prejudice the preparation and approval of the Development Plan and the vision for the land set out in this overlay.

Before granting a permit the responsible authority must be satisfied that the permit will not prejudice the future use and development of the land and will not compromise the objectives for the site as set out in this schedule.

1.1 Section 173 Agreement to provide for affordable Housing

The owner (or another person in anticipation of becoming the owner) must enter into an agreement with the responsible authority under section 173 of the Planning and Environment Act 1987 which requires that the owner must facilitate the provision of affordable housing by:

- Entering into an arrangement with a state government accredited housing association in respect of 5 percent of the total number of dwellings (across the whole of the site as set out in the approved Development Plan for the site whereby those dwellings are to be purchased or managed by that accredited housing association) ; or
- Making other arrangements for the provision of affordable housing by a Not for Profit (registered with the Australian Charities and Not-for-profits Commission) in respect of the same number of dwellings to the satisfaction of the responsible authority;
- The owner, or other person in anticipation of becoming the owner, must meet all of the expenses of the preparation and registration of the agreement, including the reasonable costs borne by the Responsible Authority.

The Section 173 Agreement must be entered into prior to a planning permit being issued in accordance with the approved Development Plan.

2.0 Conditions and requirements for Permits

2.1 Permit requirements

--/--/2017
Proposed C223

Except for a permit granted in accordance with Clause 1.0 of this Schedule, a permit must contain conditions that give effect to the provisions and requirements of the approved development plan.

2.2 Heritage Impact Statement

A permit application must include, where relevant:

- A heritage impact statement prepared by a suitably qualified professional that assesses the impact of the proposed development on the heritage values of the heritage place and nearby heritage places, as identified in the conservation management plan or similar comprehensive heritage analysis prepared for the site, along with relevant heritage studies and citations.

  A siteline analysis and 3D modeling of the proposed development from key view points in the public realm to enable an assessment of the visual impact of the development on heritage places

3.0 Requirements for Development Plan

--/--/2017
Proposed C223

A development plan must be generally in accordance with the *Indicative Framework Plan* as shown in Figure 1, and the vision set out in this schedule, to the satisfaction of the responsible authority.

A development plan must be approved for the whole site, however the land may be developed in stages.

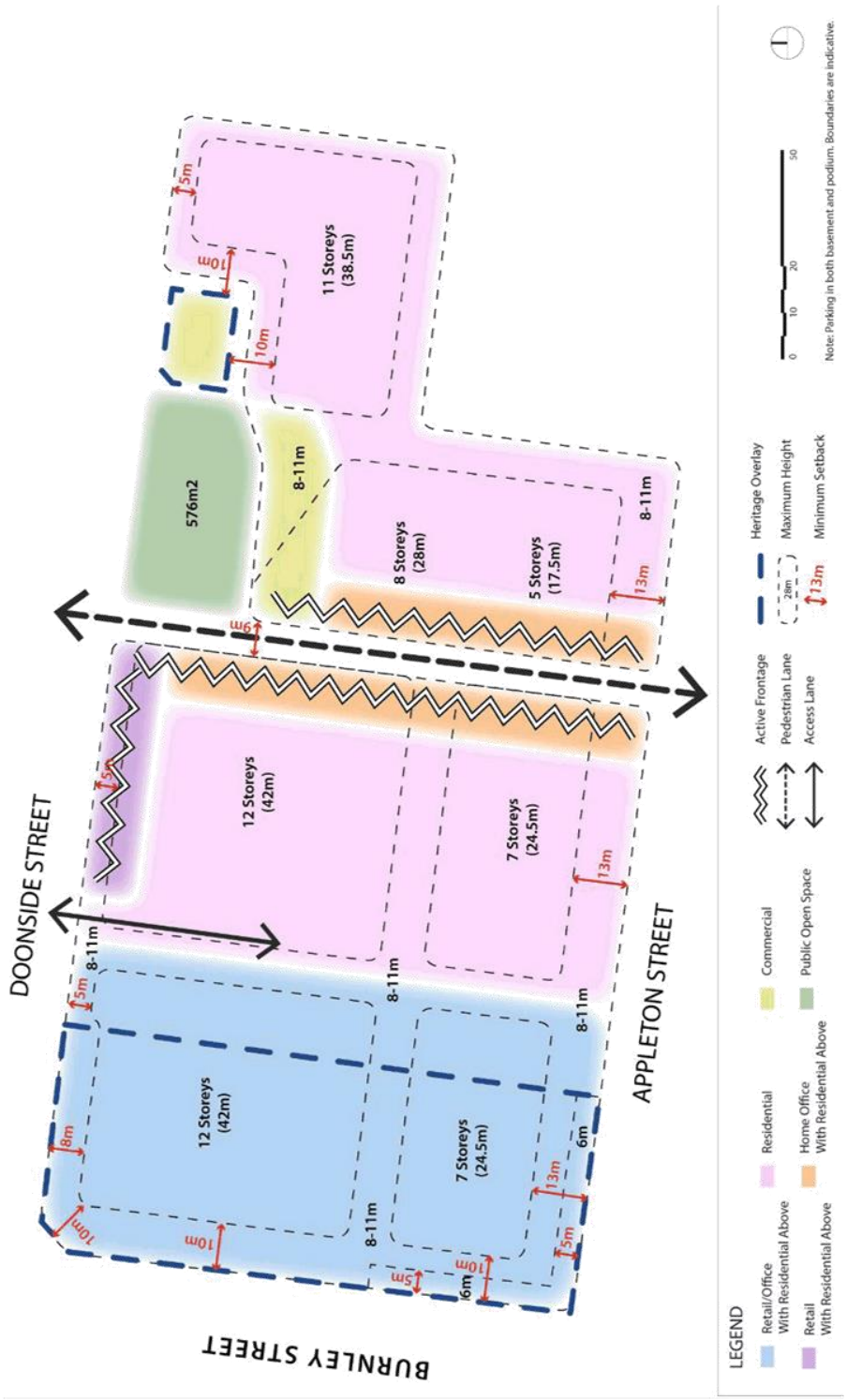
The development plan must include the following sections, all prepared to the satisfaction of the responsible authority:

3.1 Development Plan Vision

- To become a sustainable, mixed-use residential community, supported by convenience retailing services, community facilities, and employment opportunities augmenting the role of the Victoria Street Activity Centre;
- To recognise the opportunity of the site's activity centre context, whilst respecting the low rise residential development to the south;
- To protect the amenity of residential properties on the south side of Appleton Street;
- To provide improvements to the public domain, including pedestrian friendly environments along all street frontages, the provision of public open space and a pedestrian laneway;
- To provide a high standard of internal amenity, building separation and environmentally sustainable design;
- To respect the scale and form heritage places within and adjacent to the site and provide for the conservation of heritage places within the site;
- To ensure that new development mitigates any adverse impact it may generate t upon local traffic conditions; and
- To provide for the sensitive adaptive re-use of heritage buildings in accordance with the Indicative Framework Plan and informed by a comprehensive heritage analysis prepared for the site by a suitably qualified professional that:
  - articulates the significance of the heritage place, its component parts and its setting;
  - describes the relationship between the heritage place and any neighbouring or adjacent heritage place/s; and
  - establishes principles for managing the significance of the heritage place and its relationship with its surroundings.

Figure 1 Indicative Framework Plan

DRAFT


DOONSIDE STREET
APPLETON STREET
BURNLEY STREET
576m2
11 Storeys (38.5m)
8 Storeys (28m)
5 Storeys (17.5m)
12 Storeys (42m)
7 Storeys (24.5m)
12 Storeys (42m)
7 Storeys (24.5m)
8-11m
13m
9m
5m
10m
8m
6m
LEGEND
Retail/Office With Residential Above
Residential
Commercial
Public Open Space
Retail With Residential Above
Home Office With Residential Above
Active Frontage
Pedestrian Lane
Access Lane
Heritage Overlay
Maximum Height
Minimum Setback
Note: Parking in both basement and podium. Boundaries are indicative.

3.2 Components of the Development Plan

Site and Context Information

A site analysis that identifies:

- the key attributes of the land and its context;
- existing or proposed uses on adjoining land;
- other neighbourhood features such as public transport, activity centres, walking and cycling connections; and
- important views to be considered and protected, including views of existing heritage buildings.

Concept Plans

Concept plans must include:

- Set out the total number of dwellings across the entire site;
- The proposed use of each building and estimated floor area for each use;
- At least 7,000m^2 of GFA provided for employment generating activities;
- An indication of the location and approximate commercial and retail yield for the site;
- A north south pedestrian link:
  - with a minimum width of 9 metres;
  - that provides a safe and pleasant pedestrian and cycling link between Doonside Street and Appleton Street;
  - that receives sunlight between 10am and 2pm at the equinox; and
  - that remains publicly accessible in perpetuity.
- The provision of at least 4.5% of the total site (576 square metres) for open space which fronts Doonside Street and adjoins the pedestrian link. The plan must show the area of public open space in square metres and its percentage of overall site area; and
- Vehicular, pedestrian, cyclist and loading access points and connections.

Built Form Guidelines

Built form guidelines which provide the following:

- Maximum building heights and envelopes responding to the site context;
- Building setbacks from street boundaries that ensure that new future development does not overwhelm the scale of the heritage buildings on the site or on heritage places in the vicinity of the site, including dwelllings on the south side of Appleton Street;
- Building setbacks from the facades of 81-95 Burnley Street that ensure the heritage building can be understood as having a three dimensional form;
- Minimum upper level (above podium) setbacks of:
  - 13 metres from the Appleton Street site boundary
  - 10 metres from the Burnley Street site boundary
  - 8 and 5 metres from the Doonside Street site boundary
- Ensure new buildings are well spaced (minimum of 9 metres between building towers);

- Buildings set back a minimum of 10 metres (above podium) from the heritage building at 26-34 Dooside Street;
- Inter-floor heights within the heritage buildings on the site to ensure they relate to the existing floor levels and/or fenestration patterns;
- Active frontages to Burnley Street, open space and pedestrian link as appropriate;
- Massing diagrams that model the proposed built form envelopes based on the indicative heights and setbacks;
- Shadow diagrams that demonstrate:
  - no unreasonable overshadowing of Doonside Street public open space area and
  - no overshadowing of private properties on the southern side of Appleton Street beyond that caused by a building of 11m when measured between the hours of 10:00am and 2:00pm at the September Equinox.
- Indicative palette of building materials and architectural treatments throughout the site. The design and use of materials must be respectful of the industrial heritage of the site and its surrounds to the north and east, as well as to the residential heritage to the south.
- Provide for high quality architecture and spaces through the site and respond to heritage places through, as appropriate:
  - Use of lightweight materials
  - Simple architectural detail so as not to detract from significant elements of heritage buildings
  - Discouraging highly articulated facades with recessed and projecting elements
  - Ensuring the retention of solid built form behind retained facades and avoiding balconies behind existing openings
- Ensure car parking is screened by buildings and not clearly visible from the street or otherwise located in basement areas;
- Ensure buildings are designed to ameliorate adverse wind conditions at street level, public spaces and lower level dwellings;
- Ensure buildings are designed along Appleton St to break up the form of the street wall.
- Guidelines to mitigate advserse impacts of wind effects in building design.
- Minimise vehicle access and traffic movements in Appleton Street.

Open Space and Landscape

A Landscape Concept Plan must be prepared that provides:

- Dimensions of open space to the satisfaction of the responsible authority;
- An overall landscape masterplan for the site that includes landscape concepts for proposed open space and improvements along Appleton Street and Doonside Street;
- A written description of the management of the open space, pedestrian link and other landscaped areas, including sustainable irrigation principles such as water sensitive urban design opportunities; and
- Details of how the Landscape Concept Plan responds to any requirements of the site remediation strategy for the land.

Housing Diversity Report

A Housing Diversity and Adaptability Report must be prepared which provides the following information:

- A demographic analysis of the types of people and households anticipated to live within the development based on the proposed dwelling design and bedroom mix
- The model to provide 5% of the overall housing stock as affordable housing
- Demonstrate how the development plan responds to the particular housing needs of future residents across their lifetime.

Economic Assessment

An economic assessment must be prepared which identifies, as appropriate, viable employment generating uses for the site.

Transport Assessment

A Traffic Impact Assessment Report (TIAR) prepared by a suitably qualified traffic engineer to the satisfaction of the Responsible Authority. The views of VicRoads must be considered prior to the approval of the Traffic Impact Assessment. The Traffic Impact Assessment must include and demonstrate the following:

- An existing conditions assessment.
- Details of any development staging.
- A site layout plan showing convenient and safe primary vehicle access, including:
  - Primary vehicle access to and from Doonside Street,
  - Any vehicle access to Appleton Street to be a secondary access point.
  - No direct vehicle access to or from the site via Burnley Street.
- Details regarding the layout, cross section and function of any internal street or laneway network.
- On site car parking and bicycle parking provisions and allocations.
- Expected traffic volumes and impact on the existing road network, including but not necessarily limited to Doonside Street, Appleton Street and Burnley Street. This assessment is to include details of any assumptions relied upon.
- The TIAR is to include consideration of any development stages and approved/current development applications within the immediate area surrounding the site. The assessment is to:
  - identify mitigating works required for each development stage in the Development Plan
  - assess whether a two way or a four way signalised intersection between Burnley Street/Doonside Street/Buckingham Street is required and the trigger for providing the signalised intersection
  - identify a new intersection layout and operation, if required, approved by VicRoads in consultation with the Responsible Authority.
- Details of any works or treatments proposed to Doonside Street or Appleton Street or the nearby road network.
- Details regarding the impact on pedestrian and bicycle routes.
- Measures to reduce conflict and improve pedestrian and bicycle amenity.
- Details regarding loading arrangements, with loading to be undertaken on site and conflict between the loading bay(s) and car parking areas and non-motorised transport to be minimised.
- Access to the site by trucks is to be via Doonside Street.

- Details regarding on-site waste collection, with waste vehicles accessing the site from Doonside Street.

Green Travel Plan

- A Green Travel Plan must be prepared to the satisfaction of the Responsible Authority prepared by a qualified traffic engineer.

Environmentally Sustainable Design (ESD)

An environmentally sustainable design assessment which sets out how future development may achieve:

- WSUD objectives and requirements pursuant to the planning scheme; and
- ESD objectives and requirements pursuant to the planning scheme.

Drainage

A drainage assessment which includes:

- A catchment analysis of the existing storm water drainage system in Burnley Street and Doonside Street;
- A capacity assessment for the existing drainage system into which future development will be discharged; and
- A flood analysis which determines the overland flow depth within the road reserve during a 1 in 100 year flood.

Heritage

A heritage impact statement prepared by a suitably qualified professional that:

- Assesses the impact of the proposed development on the heritage values of the heritage place and nearby heritage places,
- Addresses the retention, restoration, redevelopment and adaptive reuse of the heritage buildings (81-95 Burnley Street and 26-34 Doonside Street);, and heritage façade (Burnley Street);
- Assesses the impacts on the context and setting of heritage places in the vicinity of the site;
- Addresses the retention, recording and interpretation of links to the site's industrial past; and
- Provides a siteline analysis and 3D modeling of the proposed development from key view points in the public realm to enable an assessment of the visual impact of the development on heritage places, in particular 26 Doonside Street.

Development Staging

A staging plan to provide an indication of the likely staging of the development of land, specifically:

- The expected sequencing of development;
- Likely vehicle access points, road infrastructure works and traffic management; and
- Interface/access treatments.

Community consultation

The Development Plan shall be available for public inspection and submission for 28 days prior to its consideration by the responsible authority. Any submissions must be considered by the responsible authority in its decision.

Attachment 2 - Planning report Tract Amendment C223

81-95 Burnley Street and 26-34 Doonside Street, Richmond
The 'Harry the Hirer' Site
Planning Scheme Amendment C223 Report

0316-0151 R002 December 2017

Prepared by **Tract Consultants**

- **Town Planning** - Tract Consultants
- **Urban Design** - Tract Consultants
- **Landscape Architecture** - Tract Consultants
- **3D Media** - Tract Consultants
- **Transport** - Ratio Consultants
- **Heritage** - Bryce Raworth
- **Economic & Employment** - Deep End Services
- **ESD** - GIW Consultants

Tract
Landscape Architects
Urban Designers
Town Planners

ratio:

deep end services

GIW environmental SOLUTIONS

bryce raworth
conservation • urban design

Attachment 2 - Planning report Tract Amendment C223

Contents

01 Introduction 7
- 1.1 Background 7
- 1.2 Planning Scheme Amendment Details 7
- 1.3 Report Structure 7
- 1.4 Harry the Hirer Business 7

02 Site Analysis 8
- 2.1 Site Location 8
- 2.2 Site Features 10
- 2.3 Local Services 11
- 2.4 Surrounding Developments 12
- 2.5 Site Opportunities and Constraints 13
- 2.6 Site Interfaces 18

03 Planning Provisions 20
- 3.1 State Planning Policy Framework (SPPF) 20
- 3.2 Local Planning Policy Framework (LPPF) 22
- 3.3 Zone 26
- 3.4 Overlays 26
- 3.5 Particular Provisions 28
- 3.6 Amendment C99 to the Yarra Planning Scheme 28
- 3.7 Other Planning Considerations 28
- 3.8 Ministerial Direction No. 1 - Potentially Contaminated Land 32
- 3.9 Ministerial Direction No. 11 - Strategic Assessment of Amendments 32
- 3.10 Policy Summary 32

04 The Proposal 33
- 4.1 Overview 33
- 4.2 Vision 33
- 4.3 Key Features 34
- 4.4 Built Form 36
- 4.5 Heritage 38
- 4.6 Access 39
- 4.7 Interfaces 40
- 4.8 Staging 42
- 4.9 Planning Scheme Changes 42

05 Landscape Architecture 44
- 5.1 Vision 44
- 5.2 Open Space Analysis 45
- 5.3 Key Design Elements 45

06 Heritage 52
- 6.1 Current Listings and Controls 52
- 6.2 History 52
- 6.3 Significance 52
- 6.4 Analysis 53

07 Transport 54
- 7.1 Overview 54
- 7.2 Strategic Background 54
- 7.3 Road Network, Traffic and Parking Conditions 54
- 7.4 Sustainable Transport 55
- 7.5 Recommended Parking Rates 56
- 7.6 The Proposal 56

08 Economic & Employment 58
- 8.1 Overview 58
- 8.2 Economic Context 58
- 8.3 Economic Outcomes 59
- 8.4 Summary 59

09 Environmentally Sustainable Design 60
- 9.1 Introduction 60
- 9.2 ESD State Policy 60
- 9.3 ESD Local Policy 61
- 9.4 ESD Yarra Planning Scheme 62
- 9.5 ESD Proposed Measures 62

010 Conclusion 64

Attachment 2 - Planning report Tract Amendment C223

Figures

Figure 1 Doonside Street Perspective 4
Figure 2 Site Context Map 8
Figure 3 Context Plan 9
Figure 4 Aerial Plan 10
Figure 5 Site Location Plan 11
Figure 6 Surrounding Development Plan 12
Figure 7 Site Opportunities and Constraints 13
Figure 8 Heritage Analysis 14
Figure 9 Interface Analysis 15
Figure 10 Traffic Analysis 16
Figure 11 Pedestrian Analysis 17
Figure 12 Northern Interface Images 18
Figure 13 Eastern Interface Images 18
Figure 14 Southern Interface Images 19
Figure 15 Western Interface Images 19
Figure 16 Plan Melbourne Inner Metro Region 21
Figure 17 Residential Development Opportunities Map (Yarra PS) 23
Figure 18 Built Form Character Map (Yarra PS) 23
Figure 19 Neighbourhood Character Map (Yarra PS) 25
Figure 20 Victoria Street East Precinct Framework Plan (Yarra PS) 25
Figure 21 Zone Plan 26
Figure 22 Design and Development Overlay Plan 27
Figure 23 Heritage Overlay Plan 27
Figure 24 Areas of Change (Victoria Street Structure Plan) 29
Figure 25 Future Activity (Victoria Street Structure Plan) 29
Figure 26 Pedestrian Circulation - Local Network (Victoria Street East Precinct UDF) 30
Figure 27 Ground Level Building Setbacks (Victoria Street East Precinct UDF) 31
Figure 28 Mixed Use Development Opportunities (Victoria Street East Precinct UDF) 31
Figure 29 Built Form Examples 33
Figure 30 Key Features Plans (Areas 1, 2 and 3) 34
Figure 31 Indicative Framework Plan 35
Figure 32 Public Open Space Indicative Detail 37
Figure 33 Original Repco building facade 38
Figure 34 Heritage Plan 38
Figure 35 Pedestrian Access 39
Figure 36 Vehicle Access 39
Figure 37 Burnley Street Indicative Section 41
Figure 38 Indicative Burnley Street Section 43
Figure 39 Public Open Space Context Plan 44
Figure 40 Key Design Elements Plan 45
Figure 41 Ground Level Landscape Opportunities 50
Figure 42 Roof Gardens Landscape Opportunities 51
Figure 43 1930 Plan of Subdivision for Doonside Estate 52
Figure 44 1943 Illustration of Repco Building 53
Figure 45 1957 Photograph of Repco Building 53
Figure 46 Public Transport Provision 55
Figure 47 Nearest Share Car Pod Locations 55
Figure 48 Indicative Recommended Parking Rates 56
Figure 49 Preliminary Yield Analysis 57
Figure 50 Indicative Tower Locations 57
Figure 51 Dwelling approvals by type between July '14 to September '17 58
Figure 52 Ongoing (on-site) employment estimates 59

Attachment 2 - Planning report Tract Amendment C223

Figure 1 Doonside Street Perspective

Attachment 2 - Planning report Tract Amendment C223

Attachment 2 - Planning report Tract Amendment C223

Attachment 2 - Planning report Tract Amendment C223

01 INTRODUCTION

1.1 Background

Tract Consultants Pty Ltd acts on behalf of Astrodome Hire Pty Ltd, owner of 81-95 Burnley Street and 26-34 Doonside Street, Richmond.

This report supports a Planning Scheme Amendment request of the Planning and Environment Act 1987 ('the Act') to rezone land known as 81-95 Burnley Street and 26-34 Doonside Street, Richmond (the 'Site') from the Industrial 3 Zone (IN3Z) to a Mixed Use Zone (MUZ).

Locally, the land is known as the 'Harry the Hirer' site, or formerly the 'Repco' site.

It forms part of the Victoria Street Activity Centre which is currently undergoing significant commercial and residential development. The Site comprises approximately 1.3ha of land that is envisaged to be developed for residential, commercial and open space purposes.

The rezoning of the Site to facilitate a mix of uses realises the strategic vision of the City of Yarra in relation to the site, as illustrated in the Municipal Strategic Statement (MSS). It also completes the rezoning of the 'missing part' of the Doonside precinct that was rezoned in 2009.

This rezoning request has arisen from a review of the 'Harry the Hirer' business which indentified the need to shift the warehouse component of the business to Brooklyn, and consolidate and upgrade the showroom and office components on site. This request assists in supporting a major local employer by making more efficient use of its landholding, and solidifying its position in Richmond.

1.2 Planning Scheme Amendment Details

The amendment would affect all of the land formally identified as 81-95 Burnley Street and 26-34 Doonside Street, Richmond. The land consists of 4 land titles, is bounded by Doonside, Burnley and Appleton Street and a private land holding with a permit for high density apartments.

This planning scheme amendment request would:

- Rezone land from Industrial 3 Zone (IN3Z) to Mixed Use Zone (MUZ).
- Apply a Development Plan Overlay (DPO).
- Apply an Environmental Audit Overlay (EAO).

This amendment will provide net community benefits through;

- The retention of a longstanding major local business, and a significant net increase of jobs across the site.
- The creation of new public open spaces, pedestrian laneways and other local amenity improvements.
- The provision of 5% of dwellings to be for affordable housing.
- The restoration of heritage buildings including the entire building at 26 Doonside Street.
- Contribution to the future signalisation of Doonside Street and Burnley Street intersection.
- The provision of additional residential, commercial and retail land uses within an activity centre and walking distance of its catchment.
- Implementation of the policies of the Yarra Planning Scheme and Plan Melbourne.

The amendment request is consistent with the relevant objectives for planning in Victoria, the Plan Melbourne strategy, and the Yarra Planning Scheme.

1.3 Report Structure

This report describes the Site and its context, analyses its opportunities and constraints, details the proposed planning scheme amendment, identifies relevant planning policy, and lastly assesses the proposal's compliance within the identified planning framework.

This town planning rezoning request report has been informed by specialist inputs and reports including;

- Urban Design – Tract Consultants.
- Landscape Architecture – Tract Consultants.
- 3D Media and Visualisations – Tract Consultants.
- Transport – Ratio Consultants.
- Heritage – Bryce Raworth.
- Economic and Employment – Deep End Services.
- ESD – GIW Consultants.

1.4 Harry the Hirer Business

The Site is currently utilised as a showroom, administrative office, warehouse and logistics depot for 'Harry the Hirer' party and marquee hire company and consists of a number of medium to large commercial warehouses.

As a result of changing business conditions, Harrys no longer needs the extensive Burnley Street land for storage and logistics operations, and is relocating these elements to its Brooklyn location. As part of this, they wish to consolidate and upgrade the showroom and office components on site and seek a higher and better use for the remaining areas in line with the Yarra Planning Scheme.

This rationalisation will assist a long standing local business continue to make a positive contribution to the local economy and lead to a net increase in employment on site.

Attachment 2 - Planning report Tract Amendment C223

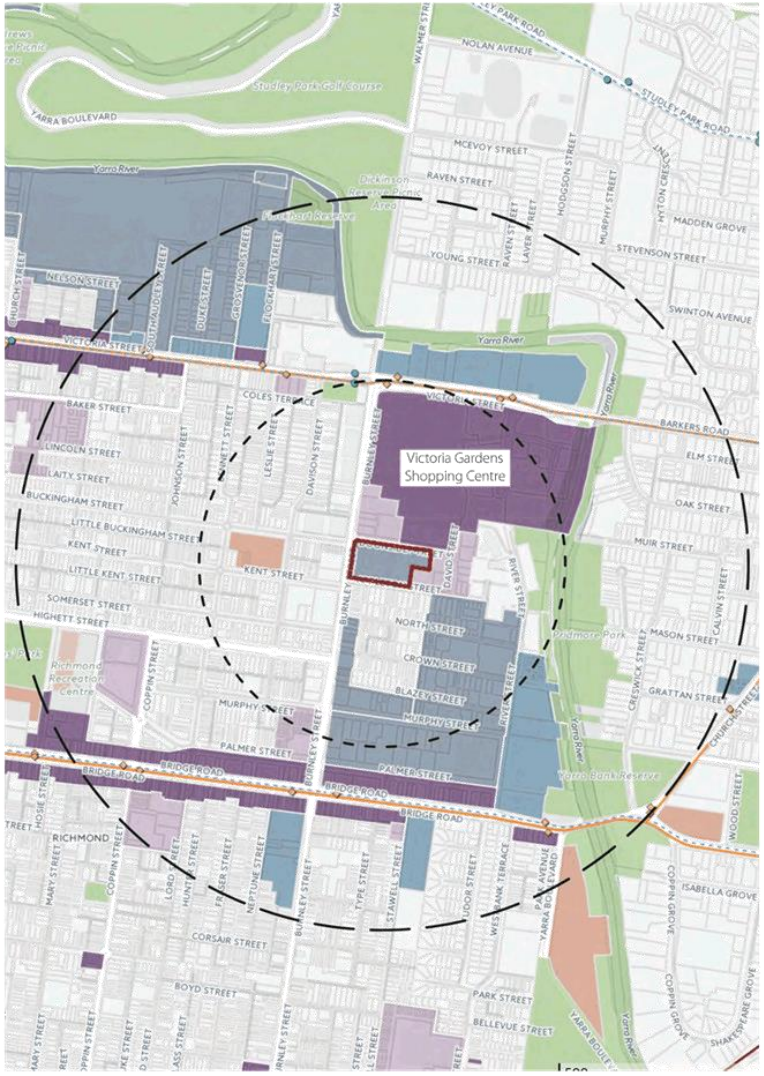
02 SITE ANALYSIS

2.1 Site Location

The subject Site is located within the suburb of Richmond in the inner east of Melbourne (refer to Figure 2 - Site Context Map) . The Site is approximately 3.5km directly east of Melbourne's CBD, and is positioned within the Victoria Street Activity Centre (refer to Figure 3 - Context Plan).

Richmond is an inner suburb generally bordered by Victoria Street to the north, Hoddle Street to the west, Swan Street to the south and the Yarra River to the east. Richmond is characterised by a growing and diverse population and showcases a wide variety of land uses consisting of residential, commercial, retail and industrial. The Site is within close proximity to the Yarra River and numerous commercial and retail services.

The area is in transition from an industrial character to an eclectic mix of uses, focused on residential and retail. The area still features a range of industrial and warehousing businesses, often adjacent to newer residential towers. Many of these are in the process of being redeveloped for retail, office and residential uses.



LEGEND

- SITE BOUNDARY
- 400M RADIUS
- 800M RADIUS

LAND USE

- RETAIL
- MIXED USE
- OFFICE / COMMERCIAL
- INDUSTRIAL
- EDUCATION
- OPEN SPACE

TRANSPORT

- TRAIN STATIONS
- TRAM STOPS
- SMARTBUS STOPS
- BUS STOPS
- TRAIN LINES
- TRAM ROUTES
- SMARTBUS ROUTES
- BUS ROUTES

Figure 2 Site Context Map

Attachment 2 - Planning report Tract Amendment C223

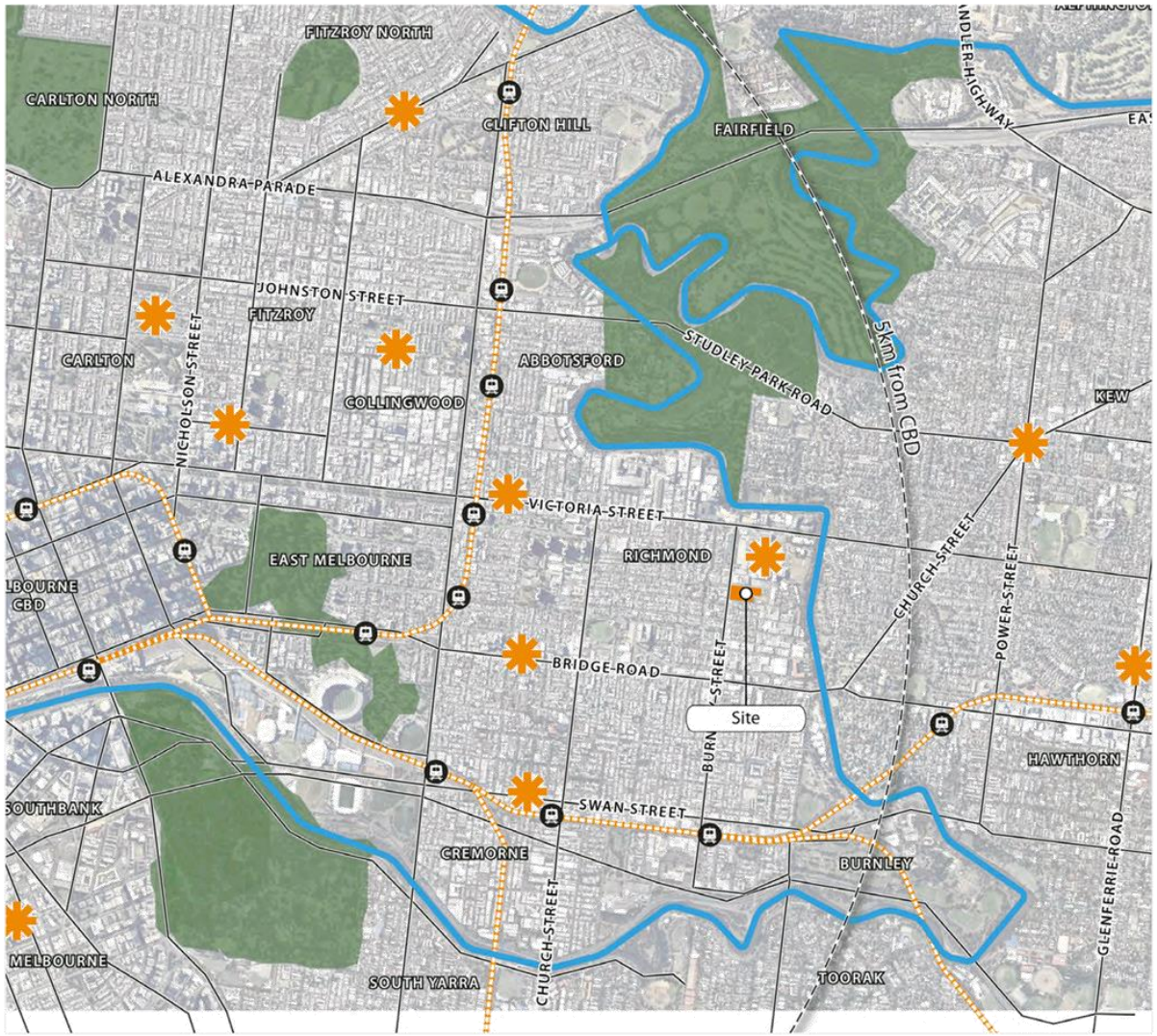


Figure 3 Context Plan

Attachment 2 - Planning report Tract Amendment C223

2.2 Site Features

The Site is approximately 13,000sqm and is located at 81-95 Burnley Street and 26 Doonside Street, Richmond.

The Site consists of 4 titles and is bounded to Burnley Street to the west, Doonside Street to the north and Appleton Street to the south. The Site is formally identified as;

- Lot 1 on PS 317466K (Volume 10142, Folio 918).
- Lot 1 on PS221640U (Volume 10021, Folio 523).
- Lot 2 on PS221640U (Volume 10021, Folio 524).
- Lot 6 on PS 221640U (Volume 10021, Folio 528).

The Site is generally flat in nature with no significant vegetation with the exception of a single large, mature tree centrally located along the northern Doonside Street boundary.

The western portion of the Site is developed with a number of large warehouse buildings used as a showroom and ancillary offices. The eastern portion of the site is used for a variety of storage and logistical operations including goods shed, overflow storage, gas storage and parking (refer to Figure 4 - Aerial Plan).

The fine grain subdivision pattern and scale of buildings in the neighbouring southern areas (Neighbourhood Residential Zone 1 (NRZ1)) contrast with the larger footprints occupied by the subject site and surrounding properties on David Street. The large, existing industrial buildings with a direct interface with these residential properties (along Appleton Street) to the south contribute little to neighbourhood amenity or local streetscapes.

2.2.1 Easement

The Certificate of Title shows an existing easement on the land identified at 26 Doonside Street (refer to Figure 4 - Aerial Plan).

Yarra City Council have issued a planning permit consenting to the easements removal (Permit No. SP16/0038) and consolidation into 2 lots and as a result, a new Plan of Subdivision (with easement removed) is being registered at the Titles Office.

Figure 4 Aerial Plan

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2.3 Local Services

The Site is well serviced by public transport, located just north along Victoria Street (tram no. 12 and 109), as well as the Bridge Road tram (no. 48). The closest train station is Burnley Station approximately 1.4km to the south.

The Site is advantageously located within close proximity to a number of vibrant activity centres and retail opportunities, namely Victoria Gardens Shopping Centre (100m), Victoria Street Shopping Precinct (400m), and Bridge Road Shopping Precinct (550m). The major activity centres of Swan Street, Kew Junction and Glenferrie Hawthorn are also nearby (refer to Figure 5 - Site Location Plan).

Within the area are a number of public open spaces including the Main Yarra Trail and Capital City Trail (400m), which leads to Burnley Park to the south (1.2km) and Andrews Reserve Trail to the north (700m). Williams Reserve is found to the north of the Site (400m) as well as Citizens Park including the Richmond Recreation Centre located to the west (700m).

Furthermore, the Site is very well serviced by schools and community facilities including; Trinity Catholic School (100m), Yarra Primary School (150m), Yarraberg Children's Centre (250m), Collingwood Childrens Farm and Abbotsford Convent (1.5km).

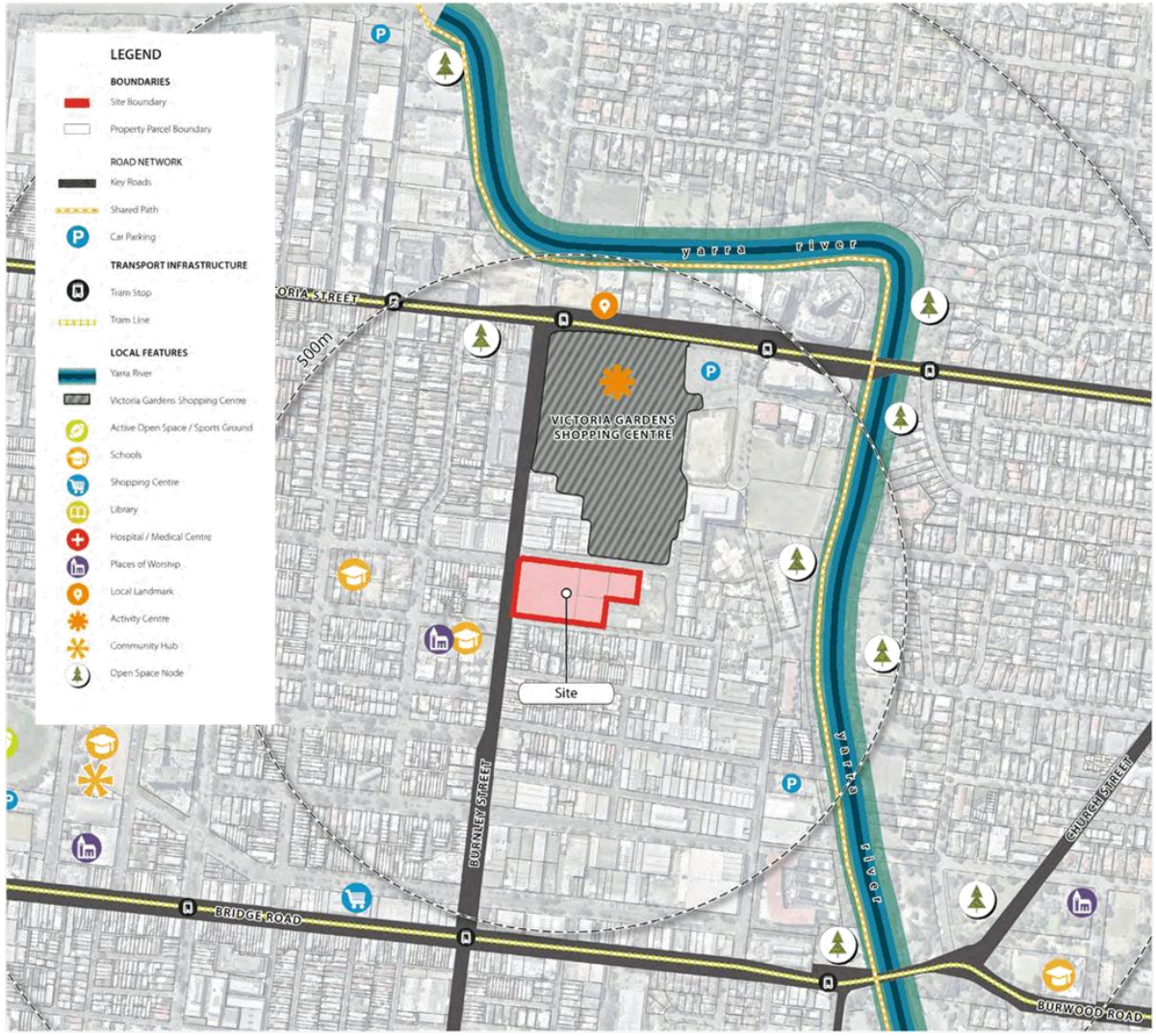
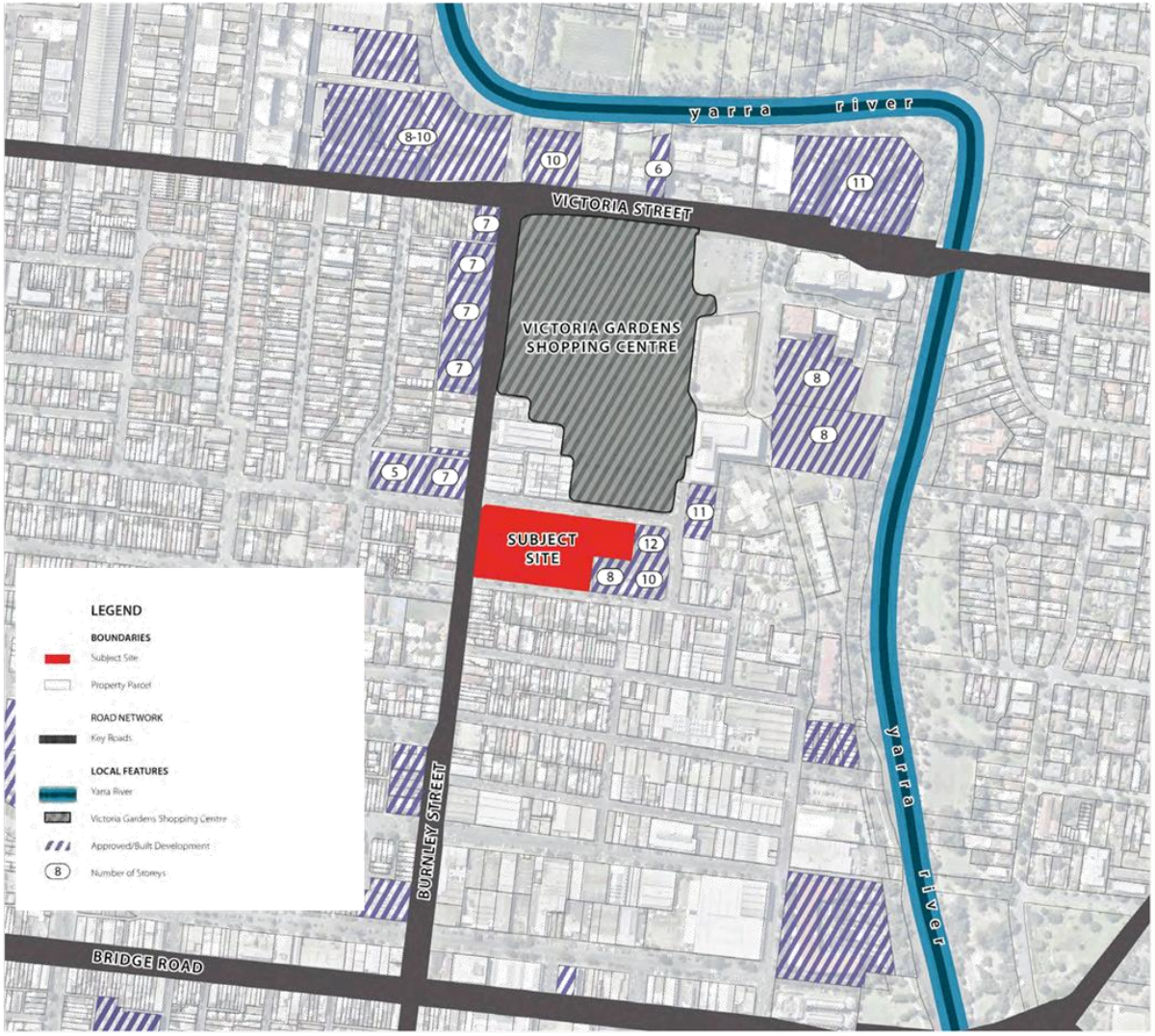


Figure 5 Site Location Plan

Attachment 2 - Planning report Tract Amendment C223



2.4 Surrounding Developments

Consistent with its character as an area in transition, a large number of planning permits allowing medium and high density development have been granted in the vicinity of the Site, many of which have been recently completed.

A planning permit has been granted to the land directly to the Sites east (known as 36-44 Doonside Street), which compromises the remaining 30% of the block (Refer to Figure 6 - Surrounding Development Plan). This allows three buildings of twelve, ten and eight storeys, and is under construction.

Surrounding properties with granted planning permits, under construction or development complete status include:

- 9-15 David Street, Richmond. 11 storeys. Approved planning permit and under construction.
- 171 Buckingham Street, Richmond. Mixed use building ranging in scale beteween 7 and 3 storeys. Approved planning permit.
- 36-44 Doonside Street, Richmond. Three towers each to a height of 12, 10 and 8 storeys. Approved planning permit and under construction.
- 2A, 2-30 Burnley Street, & 520 Victoria Street, Richmond. Ranging in scale between 3 and 7 storeys. Development complete.
- 677-679 Victoria Street, Abbotsford. 11 storeys. Development complete.
- 23-35 River Boulevard, Richmond. 5 buildings up to 8 storeys. Approved planning permit.
- 647-649 Victoria Street, Richmond. 10 storeys. Approved planning permit.

Figure 6 Surrounding Development Plan

Attachment 2 - Planning report Tract Amendment C223

2.5 Site Opportunities and Constraints

The rezoning and redevelopment of the Site offers an array of opportunities both within the site and for the wider local community. These include the potential to create a distinctive residential and mixed use precinct, while also improving the amenity of the site and surrounding area.

Particular opportunities arising from the rezoning and redevelopment of the site include;

- Creation of a new sizeable public open space that is north facing and complements the existing Doonside Street heritage building and incorporates the Sites only significant tree.
- Retaining and restoring the existing heritage buildings within the Site.
- New pedestrian walkway throughout the Site connecting to Victoria Gardens to the north.
- New development to better interface with existing residential development to the south.
- Increased diversity of housing types in line with Plan Melbourne principles.
- Diversifying uses within the Victoria Street Activity Centre.
- Increasing onsite employment by upgrading the 'Harry the Hirer' business and introducing new employment generating uses.

Refer to Figure 7 - Site Opportunities and Constraints.

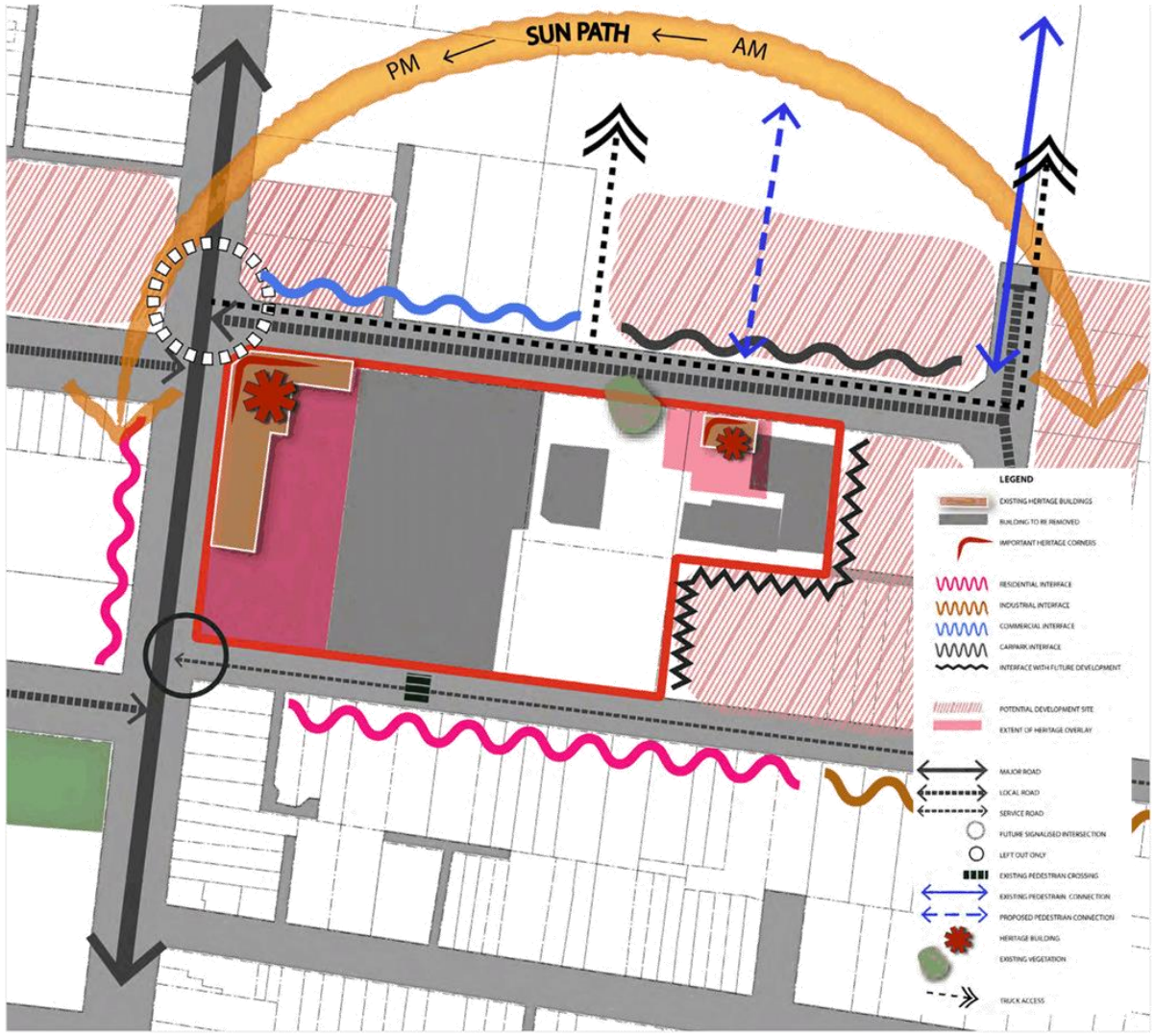


Figure 7 Site Opportunities and Constraints

Attachment 2 - Planning report Tract Amendment C223

1. Existing heritage fabric and significant architectural features to be retained and restored.
2. Retain key view to existing industrial facade from Burnley, Doonside and Appleton Streets.
3. Retain key views to former Repco Offices & Laboratories.
4. Retain entire building.

81-95 Burnley Street

26 Doonside Street

Figure 8 Heritage Analysis

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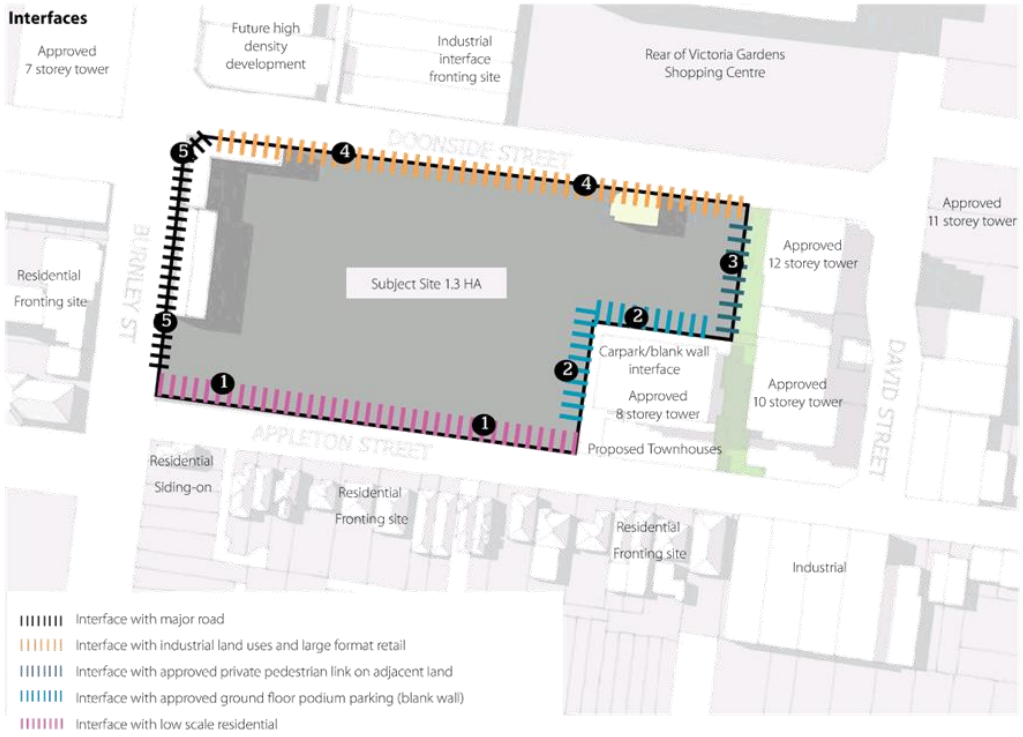


1. Opportunity for 2-3 storey residential interface or equivalent form to provide a sensitive interface with the existing character of Appleton Street.
2. Site is constrained by the ground floor and podium uses proposed by the neighbouring development. Ground and podium level uses in this location must respond to a blank wall interface. Ground floor/podium residential uses are not appropriate in this location.
3. Opportunity for residential land uses to address the approved pedestrian connection.
4. Long expanse of the site fronts existing commercial and light industrial land uses. Opportunity to provide retail, vehicle access and a loading dock along this edge as these functions are more compatible to the existing operation of Doonside Street.
5. The interface with Burnley Street is appropriate for commercial uses. There is an opportunity to use the existing Heritage Building to provide access to residential development above.

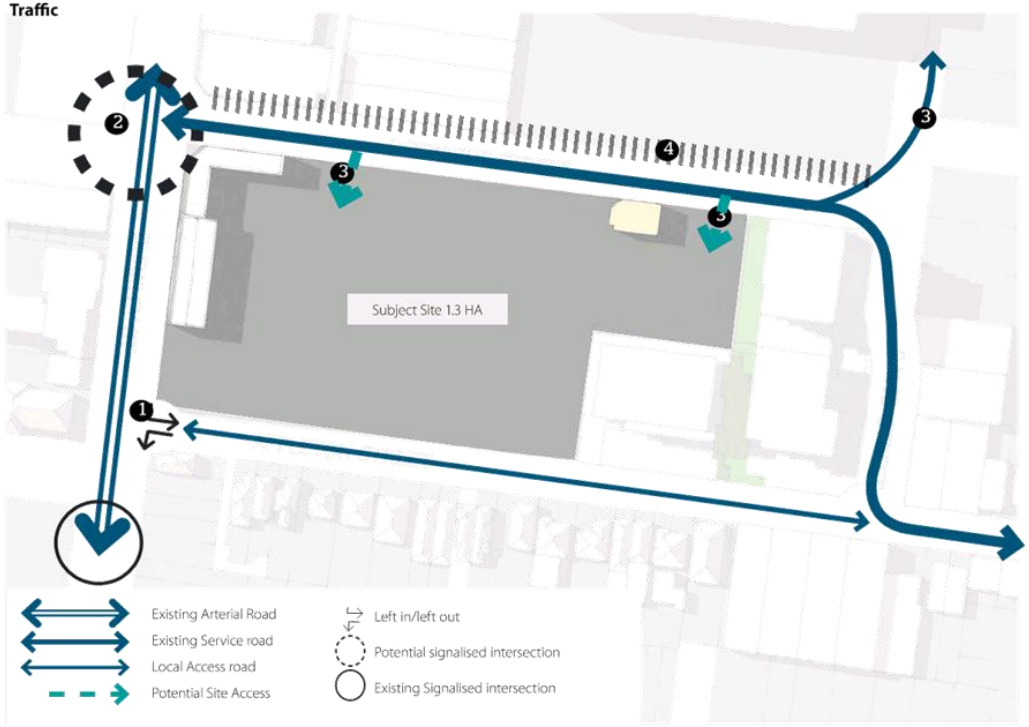
Appleton Street Interface

Burnley Street Interface

Figure 9 Interface Analysis

Attachment 2 - Planning report Tract Amendment C223

Traffic



1. Traffic movement along Appleton Street is constrained. There is currently a left out exit onto Burnley Street. Appleton Street is narrow and designed to accommodate low traffic movements.
2. The intersection at Doonside and Burnley Street is not currently signalised. Future residential development and increased traffic volumes may trigger signalisation. Opportunity to contribute to the future intersection signalisation.
3. Doonside Street supports existing truck movements and loading to the rear of Victoria Gardens Shopping Centre. There is an opportunity to provide access into the Site and basement carparking from Doonside Street.
4. Opportunity to investigate potential road widening to the north of Doonside Street (referenced in Council strategic documents).

David Street Traffic Conditions

Appleton Street Traffic Conditions

Figure 10 Traffic Analysis

Attachment 2 - Planning report Tract Amendment C223

1. A private pedestrian north/south link is proposed in the neighbouring development.
2. Opportunity to incorporate the heritage building at the corner of the pedestrian link through the Site.
3. A pedestrian link is proposed in the Victoria Street Structure Plan, linking Victoria Gardens Shopping Centre to Doonside Street and the Site. There is an opportunity to continue this link through the Site to facilitate further local connections and walkability.

Doonside Street Pedestrian Conditions

Doonside Street Pedestrian Conditions

Figure 11 Pedestrian Analysis

Attachment 2 - Planning report Tract Amendment C223

2.6 Site Interfaces

The Site currently offers a generally harsh industrial interface along Doonside Street and a residential interface on Appleton Street, while the existing heritage façade along Burnley Street remains the Sites primary visual point of interest.

The surrounding areas compromise a mixture of uses; predominantly residential areas to the south and west, with retail (with some commercial and residential) to the north and some public open space areas along the Yarra River to the east.

2.6.1 North - Doonside Street

Doonside Street is an approximate 9.7m carriageway, allowing for a single lane of traffic in each direction. It currently acts as the major east west road within the Doonside Precinct enabling delivery movements for Victoria Gardens Shopping Centre. The Street predominantly has an industrial interface with no street vegetation. An existing mature Corymbia Citriodora (Lemon-scented Gum) is located along the northern property boundary of the site.

On the opposite side of Doonside Street are a number of existing double storey warehouse buildings (closer to the Burnley Street intersection) and a large, vacant parcel of land which forms part of the southern area of the Victoria Gardens Shopping Centre (refer to Figure 12 - North Interface Images).

2.6.2 East - Adjoining Site Interface

36-44 Doonside Street and 27-41 Appleton Street, Richmond has an approved planning permit (PLN10/0790) for the construction of a mixed use building. The proposal consists of three separate built forms of 8, 10 and 12 storeys in height. The development is currently under construction.

Further east is David Street, which is a north south industrial street with a carriageway of approximately 9m in width (refer to Figure 13 - Eastern Interface Images). 9-11 David Street has an approved planning permit (PLN14/0538) for the construction of an 11 storey residential building with part ground floor retail, and is currently under construction.

Figure 12 Northern Interface Images

Figure 13 Eastern Interface Images

Attachment 2 - Planning report Tract Amendment C223

2.6.3 South - Appleton Street

Appleton Street is a residential street with a 7.9m carriageway. Appleton Street is primarily residential on the southern side, and industrial on the northern side, with the majority of its southern length consisting of detached single storey heritage dwellings. Most of these properties are Edwardian and Victorian cottages.

Appleton Street contains a consistent planting of street trees along its northern boundary (refer to Figure 14 - Southern Interface Images). Recent works have created a 'left in left out' traffic arrangement at Burnley Street, which was implemented to mitigate excessive vehicle movements.

2.6.4 West - Burnley Street

Burnley Street is an arterial road with a lane of traffic in both directions. It is within a Road Zone and acts as a major thoroughfare between the Victoria Street Precinct (north) to the Monash Freeway (south of Swan Street). Burnley Street has a number of signalised crossings between Bridge Road to the south and Victoria Street to the north including outside the Victoria Gardens Shopping Centre and opposite Trinity Catholic School.

On the western side of Burnley Street is an eclectic mix of residential developments, comprising an approved 7 storey residential development on the north-west corner of Burnley Street and Buckingham Street and a 4 storey residential development (known as Richmond Heights) at 86 Burnley Street, Richmond (refer to Figure 15 - Western Interface Images). Further, a number of higher density residential developments directly opposite Victoria Gardens Shopping Centre (along both Burnley Street and Victoria Street) range in heights between 6 and 10 storeys.

Figure 14 Southern Interface Images

Figure 15 Western Interface Images

Attachment 2 - Planning report Tract Amendment C223

03 PLANNING PROVISIONS

3.1 State Planning Policy Framework (SPPF)

To ensure local planning schemes further the objectives of planning in Victoria, planning authorities must take into account and give effect to the general principles and specific policies contained in the State Planning Policy Framework (SPPF). The SPPF clauses that are most relevant to the Site and proposed amendment are detailed below.

Clause 9 Plan Melbourne

Plan Melbourne 2017-2050, the State Government's metropolitan strategy for Melbourne, was 'refreshed' in March 2017 and outlines the vision for Melbourne's growth to the year 2050. It seeks to define the kind of city Melbourne will be and identifies the infrastructure, services and major projects which need to be in place to underpin the city's growth (refer to Figure 16 - Plan Melbourne Inner Metro Region).

The Site is located within the Inner Metro Region of Melbourne which is intended to accommodate a significant share of Melbourne's growth. The Site is strategically positioned within the Victoria Street Activity Centre, as well as within close proximity to the Bridge Road Activity Centre.

Clause 11 Settlement

This clause seeks 'to anticipate and respond to the needs of existing and future communities through the provision of zoned and services land for housing, employment, recreation and open space, commercial and community facilities and infrastructure'.

Clause 11.01-1 Settlement Networks

The objective of this clause is 'to promote the sustainable growth and development of Victoria and deliver choice and opportunity for all Victorians through a network of settlements'.

Clause 11.02-1 Supply of Urban Land

The objective of this clause seeks 'to encourage a sufficient supply of land is available for residential, commercial, retail, industrial, recreational, institutional and other community uses'.

Clause 11.03-1 Activity Centre Network

The objective of this clause seeks 'to build up activity centres as a focus for high-quality development, activity and living for the whole community by developing a network of activity centres'.

Clause 11.03-2 Activity Centre Planning

The objective of this clause is 'to encourage the concentration of major retail, residential, commercial, administrative, entertainment and cultural developments into activity centres which provide a variety of land uses and are highly accessible to the community'.

Clause 11.06-1 Jobs and Investment

The objective of this clause is 'to create a city structure that drives productivity, attracts investment, supports innovation and creates jobs'.

Clause 11.06-2 Housing Choice

This clause seeks 'to provide housing choice close to jobs and services'.

Clause 11.06-5 Neighbourhoods

The objective of this clause is 'to create a city of inclusive, vibrant and healthy neighbourhoods that promote strong communities, healthy lifestyles and good access to local services and jobs'.

Clause 11.06-6 Sustainability and Resilience

This objective seeks 'to create a more sustainable and resilient city that manages its land, biodiversity, water, energy and waste resources in a more integrated way'.

Clause 15 Built Environment and Heritage

This clause seeks 'to ensure all new land use and development appropriately responds to its landscape, valued built form and cultural context', and 'protect places and sites with significant heritage, architectural, aesthetic, scientific and cultural value'.

Clause 15.01-1 Urban Design

This clause seeks 'to create urban environments that are safe, functional and provide good quality environments with a sense of place and cultural identity'.

Clause 15-01-2 Urban Design Principles

The objective of this clause is 'to achieve architectural and urban design outcomes that contribute positively to local urban character and enhance the public realm while minimising detrimental impact on neighbouring properties'.

Clause 15.01-4 Design for Safety

The objective of this clause is 'to improve community safety and encourage neighbourhood design that makes people feel safe'.

Clause 15.02-1 Energy and Resource Efficiency

The objective of this clause is 'to encourage land use and development that is consistent with the efficient use of energy and the minimisation of greenhouse gas emissions'.

Clause 15.03-1 Heritage Conservation

The objective of this clause is 'to ensure the conservation of places of heritage significance'.

Clause 16 Housing

This clause notes that 'planning should provide for housing diversity, and ensure the efficient provision of supporting infrastructure' as well as 'new housing should have access to services and be planned for long term sustainability, including walkability to activity centres, public transport, schools and open space.'

Clause 16.01-1 Integrated Housing

This objective seeks 'to promote a housing market that meets community needs'.

Clause 16.01-2 Location of residential development

The objective of this clause is 'to locate new housing in or close to activity centres and employment corridors and at other strategic redevelopment sites that offer good access to services and transport'.

Clause 16.01-3 Housing Opportunity Sites

The objective of this clause is 'to identify areas that offer opportunities for more medium and high density housing near employment and transport in Metropolitan Melbourne' and sets up the following criteria (inter alia):

- 'In and around the Central City.
- Urban-renewal precincts.
- Areas for residential growth.
- Areas for greyfield renewal.
- Metropolitan activity centres and major activity centres.
- Neighbourhood activity centres - especially those with good public transport connections.

The Site meets the above criteria.

Attachment 2 - Planning report Tract Amendment C223

Clause 16.01-4 Housing Diversity

The objective seeks 'to provide for a range of housing types to meet increasingly diverse needs'.

Clause 16.01-5 Housing Affordability

The objective of this clause is 'to deliver more affordable housing closer to jobs, transport and services'.

Clause 17 Economic Development

This clause seeks 'to provide for a strong and innovative economy, where all sectors of the economy are critical to economic prosperity'. Planning is 'to contribute to the economic well-being of communities and the State as a whole by supporting and fostering economic growth and development by providing land, facilitating revisions, and resolving land use conflicts, so that each district may build on its strengths and achieve its economic potential'.

Clause 17.01-1 Business

The objective of this clause is 'to encourage development which meet the communities needs for retail, entertainment, office and other commercial services' and 'provides net community benefit in the relation to accessibility, efficient infrastructure use and the aggregation and sustainability of commercial facilities'.

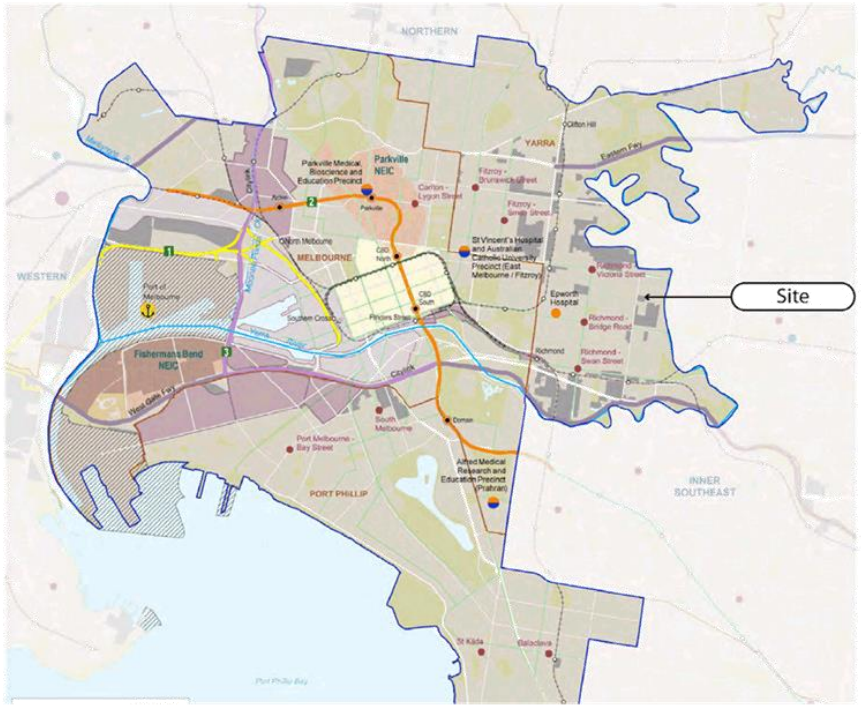


Figure 16 Plan Melbourne Inner Metro Region

Attachment 2 - Planning report Tract Amendment C223

3.2 Local Planning Policy Framework (LPPF)

The Local Planning Policy Framework (LPPF) clauses that are most relevant to the proposed amendment are detailed below.

Clause 21.02 Municipal Profile

This clause outlines the overarching trends within the City of Yarra, including a growing population, smaller households and household growth.

Clause 21.03 Vision

This clause states that a complex land use mix will provide for a range of activities to meet the needs of the community, and will have a distinctive identity as a low-rise urban form, with areas of higher development and highly valued landmarks (refer to Figure 17 - Residential Development Opportunities Map).

Clause 21.04-1 Accommodation and Housing

This clause provides a number of objectives that will assist in the provision of accommodation and housing growth within the City of Yarra.

- 'Objective 1: To accommodate forecast increases in population.
- Objective 2: To retain a diverse population and household structure.
- Objective 3: To reduce potential amenity conflicts between residential and other use'.

Clause 21.04-2 Activity Centres

This clause provides a number of objectives that will facilitate the presence of service, retail and entertainment uses to create active and vibrant activity centres with good access to services and facilities.

- 'Objective 4: To maintain a balance between local convenience and regional retail roles in Yarra's activity centres.
- Objective 5: To maintain the long term viability of activity centres'.

Clause 21.04-3 Industry, Office and Commercial

This clause provides a number of objectives that reinforce the commercial and industrial sectors that underpin a sustainable economy and provides employment. Yarra Council plans to retain and foster a diverse and viable economic base.

- 'Objective 8: To increase the number and diversity of local employment opportunities.
  - Strategy 8.1: Support rezonings, as identified on the relevant Neighbourhood plan, to permit increased commercial and office use in existing industrial areas'. The Site is specifically identified in the Neighbourhood Maps as 'support rezoning to mixed use zone (refer to Figure 19 - Neighbourhood Map).
  - Strategy 8.3: Encourage residential and business land use within the Mixed Use Zone to locate on the same site.
  - Strategy 8.5: Support opportunities for new uses on isolated industrial sites provided they all reflect the predominant surrounding uses'.

Clause 21.04-5 Parks, Gardens and Public Open Space

This clause provides a number of objectives that outline the importance of public open space in an inner city environment.

- 'Objective 12: To establish a linked open space network.
- Objective 13: To provide an open space network that meets existing and future community needs'.

Clause 21.05-1 Heritage

This clause provides objectives that seek to conserve areas of heritage significance and also support the adaptive reuse and change of buildings.

- 'Objective 14: To protect and enhance Yarra's heritage places.
  - Strategy 14.2: Support the restoration of heritage places'.

Clause 21.05-2 (Urban Design)

This clause provides a number of objectives that acknowledge that much of the municipalities built form is a low-rise typology punctuated by pockets of higher development.

- 'Objective 16: To reinforce the existing urban framework of Yarra.
- Objective 17: To retain Yarra's identity as a low-rise urban form with pockets of higher development.
  - Strategy 17.2: Development on strategic sites or within activity centres should generally be no more than 5-6 storeys unless it can be demonstrated that the proposal can achieve specific benefits such as:
    - Significant upper level setbacks.
    - Architectural design excellence.
    - Best practice environmental sustainability objectives in design and construction.
    - High quality restoration and adaptive re-use of heritage buildings.
    - Positive contribution to the enhancement of the public domain.
    - Provision of affordable housing.
- Objective 18: To retain, enhance and extend Yarra's fine grain street pattern.
  - Strategy 18.1: Encourage the re-establishment of streets and laneways through new development sites where such links were park of the historic street pattern, except where this will cause detrimental impacts.
- Objective 19: To create an inner city environment with landscaped beauty.
  - Strategy 19.2: Encourage opportunities for planting suitable trees and landscape areas in new development.
  - Strategy 19.3: Encourage the retention of mature vegetation.
- Objective 20: To ensure that new development contributes positively to Yarra's urban fabric.
  - Strategy 20.1: Ensure development is designed having regard to its urban context and specifically designed following a thorough analysis of the site, the neighbouring properties and its environs.
- Objective 21: To enhance the built form character of Yarra's activity centres.
  - Strategy 21.3: Support new development that contributes to the consolidation and viability of existing activity centres.

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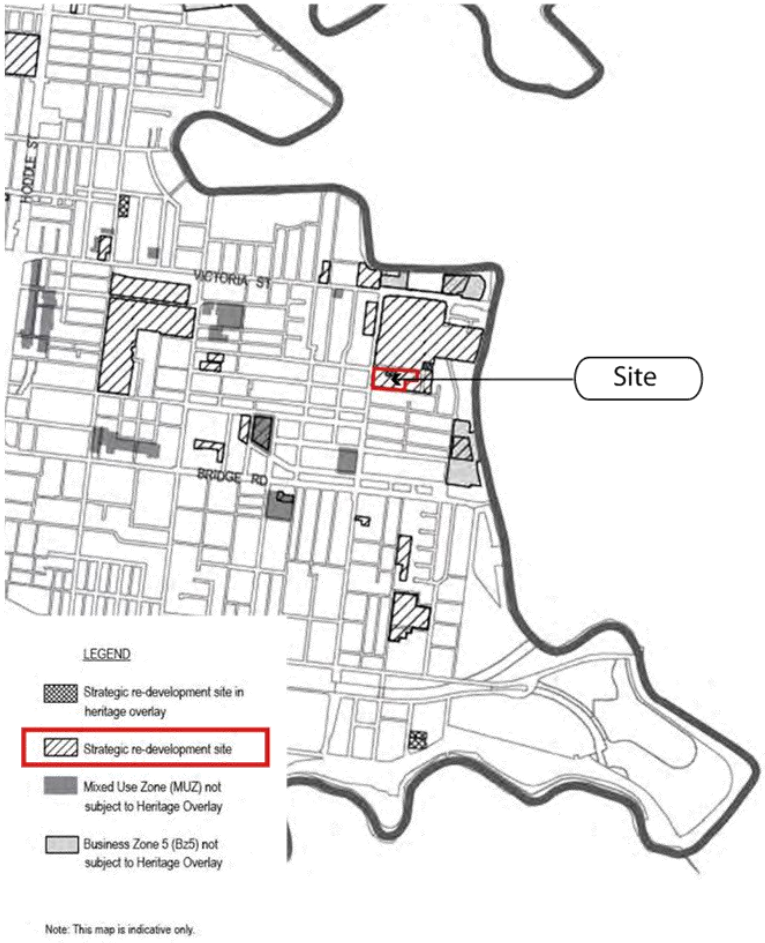

Figure 17 Residential Development Opportunities Map (Yarra PS)

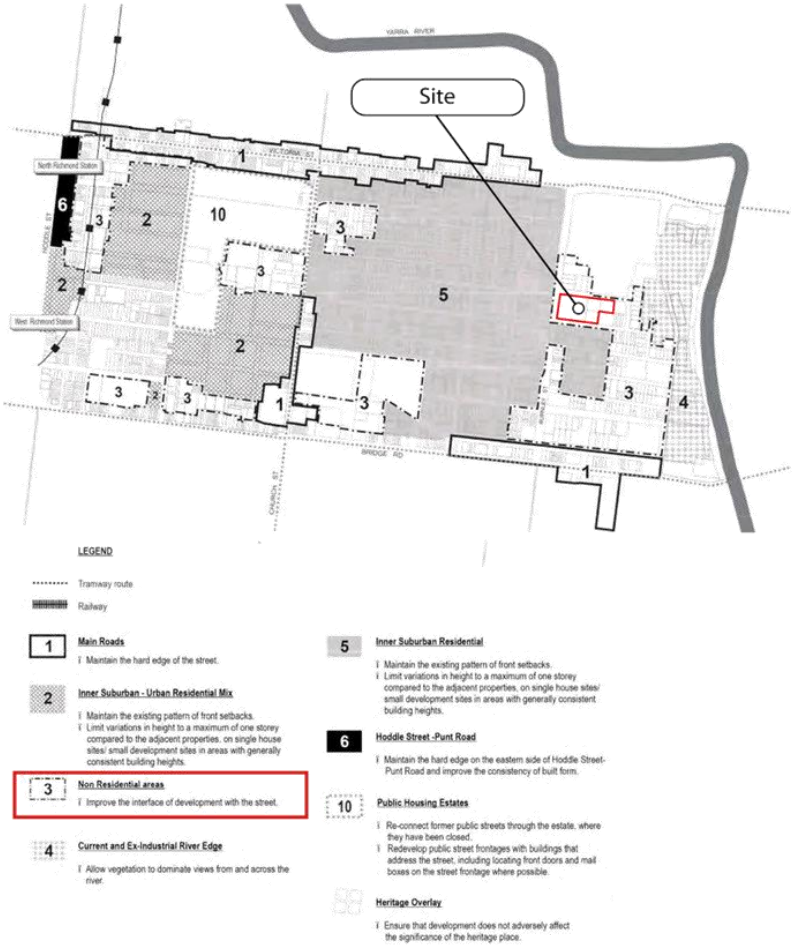

Figure 18 Built Form Character Map (Yarra PS)

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Clause 21.05.3 - Built Form Character

'New development must respond to Yarra's built and cultural character, its distinct residential neighbourhoods and individualised shopping strips, which combine to create a strong local identity'.

Non Residential Areas

- 'Objective 27: To improve the interface of development with the street in non residential areas.
  - Strategy 27.1: Allow flexibility in built form in areas with coarse urban grain (larger lots, fewer streets and lanes).
  - Strategy 27.2: Require new development to integrate with the public street system.

Clause 21.05-4 Public Environment

This clause encourages new development must add positively to Yarra's overall character and help create a safe and engaging public environment where pedestrian activity and interaction are encouraged.

- 'Objective 28: To provide a public environment that encourages community interaction and activity.
  - Strategy 28.1: Encourage universal access to all new public spaces and buildings.
  - Strategy 28.2: Ensure that buildings have a human scale at street level.
  - Strategy 28.3: Require buildings and public spaces to provide a safe and attractive public environment.
  - Strategy 28.4: Require new development to consider the opportunity to create public spaces as part of new development.
  - Strategy 28.5: Require new development to make a clear distinction between public and private spaces.
  - Strategy 28.6: Require new development to consider the creation of public access through large development sites, particularly those development sites adjacent to waterways, parkland or activity centres'.

Clause 21.06-1 Walking and cycling

- 'Objective 30: To provide safe and convenient pedestrian and bicycle environments.
  - Strategy 30.1: Improve pedestrian and cycling links in association with new development where possible'.

Clause 21.06-3 Public Transport

- 'Objective 31: To facilitate public transport usage.
  - Strategy 31.1: Require new development that generates high numbers of trips to be easily accessible by public transport'.

Clause 21.06-3 The Road system and Parking

- 'Objective 32: To reduce the reliance on the private motor car.
  - Strategy 32.2: Require all new large developments to prepare and implement integrated transport plans to reduce the use of private cars and to encourage walking, cycling and public transport.
- Objective 33: To reduce the impact of traffic.
  - Strategy 33.1: Ensure access arrangements maintain the safety and efficiency of the arterial and local road networks'.

Clause 21.08 Neighbourhoods

This clause sets out the locally specific implementation of the objectives and strategies of clauses 21.04 to 21.07 for North Richmond (north of Bridge Road).

The Site is identified as being within the Victoria Street East Precinct. This precinct includes a combination of retail, bulky goods, entertainment, residential and office land uses. Significant parts of this precinct have recently undergone extensive redevelopment and a number of key sites in the area are still waiting for redevelopment. The clause identifies the site as an existing 'non residential area which needs to improve the interface of development with the street' as well as to 'support rezoning to mixed use zone' (refer to Figure 18 - Built Form Character Map and Figure 19 - Neighbourhood Character Map).

It is important to:

- 'Provide land use close to the Victorian Gardens Activity Centre that supports the role of the centre i.e. residential plus mixed uses.
- Supporting a change of use to residential plus mixed use in the industrial area abutting the southern boundary of Victoria Gardens.
- Creating a fine-grained network of pedestrian-friendly streets east of Burnley Street and north of Doonside Street'.

Clause 22.10 Built Form and Design Policy

This policy applies to all new development not included in a heritage overlay, and comprises of 10 design elements that address the following issues; 'urban form and character, setbacks and building heights, street and public space quality, environmental sustainability, site coverage, on-site amenity, off-site amenity, landscaping and fencing, parking, traffic and access and service infrastructure'.

The policy objectives seek to ensure that new development positively responds to the context of the development and respects the scale and form of surrounding development where this is a valued feature of the neighbourhood character.

Clause 22.11 Victoria East Precinct Policy

This policy applies to all land within the Victoria Street East Precinct, and promotes new opportunities for surplus industrial land to provide sustainable local employment and economic growth while maximising residential development opportunities.

The Site is specifically identified in the Victoria Street East Precinct Framework Plan as 'mixed uses' (refer to Figure 20 - Victoria Street East Precinct Framework Plan).

Clause 22.12 Public Open Space Contributions

This policy applies to all residential proposals within the City of Yarra and identifies the need for new residential development to contribute to the provision of public open space.

This applies to the subdivision of land, and involves making either a land or cash contribution to the Council for open space.

Clause 22.17 Environmentally Sustainable Development

This policy applies throughout the City of Yarra to residential and non-residential development that requires a planning permit. The overarching objective is that development should achieve best practice in environmentally sustainable development for the design stage through to the construction and must be accompanied by a Sustainability Management Plan (SMP) as part of any future planning application.

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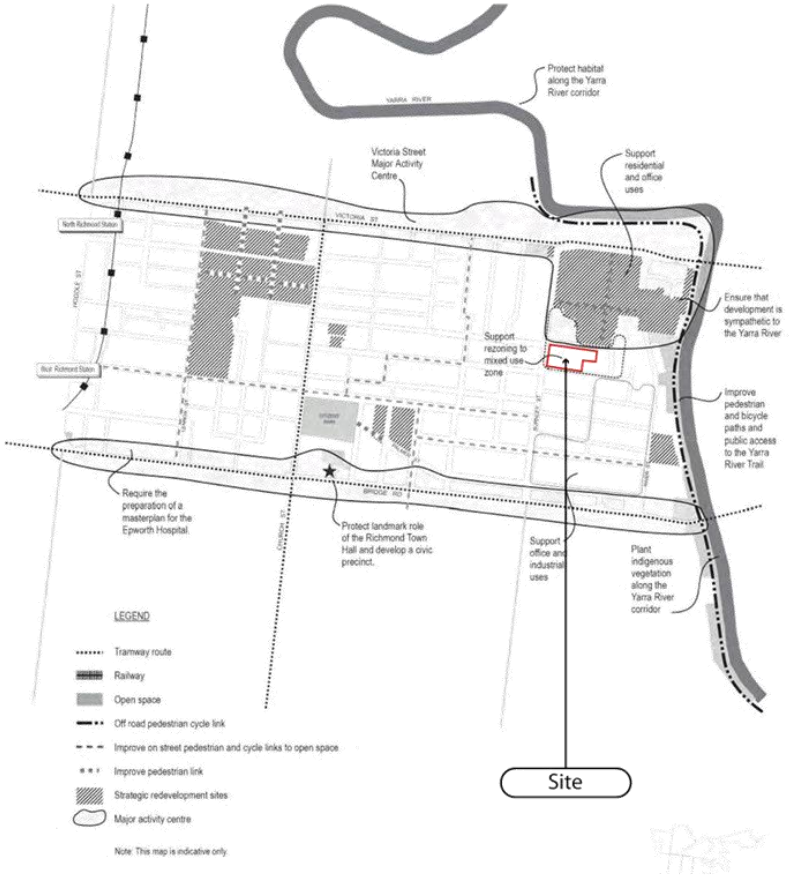

Figure 19 Neighbourhood Character Map (Yarra PS)

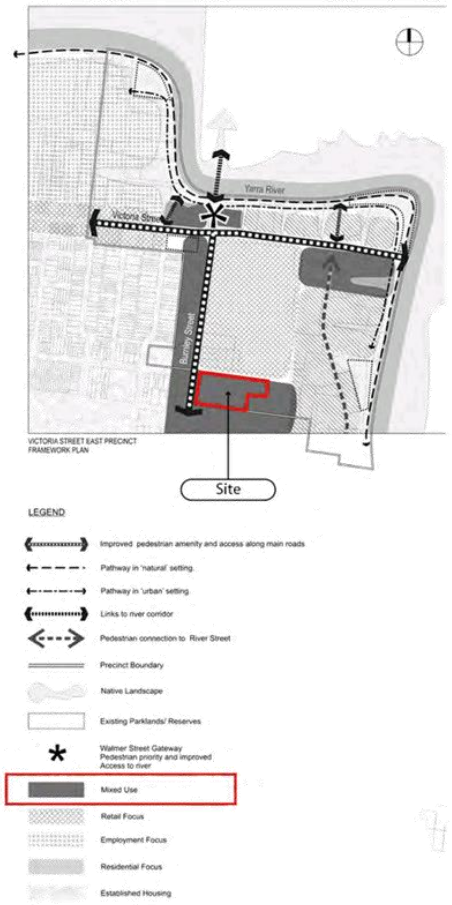

Figure 20 Victoria Street East Precinct Framework Plan (Yarra PS)

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3.3 Zone

Clause 33.01 - Industrial 3 Zone (IN3Z).

The purpose of Clause 33.03 is;

- 'To implement the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.
- To provide for industries and associated uses in specific areas where special consideration of the nature and impacts of industrial uses is required or to avoid inter-industry conflict.
- To provide a buffer between the Industrial 1 Zone or Industrial 2 Zone and local communities, this allows for industries and associated uses compatible with the nearby community.
- To allow limited retail opportunities including convenience shops, small scale supermarkets and associated shops in appropriate locations.
- To ensure that uses do not affect the safety and amenity of adjacent, more sensitive land uses'.

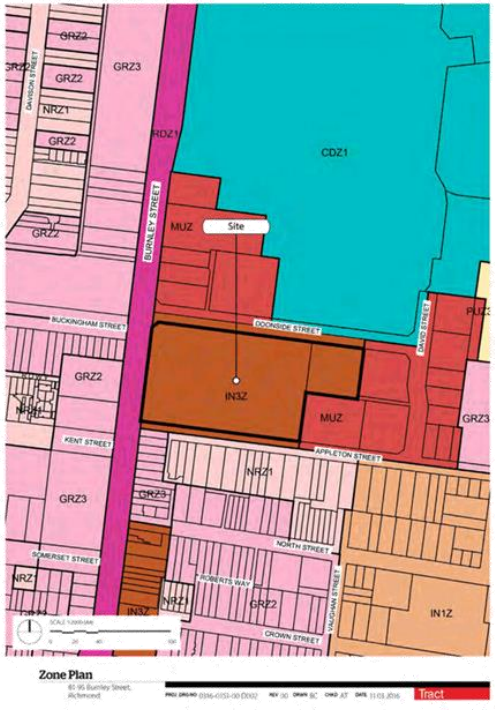

Figure 21 Zone Plan

3.4 Overlays

Clause 43.02 – Design and Development Overlay Schedule 2 (DDO2) – Main Roads and Boulevards.

The purpose of Clause 43.02 is:

- 'To implement the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.
- To identify areas which are affected by specific requirements relating to the design and built form of new development'.

The design objectives specific to Schedule 2 – Main Roads and Boulevards are:

- 'To recognise the importance of main roads to the image of the City.
- To retain existing streetscapes and places of cultural heritage significance and encourage retention of historic buildings and features which contribute to their identity.
- To reinforce and enhance the distinctive heritage qualities of main roads and boulevards.
- To recognise and reinforce the pattern of development and the character of the street, including traditional lot width, in building design.
- To encourage high quality contemporary architecture.
- To encourage urban design that provides for a high level of community safety and comfort.
- To limit visual clutter.
- To maintain and where needed, create a high level of amenity to adjacent residential uses through the design, height and form of proposed development.
- The decision guidelines the responsible authority must consider as appropriate to Schedule 2 – Main Roads and Boulevards are:
  The contribution of the proposal to the streetscape.
- The design, height and visual bulk of the development in relation to surrounding land uses and developments.
- The design, height and form of the development in relation to the built form character of the street'.

Attachment 2 - Planning report Tract Amendment C223

Clause 43.01 – Heritage Overlay (HO375 & HO252)

The purpose of Clause 43.01 is:

- 'To implement the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.
- To conserve and enhance heritage places of natural or cultural significance.
- To conserve and enhance those elements which contribute to the significance of heritage places.
- To ensure that development does not adversely affect the significance of heritage place.
- To conserve specifically identified heritage places by allowing a use that would otherwise be prohibited if this will demonstrably assist with the conservation of the significance of the heritage place'.

The site includes two buildings that have been identified by the heritage overlay:

- HO252 - 26 Doonside Street, Richmond (Former Repco Offices)
- HO375 - 81-95 Burnley Street, Richmond (Russell Manufacturing Company Pty Ltd later Repco).

Further discussion regarding heritage is found in chapter 6.

Figure 22 Design and Development Overlay Plan
Figure 23 Heritage Overlay Plan

Attachment 2 - Planning report Tract Amendment C223

3.5 Particular Provisions

Clause 52.01 – Public Open Space Contribution and Subdivision

A person who proposes to subdivide land must make a contribution to Council for public open space in an amount specified in the schedule to this clause (being a percentage of the land intended to be used for residential, industrial or commercial purposes, or a percentage of the site value of such land, or a combination of both).

The schedule to this provision states the amount of contribution for public open space is 4.5% of land or buildings intended to be used for residential purposes.

Clause 52.06 – Car Parking

The purpose of this clause is to promote the efficient use of car parking spaces through the consolidation of car parking facilities and to ensure that the design and location of car parking is of a high standard, creates a safe environment for users and enables easy and efficient use.

Clause 52.07 – Loading and Unloading of Vehicles

The purpose of this clause is to set aside land for loading and unloading commercial vehicles to prevent loss of amenity and adverse effect on traffic flow and road safety.

Clause 52.29 – Land Adjacent to a Road Zone, Category 1, or a Public Acquisition Overlay for a Category 1 Road

The purpose of this clause is to ensure appropriate access to identified roads and to ensure appropriate subdivision of land adjacent to identified roads.

Clause 52.34 – Bicycle Facilities

The purpose of this clause is to encourage cycling as a mode of transport and to provide secure, accessible and convenient bicycle parking spaces and associated shower and change facilities.

Clause 52.36 – Integrated Public Transport Planning

The purpose of this clause is to ensure development supports public transport usage and ensure that development incorporated safe, attractive and convenient pedestrian access to public transport stops.

3.6 Amendment C99 to the Yarra Planning Scheme

An amendment request was made in 2007 on behalf of the owners of No. 36-44 Doonside Street and No. 27-49 Appleton Street, immediately to the east of the Site. It included all the land within the area described as the 'Doonside Precinct', bounded by Burnley Street, Doonside Street, David Street and Appleton Street, and included the subject site.

At a meeting on 20th August 2008, Council resolved to prepare and exhibit Amendment C99 to the Yarra Planning Scheme to rezone the land to a Mixed Use Zone but excluded No. 81-95 Burnley Street and No. 26-34 Doonside Street, ('the Site'), who had expressed concerns over impacts on its business operations.

Council advised the Panel that it excluded these properties as it was of the opinion that this area of land could be included in a future rezoning proposal.

The amendment request also sought to apply a new Design and Development Overlay (DDO9) to replace an existing DDO and to apply an Environmental Audit Overlay (EAO) to the subject land.

The Panel Report found strong support for the rezoning as the amendment would provide opportunities for new economic growth and housing infill opportunities abutting a Major Activity Centre. The Panel concluded that policy supported a mix of compatible uses for this transitional area while ensuring that existing businesses continue to operate.

Harry the Hirer, in its submission, noted that it '…supported the currently proposed amendment as referred to the Panel … as noted the new zone provisions should avoid building in inevitable conflict at the interface with existing zones by including measures which have precedent in existing planning schemes'.

The Panel also noted that the Minister for Planning had advised that 'there is adequate strategic justification to rezone the Doonside Precinct to a Mixed Use Zone'.

The land was ultimately rezoned, leaving the subject site the only remaining industrially zoned land in the Doonside Precinct.

3.7 Other Planning Considerations

3.7.1 Victoria Street Structure Plan

The Victoria Street Structure Plan was adopted by Yarra Council on 20 April 2010 to guide planning for change in and around the centre for 10 to 15 years.

Council prepared the structure plan for the shopping areas and other commercial areas along Victoria Street from Hoddle Street to the Yarra River, including the subject site.

The Site is located within Precinct 11: Victoria Gardens which has been identified as the location for a 'larger scale office and residential development' as well as 'Doonside Precinct – potential housing mixed with retail and businesses'.

The structure plan identifies the Site in a number of plans including 'Areas of Change Plan', 'Future Activity Plan' and 'Future Building Plan' which recognise it as a location for substantial future change in areas including 'former industrial sites that are ready or in the process of changing to other more intensive uses' (refer to Figures 24 and 25).

The objectives emphasise the need for increased amount and diversity of housing, throughout the residential and mixed use areas but with a focus on key precincts where significant change can be managed with lower amenity impacts.

Attachment 2 - Planning report Tract Amendment C223

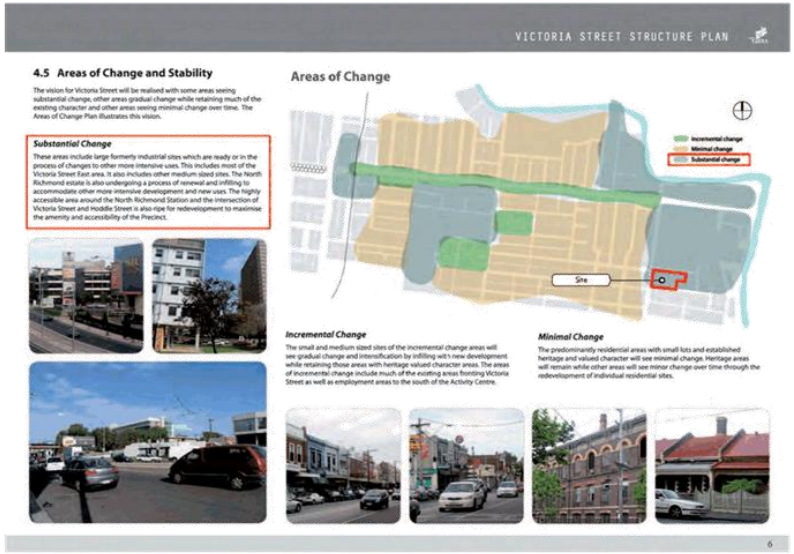

Figure 24 Areas of Change (Victoria Street Structure Plan)

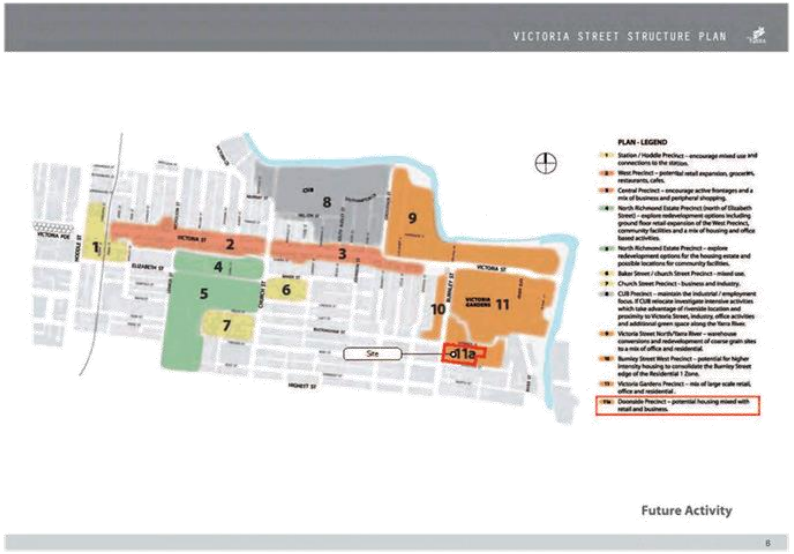

Figure 25 Future Activity (Victoria Street Structure Plan)

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3.7.2 Victoria Street East Precinct, Urban Design Framework

The Victoria Street East Precinct Urban Design Framework was prepared for the City of Yarra in November 2005. It is a reference document (listed at clause 21.11) in the Yarra Planning Scheme and aims to guide the future development within the Victoria Street East Precinct, which includes the subject site.

The Urban Design Framework provides an indication of the desired heights and associated built form outcomes (refer to Figures 26, 27 and 28).

In relation to the subject site, it states:

- 'Change Industrial 3 Zone to Mixed Use Zone to enable a mix of commercial and high density residential uses. This would help to create an appropriate transition between Victoria Gardens and nearby residential areas, and would reflect existing commercial uses along Burnley Street.
- Business and Industrial Zones in the precinct are typified by buildings with no front or side setbacks. New development should maintain this building pattern with no setbacks from streets or adjoining buildings'
- Industrial and Commercial Interface: Maximum height RL 26 along street frontages. Increases in height to RL 30.5 maximum may be acceptable with upper level setbacks to ensure no detrimental impact on views of significant landmarks and no more overshadowing of the adjoining streets, public spaces or private properties beyond that caused by building to RL 26.
- Doonside Street should be the major vehicle entry.
- A north/south pedestrian link through the site'.

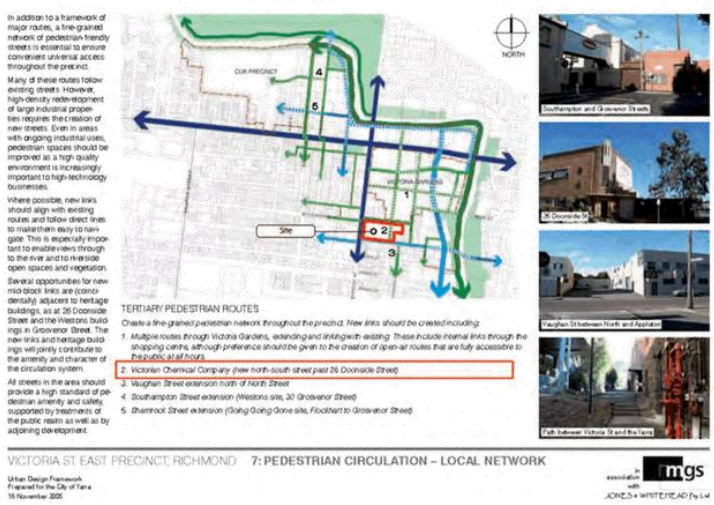

Figure 26 Pedestrian Circulation - Local Network (Victoria Street East Precinct UDF)

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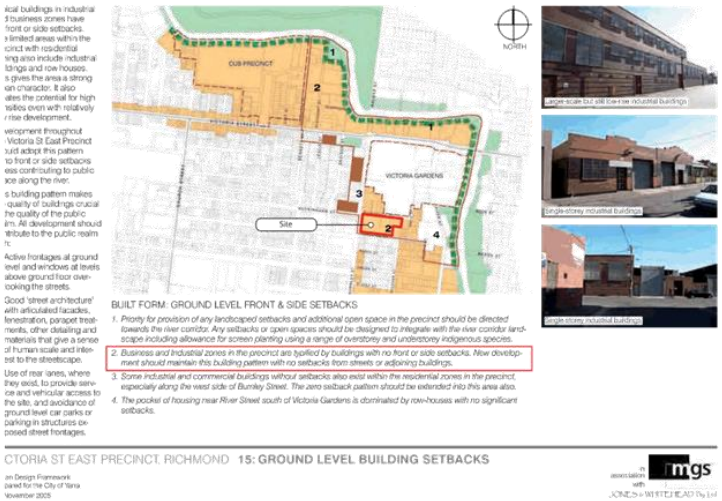

Figure 27 Ground Level Building Setbacks (Victoria Street East Precinct UDF)

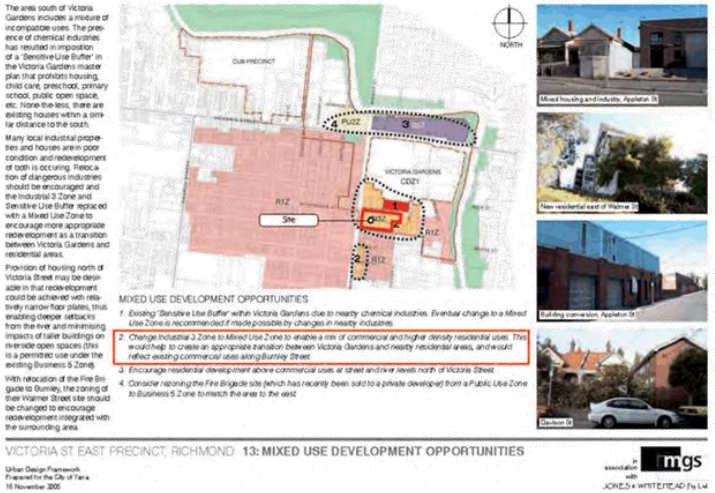

Figure 28 Mixed Use Development Opportunities (Victoria Street East Precinct UDF)

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3.8 Ministerial Direction No. 1 - Potentially Contaminated Land

The purpose of this direction is to ensure that potentially contaminated land is suitable for a use which is proposed to be allowed under an amendment to a planning scheme and which could be significantly adversely affected by any contamination.

There are two potential methods in satisfying this direction. This planning scheme amendment seeks to apply an Environmental Audit Overlay (EAO) over the site, which ensures that either a certificate of environmental audit needs to be issued for the land or an environmental auditor to make a statement that the environmental conditions of that land are suitable for the sensitive use, during the planning permit stage.

A preliminary Environmental Site Assessment was conducted in May 2016 by JBS&G to assess the potential use of the site for high density residential setting. The report concludes that ecological and human health exceedances can be reasonably managed in a subsequent high density residential development with potential commercial and associated mixed uses by removing or suitably capping the materials.

3.9 Ministerial Direction No. 11 - Strategic Assessment of Amendments

Ministerial Direction No. 11 is discussed within the associated Explanatory Report.

3.10 Policy Summary

The Yarra Planning Scheme and associated documents provide strong policy support for the proposed planning scheme amendment and development, in that it identifies the Site:

- As suitable for rezoning to a Mixed Use Zone (MUZ).
- As future mixed use land.
- As a strategic redevelopment site.
- As being in an area of substantial change.
- As having potential housing mixed with retail and business.
- As needing a new north-south street through the Site.

Attachment 2 - Planning report Tract Amendment C223

04 THE PROPOSAL

4.1 Overview

The vision and design philosophy behind the planning scheme amendment has been informed by State and local planning policy, site constraints and opportunities and the future business requirements of Harry the Hirer.

It aims to demonstrate an understanding of place and contribute to an economic, environmentally and socially sustainable outcome for the Site, building on the existing work in the Victoria Street Structure Plan.

The proposal is also underpinned by principles of liveability, and creating a site that offers a variety in housing typologies, innovation in public open space, and opportunities for integrated commercial premises to complement existing land uses and businesses.

4.2 Vision

Doonside Street is an area that is currently undergoing significant urban renewal.

This will see a marked change in the nature of the streetscape and the precinct - with many new residents calling the precinct home, and a marked visual change in the architecture of the area.

Over time, significant apartment, townhouse and mixed-use development to the north, and east of the Site will help to shape the new urban streetscape. In addition, the planned extension of Victoria Gardens to Doonside street will fill in the vacant lot, and help to complete the northern side of Doonside Street.

The vision for the western end of the Site is to create a distinct development character that celebrates and embeds the heritage building as the hero of the Doonside precinct. The former Russell Manufacturing Company facade will be restored to be the flagship 'front door' for the development.

In the east of the Site, the former Repco Offices and Laboratories building will be fully restored and is anticipated that it could be adaptivity reused as a commercial premises. Crafted around this is a new urban park, space that will help to celebrate the heritage building, and retain the existing mature tree to the street with its north facing setting. This urban park will be the breakout space for residents of the apartments and townhouses, as well as the green 'living room' for the wider Doonside Street precinct. It will feature hard and soft landscaping forms and spaces that are durable, contemporary and complement the urban nature of the surrounding development.

The park and a new north-south pedestrian street will link spaces with trees, seating, grassed and paved areas, and feature meeting spaces. This will create a clear sense of address and presence on the street for the proposed development, clearly visible from Burnley street and distinguished from the surrounding apartment projects.

Figure 29 Built Form Examples

Attachment 2 - Planning report Tract Amendment C223

4.3 Key Features

The Indicative Framework Plan (refer to Figure 31 - Indicative Framework Plan) illustrates the potential land uses and broad site elements of the proposed rezoning.

At this planning scheme amendment stage, detailed design of the site has not yet occurred. Further design refinement will occur at the planning permit stage, as such, plans should be considered conceptual only.

Area 1:

- Large commercial with residential above land use along the Burnley Street frontage, using the existing heritage facade, occupied by Harry the Hirer.
- Two residential tower forms (12 and 7 storeys respectively) above.
- Car parking in basement, accessed via lane off Doonside Street.
- 2 storey built form (retained heritage facade) to Doonside and Appleton Streets, except for the corner of Appleton and Burnley Street, which will retain the single storey facade.

Area 2:

- 2-3 storey podium with retail, residential and home office land uses at ground level.
- Retail with residential above along Doonside Street, wrapping around the western edge of the pedestrian link.
- Home office with residential above along the western edge of the pedestrian link.
- Lower scale residential interface to Appleton Street.
- Two residential tower forms (12 and 7 storeys respectively) above, scaling down to Appleton Street.
- Access lane into site from Doonside Street providing vehicle and waste access to both Area 1 and Area 2.
- Pedestrian link running north-south through entire block.
- Car parking in both basement and podium.
- Maximum 8-11m building height to Appleton with tower setback 13m behind.

Area 3:

- 2-3 storey podium with residential, community facilities, commercial, and home office land uses at ground level.
- Retention of former Repco Offices and Laboratories building and reuse as commercial premises on the western edge of the public open space.
- North facing public open space along Doonside Street frontage (with interface to both heritage building commercial and community facility land use).
- Commercial/community facility with residential above located along the southern edge of the public open space.
- Home office with residential above along the eastern edge of the pedestrian link.
- Lower scale residential interface to Appleton Street.
- Two residential tower forms (11 storeys and 8 to 5 and 3 storeys respectively) above, scaling down to Appleton Street and allowing for solar access to neighbouring development.
- Primary vehicle entrance to Doonside Street with car parking in both basement and podium.
- Maximum 8-11m building height to Appleton with tower setback 13m behind.

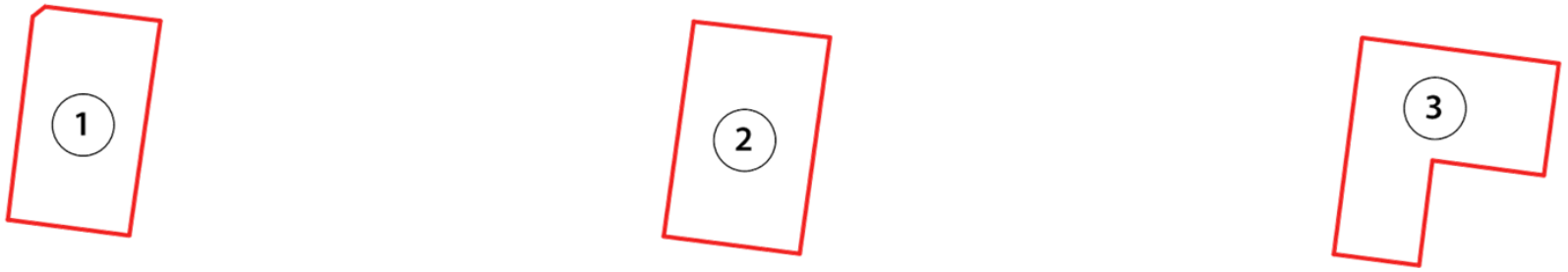

Figure 30 Key Features Plans (Areas 1, 2 and 3)

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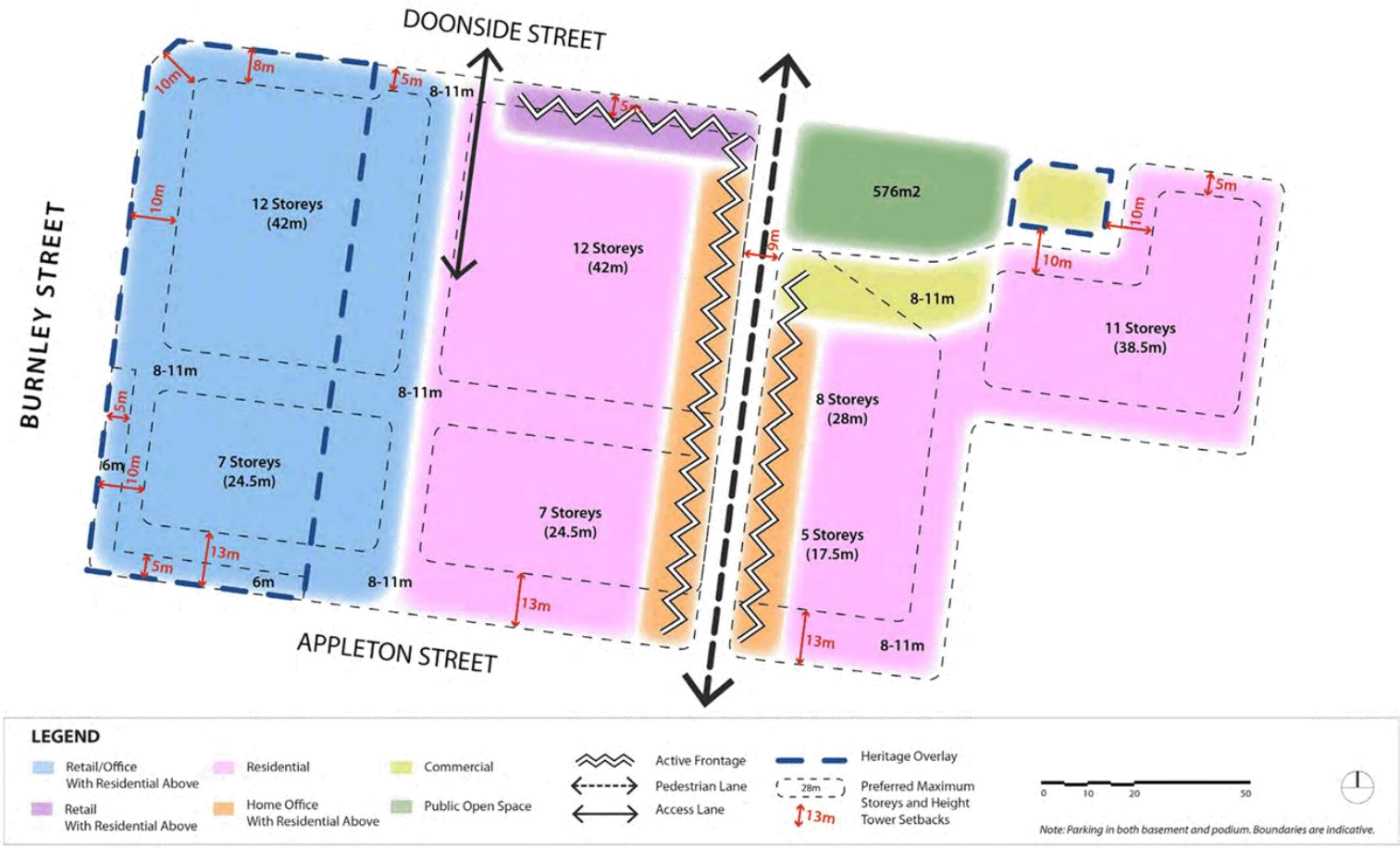


Figure 31 Indicative Framework Plan

Attachment 2 - Planning report Tract Amendment C223

4.4 Built Form

The proposed built form primarily consists of three areas, each with podium and associated residential tower forms above. Access and yields will be solidified at the planning permit stage, but as an approximation will consist of the following key elements:

- Residential: 37,038sqm (506 dwellings).
- Commercial: 6,585sqm.
- Retail: 701sqm.
- Parking: 21,367sqm (607 spaces).

Area 1:

Area 1 consists of a two storey commercial podium, incorporates the retention of the entire Burnley Street heritage facade and exhibits two separate residential towers ranging from seven to twelve storeys (refer to Figure 30 - Key Features Plans (Areas 1, 2 and 3)).

The single twelve storey residential tower on the corner of Doonside Street and Burnley Streets has been setback 10m from the Burnley Street heritage facade, a 10m setback from the heritage facade on the corner of Burnley Street and Doonside Street and 8m from the Doonside Street heritage facade, ensuring visual balance between the existing two/three storey corner brick facade and the proposed architectural response behind. This tower element will also combine with the seven storey tower immediately to the south to act as a visual 'Victoria Street Precinct' entry, effectively acting as a built form bookmark to the activity centre. Pedestrian entrance to this tower will be through the restored heritage entrance at the corner of Doonside and Burnley Streets.

Vehicle access is from the new access lane from Doonside Street (located in area 2) and provides both podium and basement levels.

The single seven storey tower sits behind the Appleton Street and Burnley Street corner and has had regard to the more sensitive detached residential interface to its south, reducing overshadowing and excessive visual bulk. Facing Appleton Street will be the retained heritage building at a single storey along the section within the heritage overlay, with a 5m setback to the podium level.

Area 2:

Area 2 consists of a three storey residential podium and exhibits two separate residential towers ranging from seven to twelve storeys (refer to Figure 30 - Key Features Plans (Areas 1, 2 and 3)).

The twelve storey residential tower located along the Sites northern boundary to Doonside Street is assessable via a pedestrian entrance along the western boundary of the north-south pedestrian link.

The minimum 9m separation between building forms allows for desired ventilation and sunlight penetration into all tower levels, while also creating physical opportunities for significant landscaping to occur on the roof of the podium.

The second seven storey tower is located along Appleton Street. This height has had regard to the more sensitive single and double storey residential interface along Appleton Street, resulting in a three storey street wall facing outwards along Appleton Street.

Area 3:

Along the eastern side of the north-south pedestrian link is a eight storey residential tower (refer to Figure 30 - Key Features Plans (Areas 1, 2 and 3)). The built form gradually steps down from its maximum height within the centre of the site to a five storey then three storey residential interface with Appleton Street. The residential tower is setback from the eastern boundary, which provides an appropriate distance between the approved eight storey development at 36-44 Appleton Street.

The residential tower located at the north-eastern point of the Site is eleven storeys in height. The eleven storey form proposes to sit alongside the approved twelve storey development at 36-44 Appleton Street to the east. The form allows for adequate solar access into the approved eight storey development to the immediate south.

The existing heritage building at 26 Doonside Street will be retained are adapted to facilitate a commercial use, and will sit comfortably alongside the new public open space and two-three storey residential and community facility podium behind.

Overall, each residential tower has had regard to the scale and land use context of surrounding properties, provides appropriate spacing between built forms to allow adequate ventilation, access to solar access and also facilitates desired view lines.

Materials and finishes will be varied and form a contemporary character that respects the built form history of the Site.

Attachment 2 - Planning report Tract Amendment C223

4.4.1 Public Open Space

The precinct does not currently enjoy good access to public open space.

The proposal allows for a generous amount of public open space (576smq) in Area 3 that directly abuts the retained and restored heritage building at 26 Doonside Street. The proposed public open space is north facing, and purposefully incorporates the existing, mature sugar gum tree.

Further, the north-south pedestrian link is centrally located within the Site and aligns with the proposed public open space. It facilitates the pedestrian routes identified in the Victoria Street Urban Design Framework which links with existing and proposed routes through Victoria Gardens Shopping Centre. The pedestrian link would exhibit a well landscaped setting, with provision for vegetation that responds to both the private realm of individual residential entrances, as well aiding the overall beautification of the public realm. The land uses abutting the proposed open space have also been considered to maximise positive interface elements such as passive surveillance and increasing active surveillance (refer to figure 32 - Public Open Space Indicative Detail).

As per the Yarra Open Space Strategy (2006) the criteria for provisions of new open space (small Local Park) is as follows:

- A minimum of 300sqm in size.
- Not located above car parks or other infrastructure.
- Minimum width in any direction of 10 metres.
- Entry from local access street (not a laneway).
- Good visual access to the reserve, preferably with two sides accessible.
- Where possible, link to the existing open space network via key pedestrian and/or cycling links.
- Receives reasonable sunlight access.
- Adjoining buildings do not disproportionately overwhelm the open space.
- Located where some borrowed green can potentially add value to the small reserve.
- Demonstrate there is no soil contamination that will limit safe use as open space.

As per these guidelines, the proposed public open space is well suited to the context of the site, and the provisions of the City of Yarra.

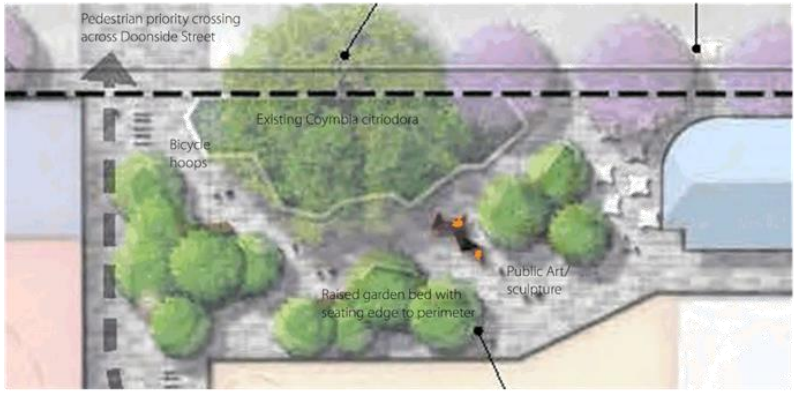


Figure 32 Public Open Space Indicative Detail

Attachment 2 - Planning report Tract Amendment C223

4.5 Heritage

There are two existing heritage buildings on the Site. The development proposes the following:

Former Russell Manufacturing Company building (Cr Burnley St & Doonside St):

- Restore the external art deco facade to its original state by stripping the blue paint.
- Reinstate original heritage signage along both Burnley Street and Doonside Street frontages.
- Utilise the existing entrance at the corner of Burnley Street and Doonside Streets to reinforce its prominance.

Former Repco Offices and Laboratories building (Doonside Street):

- Retention and restoration of the entire heritage building, including T.P Smith signage facing the proposed public open space.
- Adaptive reuse of the building as a commercial use.

Heritage considerations are discussed in greater detail within chapter 6.

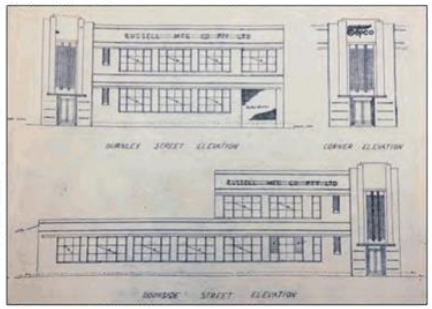


Figure 33 Original Repco building facade

Figure 34 Heritage Plan

Attachment 2 - Planning report Tract Amendment C223

4.6 Access

4.6.1 Pedestrian Access

The proposal provides for a north-south pedestrian street that acts as a break through the centre of the Site (refer to Figure 34 - Pedestrian Access).

The pedestrian link realises the vision of the Victoria Street East Precinct Urban Design Framework to create a fine grained pedestrian network throughout the precinct, and also aligns with the potential future pedestrian entrance of the Victoria Gardens Shopping Centre.

The pedestrian link increases the walkable catchment to allow existing residents along Appleton Street more permeability through the Site towards Victoria Gardens Shopping Centre.

The existing footpaths surrounding the Site are proposed to be upgraded and improved, including the introduction of street trees along the southern edge of Doonside Street and Appleton Street.

Pedestrian entries to buildings are located from Burnley Street, Doonside Street, Appleton Street and the north-south pedestrian street.

4.6.2 Vehicle Access

All major vehicle movements associated with the Site will be from Doonside Street, reflecting its busy character, including its wide streetscape, and current industrial and retail interface.

Due to the significant development within the surrounding areas, it is possible that the intersection of Burnley Street and Doonside Street may be signalised at some point in the future. There are two existing signalised crossovers along Burnley Street within close proximity to the Burnley Street and Doonside Street intersection.

Vehicle access to Areas 1 and 2 is from a proposed service lane, accessible only from Doonside Street. This will be utilised for both basement and podium parking arrangements, as well as waste movements.

Vehicle access for Area 3 is proposed from Doonside Street at the most eastern point of the Site.

Appleton Street will not be used for primary traffic movements.

A single, secondary vehicle entrance/exit is proposed along Appleton street at the furthest point away from the Burnley Street and Appleton Street intersection to secure the building facing Appleton Street.

No vehicle access is proposed along the north south pedestrian link, however provision has been made to ensure emergency vehicle access can be available if necessary.

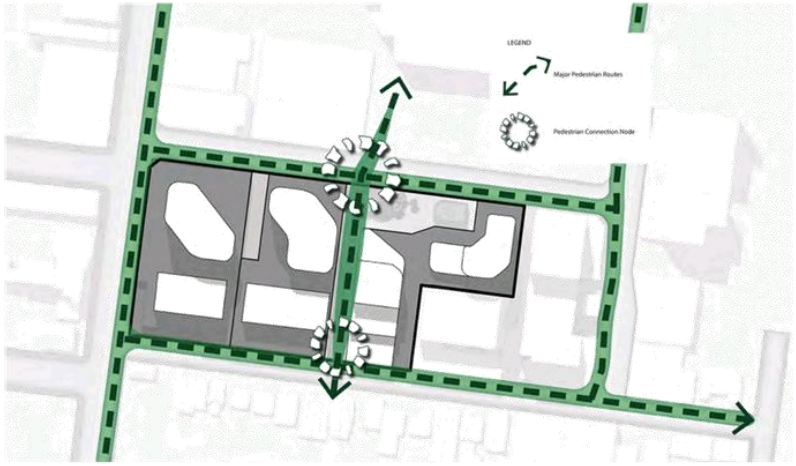


Figure 35 Pedestrian Access

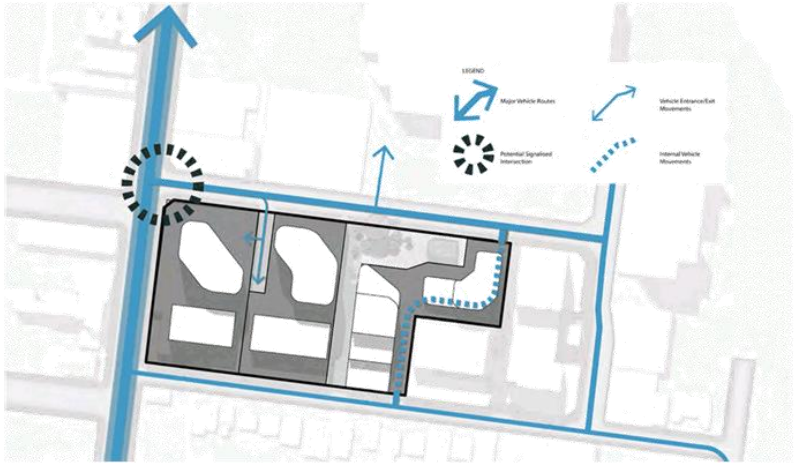


Figure 36 Vehicle Access

Attachment 2 - Planning report Tract Amendment C223

4.7 Interfaces

Southern Interface:

The southern interface along Appleton Street will consist of two built forms either side of the north-south pedestrian link.

- Towards the Burnley Street end, the existing heritage building facade will remain. There will be no change to the existing Appleton Street frontage, as it will remain an inward focused commercial with residential above land use. Above the existing street wall height will be a seven storey residential tower form setback 13m from the street frontage.
- Townhouses facing Appleton Street will exhibit a three storey street wall with a seven storey (inclusive of podium) residential tower setback 13m behind this podium.
- At either side of the north-south pedestrian street will be a three storey home office typology activating the street.
- East of the proposed north-south pedestrian street will be primarily three storey residential townhouses, with a single vehicle entrance / exit located the eastern most point. Above the streetwall will be a residential form stepping up to five and eight storeys in height.
- All existing street trees will be retained along Appleton Street.

Eastern Interface:

The eastern interface abuts a recently approved development at 36-44 Doonside Street. This approval is for the development of three residential towers at eight, ten and twelve storeys in height.

- The site abuts an approved three storey residential form with an eight storey tower setback from the street. This is responded to on the site with a similar three storey interface, graduating to five and eight storeys above.
- Along the Doonside Street frontage, the proposed built form abuts an approved pedestrian laneway, with a twelve storey residential tower form directly to it's east. The neighbouring pedestrian laneway is private and gated at both Doonside and Appleton Street ends. The proposal for the Site is for an eleven storey residential tower (including podium), which will have a strong aesthetic relationship with the approved twelve storey form directly to the east. Further, there is a significant physical break/separation between the proposed tower forms to allow adequate sunlight and views from the proposed tower directly to the south east.

Northern Interface:

The northern interface along Doonside Street will consist of four separate built forms, two on the eastern side of the north-south pedestrian street and another two along the western interface.

- To the east of the proposed public open space will be the retained and restored heritage building to be used for commercial purposes.
- Immediately to the west of the proposed north-south pedestrian street is a three storey podium with retail uses on the ground floor and residential uses on the two additional floors. Further, a twelve storey residential tower form is setback 5m above the podium height.
- Towards the Burnley Street end, the existing heritage facade will remain and be restored. Above the existing wall height will be a twelve storey residential tower form setback from the street frontage.

Western Interface:

The western interface along Burnley Street will consist of a two storey commercial podium with two residential tower forms setback behind 10m from Burnley Street.

- Towards the northern Burnley Street end, the existing heritage building will remain, and a twelve storey (in total) residential tower form setback 8m from Doonside Street and 10m from Burnley Street will be above.
- Towards the southern Burnley Street end, the existing heritage building will remain, and a seven storey (in total) residential tower form setback 10m from Burnley Street and 13m from Appleton Street will be above. The single storey heritage facade at the corner of Burnley Street and Appleton Street will be retained and the podium setback 5m behind.

Attachment 2 - Planning report Tract Amendment C223

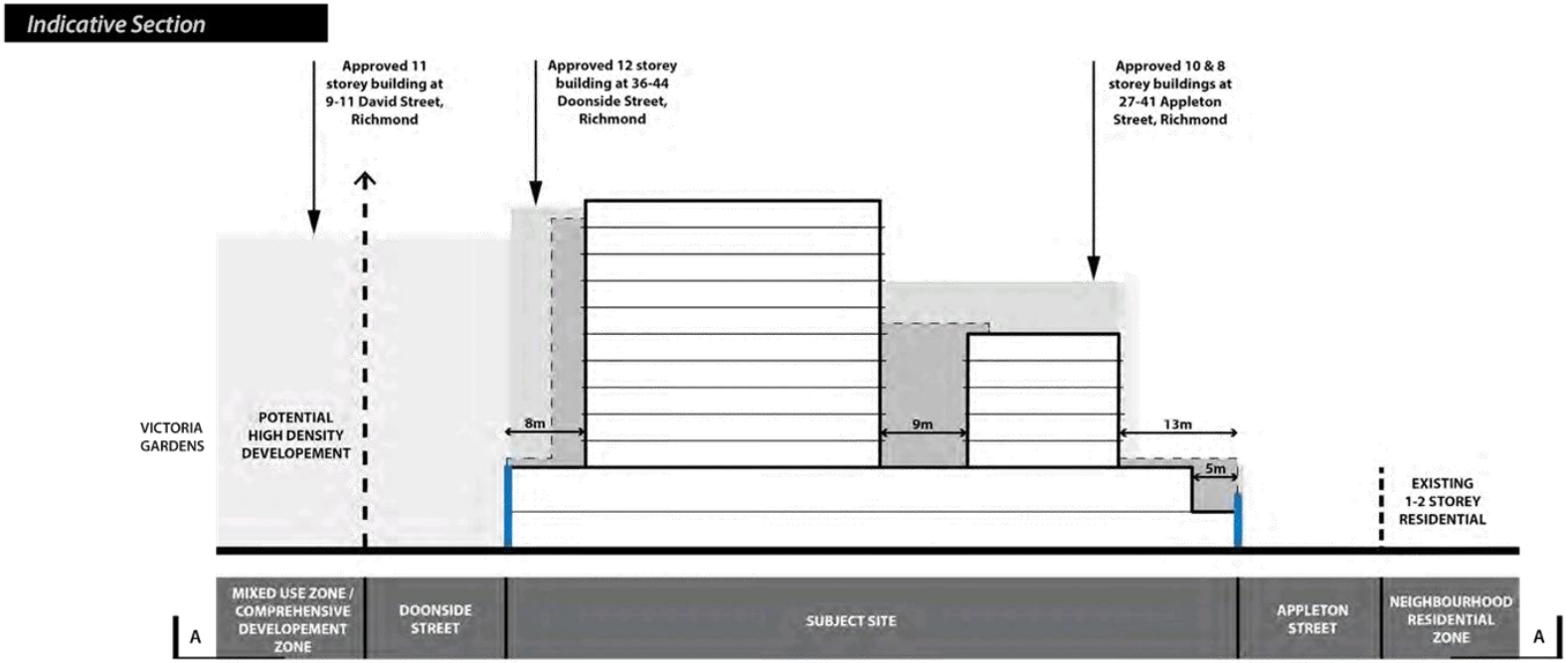


Figure 37 Burnley Street Indicative Section

Attachment 2 - Planning report Tract Amendment C223

4.8 Staging

The Harry the Hirer business will continue to operate on the Site, consolidating to Area 1. They will continue to operate primarily as a showroom and commercial use in the current building, while enabling the remaining areas 2 and 3 to be developed.

Over the longer term, Area 1 will be redeveloped with commercial and residential uses above, in line with the Indicative Framework Plan (refer to Figure 31 - Indicative Framework Plan).

In the interim, a temporary carparking structure, respectful of the heritage context, will be constructed above the existing commercial building in Area 1. A planning permit has been approved for this development. In line with this staging strategy, the Burnley Street heritage facade restoration works will occur when Area 1 is redeveloped in its ultimate form.

4.9 Planning Scheme Changes

The MUZ was chosen as the most appropriate zone because:

- The Yarra Planning Scheme has explicit statements that support a MUZ for the Site and a mixed use development outcome.
- The surrounding land is zoned MUZ.
- The Planning Panel for C99 supported the MUZ.
- The MUZ supports the preferred development outcome for the Site.

A DPO was chosen as the most appropriate overlay to guide built form because:

- It provides certainty in regards to both the future use and development.
- It allows for an integrated development of a large precinct.

The EAO is appropriate in the context of industrial land that is potentially contaminated and may be used for sensitive activities.

Attachment 2 - Planning report Tract Amendment C223

Figure 38 Indicative Burnley Street Section

Attachment 2 - Planning report Tract Amendment C223

05 LANDSCAPE ARCHITECTURE

This chapter summarises the key elements and considerations of the proposal in relation to Landscape Architecture. A further response will be undertaken at planning permit stage when more detailed plans are available.

5.1 Vision

The proposed transition from industrial to mixed use provides an opportunity to rethink the Site. Industrial heritage will be retained in part to provide architectural clues as to the Site's history.

New buildings will be constructed to accommodate new uses for living, working and leisure. Building design and public open space will encourage activation of the public realm.

Multi-level landscapes offer both public and private outdoor space. Street level public landscapes aim to forge new connections beyond the Site.

This will be a contemporary design, with improved civic outcomes for an evolving pocket of Richmond.

Figure 39 Public Open Space Context Plan

Attachment 2 - Planning report Tract Amendment C223

5.2 Open Space Analysis

Bisecting the proposed building mass will be a new north-south pedestrian spine, providing benefit not only the site, but also to the neighbourhood more broadly. It will connect Appleton Street to Doonside Street, with a potential future link through to Victoria Gardens Shopping Centre.

A pocket park at the north termination of the north-south pedestrian link would provide a community, outdoor social space. This location would capitalise on cultural and environmental factors; the heritage building with its curved frontage and the large existing Lemon-scented Gum (Corymbia citriodora).

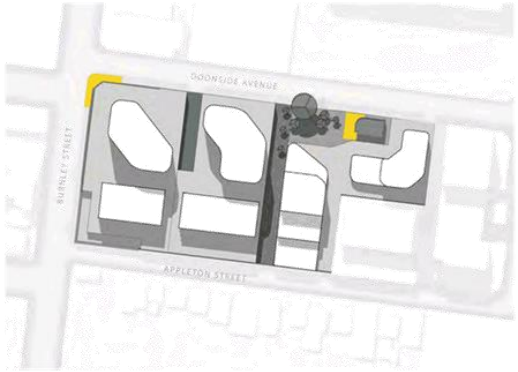
In addition to the public open space at ground level, there is the potential to create communal open spaces and private spaces at podium level.

5.3 Key Design Elements

Key organising elements of the proposed landscape design are featured in Figure 40 - Key Design Elements.

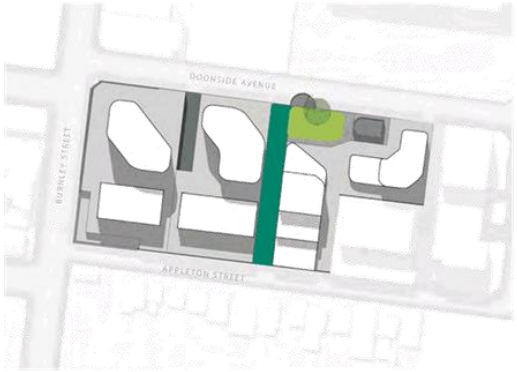
Figure 40 Key Design Elements Plan

Attachment 2 - Planning report Tract Amendment C223



Plaza

Celebrate north-west corners of the two heritage buildings with urban plaza treatments.

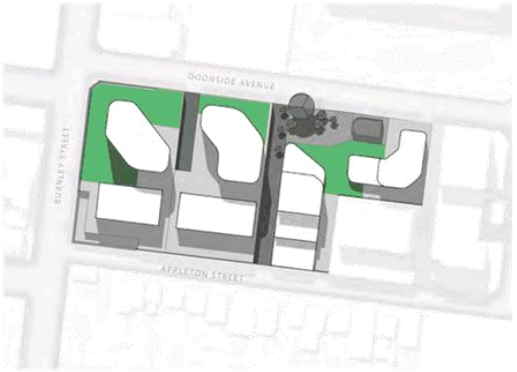


Pocket Park & Pedestrian Link

Public park with gardens and generous seating to provide pocket size urban sanctuary.

Existing mature Corymbia citriodora (Lemon-scented Gum) to be retained and made central feature of the park, up-lit at night.

Park merges with north-south pedestrian link with a seamless transition achieved through consistent material palette and geometry.

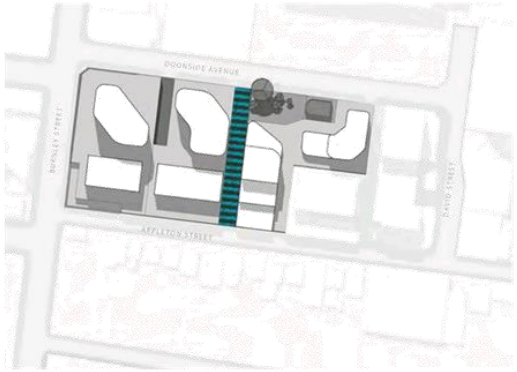


Roof Gardens

Significant opportunities for both communal and private roof gardens with social and ecological benefits.

Kitchen gardens, viewing areas, dining and play spaces suggested, as well as 'urban forest' for carbon sequestration, thermal stability and attractive outlook.

Attachment 2 - Planning report Tract Amendment C223

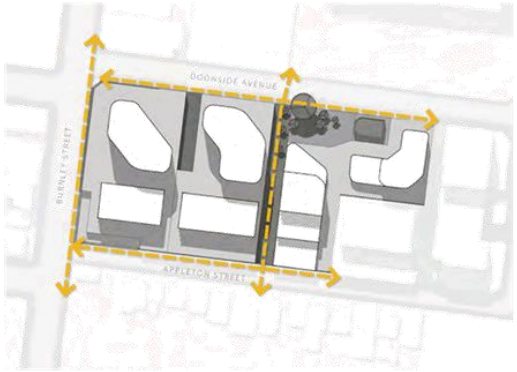


North-South Pedestrian Link

North-south oriented, publicly accessible walkway with emergency vehicle egress to connect Appleton Street and Doonside Street.

Future potential to link to shopping centre to the north.

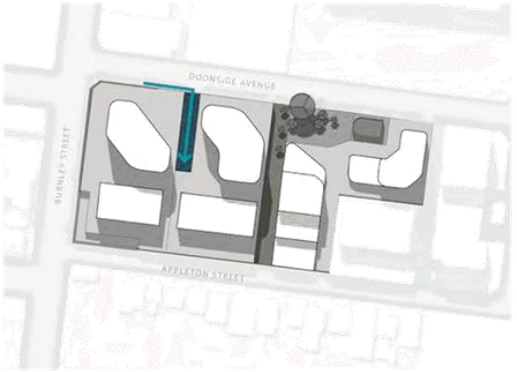
Planting, paving, furniture and lighting to create an inviting public thoroughfare, terminating in a new public pocket park to the north.



Pedestrian Movement

Currently framed on three sides by public footpaths.

Additional proposed north-south public walkway to achieve physical permeability for pedestrians and cyclists.

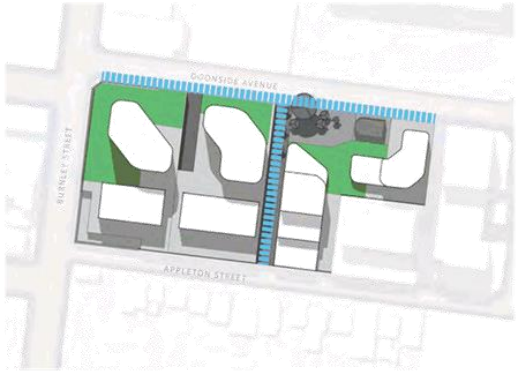


Service Lane

Vehicle access to basement and carpark podiums for service deliveries and residents.

Lane provides opportunity for public wall art and sculptural lighting to animate the space.

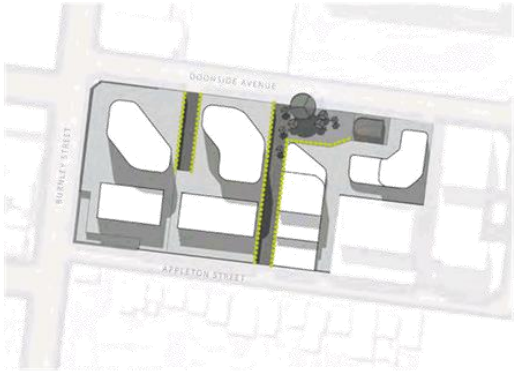
Attachment 2 - Planning report Tract Amendment C223



Water Sensitive Urban Design

Rain gardens to walkway to collect and treat storm water.

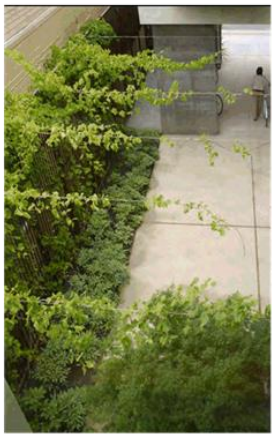
Irrigation from non-portable water supply.



Vertical Green

Priority for vertical greening to buildings facing public open space.

Aids passive building cooling.



Attachment 2 - Planning report Tract Amendment C223

Proposed Plant Schedule

Native and indigenous plant species to be showcased, providing habitat for birds and insects.

Plants selected for low-water use and microclimatic suitability.

Canopy trees to Doonside Street to reduce urban heat island by providing shade in summer.

Refer to Figure 41 - Ground Level Landscape Opportunities.

Rain Garden Vegetation

- 1. Ficinia nodosa (Knobby Club Rush)
- 2. Lomandra longifolia (Mat-rush)
- 3. Eucalyptus Caesia 'Silver Princess (Silver Princess) Indigenous tree.

Doonside Street Trees

- 4. Jacaranda mimosifolia (Jacaranda)
- 5. Pistacia chinensis (Chinese Pistache)

Attachment 2 - Planning report Tract Amendment C223

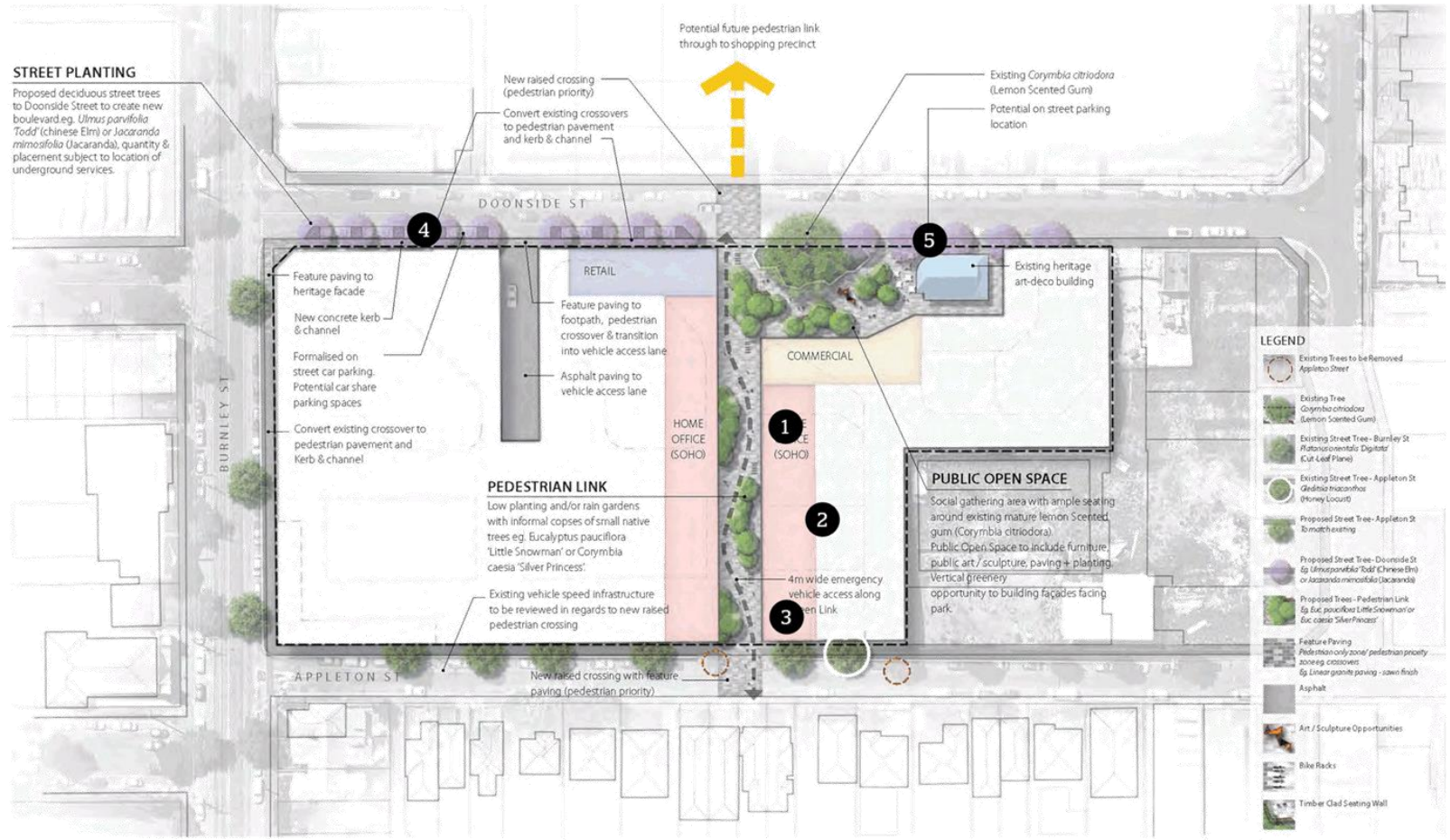


Figure 41 Ground Level Landscape Opportunities

Attachment 2 - Planning report Tract Amendment C223

Figure 42 Roof Gardens Landscape Opportunities

Attachment 2 - Planning report Tract Amendment C223

06 HERITAGE

This chapter summarises the key elements and considerations of the proposal in relation to heritage. A full report titled 'Proposed Rezoning of Land - Analysis of Heritage Issues' (revised December 2017) by Bryce Raworth is available and should be referred to in addition to these summaries.

6.1 Current Listings and Controls

The land at 81-95 Burnley Street is partially covered by a site-specific heritage overlay HO375 – 'Russell Manufacturing Pty Ltd later Repco'. The heritage overlay encompasses the pre 1945 office building and factory on the site to a depth of 34 metres from the Burnley Street frontage. The former Repco Offices at 26 Doonside Street is separately listed on the heritage overlay schedule as HO252. External paint controls apply to both HO252 and HO375 but there are no internal alteration controls or tree controls. The balance of the subject site has no heritage overlay control.

The heritage overlay control at 81-95 Burnley Street was implemented in 2015 under Amendment C149 to the Yarra Planning Scheme. Council had proposed to incorporate this site into a broader heritage overlay precinct as part of Amendment C173 (Part 2). The proposed Doonside Industrial Precinct (HO455) also encompassed the sites at 1-9 Doonside Street and 61-63, 65, 67 and 77-79 Burnley Street – all of which were subsequently excluded from consideration by the Panel. The Panel was of the view that the remaining properties did not constitute a precinct. To this end, the Panel ultimately recommended the Amendment be abandoned in so far as it affected 81-95 Burnley Street.

6.2 History

In 1929, the east side of Burnley Street in the vicinity of Appleton Street was recommended as a factory area. This area took in land originally occupied by Doonside, the residence of David Mitchell (Dame Nellie Melba's father). The Doonside Estate was subdivided and offered for sale in May 1931, providing 34 lots on Burnley, Doonside, David and Appleton Streets. Predating the Doonside Estate subdivision, a fibrous plaster factory was built c1925 on the north-east corner of Burnley Street and Appleton Street (first listed as in Sands and McDougall Directory as 89-95 Burnley Street) (refer to Figure 41 - 1930 Plan of Subdivision for Doonside Estate).

Around 1939, Repco/Russell Manufacturing constructed a double-storey office and laboratory building at present day 26 Doonside Street. In March 1942, the company purchased additional land and expanded northwards along Burnley Street towards the Doonside Street corner.

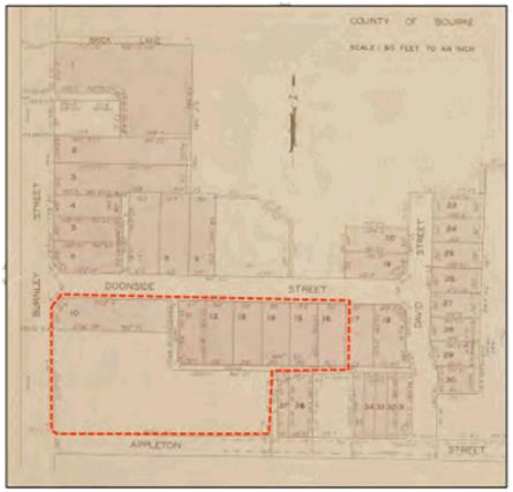


Figure 43 1930 Plan of Subdivision for Doonside Estate

6.3 Significance

The Yarra Heritage Database includes the following statement of significance for the two heritage buildings on the Site:

81-95 Burnley Street, Richmond

What is significant?

The Russell Manufacturing Company Pty Ltd (later Repco) building at 81-95 Burnley Street, Richmond is significant to the extent of the pre-1945 fabric. Built in stages for the Russell Manufacturing Company Pty Ltd, the brick (overpainted) building has a strong Moderne styling, with horizontal banding on the main elevations.

Post-1945 alterations and additions to the building are not significant.

How is it significant?

The Russell Manufacturing Company Pty Ltd (later Repco) building is aesthetically and historically significant to the locality of Richmond and the City of Yarra.

Why is it significant?

The Russell Manufacturing Company Pty Ltd (later Repco) building is esthetically significant:

- For its strong Moderne styling as ideally presented on a corner site.
- For the relationship with the significant Moderne style former Repco Building at 26 Doonside Street (HO256).

The Russell Manufacturing Company Pty Ltd (later Repco) building is historically significant (Criteria A & H):

- As tangible evidence of the large factories built during the interwar period when Richmond became a centre of manufacturing in Victoria.
- For the association with the successful motor spare parts firm of Russell Manufacturing Company Pty Ltd.

Attachment 2 - Planning report Tract Amendment C223

26 Doonside Street, Richmond

What is significant?

The building, built c.1939 as an office and laboratories for the Russell Manufacturing Co., which later became Repco at 26 Doonside Street, Richmond is significant. It is a two storey Moderne style bichromatic brick building. It is approximately square in plan, with a curved corner at the northwest. The building is oriented north-west, and the composition of the main panels of brickwork is approximately symmetrical about a diagonal axis which runs through the corner entrance, which has a cantilevered concrete canopy. The north and west elevations are of face manganese brick, whilst large panels of cream brick give the appearance of wrapping around this, leaving a vertical strip of dark brown brick above the entrance. This corner element is decorated with a narrower vertical strip of horizontally-striped tapestry brickwork, and surmounted by three white painted vertical concrete fins. The north elevation features two bands of windows, each comprising three panels of multi-paned steel-framed windows with manganese brick spandrels and sills.

These windows turn the corner to the east elevation; to their right are two vertically placed circular windows, probably to a staircase. The west wall of the building was once attached to a single-storey building which has since been demolished, with the exception of part of the front wall and cream brick parapet which adjoins No. 26.

How is it significant?

The former Repco office and laboratory building at 26 Doonside Street, Richmond, is of local architectural significance to the City of Yarra.

Why is it significant?

It is architecturally and aesthetically significant as a particularly sophisticated example of a small building in the Moderne style, which exhibits an interesting composition of limited palette of materials. It is thus distinctive for a building of its size and type. The demolition of other adjacent buildings has increased the aesthetic contribution of this building to an otherwise architecturally undistinguished industrial streetscape.

Figure 44 1943 Illustration of Repco Building

Figure 45 1957 Photograph of Repco Building

6.4 Analysis

The heritage building at 26 Doonside Street would be retained as a free standing entity with public open space to the west side exposing the principal facade to public view and more generally helping to maintain the legibility of the original building envelope. The front parts of the heritage building to Burnley Street would also be retained along with its side elevation to Doonside and Appleton Streets (to an extent corresponding with the HO boundary).

The former factory buildings on the subject site can, from a heritage perspective, sustain development of the scale proposed in the revised Indicative Framework Plan without adverse impacts. Within the Heritage Overlay, the relationship of new and existing built form at the street level is the key issue and this will be handled through the low-rise podium elements equivalent to the height and setbacks of the heritage facades.

A crucial point to be made in relation to heritage issues is that the majority of the site is outside of the heritage overlay, and is therefore not subject to heritage provisions of the Yarra Planning Scheme. In these circumstances the key design parameters for new development would be urban design requirements and other local planning policies rather than the heritage overlay.

The proposed rezoning and future redevelopment of the site as contemplated in the associated documentation will inevitably result in a substantial degree of change. However, this change is consistent with the emerging character of the area. The scale and location of multi-storey development in relation to the retained heritage buildings on the subject site also follows precedent established in other approved and existing developments within the City of Yarra involving industrial heritage places.

Attachment 2 - Planning report Tract Amendment C223

07 TRANSPORT

This chapter summarises the key elements and considerations of the proposal in relation to Traffic and Transport. A full report titled 'Proposed Mixed Use Rezoning' by Ratio (revised December 2017) is available and should be referred to in addition to these summaries. Further traffic and transport analysis will be undertaken at planning permit stage when more detailed plans are available.

7.1 Overview

The traffic impact report was prepared to address the traffic and parking needs of the proposed rezoning and development, and is based on surveys and observations in the vicinity of the site and on previous studies of similar developments elsewhere in Melbourne.

7.1.1 Existing Operation

'Harry the Hirer' currently operates during the following hours:

- 8:00am-5:30pm weekdays.
- 9:00am-12:00 noon Saturdays.
- Closed Sundays.

There is typically up to 107 staff on-site at any one time. Loading generally occurs within the above mentioned concrete hardstand area.

7.2 Strategic Background

7.2.1 Victoria Street East Precinct, Richmond Urban Design Framework (UDF)

The UDF identifies a major objective to minimise the use of private motor vehicles, stating the following:

'Encourage the use of public transport, cycling and walking for access between the precinct and other parts of Melbourne, in preference to use of private motor vehicles'.

In addition, the UDF makes the following recommendations;

- Create a new north-south pedestrian link past 26 Doonside Street (i.e. through the eastern end of the subject site); and
- Improve Doonside Street as a major vehicular entry to Victoria Gardens and the area to its south, to keep traffic away from Victoria Street and housing south of Appleton Street. Doonside Street should be widened (on the north side, to create an overall road reserve width of 18 to 20m) and extended north into Victoria Gardens via David Street.

7.3 Road Network, Traffic and Parking Conditions

7.3.1 Burnley Street

Burnley Street is a Declared Main Road which generally extends in a north-south direction from Victoria Street to Barkly Avenue.

In the vicinity of the site, Burnley Street operates with a single traffic lane and a single bicycle lane in each direction, with parallel parking permitted along both kerbs clear of traffic. Constructed footpaths are provided along both sides of Burnley Street.

7.3.2 Doonside Street

Doonside Street is a local road which generally extends in an east-west direction from David Street to Burnley Street.

Doonside Street has a carriageway width of approximately 9.7 metres, allowing for a single lane of traffic in each direction, with parallel parking permitted along both kerbs clear of traffic. Constructed footpaths are provided along both sides of Doonside Street.

The Doonside Street/Burnley Street intersection is a 'Stop' controlled t-intersection, with priority given to Burnley Street.

Approximately 15 metres to the south of the Doonside Street/Burnley Street intersection (centre to centre) is the Buckingham Street/Burnley Street intersection, forming a staggered intersection arrangement. The Buckingham Street/Burnley Street intersection is 'Give Way' controlled, with priority given to Burnley Street.

7.3.3 Appleton Street

Appleton Street is a local road which extends generally in an east-west direction from Clark Street to Burnley Street.

Appleton Street has a carriageway width of approximately 7.9 metres, which allows for two directions of travel, with parallel parking permitted along both kerbs. Where vehicles are parked on both sides, the effective width for through vehicles is reduced to a single lane, allowing for a single direction of travel at any one time in between passing opportunities.

Constructed footpaths are provided along both sides of Appleton Street.

7.3.4 Parking Conditions

During the day, parking in the survey area is generally a mixture of unrestricted and time restricted (generally 1P and 2P) parking. At night, parking in the survey area is generally unrestricted. For the purposes of this assessment, parking with a restriction of less than 1 hour has not been considered suitable for use by visitors to the site and as such, has been excluded from the results.

Attachment 2 - Planning report Tract Amendment C223

7.4 Sustainable Transport

7.4.1 Public Transport

The subject site has good access to public transport, with numerous tram routes operating within close proximity of the site. The nearest train station is Burnley Station, located an approximate 1.4 kilometre walk from the subject site, whilst North Richmond Station is readily accessible via tram routes 12 and 109 and Hawthorn Station is readily accessible via tram routes 48 and 75 (refer to Figure 46 - Public Transport Provision).

7.4.2 Bicycle Network

The subject site has good access to Melbourne's bicycle network. Specifically:

- On-street bicycle lanes along the site frontage on Burnley Street provide access to the north towards Abbotsford and to the south towards Burnley.
- On-street bicycle lanes on Church Street are accessible via Burnley Street and Highett Street and provide access to the north towards Abbotsford and Collingwood and to the south towards Cremorne and South Yarra.
- The Capital City Trail is accessible via the northern end of Burnley Street, whilst the Main Yarra Trail is accessible via the southern end of Burnley Street.

7.4.3 Car Share

The subject site is located within close proximity to a number of share car pods operated by Flexicar, GreenShareCar and GoGet (Refer to Figure 47 - Nearest Share Car Pod Locations).

The nearest pods are operated by Flexicar and GreenShareCar (1 car each), located on Burnley Street outside of the Victoria Gardens Shopping Centre, approximately 100 metres to the north of the site. A larger pod operated by GoGet (9 cars) is located on Level 3 of the IKEA car park at the Victoria Gardens Shopping Centre, an approximate 400 metre walk from the site.

Service	Route No's	Route	Nearest Stop	Approximate Walking Distance
Tram	12	Victoria Gardens – St Kilda	Burnley St / Victoria St	350m
	109	Box Hill – Port Melbourne		
	48	North Balwyn – Victoria Harbour Docklands	Burnley St / Bridge Rd	450m
	75	Etihad Stadium Docklands – Vermont South		
	78	North Richmond – Balaclava via Prahran	Kent St / Church St	850m
Train	Glen Waverley, Alamein, Belgrave and Lilydale Lines		Burnley Station	1.4km
	Hurstbridge and South Morang Lines		North Richmond Station	1.8km (accessible via tram routes 12 and 109)
	Alamein, Belgrave and Lilydale Lines		Hawthorn Station	2km (accessible via tram routes 48 and 75)

Figure 46 Public Transport Provision

Operator	Share Car Pod Location	Number of cars	Approximate Walking Distance
Flexicar	Burnley St, outside Victoria Gardens Shopping Centre	1 car	100m
	Burnley St, near corner of Palmer St	1 car	400m
GreenShareCar	Burnley St, outside Victoria Gardens Shopping Centre	1 car	100m
GoGet	Level 3 of IKEA car park at Victoria Gardens Shopping Centre	9 cars	400m

Figure 47 Nearest Share Car Pod Locations

Attachment 2 - Planning report Tract Amendment C223

7.5 Recommended Parking Rates

The site is well located for future residents who choose not to own a car, noting the site's close proximity to a wide range of retail (including a full line supermarket) at Victoria Gardens Shopping Centre and numerous sustainable transport options.

Whilst the apartment breakdown has not been prepared at this stage, the preliminary yield analysis indicates that an overall parking provision in the order of 0.8 spaces per dwelling is expected. This generally reflects the reduced indicative parking provision rates recommended in Figure 46 - Indicative Recommended Parking Rates.

Component	Indicative Recommended Parking Rate
Residents	0.7 spaces to each one bedroom dwelling
	0.9 space to each two bedroom dwelling
	1.5 spaces to each three bedroom dwelling
Visitors	0.1 spaces to each dwelling
Retail	1 space to each 100 square metres of leasable floor area for staff
Commercial	2.5 spaces to each 100 square metres of leasable floor area

Figure 48 Indicative Recommended Parking Rates

7.6 The Proposal

7.6.1 Proposed Internal Streets

An internal service lane is proposed to extend south from Doonside Street along the eastern side of the Area 1 and 2 boundary. The service lane is not proposed to connect through to Appleton Street. The primary function of the service lane is for vehicle access.

It is recommended that the service lane is a minimum of 5.5 metres wide to allow for simultaneous two way traffic flow.

An internal north-south pedestrian link is proposed along the Area 2 and 3 boundary to connect from Doonside Street through to Appleton Street. It is proposed for the link to primarily cater for pedestrians. The pedestrian link is not proposed to be used by general traffic but would be available for use by emergency vehicles as required.

7.6.2 Clause 52.06 Parking Requirements

Parking requirements for residential developments are set out under Clause 52.06 of the Yarra Planning Scheme.

Attachment 2 - Planning report Tract Amendment C223

Lot	Component	Dwellings	Commercial	Retail	Car Spaces
1	Tower A (12 storeys)	110 dwellings	-	-	-
	Tower B (7 storeys)	40 dwellings	-	-	-
	Podium	-	5,481 sqm	-	57 spaces
	Basement	-	-	-	222 spaces
2	Tower C (12 storeys)	108 dwellings	-	-	-
	Tower D (7 storeys)	36 dwellings	-	-	-
3	Podium	53 dwellings	-	551 sqm	173 spaces
	Tower E (11 storeys)	76 dwellings	-	-	-
	Tower F (8 storeys)	74 dwellings	-	-	-
	Podium	9 dwellings	953 sqm	-	51 spaces
	Heritage Building	-	150 sqm	150 sqm	-
	Basement	-	-	-	104 spaces
Total		**506 dwellings**	**6,585 sqm**	**701 sqm**	**607 spaces**

Figure 49 Preliminary Yield Analysis

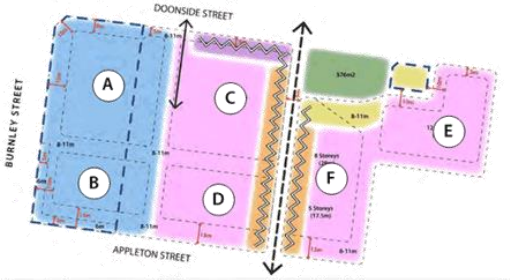


Figure 50 Indicative Tower Locations

7.6.3 Green Travel Plan

It is recommended that as part of any town planning application, a Green Travel Plan be prepared to further encourage utilisation of the wide availability of sustainable transport alternatives and subsequently reduce parking demands and traffic generation associated with the proposal.

7.6.4 Bicycle Parking

It is recommended that a bicycle parking provision in excess of the statutory requirements is considered for residents and office staff in particular, with bicycle parking provided generally in accordance with the following indicative rates:

- 1 bicycle space to each dwelling for residents.
- 1 bicycle space to each 150 square metres for office staff (double the statutory rate).

7.6.5 Loading Facilities

For smaller retail tenancies, it is typically considered impractical to provide an on-site loading bay, with loading likely to be undertaken by vans and small trucks which can undertake loading within an on-street car space.

In the case of the proposal, it is considered that loading for the smaller retail tenancies could be undertaken either on-street (Doonside Street or Burnley Street) or on-site.

For any future restricted retail use on the site, it is recommended that onsite loading be provided in accordance with the requirements of Clause 52.07.

7.6.6 Summary

The traffic anticipated to be generated by the proposal can be accommodated by the existing road network, subject to the intersection of Burnley Street and Doonside Street being upgraded during the later stages of development of the subject site and surrounding sites.

Attachment 2 - Planning report Tract Amendment C223

08 ECONOMIC & EMPLOYMENT

This chapter summarises the key elements and considerations of the proposal in relation to Economics and Employment. A full report titled '81-95 Burnley Street and 26-34 Doonside Street, Richmond Economic Assessment' by Deep End Services (revised November 2017) is available and should be referred to in addition to these summaries.

8.1 Overview

Deep End Services have assessed the proposed rezoning in terms of:

- Strategic and market justification for a mixed use development of the type proposed.
- Analysis of the local and regional economic outcomes including:
  - Capital investment on the site.
  - Employment generation during construction (direct on-site and indirect employment).
  - Ongoing employment generation (direct and indirect).
  - Indicative rates revenue.
  - Local expenditure flows associated with new residents.
  - Support for local businesses within the local area.

8.2 Economic Context

8.2.1 Residential Development

Over the period July 2014 to September 2017 permits have been granted for a total of approximately 4,096 new residential dwellings in the City of Yarra, of which 86% (or 3,507 units) were for high density apartments 4 storeys or more, and another 9% for semi-detached residences, townhouses and terraces and apartments 3 storeys or less. Permit data shows that high density apartments are focussed on the City of Melbourne and other inner-city municipalities to the north, east and south.

Information held by Deep End Services on residential projects (as at November 2017) confirms that apartment developments have been successfully delivered in the local area. Our data shows that over 3,200 new apartment units have been delivered in major developments (ie projects with 4 or more storeys) within the suburbs of Abbotsford, Richmond and Cremorne over the period 2010 to 2017, representing an average absorption rate of more than 450 units per year.

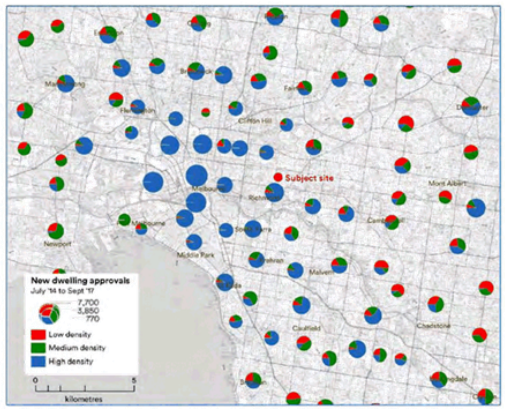
Overall, development of the site to accommodate new apartments responds to evidence of strong market demand in the area. The anticipated yield of 506 units would easily be absorbed over a staged development time frame.

8.2.2 Commercial Development

Commercial development will comprise the retention of the Harry the Hirer business, involving an estimated 100 staff remaining on-site, and the inclusion of new opportunities for local-level retail, food and beverage, home-office and community services provision.

It is understood that Harry the Hirer will be retaining their existing showroom over the medium to long term in accordance with their recent property and operations review. A substantial internal refurbishment has recently been completed to support the medium to long term retention of the business in this location.

With respect to other small-scale retail opportunities, the total future population would support the provision of local level retailing services at the scale proposed, involving small convenience shopping, food and beverage and retail services, and possibly some non-retail business services.



Source: ABS Building Approvals Australia by SA2 geography (Cat. No. 8731.0)

Figure 51 Dwelling approvals by type between July '14 to September '17

Attachment 2 - Planning report Tract Amendment C223

8.3 Economic Outcomes

8.3.1 Capital Investment

Advice provided by Astrodome Hire is that the total investment in the construction of the project (excluding land purchase costs) is estimated at approximately $170 million.

This represents a significant investment that will improve the value and amenity of the property and lead to more intensive use of the land.

8.3.2 Construction Employment

An estimated 890 construction-related jobs will be directly created on-site over the life of the project, representing a total of 390 full-time equivalent job-years over an indicative 30 month construction period.

Another 1,295 job-years will be created indirectly throughout the local and wider economy as a result of consumption and production multiplier effects.

8.3.3 Ongoing Employment

Ongoing employment will be generated as a result of:

- The retention of Harry the Hirer.
- Commercial office space above Harry the Hirer.
- The inclusion of small-scale retail development opportunities.

Use	Employment rate	Positions	Direct (FTE)	Indirect (FTE)	Total (FTE)
Harry the Hirer					
- existing staff		115	98	205	303
- future staff		100	90	185	275
Net change		*-15*	*-8*	*-20*	*-28*
Other uses					
Retail	40 sqm/job	18	13	20	33
Office	25 sqm/job	107	81	210	291
Home-based employment	3% of population work from home	25	24	65	89
Net employment		+135	+110	+275	+385
Total on-site employment		*250*	*210*	*480*	*690*

Figure 52 Ongoing (on-site) employment estimates

- Home-based businesses including those within the proposed home-office housing product.
- Proposed commercial/community facilities at podium level in the north-east corner of the site.

The total net employment increase arising from the development is estimated at approximately +135 jobs (refer to Figure 52 - Ongoing (on-site) employment estimates).

8.3.4 Rates and Charges

The development would generate a significant increase in rates revenue for the City of Yarra as a result of the uplift in the capital improved value of the site.

Based on information from Astrodome Hire, the completed project would have a capital value of approximately $375m. Assuming that this is equivalent to the capital improved value used for the purposes of calculating rates, the development would generate:

- $866,500 in annual rates income for the City of Yarra.
- $82,500 pa contribution to the State Government's Fire Services Levy (FSL).

Land tax would be in addition to the figures above, but has not been calculated in this case because the proportion of owner-occupier residents (not subject to land tax) is uncertain.

8.3.5 Expenditure Flows

On completion the development would have a resident population of approximately 760 persons based on an average 1.5 persons per dwelling.

The residential community will generate a total of $16.4m of annual spending on retail goods and services. This is presented as an estimate for the year 2026 to indicate approximately when the development may be completed, and is based on modelling the existing expenditure levels for residents living in Richmond and Abbotsford.

Given the proximity and extent of retail services available at Victoria Gardens and elsewhere in the City of Yarra, a substantial share of resident spending (probably up to 75% or $12.3m per year) will be retained to support retail businesses in the local area.

8.4 Summary

In summary, the proposed construction of a mixed use development on the property at 81-95 Burnley Street and 26-34 Doonside Street would generate the following potential economic outcomes:

Investment

- Total construction investment of $170 million.

Construction effects

- Total of 390 direct full-time equivalent jobs created over the approximately 30 month construction period, and another 1,295 jobs (FTE) created indirectly over the construction period.

Ongoing employment

- 250 positions created on-site, equivalent to 210 FTE jobs
- 480 FTE jobs indirectly generated in the wider economy, bringing total employment creation to 690 FTE jobs
- A net increase of around 110 FTE jobs.

Rates and charges

- An estimated contribution of $866,500 each year to the City of Yarra in rates income and an estimated $82,500 pa contribution to the State government as a fire service levy.

Resident spending

- An injection of $16.4m pa in retail spending by new residents, around $12.3m pa of which would be captured by local retailers, as well as approximately $20m pa in other spending, some of which would support local providers of health, personal and business services.

Other outcomes

- Opportunities for local businesses during the construction and ongoing operation stages.
- Increased activity within the Victoria Road Activity Centre and provision of local facilities for nearby residents.
- Support for local policy which anticipates transition of the area to mixed use development.
- Increased employment intensity in accordance with local and State planning policy.

Attachment 2 - Planning report Tract Amendment C223

09 ENVIRONMENTALLY SUSTAINABLE DESIGN

This chapter summarises the key elements and considerations of the proposal in relation to Environmentally Sustainable Design. A full report titled 'EDS Report 81-95 Burnley Street, Richmond' by GIW Environmental Solutions (revised November 2017) is available and should be referred to in addition to these summaries.

9.1 Introduction

GIW Environmental Solutions were engaged to provide ESD consulting in relation to the site. The report discusses the ESD benefits of rezoning and potential future redevelopment of the site as a mixed use offering, identifies ESD objectives which are to be addressed as part of the proposed rezoning and which are to be instilled into any future development on the site such that the facilities demonstrate leading design and ESD best practice.

9.2 ESD State Policy

The following State and local policies set the strategic objectives by which the proposal to change the site zoning is to be contextualized.

9.2.1 Urban Sustainable Development

"Melbourne 2030" (2002) and "The Inner Melbourne Action Plan" (2005) have the objective of making the Melbourne region more livable and encourage sustainable growth, by effectively linking sustainable transport (walking, cycling and public transport) routes, improving the environmental performance of the Melbourne region and creating a greener city.

9.2.2 Energy

"Victoria's Renewable Energy Road map" (2015) addresses the priority to encourage households and communities to generate renewable energy through the following strategies: provide support for pioneering community energy projects; ensure fair compensation for distributed generation and ensure fair treatment of distributed generation customers.

9.2.3 Water

In order to secure a strong and resilient water future, State Government is currently developing a final water plan base on the "Water for Victoria White Paper" (2016). In this strategy the need for responding to climate change and developing resilient and livable cities and towns, is addressed. The discussion paper proposes to ensure a range of water sources to support urban livability, developing place-based solutions with input from communities to deliver, safe and secure supplies, wastewater management, flood resilience, healthy urban waterways, healthy urban landscapes, integrated water management, with water corporations working in partnership with local government, engaging and empowering the community to help achieve water management outcomes together.

9.2.4 Waste

In 2015 State Government published the "Statewide Waste and Resources Recovery Infrastructure Plan". This plan will form a road map to achieve the following four goals:

- Goal 1: Landfills will only be for receiving and treating waste streams from which all materials that can be viably recovered have been extracted.
- Goal 2: Materials are made available to the resource recovery market through aggregation and consolidation of volumes to create viability in recovering valuable resources from waste.
- Goal 3: Waste and resource recovery facilities including landfills are established and managed over their lifetime to provide best economic, community, environment and public health outcomes for local communities and the state and ensure their impacts are not disproportionately felt across communities.
- Goal 4: Targeted information provides the evidence base to inform integrated statewide waste and resource recovery infrastructure planning and investment at the state, regional and local levels by industry, local government, waste and resource recovery groups, government agencies and the broader community.

9.2.5 Ecology and Biodiversity

The EPA state environmental protection policies (SEPPs) consist of policies concerning the protection of air quality, land and groundwater, noise nuisance and water.

Attachment 2 - Planning report Tract Amendment C223

9.3 ESD Local Policy

Sustainable Living and Working

City of Yarra aims to enhance community awareness and increased engagement with Yarra's local environment and nature. Moreover, Yarra seeks to support and connect neighbours to take collective action for sustainable consumption and lifestyles.

Ecology and Natural Environment

City of Yarra's Open Space Strategy (2006) seeks to stimulate open space, completing the gaps in the linear trail networks along the waterways, providing more park infrastructure i.e. seats, tables, shelters, toilets etc., improving the environmental values along the waterway corridors including the Yarra River, incorporating environmental sustainability principles into future park design, maintenance and management practices.

Sustainable Transport

City of Yarra has produced different strategic documents including "Strategic Transport Statement" (2006), "Encouraging and Increasing Walking Strategy" (2005), "Bicycle Strategy" (2010) and "Parking Management Strategy" (2013) that aim to reduce car dependency by promoting walking, cycling and public transport use as a viable and preferable alternatives.

Sustainable Business

City of Yarra is committed to support sustainable business operations in order to increase efficiency and reduce environmental impact. To that end the Yarra Energy Foundation has been created with the express charter to inspire and empower the Yarra community to reduce their carbon footprint to zero.

Sustainable Built Environment

In 2011 the Sustainable Design Assessment in the Planning Process (SDAPP) framework was introduced to ensure Councils' achieve their environmental targets in regards to sustainability in the built environment. SDAPP describes 10 key sustainability categories that need to be addressed in planning applications and seeks to go beyond the requirements as prescribed in the Building Code of Australia (BCA).

- Indoor Environment Quality
- Transport
- Energy Efficiency
- Waste Management
- Water Efficiency
- Urban Ecology
- Stormwater Management
- Innovation
- Building Materials
- Construction and Building Management

Furthermore, several strategic documents "City of Yarra Urban Design Strategy" (2011), "Open Space and Recreation Shading" (2011), "ESD Building Policy" (2014), "Asset Management Policy" (2011) have been introduced to ensure sustainable infrastructure and built form.

Waste Management

The waste strategy developed in "City of Yarra Waste and Resource Recovery Strategy 2014-2018" (2014) is guided by three objectives; generate less waste, increase the amount of materials which are recycled and reduce damage to our environment caused by waste.

Greenhouse Gas Emission

City of Yarra has set the following targets for Greenhouse Gas reduction by 2020:

- 20% reduction of Greenhouse gasses by the commercial and industrial sector.
- Carbon Neutral operations for City of Yarra.

Furthermore, greenhouse gas reduction in the built environment will be guided by SDAPP which is currently prescribing a 10% reduction of greenhouse gas for new building and extensions against the provided benchmark.

Water Consumption and Quality

City of Yarra has made a commitment to provide leadership in sustainable water management. In order to achieve this target the following overarching objectives have been formulated in the "Water Action Plan" (2006): Reduce water consumption, improve stormwater quality entering the Yarra River and local creeks, and demonstrate positive change within Council to facilitate community action in water conservation and stormwater quality improvement.

This strategy was reinforced and expanded upon in the "Water Sensitive Urban Design Guidelines" (2011) in which Council commits to the following goals: reduction of potable water use, maximising water re-use, reduction of wastewater discharge, minimisation of stormwater pollution before discharge to receiving waters, and protection of groundwater.

Climate Change Resilience and Food Systems

City of Yarra aims to stimulate Urban Agriculture through the introduction of the "Urban Architecture Strategy 2014-2018" (2014). The strategy seeks to strengthen and facilitate education and training in the area of urban agriculture, support community leaders in developing urban agriculture in their local communities, facilitate opportunities for the inclusion of urban agriculture into the community.

Attachment 2 - Planning report Tract Amendment C223

9.4 ESD Yarra Planning Scheme

The following clauses of the Yarra Planning Scheme are relevent to ESD for the proposed development:

- Cl 21.05 Built Form - focusses on protecting heritage places and a built form that maintains identified local character, encourage community interaction and activity and protect and enhance landscape qualities.
- Cl 21.06 Transport - aims to reduce car dependency by promoting walking, cycling and public transport.
- Cl 21.07 Environmental Sustainability - has the objective to promote environmentally sustainable development, protect wildlife, balance ecological and recreational values of waterway corridors and improve water quality and flow characteristics of storm water run-off.
- Cl 22.10 Built Form and Design Policy - addresses environmental sustainability as one of the building blocks that guide built form and design. The following environmental sustainable design objectives are formulated; ensure that new development is environmentally sustainable, minimise the use of energy and natural resources in the construction and operation of buildings, facilitate on-site stormwater infiltration or collection for reuse, reduce the impact of stormwater run-off on the drainage system, reduce the impact of stormwater on the water quality of the Yarra River, Darebin and Merri Creeks.
- Cl 22.16 Stormwater Management (Water Sensitive Urban Design) - prescribes that all new building, extensions (larger than 50m2) and subdivision in business zones will need to implement a water sensitive urban design strategy in order to achieve improved stormwater quality and reduce the impact of urban development on waterways and receiving water bodies.
- Cl 22.17 Environmentally Sustainable Development - prescribes that all residential and on-residential buildings will need to achieve a best practice standard in environmental sustainable development from the design stage through to construction and operation. Buildings will be benchmarked against the Built environment Sustainability Scorecard (BESS) that is based on the SDAPP framework.
- Cl 52.06 Car parking - has the objective to ensure an appropriate number of car parking spaces have been provided to the development in accordance with table 1 to Clause 52.06-5.
- Cl 52.34 Bicycle Facilities - prescribes that all new buildings are required to implement bicycle facilities in accordance with this Table 1 to Clause 52.34-3 in order to encourage cycling as a mode of transport.

9.5 ESD Proposed Measures

Sustainable Living and Working

The rezoning of the site will create interconnectedness and stimulate social cohesion by combining all proposed uses identified, on one site. Additionally, the site will include open landscaped areas available for residents, employees and the public to stimulate increased engagement with the local environment and nature. Adjacent to the open landscaped area is a communal activity area, located in the heritage building, will be provided for residents and the local community. It is proposed to have a workshop which in itself references the heritage usage of this building.

The rooftops of the proposed residential buildings will be activated for communal activities, engaging with nature and on-site renewables.

Ecology and Natural Environment

With the rezoning of the site a commitment will be made to incorporate public open space that will include seating options and extensive native vegetation. The proposed site will provide an improved connection with the Yarra River bicycle trail and waterway corridors which will stimulate local residents and visitors to use these facilities on a more frequent basis.

A commitment will be made to include sufficient permeable area on site and integrate native vegetation and vegetable gardens on rooftops and greening of building facades.

The existing tree along Doonside Street is to be retained and shall become a focal point for social gatherings.

Attachment 2 - Planning report Tract Amendment C223

9.5.1 Sustainable Transport

The site is in close proximity of public transport, bicycle trails and primary walking ways which connect the site to shops, local activities and the Melbourne CBD. From a sustainable transport perspective this location is very suitable for mixed use development.

The proposed development will promote cycling and pedestrian activity through the following:

- Residents will be stimulated to reduce their car dependency by incorporating 1 secure bicycle space per apartment and a site design that promotes the use and accessibility of the Yarra River bicycle trail and on street bicycle paths;
- Commercial tenancy parking to be provided at a ratio of 1 bicycle space per 100sqm of food and drinks tenancy and 1 bicycle space per 300sqm of office space;
- Sufficient visitor bicycle parking spaces will be provided on-site.
- Walkway connectivity through the implementation of on-site primary and secondary links throughout the site and connecting with the adjacent site to the north.

9.5.2 Sustainable Business

The proposed development will include ancillary shared office space, community facilities and a workshop / café. All of these office and commercial spaces will seek to increase their energy efficiency and reduce their environmental impact.

9.5.3 Sustainable Built Environment

The proposed development and any future development at 81-95 Burnley Street, Richmond should seek to achieve a total BESS score ≥60%, with no critical category (Management, Energy, Water, Stormwater, IEQ) achieving below 50%.

Any development will take into consideration the following built form priorities:

- The majority of apartments have been designed to take advantage of north solar gains.
- Buildings are of mid- to high-rise and demonstrate residential densities in line with Metropolitan policy.
- Consider natural cross-flow ventilation strategies to apartments.
- Consider spacing of apartment blocks to minimise overshadowing.

9.5.4 Waste Management

A commitment will be made to recycle 90% of demolition and construction waste when redeveloping the site. In a bid to minimise demolition waste, all heritage listed structures will be retained thus reducing landfill.

All future buildings will be equipped with separate on-site general, recycling, green and hard waste areas easily accessible for all occupants and tenants.

9.5.5 Greenhouse Gas Emission

The following measures will secure efficient use of fossil fuels:

- Existing commercial areas will be retrofitted to achieve a 20% energy efficiency improvement on the reference case defined in BCA Section J;
- Any development will integrate on-site renewable energy generation;
- Passive design strategies will be incorporated in any future development;
- The development will seek to install energy efficient systems.

9.5.6 Water Consumption and Quality

The following measures will secure efficient use of potable water and sufficient water sensitive urban design strategies:

- 10% reduction of total potable water use relative to the BESS benchmark;
- A water efficient fire test system will in included by installing onsite recapture of test water;
- Rainwater will be harvesting from all roof areas and reticulated to rainwater tanks on site;
- Any development will achieve ≥100% STORM score for the total site.

9.5.7 Climate Change Resilience and Food Systems

- The rooftops of the residential buildings will be used in part for food cultivation and composting of green waste.
- On-site rainwater harvesting and reuse.
- Passive solar design will seek to address temperature rise by providing a high efficiency thermal envelope.

Attachment 2 - Planning report Tract Amendment C223

010 CONCLUSION

This report has detailed a request on behalf of Astrodome Hire Pty Ltd to rezone the land known as 81-95 Burnley Street and 26 -34 Doonside Street, Richmond from the Industrial 3 Zone (IN3Z) to a Mixed Use Zone (MUZ).

In conjunction with the rezoning, it is intended to apply a Development Plan Overlay and Environmental Audit Overlay to the land.

This Planning Scheme Amendment will provide net community benefits through:

- The retention of a longstanding major local business, and a significant net increase of jobs across the site.
- The creation of new public open spaces, pedestrian laneways and other local amenity improvements.
- The provision of 5% of dwellings to be for affordable housing.
- The restoration of heritage buildings including the entire building at 26 Doonside Street.
- Contribution to the future signalisation of Doonside Street and Burnley Street intersection.
- The provision of additional residential, commercial and retail land uses within an activity centre and walking distance of its catchment.
- Implementation of the policies of the Yarra Planning Scheme and Plan Melbourne.

The proposed rezoning is consistent with the planning objectives in Victoria, and in particular, Plan Melbourne. The proposed rezoning to Mixed Use Zone (MUZ) is explicitly supported by a number of references in the Yarra Planning Scheme. It furthers the objectives of the Yarra Planning Scheme in relation to housing, heritage, employment, public open space, and urban design.

We respectfully request that this rezoning request to amend the Yarra Planning Scheme be forwarded to the Minister for Planning pursuant to section 9(2) of the Planning and Environment act 1987 for authorisation to formally prepare the amendment.

Attachment 2 - Planning report Tract Amendment C223

Attachment 3 - Heritage Report Bryce Raworth Amendment C223

Former Repco Factory
85-91 Burnley Street & 26-34 Doonside Street, Richmond

Proposed rezoning of land - Analysis of heritage issues

Report to Council

Revised December 2017

Bryce Raworth Pty Ltd Conservation Urban Design

Attachment 3 - Heritage Report Bryce Raworth Amendment C223

Former Repco Factory
81-95 Burnley Street & 26-34 Doonside Street
Richmond

Proposed rezoning of land
- Analysis of heritage issues

Revised December 2017

1.0 Introduction

This report was commissioned by Charter Keck Cramer on behalf of the owners of the former Repco Factory at 81-95 Burnley Street and 26-34 Doonside Street, Richmond. It has been prepared in relation to a request for a planning scheme amendment to introduce a Development Plan Overlay and allow for the rezoning of the land from an Industrial Zone 3 to a Mixed Use Zone. The purpose of the rezoning is to facilitate the site's redevelopment. The report comments on the potential impacts that the proposed development concept may have upon the character and significance of the heritage buildings on the subject site and heritage places within the site's immediate environs.

This report is a revised and updated version of an earlier heritage report to Council prepared by this office. It has been revised to address changes to the proposed *Indicative Framework Plan* for the site and associated Development Plan Overlay (27 November 2017).

Subsequent to a rezoning of the subject site, a planning application will be required for redevelopment proposals involving the heritage overlay buildings, at which time a further heritage assessment will be provided.

2.0 Sources of Information

The analysis below draws upon external inspections of the subject site and its environs, along with a review of the Yarra Heritage Overlay at Clause 43.01, local heritage policy set out under Clause 22.02, and the memorandum of advice prepared by GJM Heritage in relation to the subject site (8 March 2017). The relevant heritage studies have also been reviewed, including the [City of Yarra] *Heritage Gap Study: Review of 17 Heritage Precincts* (Context Pty Ltd, 2014), *City of Yarra Heritage Gap Study* (Graeme Butler & Assoc., 2007) and the *City of Yarra Heritage Review* (Allom Lovell & Assoc., 1998). Reference has also been made to Panel reports relating to Amendments C173 (Part 2) and C214 to the *Yarra Planning Scheme*.

3.0 Current Listings and Controls

The land at 81-95 Burnley Street is partially covered by a site-specific heritage overlay HO375 – '*Russell Manufacturing Pty Ltd later Repco*'. The heritage overlay encompasses the pre-1945 office building and factory to a depth of 34 metres from the Burnley Street frontage. The former Repco Offices at 26 Doonside Street is separately listed on the heritage overlay schedule as HO252. External paint controls apply to both HO252 and HO375 but there are no internal alteration controls or tree controls. The balance of the subject site has no heritage overlay control.

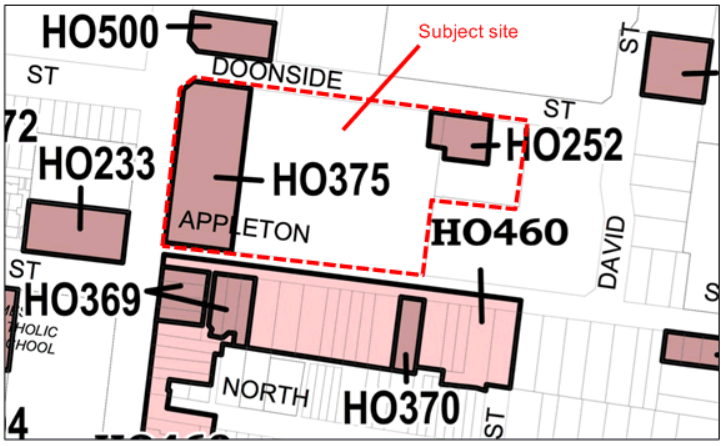


Figure 1 *Heritage overlay map showing heritage overlays applying to 26 Doonside Street (HO252) and the front of the property at 81-95 Burnley Street (HO375).*

The heritage overlay control at 81-95 Burnley Street was implemented in 2015 under Amendment C149 the *Yarra Planning Scheme*. Council had proposed to incorporate this site into a broader heritage overlay precinct as part of Amendment C173 (Part 2). The proposed Doonside Industrial Precinct (HO455) also encompassed the sites at 1-9 Doonside Street and 61-63, 65, 67 and 77-79 Burnley Street – all of which were subsequently excluded from consideration by the Panel. The Panel was of the view that the remaining properties did not constitute a precinct. To this end, the Panel ultimately recommended the amendment be abandoned in so far as it affected 81-95 Burnley Street.

Heritage overlay controls were again proposed for the Doonside Industrial Precinct under Amendment C214, with the precinct this time reduced to the buildings at 77-79 and 81-95 Burnley Street and 1-9 Doonside Street. Council ultimately resolved to abandon this amendment on the recommendations of the C214 Panel. The interim heritage overlay control applying to 77-79 Burnley Street (HO500) expired on 30 November of this year.

South of the subject site, residential properties at 2-38 Appleton Street form part of the recently introduced Yarraberg Precinct (HO460). The houses at 2-6 Appleton Street and adjoining properties at 97-103 Burnley Street are separately listed as HO369. An individual heritage overlay control applies to 24 Appleton Street (HO370).

4.0 History

At the end of the nineteenth century, a number of tanneries were dispersed along the banks of the Yarra River on the east side of Burnley Street. Industrial development in this area continued into the early twentieth century with the establishment of the Vickers Ruwolt engineering works on Victoria Street around 1915.

Concerns about the impact of noxious industries eventually resulted in zoning by-laws that divided Richmond into residential and factory areas. In 1929, the east side of Burnley Street in the vicinity of Appleton Street was recommended as a factory area. This area took in land originally occupied by Doonside, the residence of David Mitchell (Dame Nellie Melba's father). The Doonside Estate was subdivided and offered for sale in May 1931, providing 34 lots on Burnley, Doonside, David and Appleton Streets. Predating the Doonside Estate subdivision, a fibrous plaster factory was built c1925 on the north-east corner of Burnley Street and Appleton Street (first listed as in Sands and McDougall Directory as 89-95 Burnley Street).

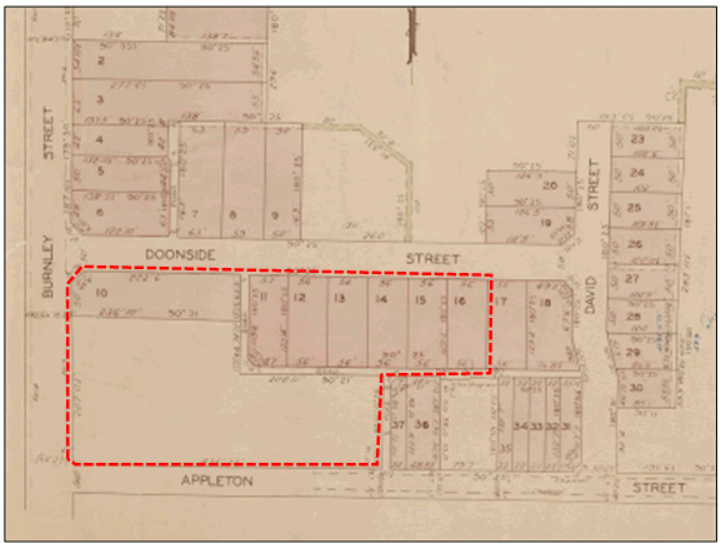


Figure 2 *Part of the 1930 plan of subdivision for the Doonside Estate. Source: Yarra Ranges Regional Museum. The dashed lined marks the extent of the subject site.*

In 1930, the plaster factory listed at 89-95 Burnley Street was taken over by Russell Manufacturing Co. Pty Ltd, a firm allied with Repco. Established in 1926 as the Replacement Parts Co. (later abbreviated to become 'Repco') the company distributed automotive spare parts and accessories, stocking its own products and those of other firms.

Russell Manufacturing are first listed at 89-95 Burnley Street in the 1931 Sands and McDougall directory. East of the factory, Appleton Street remained a residential streetscape.[1]

Repco reportedly selected the Burnley Street site because it was bound on three sides by roads and could accommodate a reasonably straight and uninterrupted production sequence.[2] The foundry buildings were open sided according to a 1933 description:

> *…climatic conditions are such that no glazing is necessary for the shops, nor is there need for heating, whilst obviously, ventilation is automatic. The main foundry shop, which is 100 feet by 200 ft, is divided into two main bays, wherein the two main products, pistons and rings are self contained units…*[3]

Around 1939, Repco/Russell Manufacturing constructed a double-storey office and laboratory building at present day 26 Doonside Street.[4] In March 1942, the company purchased additional land and expanded northwards along Burnley Street towards the Doonside Street corner.[5] A new building erected on this site in the same year was described in a contemporary advertisement:

> *'Repco is closely associated with Australia's war effort on the industrial front and is engaged in producing its quota of munitions and war equipment as well as a steady flow of automotive parts for essential services. To meet the extra strain placed on the manufacturing division, a new plant and extension of its foundry were completed…*[6]

During the Second World War, Russell Manufacturing supplied the United States Army with pistons and piston rings.[7] To meet wartime demand Repco acquired additional foundry floorspace in 1943, measuring 40 ft by 132 ft (12.2m x 40.2m).[8] Also in 1943, residences at 5 and 7 Appleton Street disappear from Sands and McDougall directories. These sites were presumably absorbed into the expanding Russell factory.

1 Sands and McDougall directory, 1931.
2 'A Modern Australian Foundry', in *Foundry Trade Journal*, September 7, 1933, p.129. Repco Ltd company records, University of Melbourne.
3 Ibid.
4 Allom Lovell & Assoc., *City of Yarra Heritage Review: Building Citations*, p.122.
5 Repco Company records, University of Melbourne Archives.
6 Sands and McDougall Directory, 1943, p. a58.
7 Repco Company records, University of Melbourne Archives.
8 Repco Company records, University of Melbourne Archives.

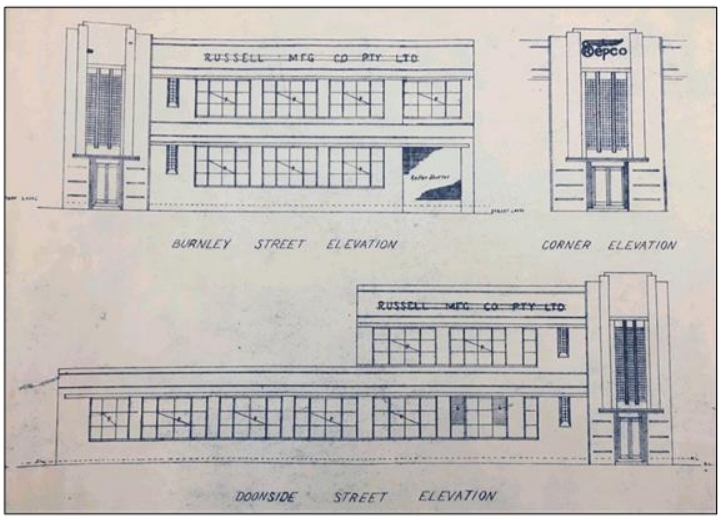


Figure 3 *Undated (c1942) architectural drawings for the Repco building on the corner of Burnley and Doonside Streets. Source: Public Record Office Victoria.*

Figure 4 *A 1945 aerial photograph showing the Repco factory with large scale industrial development to its north. (the subject site is indicated by a dashed line). Source: University of Melbourne library.*

A 1945 aerial photograph shows factory buildings encompassing almost all of the subject site apart from the eastern end of the Appleton Street boundary where a small number residences remained (Figure 4).

In the post war years Repco steadily increased its business, thriving on its manufacture of parts for the Holden motor car. By 1958, the Russell factory had expanded further east along Appleton Street to take over sites at no.13 and 15. From 1962, Sands and McDougall directories list a Russell Manufacturing storage facility at 21-27 Appleton Street.[9]

Figure 5 *A 1957 photograph Repco building on the corner of Burnley Street and Doonside Street. Source: 'Repco Record' (University of Melbourne Archives).*

Repco moved into high-performance engine construction in the 1960s in association with Formula 1 driver Jack Brabham. They developed a new V8 engine for Brabham, which had its first successful testing at the Burnley Street factory in March 1965.[10] The following year, Brabham won the French, British, Dutch and German Grands Prix and World Formula One Driver's Championship. Around the same time the production of the Repco-Brabham engine was relocated from Richmond to another Repco facility at Maidstone.[11]

[9] Sands and McDougall Directory, 1962.
[10] www.motorsportsalmanac.com/mastuff/articles/RN_020310.pdf
[11] http://repco.com.au/CA2571B70016E7AE/page/About+Us?OpenDocument&1=06-About+Us~&2=~&3=~

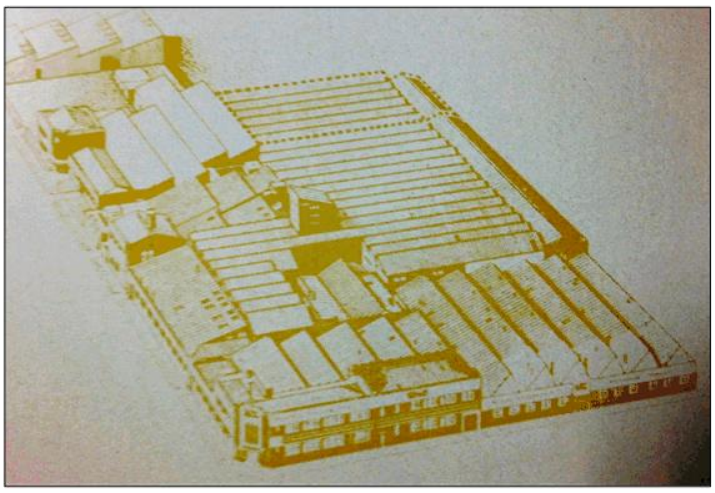


Figure 6 *A c1962 illustration of the Repco/Russell factory. Source: University of Melbourne Archives.*

Figure 7 *Recent oblique aerial of the site.*

5.0 Description

The subject site occupies the greater part of the block bound by Burnley Street to the west, Doonside Street to the north, David Street to the east and Appleton Street to the south. The factory complex on the site was constructed in stages from c1930 through to the late twentieth century. It is broadly intact to its mature post war state, although substantial demolition has occurred at the eastern end of the site.

The north-west corner of the site is occupied by a double-storey Moderne style

building erected for Repco in 1942. It is chamfered at the corner and has no setback on both street frontages. External walls are constructed of face brick with rendered dressings and a rendered parapet concealing the roofline. There are regularly spaced multi-pane steel-framed windows on both sides, linked together by continuous projecting concrete hoods. The corner entry is surmounted by vertical glass brick windows with a rendered surround. The building exterior remains broadly intact to its 1942 state but alterations have been made which detract from its overall appearance. These alterations include:

- An upper storey addition on the Doonside Street façade with a series of narrow fixed sash windows (in contrast to the wide multi-pane steel-framed windows on the original).
- Removal of original signage from the Doonside Street and Burnley Street parapets. Early photographs show metal ribbon type signage in these locations (refer figure 5 above). It is not clear if the 'Repco' sign on the corner parapet was also removed, or if it survives behind the present 'Harry the Hirer' sign.
- Two first floor window frames on the Doonside Street façade have been removed and the openings divided into smaller windows with modern fixed sash glazing.
- Four of the ground floor windows on the Doonside Street façade have replaced by modern glass brick.
- All external wall surfaces have been overpainted a uniform blue colour (obscuring the original design of face brick with contrasting bands of render).
- The corner flagpole has been removed.

The Appleton Street corner is occupied by a single-storey building with a south facing sawtooth roof. This part of the site was occupied by a plaster factory from c1925 but the appearance of the extant building is more suggestive of a late 1930s construction date. The building has brick walls to the street boundaries with regularly spaced steel framed windows under a rendered parapet. The parapet has simple Moderne style detailing in the form of horizontal incisions, found at either end and at the corner. The stepped pediment on the Burnley Street frontage is also typical of the Moderne style. The pediment seems to have originally marked the location of the principal entry but this has been infilled with brick and windows. There is also a plainly designed first floor addition on the Burnley Street frontage (abutting the southern end of the 1942 double storey building).

The middle portions of the site are occupied by utilitarian factory buildings of one to two storeys, typically with sawtooth roofs and overpainted brick walls. A modern tilt-up concrete building with a roof top carpark deck is located at the eastern end of the Appleton Street boundary.

Figure 8 *The 1942 former Repco building on the corner of Burnley and Doonside Streets.*

Figure 9 *The c1930s factory on the corner of Burnley and Appleton Streets.*

Figure 10 *The subject site looking west along Doonside Street.*

Figure 11 *The subject site looking west along Appleton Street. The building in the foreground is a modern concrete structure with a roof top carpark.*

A double-storey interwar Moderne style office building stands at the eastern end of the Doonside Street boundary. The office building has a parapeted façade with manganese brick walls and contrasting panels of cream brick. The curved corner has three vertical fins and a cantilevered concrete canopy above the entry. Port hole windows on the façade are typical of the Moderne style. The outline of a demolished gable roofed structure is visible on the west elevation. A sawtooth roofed factory visible to the rear of the office building in the 1945 aerial photograph has also been demolished.

Figure 12 *The c1939 interwar Moderne style office building at 26 Doonside Street.*

In terms of its context, the subject site is located in an industrial area of Richmond that was historically characterised by sawtooth roofed factories of one to two storeys. The area is currently experiencing substantial change with many of the redundant factory sites earmarked for redevelopment. A large scale shopping centre development occurred in 2003 replacing the Vickers Ruwolt engineering works on the corner Burnley Street and Victoria Street. The shopping centre backs onto Doonside Street and is clearly visible from the subject site. The northern end of Burnley Street, and adjoining parts of Victoria Street, has also experienced intensive apartment development in recent years.

Closer to the subject site, a four storey apartment building can be found on the opposite side of Burnley Street (ie 86 Burnley Street). Immediately east of the subject site, construction of an apartment complex of 8, 10 and 12 storeys has commenced on the large parcel of land at 36-44 Doonside Street and 27-41 Appleton Street. Also within the immediate environs of the subject site, an 11 storey apartment development is under construction at 9-11 David Street, retaining only the façade of the existing interwar factory (known as the former Builders' Steel Form Supply Co., HO250).

The subject site has an interface with a finer grain residential streetscape on Appleton Street. Notwithstanding that this streetscape partially included in a Heritage Overlay precinct, Appleton Street does not present as a homogenous or highly intact heritage environment. The south side of the street contains a relatively diverse mix of late-Victorian, Edwardian and interwar residences in varying states of intactness, along with double storey infill at 14/14A Appleton Street and some architecturally non-descript single-storey post war workshops further to the east.

Figure 13 *View south along Burnley Street showing a modern apartment building opposite the subject site.*

Figure 14 *The north side of Doonside Street looking west. The rear of the Victoria Gardens Shopping centre is visible behind the carpark in the foreground.*

Figure 15 *36-44 Doonside Street, abutting the east side of the subject site. A large multi-storey apartment complex is under construction on this site.*

Figure 16 *Multi-storey development under construction at 9-11 David Street. The development retains the façade of the former Builders' Steel Form Supply Co (HO250) (far left).*

Figure 17 *Residential streetscape on the south side of Appleton Street forming part of the Yarraberg Precinct (HO460).*

6.0 Significance

The Yarra Heritage Database includes the following statement of significance for the former Russell Manufacturing/Repco Factory at 81-95 Burnley Street:

> *What is significant?*
> *The Russell Manufacturing Company Pty Ltd (later Repco) building at 81-95 Burnley Street, Richmond is significant to the extent of the pre-1945 fabric. Built in stages for the Russell Manufacturing Company Pty Ltd, the brick (overpainted) building has a strong Moderne styling, with horizontal banding on the main elevations.*
>
> *Post-1945 alterations and additions to the building are not significant.*
>
> *How is it significant?*
> *The Russell Manufacturing Company Pty Ltd (later Repco) building is aesthetically and historically significant to the locality of Richmond and the City of Yarra.*
>
> *Why is it significant?*
> *The Russell Manufacturing Company Pty Ltd (later Repco) building is aesthetically significant (Criterion E):*
> - *for its strong Moderne styling as ideally presented on a corner site.*
> - *for the relationship with the significant Moderne style former Repco Building at 26 Doonside Street (HO256).*
>
> *The Russell Manufacturing Company Pty Ltd (later Repco) building is historically significant (Criteria A & H):*
> - *as tangible evidence of the large factories built during the interwar period when Richmond became a centre of*
> - *manufacturing in Victoria.*
> - *for the association with the successful motor spare parts firm of Russell Manufacturing Company Pty Ltd.*

A separate statement of significance exists for the former Russell Manufacturing/Repco building at 26 Doonside Street:

What is significant?
The building, built c.1939 as an office and laboratories for the Russell Manufacturing Co., which later became Repco at 26 Doonside Street, Richmond is significant. It is a two storey Moderne style bichromatic brick building. It is approximately square in plan, with a curved corner at the northwest. The building is oriented north-west, and the composition of the main panels of brickwork is approximately symmetrical about a diagonal axis which runs through the corner entrance, which has a cantilevered concrete canopy. The north and west elevations are of face manganese brick, whilst large panels of cream brick give the appearance of wrapping around this, leaving a vertical strip of dark brown brick above the entrance. This corner element is decorated with a narrower vertical strip of horizontally-striped tapestry brickwork, and surmounted by three white painted vertical concrete fins. The north elevation features two bands of windows, each comprising three panels of multi-paned steel-framed windows with manganese brick spandrels and sills.

These windows turn the corner to the east elevation; to their right are two vertically placed circular windows, probably to a staircase. The west
wall of the building was once attached to a single-storey building which has since been demolished, with the exception of part of the front wall and cream brick parapet which adjoins No. 26.

How is it significant?
The former Repco office and laboratory building at 26 Doonside Street, Richmond, is of local architectural significance of the City of Yarra.

Why is it significant?
It is architecturally and aesthetically significant as a particularly sophisticated example of a small building in the Moderne style, which exhibits an interesting composition of a limited palette of materials. It is thus distinctive for a building of its size and type. The demolition of other adjacent buildings has increased the aesthetic contribution of this building to an otherwise architecturally undistinguished industrial streetscape. (Criteria D & E)

7.0 Heritage Overlay

As noted above, heritage overlay controls apply to two discrete areas of the subject site. These parts of the site are therefore subject to the provisions of Clause 43.01 of the *Yarra Planning Scheme*, the Heritage Overlay. The schedule specifies that there are external paint controls but no internal alteration controls or tree controls under this overlay.

The purpose of this overlay is as follows:

- *To implement the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.*
- *To conserve and enhance heritage places of natural or cultural significance.*
- *To conserve and enhance those elements which contribute to the significance of heritage places.*

- *To ensure that development does not adversely affect the significance of heritage places.*
- *To conserve specifically identified heritage places by allowing a use that would*

otherwise be prohibited if this will demonstrably assist with the conservation of the significance of the heritage place.

Before deciding on an application, in addition to the decision guidelines in Clause 65, the responsible authority will need to consider, as appropriate:

- *The State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.*
- *The significance of the heritage place and whether the proposal will adversely affect the natural or cultural significance of the place.*
- *Any applicable statement of significance, heritage study and any applicable conservation policy.*
- *Whether the location, bulk, form or appearance of the proposed building will adversely affect the significance of the heritage place.*
- *Whether the location, bulk, form and appearance of the proposed building is in keeping with the character and appearance of adjacent buildings and the heritage place.*
- *Whether the demolition, removal or external alteration will adversely affect the significance of the heritage place.*
- *Whether the proposed works will adversely affect the significance, character or appearance of the heritage place.*
- *Whether the proposed subdivision will adversely affect the significance of the heritage place.*
- *Whether the proposed subdivision may result in development which will adversely affect the significance, character or appearance of the heritage place.*
- *Whether the proposed sign will adversely affect the significance, character or appearance of the heritage place.*
- *Whether the lopping or development will adversely affect the health, appearance or significance of the tree.*

Proposals for redevelopment of heritage overlay sites must also be assessed in terms of Council's Heritage Policy, Clause 22.02, the relevant sections of which are reproduced below:

Removal of Part of a Heritage Place or Contributory Elements
Encourage the removal of inappropriate alterations, additions and works that detract from the cultural significance of the place.

Generally discourage the demolition of part of an individually significant or contributory building or removal of contributory elements unless:

- *That part of the heritage place has been changed beyond recognition of its original or subsequent contributory character(s).*
- *For a contributory building:*
  - *that part is not visible from the street frontage (other than a laneway), abutting park or public open space, and the main building form including roof form is maintained; or*
  - *the removal of the part would not adversely affect the contribution of the building to the heritage place.*
- *For individually significant building or works, it can be demonstrated that the removal of part of the building or works does not negatively affect the significance of the place.*

[…]

Attachment 3 - Heritage Report Bryce Raworth Amendment C223

22.02-5.7 New Development, Alterations or Additions

22.02-5.7.1 General

Encourage the design of new development and alterations and additions to a heritage place or a contributory element to a heritage place to:

- *Respect the pattern, rhythm, orientation to the street, spatial characteristics, fenestration, roof form, materials and heritage character of the surrounding historic streetscape.*
- *Be articulated and massed to correspond with the prevailing building form of the heritage place or contributory elements to the heritage place.*
- *Be visually recessive and not dominate the heritage place.*
- *Be distinguishable from the original historic fabric.*
- *Not remove, cover, damage or change original historic fabric.*
- *Not obscure views of principle façades.*
- *Consider the architectural integrity and context of the heritage place or contributory element.*

Encourage setbacks from the principal street frontage to be similar to those of adjoining contributory buildings; where there are differing adjoining setbacks, the greater setback will apply.

Encourage similar façade heights to the adjoining contributory elements in the street. Where there are differing façade heights, the design should adopt the lesser height.

Minimise the visibility of new additions by:

- *Locating ground level additions and any higher elements towards the rear of the site.*
- *Encouraging ground level additions to contributory buildings to be sited within the 'envelope' created by projected sight lines (see Figure 1)*
- *Encouraging upper level additions to heritage places to be sited within the 'envelope' created by projected sight lines (for Contributory buildings refer to Figure 2 and for Individually significant buildings refer to Figure 3).*
- *Encouraging additions to individually significant places to, as far as possible, be concealed by existing heritage fabric when viewed from the front street and to read as secondary elements when viewed from any other adjoining street.*

Discourage elements which detract from the heritage fabric or are not contemporary with the era of the building such as unroofed or open upper level decks or balconies, reflective glass, glass balustrades and pedestrian entrance canopies.
[…]

22.02-5.7.2 Specific Requirements (where there is a conflict or inconsistency between the general and specific requirements, the specific requirements prevail)

Corner Sites and Sites with Dual Frontages

Encourage new building and additions on a site with frontages to two streets, being either a corner site or a site with dual street frontages, to respect the built form and character of the heritage place and adjoining or adjacent contributory elements to the heritage place.

Encourage new buildings on corner sites to reflect the setbacks of buildings that occupy other corners of the intersection.

Industrial, Commercial and Retail Heritage Place or Contributory Elements

Encourage new upper level additions and works to:

- *Respect the scale and form of the existing heritage place or contributory elements to the heritage place by being set back from the lower built form elements. Each higher element*

should be set further back from lower heritage built forms.
- *Incorporate treatments which make them less apparent.*

[…]

8.0 Analysis

As noted, the owners of the subject site are requesting a planning scheme amendment to rezone the land (from Industrial Zone 3 to a Mixed Use Zone) and introduce a Development Plan Overlay (DPO). The purpose of the rezoning and DPO is to facilitate the site's redevelopment. The development concept is schematic only at this stage and is to be adapted as required to satisfy Council heritage policy.

The proposed DPO has been revised in accordance with the GJM recommendations to include the following conditions and requirements for permits relevant to heritage considerations:

A permit application must include, where relevant:
- *A heritage impact statement prepared by a suitably qualified professional that assesses the impact of the proposed development on the heritage values of the heritage place and nearby heritage places, as identified in the conservation management plan or similar comprehensive heritage analysis prepared for the site, along with relevant heritage studies and citations.*
- *A sightline analysis and 3D modeling of the proposed development from key view points in the public realm to enable an assessment of the visual impact of the development on heritage places*

As was also recommended by GJM, heritage related provisions of the DPO have been strengthened. Specifically, the Development Plan Vision at clause 3.1 of the DPO has the following requirements:

- *To respect the scale and form heritage places within and adjacent to the site and provide for the conservation of heritage places within the site;*

  […]

- *To provide for the sensitive adaptive re-use of heritage buildings in accordance with the Indicative Framework Plan and informed by a comprehensive heritage analysis prepared for the site by a suitably qualified professional that:*
  - *articulates the significance of the heritage place, its component parts and its setting;*
  - *describes the relationship between the heritage place and any neighbouring or adjacent heritage place/s; and*
  - *establishes principles for managing the significance of the heritage place and its relationship with its surroundings.*

Further to heritage matters the revised DPO include the following built form guidelines, inter alia:

- *Building setbacks from street boundaries that ensure that new future development does not overwhelm the scale of the heritage buildings on the site or on heritage places in the vicinity of the site, including dwelllings on the south side of Appleton Street;*
- *Building setbacks from the facades of 81-95 Burnley Street that ensure the heritage*

building can be understood as having a three dimensional form;

- *Buildings setback a minimum of 10 metres (above podium) from the heritage building at 26-34 Dooside Street;*
- *Inter-floor heights within the heritage buildings on the site to ensure they relate to the existing floor levels and/or fenestration patterns;*
- *Massing diagrams that model the proposed built form envelopes based on the indicative heights and setbacks;*
- *Indicative palette of building materials and architectural treatments throughout the site. The design and use of materials must be respectful of the industrial heritage of the site and its surrounds to the north and east, as well as to the residential heritage to the south.*

The revised *Indicative Framework Plan* for the site envisages a mix of residential, commercial and retail development with a public open space component and provision for a community/commercial facilities. A pedestrian laneway 'spine' will run north-south through the centre of the site. The heritage building at 26 Doonside Street would be retained as a freestanding entity with public open space to the west side exposing the principal façade to public view and more generally helping to maintain the legibility of the original building envelope. The front parts of the heritage building to Burnley Street would also be retained along with its side elevations to Doonside and Appleton Streets (to an extent corresponding with the HO boundary).

New built form adopts a low-rise podium expression to the street edge with a series of multi storey elements rising above. The podium is typically 3 storeys (8-11 metres) across most of the site but steps down to 6 metres behind the single-storey heritage facade at the corner of Burnley and Appleton Streets.

The preferred maximum for the two tower elements along the Doonside Street frontage (west of the pedestrian spine) is 12 storeys (42 metres). The third tower adjacent to the heritage building at 26 Doonside Street has been reduced in height from 13 to 11 storeys (38.5 metres). The preferred maximum height of the tower elements on the Appleton Street frontage (west of the pedestrian spine) is 7 storeys. The tower to the east side of the pedestrian spine has a preferred maximum height of 5 storeys with an 8 storey element at a greater setback.

The tower elements have a 10 metre setback (above the podium) from the Burnley Street heritage façade, an 8 metre setback on the Doonside Street frontage and a 13 metre setback on the Appleton Street frontage (which is generally commensurate with the 15 metre setback recommended by GJM Heritage). The tower at the north-east corner of the site will be setback 10 metres from side and rear of the heritage building at 26 Doonside Street.

The former factory buildings on the subject site can, from a heritage perspective, sustain development of the scale proposed in the revised *Indicative Framework Plan* without adverse impacts. The tower heights contemplated in the plan are not greatly in excess of those set out in earlier versions of the document (generally no more than an additional one or two storeys). Issues in respect to the potential to achieve greater heights outside of the Heritage Overlay are mainly influenced by

urban design requirements and other non-heritage planning policies. Within the Heritage Overlay, the relationship of new and existing built form at the street level is the key issue and this will be handled through the low-rise podium elements equivalent to the height and setbacks of the heritage façades.

GJM had recommended lower heights for the tower adjacent to the heritage building at 26 Doonside Street of 5-8 storeys. GJM were of the view that development of around 10-13 storeys would overwhelm the heritage building. Impacts on the heritage building arising from the development at the preferred height of 11 storeys will be mitigated by 10 metre (above the podium) setbacks from the heritage fabric, coupled with the use of a low rise streetwall element to establish a sympathetic scale relationship. Moreover, the significance of 26 Doonside Street is not dependant on it retaining a low to medium rise context. The building is identified as being of architectural/aesthetic significance as sophisticated example of the Moderne style – it can still be understood as such regardless of the height of the development on adjacent parts of the subject site.

In respect to the GJM's recommendations that setbacks should be sufficient to retain part of the Burnley Street building's industrial saw-tooth roofs as visible from the public realm, the roofs in question are neither readily visible from the public realm nor a major element in the architectural composition of the place. The roofs are not of any special significance as sawtooth roofs per se, this being a relatively common roof type. Furthermore, the statement of significance for 81-95 Burnley Street makes no reference to existing roof forms. The building is considered significant for its strong Moderne style, its relationship to the other Repco building at 26 Doonside Street, as tangible evidence of large factories built during the interwar period, and for the association with Russell Manufacturing (a company affiliated with Repco). These aspects of significance will not be unduly compromised by the removal of the sawtooth roofs. The significance of the building primarily resides in the street facades and to that extent retention of the facades as proposed is deemed appropriate.

It is acknowledged that the proposed DPO could lead to outcomes where the facade only is retained, but this should not be judged inappropriate on the basis that facadism is inappropriate per se. While it has long been the aim of heritage planning to retain historic buildings to a certain depth, there are numerous approved and constructed precedent developments in heritage overlay sites in the City of Yarra and the inner metropolitan area more generally that retain only street elevations. There have also been a number of VCAT determinations that comment on such matters and discourage criticism reliant upon a pejorative use of the term 'facadism'.

In respect to the scale and setbacks of new built form behind the heritage facades, the proposal is comparable with a number of recently completed or approved developments in Yarra involving industrial heritage buildings. Notably, a precedent for this form of development already occurs within the immediate environs of the subject site. The scheme presently under construction at 9-11 David Street involves an 11 storey apartment complex at a very modest setback from the retained façade of the former Builders' Steel Form Supply Co. (HO250)

(Figure 18). Many other developments of recent years demonstrate the capacity of low-rise industrial heritage buildings to accommodate higher built form in accordance with Council's heritage policy.

Figure 18 *Illustration of the approved development at 9-11 David Street.*

A crucial point to made in relation to heritage issues is that the majority of the site is outside of the heritage overlay, and is therefore not subject to heritage provisions of the Yarra Planning Scheme. In these circumstances the key design parameters for new development would be urban design requirements and other local planning policies rather than the heritage overlay.

That said, the City of Yarra has a planning policy whereby new development is required to respond to the scale of adjoining heritage overlay places. The Built Form and Design Policy at Clause 22.10 states that the height of new development abutting land in a heritage overlay should:

- *Adopt a façade height to the street frontage which is no higher than the adjacent building within the Heritage Overlay;*
- *Design and site taller structures so that they do not visually dominate surrounding heritage places; and*
- *Match the floor levels of the adjacent heritage building.*

The development concept for the subject site responds to the policy at Clause 22.10 by having podium elements at the street edge commensurate with the height of the Burnley Street heritage building. A low rise podium also flanks open space around the Doonside Street heritage building with taller elements at a greater setback. The issue of whether new floors levels need to match those of the heritage buildings is addressed in the built form guidelines within the proposed DPO as follows:

- *Inter-floor heights within the heritage buildings on the site to ensure they relate to the*

existing floor levels and/or fenestration patterns …

In respect to the visual prominence of taller structures, it is a key consideration in terms of heritage impacts that the subject site is not highly sensitive to change. The built form character of this area is mixed with an emphasis on multi-level apartment buildings starting to take precedence. Notably, the construction of a large scale apartment complex with towers of 8, 10 and 12 storeys has commenced on land directly abutting the east side of the subject site (ie 36-44 Doonside Street and 27-41 Appleton Street) (Figure 19). It may reasonably be anticipated that the area will experience more intensive development of the scale and character proposed for the subject site in future.

Figure 19 *Illustration of the approved development at 36-44 Doonside Street.*

Looking at impacts on neighbouring sites, the interwar factories surviving opposite the subject site on the north side of Doonside Street exhibit a low degree of integrity and/or low architectural and historical interest, and collectively do not posses a strong heritage character. This is borne out by Council's decision in July of this year to abandon heritage overlay controls for these sites. It is noted also that the interim heritage overlay control applying to the interwar factory at 77-79 Burnley Street expired on 30 November of this year. As such, the redevelopment concept for the subject site gives rise to no heritage concerns on the Doonside Street interface.

That heritage overlay controls have recently been applied to residential properties on the south side of Appleton Street should not prevent large scale development from occurring on the subject site. The proposed development would not adversely affect the integrity, significance and character of the Yarraberg Precinct even though it would alter the broader context of the precinct. Moreover, the context of Appleton Street is set to change dramatically with the redevelopment of

36-44 Doonside Street and 27-41 Appleton Street. The multi-storey buildings under construction on that site stand opposite single-storey dwellings within the Yarraberg Precinct.

Furthermore, this is an area of Richmond where marked contrasts in scale and built form character can already be experienced. Multi-storey development can be found in close proximity to streetscapes that are predominately residential and single-storey. This situation occurs at the northern end of Burnley Street where recent multi-storey apartment developments interface with single-storey dwellings within the Cole's Paddock heritage overlay precinct (HO363) (Figure 20).

Figure 20 *The dwelling at 1 Davison Street, Richmond with multi-storey development to the rear. The dwelling is identified as an individually significant place in the Cole's Paddock heritage overlay precinct (HO363).*

Finally, while the rezoning of the subject site creates a policy framework within which multi-storey development can be contemplated, it does not nullify or replace the requirement for permits under clause 43.01, at least in relation to parts of the site within the heritage overlay.

9.0 Conclusion

The proposed re-zoning and future redevelopment of the site as contemplated in the associated documentation will inevitably result in a substantial degree of change. However, this change is consistent with the emerging character of the area. The scale and location of multi-storey development in relation to the retained heritage buildings on the subject site also follows precedent established in

other approved and existing developments the City of Yarra involving industrial heritage places.

Having regard for this, the proposal to rezone the subject land in order to accommodate multi-storey mixed-use development is acceptable and can be tailored in response to Council heritage policy so as not to result in adverse impacts with regard to the heritage buildings within the subject site and the other heritage places in the vicinity of the site.

Attachment 4 - Traffic Report Ratio Amendment C223

ratio:

Prepared for
Astrodome Hire Pty Ltd

Our reference: 13373rep01d04

5 December 2017

Traffic:impact

Proposed Mixed Use Rezoning

81-95 Burnley Street and 26-34 Doonside Street, Richmond

r:

Attachment 4 - Traffic Report Ratio Amendment C223

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Attachment 4 - Traffic Report Ratio Amendment C223

Table of contents:

Chapter / Section		Page No.
1	**Introduction:**	**4**
2	**Existing Conditions:**	**5**
2.1	Location and Environment	5
2.2	Existing Operation	6
2.3	Victoria Street East Precinct, Richmond Urban Design Framework	7
2.4	Nearby Development	8
2.5	Road Network	9
2.6	Traffic Conditions	11
2.7	Parking Conditions	18
2.8	Sustainable Transport	20
3	**The Proposal:**	**23**
3.1	General	23
3.2	Yield Analysis	23
3.3	Proposed Internal Streets	25
3.4	Vehicle Access	25
3.5	Pedestrian Access	25
4	**Parking Assessment:**	**27**
4.1	Clause 52.06 Parking Requirements	27
4.2	Recommended Parking Rates	28
5	**Bicycle Parking:**	**31**
6	**Loading Facilities:**	**32**
7	**Traffic Assessment:**	**33**
7.1	Traffic Generation	33
7.2	Traffic Distribution	35
7.3	Traffic Impact	39
8	**Conclusions:**	**41**

Appendices:

Appendix A	**Traffic Volume Diagrams**
Appendix B	**SIDRA Results**
Appendix C	**Parking Survey Results**

Attachment 4 - Traffic Report Ratio Amendment C223

1 Introduction:

Ratio Consultants Pty Ltd was commissioned by Astrodome Hire Pty Ltd to review at a strategic level the transport and parking aspects of the proposed rezoning of the site at 81-95 Burnley Street and 26-34 Doonside Street, Richmond from Industrial 3 Zone to Mixed Use Zone. The report also includes recommendations for encouraging sustainable transport.

The preliminary concept plans prepared for the proposed rezoning show six new towers across three lots and varying between 7-12 storeys in height. Parking is proposed within basement and podium levels, with the majority of access provided to Doonside Street and some access provided to Appleton Street. Preliminary yield analysis indicates the development will comprise in the order of:

- 506 dwellings;
- 6,585 square metres of commercial floor area;
- 701 square metres of retail floor area;
- 607 car spaces.

This report has been prepared to address the traffic and parking needs of the proposed rezoning, and is based on surveys and observations in the vicinity of the site and on previous studies of similar developments elsewhere in Melbourne.

Further detailed transport and parking analysis will be required at the planning permit stage.

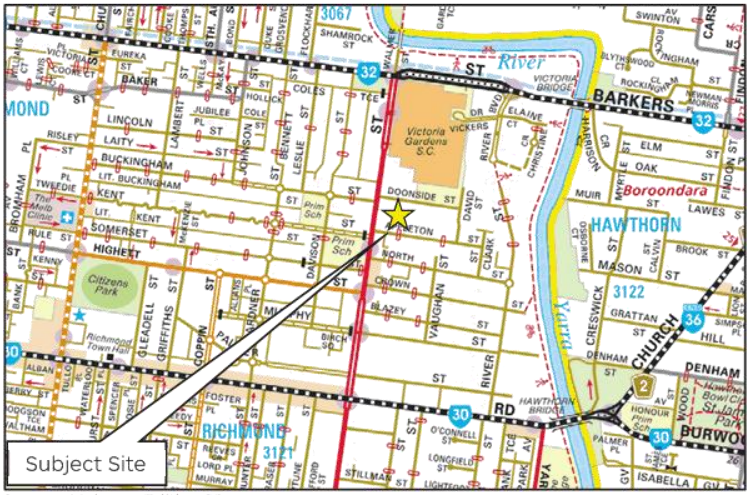
2 Existing Conditions:

2.1 Location and Environment

The subject site is located on the east side of Burnley Street, between Doonside Street and Appleton Street in Richmond. The site's location relative to the surrounding road network is shown in Figure 2.1, whilst an aerial view of the subject site and surrounds is provided in Figure 2.2.

The site is irregular in shape, with frontages of approximately 80 metres, 170 metres and 130 metres to Burnley Street, Doonside Street and Appleton Street respectively and an area of approximately 1.3 hectares.

Figure 2.1: Site Location



Source: Melways Edition 39

Figure 2.2: Aerial View of the Site and Surrounds

Source: www.nearmap.com

The subject site is currently occupied by the 'Harry the Hirer' party and marquee hire company.

A concrete hardstand area located at the eastern end of the site and accessed from Doonside Street provides for the parking and loading needs. Additional parking is located on a raised deck car park in the southeast corner of the site (accessed via the concrete hardstand) and within a recessed area in the northeast corner of the site (accessed directly from Doonside Street). Across the site, there is an estimated 85 car spaces.

The site also includes a one-way drive-through arrangement, allowing for customers to enter the building via a crossover to Doonside Street to rent or return equipment and subsequently exit via the abovementioned concrete hardstand area.

There are eight existing crossovers to the site, a number of which are redundant. The existing crossovers are summarised as follows:

- Six crossovers to Doonside Street, including one crossover servicing the customer entry, one crossover to the concrete hardstand, one crossover to the parking within the recessed area in the northeast corner of the site and three redundant crossovers.
- One crossover to Appleton Street servicing a roller door.
- One crossover to Burnley Street servicing a roller door.

Surrounding land use is mixed in nature. To the east, land use comprises a mix of ongoing industrial and also industrial uses being converted to residential, whilst to the south and west, land use is primarily established residential. To the north is Victoria Gardens Shopping Centre which comprises a wide variety of retail uses, including a full line supermarket, while further north are a number of higher density residential developments.

A number of mixed use developments have been proposed and/or approved in the vicinity of the site, as discussed further in Section 2.4.

2.2 Existing Operation

'Harry the Hirer' currently operates during the following hours:

- 8:00am-5:30pm weekdays.
- 9:00am-12:00 noon Saturdays.
- Closed Sundays.

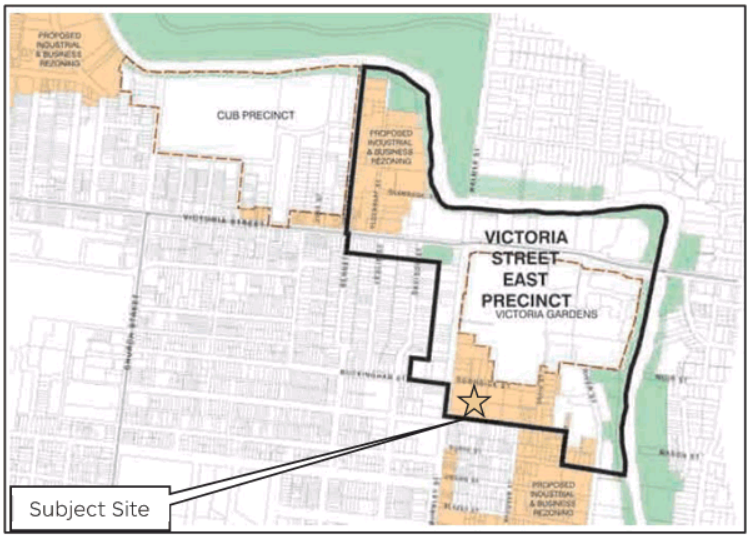
There is typically up to 107 staff on-site at any one time. Loading generally occurs within the abovementioned concrete hardstand area.

2.3 Victoria Street East Precinct, Richmond Urban Design Framework

The Victoria Street East Precinct, Richmond Urban Design Framework (UDF) was prepared as a guide for change in the area after the State Government identified the precinct as a new activity centre. The UDF was linked to the Yarra Planning Scheme through references to the approved version dated 16 November 2005.

The subject site is located within the Victoria Street East Precinct, as shown in Figure 2.3, and is identified as an opportunity for a mixed use rezoning to enable a mix of commercial and higher density residential uses.

Figure 2.3: Victoria Street East Precinct



The UDF identifies a major objective to minimise the use of private motor vehicles, stating the following:

> *"Encourage the use of public transport, cycling and walking for access between the precinct and other parts of Melbourne, in preference to use of private motor vehicles."*

In addition to the above, the UDF makes the following recommendations which are of relevance to this report:

- Create a new north-south pedestrian link past 26 Doonside Street (i.e. through the eastern end of the subject site); and
- Improve Doonside Street as a major vehicular entry to Victoria Gardens and the area to its south, to keep traffic away from Victoria Street and housing south of Appleton Street. Doonside Street should be widened (on the north side, to create an overall road reserve width of 18 to 20m) and extended north into Victoria Gardens via David Street.

GTA Consultants prepared a traffic report titled "*Victoria Street East Precinct – Priority Development Panel Traffic and Car Parking Report*". The Transport Impact Assessment notes that this report considered the future transport needs of the overall Victoria Street Precinct and detailed a number of required mitigating works to cater for the precinct, including the following:

- Creation of road link between River Boulevard and David Street;
- Signalisation of intersection of Doonside Street/Burnley Street;
- Creation of pedestrian and cycle links to Doonside Street;

2.4 Nearby Development

Introduction

A number of mixed use development proposals have been proposed and/or approved in the vicinity of the site. These are discussed below.

36-44 Doonside Street and 27-41 Appleton Street, Richmond

The proposed development at 36-44 Doonside Street and 27-41 Appleton Street, Richmond comprises 303 dwellings, 337 square metres of retail and 320 car spaces.

9-15 David Street, Richmond

The proposed development at 9-15 David Street, Richmond comprises 140 dwellings and 183 car spaces.

171 Buckingham Street, Richmond

The approved development at 171 Buckingham Street, Richmond includes 176 apartments and three townhouses.

2.5 Road Network

Burnley Street is a Declared Main Road which generally extends in a north-south direction from Victoria Street to Barkly Avenue.

In the vicinity of the site, Burnley Street operates with a single traffic lane and a single bicycle lane in each direction, with parallel parking permitted along both kerbs clear of traffic, as shown in Figure 2.4. Constructed footpaths are provided along both sides of Burnley Street.

Along the site frontage, a posted speed limit of 40 kilometres per hour applies between 8:00am-9:30am and 2:30pm-4:00pm on school days. Outside of these times, a posted speed limit of 60 kilometres per hour applies.

Figure 2.4: View of Burnley Street in the Vicinity of the Site

Doonside Street is a local road which generally extends in an east-west direction from David Street to Burnley Street.

Doonside Street has a carriageway width of approximately 9.7 metres, allowing for a single lane of traffic in each direction, with parallel parking permitted along both kerbs clear of traffic. Constructed footpaths are provided along both sides of Doonside Street.

A posted speed limit of 40 kilometres per hour applies along the length of Doonside Street.

A view of Doonside Street in the vicinity of the site is provided in Figure 2.5.

The Doonside Street/Burnley Street intersection is a 'Stop' controlled t-intersection, with priority given to Burnley Street.

Approximately 15 metres to the south of the Doonside Street/Burnley Street intersection (centre to centre) is the Buckingham Street/Burnley Street t-intersection, forming a staggered t-intersection arrangement, as shown in Figure 2.6. The Buckingham Street/Burnley Street intersection is 'Give Way' controlled, with priority given to Burnley Street.

Figure 2.5: View of Doonside Street in the Vicinity of the Site

Figure 2.6: Aerial of the Doonside Street/Burnley Street and Buckingham Street/Burnley Street Staggered t-intersection Arrangement

Appleton Street is a local road which extends generally in an east-west direction from Clark Street to Burnley Street.

Appleton Street has a carriageway width of approximately 7.9 metres, which allows for two directions of travel, with parallel parking permitted along both kerbs. Where vehicles are parked on both sides, the effective width for through vehicles is reduced to a single lane, allowing for a single direction of travel at any one time in between passing opportunities. Constructed footpaths are provided along both sides of Appleton Street.

A posted speed limit of 40 kilometres per hour applies along the length of Appleton Street.

The Appleton Street/Burnley Street intersection is a 'Stop' controlled t-intersection, with priority given to Burnley Street. Movements in and out of Appleton Street are restricted to left in/left out only.

A view of Appleton Street in the vicinity of the site is provided in Figure 2.7.

Figure 2.7: View of Appleton Street in the Vicinity of the Site

2.6 Traffic Conditions

Existing Peak Hour Turning Movements

The existing peak hour turning movements in the vicinity of the site were surveyed on Thursday 14 April 2016 between 7:30am-9:30am and 4:30pm-6:30pm at the following intersections:

- Burnley Street/Victoria Street/Walmer Street;
- Burnley Street/Doonside Street;
- Burnley Street/Buckingham Street;
- Burnley Street/Appleton Street;
- Burnley Street/Highett Street; and
- Burnley Street/Bridge Road.

The overall network peaks occurred between 7:45am-8:45am in the AM peak and between 5:00pm-6:00pm in the PM peak. The peak hour results are provided in Figure 2.8 and Appendix A. It is noted that some right turn movements were recorded at the Burnley Street/Appleton Street intersection, despite the left in/left out restriction. Review of these volumes against previously collected volumes indicates that traffic growth along Burnley Street is minimal.

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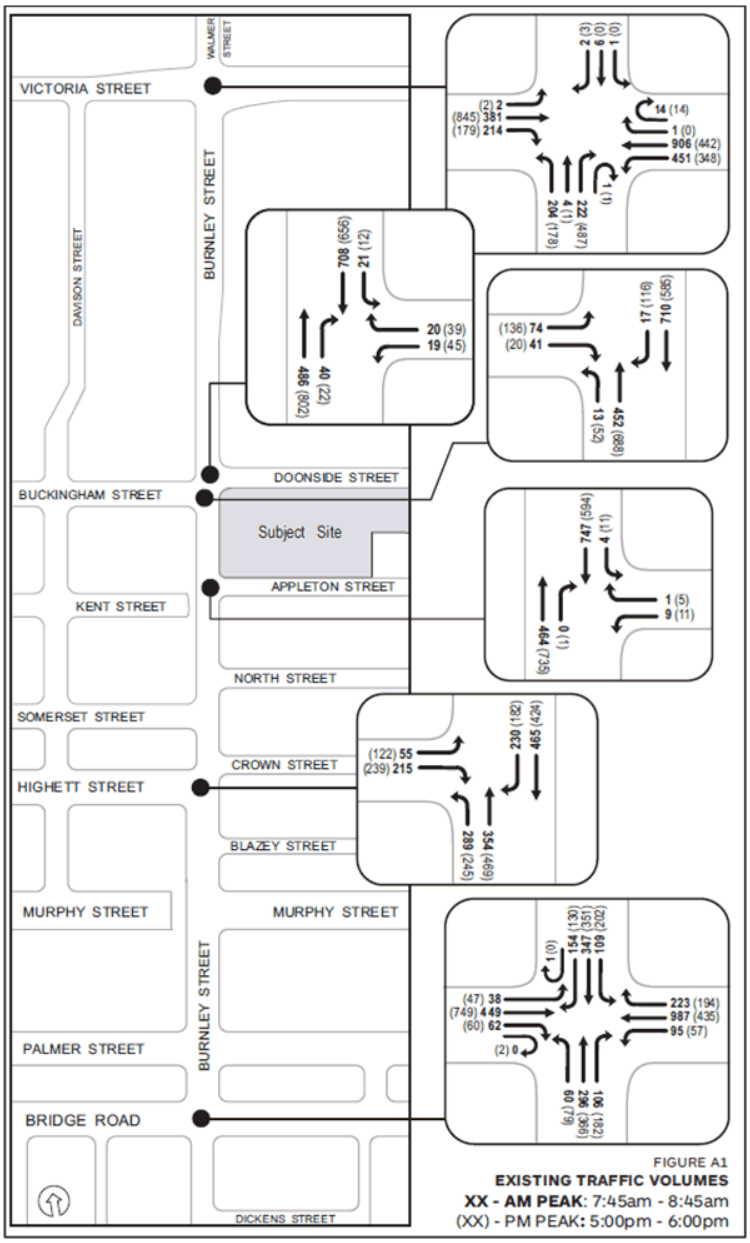
Figure 2.8: Existing Peak Hour Turning Movements



Base Case Peak Hour Turning Movements

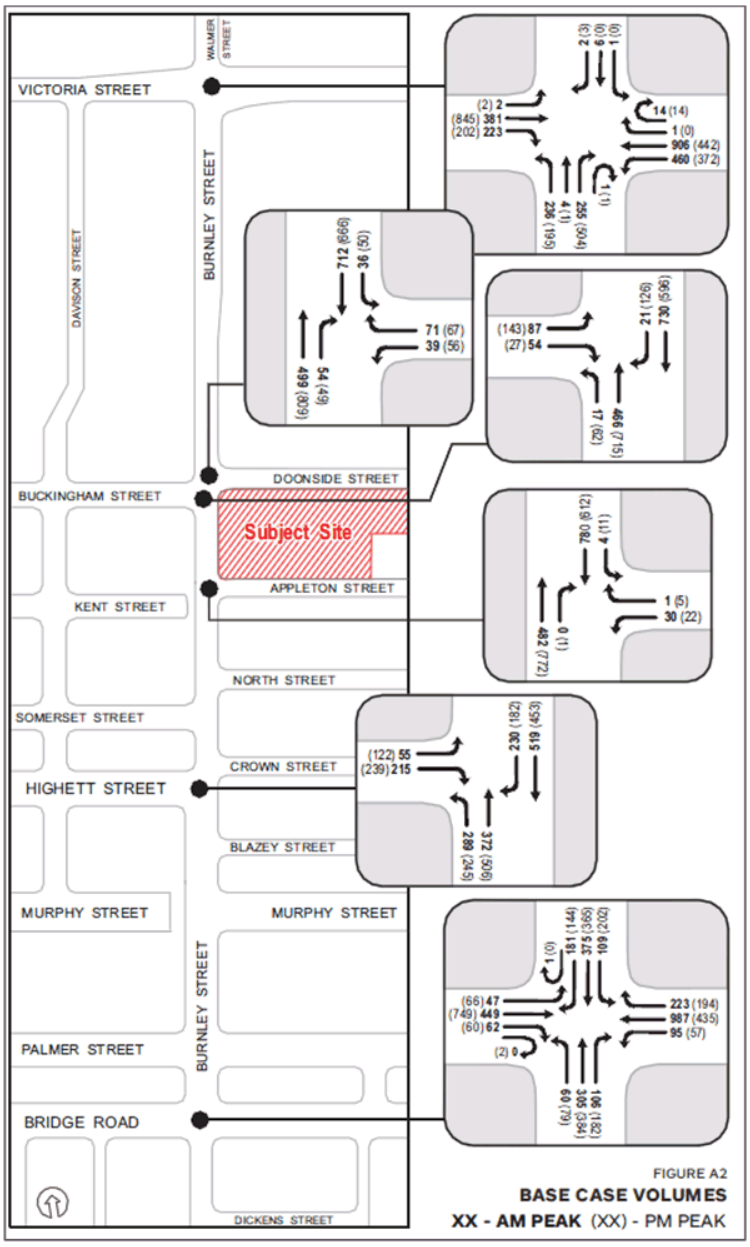
Based on the existing peak hour turning movements and the anticipated traffic generation associated with the nearby developments discussed in Section 2.4, base case peak hour turning movements have been developed and are provided in Figure 2.9 and Appendix A.

The anticipated traffic generation and distribution has been taken from the respective traffic reports discussed in Section 2.4. Where the distribution detailed in the respective traffic reports does not extend as far as the surveyed intersections, the distribution adopted in Section 7.2 for the proposed development was applied.

It is also noted that due to movements into and out of Appleton Street now being restricted to left in/left out only, movements previously assumed to be right turns in or out of Appleton Street have been relocated to Doonside Street.

Attachment 4 - Traffic Report Ratio Amendment C223

Figure 2.9: Base Case Peak Hour Turning Movements



(Includes existing traffic plus anticipated traffic generation of 36-44 Doonside Street and 27-41 Appleton Street, 9-15 David Street and 171 Buckingham Street developments).

Attachment 4 - Traffic Report Ratio Amendment C223

Existing Two-Way Traffic Volumes – Burnley Street

In order to determine the existing daily traffic volumes along Burnley Street, an automatic tube count was undertaken from Thursday 14 – Thursday 21 April 2016. The tube count was located on Burnley Street, between Buckingham Street and Appleton Street.

A summary of the results is provided in Table 2.1.

Table 2.1: Summary of Automatic Tube Count Results – Burnley Street

Type	Combined	Northbound	Southbound
Weekday Daily Average (veh/d)	16,321	8,185	8,136
AM Peak Average 8:00am-9:00am (veh/h)	1,174	470	704
PM Peak Average 5:00pm-6:00pm (veh/h)	1,360	738	622

Existing and Base Case Intersection Operation

The existing operation and the anticipated base case operation of the surveyed intersections was analysed using SIDRA Intersection. The parameters used to assess the intersections are summarised below.

Degree of Saturation (D.O.S.) is a ratio of arrival (or demand) flow to capacity. Degrees of saturation above 1.00 represent oversaturated conditions and degrees of saturation below 1.00 represent under saturated conditions. The D.O.S. ratings are detailed in Table 2.2.

Although operating conditions with a degree of saturation of close to 1.00 are undesirable, it is acknowledged that this level of congestion is typical of many metropolitan intersections during the AM and PM peak hours.

Table 2.2: Degree of Saturation Ratings

Degree of Saturation (D.O.S.)	Rating
Up to 0.6	Excellent
0.61 – 0.70	Very Good
0.71 – 0.80	Good
0.81 – 0.90	Fair
0.91 – 1.00	Poor
Greater than 1.00	Very poor

The **95th percentile queue length (95%ile queue)** is the value below which 95 percent of all observed cycle queue lengths fall, or 5 percent of all observed queue lengths exceed.

Average Delay is the average time, in seconds, that vehicles can be expected to wait at an intersection.

The results for the AM and PM peaks are provided in Table 2.3 and Table 2.4 respectively, whilst detailed results are attached in Appendix B. Review of the results indicates the following:

- Burnley Street/Victoria Street/Walmer Street is currently operating within the 'Fair' category during both the AM and PM peak periods. This is expected to continue for the base case conditions;
- The Burnley Street/Doonside Street, Burnley Street/Buckingham Street and Burnley Street/Appleton Street intersections are all currently operating within the 'Excellent' category during both the AM and PM peak periods. This is expected to continue for the base case conditions;
- The Burnley Street/Highett Street intersection is currently operating within the 'Very Good' category during the AM peak and within the 'Good' category during the PM peak. This is expected to continue for the base case conditions; and
- The Burnley Street/Bridge Road intersection is currently saturated, with a Degree of Saturation over 1.00 during the PM peak period. This is expected to continue for the base case conditions. The intersection is currently operating within the 'Poor' category during the PM peak period. This is not expected to change during the base case conditions.

Table 2.3: SIDRA Results - Existing and Base Case Operation - AM Peak

Intersection	Existing Conditions			Base Case Conditions*		
	D.O.S.	95%ile Queue (m)	Average Delay (s)	D.O.S.	95%ile Queue (m)	Average Delay (s)
Burnley Street/Victoria Street/Walmer Street	0.83	246	32	0.86	259	34
Burnley Street/Doonside Street	0.40	7	1	0.40	10	3
Burnley Street/Buckingham Street	0.41	4	1	0.43	6	2
Burnley Street/Appleton Street	0.42	1	0	0.44	2	1
Burnley Street/Highett Street	0.68	111	21	0.72	117	21
Burnley Street/Bridge Road	1.02	422	75	1.01	419	71

*(Includes existing traffic plus anticipated traffic generation of 36-44 Doonside Street and 27-41 Appleton Street, 9-15 David Street and 171 Buckingham Street developments).

Table 2.4: SIDRA Results - Existing and Base Case Operation – PM Peak

Intersection	Existing Conditions			Base Case Conditions*		
	D.O.S.	95%ile Queue (m)	Average Delay (s)	D.O.S.	95%ile Queue (m)	Average Delay (s)
Burnley Street/Victoria Street/Walmer Street	0.80	160	31	0.84	171	33
Burnley Street/Doonside Street	0.48	7	2	0.54	15	3
Burnley Street/Buckingham Street	0.48	25	3	0.51	29	4
Burnley Street/Appleton Street	0.41	1	0	0.43	2	0
Burnley Street/Highett Street	0.78	158	25	0.80	174	25
Burnley Street/Bridge Road	0.95	247	54	0.96	263	53

*(Includes existing traffic plus anticipated traffic generation of 36-44 Doonside Street and 27-41 Appleton Street, 9-15 David Street and 171 Buckingham Street developments).

Gap Analysis – Burnley Street/Doonside Street

In order to provide further assessment of the Burnley Street/Doonside Street intersection, a gap analysis was undertaken of the number of available gaps in the Burnley Street traffic flow for vehicles to utilise when turning in and out of Doonside Street.

The gap analysis was undertaken for the AM peak period (7:45am-8:45am) and the PM peak period (5:00pm-6:00pm) on Thursday 14 April 2016. The analysis assumed critical acceptance gap and follow up headway in accordance with Table 3.4 of the Austroads Guide to Road Design – Part 4A: Unsignalised and Signalised Intersections as follows:

— 5 second critical gap and 3 second follow up headway for left and right turns out of Doonside Street; and
— 4 second critical gap and 2 second follow up headway for right turns into Doonside Street.

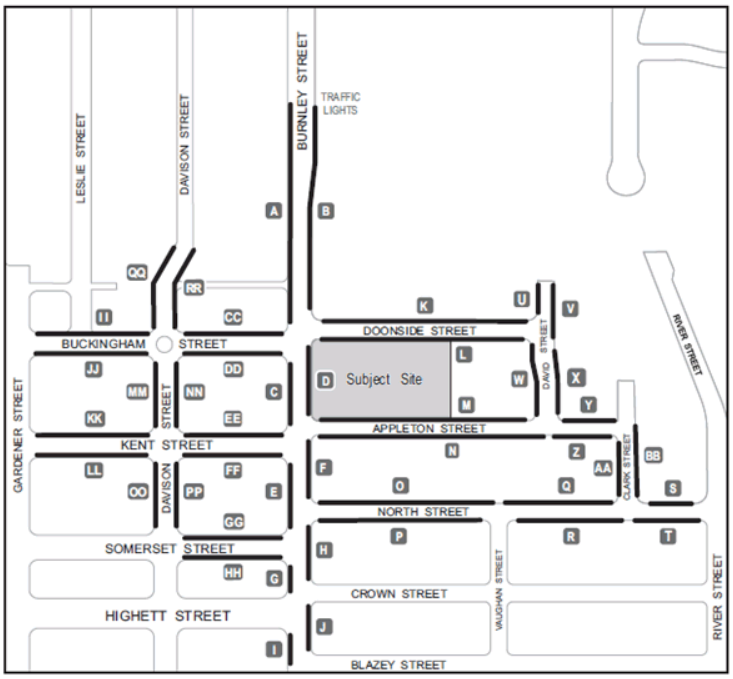
The analysis indicated sufficient capacity for the following number of movements into and out of Burnley Street:

— During the AM Peak (7:45am-8:45am)
  - 393 right turns out of Doonside Street onto Burnley Street
  - 545 left turns out of Doonside Street onto Burnley Street
  - 933 right turns into Doonside Street from Burnley Street
— PM Peak (5:00pm-6:00pm)
  - 321 right turns out of Doonside Street onto Burnley Street
  - 660 left turns out of Doonside Street onto Burnley Street
  - 1,077 right turns into Doonside Street from Burnley Street

2.7 Parking Conditions

Ratio Consultants commissioned car park occupancy surveys on Thursday 14 April 2016 between 11:00am-8:00pm and Saturday 16 April 2016 between 11:00am-8:00pm. The surveys were undertaken in hourly intervals and on-street parking within an approximate 300 metre walk of the subject site was included in the survey. The extent of the survey area is shown in Figure 2.10 and detailed survey results are presented in Appendix C.

Figure 2.10: Car Parking Survey Area



During the day, parking in the survey area is generally a mixture of unrestricted and time restricted (generally 1P and 2P) parking. At night, parking in the survey area is generally unrestricted. For the purposes of this assessment, parking with a restriction of less than 1 hour has not been considered suitable for use by visitors to the site and as such, has been excluded from the results.

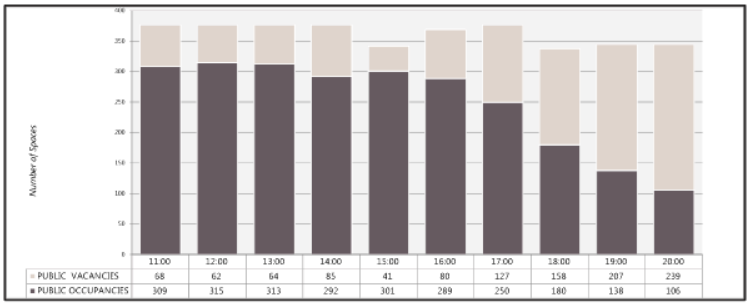
In summary, the survey results showed:

Thursday 14 April 2016

- There was a supply of 338-377 car spaces within the survey area, with the variation in supply generally a result of permit zones, loading zones, no stopping restrictions and short term time restrictions (i.e. less than 1 hour) which are only in effect during certain times.
- The peak occupancy occurred at 3:00pm when 301 out of 342 spaces were occupied (88% occupancy). At this time, 41 spaces remained vacant. It is noted that the peak occurred around typical school pick up time, with Yarra Primary School and Trinity Catholic School both located within the survey area.
- During the evening (i.e. 6:00pm and later), there was no less than 158 spaces available within the survey area.
- Along the site frontages (zones D, L and M), there was a total supply of 60 spaces. During the survey period, parking within zones D, L and M was 100% occupied between 11:00am-1:00pm and at 3:00pm.
- During the evening (i.e. 6:00pm and later), there was no less than 23 spaces available within zones D, L and M.

Figure 2.11 provides a graphical representation of the Thursday parking demands.

Figure 2.11: Car Parking Profile – Thursday 14 April 2016

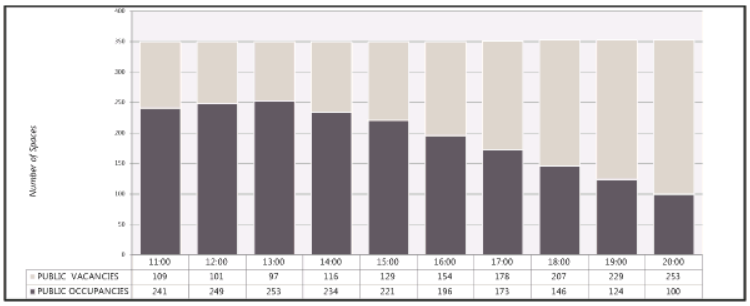


Saturday 16 April 2016

- There was a supply of 350-353 car spaces within the survey area, with the variation in supply a result of short term time restrictions (i.e. less than 1 hour) which are only in effect during certain times.
- The peak occupancy occurred at 1:00pm when 253 out of 350 spaces were occupied (72% occupancy). At this time, 97 spaces remained vacant.
- During the evening (i.e. 6:00pm and later), there was no less than 207 spaces available within the survey area.
- Along the site frontages (zones D, L and M), there was a total supply of 60 spaces. During the survey period, the peak occupancy of zones D, L and M occurred at 12:00pm and 1:00pm when 44 out of 60 spaces were occupied (73% occupancy). At this time, 16 spaces remained vacant.
- During the evening (i.e. 6:00pm and later), there was no less than 36 spaces available within zones D, L and M.

Figure 2.12 provides a graphical representation of the Saturday parking demands.

Figure 2.12: Car Parking Profile – Saturday 16 April 2016



2.8 Sustainable Transport

Public Transport

The subject site has good access to public transport, with numerous tram routes operating within close proximity of the site.

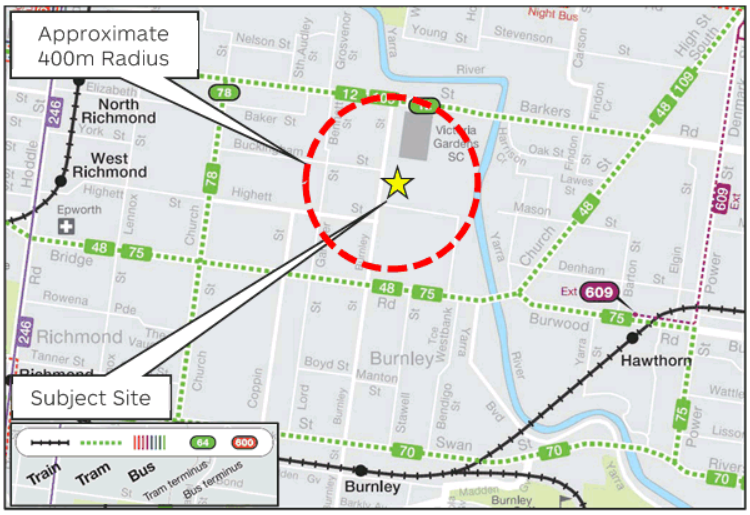
The nearest train station is Burnley Station, located an approximate 1.4 kilometre walk from the subject site, whilst North Richmond Station is readily accessible via tram routes 12 and 109 and Hawthorn Station is readily accessible via tram routes 48 and 75.

The public transport services available in the vicinity of the site are detailed in Table 2.5 and are illustrated in Figure 2.13.

Table 2.5: Public Transport Provision

Service	Route No's	Route	Nearest Stop	Approximate Walking Distance
Tram	12	Victoria Gardens – St Kilda	Burnley St / Victoria St	350m
	109	Box Hill – Port Melbourne		
	48	North Balwyn – Victoria Harbour Docklands	Burnley St / Bridge Rd	450m
	75	Etihad Stadium Docklands – Vermont South		
	78	North Richmond – Balaclava via Prahran	Kent St / Church St	850m
Train	Glen Waverley, Alamein, Belgrave and Lilydale Lines		Burnley Station	1.4km
	Hurstbridge and South Morang Lines		North Richmond Station	1.8km (accessible via tram routes 12 and 109)
	Alamein, Belgrave and Lilydale Lines		Hawthorn Station	2km (accessible via tram routes 48 and 75)

Figure 2.13: Public Transport Map



Bicycle Network

The subject site has good access to Melbourne's bicycle network. Specifically:

- On-street bicycle lanes along the site frontage on Burnley Street provide access to the north towards Abbotsford and to the south towards Burnley.
- On-street bicycle lanes on Church Street are accessible via Burnley Street and Highett Street and provide access to the north towards Abbotsford and Collingwood and to the south towards Cremorne and South Yarra.
- The Capital City Trail is accessible via the northern end of Burnley Street, whilst the Main Yarra Trail is accessible via the southern end of Burnley Street.

Car Share

The subject site is located within close proximity to a number of share car pods operated by Flexicar, GreenShareCar and GoGet. The nearest pods are operated by Flexicar and GreenShareCar (1 car each), located on Burnley Street outside of the Victoria Gardens Shopping Centre, approximately 100 metres to the north of the site. A larger pod operated by GoGet (9 cars) is located on Level 3 of the IKEA car park at the Victoria Gardens Shopping Centre, an approximate 400 metre walk from the site.

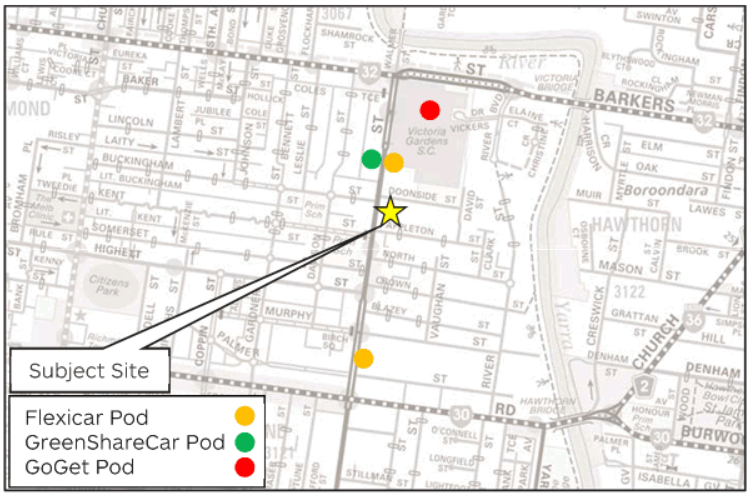
A summary of the nearest share car pods is detailed in Table 2.6 and is illustrated in Figure 2.14.

Attachment 4 - Traffic Report Ratio Amendment C223

Table 2.6: Nearest Share Car Pod Locations

Operator	Share Car Pod Location	Number of cars	Approximate Walking Distance
Flexicar	Burnley St, outside Victoria Gardens Shopping Centre	1 car	100m
	Burnley St, near corner of Palmer St	1 car	400m
GreenShareCar	Burnley St, outside Victoria Gardens Shopping Centre	1 car	100m
GoGet	Level 3 of IKEA car park at Victoria Gardens Shopping Centre	9 cars	400m

Figure 2.14: Car Share Map



3 The Proposal:

3.1 General

It is proposed to rezone the land to Mixed Use Zone (MUZ).

The preliminary concept plans prepared for the proposed rezoning show six new towers across three lots and varying between 7-12 storeys.

3.2 Yield Analysis

Preliminary yield analysis indicates the development will comprise in the order of:

- 506 dwellings. It is proposed for the ground floor dwellings fronting the new pedestrian link to be home office (SOHO) dwellings. For the purposes of the following assessment, it is assumed that external staff are not employed at the home office dwellings, thereby not generating additional traffic or parking demands.
- 6,585 square metres of commercial floor area. More specifically the commercial use will operate as follows:
  - Harry the Hirer will operate its office activities within Lot 1 of the development and will occupy 3,899sqm of floor area;
  - An additional 1,583sqm of commercial use will also be located within Lot 1 of the development; and
  - 1,103sqm of commercial / community space will be located within Lot 3 of the development.
- 701 square metres of retail floor area; and
- 607 car spaces.

The preliminary yield analysis is summarised in Table 3.1.

Attachment 4 - Traffic Report Ratio Amendment C223

Table 3.1: Preliminary Yield Analysis

Lot	Component	Dwellings	Commercial	Retail	Car Spaces
1	Tower A (12 storeys)	110 dwellings	-	-	-
	Tower B (7 storeys)	40 dwellings	-	-	-
	Podium	-	5,481 sqm	-	57 spaces
	Basement	-	-	-	222 spaces
2	Tower C (12 storeys)	108 dwellings	-	-	-
	Tower D (7 storeys)	36 dwellings	-	-	-
	Podium	53 dwellings	-	551 sqm	173 spaces
3	Tower E (11 storeys)	76 dwellings	-	-	-
	Tower F (8 storeys)	74 dwellings	-	-	-
	Podium	9 dwellings	953 sqm	-	51 spaces
	Heritage Building	-	150 sqm	150 sqm	-
	Basement	-	-	-	104 spaces
Total		**506 dwellings**	**6,585 sqm**	**701 sqm**	**607 spaces**

3.3 Proposed Internal Streets

An internal service lane is proposed to extend south from Doonside Street along the eastern side of the lot 1 and 2 boundary. The service lane is not proposed to connect through to Appleton Street. The primary function of the service lane is for vehicle access.

It is recommended that the service lane is a minimum of 5.5 metres wide to allow for simultaneous two way traffic flow.

An internal north-south pedestrian link is proposed along the Lot 2 and 3 boundary to connect from Doonside Street through to Appleton Street. It is proposed for the pedestrian link to primarily cater for pedestrians. The pedestrian link is not proposed to be used by general traffic but would be available for use by emergency vehicles as required.

It is recommended that the pedestrian link is at least 3.5 metres wide to allow for emergency vehicle use when required.

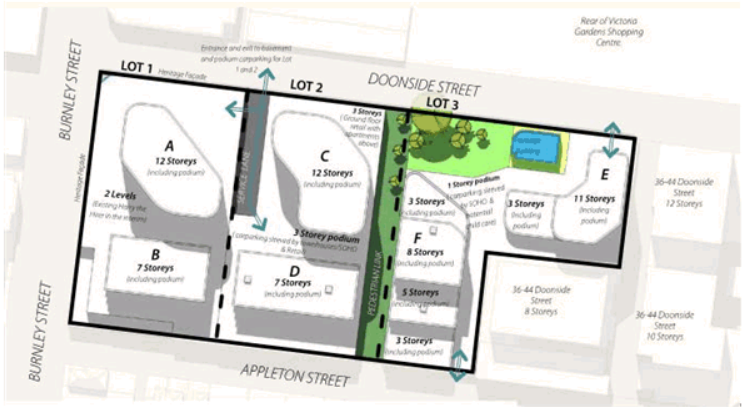
3.4 Vehicle Access

Vehicle access to parking is proposed as follows:

- To Tower A via the service lane;
- To Tower B via the service lane;
- To Tower C via the service lane;
- To Tower D via the service lane;
- To Tower E via a new crossover to Doonside Street; and
- To Tower F via a new crossover to Appleton Street.

Figure 3.1 graphically shows the proposed vehicle access arrangements.

Figure 3.1: Proposed Access Arrangements



The existing crossovers to the site will be removed and reinstated with kerb and channel.

Service vehicle (including waste collection vehicle) access is proposed to be available along the service lane.

3.5 Pedestrian Access

Pedestrian access is proposed as follows:

- To the commercial use from the southeast corner of the Burnley Street/Doonside Street intersection;
- To Tower A from the southeast corner of the Burnley Street/Doonside Street intersection;
- To Tower B from Burnley Street;
- To Tower C from the pedestrian link;
- To Tower D from the pedestrian link;
- To Tower E from Doonside Street; and
- To Tower F from the pedestrian link.

4 Parking Assessment:

4.1 Clause 52.06 Parking Requirements

Parking requirements for residential developments are set out under Clause 52.06 of the Victoria Planning Scheme, which was last updated on 1 July 2014 through the incorporation of Amendment VC116. The purpose of the Clause, among other things, is:

- To ensure that car parking is provided in accordance with the State Planning Policy Framework and Local Planning Policy Framework.
- To ensure the provision of an appropriate number of car parking spaces having regard to the demand likely to be generated, the activities on the land and the nature of the locality.
- To support sustainable transport alternatives to the motor car.
- To promote the efficient use of car parking spaces through the consolidation of car parking facilities.
- To ensure that car parking does not affect the amenity of the locality.
- To ensure that the design and location of car parking is of a high standard, creates a safe environment for users and enables easy and efficient use.

The statutory car parking requirements for uses relevant to the proposed rezoning are detailed in Table 4.1.

Table 4.1: Clause 52.06 Parking Requirements

Use	Requirement
Dwellings	1 space to each one or two bedroom dwelling
	2 spaces to each three or more bedroom dwelling (with studies or studios that are separate rooms counted as bedrooms)
	1 space for visitors to every 5 dwellings for developments of 5 or more dwellings
Shop	4 spaces to each 100 sqm of leasable floor area
Restricted Retail Premises	3 spaces to each 100 sqm of leasable floor area
Office	3.5 spaces to each 100sqm of net floor area
Food and Drink Premises	4 spaces to each 100sqm of leasable floor area

Under Clause 52.06-6 of the Planning Scheme, the Responsible Authority is able to reduce the parking requirements (including to zero), provided the applicant satisfies the responsible authority that the provision of car parking is justified on the basis of:

- The car parking demand likely to be generated by the use;
- Whether it is appropriate to allow fewer spaces to be provided than the number likely to be generated by the use.

4.2 Recommended Parking Rates

Residents

Car ownership data for residents living within the suburb of Richmond in apartment buildings of three storeys or less was obtained from the Australian Bureau of Statistics (ABS) 2016 Census. Average car ownership rates for one, two and three-bedroom apartments in the Richmond suburb are set out in Table 4.2.

Table 4.2: ABS 2016 Census Car Ownership Data - Suburb of Richmond

Dwelling Type		Average Car Ownership
Flat, unit or apartment in a three storey block or less	1 Bedroom	0.68 cars per dwelling
	2 Bedroom	1.01 cars per dwelling
	3 Bedroom	1.52 cars per dwelling

The site is well located for future residents who choose not to own a car, noting the site's close proximity to a wide range of retail (including a full line supermarket) at Victoria Gardens Shopping Centre and numerous sustainable transport options as detailed in Section 2.8.

Based on the above, it is recommended that resident parking is provided generally in accordance with the following indicative rates for future development on the site:

- **0.7 spaces to each one bedroom dwelling;**
- **0.9 spaces to each two bedroom dwelling; and**
- **1.5 spaces to each three bedroom dwelling.**

Whilst the apartment breakdown has not been prepared at this stage, the preliminary yield analysis indicates that an overall parking provision in the order of 0.8 spaces per dwelling is expected. This generally reflects the reduced indicative parking provision rates recommended above.

Visitors

Empirical surveys of visitor parking demands of residential developments in inner urban suburbs of the City indicate that visitor parking demand varies throughout the day, with peak demands occurring between 6.00pm and 9.00am.

Previous surveys conducted by Cardno Pty Ltd between 6:00am Friday 19 November 2010 to midnight Saturday 20 November 2010 at apartment developments located at 127 & 147 Beach Street, Beacon Cove demonstrated peak visitor parking demands during business hours between 9.00am and 5.00pm on a weekday of 0.06 spaces per apartment, with higher figures of 0.09 spaces per apartment outside weekday business hours.

Based on the foregoing, it is recommended that on-site visitor parking is provided generally in accordance with the following indicative rate for future developments on the site:

- 0.1 spaces to each dwelling.

Retail

Parking demands in Activity Centres are typically generated by the Activity Centre itself rather than individual smaller tenancies. In this instance, it is considered likely that a large portion of customers associated with the proposed retail would already be in the area either as local residents or staff or as part of a trip to Victoria Gardens Shopping Centre. Additional parking demands associated with customers are therefore expected to be minimal.

For any retail use at the subject site, it is considered appropriate for staff parking to be provided on-site, with customers accommodated on-street and surrounding areas. As such, it is recommended that retail parking is provided generally in accordance with the following indicative rate for future development on the site:

— **1 space to each 100 square metres of leasable floor area for staff.**

Commercial

The parking rates listed within Column B of Table 1 to Clause 52.06-5 apply where specified in a schedule to the Parking Overlay. Whilst the City of Yarra has not prepared any Parking Overlays and the Column B rates do not strictly apply in this instance, it is noted that these rates are generally intended for use in Activity Centres.

Column B of Table 1 to Clause 52.06-5 requires that parking for restricted retail is provided at a rate of 2.5 spaces to each 100 square metres of leasable floor area. In the context of the site's location, this is considered an appropriate rate. This rate is also considered appropriate for office use in this location. As such, it is recommended that commercial parking is provided generally in accordance with the following indicative rate for future development on the site:

— 2.5 spaces to each 100 square metres of leasable floor area.

Summary

Based on the foregoing, it is recommended that parking for any future development on the site is provided generally in accordance with the indicative rates provided in Table 4.3.

Table 4.3: Indicative Recommended Parking Rates

Component	Indicative Recommended Parking Rate
Residents	0.7 spaces to each one bedroom dwelling
	0.9 space to each two bedroom dwelling
	1.5 spaces to each three bedroom dwelling
Visitors	0.1 spaces to each dwelling
Retail	1 space to each 100 square metres of leasable floor area for staff
Commercial	2.5 spaces to each 100 square metres of leasable floor area

It is recommended that as part of any town planning application, a Green Travel Plan be prepared to further encourage utilisation of the wide availability of sustainable transport alternatives and subsequently reduce parking demands and traffic generation associated with the proposal.

5 Bicycle Parking:

The provisions set out under Clause 52.34-3 (Bicycle Facilities) of the Yarra Planning Scheme require that bicycle parking for the relevant uses be provided at the rates shown in Table 5.1. Clause 52.34-3 also requires that showers and change rooms are provided in accordance with the rates in Table 5.2.

Table 5.1: Bicycle Parking Statutory Requirements

Use	Requirement
Dwellings	In developments of four or more storeys, 1 to each 5 dwellings for residents
	In developments of four or more storeys, 1 to each 10 dwellings for visitors
Shop	1 to each 600 sqm of leasable floor area for employees if the leasable floor area exceeds 1,000 sqm
	1 to each 500 sqm of leasable floor area for shoppers if the leasable floor area exceeds 1,000 sqm
Retail	1 to each 600 sqm of leasable floor area for employees if the leasable floor area exceeds 1,000 sqm
	1 to each 500 sqm of leasable floor area for shoppers if the leasable floor area exceeds 1,000 sqm
Office	1 to each 300 sqm of net floor area for employees if the net floor area exceeds 1,000 sqm
	1 to each 1,000 sqm of net floor area for visitors if the net floor area exceeds 1,000 sqm

Table 5.2: Employee Shower and Change Room Statutory Requirements

Component	Employee Requirement
Showers	If 5 or more employee bicycle spaces are required, 1 shower for the first 5 employee bicycle spaces, plus 1 to each 10 employee bicycle spaces thereafter.
Change Rooms	1 change room or direct access to a communal change room to each shower. The change room may be a combined shower and change room.

For future development on the site, bicycle parking should be provided in accordance with the requirements set out in Table 5.1 as a minimum, whilst employee shower and change rooms should be provided in accordance with the requirements set out in Table 5.2.

Notwithstanding the above, in consideration of sustainable transport initiatives and the site's proximity to Melbourne's bicycle network, it is recommended that a bicycle parking provision in excess of the statutory requirements is considered for residents and office staff in particular, with bicycle parking provided generally in accordance with the following indicative rates:

- **1 bicycle space to each dwelling for residents.**
- **1 bicycle space to each 150 square metres for office staff (double the statutory rate).**

6 Loading Facilities:

The provisions set out under Clause 52.07 (Loading and Unloading of Vehicles) of the Yarra Planning Scheme states that no building or works may be constructed for the manufacture, servicing, storage or sale of goods or materials unless space is provided on the land for loading and unloading vehicles.

Retail components of any development on the site would generate a requirement for a loading bay. The requirements are summarised in Table 6.1.

Table 6.1: Loading Requirements - Clause 52.07

Floor Area of Building	Minimum Loading Bay Dimensions	
2,600sqm or less in single occupation	Area	27.4sqm
	Length	7.6m
	Width	3.6m
	Height Clearance	4.0m
For every additional 1,800sqm or part	Additional 18sqm	

Clause 52.07 states that a permit may be granted to reduce or waive these requirements if either:

- The land area is insufficient.
- Adequate provision is made for loading and unloading vehicles to the satisfaction of the responsible authority.

For smaller retail tenancies, it is typically considered impractical to provide an on-site loading bay, with loading likely to be undertaken by vans and small trucks which can undertake loading within an on-street car space.

In the case of the proposal, it is considered that loading for the smaller retail tenancies could be undertaken either on-street (Doonside Street or Burnley Street) or on-site.

For any future restricted retail use on the site, it is recommended that on-site loading be provided in accordance with the requirements of Clause 52.07.

7.1 Traffic Generation

Residents

In consideration of the locality of the site, it is estimated that the residential component of the development will generate traffic at a rate of 3 daily vehicle movements per dwelling with an allocated car space, inclusive of 0.3 peak hour vehicle movements per dwelling with an allocated car space during the AM and PM peak hours.

Assuming 80% of the proposed 506 dwellings (405 dwellings) are provided with a car space as discussed in Section 4.2 and applying the above rates results in an anticipated daily traffic volume of 1,206 daily vehicle movements, including an anticipated 122 peak hour vehicle movements during the AM and PM peak hours.

It is noted that Tower F, is proposed to be accessed via Appleton Street. As such, the anticipated traffic generation associated with Tower F is expected to be to Appleton Street, with the remaining residential traffic to Doonside Street. Assuming 80% of the proposed 74 dwellings in Tower F (59 dwellings) are provided with a car space equates to an anticipated 18 peak hour vehicle movements to Appleton Street, with the remaining 104 anticipated vehicle movements to Doonside Street.

Traffic generation during the AM peak hour typically comprises 20% inbound movements and 80% outbound movements whilst traffic generation during the PM peak hour typically comprises 60% inbound movements and 40% outbound movements.

The resultant anticipated residential traffic generation is summarised in Table 7.1.

Table 7.1: Anticipated Residential Traffic Generation

Location	AM Peak			PM Peak		
	Inbound	Outbound	Total	Inbound	Outbound	Total
To Doonside Street	21	83	104	62	42	104
To Appleton Street	4	14	18	11	7	18
Total	**25**	**97**	**122**	**73**	**49**	**122**

Commercial

For commercial use, it is assumed that 50% of the parking allocated to the use is turned over during the peak hours. Assuming parking is allocated at the recommended rate of 2.5 spaces to each 100 square metres, this equates to a parking provision of approximately 165 spaces for the 6,585 square metres of floor area and an anticipated peak hour traffic generation of 82 vehicle movements.

It is expected that the majority (90%) of movements will be inbound during the AM peak and outbound during the PM peak.

The resultant anticipated commercial traffic generation is summarised in Table 7.2.

Table 7.2: Anticipated Commercial Traffic Generation

Location	AM Peak			PM Peak		
	Inbound	Outbound	Total	Inbound	Outbound	Total
To Doonside Street	74	8	82	8	74	82
To Appleton Street	0	0	0	0	0	0
Total	**74**	**8**	**82**	**8**	**74**	**82**

Retail

During the AM and PM peak hours, it is expected that traffic associated with the retail will be limited to staff arrivals and departures.

Assuming 7 spaces are allocated to the retail uses (at the recommended parking rate of 1 space to each 100 square metres) and 50% turnover of these parking spaces during the peak hours equates to an estimated 4 vehicle movements during both the AM and PM peak.

It is expected that the majority (90%) of movements will be inbound during the AM peak and outbound during the PM peak.

The resultant anticipated retail traffic generation is summarised in Table 7.3.

Table 7.3: Anticipated Retail Traffic Generation

Location	AM Peak			PM Peak		
	Inbound	Outbound	Total	Inbound	Outbound	Total
To Doonside Street	4	0	4	0	4	4
To Appleton Street	0	0	0	0	0	0
Total	**4**	**0**	**4**	**0**	**4**	**4**

Existing Traffic Generation Associated with the Site (To be Deducted)

As discussed in Section 2.2, it is understood that there is currently 107 staff employed at the site.

Journey to Work data from the 2011 ABS census indicates that 61 percent of work journeys to the City of Yarra were by car as a driver.

Adopting a driver rate of 61 percent for the existing 107 staff and assuming 50% turnover during the peak hours equates to 33 vehicle movements associated with the existing operations.

It is expected that the majority (90%) of movements are inbound during the AM peak and outbound during the PM peak.

The resultant existing traffic generation expected to be associated with the site is summarised in Table 7.2. This traffic will be deducted from the anticipated traffic generation associated with the site.

Table 7.4: Expected Existing Traffic Generation Associated with the Site (To be Deducted)

Location	AM Peak			PM Peak		
	Inbound	Outbound	Total	Inbound	Outbound	Total
To Doonside Street	-30	-3	-33	-3	-30	-33
To Appleton Street	0	0	0	0	0	0
Total	**-30**	**-3**	**-33**	**-3**	**-30**	**-33**

Summary

Based on the foregoing, the total anticipated additional traffic generation associated with the proposal is provided in Table 7.5.

Table 7.5: Total Anticipated Traffic Generation

Location	AM Peak			PM Peak		
	Inbound	Outbound	Total	Inbound	Outbound	Total
To Doonside Street	68	88	156	67	89	156
To Appleton Street	4	14	18	11	7	18
Total	**72**	**102**	**174**	**78**	**96**	**174**

As discussed in Section 4.2, it is recommended that as part of any town planning application, a Green Travel Plan be prepared to further encourage utilisation of sustainable transport alternatives and subsequently reduce parking demands and traffic generation associated with the proposal.

7.2 Traffic Distribution

For the purposes of this assessment, the following has been assumed

— 50% of traffic is to/from the north, including:
- 25% to/from the east on Victoria Street via Burnley Street; and
- 25% to/from the west on Victoria Street via Burnley Street.

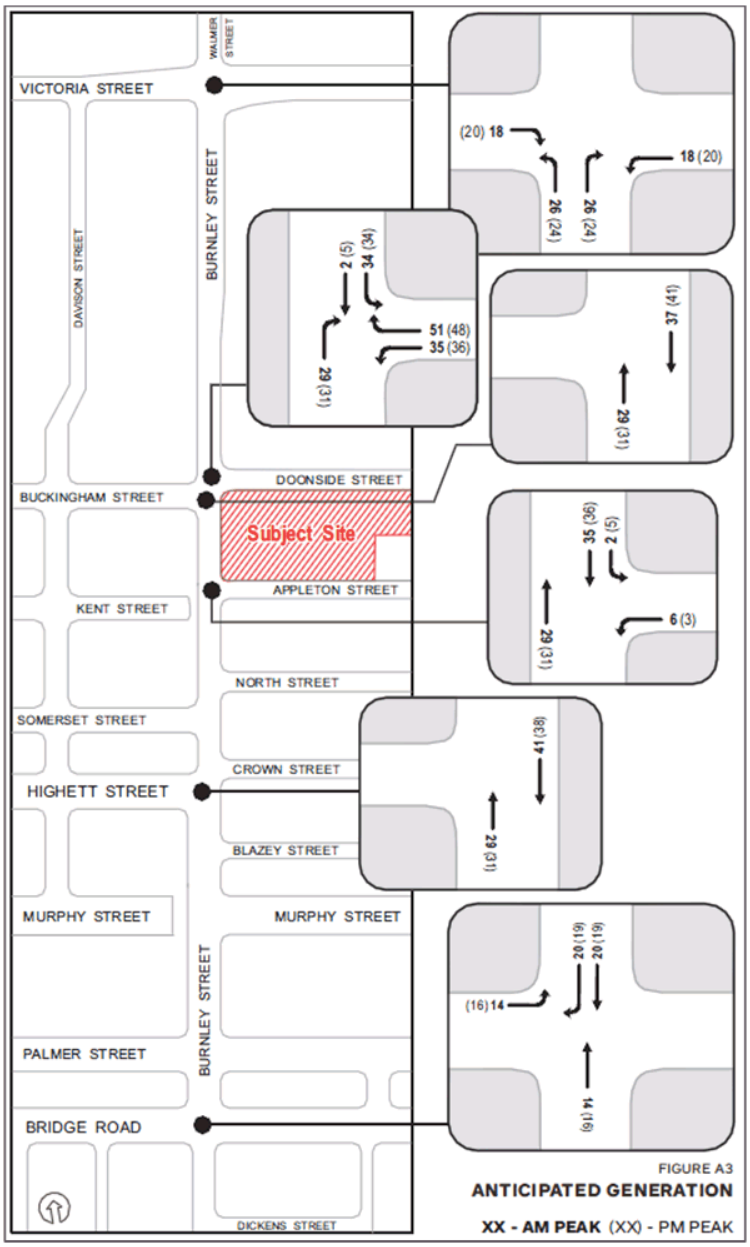
— 50% of traffic is to/from the south, including:
  - 10% to/from the east on Bridge Road via the local road network, with access to/from Bridge Road via River Street;
  - 20% to/from the south on Burnley Street past Bridge Road; and
  - 20% to/from the west on Bridge Road via Burnley Street.

It is noted that this is consistent with the traffic distribution adopted in the Transport Impact Assessment prepared for 36-44 Doonside Street and 27-41 Appleton Street, Richmond.

The anticipated traffic generation of the proposal is subsequently detailed in Figure 7.1, whilst the anticipated post development volumes are provided in Figure 7.2. Both figures are also provided in Appendix A.

Attachment 4 - Traffic Report Ratio Amendment C223

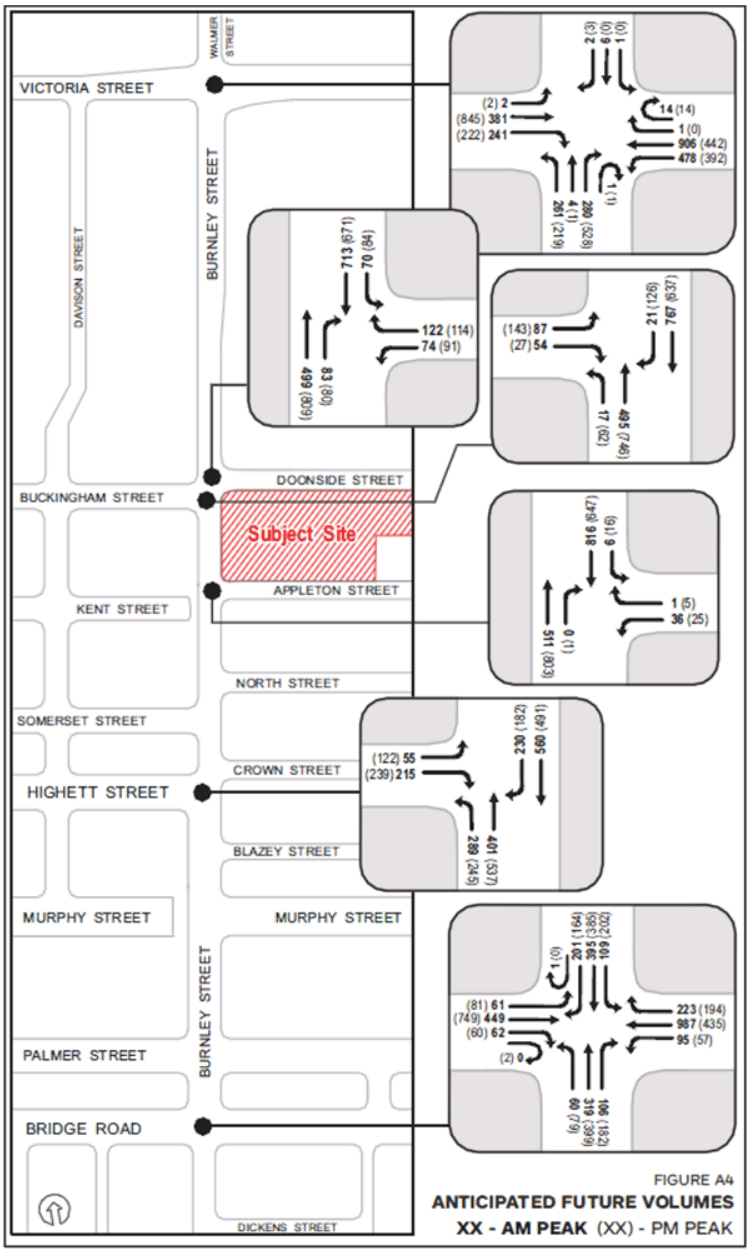
Figure 7.1: Anticipated Traffic Generation



Attachment 4 - Traffic Report Ratio Amendment C223

Figure 7.2: Anticipated Post Development Volumes

(Includes Base Case volumes plus anticipated traffic generation of proposed rezoning)



7.3 Traffic Impact

In order to assess the impact of the proposal, a SIDRA analysis of the post development conditions has been undertaken.

The results are compared against the Base Case conditions in Table 7.6 and Table 7.7 respectively. Detailed results are provided in Appendix B.

Table 7.6: SIDRA Results - Post Development Operation - AM Peak

Intersection	Base Case Conditions*			Post Development Conditions		
	D.O.S.	95%ile Queue (m)	Average Delay (s)	D.O.S.	95%ile Queue (m)	Average Delay (s)
Burnley Street/Victoria Street/Walmer Street	0.86	259	34	0.88	277	37
Burnley Street/Doonside Street	0.40	10	3	0.63	21	5
Burnley Street/Buckingham Street	0.43	6	2	0.45	7	2
Burnley Street/Appleton Street	0.44	2	1	0.46	3	1
Burnley Street/Highett Street	0.72	117	21	0.75	125	21
Burnley Street/Bridge Road	1.01	419	71	1.07	500	96

(Includes existing traffic plus anticipated traffic generation of 36-44 Doonside Street and 27-41 Appleton Street, 9-15 David Street and 171 Buckingham Street developments).

Table 7.7: SIDRA Results – Post Development Operation – PM Peak

Intersection	Base Case Conditions*			Post Development Conditions		
	D.O.S.	95%ile Queue (m)	Average Delay (s)	D.O.S.	95%ile Queue (m)	Average Delay (s)
Burnley Street/Victoria Street/Walmer Street	0.84	171	33	0.90	188	36
Burnley Street/Doonside Street	0.54	15	3	1.01	65	11
Burnley Street/Buckingham Street	0.51	29	4	0.54	33	4
Burnley Street/Appleton Street	0.43	2	0	0.45	2	0
Burnley Street/Highett Street	0.80	174	25	0.83	193	26
Burnley Street/Bridge Road	0.96	263	53	1.01	337	68

(Includes existing traffic plus anticipated traffic generation of 36-44 Doonside Street and 27-41 Appleton Street, 9-15 David Street and 171 Buckingham Street developments).

A review of the results indicates that the intersections analysed are generally expected to operate under similar conditions to the base case, with minimal increases to queues and delays.

The critical intersection is the Burnley Street/Doonside Street intersection during the PM peak hour. As can be seen in Table 7.8 above, the DOS of saturation for this intersection has increased to 1.01 under post development conditions, indicating that the intersection in its current form will ultimately reach capacity as the subject site and surrounding sites reach their full development potential. It is noted that this intersection has previously been identified by traffic studies as ultimately requiring signalisation following the development of the overall Victoria Street East Precinct.

Based on the analysis undertaken above, it is considered that the intersection of Burnley Street/Doonside Street may require signalisation to facilitate vehicle movements in and out of Doonside Street and improve the safety of these movements. The analysis suggests this is unlikely to be warranted until the final stages of development of the subject site and surrounding sites.

8 Conclusions:

It is proposed to rezone the subject site to Mixed Use Zone (MUZ). The preliminary concept plans prepared for the proposed rezoning show six new towers across three lots and varying between 7-12 storeys.

Preliminary yield analysis indicates in the order of 508 dwellings, with ground floor retail and commercial uses.

Two internal roads are proposed to service the development (one primarily for vehicles and one primarily for pedestrians), with primary vehicle access provided to Doonside Street. One tower will be accessed via Appleton Street.

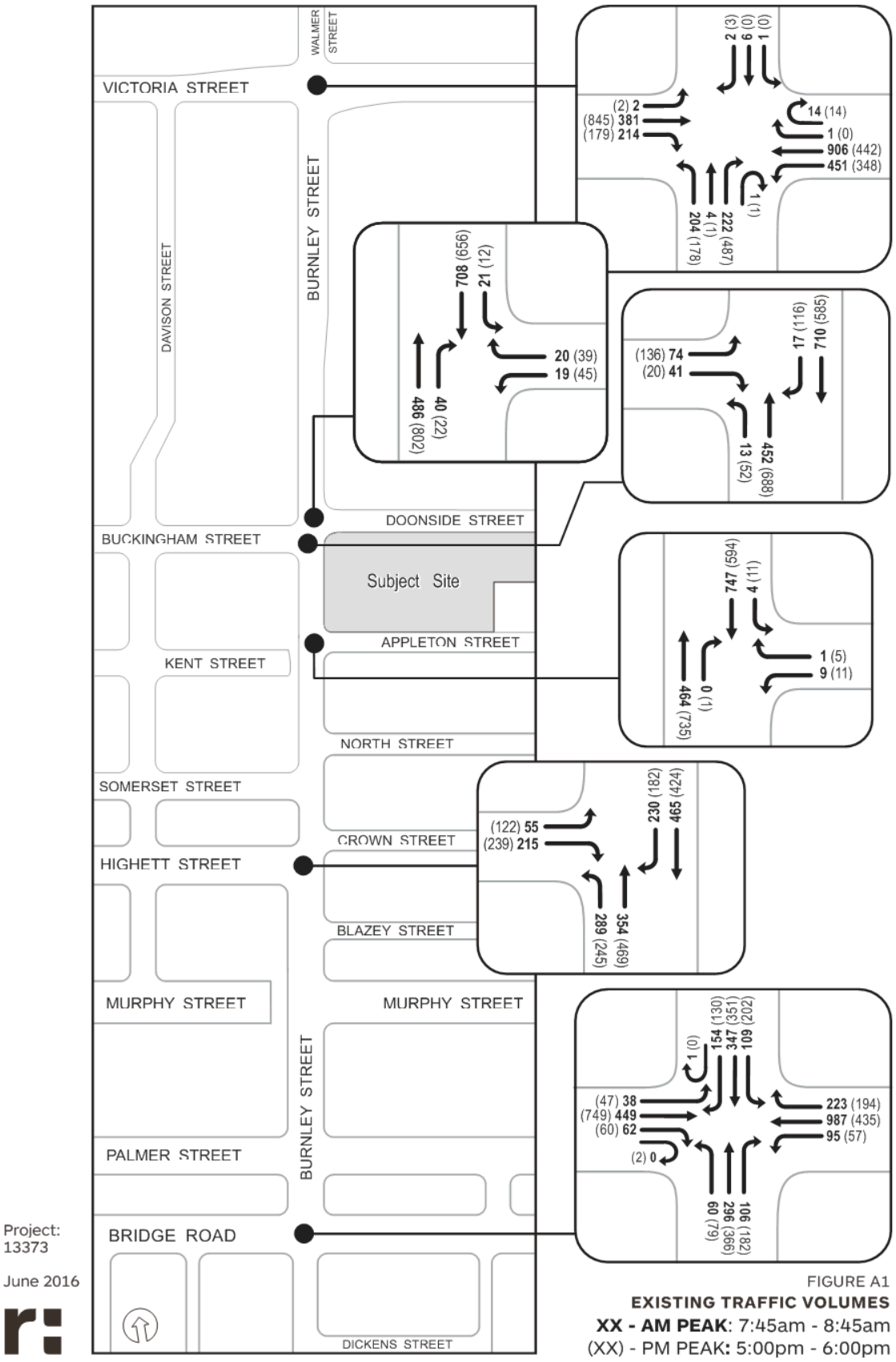
Based on the assessment undertaken above, it is concluded as follows:

- **It is recommended that reduced on-site parking provision rates be considered for development applications within the site. Indicative rates are provided as follows:**
  - **Residents**
    - **0.7 spaces to each one bedroom dwelling;**
    - **0.9 spaces to each two bedroom dwelling; and**
    - **1.5 spaces to each three bedroom dwelling.**
  - **Residential Visitors**
    - **0.1 spaces to each dwelling.**
  - **Retail**
    - **1 space to each 100 square metres of leasable floor area for staff.**
  - **Commercial**
    - **2.5 spaces to each 100 square metres of leasable floor area.**
- It is recommended that bicycle parking and end of trip facilities are provided in accordance with the statutory requirements as a minimum, with consideration given to providing bicycle parking in excess of the statutory requirements for residents and office staff in particular.
- It is considered that loading for smaller retail tenancies could be provided either on-street or on-site, whilst loading for any future restricted retail use on the site is recommended to be provided on-site in accordance with the requirements of Clause 52.07.
- The traffic anticipated to be generated by the proposal can be accommodated by the existing road network, subject to the intersection of Burnley Street and Doonside Street being upgraded during the later stages of development of the subject site and surrounding sites.

Attachment 4 - Traffic Report Ratio Amendment C223

Appendix A Traffic Volume Diagrams

Attachment 4 - Traffic Report Ratio Amendment C223



Project:
13373

June 2016

FIGURE A1
EXISTING TRAFFIC VOLUMES
XX - AM PEAK: 7:45am - 8:45am
(XX) - PM PEAK: 5:00pm - 6:00pm

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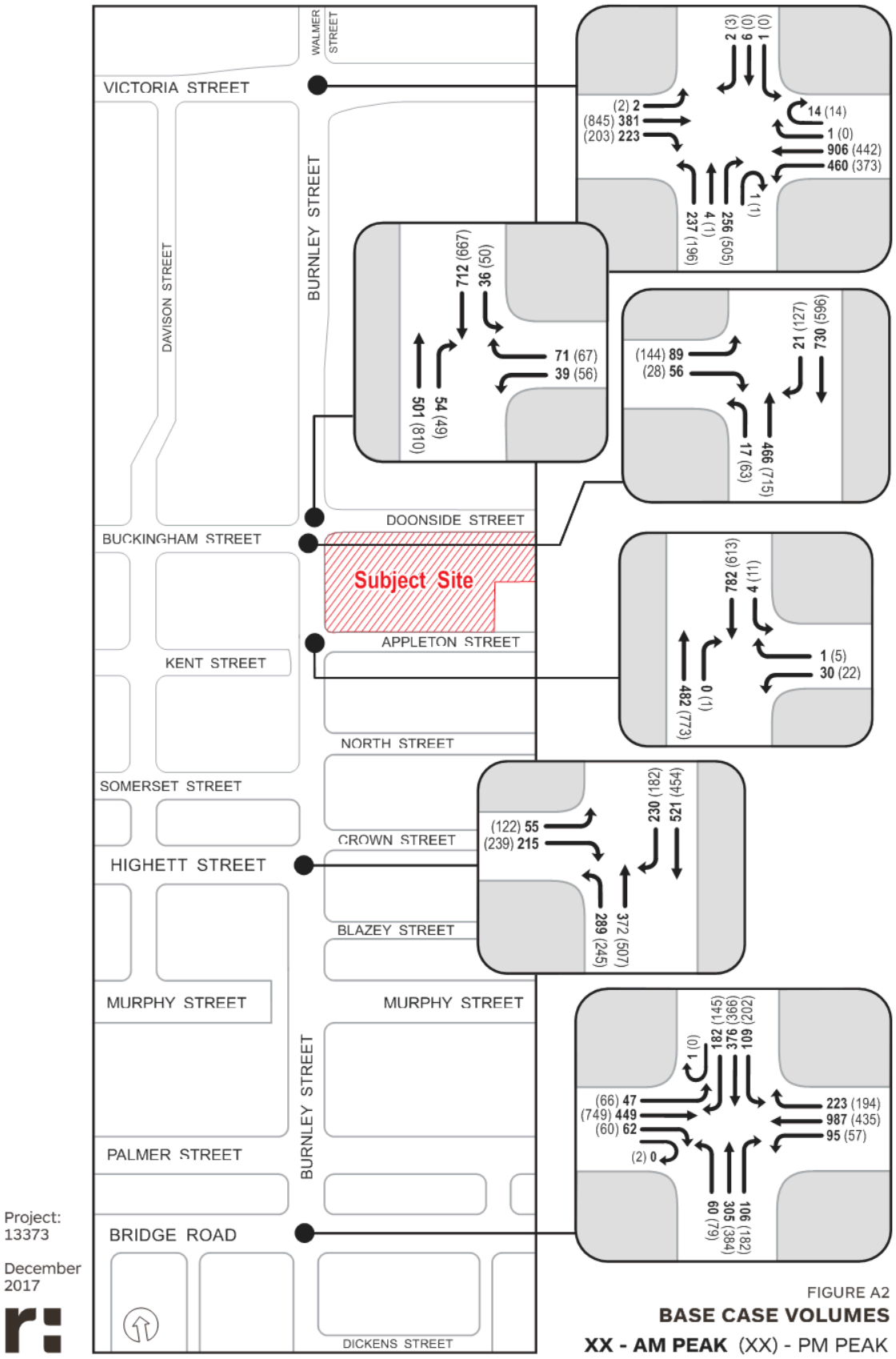

Project: 13373

December 2017

FIGURE A2

BASE CASE VOLUMES

XX - AM PEAK (XX) - PM PEAK

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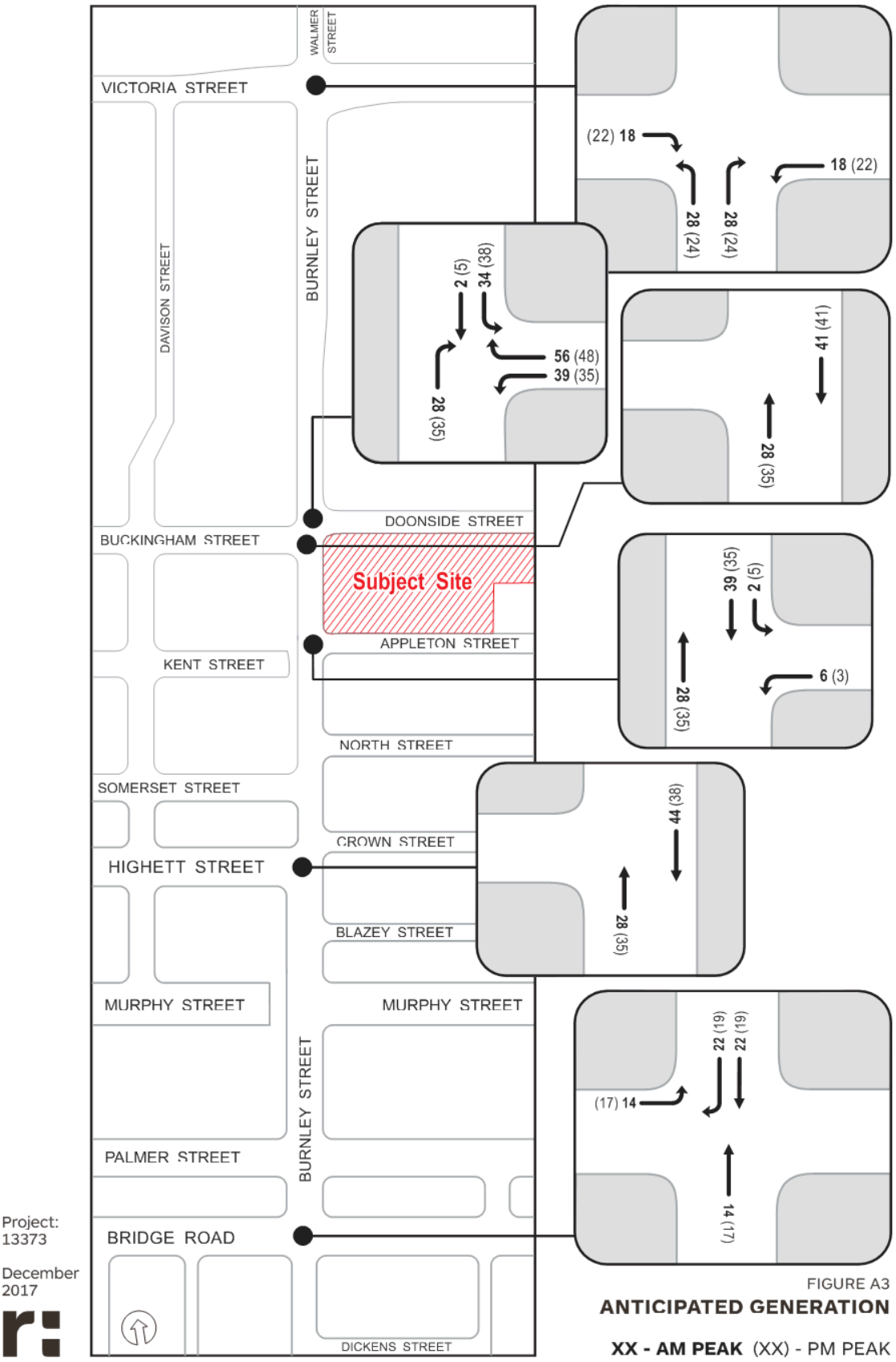

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December 2017

FIGURE A3

ANTICIPATED GENERATION

XX - AM PEAK (XX) - PM PEAK

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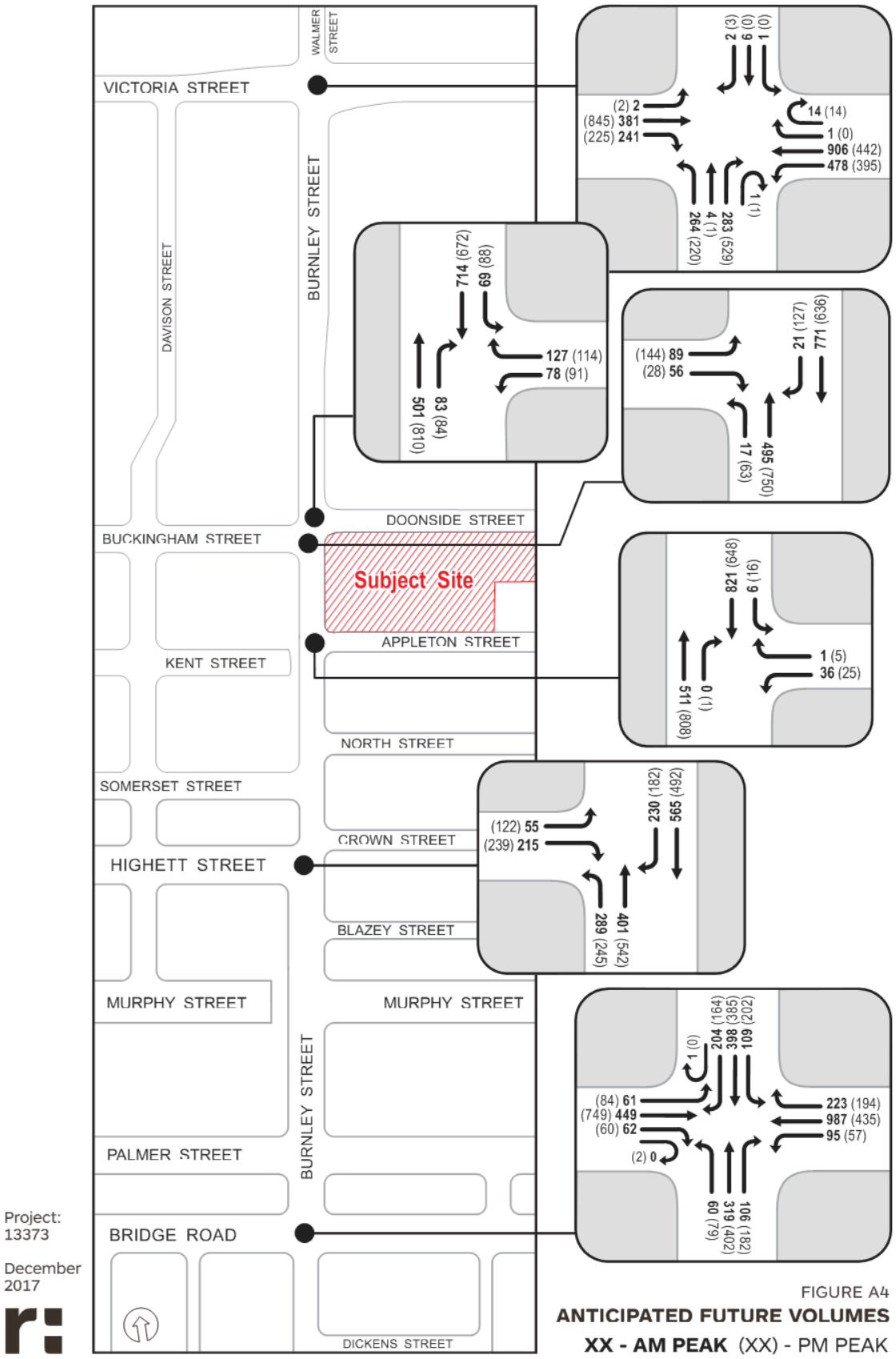

Project: 13373

December 2017

FIGURE A4

ANTICIPATED FUTURE VOLUMES

XX - AM PEAK (XX) - PM PEAK

Attachment 4 - Traffic Report Ratio Amendment C223

Appendix B SIDRA Results

Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuApAME1]

Burnley Street/Appleton Street
AM Peak Existing Conditions Run 1
Stop (Two-Way)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
2	T1	488	5.0	0.260	0.0	LOS A	0.0	0.2	0.01	0.00	59.9
3	R2	1	5.0	0.260	11.7	LOS B	0.0	0.2	0.01	0.00	57.4
Approach		489	5.0	0.260	0.1	NA	0.0	0.2	0.01	0.00	59.9
East: Appleton Street (E)											
4	L2	9	5.0	0.029	15.4	LOS C	0.1	0.7	0.69	0.96	47.2
6	R2	1	5.0	0.029	22.9	LOS C	0.1	0.7	0.69	0.96	46.8
Approach		11	5.0	0.029	16.2	LOS C	0.1	0.7	0.69	0.96	47.1
North: Burnley Street (N)											
7	L2	4	5.0	0.419	5.7	LOS A	0.0	0.0	0.00	0.00	58.0
8	T1	786	5.0	0.419	0.1	LOS A	0.0	0.0	0.00	0.00	59.9
Approach		791	5.0	0.419	0.1	NA	0.0	0.0	0.00	0.00	59.8
All Vehicles		1291	5.0	0.419	0.2	NA	0.1	0.7	0.01	0.01	59.7

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Minor Road Approach LOS values are based on average delay for all vehicle movements.
NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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MOVEMENT SUMMARY

Site: 101 [BuApPME1]

Burnley Street/Appleton Street
PM Peak Existing Conditions Run 1
Stop (Two-Way)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
2	T1	774	5.0	0.411	0.0	LOS A	0.0	0.2	0.00	0.00	60.0
3	R2	1	5.0	0.411	10.9	LOS B	0.0	0.2	0.00	0.00	57.4
Approach		775	5.0	0.411	0.0	NA	0.0	0.2	0.00	0.00	60.0
East: Appleton Street (E)											
4	L2	12	5.0	0.050	12.9	LOS B	0.2	1.2	0.68	0.96	46.7
6	R2	5	5.0	0.050	25.9	LOS D	0.2	1.2	0.68	0.96	46.3
Approach		17	5.0	0.050	17.0	LOS C	0.2	1.2	0.68	0.96	46.5
North: Burnley Street (N)											
7	L2	12	5.0	0.338	5.6	LOS A	0.0	0.0	0.00	0.01	57.9
8	T1	625	5.0	0.338	0.0	LOS A	0.0	0.0	0.00	0.01	59.8
Approach		637	5.0	0.338	0.1	NA	0.0	0.0	0.00	0.01	59.8
All Vehicles		1428	5.0	0.411	0.3	NA	0.2	1.2	0.01	0.02	59.7

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Minor Road Approach LOS values are based on average delay for all vehicle movements.
NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuBrAME1]

Burnley Street/Bridge Road
AM Peak Existing Conditions Run 1
Signals - Fixed Time Isolated Cycle Time = 120 seconds (Practical Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	63	5.0	1.017	115.5	LOS F	34.2	249.8	1.00	1.35	21.0
2	T1	312	5.0	1.017	109.9	LOS F	34.2	249.8	1.00	1.35	21.2
3	R2	112	5.0	0.487	38.9	LOS D	4.6	33.4	0.98	0.77	36.0
Approach		486	5.0	1.017	94.3	LOS F	34.2	249.8	1.00	1.22	23.4
East: Bridge Road (E)											
4	L2	100	5.0	1.000	97.5	LOS F	57.8	421.9	1.00	1.29	23.5
5	T1	1039	5.0	1.000	94.8	LOS F	57.8	421.9	1.00	1.31	23.4
6	R2	235	5.0	0.595	26.9	LOS C	7.8	57.0	0.87	0.81	40.9
Approach		1374	5.0	1.000	83.4	LOS F	57.8	421.9	0.98	1.22	25.3
North: Burnley Street (N)											
7	L2	115	5.0	0.157	29.4	LOS C	4.2	30.6	0.67	0.73	39.6
8	T1	365	5.0	1.010	105.5	LOS F	32.5	237.4	1.00	1.32	22.0
9	R2	162	5.0	0.713	41.2	LOS D	7.1	52.0	1.00	0.84	35.2
9u	U	1	5.0	0.713	42.4	LOS D	7.1	52.0	1.00	0.84	34.9
Approach		643	5.0	1.010	75.6	LOS E	32.5	237.4	0.94	1.09	26.6
West: Bridge Road (W)											
10	L2	40	5.0	0.259	32.8	LOS C	4.3	31.0	0.71	0.63	39.7
11	T1	473	5.0	0.795	36.9	LOS D	20.8	152.1	0.86	0.80	37.3
12	R2	65	5.0	0.291	58.3	LOS E	3.6	26.0	0.95	0.76	30.2
Approach		578	5.0	0.795	39.0	LOS D	20.8	152.1	0.86	0.78	36.5
All Vehicles		3081	5.0	1.017	75.2	LOS E	57.8	421.9	0.95	1.11	26.7

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Average Back of Queue Distance m	Prop. Queued	Effective Stop Rate per ped
P1	South Full Crossing	53	31.6	LOS D	0.1	0.1	0.73	0.73
P2	East Full Crossing	53	47.8	LOS E	0.2	0.2	0.89	0.89
P3	North Full Crossing	53	33.8	LOS D	0.1	0.1	0.75	0.75
P4	West Full Crossing	53	47.8	LOS E	0.2	0.2	0.89	0.89
All Pedestrians		211	40.3	LOS E			0.82	0.82

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuBrPME1]

Burnley Street/Bridge Road
PM Peak Existing Conditions Run 1
Signals - Fixed Time Isolated Cycle Time = 120 seconds (Practical Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	83	5.0	0.954	75.6	LOS E	33.8	246.5	0.93	1.13	27.4
2	T1	385	5.0	0.954	70.0	LOS E	33.8	246.5	0.93	1.13	27.8
3	R2	192	5.0	0.631	30.7	LOS C	7.0	51.1	0.91	0.80	39.1
Approach		660	5.0	0.954	59.3	LOS E	33.8	246.5	0.93	1.04	30.3
East: Bridge Road (E)											
4	L2	60	5.0	0.280	43.3	LOS D	6.4	46.6	0.84	0.73	35.5
5	T1	458	5.0	0.916	59.5	LOS E	25.8	188.5	0.97	1.03	30.3
6	R2	204	5.0	0.718	38.6	LOS D	8.4	61.0	1.00	0.83	36.2
Approach		722	5.0	0.916	52.2	LOS D	25.8	188.5	0.97	0.95	32.2
North: Burnley Street (N)											
7	L2	213	5.0	0.229	22.4	LOS C	6.7	49.0	0.58	0.73	42.9
8	T1	369	5.0	0.794	39.1	LOS D	19.0	139.0	0.87	0.82	36.6
9	R2	137	5.0	0.433	31.4	LOS C	4.8	35.2	0.91	0.78	38.8
Approach		719	5.0	0.794	32.7	LOS C	19.0	139.0	0.80	0.79	38.7
West: Bridge Road (W)											
10	L2	49	5.0	0.938	75.5	LOS E	32.3	236.0	1.00	1.14	27.4
11	T1	788	5.0	0.938	70.0	LOS E	32.3	236.0	1.00	1.14	27.9
12	R2	63	5.0	0.294	58.3	LOS E	3.6	26.0	0.95	0.76	30.2
12u	U	2	5.0	0.294	59.5	LOS E	3.6	26.0	0.95	0.76	30.0
Approach		903	5.0	0.938	69.4	LOS E	32.3	236.0	1.00	1.12	28.0
All Vehicles		3004	5.0	0.954	54.3	LOS D	33.8	246.5	0.93	0.98	31.6

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Average Back of Queue Distance m	Prop. Queued	Effective Stop Rate per ped
P1	South Full Crossing	53	40.9	LOS E	0.1	0.1	0.83	0.83
P2	East Full Crossing	53	36.9	LOS D	0.1	0.1	0.79	0.79
P3	North Full Crossing	53	43.4	LOS E	0.2	0.2	0.85	0.85
P4	West Full Crossing	53	36.9	LOS D	0.1	0.1	0.79	0.79
All Pedestrians		211	39.5	LOS D			0.81	0.81

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

▽ Site: 101 [BuBuAME1]

Burnley Street/Buckingham Street
AM Peak Existing Conditions Run 1
Giveway / Yield (Two-Way)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	14	5.0	0.008	5.6	LOS A	0.0	0.0	0.00	0.58	53.4
2	T1	476	5.0	0.252	0.0	LOS A	0.0	0.0	0.00	0.00	59.9
Approach		489	5.0	0.252	0.2	NA	0.0	0.0	0.00	0.02	59.6
North: Burnley Street (N)											
8	T1	747	5.0	0.412	0.2	LOS A	0.3	2.5	0.04	0.01	59.4
9	R2	18	5.0	0.412	5.8	LOS A	0.3	2.5	0.04	0.01	55.5
Approach		765	5.0	0.412	0.3	NA	0.3	2.5	0.04	0.01	59.3
West: Buckingham Street (W)											
10	L2	78	5.0	0.114	8.8	LOS A	0.4	3.2	0.51	0.72	45.6
12	R2	43	5.0	0.176	20.1	LOS C	0.6	4.3	0.82	0.93	43.9
Approach		121	5.0	0.176	12.8	LOS B	0.6	4.3	0.62	0.80	44.7
All Vehicles		1376	5.0	0.412	1.4	NA	0.6	4.3	0.08	0.08	57.3

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Minor Road Approach LOS values are based on average delay for all vehicle movements.
NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

▽ Site: 101 [BuBuPME1]

Burnley Street/Buckingham Street
PM Peak Existing Conditions Run 1
Giveway / Yield (Two-Way)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	55	5.0	0.031	5.6	LOS A	0.0	0.0	0.00	0.58	53.4
2	T1	724	5.0	0.383	0.1	LOS A	0.0	0.0	0.00	0.00	59.9
Approach		779	5.0	0.383	0.4	NA	0.0	0.0	0.00	0.04	59.0
North: Burnley Street (N)											
8	T1	616	5.0	0.483	2.8	LOS A	3.4	24.7	0.42	0.13	53.4
9	R2	122	5.0	0.483	9.5	LOS A	3.4	24.7	0.42	0.13	50.1
Approach		738	5.0	0.483	3.9	NA	3.4	24.7	0.42	0.13	52.8
West: Buckingham Street (W)											
10	L2	143	5.0	0.307	13.5	LOS B	1.3	9.8	0.69	0.91	41.1
12	R2	21	5.0	0.116	24.9	LOS C	0.4	2.6	0.86	0.94	41.5
Approach		164	5.0	0.307	14.9	LOS B	1.3	9.8	0.71	0.91	41.2
All Vehicles		1681	5.0	0.483	3.4	NA	3.4	24.7	0.25	0.17	53.9

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Minor Road Approach LOS values are based on average delay for all vehicle movements.
NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuDoAME1]

Burnley Street/Doonside Street
AM Peak Existing Conditions Run 1
Stop (Two-Way)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
2	T1	512	5.0	0.327	1.0	LOS A	0.9	6.7	0.18	0.05	57.1
3	R2	42	5.0	0.327	8.2	LOS A	0.9	6.7	0.18	0.05	53.5
Approach		554	5.0	0.327	1.6	NA	0.9	6.7	0.18	0.05	56.8
East: Doonside Street (E)											
4	L2	20	5.0	0.046	14.8	LOS B	0.2	1.1	0.64	0.98	41.2
6	R2	21	5.0	0.092	23.0	LOS C	0.3	2.2	0.82	1.00	43.3
Approach		41	5.0	0.092	19.0	LOS C	0.3	2.2	0.73	0.99	42.6
North: Burnley Street (N)											
7	L2	22	5.0	0.012	5.6	LOS A	0.0	0.0	0.00	0.58	53.4
8	T1	745	5.0	0.395	0.1	LOS A	0.0	0.0	0.00	0.00	59.9
Approach		767	5.0	0.395	0.2	NA	0.0	0.0	0.00	0.02	59.5
All Vehicles		1362	5.0	0.395	1.3	NA	0.9	6.7	0.10	0.06	57.5

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Minor Road Approach LOS values are based on average delay for all vehicle movements.
NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuDoPME1]

Burnley Street/Doonside Street
PM Peak Existing Conditions Run 1
Stop (Two-Way)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
2	T1	844	5.0	0.475	0.4	LOS A	0.7	5.1	0.08	0.02	58.8
3	R2	23	5.0	0.475	9.1	LOS A	0.7	5.1	0.08	0.02	54.9
Approach		867	5.0	0.475	0.7	NA	0.7	5.1	0.08	0.02	58.7
East: Doonside Street (E)											
4	L2	47	5.0	0.099	14.2	LOS B	0.3	2.5	0.62	1.00	41.8
6	R2	41	5.0	0.277	35.6	LOS E	0.9	6.7	0.91	1.03	37.7
Approach		88	5.0	0.277	24.1	LOS C	0.9	6.7	0.75	1.01	39.1
North: Burnley Street (N)											
7	L2	13	5.0	0.007	5.6	LOS A	0.0	0.0	0.00	0.58	53.4
8	T1	691	5.0	0.366	0.0	LOS A	0.0	0.0	0.00	0.00	59.9
Approach		703	5.0	0.366	0.1	NA	0.0	0.0	0.00	0.01	59.7
All Vehicles		1659	5.0	0.475	1.7	NA	0.9	6.7	0.08	0.07	57.0

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Minor Road Approach LOS values are based on average delay for all vehicle movements.
NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuHiAME1]

Burnley Street/Highett Street
AM Peak Existing Conditions Run 1
Signals - Fixed Time Isolated Cycle Time = 120 seconds (User-Given Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	304	5.0	0.234	11.3	LOS B	5.8	42.0	0.35	0.68	49.3
2	T1	373	5.0	0.676	24.8	LOS C	15.0	109.6	0.74	0.64	42.6
Approach		677	5.0	0.676	18.8	LOS B	15.0	109.6	0.57	0.66	45.4
North: Burnley Street (N)											
8	T1	489	5.0	0.671	9.5	LOS A	12.6	92.2	0.48	0.43	51.9
9	R2	242	5.0	0.390	16.9	LOS B	5.6	40.8	0.67	0.77	45.4
Approach		732	5.0	0.671	12.0	LOS B	12.6	92.2	0.55	0.54	49.5
West: Highett Street (W)											
10	L2	58	5.0	0.667	50.4	LOS D	15.2	111.1	0.96	0.84	32.3
12	R2	226	5.0	0.667	50.3	LOS D	15.2	111.1	0.96	0.84	32.2
Approach		284	5.0	0.667	50.3	LOS D	15.2	111.1	0.96	0.84	32.2
All Vehicles		1693	5.0	0.676	21.1	LOS C	15.2	111.1	0.62	0.64	43.9

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Average Back of Queue Distance m	Prop. Queued	Effective Stop Rate per ped
P3	North Full Crossing	53	40.9	LOS E	0.1	0.1	0.83	0.83
P4	West Full Crossing	53	21.6	LOS C	0.1	0.1	0.60	0.60
All Pedestrians		105	31.3	LOS D			0.71	0.71

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

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Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuHiPME1]

Burnley Street/Highett Street
PM Peak Existing Conditions Run 1
Signals - Fixed Time Isolated Cycle Time = 120 seconds (User-Given Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	258	5.0	0.184	9.0	LOS A	3.7	27.2	0.27	0.65	50.9
2	T1	494	5.0	0.779	26.7	LOS C	21.6	157.8	0.78	0.71	41.6
Approach		752	5.0	0.779	20.7	LOS C	21.6	157.8	0.60	0.69	44.4
North: Burnley Street (N)											
8	T1	446	5.0	0.609	11.1	LOS B	12.3	89.8	0.52	0.45	50.7
9	R2	192	5.0	0.408	20.2	LOS C	4.8	35.0	0.75	0.78	43.6
Approach		638	5.0	0.609	13.9	LOS B	12.3	89.8	0.59	0.55	48.3
West: Highett Street (W)											
10	L2	128	5.0	0.770	50.5	LOS D	21.3	155.3	0.98	0.88	32.2
12	R2	252	5.0	0.770	50.5	LOS D	21.3	155.3	0.98	0.88	32.2
Approach		380	5.0	0.770	50.5	LOS D	21.3	155.3	0.98	0.88	32.2
All Vehicles		1769	5.0	0.779	24.6	LOS C	21.6	157.8	0.68	0.68	42.2

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Average Back of Queue Distance m	Prop. Queued	Effective Stop Rate per ped
P3	North Full Crossing	53	37.7	LOS D	0.1	0.1	0.79	0.79
P4	West Full Crossing	53	19.9	LOS B	0.1	0.1	0.58	0.58
All Pedestrians		105	28.8	LOS C			0.68	0.68

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

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Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuViAME1]

Burnley Street/Victoria Street/Walmer Street
AM Peak Existing Conditions Run 1
Signals - Fixed Time Isolated Cycle Time = 120 seconds (User-Given Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	215	5.0	0.339	35.0	LOS C	9.2	67.3	0.77	0.78	37.4
2	T1	4	5.0	0.339	29.4	LOS C	9.2	67.3	0.77	0.78	38.2
3	R2	234	5.0	0.492	59.1	LOS E	6.6	48.0	0.97	0.79	30.1
3u	U	1	5.0	0.492	60.2	LOS E	6.6	47.8	0.97	0.79	29.9
Approach		454	5.0	0.492	47.4	LOS D	9.2	67.3	0.87	0.79	33.2
East: Victoria Street (E)											
4	L2	475	5.0	0.385	8.4	LOS A	6.9	50.1	0.34	0.66	51.9
5	T1	954	5.0	0.834	40.0	LOS D	33.7	246.2	0.92	0.89	36.2
6	R2	1	5.0	0.834	46.0	LOS D	33.7	246.2	0.98	0.93	35.3
6u	U	15	5.0	0.834	47.2	LOS D	33.7	246.2	0.98	0.93	35.2
Approach		1444	5.0	0.834	29.7	LOS C	33.7	246.2	0.73	0.81	40.2
North: Walmer Street (N)											
7	L2	1	5.0	0.077	64.2	LOS E	0.5	3.9	0.96	0.66	29.7
8	T1	6	5.0	0.077	58.6	LOS E	0.5	3.9	0.96	0.66	30.2
9	R2	2	5.0	0.077	64.2	LOS E	0.5	3.9	0.96	0.66	29.7
Approach		9	5.0	0.077	60.5	LOS E	0.5	3.9	0.96	0.66	30.0
West: Victoria Street (W)											
10	L2	2	5.0	0.164	14.2	LOS B	4.5	32.9	0.42	0.36	51.1
11	T1	401	5.0	0.164	8.6	LOS A	4.5	32.9	0.42	0.36	52.6
12	R2	225	5.0	0.829	59.5	LOS E	13.3	97.1	0.95	0.91	30.0
Approach		628	5.0	0.829	26.9	LOS C	13.3	97.1	0.61	0.56	41.4
All Vehicles		2536	5.0	0.834	32.3	LOS C	33.7	246.2	0.72	0.74	39.0

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Average Back of Queue Distance m	Prop. Queued	Effective Stop Rate per ped
P1	South Full Crossing	53	33.1	LOS D	0.1	0.1	0.74	0.74
P21	East Stage 1	53	16.1	LOS B	0.1	0.1	0.52	0.52
P22	East Stage 2	53	54.3	LOS E	0.2	0.2	0.95	0.95
P3	North Full Crossing	53	9.2	LOS A	0.1	0.1	0.39	0.39
P4	West Full Crossing	53	54.3	LOS E	0.2	0.2	0.95	0.95
All Pedestrians		263	33.4	LOS D			0.71	0.71

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuViPME1]

Burnley Street/Victoria Street/Walmer Street
PM Peak Existing Conditions Run 1
Signals - Fixed Time Isolated Cycle Time = 120 seconds (User-Given Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	187	5.0	0.209	22.8	LOS C	6.0	43.6	0.58	0.73	42.7
2	T1	1	5.0	0.209	17.2	LOS B	6.0	43.6	0.58	0.73	43.7
3	R2	513	5.0	0.787	55.7	LOS E	14.7	107.0	0.96	0.89	31.0
3u	U	1	5.0	0.787	56.8	LOS E	14.7	107.0	0.96	0.89	30.7
Approach		702	5.0	0.787	46.8	LOS D	14.7	107.0	0.86	0.84	33.4
East: Victoria Street (E)											
4	L2	366	5.0	0.308	7.5	LOS A	4.0	29.1	0.27	0.64	52.6
5	T1	465	5.0	0.784	44.7	LOS D	22.0	160.3	0.95	0.86	34.6
6	R2	1	5.0	0.784	52.4	LOS D	22.0	160.3	0.99	0.91	33.2
6u	U	15	5.0	0.784	53.6	LOS D	22.0	160.3	0.99	0.91	33.1
Approach		847	5.0	0.784	28.8	LOS C	22.0	160.3	0.66	0.76	40.6
North: Walmer Street (N)											
7	L2	1	5.0	0.044	63.8	LOS E	0.3	2.2	0.96	0.64	29.1
8	T1	1	5.0	0.044	58.2	LOS E	0.3	2.2	0.96	0.64	29.6
9	R2	3	5.0	0.044	63.9	LOS E	0.3	2.2	0.96	0.64	29.1
Approach		5	5.0	0.044	62.7	LOS E	0.3	2.2	0.96	0.64	29.2
West: Victoria Street (W)											
10	L2	2	5.0	0.480	22.5	LOS C	17.6	128.5	0.65	0.58	45.8
11	T1	889	5.0	0.480	16.3	LOS B	17.6	128.5	0.63	0.55	47.3
12	R2	188	5.0	0.795	54.2	LOS D	10.4	75.9	0.90	0.88	31.4
Approach		1080	5.0	0.795	23.0	LOS C	17.6	128.5	0.67	0.61	43.5
All Vehicles		2635	5.0	0.795	31.3	LOS C	22.0	160.3	0.72	0.72	39.4

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Average Back of Queue Distance m	Prop. Queued	Effective Stop Rate per ped
P1	South Full Crossing	53	44.3	LOS E	0.2	0.2	0.86	0.86
P21	East Stage 1	53	9.6	LOS A	0.1	0.1	0.40	0.40
P22	East Stage 2	53	54.3	LOS E	0.2	0.2	0.95	0.95
P3	North Full Crossing	53	14.0	LOS B	0.1	0.1	0.48	0.48
P4	West Full Crossing	53	48.7	LOS E	0.2	0.2	0.90	0.90
All Pedestrians		263	34.2	LOS D			0.72	0.72

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuApAMB1]

Burnley Street/Appleton Street
AM Peak Base Case Conditions Run 1
Stop (Two-Way)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
2	T1	507	5.0	0.270	0.0	LOS A	0.0	0.2	0.01	0.00	59.9
3	R2	1	5.0	0.270	12.4	LOS B	0.0	0.2	0.01	0.00	57.4
Approach		508	5.0	0.270	0.1	NA	0.0	0.2	0.01	0.00	59.9
East: Appleton Street (E)											
4	L2	32	5.0	0.090	16.5	LOS C	0.3	2.2	0.71	1.00	46.8
6	R2	1	5.0	0.090	25.6	LOS D	0.3	2.2	0.71	1.00	46.4
Approach		33	5.0	0.090	16.8	LOS C	0.3	2.2	0.71	1.00	46.8
North: Burnley Street (N)											
7	L2	4	5.0	0.437	5.7	LOS A	0.0	0.0	0.00	0.00	58.0
8	T1	821	5.0	0.437	0.1	LOS A	0.0	0.0	0.00	0.00	59.8
Approach		825	5.0	0.437	0.1	NA	0.0	0.0	0.00	0.00	59.8
All Vehicles		1366	5.0	0.437	0.5	NA	0.3	2.2	0.02	0.03	59.5

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Minor Road Approach LOS values are based on average delay for all vehicle movements.
NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuApPMB1]

Burnley Street/Appleton Street
PM Peak Base Case Conditions Run 1
Stop (Two-Way)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
2	T1	813	5.0	0.432	0.0	LOS A	0.0	0.2	0.00	0.00	60.0
3	R2	1	5.0	0.432	11.4	LOS B	0.0	0.2	0.00	0.00	57.4
Approach		814	5.0	0.432	0.0	NA	0.0	0.2	0.00	0.00	60.0
East: Appleton Street (E)											
4	L2	23	5.0	0.076	13.3	LOS B	0.3	1.9	0.66	0.98	47.2
6	R2	5	5.0	0.076	28.8	LOS D	0.3	1.9	0.66	0.98	46.7
Approach		28	5.0	0.076	16.2	LOS C	0.3	1.9	0.66	0.98	47.1
North: Burnley Street (N)											
7	L2	12	5.0	0.348	5.6	LOS A	0.0	0.0	0.00	0.01	57.9
8	T1	644	5.0	0.348	0.0	LOS A	0.0	0.0	0.00	0.01	59.8
Approach		656	5.0	0.348	0.1	NA	0.0	0.0	0.00	0.01	59.8
All Vehicles		1498	5.0	0.432	0.4	NA	0.3	1.9	0.01	0.02	59.6

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Minor Road Approach LOS values are based on average delay for all vehicle movements.
NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuBrAMB1]

Burnley Street/Bridge Road
AM Peak Base Case Conditions Run 1
Signals - Fixed Time Isolated Cycle Time = 120 seconds (Practical Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	63	5.0	0.976	90.5	LOS F	30.2	220.3	1.00	1.24	24.6
2	T1	321	5.0	0.976	84.9	LOS F	30.2	220.3	1.00	1.24	25.0
3	R2	112	5.0	0.487	38.0	LOS D	4.4	32.5	0.98	0.77	36.3
Approach		496	5.0	0.976	75.0	LOS E	30.2	220.3	1.00	1.13	26.8
East: Bridge Road (E)											
4	L2	100	5.0	0.999	96.6	LOS F	57.4	419.3	1.00	1.28	23.6
5	T1	1039	5.0	0.999	91.5	LOS F	57.4	419.3	0.97	1.28	23.9
6	R2	235	5.0	0.677	29.8	LOS C	8.3	60.3	0.92	0.83	39.6
Approach		1374	5.0	0.999	81.3	LOS F	57.4	419.3	0.96	1.20	25.6
North: Burnley Street (N)											
7	L2	115	5.0	0.157	29.4	LOS C	4.2	30.6	0.67	0.73	39.6
8	T1	395	5.0	1.011	105.7	LOS F	35.4	258.5	1.00	1.33	21.9
9	R2	191	5.0	0.729	40.1	LOS D	8.4	61.1	1.00	0.85	35.5
9u	U	1	5.0	0.729	41.3	LOS D	8.4	61.1	1.00	0.85	35.3
Approach		701	5.0	1.011	75.3	LOS E	35.4	258.5	0.95	1.10	26.7
West: Bridge Road (W)											
10	L2	49	5.0	0.223	34.3	LOS C	6.1	44.2	0.74	0.66	39.1
11	T1	473	5.0	0.683	32.8	LOS C	17.6	128.6	0.84	0.73	38.9
12	R2	65	5.0	0.336	60.7	LOS E	3.7	26.7	0.97	0.76	29.6
Approach		587	5.0	0.683	36.0	LOS D	17.6	128.6	0.84	0.73	37.6
All Vehicles		3158	5.0	1.011	70.5	LOS E	57.4	419.3	0.94	1.08	27.7

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Average Back of Queue Distance m	Prop. Queued	Effective Stop Rate per ped
P1	South Full Crossing	53	31.6	LOS D	0.1	0.1	0.73	0.73
P2	East Full Crossing	53	46.0	LOS E	0.2	0.2	0.88	0.88
P3	North Full Crossing	53	33.8	LOS D	0.1	0.1	0.75	0.75
P4	West Full Crossing	53	46.0	LOS E	0.2	0.2	0.88	0.88
All Pedestrians		211	39.4	LOS D			0.81	0.81

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuBrPMB1]

Burnley Street/Bridge Road
PM Peak Base Case Conditions Run 1
Signals - Fixed Time Isolated Cycle Time = 120 seconds (Practical Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	83	5.0	0.963	78.7	LOS E	36.0	262.7	0.94	1.16	26.8
2	T1	404	5.0	0.963	73.1	LOS E	36.0	262.7	0.94	1.16	27.1
3	R2	192	5.0	0.603	29.3	LOS C	6.7	49.2	0.89	0.80	39.7
Approach		679	5.0	0.963	61.4	LOS E	36.0	262.7	0.92	1.05	29.7
East: Bridge Road (E)											
4	L2	60	5.0	0.269	42.5	LOS D	6.5	47.4	0.83	0.72	35.8
5	T1	458	5.0	0.878	52.6	LOS D	23.8	173.4	0.96	0.97	32.1
6	R2	204	5.0	0.847	45.6	LOS D	9.4	68.5	1.00	0.91	33.9
Approach		722	5.0	0.878	49.8	LOS D	23.8	173.4	0.96	0.93	32.9
North: Burnley Street (N)											
7	L2	213	5.0	0.237	23.6	LOS C	6.9	50.7	0.60	0.74	42.3
8	T1	384	5.0	0.802	39.0	LOS D	19.9	145.0	0.87	0.83	36.7
9	R2	152	5.0	0.462	30.6	LOS C	5.2	37.9	0.91	0.79	39.1
Approach		748	5.0	0.802	32.9	LOS C	19.9	145.0	0.80	0.79	38.6
West: Bridge Road (W)											
10	L2	69	5.0	0.929	72.2	LOS E	32.3	236.1	1.00	1.12	28.1
11	T1	788	5.0	0.929	66.6	LOS E	32.3	236.1	1.00	1.12	28.6
12	R2	63	5.0	0.368	62.0	LOS E	3.7	27.1	0.97	0.76	29.3
12u	U	2	5.0	0.368	63.2	LOS E	3.7	27.1	0.97	0.76	29.1
Approach		923	5.0	0.929	66.7	LOS E	32.3	236.1	1.00	1.09	28.6
All Vehicles		3073	5.0	0.963	53.3	LOS D	36.0	262.7	0.93	0.97	31.9

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Distance m	Prop. Queued	Effective Stop Rate per ped
P1	South Full Crossing	53	40.1	LOS E	0.1	0.1	0.82	0.82
P2	East Full Crossing	53	36.1	LOS D	0.1	0.1	0.78	0.78
P3	North Full Crossing	53	42.6	LOS E	0.2	0.2	0.84	0.84
P4	West Full Crossing	53	36.1	LOS D	0.1	0.1	0.78	0.78
All Pedestrians		211	38.7	LOS D			0.80	0.80

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

▽ Site: 101 [BuBuAMB1]

Burnley Street/Buckingham Street
AM Peak Base Case Conditions Run 1
Giveway / Yield (Two-Way)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	18	5.0	0.010	5.6	LOS A	0.0	0.0	0.00	0.58	53.4
2	T1	491	5.0	0.260	0.0	LOS A	0.0	0.0	0.00	0.00	59.9
Approach		508	5.0	0.260	0.2	NA	0.0	0.0	0.00	0.02	59.5
North: Burnley Street (N)											
8	T1	768	5.0	0.428	0.2	LOS A	0.5	3.3	0.05	0.02	59.3
9	R2	22	5.0	0.428	6.1	LOS A	0.5	3.3	0.05	0.02	55.3
Approach		791	5.0	0.428	0.4	NA	0.5	3.3	0.05	0.02	59.2
West: Buckingham Street (W)											
10	L2	92	5.0	0.136	9.0	LOS A	0.5	3.9	0.52	0.74	45.4
12	R2	57	5.0	0.249	22.7	LOS C	0.9	6.4	0.85	0.96	42.6
Approach		148	5.0	0.249	14.3	LOS B	0.9	6.4	0.65	0.82	43.8
All Vehicles		1447	5.0	0.428	1.8	NA	0.9	6.4	0.09	0.10	56.6

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Minor Road Approach LOS values are based on average delay for all vehicle movements.
NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

▽ Site: 101 [BuBuPMB1]

Burnley Street/Buckingham Street
PM Peak Base Case Conditions Run 1
Giveway / Yield (Two-Way)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	65	5.0	0.036	5.6	LOS A	0.0	0.0	0.00	0.58	53.4
2	T1	753	5.0	0.399	0.1	LOS A	0.0	0.0	0.00	0.00	59.9
Approach		818	5.0	0.399	0.5	NA	0.0	0.0	0.00	0.05	58.9
North: Burnley Street (N)											
8	T1	627	5.0	0.511	3.4	LOS A	4.0	29.1	0.47	0.15	52.3
9	R2	133	5.0	0.511	10.4	LOS B	4.0	29.1	0.47	0.15	49.2
Approach		760	5.0	0.511	4.6	NA	4.0	29.1	0.47	0.15	51.8
West: Buckingham Street (W)											
10	L2	151	5.0	0.340	14.4	LOS B	1.5	11.2	0.71	0.93	40.3
12	R2	28	5.0	0.172	27.6	LOS D	0.5	3.9	0.88	0.96	40.3
Approach		179	5.0	0.340	16.5	LOS C	1.5	11.2	0.74	0.94	40.3
All Vehicles		1757	5.0	0.511	3.9	NA	4.0	29.1	0.28	0.18	53.1

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Minor Road Approach LOS values are based on average delay for all vehicle movements.
NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuDoAMB1]

Burnley Street/Doonside Street
AM Peak Base Case Conditions Run 1
Stop (Two-Way)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
2	T1	525	5.0	0.355	1.4	LOS A	1.3	9.6	0.24	0.07	56.2
3	R2	57	5.0	0.355	8.6	LOS A	1.3	9.6	0.24	0.07	52.6
Approach		582	5.0	0.355	2.1	NA	1.3	9.6	0.24	0.07	55.8
East: Doonside Street (E)											
4	L2	41	5.0	0.096	15.1	LOS C	0.3	2.4	0.66	1.00	41.0
6	R2	75	5.0	0.346	28.1	LOS D	1.3	9.4	0.87	1.05	40.9
Approach		116	5.0	0.346	23.5	LOS C	1.3	9.4	0.80	1.03	40.9
North: Burnley Street (N)											
7	L2	38	5.0	0.021	5.6	LOS A	0.0	0.0	0.00	0.58	53.4
8	T1	749	5.0	0.397	0.1	LOS A	0.0	0.0	0.00	0.00	59.9
Approach		787	5.0	0.397	0.3	NA	0.0	0.0	0.00	0.03	59.3
All Vehicles		1485	5.0	0.397	2.8	NA	1.3	9.6	0.16	0.12	55.1

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Minor Road Approach LOS values are based on average delay for all vehicle movements.
NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuDoPMB1]

Burnley Street/Doonside Street
PM Peak Base Case Conditions Run 1
Stop (Two-Way)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
2	T1	852	5.0	0.517	1.1	LOS A	1.7	12.4	0.18	0.04	57.1
3	R2	52	5.0	0.517	10.1	LOS B	1.7	12.4	0.18	0.04	53.4
Approach		903	5.0	0.517	1.6	NA	1.7	12.4	0.18	0.04	56.9
East: Doonside Street (E)											
4	L2	59	5.0	0.126	14.4	LOS B	0.4	3.3	0.63	1.00	41.5
6	R2	71	5.0	0.534	48.6	LOS E	2.0	14.5	0.94	1.09	33.3
Approach		129	5.0	0.534	33.0	LOS D	2.0	14.5	0.80	1.05	35.4
North: Burnley Street (N)											
7	L2	53	5.0	0.029	5.6	LOS A	0.0	0.0	0.00	0.58	53.4
8	T1	701	5.0	0.371	0.1	LOS A	0.0	0.0	0.00	0.00	59.9
Approach		754	5.0	0.371	0.4	NA	0.0	0.0	0.00	0.04	59.0
All Vehicles		1786	5.0	0.534	3.4	NA	2.0	14.5	0.15	0.11	54.3

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Minor Road Approach LOS values are based on average delay for all vehicle movements.
NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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MOVEMENT SUMMARY

Site: 101 [BuHiAMB1]

Burnley Street/Highett Street
AM Peak Base Case Conditions Run 1
Signals - Fixed Time Isolated Cycle Time = 120 seconds (User-Given Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	304	5.0	0.237	11.7	LOS B	5.9	43.3	0.37	0.68	49.0
2	T1	392	5.0	0.710	25.1	LOS C	16.0	116.6	0.75	0.65	42.4
Approach		696	5.0	0.710	19.3	LOS B	16.0	116.6	0.58	0.67	45.1
North: Burnley Street (N)											
8	T1	546	5.0	0.716	9.4	LOS A	14.3	104.6	0.49	0.44	51.9
9	R2	242	5.0	0.389	16.8	LOS B	5.4	39.7	0.67	0.77	45.4
Approach		788	5.0	0.716	11.7	LOS B	14.3	104.6	0.55	0.54	49.8
West: Highett Street (W)											
10	L2	58	5.0	0.691	51.7	LOS D	15.5	113.0	0.97	0.85	31.9
12	R2	226	5.0	0.691	51.7	LOS D	15.5	113.0	0.97	0.85	31.8
Approach		284	5.0	0.691	51.7	LOS D	15.5	113.0	0.97	0.85	31.8
All Vehicles		1768	5.0	0.716	21.1	LOS C	16.0	116.6	0.63	0.64	44.0

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Average Back of Queue Distance m	Prop. Queued	Effective Stop Rate per ped
P3	North Full Crossing	53	41.8	LOS E	0.1	0.1	0.84	0.84
P4	West Full Crossing	53	21.6	LOS C	0.1	0.1	0.60	0.60
All Pedestrians		105	31.7	LOS D			0.72	0.72

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

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Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuHiPMB1]

Burnley Street/Highett Street
PM Peak Base Case Conditions Run 1
Signals - Fixed Time Isolated Cycle Time = 120 seconds (User-Given Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	258	5.0	0.182	8.8	LOS A	3.6	26.1	0.26	0.65	51.0
2	T1	533	5.0	0.802	27.1	LOS C	23.8	173.7	0.77	0.73	41.4
Approach		791	5.0	0.802	21.1	LOS C	23.8	173.7	0.61	0.70	44.2
North: Burnley Street (N)											
8	T1	477	5.0	0.654	10.9	LOS B	13.1	95.8	0.52	0.46	50.9
9	R2	192	5.0	0.430	20.6	LOS C	4.7	34.4	0.76	0.78	43.4
Approach		668	5.0	0.654	13.7	LOS B	13.1	95.8	0.59	0.55	48.5
West: Highett Street (W)											
10	L2	128	5.0	0.794	52.8	LOS D	21.9	160.0	0.99	0.90	31.6
12	R2	252	5.0	0.794	52.8	LOS D	21.9	160.0	0.99	0.90	31.5
Approach		380	5.0	0.794	52.8	LOS D	21.9	160.0	0.99	0.90	31.5
All Vehicles		1839	5.0	0.802	25.0	LOS C	23.8	173.7	0.68	0.69	42.0

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Average Back of Queue Distance m	Prop. Queued	Effective Stop Rate per ped
P3	North Full Crossing	53	38.5	LOS D	0.1	0.1	0.80	0.80
P4	West Full Crossing	53	18.7	LOS B	0.1	0.1	0.56	0.56
All Pedestrians		105	28.6	LOS C			0.68	0.68

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

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Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuViAMB1]

Burnley Street/Victoria Street/Walmer Street
AM Peak Base Case Conditions Run 1
Signals - Fixed Time Isolated Cycle Time = 120 seconds (User-Given Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	248	5.0	0.379	34.8	LOS C	10.7	78.2	0.78	0.79	37.5
2	T1	4	5.0	0.379	29.2	LOS C	10.7	78.2	0.78	0.79	38.2
3	R2	268	5.0	0.564	59.7	LOS E	7.6	55.7	0.99	0.80	29.9
3u	U	1	5.0	0.564	60.8	LOS E	7.6	55.6	0.99	0.80	29.7
Approach		522	5.0	0.564	47.6	LOS D	10.7	78.2	0.88	0.79	33.2
East: Victoria Street (E)											
4	L2	484	5.0	0.396	8.7	LOS A	7.4	54.3	0.35	0.67	51.7
5	T1	954	5.0	0.857	43.7	LOS D	35.5	259.1	0.93	0.93	35.0
6	R2	1	5.0	0.857	49.5	LOS D	35.5	259.1	0.99	0.97	34.1
6u	U	15	5.0	0.857	50.7	LOS D	35.5	259.1	0.99	0.97	34.0
Approach		1454	5.0	0.857	32.1	LOS C	35.5	259.1	0.74	0.84	39.2
North: Walmer Street (N)											
7	L2	1	5.0	0.077	64.2	LOS E	0.5	3.9	0.96	0.66	29.7
8	T1	6	5.0	0.077	58.6	LOS E	0.5	3.9	0.96	0.66	30.2
9	R2	2	5.0	0.077	64.2	LOS E	0.5	3.9	0.96	0.66	29.7
Approach		9	5.0	0.077	60.5	LOS E	0.5	3.9	0.96	0.66	30.0
West: Victoria Street (W)											
10	L2	2	5.0	0.164	14.2	LOS B	4.5	32.9	0.42	0.36	51.1
11	T1	401	5.0	0.164	8.6	LOS A	4.5	32.9	0.42	0.36	52.6
12	R2	235	5.0	0.828	58.6	LOS E	13.8	100.6	0.95	0.91	30.2
Approach		638	5.0	0.828	27.1	LOS C	13.8	100.6	0.61	0.56	41.3
All Vehicles		2623	5.0	0.857	34.1	LOS C	35.5	259.1	0.74	0.76	38.3

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Average Back of Queue Distance m	Prop. Queued	Effective Stop Rate per ped
P1	South Full Crossing	53	33.8	LOS D	0.1	0.1	0.75	0.75
P21	East Stage 1	53	15.5	LOS B	0.1	0.1	0.51	0.51
P22	East Stage 2	53	54.3	LOS E	0.2	0.2	0.95	0.95
P3	North Full Crossing	53	9.2	LOS A	0.1	0.1	0.39	0.39
P4	West Full Crossing	53	54.3	LOS E	0.2	0.2	0.95	0.95
All Pedestrians		263	33.4	LOS D			0.71	0.71

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuViPMB1]

Burnley Street/Victoria Street/Walmer Street
PM Peak Base Case Conditions Run 1
Signals - Fixed Time Isolated Cycle Time = 120 seconds (User-Given Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	205	5.0	0.222	21.8	LOS C	6.4	46.6	0.57	0.73	43.2
2	T1	1	5.0	0.222	16.2	LOS B	6.4	46.6	0.57	0.73	44.2
3	R2	531	5.0	0.838	59.7	LOS E	15.9	116.4	0.97	0.92	29.9
3u	U	1	5.0	0.838	60.9	LOS E	15.9	116.4	0.97	0.92	29.7
Approach		738	5.0	0.838	49.1	LOS D	15.9	116.4	0.86	0.87	32.7
East: Victoria Street (E)											
4	L2	392	5.0	0.338	7.9	LOS A	4.9	35.9	0.30	0.65	52.3
5	T1	465	5.0	0.836	49.4	LOS D	23.4	171.1	0.97	0.91	33.1
6	R2	1	5.0	0.836	57.9	LOS E	23.4	171.1	1.00	0.97	31.6
6u	U	15	5.0	0.836	59.1	LOS E	23.4	171.1	1.00	0.97	31.5
Approach		873	5.0	0.836	31.0	LOS C	23.4	171.1	0.67	0.79	39.6
North: Walmer Street (N)											
7	L2	1	5.0	0.044	63.8	LOS E	0.3	2.2	0.96	0.64	29.1
8	T1	1	5.0	0.044	58.2	LOS E	0.3	2.2	0.96	0.64	29.6
9	R2	3	5.0	0.044	63.9	LOS E	0.3	2.2	0.96	0.64	29.1
Approach		5	5.0	0.044	62.7	LOS E	0.3	2.2	0.96	0.64	29.2
West: Victoria Street (W)											
10	L2	2	5.0	0.485	22.6	LOS C	17.8	130.2	0.65	0.58	45.8
11	T1	889	5.0	0.485	16.4	LOS B	17.8	130.2	0.63	0.56	47.3
12	R2	213	5.0	0.836	56.8	LOS E	12.2	88.9	0.89	0.91	30.7
Approach		1104	5.0	0.836	24.2	LOS C	17.8	130.2	0.68	0.62	42.9
All Vehicles		2720	5.0	0.838	33.2	LOS C	23.4	171.1	0.72	0.74	38.6

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Average Back of Queue Distance m	Prop. Queued	Effective Stop Rate per ped
P1	South Full Crossing	53	46.0	LOS E	0.2	0.2	0.88	0.88
P21	East Stage 1	53	8.8	LOS A	0.1	0.1	0.38	0.38
P22	East Stage 2	53	54.3	LOS E	0.2	0.2	0.95	0.95
P3	North Full Crossing	53	14.0	LOS B	0.1	0.1	0.48	0.48
P4	West Full Crossing	53	48.7	LOS E	0.2	0.2	0.90	0.90
All Pedestrians		263	34.4	LOS D			0.72	0.72

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuApAMF1]

Burnley Street/Appleton Street
AM Peak Future Conditions Run 1
Stop (Two-Way)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
2	T1	538	5.0	0.287	0.0	LOS A	0.0	0.2	0.01	0.00	59.9
3	R2	1	5.0	0.287	13.3	LOS B	0.0	0.2	0.01	0.00	57.4
Approach		539	5.0	0.287	0.1	NA	0.0	0.2	0.01	0.00	59.9
East: Appleton Street (E)											
4	L2	38	5.0	0.116	17.5	LOS C	0.4	2.8	0.74	1.00	46.3
6	R2	1	5.0	0.116	28.7	LOS D	0.4	2.8	0.74	1.00	45.9
Approach		39	5.0	0.116	17.8	LOS C	0.4	2.8	0.74	1.00	46.3
North: Burnley Street (N)											
7	L2	6	5.0	0.458	5.7	LOS A	0.0	0.0	0.00	0.00	58.0
8	T1	859	5.0	0.458	0.1	LOS A	0.0	0.0	0.00	0.00	59.8
Approach		865	5.0	0.458	0.1	NA	0.0	0.0	0.00	0.00	59.8
All Vehicles		1443	5.0	0.458	0.6	NA	0.4	2.8	0.02	0.03	59.4

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Minor Road Approach LOS values are based on average delay for all vehicle movements.
NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuApPMF1]

Burnley Street/Appleton Street
PM Peak Future Conditions Run 1
Stop (Two-Way)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
2	T1	845	5.0	0.449	0.0	LOS A	0.0	0.2	0.00	0.00	60.0
3	R2	1	5.0	0.449	12.3	LOS B	0.0	0.2	0.00	0.00	57.4
Approach		846	5.0	0.449	0.0	NA	0.0	0.2	0.00	0.00	60.0
East: Appleton Street (E)											
4	L2	26	5.0	0.090	13.9	LOS B	0.3	2.2	0.68	1.00	46.7
6	R2	5	5.0	0.090	32.3	LOS D	0.3	2.2	0.68	1.00	46.3
Approach		32	5.0	0.090	16.9	LOS C	0.3	2.2	0.68	1.00	46.7
North: Burnley Street (N)											
7	L2	17	5.0	0.370	5.6	LOS A	0.0	0.0	0.00	0.01	57.9
8	T1	681	5.0	0.370	0.1	LOS A	0.0	0.0	0.00	0.01	59.8
Approach		698	5.0	0.370	0.2	NA	0.0	0.0	0.00	0.01	59.7
All Vehicles		1576	5.0	0.449	0.4	NA	0.3	2.2	0.02	0.03	59.5

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Minor Road Approach LOS values are based on average delay for all vehicle movements.
NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuBrAMF1]

Burnley Street/Bridge Road
AM Peak Future Conditions Run 1
Signals - Fixed Time Isolated Cycle Time = 120 seconds (Practical Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	63	5.0	1.008	109.5	LOS F	35.6	260.0	1.00	1.33	21.8
2	T1	336	5.0	1.008	103.9	LOS F	35.6	260.0	1.00	1.33	22.1
3	R2	112	5.0	0.431	36.4	LOS D	4.3	31.3	0.96	0.77	36.9
Approach		511	5.0	1.008	89.8	LOS F	35.6	260.0	0.99	1.21	24.1
East: Bridge Road (E)											
4	L2	100	5.0	1.074	146.7	LOS F	68.5	500.4	1.00	1.55	17.6
5	T1	1039	5.0	1.074	143.0	LOS F	68.5	500.4	1.00	1.56	17.6
6	R2	235	5.0	0.685	31.9	LOS C	8.5	62.1	0.96	0.84	38.7
Approach		1374	5.0	1.074	124.3	LOS F	68.5	500.4	0.99	1.44	19.4
North: Burnley Street (N)											
7	L2	115	5.0	0.151	28.1	LOS C	4.1	29.7	0.65	0.73	40.2
8	T1	416	5.0	1.056	133.6	LOS F	42.1	307.6	1.00	1.48	18.6
9	R2	212	5.0	0.822	43.3	LOS D	9.6	69.8	1.00	0.89	34.5
9u	U	1	5.0	0.822	44.5	LOS D	9.6	69.8	1.00	0.89	34.2
Approach		743	5.0	1.056	91.5	LOS F	42.1	307.6	0.95	1.19	23.7
West: Bridge Road (W)											
10	L2	64	5.0	0.251	36.9	LOS D	6.7	48.6	0.77	0.69	37.9
11	T1	473	5.0	0.768	38.3	LOS D	19.8	144.7	0.89	0.80	36.7
12	R2	65	5.0	0.291	58.3	LOS E	3.6	26.0	0.95	0.76	30.2
Approach		602	5.0	0.768	40.3	LOS D	19.8	144.7	0.88	0.79	36.0
All Vehicles		3229	5.0	1.074	95.6	LOS F	68.5	500.4	0.96	1.22	23.1

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Average Back of Queue Distance m	Prop. Queued	Effective Stop Rate per ped
P1	South Full Crossing	53	34.6	LOS D	0.1	0.1	0.76	0.76
P2	East Full Crossing	53	46.0	LOS E	0.2	0.2	0.88	0.88
P3	North Full Crossing	53	36.9	LOS D	0.1	0.1	0.79	0.79
P4	West Full Crossing	53	46.0	LOS E	0.2	0.2	0.88	0.88
All Pedestrians		211	40.9	LOS E			0.82	0.82

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuBrPMF1]

Burnley Street/Bridge Road
PM Peak Future Conditions Run 1
Signals - Fixed Time Isolated Cycle Time = 120 seconds (Practical Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	83	5.0	1.009	107.9	LOS F	46.2	337.0	1.00	1.35	22.0
2	T1	420	5.0	1.009	102.3	LOS F	46.2	337.0	1.00	1.35	22.3
3	R2	192	5.0	0.603	29.8	LOS C	6.7	49.0	0.91	0.80	39.5
Approach		695	5.0	1.009	83.0	LOS F	46.2	337.0	0.98	1.20	25.3
East: Bridge Road (E)											
4	L2	60	5.0	0.257	45.3	LOS D	8.6	62.8	0.87	0.75	35.0
5	T1	458	5.0	0.842	49.5	LOS D	20.5	149.7	0.96	0.91	33.0
6	R2	204	5.0	0.745	39.7	LOS D	8.7	63.2	1.00	0.85	35.8
Approach		722	5.0	0.842	46.4	LOS D	20.5	149.7	0.96	0.88	33.9
North: Burnley Street (N)											
7	L2	213	5.0	0.233	23.0	LOS C	6.8	49.9	0.59	0.74	42.6
8	T1	405	5.0	0.865	47.0	LOS D	23.4	170.6	0.90	0.92	33.9
9	R2	173	5.0	0.707	37.8	LOS D	6.3	46.2	1.00	0.83	36.4
Approach		791	5.0	0.865	38.5	LOS D	23.4	170.6	0.84	0.85	36.5
West: Bridge Road (W)											
10	L2	85	5.0	1.008	106.8	LOS F	40.6	296.6	1.00	1.33	22.1
11	T1	788	5.0	1.008	102.0	LOS F	40.6	296.6	1.00	1.33	22.3
12	R2	63	5.0	0.315	59.5	LOS E	3.6	26.4	0.96	0.76	29.9
12u	U	2	5.0	0.315	60.7	LOS E	3.6	26.4	0.96	0.76	29.7
Approach		939	5.0	1.008	99.5	LOS F	40.6	296.6	1.00	1.29	22.7
All Vehicles		3146	5.0	1.009	68.3	LOS E	46.2	337.0	0.94	1.07	28.1

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Average Back of Queue Distance m	Prop. Queued	Effective Stop Rate per ped
P1	South Full Crossing	53	41.8	LOS E	0.1	0.1	0.84	0.84
P2	East Full Crossing	53	36.9	LOS D	0.1	0.1	0.79	0.79
P3	North Full Crossing	53	44.3	LOS E	0.2	0.2	0.86	0.86
P4	West Full Crossing	53	36.9	LOS D	0.1	0.1	0.79	0.79
All Pedestrians		211	40.0	LOS D			0.82	0.82

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

▽ Site: 101 [BuBuAMF1]

Burnley Street/Buckingham Street
AM Peak Future Conditions Run 1
Giveway / Yield (Two-Way)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	18	5.0	0.010	5.6	LOS A	0.0	0.0	0.00	0.58	53.4
2	T1	521	5.0	0.276	0.0	LOS A	0.0	0.0	0.00	0.00	59.9
Approach		539	5.0	0.276	0.2	NA	0.0	0.0	0.00	0.02	59.5
North: Burnley Street (N)											
8	T1	807	5.0	0.449	0.2	LOS A	0.5	3.7	0.05	0.01	59.2
9	R2	22	5.0	0.449	6.5	LOS A	0.5	3.7	0.05	0.01	55.3
Approach		829	5.0	0.449	0.4	NA	0.5	3.7	0.05	0.01	59.1
West: Buckingham Street (W)											
10	L2	92	5.0	0.142	9.4	LOS A	0.6	4.0	0.54	0.76	45.1
12	R2	57	5.0	0.281	25.7	LOS D	1.0	7.2	0.87	0.98	41.1
Approach		148	5.0	0.281	15.6	LOS C	1.0	7.2	0.67	0.84	42.8
All Vehicles		1517	5.0	0.449	1.8	NA	1.0	7.2	0.09	0.10	56.5

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Minor Road Approach LOS values are based on average delay for all vehicle movements.
NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

▽ Site: 101 [BuBuPMF1]

Burnley Street/Buckingham Street
PM Peak Future Conditions Run 1
Giveway / Yield (Two-Way)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	65	5.0	0.036	5.6	LOS A	0.0	0.0	0.00	0.58	53.4
2	T1	785	5.0	0.416	0.1	LOS A	0.0	0.0	0.00	0.00	59.9
Approach		851	5.0	0.416	0.5	NA	0.0	0.0	0.00	0.04	58.9
North: Burnley Street (N)											
8	T1	671	5.0	0.543	3.8	LOS A	4.5	32.8	0.49	0.14	51.8
9	R2	133	5.0	0.543	11.4	LOS B	4.5	32.8	0.49	0.14	48.8
Approach		803	5.0	0.543	5.0	NA	4.5	32.8	0.49	0.14	51.3
West: Buckingham Street (W)											
10	L2	151	5.0	0.361	15.4	LOS C	1.6	11.9	0.74	0.95	39.4
12	R2	28	5.0	0.198	31.8	LOS D	0.6	4.5	0.90	0.97	38.5
Approach		179	5.0	0.361	18.0	LOS C	1.6	11.9	0.76	0.96	39.2
All Vehicles		1833	5.0	0.543	4.2	NA	4.5	32.8	0.29	0.18	52.8

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Minor Road Approach LOS values are based on average delay for all vehicle movements.
NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuDoAMF1]

Burnley Street/Doonside Street
AM Peak Future Conditions Run 1
Stop (Two-Way)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
2	T1	525	5.0	0.401	2.4	LOS A	2.2	15.8	0.36	0.11	54.2
3	R2	87	5.0	0.401	9.3	LOS A	2.2	15.8	0.36	0.11	50.9
Approach		613	5.0	0.401	3.4	NA	2.2	15.8	0.36	0.11	53.7
East: Doonside Street (E)											
4	L2	78	5.0	0.182	15.5	LOS C	0.6	4.7	0.68	1.00	40.7
6	R2	128	5.0	0.634	38.3	LOS E	2.9	21.2	0.93	1.16	36.7
Approach		206	5.0	0.634	29.7	LOS D	2.9	21.2	0.83	1.10	37.6
North: Burnley Street (N)											
7	L2	74	5.0	0.041	5.6	LOS A	0.0	0.0	0.00	0.58	53.4
8	T1	751	5.0	0.397	0.1	LOS A	0.0	0.0	0.00	0.00	59.9
Approach		824	5.0	0.397	0.6	NA	0.0	0.0	0.00	0.05	58.8
All Vehicles		1643	5.0	0.634	5.3	NA	2.9	21.2	0.24	0.20	51.9

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Minor Road Approach LOS values are based on average delay for all vehicle movements.
NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuDoPMF1]

Burnley Street/Doonside Street
PM Peak Future Conditions Run 1
Stop (Two-Way)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
2	T1	852	5.0	0.564	2.0	LOS A	3.0	22.2	0.31	0.07	55.1
3	R2	84	5.0	0.564	11.2	LOS B	3.0	22.2	0.31	0.07	51.7
Approach		936	5.0	0.564	2.8	NA	3.0	22.2	0.31	0.07	54.8
East: Doonside Street (E)											
4	L2	96	5.0	0.206	14.9	LOS B	0.8	5.6	0.66	1.01	41.2
6	R2	120	5.0	1.008	143.7	LOS F	8.9	64.9	1.00	1.66	17.8
Approach		216	5.0	1.008	86.5	LOS F	8.9	64.9	0.85	1.37	21.4
North: Burnley Street (N)											
7	L2	88	5.0	0.049	5.6	LOS A	0.0	0.0	0.00	0.58	53.4
8	T1	706	5.0	0.374	0.1	LOS A	0.0	0.0	0.00	0.00	59.9
Approach		795	5.0	0.374	0.7	NA	0.0	0.0	0.00	0.06	58.5
All Vehicles		1946	5.0	1.008	11.2	NA	8.9	64.9	0.24	0.21	45.2

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Minor Road Approach LOS values are based on average delay for all vehicle movements.
NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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MOVEMENT SUMMARY

Site: 101 [BuHiAMF1]

Burnley Street/Highett Street
AM Peak Future Conditions Run 1
Signals - Fixed Time Isolated Cycle Time = 120 seconds (User-Given Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	304	5.0	0.237	11.7	LOS B	5.9	43.3	0.37	0.68	49.0
2	T1	422	5.0	0.729	24.3	LOS C	17.1	125.1	0.75	0.66	42.8
Approach		726	5.0	0.729	19.0	LOS B	17.1	125.1	0.59	0.67	45.2
North: Burnley Street (N)											
8	T1	589	5.0	0.747	8.7	LOS A	15.1	110.6	0.48	0.43	52.4
9	R2	242	5.0	0.392	16.4	LOS B	5.2	37.7	0.67	0.77	45.7
Approach		832	5.0	0.747	11.0	LOS B	15.1	110.6	0.54	0.53	50.3
West: Highett Street (W)											
10	L2	58	5.0	0.745	55.4	LOS E	16.2	118.6	0.99	0.87	30.9
12	R2	226	5.0	0.745	55.3	LOS E	16.2	118.6	0.99	0.87	30.8
Approach		284	5.0	0.745	55.4	LOS E	16.2	118.6	0.99	0.87	30.8
All Vehicles		1842	5.0	0.747	21.0	LOS C	17.1	125.1	0.63	0.64	44.0

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Average Back of Queue Distance m	Prop. Queued	Effective Stop Rate per ped
P3	North Full Crossing	53	43.4	LOS E	0.2	0.2	0.85	0.85
P4	West Full Crossing	53	20.5	LOS C	0.1	0.1	0.58	0.58
All Pedestrians		105	32.0	LOS D			0.72	0.72

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

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Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuHiPMF1]

Burnley Street/Highett Street
PM Peak Future Conditions Run 1
Signals - Fixed Time Isolated Cycle Time = 120 seconds (User-Given Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	258	5.0	0.182	8.8	LOS A	3.6	26.1	0.26	0.65	51.0
2	T1	565	5.0	0.829	29.2	LOS C	26.5	193.4	0.78	0.76	40.5
Approach		823	5.0	0.829	22.8	LOS C	26.5	193.4	0.62	0.73	43.3
North: Burnley Street (N)											
8	T1	517	5.0	0.687	10.7	LOS B	14.3	104.3	0.52	0.46	51.0
9	R2	192	5.0	0.443	20.9	LOS C	4.6	33.6	0.78	0.78	43.2
Approach		708	5.0	0.687	13.4	LOS B	14.3	104.3	0.59	0.55	48.6
West: Highett Street (W)											
10	L2	128	5.0	0.820	55.5	LOS E	22.7	165.4	1.00	0.91	30.9
12	R2	252	5.0	0.820	55.5	LOS E	22.7	165.4	1.00	0.91	30.8
Approach		380	5.0	0.820	55.5	LOS E	22.7	165.4	1.00	0.91	30.8
All Vehicles		1912	5.0	0.829	25.8	LOS C	26.5	193.4	0.68	0.70	41.6

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Average Back of Queue Distance m	Prop. Queued	Effective Stop Rate per ped
P3	North Full Crossing	53	39.3	LOS D	0.1	0.1	0.81	0.81
P4	West Full Crossing	53	18.2	LOS B	0.1	0.1	0.55	0.55
All Pedestrians		105	28.7	LOS C			0.68	0.68

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

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Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuViAMF1]

Burnley Street/Victoria Street/Walmer Street
AM Peak Future Conditions Run 1
Signals - Fixed Time Isolated Cycle Time = 120 seconds (User-Given Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	275	5.0	0.408	34.5	LOS C	11.9	86.6	0.78	0.79	37.6
2	T1	4	5.0	0.408	28.9	LOS C	11.9	86.6	0.78	0.79	38.4
3	R2	295	5.0	0.619	60.3	LOS E	8.5	61.8	0.99	0.81	29.8
3u	U	1	5.0	0.619	61.4	LOS E	8.4	61.7	0.99	0.81	29.6
Approach		575	5.0	0.619	47.7	LOS D	11.9	86.6	0.89	0.80	33.1
East: Victoria Street (E)											
4	L2	503	5.0	0.418	9.1	LOS A	8.3	60.8	0.38	0.68	51.4
5	T1	954	5.0	0.884	48.8	LOS D	37.9	276.9	0.94	0.98	33.3
6	R2	1	5.0	0.884	54.3	LOS D	37.9	276.9	1.00	1.02	32.7
6u	U	15	5.0	0.884	55.5	LOS E	37.9	276.9	1.00	1.02	32.6
Approach		1473	5.0	0.884	35.3	LOS D	37.9	276.9	0.75	0.87	37.9
North: Walmer Street (N)											
7	L2	1	5.0	0.077	64.2	LOS E	0.5	3.9	0.96	0.66	29.7
8	T1	6	5.0	0.077	58.6	LOS E	0.5	3.9	0.96	0.66	30.2
9	R2	2	5.0	0.077	64.2	LOS E	0.5	3.9	0.96	0.66	29.7
Approach		9	5.0	0.077	60.5	LOS E	0.5	3.9	0.96	0.66	30.0
West: Victoria Street (W)											
10	L2	2	5.0	0.164	14.2	LOS B	4.5	32.9	0.42	0.36	51.1
11	T1	401	5.0	0.164	8.6	LOS A	4.5	32.9	0.42	0.36	52.6
12	R2	254	5.0	0.860	61.5	LOS E	15.5	112.8	0.95	0.94	29.5
Approach		657	5.0	0.860	29.1	LOS C	15.5	112.8	0.62	0.58	40.4
All Vehicles		2714	5.0	0.884	36.5	LOS D	37.9	276.9	0.75	0.79	37.3

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Average Back of Queue Distance m	Prop. Queued	Effective Stop Rate per ped
P1	South Full Crossing	53	34.6	LOS D	0.1	0.1	0.76	0.76
P21	East Stage 1	53	15.0	LOS B	0.1	0.1	0.50	0.50
P22	East Stage 2	53	54.3	LOS E	0.2	0.2	0.95	0.95
P3	North Full Crossing	53	9.2	LOS A	0.1	0.1	0.39	0.39
P4	West Full Crossing	53	54.3	LOS E	0.2	0.2	0.95	0.95
All Pedestrians		263	33.5	LOS D			0.71	0.71

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

Attachment 4 - Traffic Report Ratio Amendment C223

MOVEMENT SUMMARY

Site: 101 [BuViPMF1]

Burnley Street/Victoria Street/Walmer Street
PM Peak Future Conditions Run 1
Signals - Fixed Time Isolated Cycle Time = 120 seconds (User-Given Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Burnley Street (S)											
1	L2	231	5.0	0.241	20.9	LOS C	7.0	51.3	0.56	0.73	43.7
2	T1	1	5.0	0.241	15.3	LOS B	7.0	51.3	0.56	0.73	44.7
3	R2	556	5.0	0.872	63.2	LOS E	17.4	126.9	0.96	0.95	29.1
3u	U	1	5.0	0.872	64.4	LOS E	17.4	126.9	0.96	0.95	28.9
Approach		788	5.0	0.872	50.8	LOS D	17.4	126.9	0.85	0.89	32.3
East: Victoria Street (E)											
4	L2	413	5.0	0.364	8.3	LOS A	5.6	41.1	0.32	0.66	52.0
5	T1	465	5.0	0.896	57.0	LOS E	25.7	187.6	0.97	0.98	31.0
6	R2	1	5.0	0.896	67.0	LOS E	25.7	187.6	1.00	1.05	29.3
6u	U	15	5.0	0.896	68.2	LOS E	25.7	187.6	1.00	1.05	29.3
Approach		894	5.0	0.896	34.7	LOS C	25.7	187.6	0.67	0.83	38.1
North: Walmer Street (N)											
7	L2	1	5.0	0.044	63.8	LOS E	0.3	2.2	0.96	0.64	29.1
8	T1	1	5.0	0.044	58.2	LOS E	0.3	2.2	0.96	0.64	29.6
9	R2	3	5.0	0.044	63.9	LOS E	0.3	2.2	0.96	0.64	29.1
Approach		5	5.0	0.044	62.7	LOS E	0.3	2.2	0.96	0.64	29.2
West: Victoria Street (W)											
10	L2	2	5.0	0.497	23.3	LOS C	18.4	134.5	0.67	0.60	45.4
11	T1	889	5.0	0.497	17.0	LOS B	18.4	134.5	0.64	0.57	46.9
12	R2	234	5.0	0.883	63.4	LOS E	14.4	105.0	0.90	0.96	29.1
Approach		1125	5.0	0.883	26.6	LOS C	18.4	134.5	0.69	0.65	41.6
All Vehicles		2813	5.0	0.896	36.0	LOS D	25.7	187.6	0.73	0.77	37.4

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Average Back of Queue Distance m	Prop. Queued	Effective Stop Rate per ped
P1	South Full Crossing	53	47.8	LOS E	0.2	0.2	0.89	0.89
P21	East Stage 1	53	8.1	LOS A	0.1	0.1	0.37	0.37
P22	East Stage 2	53	54.3	LOS E	0.2	0.2	0.95	0.95
P3	North Full Crossing	53	14.5	LOS B	0.1	0.1	0.49	0.49
P4	West Full Crossing	53	47.8	LOS E	0.2	0.2	0.89	0.89
All Pedestrians		263	34.5	LOS D			0.72	0.72

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

Attachment 4 - Traffic Report Ratio Amendment C223

Appendix C Parking Survey Results

Attachment 4 - Traffic Report Ratio Amendment C223

Car Park Occupancy Survey Results - Thursday 14 April 2016

BVY TRAFFIC SURVEY
Quality data services proven since 2000

QUALITY ENDORSED COMPANY BY AS/NZS ISO 9001:2008
OH&S SYSTEM CERTIFIED TO AS/NZS ISO 4801:2001

Parking Occupancy Survey

Date:	Thursday, 14 April 2016
Location:	Burnley St and Appleton St
Weather:	Fine
Customer	Ratio

Map Ref	Street	Section	Side	Restriction	Clear Way	Capacity	Parking Occupancy									
							11:00	12:00	13:00	14:00	15:00	16:00	17:00	18:00	19:00	20:00
A	Burnley St	From Victoria Garden Sc To Doonside St	W	Unrestricted	No Stopping 7:30am-9:30am; 4:30pm-6:30pm Mon-Fri	4	4	3	3	3	3	2	0	0	1	2
				2P 7am-6pm Mon-Fri		10	8	9	9	8	9	8	7	7	8	8
				No Standing, Car Share Vehicles Only		1	1	1	1	1	1	1	1	1	1	1
B			E	1P 8am-5pm		1	1	1	1	1	1	1	1	1	1	0
				Permit Zone Car Share		1	1	1	1	1	1	1	1	1	1	1
				1/4P 8am-5pm		1	0	1	1	0	0	1	0	0	1	0
				1P 8am-5pm		5	3	4	3	4	4	3	3	3	2	2
C		From Doonside St To Appleton St	W	Unrestricted		9	9	9	9	9	9	9	9	8	7	6
D			E	1P 8am-5pm Mon-Fri; 8:30am-12:30pm Sat		10	10	10	10	9	10	10	9	8	8	7
E		From Appleton St To North St	W	Unrestricted		3	3	3	3	3	3	2	2	2	1	1
		From North St To Somerset St	W	1P 8am-5pm Mon-Fri; 8:30am-12:30pm Sat		4	3	4	3	1	4	3	2	3	2	2
F		From Appleton St To North St	E	2P 7am-7pm Mon-Sat		2	1	1	1	0	1	1	1	1	0	0
G		From Somerset St To Highett St	W	No Stopping		0	0	0	0	0	0	0	0	0	0	0
H		From North St To Somerset St	E	1P 8am-5pm Mon-Fri; 8:30am-12:30pm Sat		3	2	3	3	2	3	3	2	2	1	1
		From Somerset St To Highett St	E	1P 8am-5pm Mon-Fri; 8:30am-12:30pm Sat		5	4	5	4	3	4	4	3	4	3	2
I		From Highett St To Blazey St	W	Unrestricted		2	2	2	2	2	2	2	2	2	1	1
J			E	1P 7am-7pm Mon-Sat		4	2	3	3	2	3	3	2	1	2	2
K	Doonside St	From Burnley St To David St	N	1/2P 7am-7pm Mon-Fri		3	2	1	2	2	2	1	1	1	0	0
				Unrestricted		1	1	1	1	0	1	1	1	1	0	0
				1/4P 8am-6pm Mon-Sat		2	1	1	2	2	1	1	0	0	0	0
				Unrestricted		18	18	18	18	18	18	17	15	10	6	3

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Attachment 4 - Traffic Report Ratio Amendment C223

Car Park Occupancy Survey Results - Thursday 14 April 2016

L			S	Unrestricted		23	23	23	23	23	23	22	20	15	7	4
M	Appleton St	From Burnley St To David St	N	Unrestricted		27	27	27	27	27	27	26	24	14	10	7
N			S	Permit Zone		18	9	10	10	9	9	8	10	8	6	5
				Unrestricted		5	5	5	5	4	4	4	3	2	1	1
O	North St	From Burnley St To Vaughan St	N	2P 7am-6pm Mon-Fri		12	8	8	9	9	8	8	7	8	9	10
P			S	2P 7am-6pm Mon-Fri; Pemit Zone All Other Times		1	1	1	1	1	1	1	0	1	1	0
				Permit Zone		15	13	13	12	12	10	10	12	13	14	14
Q		From Vaughan St To Clark St	N	Unrestricted		11	11	11	11	10	10	9	8	6	3	1
				Loading Zone 4:30am-6:30am		1	1	1	1	1	1	1	1	0	0	0
R			S	Unrestricted		13	13	13	13	13	13	11	9	6	4	2
S		From Clark St To River St	N	Loading Zone 7am-5pm Mon-Fri		1	1	1	1	0	0	0	0	0	0	0
				Unrestricted	No Parking 7am-5pm Mon-Fri	2	0	0	0	0	0	0	0	1	1	1
				Unrestricted		3	3	3	3	3	3	3	2	3	3	2
T			S	Unrestricted		10	9	9	9	9	9	9	6	4	3	2
U	David St	From Doonside St To End (North)	W	1P 7am-7pm		7	6	7	7	6	6	5	4	2	1	0
V			E	Unrestricted		9	9	9	9	9	9	9	8	5	3	1
W		From Doonside St To Appleton St	W	2P 7am-7pm Mon-Fri		13	13	12	13	12	10	9	7	4	2	1
X			E	2P 7am-7pm Mon-Fri		9	9	9	9	8	8	7	6	3	1	0
Y	Appleton St	From David St To Clark St	N	2P 7am-7pm Mon-Fri		2	2	2	2	1	1	2	2	2	2	2
				Unrestricted		2	2	2	2	2	2	2	2	2	2	2
Z			S	Unrestricted		1	1	1	1	1	1	1	1	1	1	1
				2P 7am-6pm Mon-Fri; Pemit Zone All Other Times		6	5	5	5	5	4	4	5	6	6	6
AA	Clark St	From North St To Appleton St	W	Unrestricted		5	5	5	5	5	5	5	4	4	3	2
BB			E	Unrestricted		4	4	4	4	4	4	3	3	2	1	2
CC	Buckingham St	From Burnley St To Davison St	N	2P 7am-6pm Mon-Fri		3	3	2	3	3	2	3	2	2	1	1
				Loading Zone 15mins		1	0	1	0	0	1	0	0	0	0	0
				2P 7am-6pm Mon-Fri		9	7	7	6	5	7	6	5	4	3	2
DD		From Burnley St To Davison St	S	Permit Zone		21	8	9	11	10	10	9	9	8	9	10
				P Disabled		1	0	0	0	0	0	0	0	0	0	0

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Attachment 4 - Traffic Report Ratio Amendment C223

Car Park Occupancy Survey Results - Thursday 14 April 2016

EE	Kent St	From Burnley St To Davison St	N	Permit Zone Mon-Fri		8	1	1	1	1	1	1	1	1	0	0
				2P 7am-6pm Mon-Fri; Permit Zone All Other Times		6	4	4	4	4	5	4	3	2	1	1
			Middle	Unrestricted		4	4	4	4	4	4	4	3	2	2	1
FF		From Burnley St To Davison St	S	P10mins 8am-9:30am; 3pm-4pm School Days		10	3	3	2	2	7	4	2	2	1	1
				P 2mins 8am-9:30am; 3pm-4pm School Days		4	0	1	1	0	3	2	1	1	1	1
GG	Somerset St	From Burnley St To Davison St	N	2P 7am-6pm Mon-Fri		6	4	3	2	2	3	2	2	2	3	2
				8P 7am-6pm Mon-Fri		10	10	10	10	10	10	10	9	7	5	4
HH			S	2P 7am-6pm Mon-Fri; Permit Zone All Other Times		9	3	4	4	3	6	5	4	4	5	6
II	Buckingham St	From Davison St To Leslie St	N	Permit Zone All Other Times	No Standing 8am-9:30am; 3pm-4:15pm School Days	3	0	0	0	0	0	0	0	0	0	0
		From Leslie St To Gardner St	N	Permit Zone		2	1	1	1	1	1	1	1	1	1	1
JJ		From Davison St To Leslie St	S	P 2mins 8am-9:30am; 3pm-4:15pm School Days		6	3	2	3	2	4	3	1	1	0	0
		From Leslie St To Gardner St	S	8P 7am-6pm Mon-Fri		7	6	6	6	6	6	6	7	4	3	2
KK	Kent St	From Davison St To Gardner St	N	P 2mins 8am-9:30am; 3pm-4pm School Days		3	2	1	2	2	3	2	1	1	1	0
				8P 7am-6pm Mon-Fri		10	10	10	9	8	10	8	6	5	6	4
				2P Disabled 7am-6pm Mon-Fri		1	0	0	0	0	0	0	0	0	0	0
			Middle	Permit Zone All Other Times	No Standing 7am-6pm Mon-Fri	6	0	0	0	0	0	0	0	0	1	2
LL			S	P 2mins 8am-9:30am; 3pm-4:15pm School Days		2	1	2	1	1	2	1	1	2	1	0
				2P 7am-6pm Mon-Fri; Permit Zone All Other Times		9	5	5	5	4	8	7	6	6	7	7
				P Disabled		1	1	1	1	1	1	1	1	1	1	1
MM	Davison St	From Kent St To Buckingham St	W	Permit Zone		8	8	8	7	7	8	6	4	3	2	2
				P Disabled		1	0	0	1	1	1	0	0	0	0	0
NN			E	2P 7am-6pm Mon-Fri; Permit Zone All Other Times		5	3	4	4	3	5	4	4	4	3	2
OO		From Somerset St To Kent St	W	2P 7am-6pm Mon-Fri; Permit Zone All Other Times		5	4	4	4	4	4	3	2	2	1	1
				Unrestricted	No Standing 8am-9:30am; 3pm-4pm Mon-Fri	3	0	0	0	0	0	0	0	0	0	0
PP			E	1/4P 8am-9:30am; 3pm-4pm Mon-Fri		4	4	4	3	3	4	4	3	2	1	0
				No Standing, Bus Stopping Less Than 15mins Excepted		1	0	0	0	0	0	0	0	0	0	0
				1/4P 8am-9:30am; 3pm-4pm Mon-Fri		3	0	0	0	0	0	0	0	0	0	0
QQ		From Buckingham St To Speed Hump	W	2P 7am-11pm Mon-Fri		10	4	5	6	6	5	4	6	7	8	9
RR			E	Loading Zone 7am-6pm Mon-Fri; Permit Zone All Other Times		2	1	1	1	1	1	1	1	1	0	0

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Attachment 4 - Traffic Report Ratio Amendment C223

Car Park Occupancy Survey Results - Thursday 14 April 2016

				Permit Zone		6	5	6	6	6	6	5	5	4	4	3
PUBLIC CAPACITY							377	377	377	377	342	369	377	338	345	345
PUBLIC OCCUPANCIES							309	315	313	292	301	289	250	180	138	106
PUBLIC VACANCIES							68	62	64	85	41	80	127	158	207	239
PUBLIC % OCCUPANCIES							82%	84%	83%	77%	88%	78%	66%	53%	40%	31%

not available for public parking

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Attachment 4 - Traffic Report Ratio Amendment C223

Car Park Occupancy Survey Results - Saturday 16 April 2016

BVY TRAFFIC SURVEY
Quality data services proven since 2000

QUALITY ENDORSED COMPANY BY AS/NZS ISO 9001:2008
OH&S SYSTEM CERTIFIED TO AS/NZS ISO 4801:2001

Parking Occupancy Survey

Date:	Saturday, 16 April 2016
Location:	Burnley St and Appleton St
Weather:	Fine
Customer	Ratio

Map Ref	Street	Section	Side	Restriction	Clear Way	Capacity	Parking Occupancy									
							11:00	12:00	13:00	14:00	15:00	16:00	17:00	18:00	19:00	20:00
A	Burnley St	From Victoria Garden Sc To Doonside St	W	Unrestricted	No Stopping 7:30am-9:30am; 4:30pm-6:30pm Mon-Fri	4	4	4	4	3	3	4	3	2	3	3
				2P 7am-6pm Mon-Fri		10	8	8	9	9	9	9	8	7	8	7
				No Standing, Car Share Vehicles Only		1	1	1	1	1	1	1	1	1	1	1
B			E	1P 8am-5pm		1	1	1	1	1	1	1	1	1	1	0
				Permit Zone Car Share		1	1	1	1	1	0	0	0	0	0	0
				1/4P 8am-5pm		1	0	1	1	0	0	0	1	1	0	0
				1P 8am-5pm		5	3	4	3	4	4	3	3	2	2	1
C		From Doonside St To Appleton St	W	Unrestricted		9	6	7	8	8	6	5	5	4	4	3
D			E	1P 8am-5pm Mon-Fri; 8:30am-12:30pm Sat		10	6	7	8	8	9	9	9	8	7	6
E		From Appleton St To North St	W	Unrestricted		3	3	3	3	3	2	2	2	3	3	2
		From North St To Somerset St	W	1P 8am-5pm Mon-Fri; 8:30am-12:30pm Sat		4	1	2	3	2	1	0	1	2	3	3
F		From Appleton St To North St	E	2P 7am-7pm Mon-Sat		2	0	1	1	1	0	0	0	1	1	1
G		From Somerset St To Highett St	W	No Stopping		0	0	0	0	0	0	0	0	0	0	0
H		From North St To Somerset St	E	1P 8am-5pm Mon-Fri; 8:30am-12:30pm Sat		3	3	3	3	2	2	0	1	2	1	1
		From Somerset St To Highett St	E	1P 8am-5pm Mon-Fri; 8:30am-12:30pm Sat		5	4	5	4	4	3	2	3	4	3	2
I		From Highett St To Blazey St	W	Unrestricted		2	1	1	2	2	2	1	1	1	1	1
J			E	1P 7am-7pm Mon-Sat		4	1	2	3	2	2	0	1	2	2	1
K	Doonside St	From Burnley St To David St	N	1/2P 7am-7pm Mon-Fri		3	3	3	2	2	2	2	1	1	0	0
				Unrestricted		1	1	1	1	1	1	1	1	0	0	0
				1/4P 8am-6pm Mon-Sat		2	0	1	2	2	1	0	0	0	0	0
				Unrestricted		18	17	18	18	18	18	16	14	8	6	4

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Attachment 4 - Traffic Report Ratio Amendment C223

Car Park Occupancy Survey Results - Saturday 16 April 2016

L			S	Unrestricted		23	22	22	21	20	18	14	12	8	5	3
M	Appleton St	From Burnley St To David St	N	Unrestricted		27	15	15	15	14	13	12	10	8	6	5
N			S	Permit Zone		18	4	6	7	6	5	4	5	5	4	4
				Unrestricted		5	1	1	1	1	1	1	1	1	0	0
O	North St	From Burnley St To Vaughan St	N	2P 7am-6pm Mon-Fri		12	12	11	12	10	11	11	10	10	11	11
P			S	2P 7am-6pm Mon-Fri; Pemit Zone All Other Times		1	1	1	1	1	1	1	1	0	1	1
				Permit Zone		15	15	15	14	13	14	14	13	12	14	14
Q		From Vaughan St To Clark St	N	Unrestricted		11	6	6	6	6	5	4	3	2	1	0
				Loading Zone 4:30am-6:30am		1	1	1	1	1	1	0	0	0	0	0
R			S	Unrestricted		13	6	7	8	8	7	6	6	4	2	1
S		From Clark St To River St	N	Loading Zone 7am-5pm Mon-Fri		1	1	1	1	0	0	0	0	0	0	0
				Unrestricted	No Parking 7am-5pm Mon-Fri	2	1	1	2	2	2	2	1	1	2	2
				Unrestricted		3	3	3	3	3	2	2	2	2	3	3
T			S	Unrestricted		10	7	7	7	7	6	6	5	4	4	3
U	David St	From Doonside St To End (North)	W	1P 7am-7pm		7	7	7	7	6	6	6	4	2	1	0
V			E	Unrestricted		9	7	7	7	6	5	4	3	2	1	1
W		From Doonside St To Appleton St	W	2P 7am-7pm Mon-Fri		13	11	10	12	12	11	10	8	4	2	1
X			E	2P 7am-7pm Mon-Fri		9	8	9	9	8	8	7	5	3	2	1
Y	Appleton St	From David St To Clark St	N	2P 7am-7pm Mon-Fri		2	2	2	2	1	2	2	1	1	2	2
				Unrestricted		2	1	1	2	2	2	2	2	2	2	2
Z			S	Unrestricted		1	1	1	1	1	0	0	1	1	1	1
				2P 7am-6pm Mon-Fri; Pemit Zone All Other Times		6	4	5	5	5	4	4	3	4	5	6
AA	Clark St	From North St To Appleton St	W	Unrestricted		5	3	4	4	4	3	3	2	2	1	1
BB			E	Unrestricted		4	2	3	3	2	1	0	0	0	1	1
CC	Buckingham St	From Burnley St To Davison St	N	2P 7am-6pm Mon-Fri		3	1	2	2	2	3	3	2	2	1	1
				Loading Zone 15mins		1	0	0	0	0	0	0	0	0	0	0
				2P 7am-6pm Mon-Fri		9	7	7	6	5	5	4	4	3	2	2
DD		From Burnley St To Davison St	S	Permit Zone		21	11	10	10	11	10	9	8	9	10	11
				P Disabled		1	0	0	0	0	0	0	0	0	0	0

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Attachment 4 - Traffic Report Ratio Amendment C223

Car Park Occupancy Survey Results - Saturday 16 April 2016

EE	Kent St	From Burnley St To Davison St	N	Permit Zone Mon-Fri		8	0	0	0	0	0	0	0	0	0	0
				2P 7am-6pm Mon-Fri; Permit Zone All Other Times		6	1	1	1	1	0	0	0	0	1	1
			Middle	Unrestricted		4	3	3	2	2	1	1	1	1	0	0
FF		From Burnley St To Davison St	S	P10mins 8am-9:30am; 3pm-4pm School Days		10	7	6	5	4	3	2	2	1	1	1
				P 2mins 8am-9:30am; 3pm-4pm School Days		4	3	3	2	2	1	1	1	2	1	0
GG	Somerset St	From Burnley St To Davison St	N	2P 7am-6pm Mon-Fri		6	4	3	2	2	3	3	2	3	3	2
				8P 7am-6pm Mon-Fri		10	6	6	6	5	4	3	4	5	5	4
HH			S	2P 7am-6pm Mon-Fri; Permit Zone All Other Times		9	6	6	5	6	7	8	6	5	7	7
II	Buckingham St	From Davison St To Leslie St	N	Permit Zone All Other Times	No Standing 8am-9:30am; 3pm-4:15pm School Days	3	1	1	1	2	2	2	1	1	1	0
		From Leslie St To Gardner St	N	Permit Zone		2	2	2	2	2	2	2	2	2	2	2
JJ		From Davison St To Leslie St	S	P 2mins 8am-9:30am; 3pm-4:15pm School Days		6	5	4	3	2	3	3	2	2	1	0
		From Leslie St To Gardner St	S	8P 7am-6pm Mon-Fri		7	7	7	7	6	6	5	5	4	3	2
KK	Kent St	From Davison St To Gardner St	N	P 2mins 8am-9:30am; 3pm-4pm School Days		3	1	1	2	2	2	2	1	1	0	0
				8P 7am-6pm Mon-Fri		10	6	7	8	8	9	9	8	7	6	5
				2P Disabled 7am-6pm Mon-Fri		1	0	0	0	0	0	0	0	0	0	0
			Middle	Permit Zone All Other Times	No Standing 7am-6pm Mon-Fri	6	0	0	1	2	3	3	2	2	1	1
LL			S	P 2mins 8am-9:30am; 3pm-4:15pm School Days		2	2	2	1	1	2	2	1	2	1	1
				2P 7am-6pm Mon-Fri; Permit Zone All Other Times		9	7	7	8	8	7	6	6	5	6	7
				P Disabled		1	0	0	0	0	0	0	0	0	0	0
MM	Davison St	From Kent St To Buckingham St	W	Permit Zone		8	0	1	2	2	1	1	1	2	2	2
				P Disabled		1	0	0	0	0	0	0	0	0	0	0
NN			E	2P 7am-6pm Mon-Fri; Permit Zone All Other Times		5	2	3	3	3	2	2	2	3	3	2
OO		From Somerset St To Kent St	W	2P 7am-6pm Mon-Fri; Permit Zone All Other Times		5	3	3	2	2	1	1	1	1	0	0
				Unrestricted	No Standing 8am-9:30am; 3pm-4pm Mon-Fri	3	1	1	0	0	1	1	0	0	0	0
PP			E	1/4P 8am-9:30am; 3pm-4pm Mon-Fri		4	0	0	1	1	0	1	1	0	0	0
				No Standing, Bus Stopping Less Than 15mins Excepted		1	0	0	0	0	1	1	0	0	0	0
				1/4P 8am-9:30am; 3pm-4pm Mon-Fri		3	0	0	0	0	0	0	0	0	0	0
QQ		From Buckingham St To Speed Hump	W	2P 7am-11pm Mon-Fri		10	9	8	9	8	9	9	8	7	8	9
RR			E	Loading Zone 7am-6pm Mon-Fri; Permit Zone All Other Times		2	1	1	1	1	1	1	1	1	1	1

Ratio Consultants

Attachment 4 - Traffic Report Ratio Amendment C223

Car Park Occupancy Survey Results - Saturday 16 April 2016

				Permit Zone		6	3	4	5	5	4	4	4	4	4	4	4
PUBLIC CAPACITY							350	350	350	350	350	350	351	353	353	353	
PUBLIC OCCUPANCIES							241	249	253	234	221	196	173	146	124	100	
PUBLIC VACANCIES							109	101	97	116	129	154	178	207	229	253	
PUBLIC % OCCUPANCIES							69%	71%	72%	67%	63%	56%	49%	41%	35%	28%	

not available for public parking

Ratio Consultants

Planning and Environment Act 1987

YARRA PLANNING SCHEME

AMENDMENT C223

EXPLANATORY REPORT

Who is the planning authority?

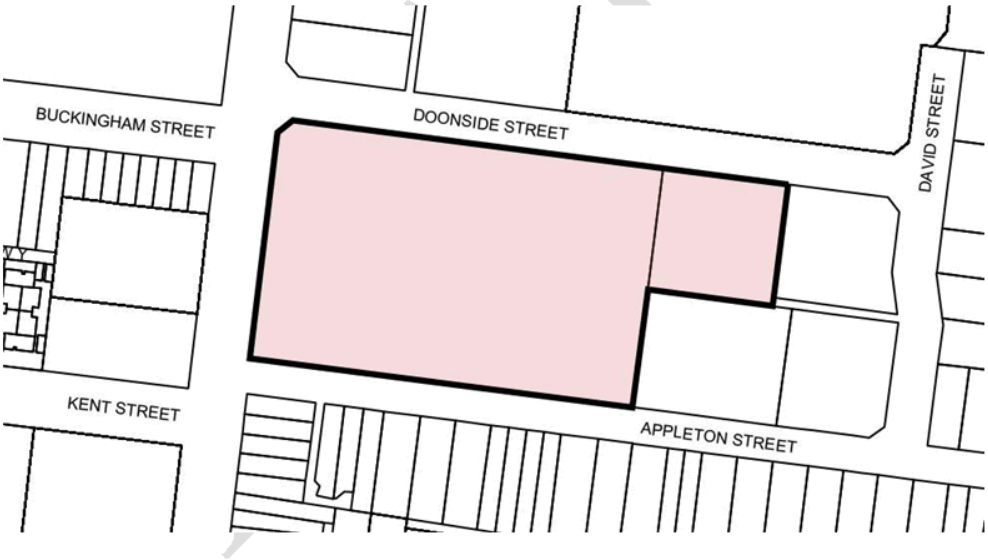
This amendment has been prepared by Yarra City Council, which is the planning authority for this amendment.

The Amendment has been made at the request of Tract Consultants, on behalf of the owner of the land, Astrodome Hire Pty Ltd.

Land affected by the Amendment

The Amendment applies to the land known as 81-95 Burnley Street and 26 Doonside Street, Richmond, shown below.

Properties affected by the Amendment (shaded below):



What the amendment does

The Amendment makes the following changes to the Yarra Planning Scheme:

- Rezone the site from Industrial 3 Zone (IN3Z) to Mixed Use Zone (MUZ).
- Apply the Development Plan Overlay Schedule 15 (DPO15) to the site.
- Apply an Environmental Audit Overlay (EAO) to the site.

Strategic assessment of the Amendment

Why is the Amendment required?

The Planning Scheme Amendment will allow an application to be made for, and give effect to, the strategic direction identified for the site by the Yarra Planning Scheme and its reference documents. This Planning Scheme Amendment provides opportunities for economic and employment growth and infill housing opportunities within a Major Activity Centre.

The Amendment will assist in the identified transition of the site from industrial to commercial and residential, in line with the emerging direction of the wider Doonside Precinct and Victoria Street/Gardens Activity Centre as a 'substantial change' area.

The Victoria Street East Urban Design Framework, adopted by Council on 16 November 2005, recommends that this precinct be rezoned from an Industrial 3 Zone to a Mixed Use Zone to create a transitional area between Victoria Gardens and the residential development on the south side of Appleton Street. Further, the Victoria Street Structure Plan, adopted in April 2010, has identified the location for 'larger scale office and residential' as well as 'potential housing mixed with retail and businesses'.

The Development Plan Overlay 15 provides for site specific planning controls for the land that includes design objectives to achieve an appropriate built form outcome that transitions from approved higher density buildings to the north and east, and lower detached single dwellings to the south. It also provides for a Section 173 Agreement to be entered into to provide 5% of the total number of dwellings for affordable housing.

The EAO will address any site contamination issues prior to a sensitive use (such as residential) commencing on the site, and to meet the requirements of Ministerial Direction No. 1.

How does the Amendment implement the objectives of planning in Victoria?

This Planning Scheme Amendment implements a number of objectives of planning in Victoria under Section 4 of the Planning and Environment Act, in particular:

- To provide for the fair, orderly, economic and sustainable use, and Development of land;
- To secure a pleasant, efficient and safe working, living and recreational Environment for all Victorians and visitors to Victoria.
- To facilitate development in accordance with the other objectives; and
- To balance the present and future interests of all Victorians.

How does the Amendment address any environmental, social and economic effects?

The amendment adequately addresses the environmental effects of a rezoning that will allow for sensitive uses by the application of an Environmental Audit Overlay. Further, the amendment proposes to implement new public open space incorporating a significant on site tree.

The amendment will have positive economic and social effects through rezoning an underutilised industrial site and encouraging a mix of uses within an identified Major Activity Centre with good access to public transport and strong prospects for job creation. It also provides 5% of the total number of dwellings for affordable housing.

Does the Amendment address relevant bushfire risk?

There is no relevant bushfire risk associated with this Planning Scheme Amendment. No views or comments were sought from the relevant fire authority through this process.

DRAFT

Does the Amendment comply with the requirements of any Minister's Direction applicable to the amendment?

The amendment is consistent with the *Ministerial Direction on the Form and Content of Planning Schemes* under section 7(5) of the Act.

The amendment is affected by *Ministerial Direction No 11 Strategic Assessment of Amendments* and complies with the requirements.

The amendment is consistent with *Ministerial Direction No 1 – Potentially contaminated Land.* The application of the Environmental Audit Overlay ensures that potentially contaminated land issues are addressed before development occurs.

How does the Amendment support or implement the State Planning Policy Framework and any adopted State policy?

The main provisions of the SPPF that are relevant to this amendment are:

Clause 9 – Plan Melbourne

The proposal will concentrate new development within an existing activity centre, with good provision of public infrastructure and in an area able to accommodate change.

Clause 11 - Settlement

The proposal will respond to the need to supply housing, employment, open space, commercial and community facilities within a designated substantial change area.

Clause 15 - Built Environment and Heritage

The proposal will enable a mix of uses, including residential, and will retain and restore existing heritage buildings.

Clause 16 - Housing

The proposal is located within an activity centre, offers good access to services and transport, and is identified as a strategic development site for large scale residential development in Metropolitan Melbourne.

Clause 17 - Economic Development

The proposal accords with the objective to facilitate the efficient use of existing urban infrastructure while providing for a net increase in employment on site.

How does the Amendment support or implement the Local Planning Policy Framework, and specifically the Municipal Strategic Statement?

The amendment gives effect to several objectives under Clause 21 and 22 of the Yarra Planning Scheme including:

- Clause 21.03 (Vision) specifically identifies the site as a strategic redevelopment site.
- Clause 21.08 (Neighbourhoods) specifically identifies the site as suitable for rezoning to a Mixed Use Zone (MUZ).
- Clause 22.11 (Victoria East Precinct Policy) specifically identifies the site as a future mixed use land use.

Does the Amendment make proper use of the Victoria Planning Provisions?

The amendment makes proper use of the VPP by utilising a zone (MUZ) that encourages uses deemed as appropriate for the site, and manages development through the application of an existing DDO that was originally intended to apply to the site. It uses the EAO to manage potentially contaminated land.

DRAFT

How does the Amendment address the views of any relevant agency?

Known directions of relevant agencies have been incorporated into the proposal.

Does the Amendment address relevant requirements of the Transport Integration Act 2010?

The amendment is considered consistent with the objectives of the Transport Integration Act 2010. It is not expected that the proposed amendment will have a discernible impact on the existing road network, nor will it compromise the safety or operation of the existing transport system.

Resource and administrative costs

- **What impact will the new planning provisions have on the resource and administrative costs of the responsible authority?**

In the long term, the new planning provisions will have minimal impact on the resource and administrative costs of the responsible authority. However, to the extent that the Amendment will encourage new residential, commercial and retail development, it is anticipated that in the short term there will be an increase in applications for planning permits.

Where you may inspect this Amendment

The Amendment is available for public inspection, free of charge, during office hours at the following places:

City of Yarra Customer Service Centres:

City of Yarra
Richmond Town Hall
Town Planning Counter
333 Bridge Road, Richmond 3121

City of Yarra website:
https://www.yarracity.vic.gov.au/the-area/yarras-future/yarra-planning-scheme

The Amendment can also be inspected free of charge at the Department of Environment, Land, Water and Planning website at www.delwp.vic.gov.au/public-inspection.

DRAFT

Attachment 6 - Appleton Street, Burnley Street and Doonside Render Am C223

View from Appleton Street

Attachment 6 - Appleton Street, Burnley Street and Doonside Render Am C223

View from Burnley Street

Attachment 6 - Appleton Street, Burnley Street and Doonside Render Am C223

View from Doonside Street

11.6 Yarra Planning Scheme Amendment C225 - 351 Church Street Richmond - residential aged care facility - height limit exemption

Executive Summary

Purpose

The purpose of this report is for Council to consider seeking authorisation to exhibit Amendment C225 in accordance with the *Planning and Environment Act 1987,* which seeks to use Clause 52.03 of the Yarra Planning Scheme to introduce an Incorporated Document to a particular development as shown in the plans specified in the Incorporated Document. The Incorporated Document would permit a 5 storey purpose-built residential aged care facility.

Amendment C225 would create an exemption from the mandatory 9m height control in the Neighbourhood Residential Zone which applies to the land.

Key Issues

An amendment request has been submitted by Mecwacare seeking an exemption from the mandatory 9 metre height limit in the Neighbourhood Residential Zone. The proposal involves a five storey building, with a total height of up to 23 metres, which significantly exceeds the 9 metre limit in the zone.

The strategic justification for the amendment is based on community benefit from the proposed residential aged care facility. The proponent submits there is a significant shortfall in the supply of residential aged care and that this is likely to increase in the foreseeable future. The proposal includes subsidised and specialised care for local communities, in particular people from disadvantaged and lower socio-economic backgrounds and from within at risk or high need cohorts.

The proposal has been assessed and significantly modified to reduce the likely impacts on the adjoining area and changes to neighbourhood character and heritage significance.

Expert advice to Council indicates the urban design and heritage implications are acceptable. The community benefit from subsidised residential aged care provides a solid basis for seeking a community response to the proposed exemption and non-compliance with the specific heritage related sight-line requirements in Clause 22.02 of the Yarra Planning Scheme.

Financial Implications

There are no significant financial implications. The proponent would meet the costs of the Amendment.

PROPOSAL

To prepare Amendment C225 to the Yarra Planning Scheme, seek Authorisation from the Minister for Planning and exhibit the Amendment for public consultation.

11.6 Yarra Planning Scheme Amendment C225 - 351 Church Street Richmond - residential aged care facility - height limit exemption

Trim Record Number: D17/196639
Responsible Officer: Director Planning and Place Making

Purpose

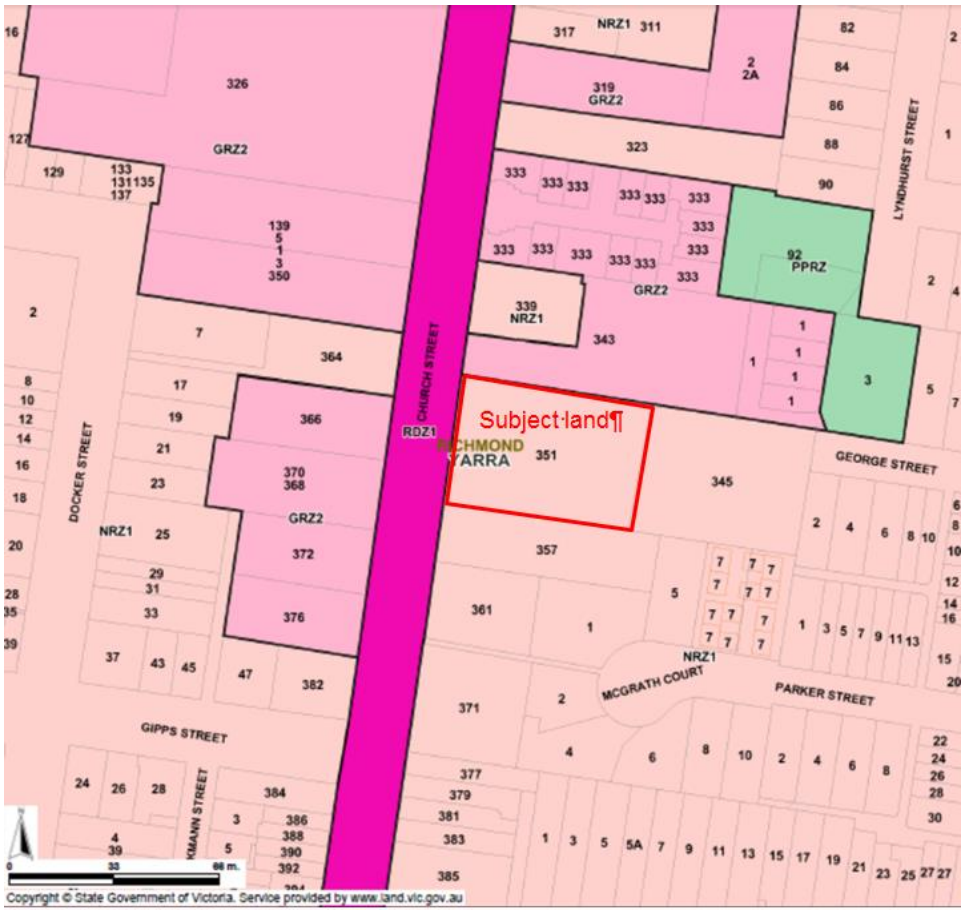
1. The purpose of this report is for Council to consider seeking authorisation to exhibit Amendment C225 in accordance with the *Planning and Environment Act 1987,* which seeks to use Clause 52.03 of the Yarra Planning Scheme to introduce an Incorporated Document to a particular development as shown in the plans specified in the Incorporated Document. The Incorporated Document would permit a 5 storey purpose-built residential aged care facility.
2. Amendment C225 would create an exemption from the mandatory 9m height control in the Neighbourhood Residential Zone which applies to the land.

Background

Amendment Request

3. Council received an amendment request from Urbis Pty Ltd on behalf of Mecwacare, who is a not-for-profit aged care provider that currently operates the 'Rositano House' facility at 273 Church Street, Richmond. Rositano House has a long waiting list of people requesting the support of its services. The residents of Rositano are from a long term "old Richmond", or CALD or GLBTIQ background. All are from backgrounds of disadvantage and, or have significant health issues. Most residents are in transition through the palliative care phase of life.
4. On 29 April 2016, the Australian Government Department of Health granted Mecwacare the provisional allocation of 100 places to provide residential aged care services at 351-353 Church Street, Richmond.
5. The land is on the east side of Church Street, Richmond, just south of St Ignatius church which is on the west side of Church Street. The location and planning scheme zones are shown on the plan below. The area includes a mix of 19th Century detached and terrace housing, more recent multi-dwelling development and some institutional and specialised accommodation – 357 and 345 Church Street are both short term accommodation. The land is currently occupied by a residential hotel.
6. Building heights in the immediate area range from single and 2 storey to a 7 storey building at the rear of the subject land (345 Church Street).
7. The amendment would create an exemption from the mandatory 9m height control in the Neighbourhood Residential Zone which applies to the land. This is done using Clause 52.03 of the Yarra Planning Scheme. The Amendment would introduce a proposed Incorporated Document (see Attachment 1), which would allow a particular development as shown in the architectural plans specified in the Incorporated Document (see Attachment 2).

Location and Planning Scheme zones



8. The Incorporated Document would operate like a planning permit, allowing the proposed development subject to a range of conditions. If the purpose-built multi-level residential aged care facility, as set out in the Incorporated Document, is not constructed then the underlying zoning controls would relate to any other development.

9. The land is in Heritage Overlay - HO315 - 'Church Street Precinct, Richmond'. A pair of individually significant terrace houses is located on the subject land close to the Church Street frontage and would be retained as part of the proposed redevelopment. The 2 terraces were consolidated in 1917 to provide accommodation for the YWCA.

10. The initial proposal residential aged care facility included:

    (a) 84 resident rooms;

    (b) 25 basement car-parks;

    (c) lower ground residential and staff amenity;

    (d) resident communal spaces including living/dining/activity spaces;

    (e) landscaped resident outdoor areas, roof terrace and garden areas; and

    (f) five storeys including the ground floor at the rear of the existing heritage buildings.

11. Council officers' initial assessment considered building bulk, height, off-site and internal amenity impacts and heritage. Expert advice was sought in relation to urban design and heritage impacts. This resulted in two sets of revisions between December 2016 and November 2017. The revisions resulted in:

    (a) an overall reduction in the scale of the residential aged care facility to 79 rooms;

    (b) greater upper level set-backs; and

    (c) changes to the design and appearance of the building.

12. The proposed overall building height has not changed significantly through the revisions. The height is 5 storeys including the ground floor, plus roof top plant facilities. The existing two storey heritage terraces on site would remain. The height of the new building at the rear of the heritage buildings would range from 18.3m near the existing terraces to 20.3m plus 2.6m for the plant room on the roof at the rear. The height above ground level would increase towards the rear eastern end of the land because the land slopes down from Church Street to the rear boundary.

13. The applicant outlines the strategic justification for the Amendment is based on the demand and community benefit from the aged care services. This is consistent with recent VCAT decision (Brotherhood of St Laurence v Yarra CC [2016] VCAT 1648), which used the concept of community benefit to justify allowing a proposal which would normally not meet some Planning Scheme requirements or policies. Further discussion is outlined in the **Heritage** and **Community Benefit** sections below.

Assessment of the proposal

Heritage

14. The Amendment application included a 'Heritage Appraisal: Richmond Hill Hotel, 351-353 Church Street, Richmond' (October 2016) and subsequently a Heritage Impact Statement entitled 'Richmond Hill Hotel' prepared by Lovell Chen, Architects & Heritage Consultants (dated May 2017) (HIS). The HIS was requested by Council officers after a preliminary assessment of the application in December 2016.

15. Clause 22.02-5.7.1 of the Planning Scheme promotes the delivery of new development that is respectful of the prevailing characteristics and valued attributes of heritage contexts. New works should be readily understood as such, and should not dominate or overwhelm places of individual significance or contributory value.

16. Assessment by Lovell Chen noted that the proposed aged care facility satisfies the expectations of the heritage policy guidelines to the extent that the:

    (a) new works are distinguishable from the original historic fabric;

    (b) proposal will not remove or damage original historic fabric; and

    (c) principle façade of the 1850s terrace will not be obscured.

    In addition, the architectural treatment of the visible sections of the west elevation, rising above the retained terrace, is well resolved, responding in an interpretive manner to the balance of solids to voids in the west elevation of the historic building. The windows openings in the proposed development, while larger than the 1850s building – to meet contemporary needs and requirements – nonetheless reference the traditional rectangular windows seen in the historic building through the presence of vertical glazing bars, to break down the extent of glazing. The proposal, however, does not align with the heritage policy guidelines in terms of its visibility and massing.

17. The extent of the non-compliance with Clause 22.02 is shown on plan TP20 in Attachment 2. These show the upper levels of the new building are significantly above the sight-line tests in Clause 22.02-5.7.1. These sight lines are shown on the proposal plans. The impact of this non-compliance is off-set by the distance from the Church Street frontage which is 33.5 metres. Expert advice to Council from GJM Heritage consultants generally accepts that the non-compliance is acceptable in the context of the large set-back and the overall design of the proposal.

18. The proposal has been assessed by GJM Heritage (Council consultants) who advised that the final revised proposal is acceptable from a heritage perspective, as the:

    (a) two heritage significant terrace houses on the subject land are retained;

    (b) the massing of the new rear development is generally acceptable given the setback of the taller form from the terraces and highly variable built surroundings; and

    (c) façade detailing and proposed matt finish colours reduce the visual prominence of the new built form.

19. A higher than normal palisade fence is proposed for security and safety reasons associated with the aged care use of the site.

20. The proposal challenges the expectations of the Planning Scheme. It would be a visible addition to an 'individually significant' heritage place. As noted by Lovell Chen, however, the proposed facility has been designed to defer to the heritage building fabric, through the use of a muted colour palette, simple, contemporary and interpretive architectural form and deep upper level setbacks. Critically, from a heritage perspective, the terrace would continue to be understood as mid-nineteenth century building in the Church Street streetscape.

Urban design

21. David Lock Associates (DLA), Council consultant, has assessed the urban design aspects of the proposal. Key issues included the compatibility of the design with the urban character of this part of Church Street, amenity impacts on adjoining sites and performance of the development measured against the relevant parts of Res-Code.

Character

22. The DLA assessment of the revised proposal indicated:

> *The existing character of the context consists of a number of residential buildings of varying height. In this part of Church Street there are a number of 3-4 storey buildings, which exceed the mandatory maximum height that applies to the site (in the zone). The proposed building is 5-6 storeys high, which exceeds the prevailing height of the buildings. However, the plans show that the proposed new building is setback approximately 25.8m from the front boundary, behind the existing heritage dwelling and the existing mature trees within the front setback are proposed to be retained. As a result, the existing heritage building will remain the dominant element within the streetscape.*
>
> *Given that the proposed building is well set-back, its visual presence will not disrupt the existing character, when viewed from the street. Additionally, the retention of the tall canopy trees within the front setback will further ameliorate any additional height effects, when viewed from Church Street*.

Off-site amenity impacts

23. The DLA assessment considered a range of impacts on the adjoining properties immediately to the north and south of the subject land (343 and 357 Church Street) and to a lesser extent to the taller building to the east. These impacts relate to visual bulk, overlooking and overshadowing. The initial assessments led to a range of modifications to the design, including a sloping upper level set-back on the south side of the development, alterations to minimise overlooking and clarification of the floor layouts and use of the adjoining buildings.

24. As a result of the changes to the design DLA are satisfied that the amenity impacts are now acceptable.

Other statutory planning issues

25. The purpose and mechanisms in the Amendment work like a form of planning permit. The proposals have been assessed by internal Council officers as if it was a permit application. The issues raised with the proponent include: the design and quality of landscaping in the street front area between the existing terraces and Church Street, including an arborists report on existing trees, provision of bike parking and support facilities and the quality of the internal layout and design from an ESD perspective.

26. The Incorporated Document forms part of the Amendment. If approved it cannot be changed without a further Planning Scheme Amendment. The Incorporated Document refers to the proposed plans and includes conditions which would require some changes to the plans. The plans must be approved by Council as the Responsible Authority.

Community benefit

27. Assessment of community benefit relates to recent VCAT decisions including a recent aged care facility in Clifton Hill (Brotherhood of St Laurence v Yarra CC [2016] VCAT 1648). That decision points to the importance of how net community benefit is assessed and makes a very clear distinction between conventional 'for profit' aged care facilities and services like the subject Applicant which address special issues of disadvantage and disability.

28. The *Residential Aged Care - A Snapshot of Homes in the City of Yarra in 2015* (report prepared by Council officers in Aged and Disability Services) noted that there is a significant shortfall in the number of residential aged care beds to cater for growing demand in the City of Yarra. It noted that:

    (a) a major factor impacting on the provision of residential care in the inner city has been land prices coupled with the service model requiring 100 plus bed facilities;

    (b) City of Yarra has 304 beds which is significantly below (166 less beds) the national planning standards;

    (c) the number of beds has also decreased approximately 130 since 2007;

    (d) unless bed/facility numbers are increased – by 2025 (based on projected population) City of Yarra will be almost 50% under required operational numbers; and

    (e) there is significant demand in the Richmond area for quality residential aged care, with ageing residents of the numerous public housing units in the area in need of aged care with specialised capability in caring for those with complex needs.

29. The current proposal has been assessed in terms of:

    (a) the target population for the facility;

    (b) how it will address disability and / or disadvantage;

    (c) subsidised care for disadvantaged groups;

    (d) the mix of subsidised care to non-subsidised care; and

    (e) how this relates to the Federal funding.

30. Recent information from the State Government indicated that:

    (a) there is continuing growth in demand for residential aged care facilities;

    (b) in the inner metropolitan region meeting this demand is complicated by increasing land values and a trend to provide residential aged care facilities in taller and denser forms which reduce the relative land cost; and

    (c) the State department (DELWP) is investigating ways to make it easier for residential aged care facilities to obtain development approval, including exempting these facilities from current mandatory height restrictions.

31. Information provided by Mecwacare, which has been reviewed by Council officers in Aged and Disability Services, indicated that they have a strong focus on supporting people from disadvantaged and lower socio-economic backgrounds and from within at risk or high need cohorts, in particular:

    (a) they currently support 65% subsidised and 35% non-subsidised residents and that they will continue to offer this mix of subsidised to non-subsidised care to its clientele in Victoria; and

    (b) the beds have been allocated by the Federal Government to be used for the care of people who are from CALD, LGBTIQ, veterans and people living with dementia.

32. In general terms, the current proposal demonstrates a positive community benefit. It is consistent with the recent precedents using community benefit as a justification for allowing a development which would not otherwise comply with Planning Scheme requirements. The proposal is for a not-for-profit service which would provide subsidised and specialised care for the local community.

External Consultation

33. If Council resolves to seek '*authorisation'* for the amendment and it is approved by DELWP for formal exhibition, the amendment documentation would be exhibited for a period of one month in accordance with the requirements of the *Planning and Environment Act 1987*. This includes giving notice of the proposal and would include all owners and occupiers of land within and immediately adjoining the amendment area. Exhibition of an Amendment would also include formal notice in the Age newspaper and information on the Council website. This would provide the community an opportunity to submit on the proposed controls. Exhibition would include statutory documents including the draft Explanatory Report in Attachment 3.

34. Issues raised in submissions must be considered by Council and may be subject to review by an independent panel if Council does not initially accept the issues in submissions.

Internal Consultation (One Yarra)

35. The proposed Amendment has been circulated internally with comments from different units of Council forming part of the assessment process leading to this report.

Financial Implications

36. There are no significant financial implications. The proponent would meet the costs of the Amendment.

Economic Implications

37. The proposed residential aged care facility is part of a growing health care network in the City of Yarra. This growing sector provides important local employment and economic activity.

Sustainability Implications

38. Environmentally sustainable development requirements have been part of the assessment for this development.

Social Implications

39. The Amendment would allow provision of about 80 residential aged care rooms. There is a growing demand for these facilities across the State and in the inner region. Local facilities, such as these, allow people to age-in-place, reducing disruption for elderly people who need care and supported accommodation.

Human Rights Implications

40. There are no anticipated negative human rights implications.

Communications with CALD Communities Implications

41. This would be part of the normal Planning Scheme Amendment consultation process.

Council Plan, Strategy and Policy Implications

42. The proposal is consistent with the Council Plan which addresses aged care at:

Strategy 1.2	Promote a community that is inclusive, resilient, connected and enjoys strong mental and physical health and wellbeing.
Initiative 1.2.3	Continue to implement the National Aged and Disability Care reforms and develop new strategic directions for support of older people and people with disability.

43. The Amendment is also consistent with '*Objective 4 – A liveable Yarra – Development and growth are managed to maintain and enhance the character and heritage of the city'*. The Amendment would help respond to the pressures of population growth and demographic change which has resulted in changes in the demand for and supply of residential aged care facilities.

44. The Amendment seeks to balance the response to social need and heritage / character. The individually significant terraces are to be retained and although the upper levels will be visible the depth of the setback from the street (30+m) is such that these upper levels will read as quite separate and recessive elements. This approach will maximise the primacy of the 1850s terrace building at 351 Church Street, and its prominence in the HO precinct. The surrounding area has highly variable built form, including a number of taller buildings to the rear of lots on the east side of Church Street, such as the YWCA Richmond House (included in HO319, the 'Elm Grove Precinct') and an apartment building to the north (included in HO315) which have not been seen to detract from the values of the HO precincts.

Legal Implications

45. As Planning Authority under the Planning and Environment Act 1987 Council must follow the legal processes for a Planning Scheme amendment.

Other Issues

Working Group – review of residential zone provisions – residential aged care facilities

46. The DELWP are currently investigating possible changes to the residential zones as they relate to residential aged care facilities. This involves a working group of local government and industry representatives. The purpose of this investigation is to see if there are ways to relax restrictions including the current mandatory height limits, to encourage residential aged care facilities.

47. It seems likely that this investigation may mean mandatory height limits will not apply to residential aged care facilities. If the Minister for Planning approves changes, they could come into effect by mid-2018.

48. If the Minister approves the proposed changes Amendment C225 would become redundant.

Options

49. Council can consider the following options:

    (a) prepare Amendment C225 and seek authorisation from the Minister for Planning for the amendment to go on public exhibition; or

    (b) refuse the request for an amendment.

Agree to the request for an amendment and decide to prepare Amendment C225

50. Council could decide to prepare an amendment on the basis that the community benefit from the proposal is significant and justifies the height exemption. The assessment discussed in this report indicates that the off-site and heritage impacts from the proposal are broadly acceptable.

Refuse the amendment request

51. Council could refuse the amendment on the grounds that:

    (a) the proposal sets an inappropriate precedent for changes to mandatory height provisions in the residential zones; and

    (b) the proposed height limit exemption is not justified by the community benefit from the proposed aged care facilities.

52. This option does not address the significant shortfall in the number of residential aged care beds to cater for growing demand in the City of Yarra.

Preferred option

53. The preferred option is (a) – prepare Amendment C225 and seek authorisation from the Minister for Planning for the amendment to go on public exhibition in order to specifically seek comment from the community. The due process that would follow would then likely involve a Panel hearing which would be reported back to Council.

Conclusion

54. An amendment request has been submitted by Mecwacare seeking an exemption from the mandatory 9 metre height limit in the Neighbourhood Residential Zone. The proposal involves a five storey building, with a total height of up to 23 metres, which significantly exceeds the 9 metre limit in the zone.

55. The strategic justification for the amendment put by the applicant is based on community benefit from the proposed residential aged care facility. The proponent submits there is a significant shortfall in the supply of residential aged care and that this is likely to increase in the foreseeable future. The proposal includes subsidised and specialised care for local communities, in particular people from disadvantaged and lower socio-economic backgrounds and from within at risk or high need cohorts.

56. The proposal has been assessed and significantly modified to reduce the likely impacts on the adjoining area and changes to neighbourhood character and heritage significance. Expert advice to Council indicates the urban design and heritage implications are broadly acceptable.

57. It is also noted that the State Government has indicated a willingness, to at least entertain, relaxed height controls on residential aged care facilities. If the State Government decides to change the residential zones to relax the current mandatory height controls (as they apply to residential aged care facilities) the current amendment may become redundant. A decision on these changes is likely in mid-2018.

RECOMMENDATION

1. That Council:

   (a) note the officer report on the proposed request by Mecwacare for an amendment to the Yarra Planning Scheme in relation to the site known as 351 Church Street, Richmond;

   (b) note the contents of the officers report on Amendment C225 which seeks to use Clause 52.03 of the Yarra Planning Scheme to introduce an Incorporated Document to create an exemption from the mandatory 9m height control in the Neighbourhood Residential Zone to permit a 5 storey purpose-built residential aged care facility managed by Mecwacare being a not for profit organisation providing care for disadvantaged persons; and

   (c) seek authorisation from the Minister for Planning to prepare Amendment C225 to the Yarra Planning Scheme in accordance with section 8A of the *Planning and Environment Act 1987*.

2. If authorisation is granted by the Minister for Planning, officers prepare to exhibit Amendment C225 in accordance with Section 19 the requirements of the *Planning and Environment Act 1987.*

CONTACT OFFICER: **Peter Mollison**
TITLE: **Senior Strategic Planner**
TEL: **9205 5023**

Attachments

1 Incorporated Document 351 Church Street October 2017
2 351 Church Street Richmond - architectural plans
3 Draft Explanatory Report - Amendment C225 - 351 Church Street Richmond

351 - 353 Church Street, Richmond

Incorporated Document,

November2017

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Attachment 1 - Incorporated Document 351 Church Street October 2017

This document is an incorporated document in the Yarra Planning Scheme pursuant to Section 6 (2) (j) of the *Planning and Environment Act 1987* (Vic).

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1. INTRODUCTION

This document is an Incorporated Document in the Schedule to Clause 52.03 and the Schedule of Clause 81.01 of the Yarra Planning Scheme (**the Scheme**).

This document gives effect to specific controls for the use and development of a Residential aged care facility on the land described below (**the Proposal**) pursuant to clause 52.03 of the Scheme.

In the event of any inconsistency between the specific controls contained in this document and any provision of the Scheme, the specific controls contained in this Incorporated Document will prevail.

2. ADDRESS OF THE LAND:

The land is known as 351-353 Church Street, Richmond and described in Certificate of Title Volume 09441 Folio 758 on Plan of Subdivision 138330 (**the Land**).

3. PURPOSE:

The purpose of this Incorporated Document is to permit the use and development of the Land for the purposes of the Proposal.

4. THIS DOCUMENT ALLOWS:

Despite any provision to the contrary, or any inconsistent provision in the Scheme, no planning permit is required for, and no planning provision in the Scheme operates to prohibit, restrict or regulate the use or development of the Land for the purposes of, or related to, the use and development of the Land for the purposes of the Proposal.

The Proposal includes:

- the use of the Land for the purposes of a 'Residential aged care facility' as defined by Clause 74 of the Scheme;
- partial demolition of the existing heritage building on the Land in accordance with the plans endorsed by Council under this Incorporated Document; and
- construction of a multi-storey residential aged care facility on the Land in accordance with the plans endorsed by Council under this Incorporated Document,

subject to the conditions in Clause 6 of this Incorporated Document.

5. ENDORSED INCORPORATED PLANS:

The plans endorsed in accordance with this Incorporated Document are the plans prepared by CHT Architects, 'MECWACARE 351 Church Street, Richmond', Revision C - plan numbers TP00 to TP44 (dated 4 November 2017) and include any amendments to those plans that may be approved by the Responsible Authority under the conditions of this Incorporated Document. Once approved, these plans will be known as the 'Endorsed Incorporated Plans'.

6. THE FOLLOWING CONDITIONS APPLY TO THIS DOCUMENT:

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Endorsed Incorporated Plans

1. Before the development starts, amended plans to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. When approved, the plans will be endorsed under this document (**the Endorsed Incorporated Plans**). The plans must be drawn to scale with dimensions and three copies must be provided. The plans must be generally in accordance with the plans prepared by CHT Architects, 'MECWACARE 351 Church Street, Richmond', Revision C - plan numbers TP00 to TP44 (dated 4 November 2017) but modified to show:

    a) Changes required by the Landscape Plan in condition 6
    b) Any changes required by the SMP in condition 9

2. The development as shown on the Endorsed Incorporated Plans must not be altered without the prior written consent of the Responsible Authority.

3. Without the prior written consent of the Responsible authority, no more than 80 lodging rooms are permitted on the land at any one time.

4. Finished floor levels shown on the Endorsed Incorporated Plans must not be altered or modified without the prior written consent of the Responsible Authority.

Ongoing architect involvement

5. As part of the ongoing consultant team, CHT Architects or an architectural firm to the satisfaction of the Responsible Authority must be engaged to:

    a) oversee design and construction of the development; and
    b) ensure the design quality and appearance of the development is realised as shown in the Endorsed Incorporated Plans or otherwise to the satisfaction of the Responsible Authority.

Landscaping

6. Before the development starts, an amended Landscape Plan to the satisfaction of the Responsible Authority must be submitted to and approved by the responsible authority. When approved, the amended Landscape Plan will be endorsed and will form part of this Incorporated Document The amended Landscape Plan must be generally in accordance with the Landscape Plan prepared by John Patrick Landscape Architects Pty Ltd (revisions c) and dated 23 May 2017 but modified to include (or show):

    a) The location of the proposed "lightweight planter box".
    b) Ground level
        i. Permeable paving surrounding the Oak tree to ensure the roots are able to receive sufficient water.
        ii. Delete the two eastern-most trees from the line of Capital Pears along the southern boundary,.
    c) Level 2
        i. plans for the level 2 rooftop terrace.

7. Before the building is occupied or by such later date as is approved by the Responsible Authority in writing, the landscaping works shown on the endorsed Landscape Plan

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must be carried out and completed to the satisfaction of the Responsible Authority. The landscaping shown on the endorsed Landscape Plan must be maintained by:

a) implementing and complying with the provisions, recommendations and requirements of the endorsed Landscape Plan;
b) not using the areas set aside on the endorsed Landscape Plan for landscaping for any other purpose; and
c) replacing any dead, diseased, dying or damaged plants,

to the satisfaction of the Responsible Authority.

8. A cash bond for $5,000 plus a non-refundable 5% service charge of $250 shall be lodged with the Responsible Authority prior to the approval of the Landscape Plan to ensure the completion and establishment of landscaped areas. This cash bond will only be refunded upon the expiry of a 13 week establishment period, beginning when the Responsible Authority is satisfied with the completed landscaping works, and provided that the landscaped areas are being maintained to the satisfaction of the Responsible Authority.

ESD Report

9. Before the development starts, an amended Sustainable Management Plan to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. When approved, the amended Sustainable Management Plan will be endorsed and will form part of this Incorporated Document. The amended Sustainable Management Plan must be generally in accordance with the Sustainable Management Plan prepared by Sustainable Built Environments Pty Ltd and dated 23/11/2016, but modified to include or show:

a) The relevant changes to the Endorsed Incorporated Plans
b) All relevant ESD features described and detailed in the SMP should be marked on an updated set of architectural drawings and landscape plans, including;
    i. The 5m2 of raingardens, and
    ii. The 25 kW solar PV array.

10. The provisions, recommendations and requirements of the endorsed Sustainable Management Plan must be implemented and complied with to the satisfaction of the Responsible Authority.

Waste Management

11. The provisions, recommendations and requirements of the endorsed Waste Management Plan must be implemented and complied with to the satisfaction of the Responsible Authority.

12. Rubbish, including bottles and packaging material, must at all times be stored within the building and screened from external view. All waste collection and recycling collection is to be undertaken in accordance with Council's Local Law No. 3.

Car Parking and Traffic

13. The car parking area must be used for no other purpose to the satisfaction of the Responsible Authority.

Vehicle crossings and accessways

14. Before the building is occupied, or by such later date as approved in writing by the Responsible Authority, any new vehicle crossing must be constructed:

    a) in accordance with any requirements or conditions imposed by Council;
    b) at the permit holder's cost; and
    c) to the satisfaction of the Responsible Authority.

Drainage

15. Areas must be provided inside the property line and adjacent to the footpath to accommodate pits and meters. No private pits, valves or meters on Council property will be accepted.

Construction

16. Before the building is occupied, or by such later date as approved in writing by the Responsible Authority, any damage to Council infrastructure resulting from the development must be reinstated (including by the re-sheeting of the entire Church Street footpath for the width of the property frontage if required by the Responsible Authority):

    a) at the permit holder's cost; and
    b) to the satisfaction of the Responsible Authority.

17. All pipes, fixtures, fittings and vents servicing any building on the land must be concealed in service ducts or otherwise hidden from view to the satisfaction of the Responsible Authority.

Amenity

18. Any noise emanating from the development including plant and other equipment, must comply with the State Environment Protection Policy or any other standard recommended by the Environment Protection Authority (EPA), to the satisfaction of the Responsible Authority.

Privacy screens

19. Before the building is occupied, or by such later date as approved in writing by the Responsible Authority, all screening and other measures to prevent overlooking as shown on the Endorsed Incorporated Plans must be installed to the satisfaction of the Responsible Authority. Once installed the screening and other measures must be maintained to the satisfaction of the Responsible Authority.

Tree Management Plan Required

20. Before the development starts, an amended Tree Management Plan to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. When approved, the amended Tree Management Plan will be endorsed and will form part of this Incorporated Document. The amended Tree Management Plan must be generally in accordance with the Tree Management Plan prepared by Tree Logic and dated 20 October 2015 and supplemented by an addendum report dated 11 November 2017, but modified to include (or show):

    a) any changes resulting from the Endorsed Incorporated Plans
    b) any changes resulting from the endorsed Landscape Plans

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21. The provisions, recommendations and requirements of the endorsed Tree Management Plan must be complied with and implemented to the satisfaction of the Responsible Authority.

Lighting

22. Before the building is occupied, or by such later date as approved in writing by the Responsible Authority, external lighting capable of illuminating access to the basement car park, pedestrian walkway, and building entrances must be provided within the property boundary. Lighting must be:

    a) located;
    b) directed;
    c) shielded; and
    d) of limited intensity,

    to the satisfaction of the Responsible Authority

Construction Management Plan

23. Before development commences, a Construction Management Plan to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. When approved, the plan will be endorsed and will form part of this Incorporated Document. The plan must provide for:

    a) a pre-conditions survey (dilapidation report) of the land and all adjacent Council roads frontages and nearby road infrastructure;
    b) works necessary to protect road and other infrastructure;
    c) remediation of any damage to road and other infrastructure;
    d) containment of dust, dirt and mud within the land and method and frequency of clean up procedures to prevent the accumulation of dust, dirt and mud outside the land;
    e) facilities for vehicle washing, which must be located on the land;
    f) the location of loading zones, site sheds, materials, cranes and crane/hoisting zones, gantries and any other construction related items or equipment to be located in any street;
    g) site security;
    h) management of any environmental hazards including, but not limited to:
        i. contaminated soil;
        ii. materials and waste;
        iii. dust;
        iv. storm-water contamination from run-off and wash-waters;
        v. sediment from the land on roads;
        vi. washing of concrete trucks and other vehicles and machinery; and
        vii. spillage from re-fueling cranes and other vehicles and machinery;
    i) the construction program;
    j) preferred arrangements for trucks delivering to the land, including delivery and unloading points and expected duration and frequency;
    k) parking facilities for construction workers;
    l) measures to ensure that all work on the land will be carried out in accordance with the Construction Management Plan;
    m) an outline of requests to occupy public footpaths or roads, or anticipated disruptions to local services;

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n) an emergency contact that is available for 24 hours per day for residents and the Responsible Authority in the event of relevant queries or problems experienced;
o) the provision of a traffic management plan to comply with provisions of AS 1742.3-2002 Manual of uniform traffic control devices - Part 3: Traffic control devices for works on roads;
p) a Noise and Vibration Management Plan showing methods to minimise noise and vibration impacts on nearby properties and to demonstrate compliance with Noise Control Guideline 12 for Construction (Publication 1254) as issued by the Environment Protection Authority in October 2008. The Noise and Vibration Management Plan must be prepared to the satisfaction of the Responsible Authority. In preparing the Noise and Vibration Management Plan, consideration must be given to:
    i. using lower noise work practice and equipment;
    ii. the suitability of the land for the use of an electric crane;
    iii. silencing all mechanical plant by the best practical means using current technology;
    iv. fitting pneumatic tools with an effective silencer;
    v. other relevant considerations; and
    vi. any site-specific requirements.
q) any storm-water discharged into the storm-water drainage system must be in compliance with Environment Protection Authority guidelines;
r) storm-water drainage system protection measures must be installed as required to ensure that no solid waste, sediment, sand, soil, clay or stones from the land enters the storm-water drainage system;
s) vehicle borne material must not accumulate on the roads abutting the land;
t) the cleaning of machinery and equipment must take place on the land and not on adjacent footpaths or roads; and
u) all litter (including items such as cement bags, food packaging and plastic strapping) must be disposed of responsibly.

24. If required, the Construction Management Plan may be approved in stages. Construction of each stage must not commence until a Construction Management Plan has been endorsed for that stage, to the satisfaction of the Responsible Authority.

25. The provisions, recommendations and requirements of the endorsed Construction Management Plan must be implemented and complied with to the satisfaction of the Responsible Authority.

Construction hours and noise

26. Except with the prior written consent of the Responsible Authority, demolition or construction works must not be carried out:

    a) Monday-Friday (excluding public holidays) before 7 am or after 6 pm;
    b) Saturdays and public holidays (other than ANZAC Day, Christmas Day and Good Friday) before 9 am or after 3 pm; or
    c) Sundays, ANZAC Day, Christmas Day and Good Friday at any time.

7. EXPIRY

27. The controls in this Incorporated Document expire if any one of the following circumstances applies:

    a) The development allowed by the controls is not commenced within two (2) years from the date of approval of Amendment C225 to the Yarra Planning Scheme.

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b) The development allowed by the controls is not completed within four (4) years from the date of approval of Amendment C225 to the Yarra Planning Scheme.

The Responsible Authority may extend the periods referred to if a request is made in writing before these controls expire or within 6 months afterwards in the case of the commencement of the development, or within twelve months afterwards in the case of the completion of the development.

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Notes:

The site must be drained to the legal point of discharge to the satisfaction of the Responsible Authority. Please contact Council's Building Services on 9205 5585 for further information.

A building permit may be required before development is commenced. Please contact Council's Building Services on 9205 5585 to confirm.

A local law permit (e.g. Asset Protection Permit, Road Occupation Permit) may be required before development is commenced. Please contact Council's Construction Management Branch on Ph. 9205 5585 to confirm.

All future property owners, residents, business owners and employees within the development approved under this permit will not be permitted to obtain resident, employee or visitor parking permits.

A local law permit may be required for tree removal. Please contact Council's Compliance Branch on 9205 5166.

A vehicle crossing permit is required for the construction of the vehicle crossing(s). Please contact Council's Construction Management Branch on 9205 5585 for further information.

End of Document

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Attachment 2 - 351 Church Street Richmond - architectural plans

NO.	SHEET NAME	REV.
TP00	COVER PAGE	C
TP01	EXISTING SURVEY	C
TP02	EXISTING SITE PLAN	C
TP03	PROPOSED SITE PLAN	C
TP04	DEMOLITION PLAN - GROUND FLOOR	C
TP05	DEMOLITION PLAN - FIRST FLOOR	P4
TP06	DEMOLITION PLAN - ROOF	P4
TP07	PROPOSED BASEMENT	C
TP08	PROPOSED LOWER GROUND PLAN	C
TP09	PROPOSED GROUND FLOOR PLAN	C
TP10	PROPOSED LEVEL 1 PLAN	C
TP11	PROPOSED LEVEL 2 PLAN	C
TP12	PROPOSED LEVEL 3 PLAN	C
TP13	PROPOSED LEVEL 4 PLAN	C
TP14	PROPOSED ROOF PLAN	C
TP20	ELEVATIONS	C
TP21	ELEVATIONS	C
TP25	STREETSCAPE ELEVATION/ CONTEXT SECTIONS	C
TP26	SECTIONS	C
TP30	SHADOW STUDY - 22ND SEPTEMBER	C
TP31	SHADOW STUDY - 22ND SEPTEMBER	C
TP32	SHADOW STUDY - 22ND SEPTEMBER	C
TP33	SHADOW STUDY - 22ND SEPTEMBER	C
TP34	SHADOW STUDY - 22ND SEPTEMBER	C
TP35	SHADOW STUDY - 22ND SEPTEMBER	C
TP36	SHADOW STUDY - 22ND SEPTEMBER	C
TP37	SHADOW STUDY - 22ND SEPTEMBER	C
TP38	SHADOW STUDY - 22ND SEPTEMBER	C
TP39	SHADOW SECTIONS - 22ND SEPTEMBER @ 1PM	C
TP40	OVERSHADOWING ANALYSIS - 22ND SEPTEMBER	C
TP41	OVERLOOKING SECTIONS - SOUTH	C
TP42	OVERLOOKING SECTIONS - SOUTH	C
TP43	OVERLOOKING SECTIONS - NORTH	C
TP44	OVERLOOKING SECTIONS - NORTH	C

GROSS FLOOR AREA (GFA)	
LEVEL	AREA
-02 - BASEMENT	1154 m²
-01 - LOWER GROUND FLOOR	1055 m²
00 - GROUND FLOOR	1249 m²
01 - FIRST FLOOR	1244 m²
02 - SECOND FLOOR	845 m²
03 - THIRD FLOOR	638 m²
04 - FOURTH FLOOR	517 m²
TOTAL GFA	6735 m²

RESIDENT ROOM COUNT - PER LEVEL	
LEVEL	COUNT
00 - GROUND FLOOR	19
01 - FIRST FLOOR	19
02 - SECOND FLOOR	17
03 - THIRD FLOOR	14
04 - FOURTH FLOOR	10
GRAND TOTAL	79

GFA EXCLUDING BASEMENT	
LEVEL	AREA
-01 - LOWER GROUND FLOOR	562 m²
00 - GROUND FLOOR	1249 m²
01 - FIRST FLOOR	1244 m²
02 - SECOND FLOOR	845 m²
03 - THIRD FLOOR	638 m²
04 - FOURTH FLOOR	517 m²
TOTAL GFA	5055 m²

M² PER RESIDENT
71 m²

CHT ARCHITECTS

MECWACARE
351 CHURCH ST, RICHMOND, VIC, 3121

mecwacare

COVER PAGE

PRELIMINARY
NOT FOR CONSTRUCTION

TP00 C

4 NOV 2017

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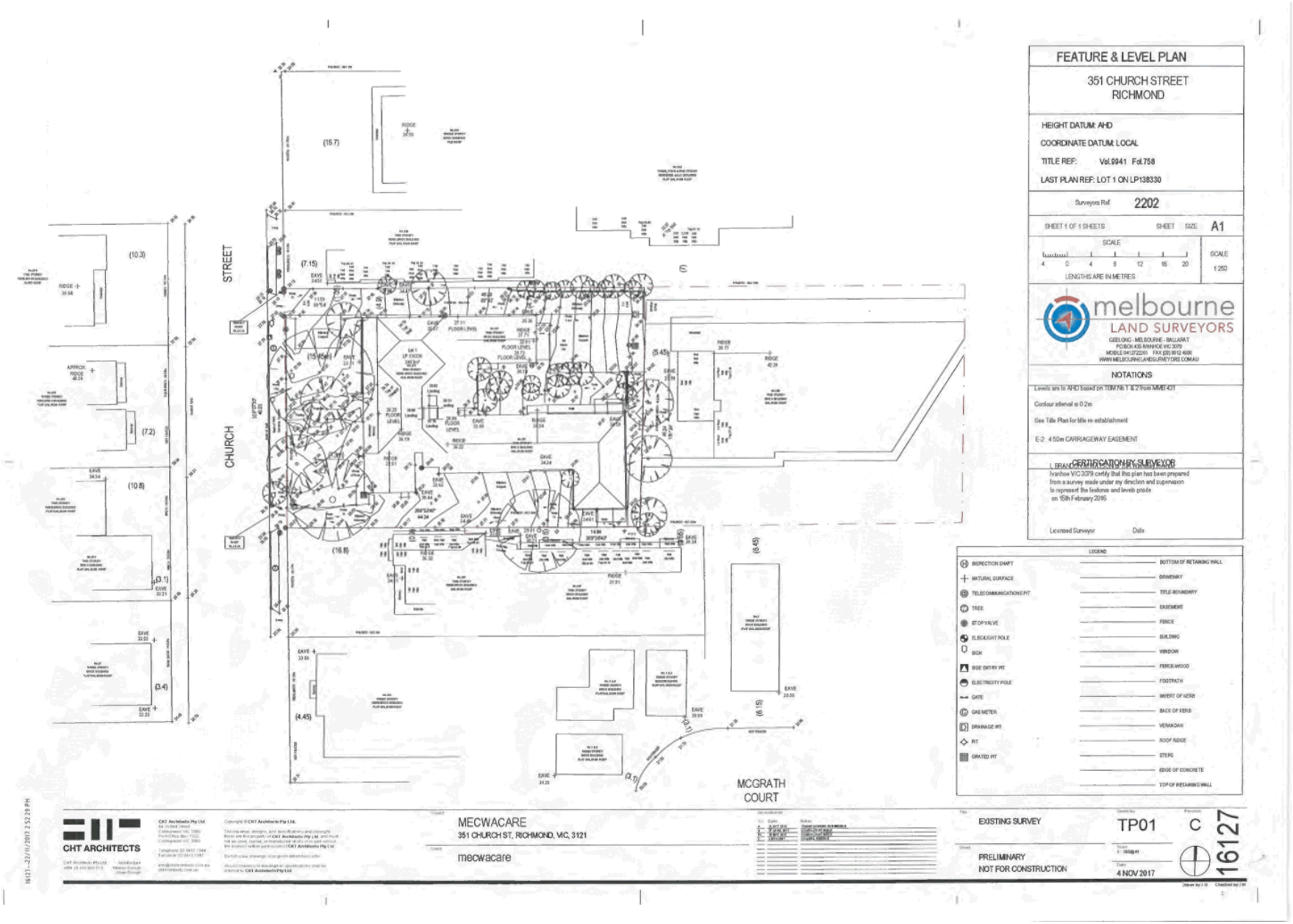

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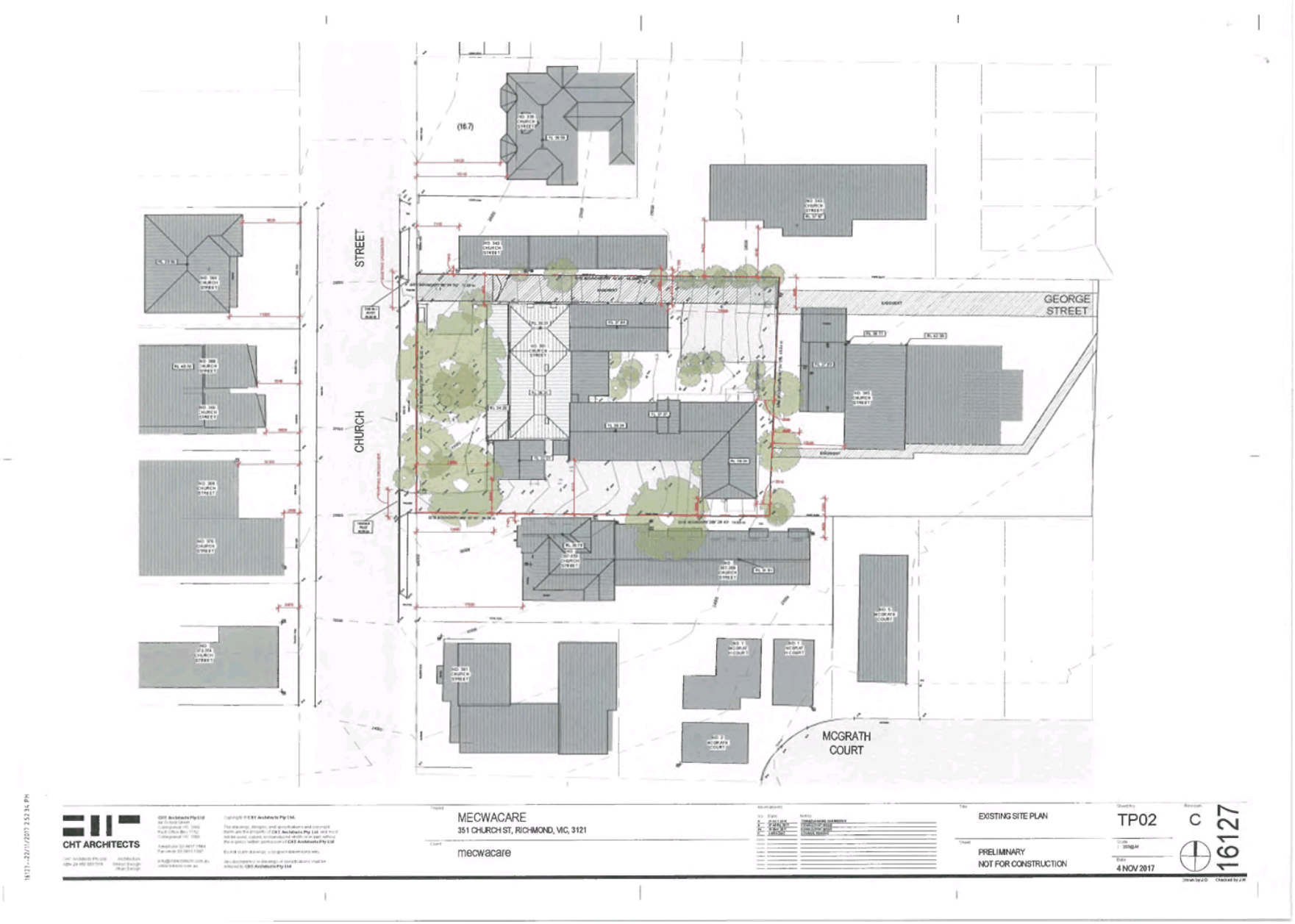

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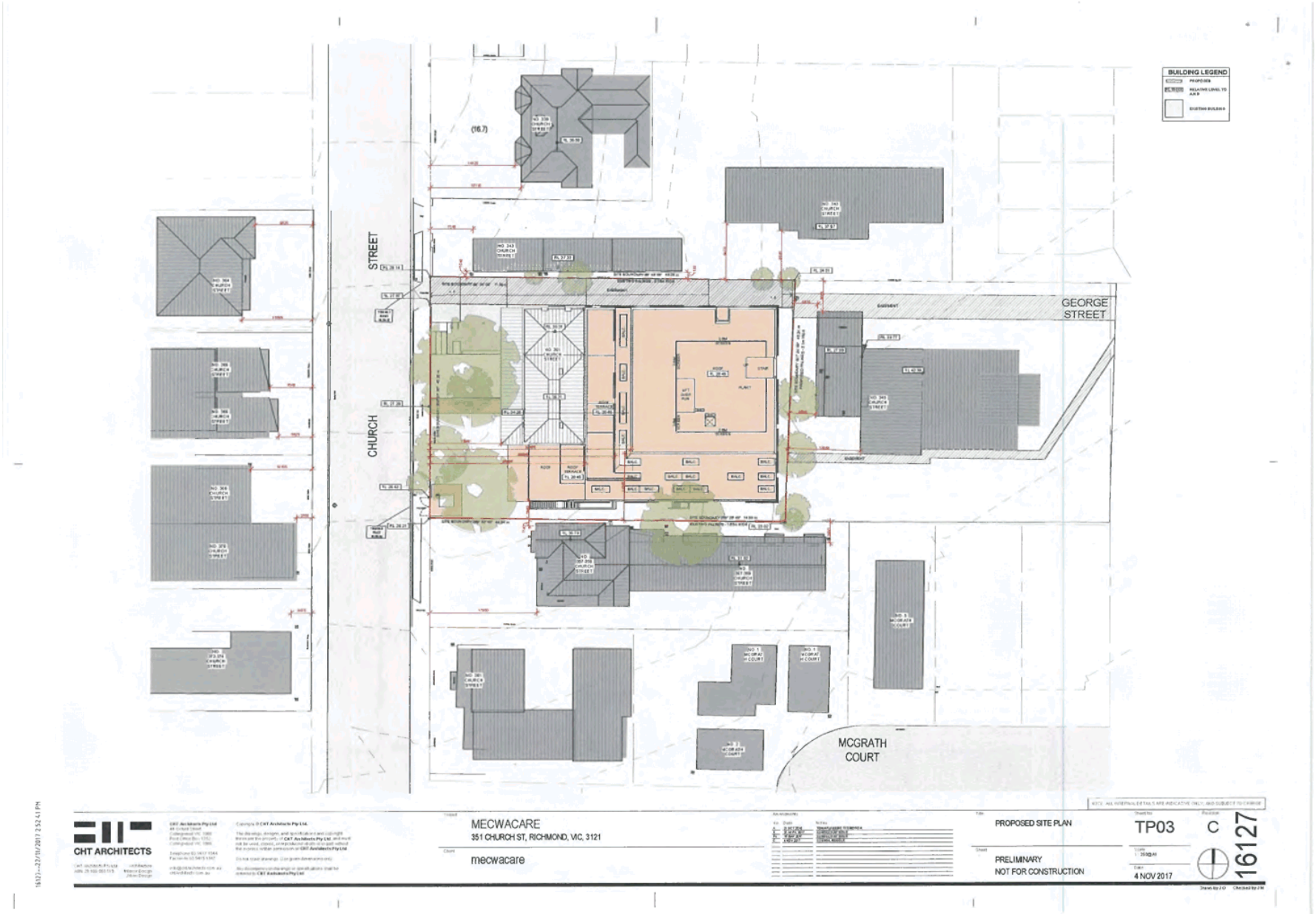

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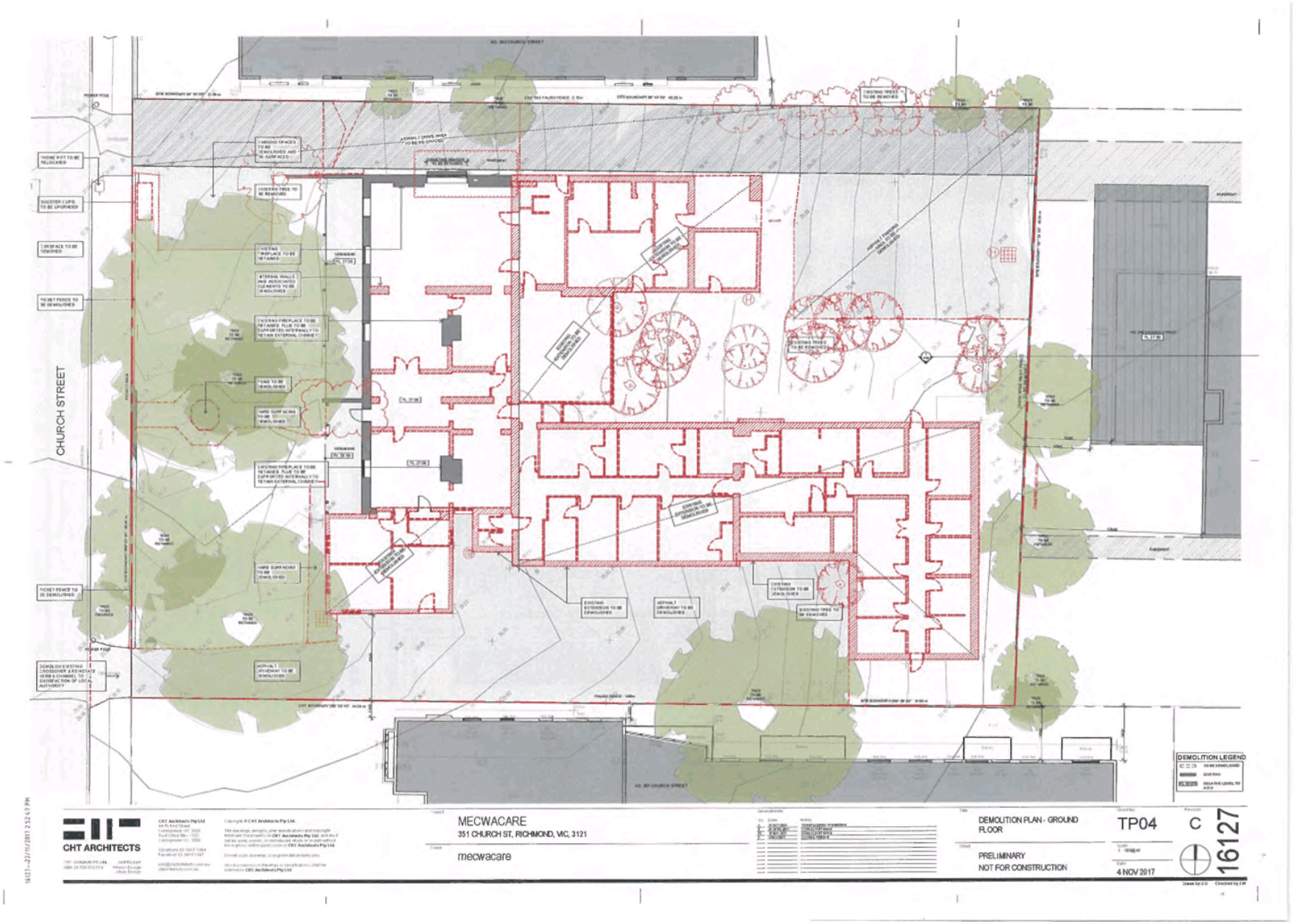

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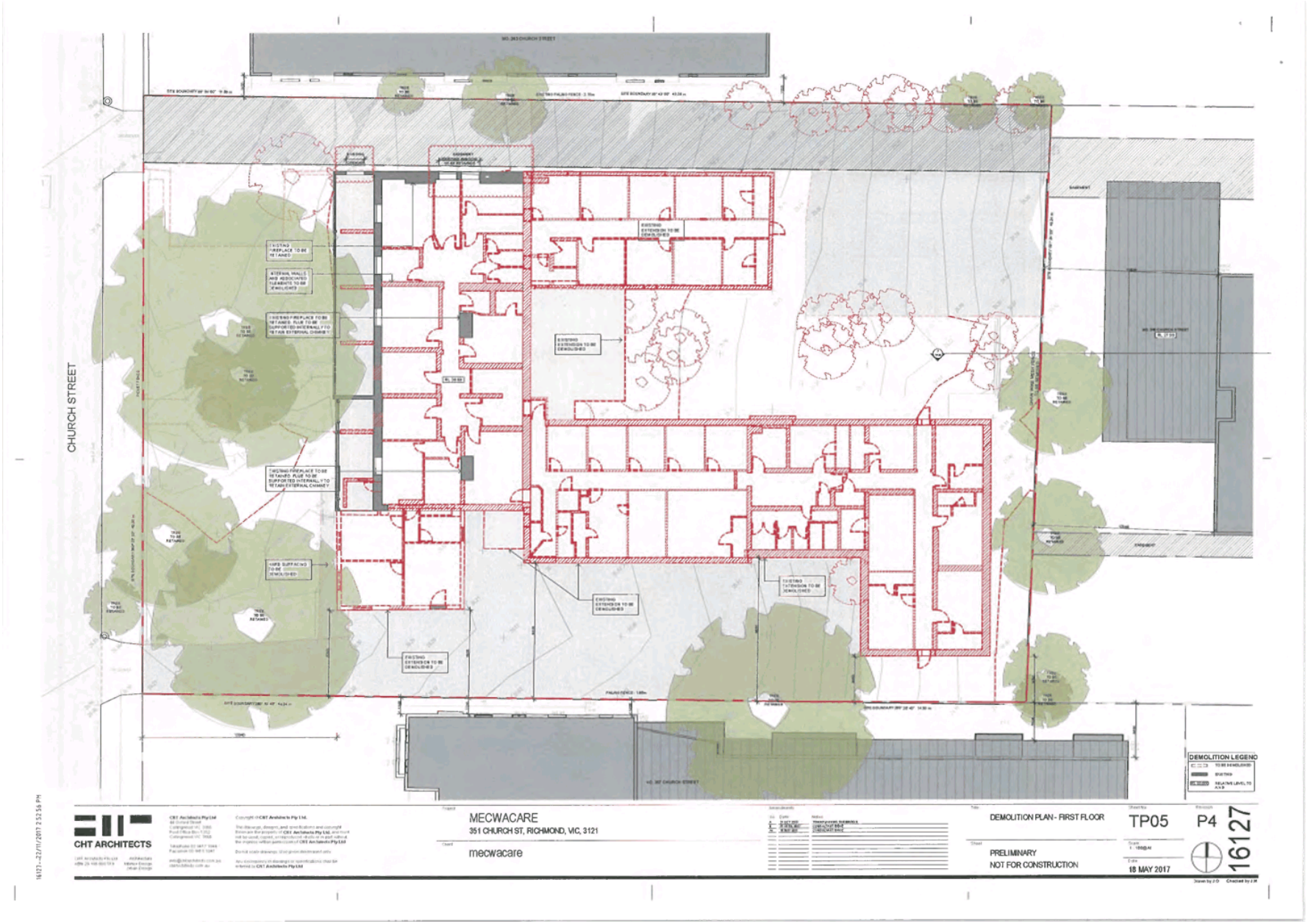

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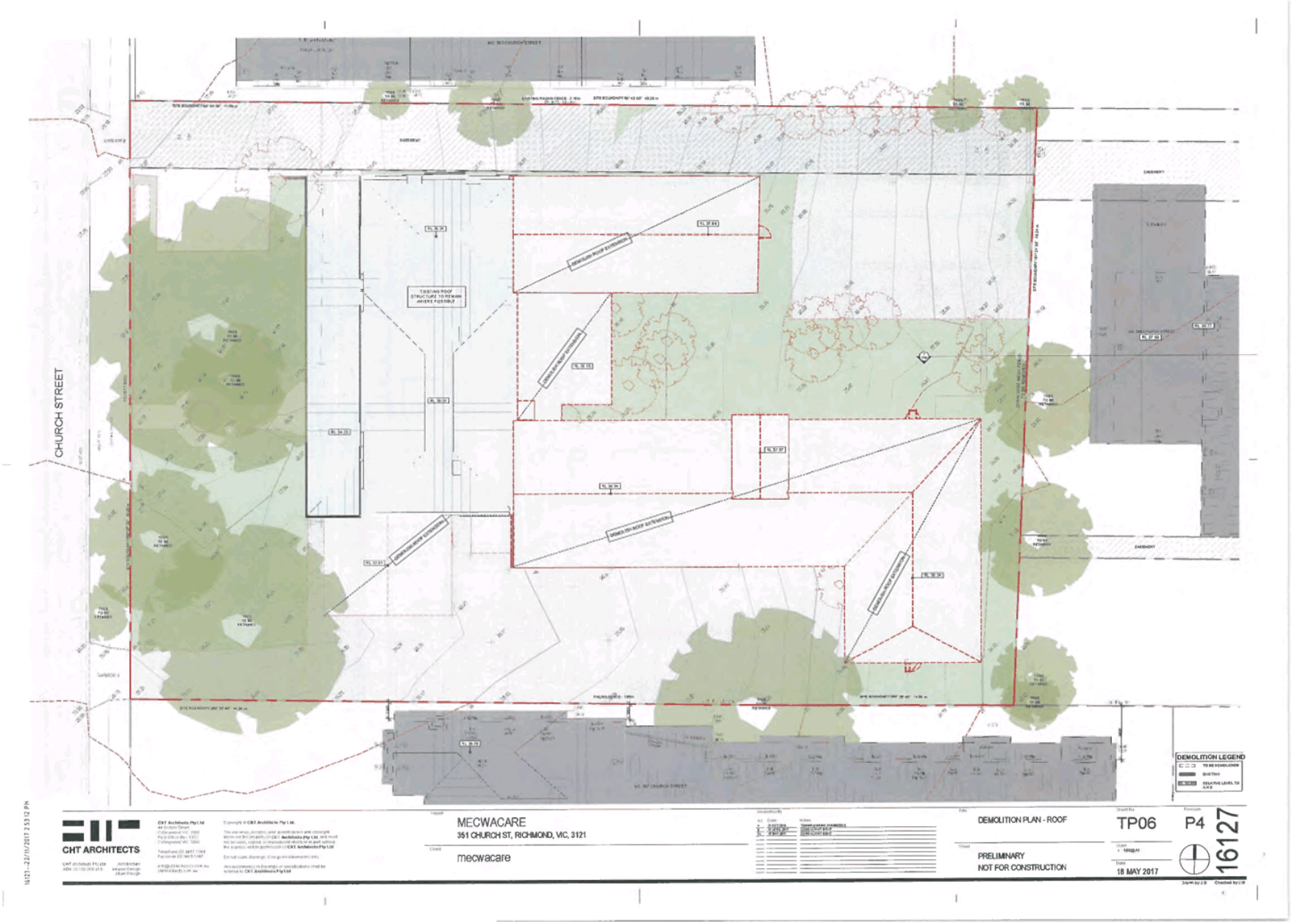

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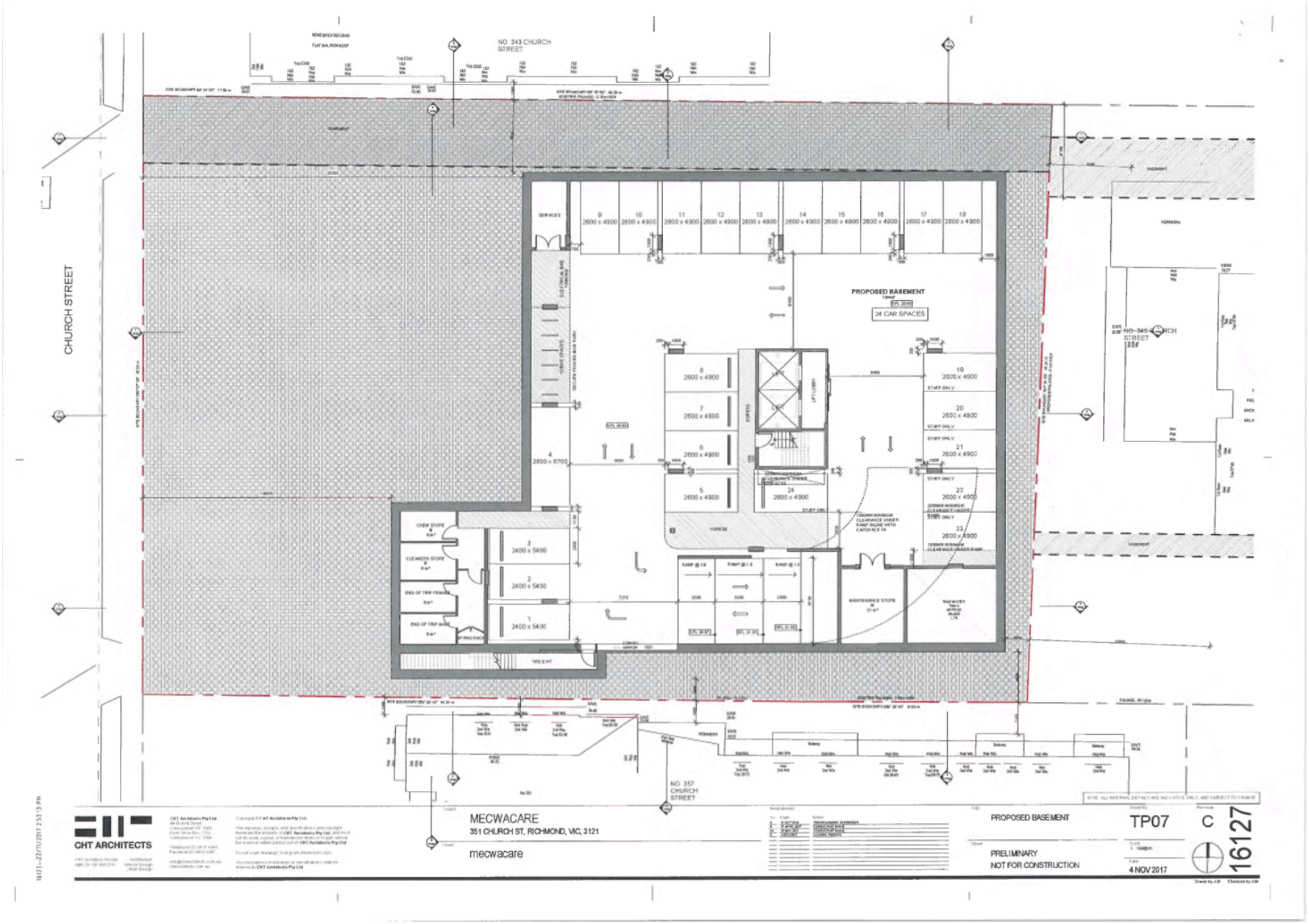

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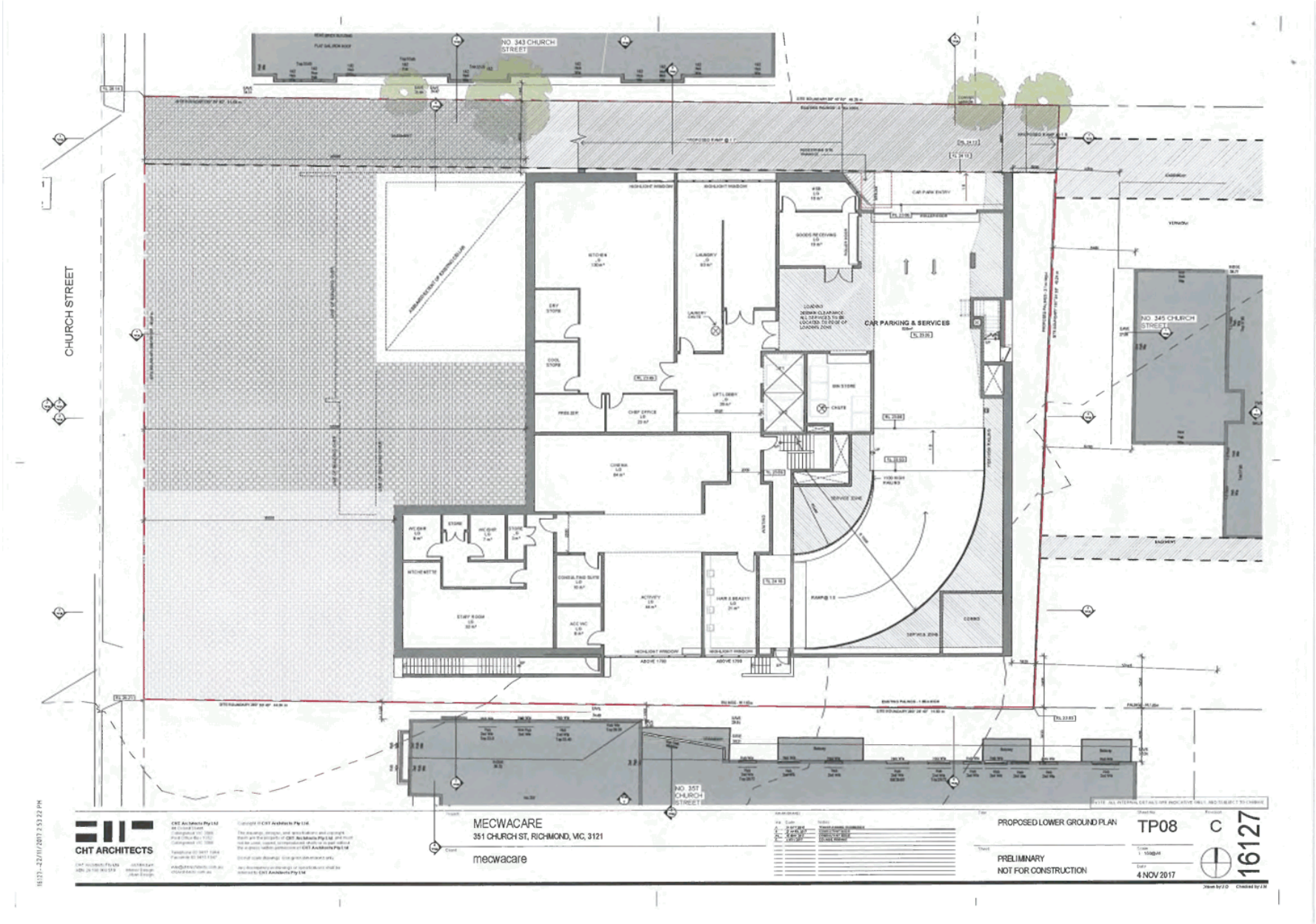

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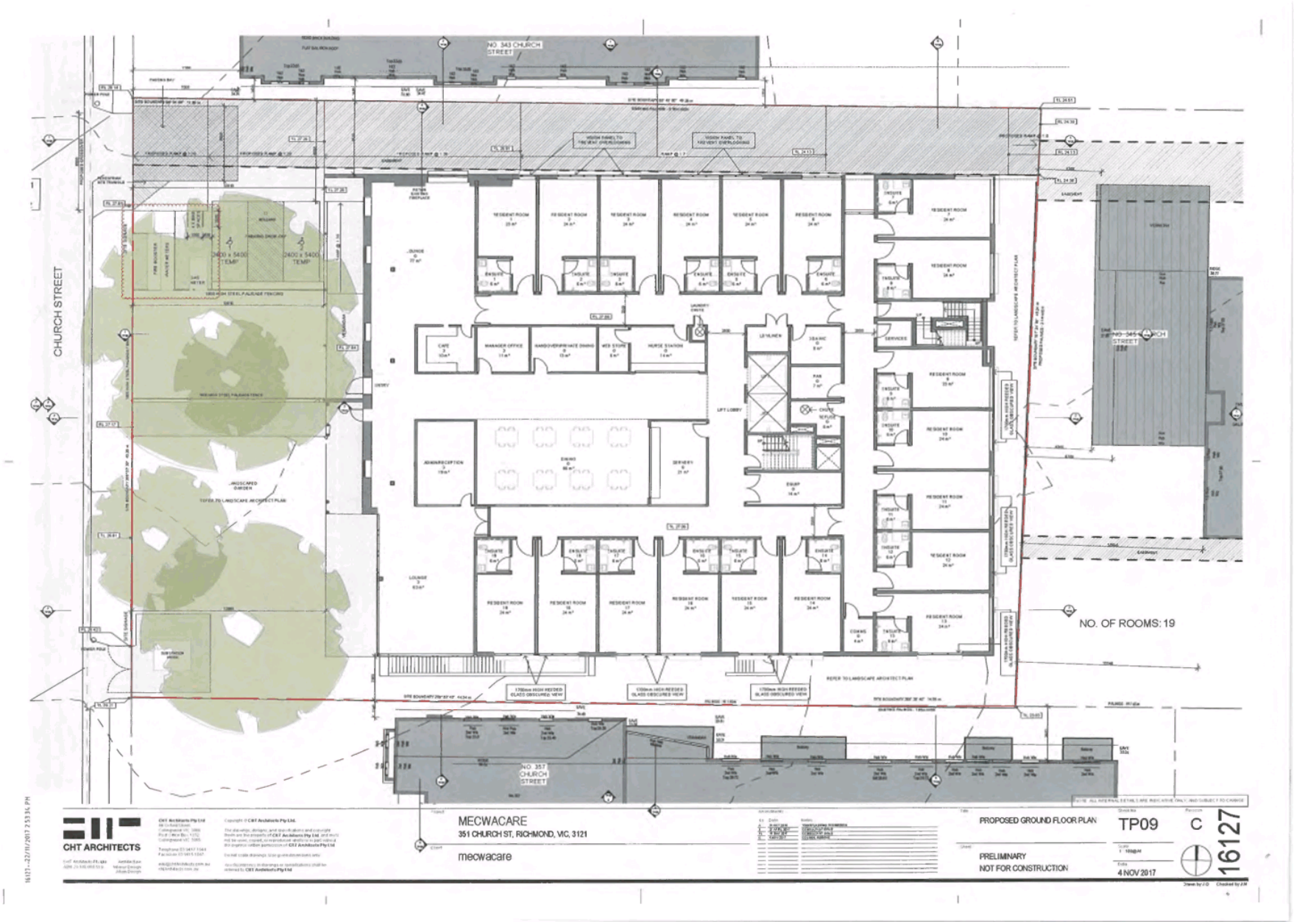

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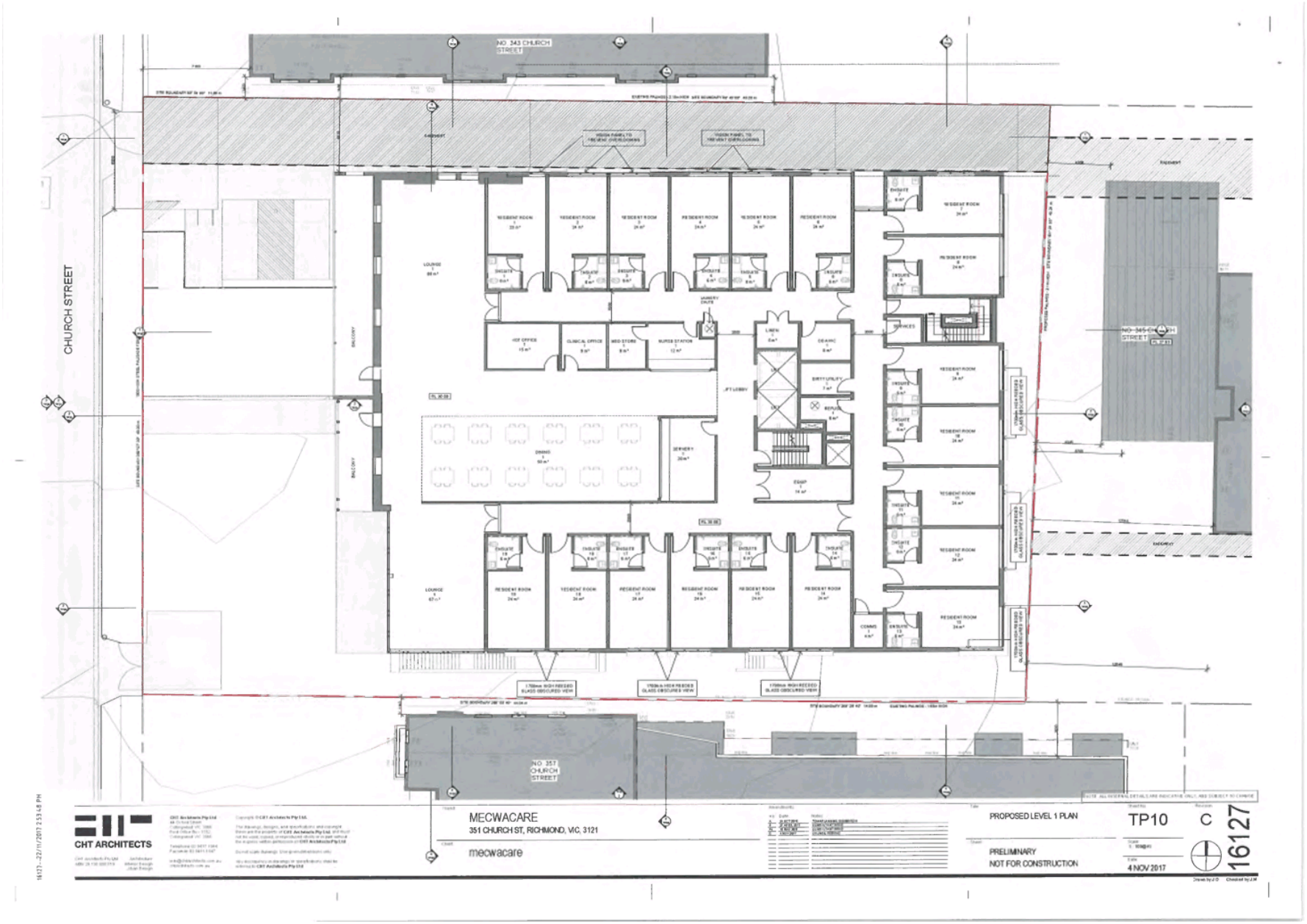

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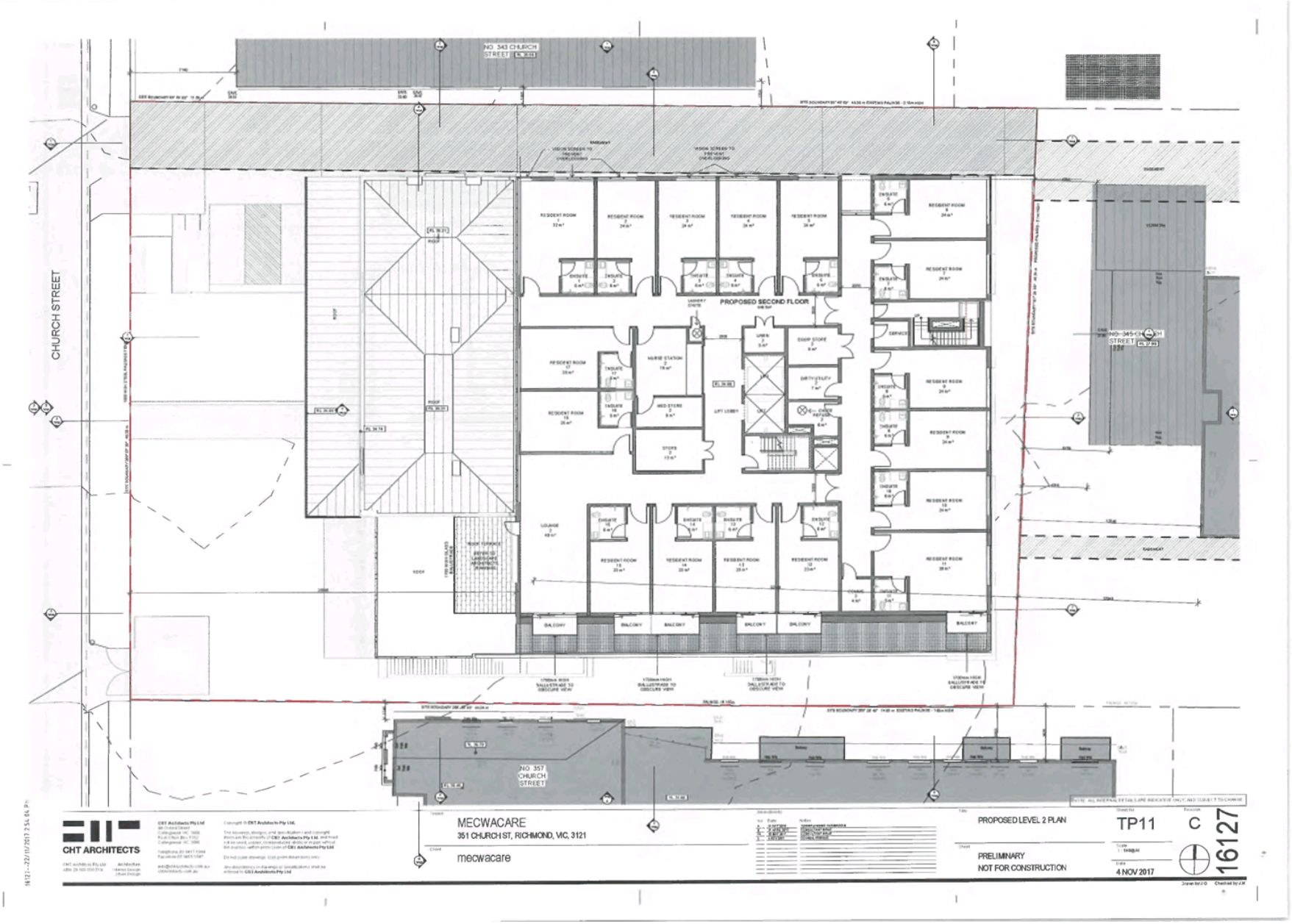

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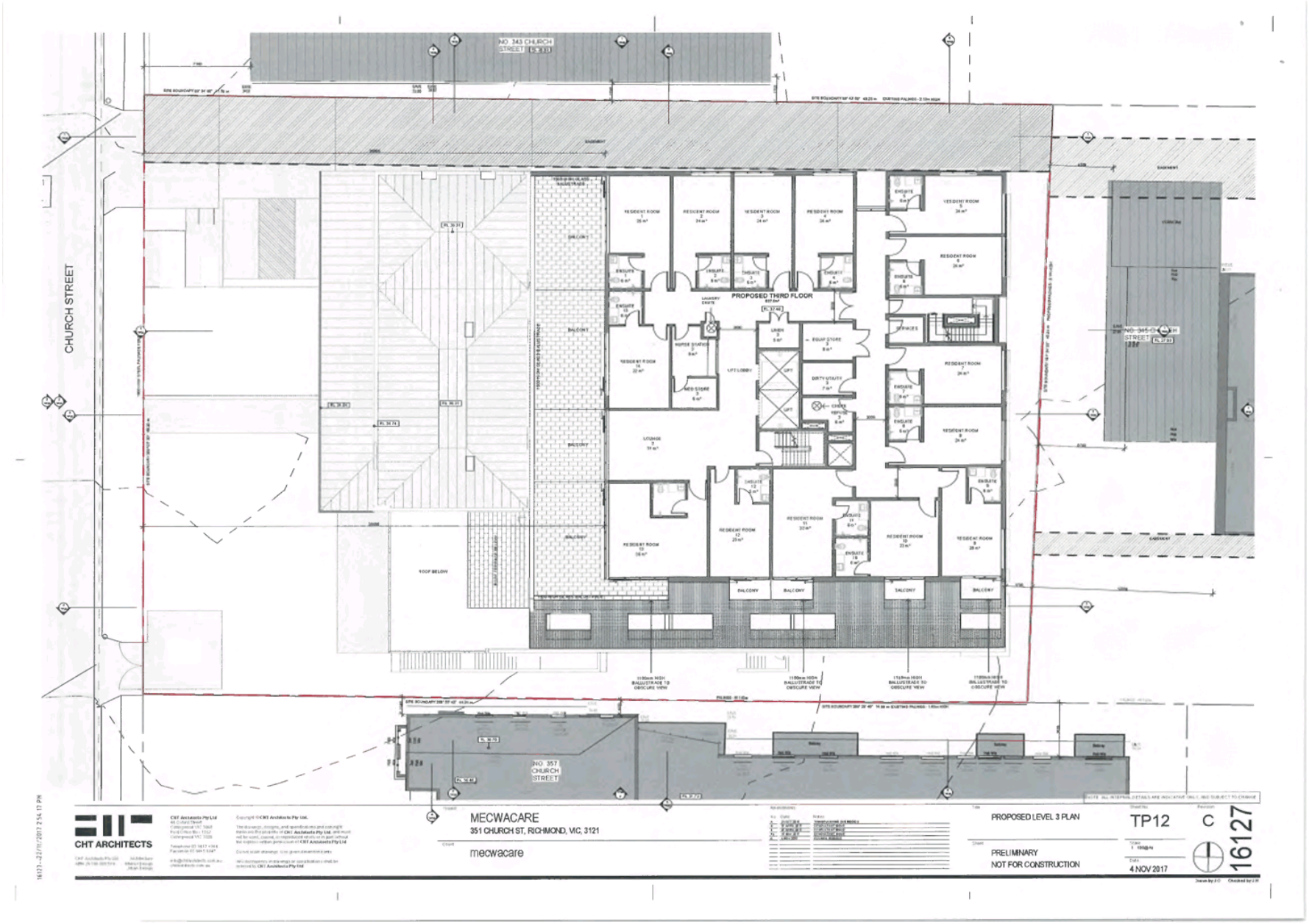

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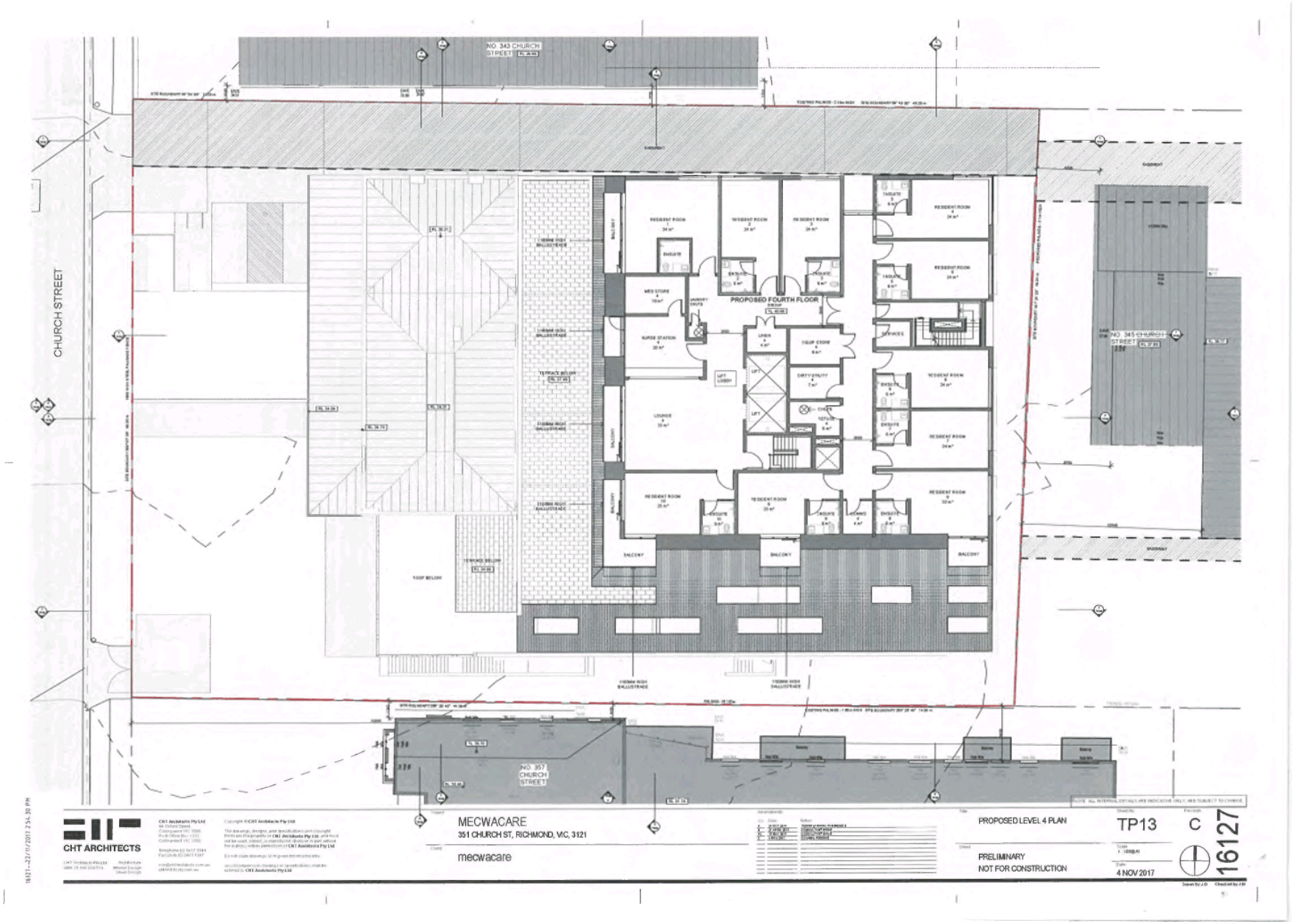

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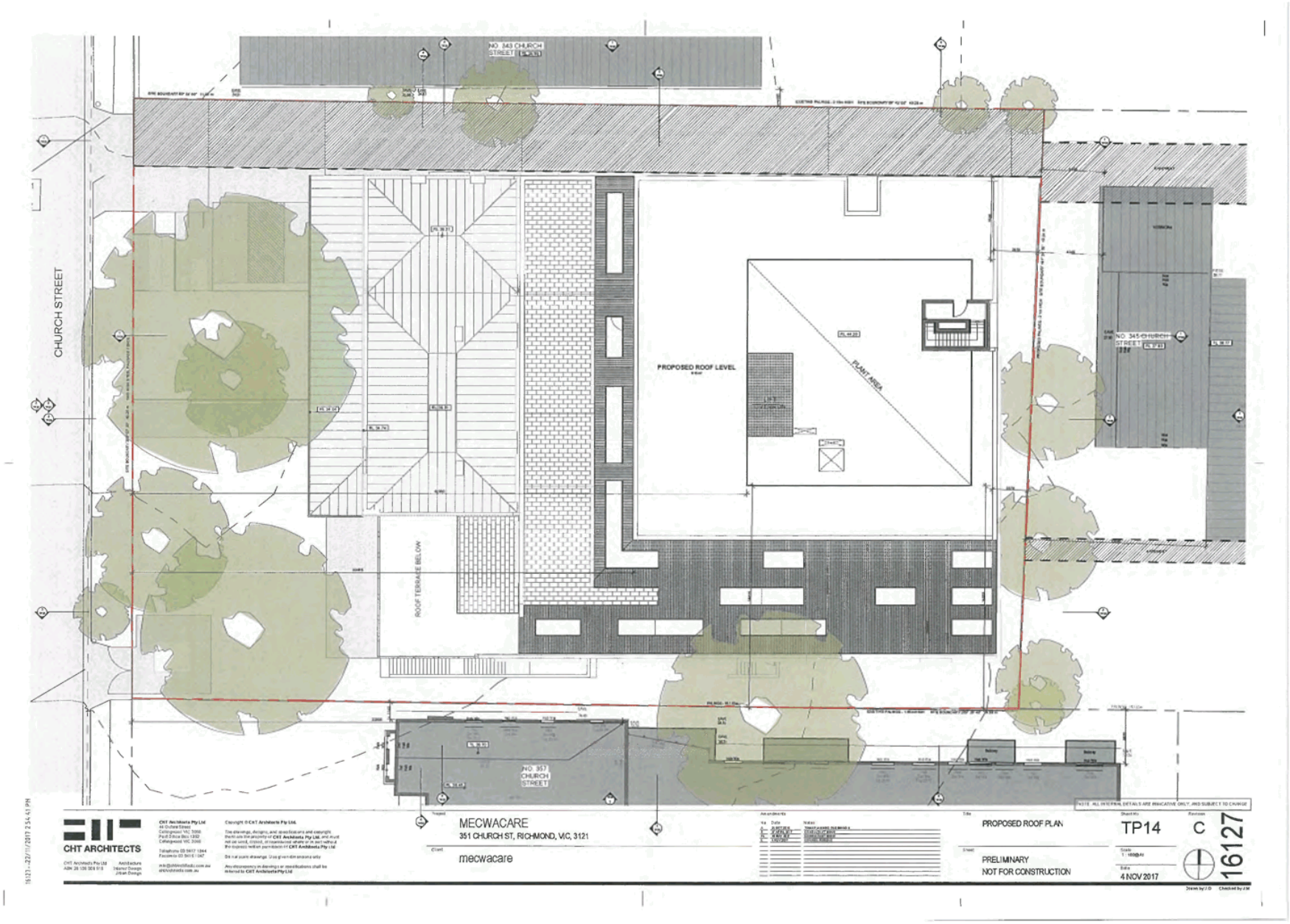

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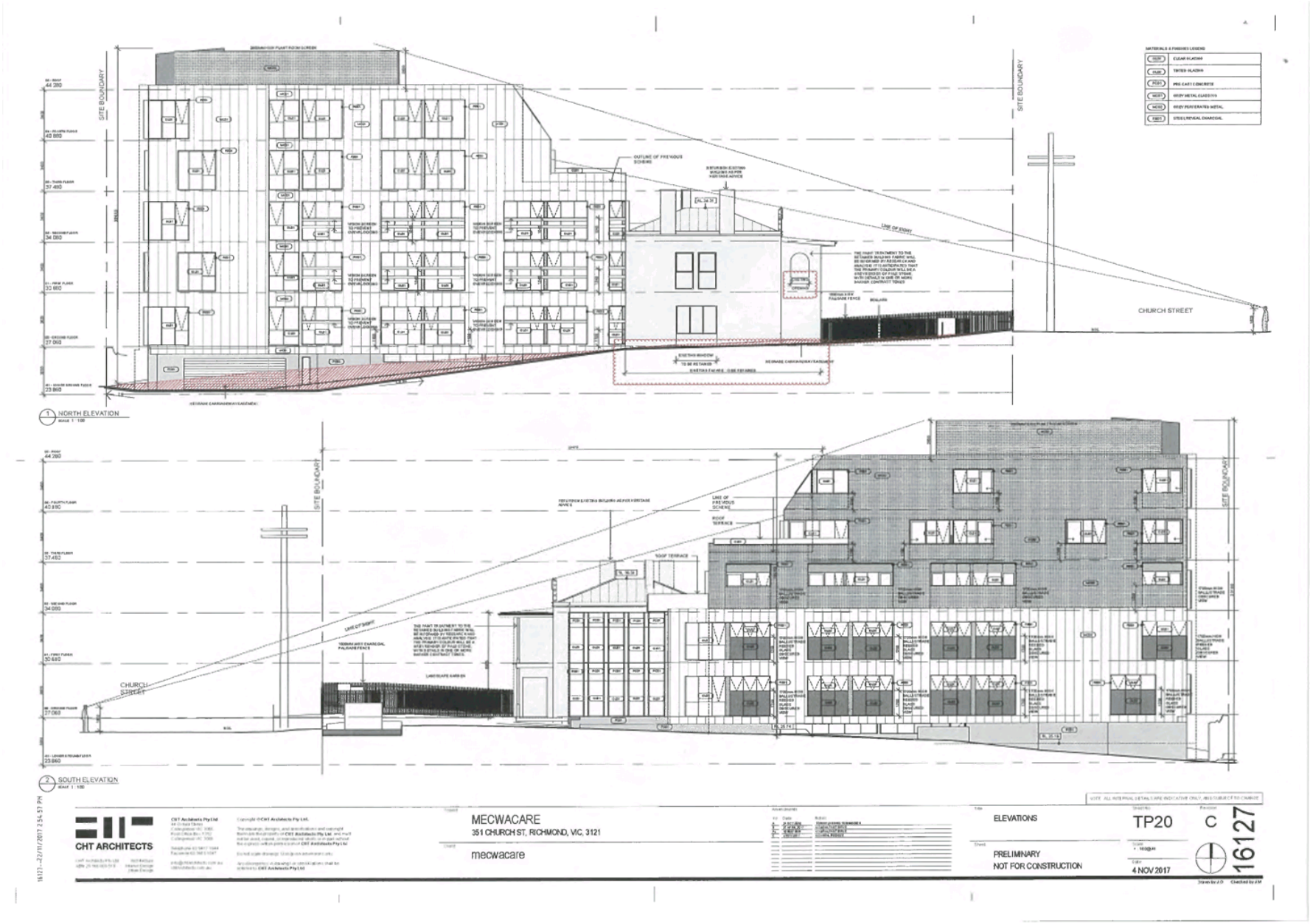

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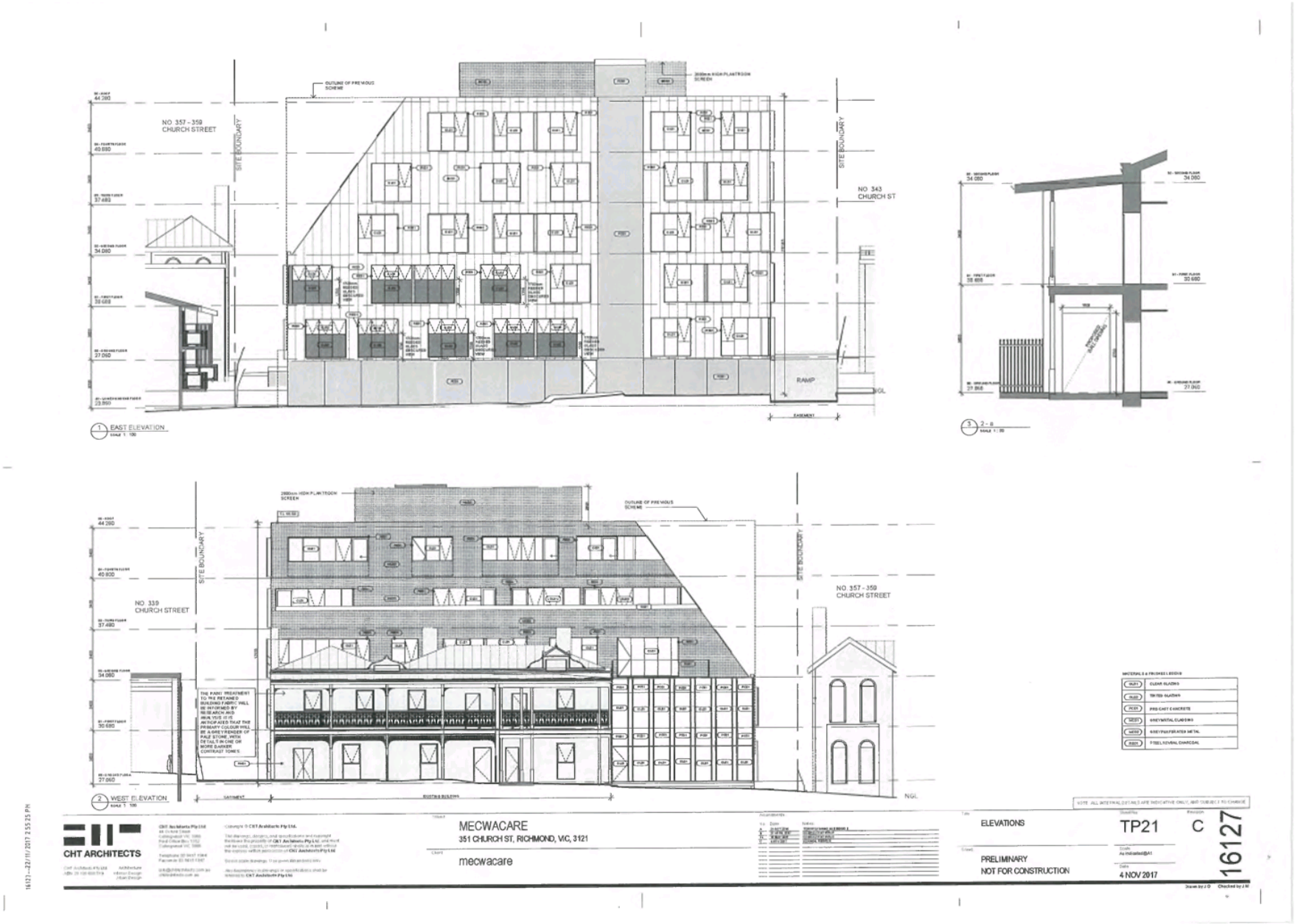

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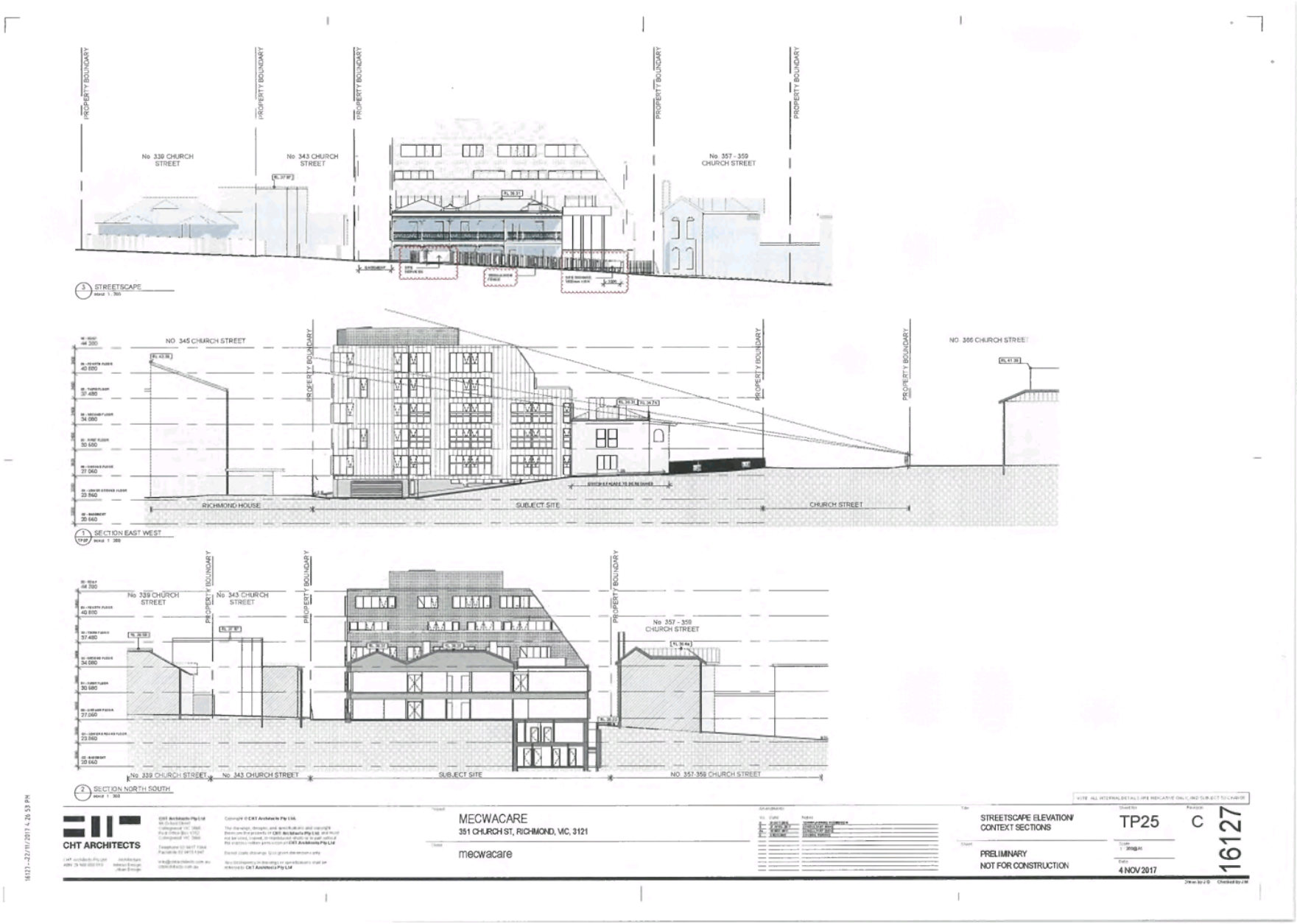

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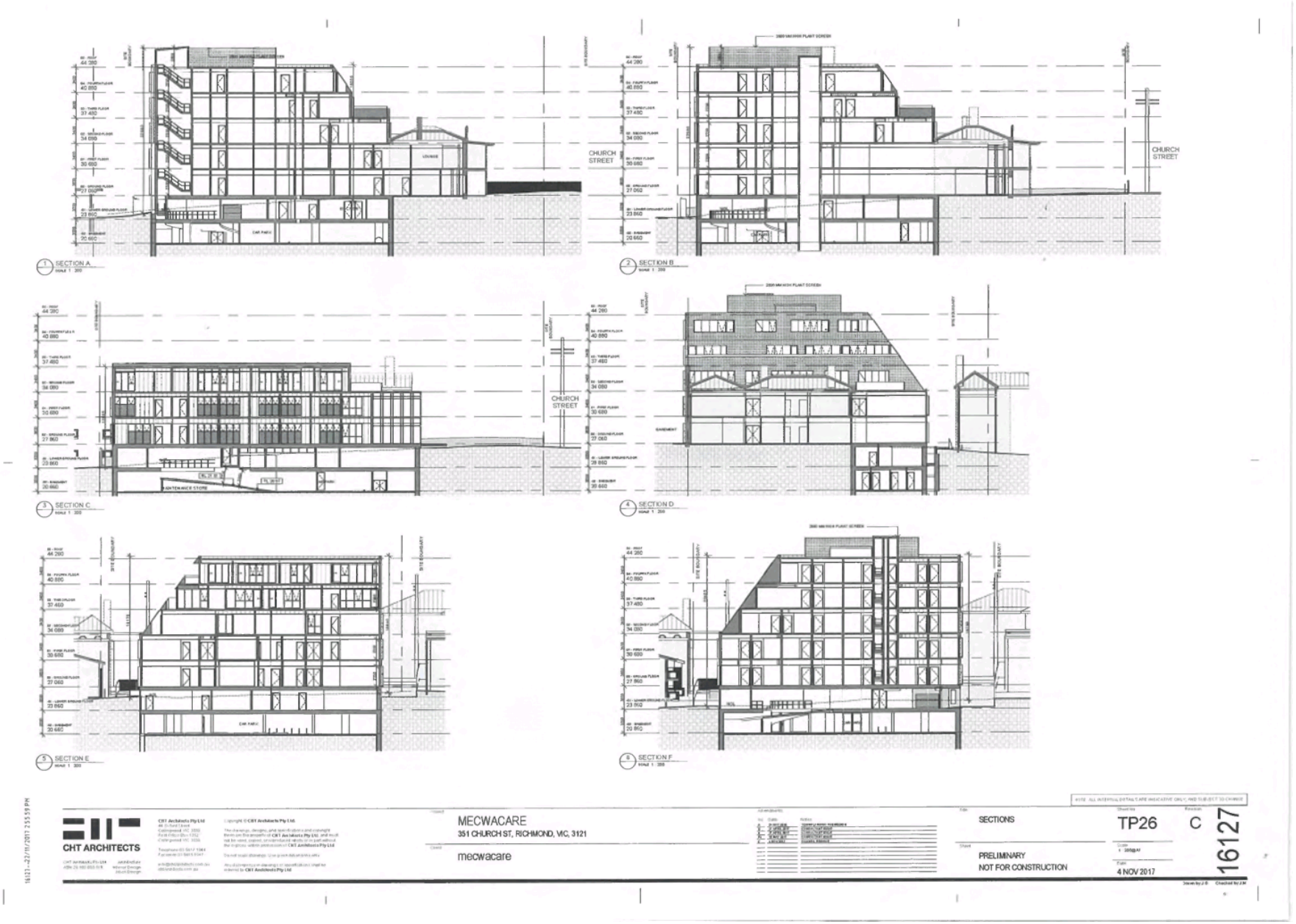

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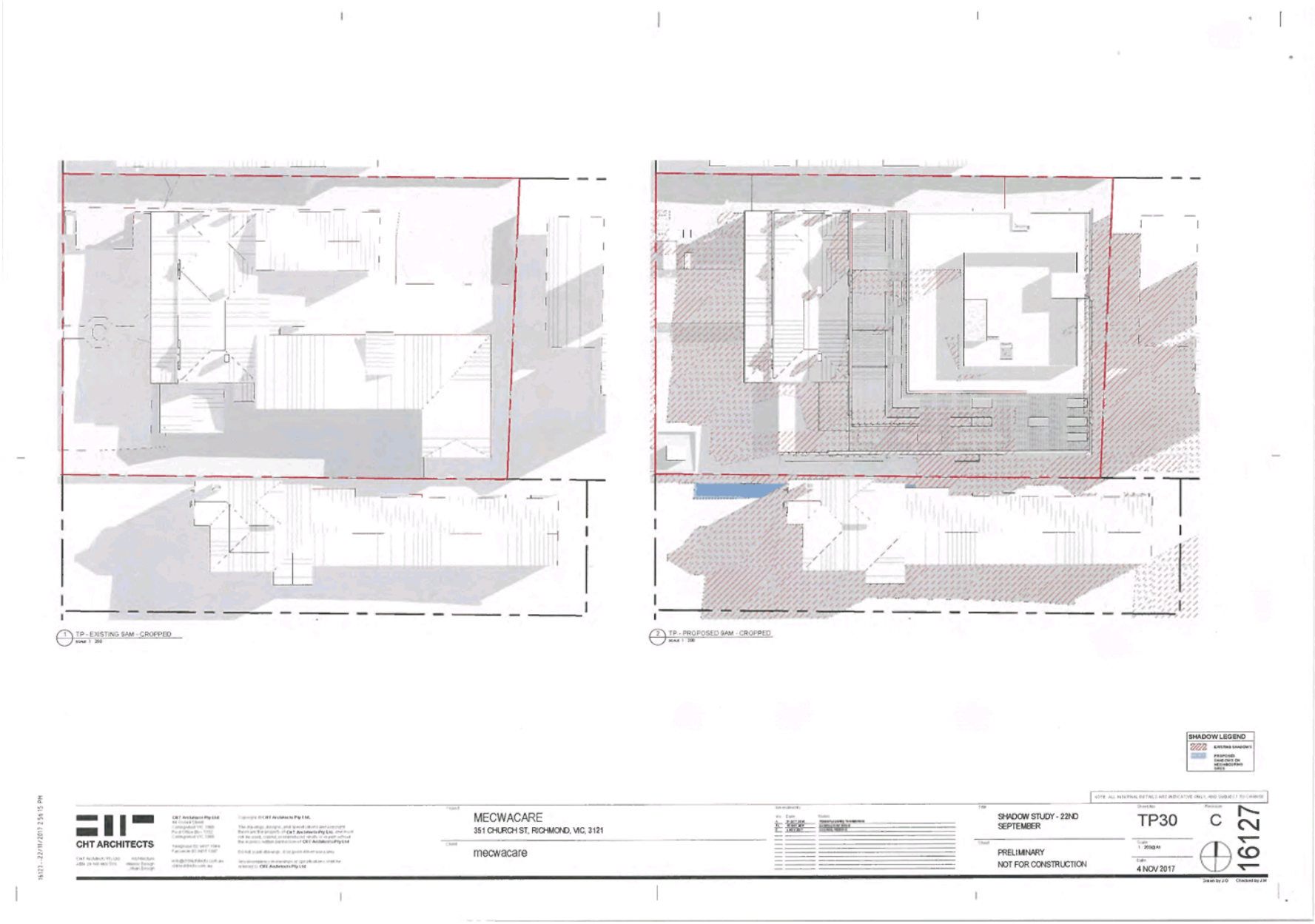

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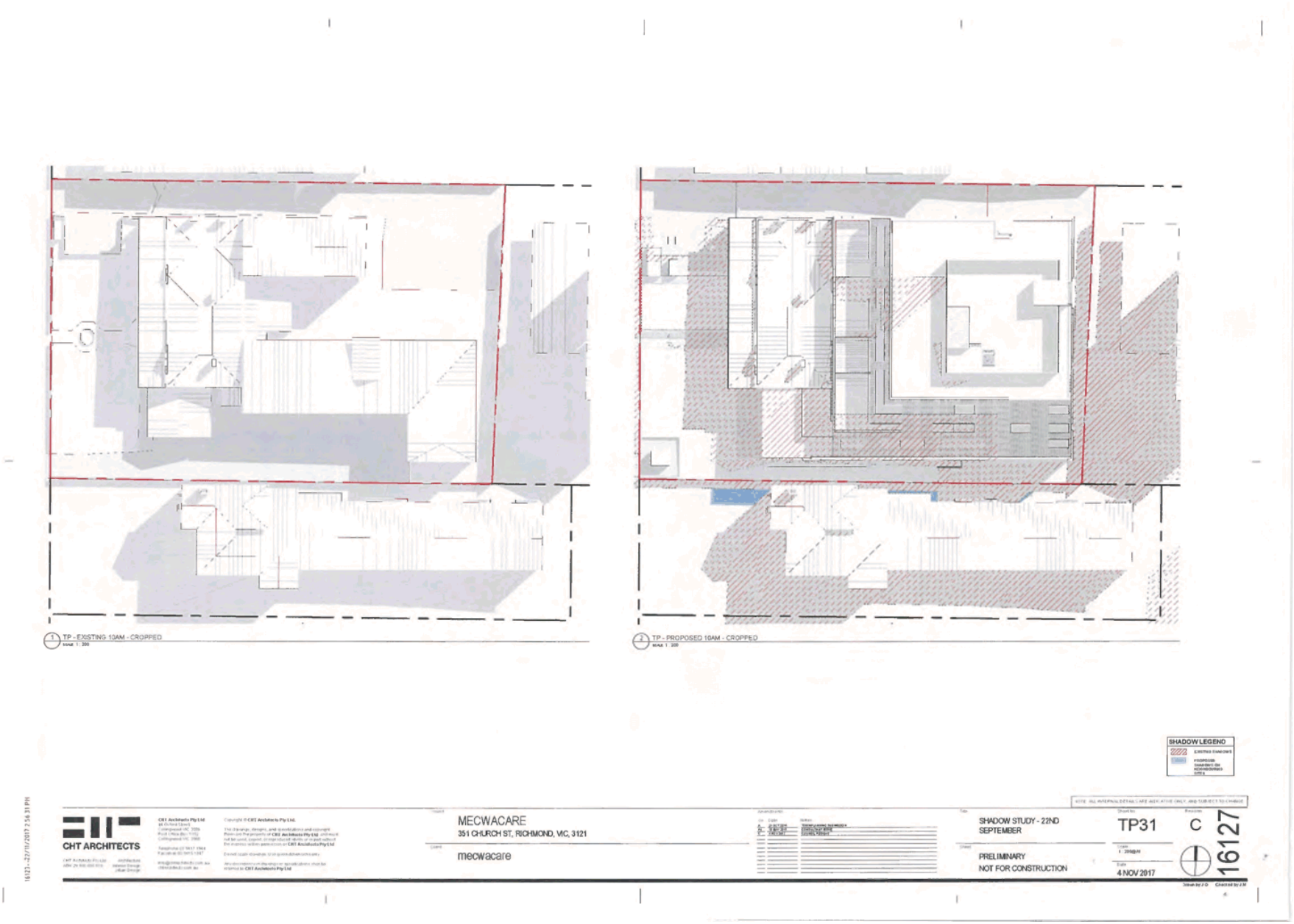

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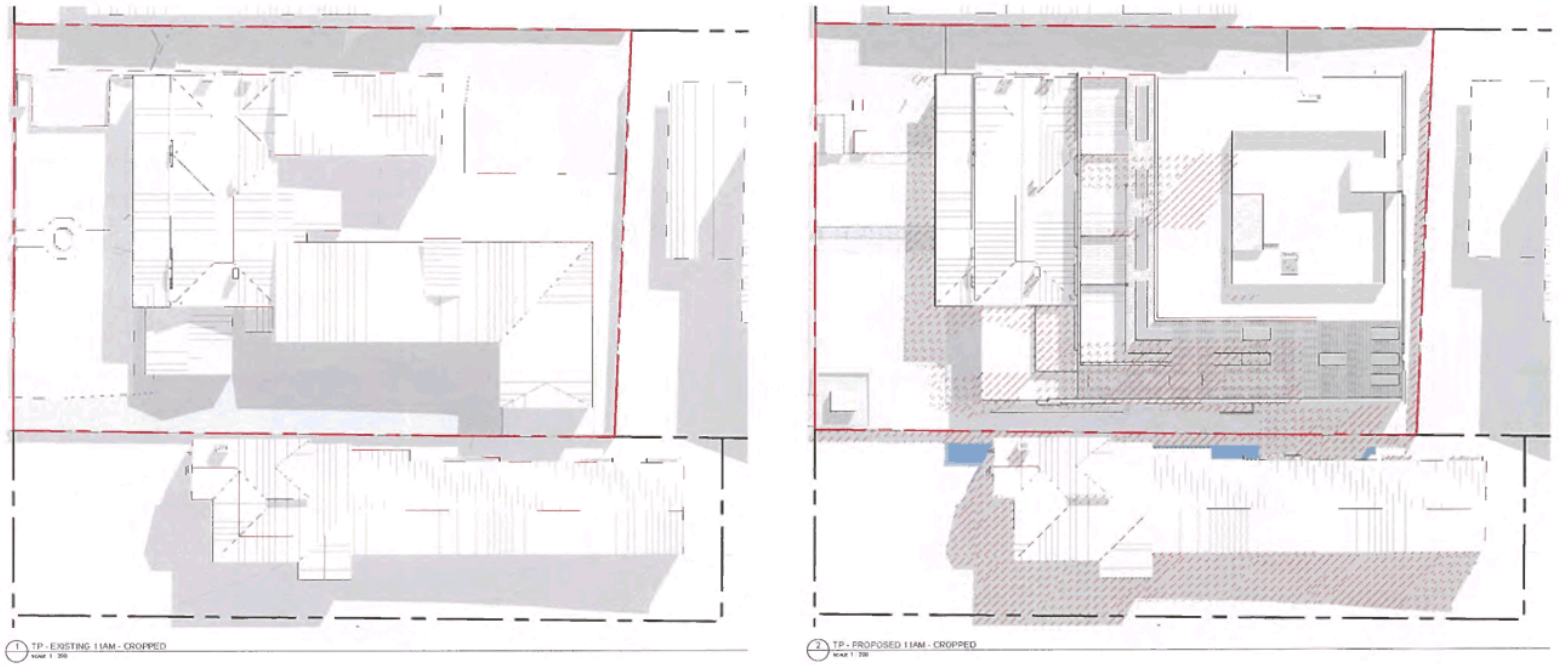

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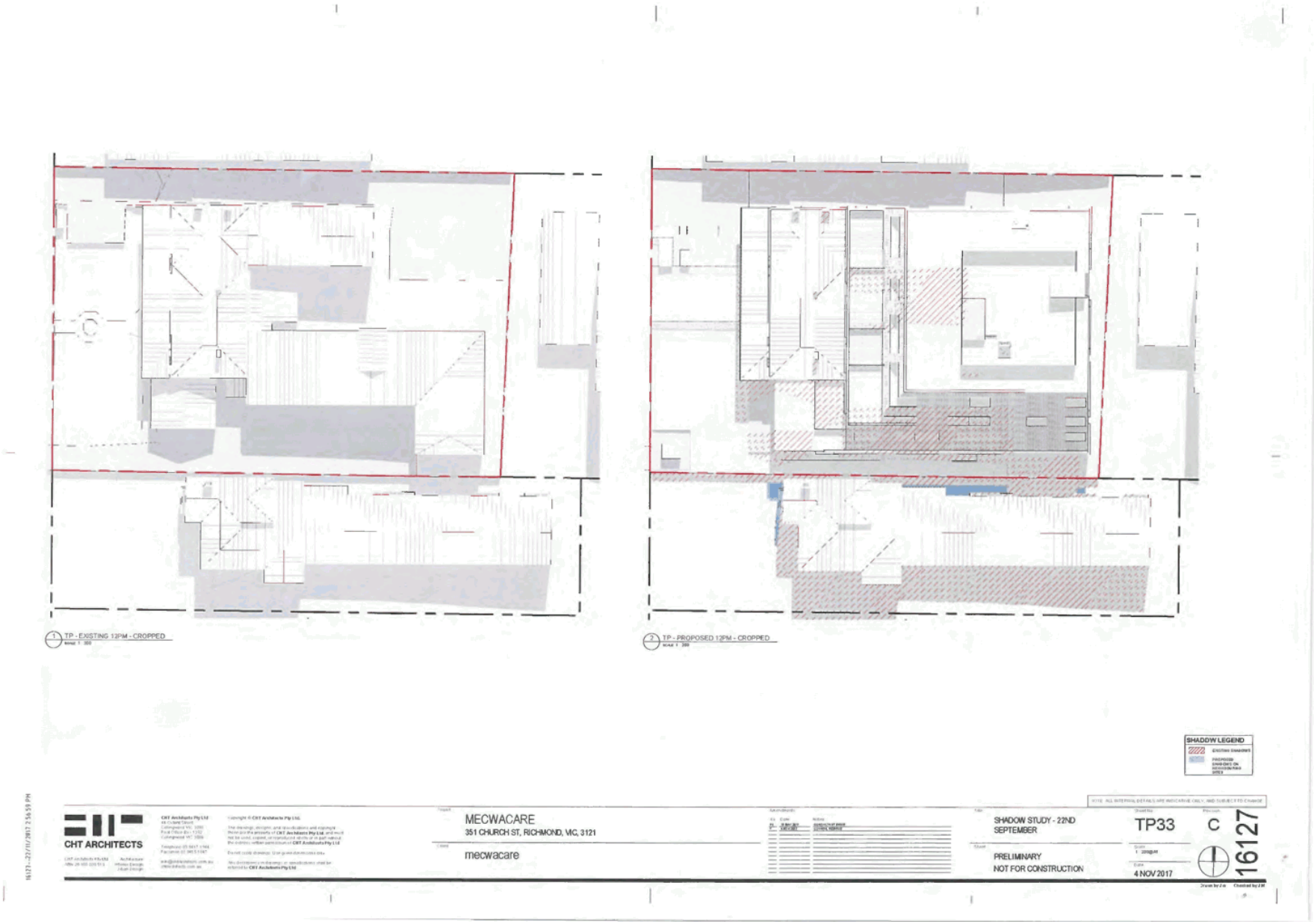
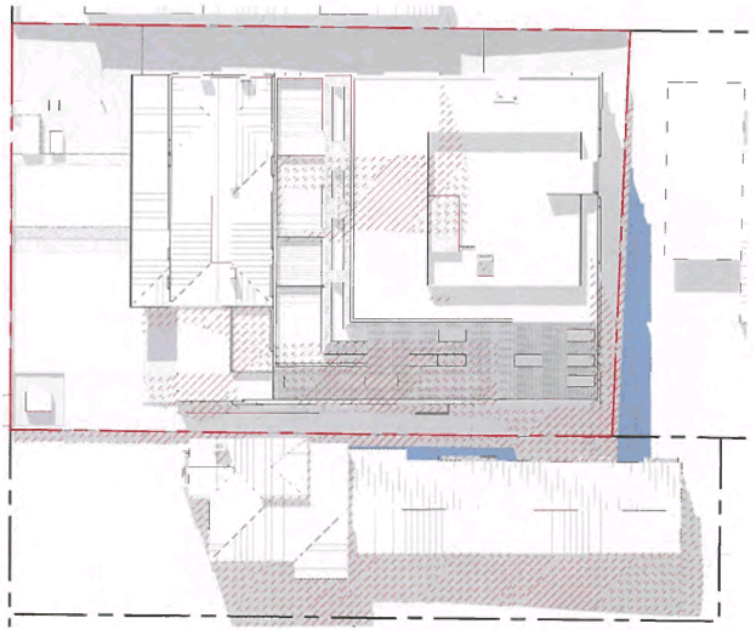


Attachment 2 - 351 Church Street Richmond - architectural plans

2 TP - EXISTING 1PM - CROPPED



5 TP - PROPOSED 1PM - CROPPED

SHADOW LEGEND

CHT ARCHITECTS

MECWACARE
351 CHURCH ST, RICHMOND, VIC, 3121

mecwacare

SHADOW STUDY - 22ND SEPTEMBER

PRELIMINARY
NOT FOR CONSTRUCTION

TP34

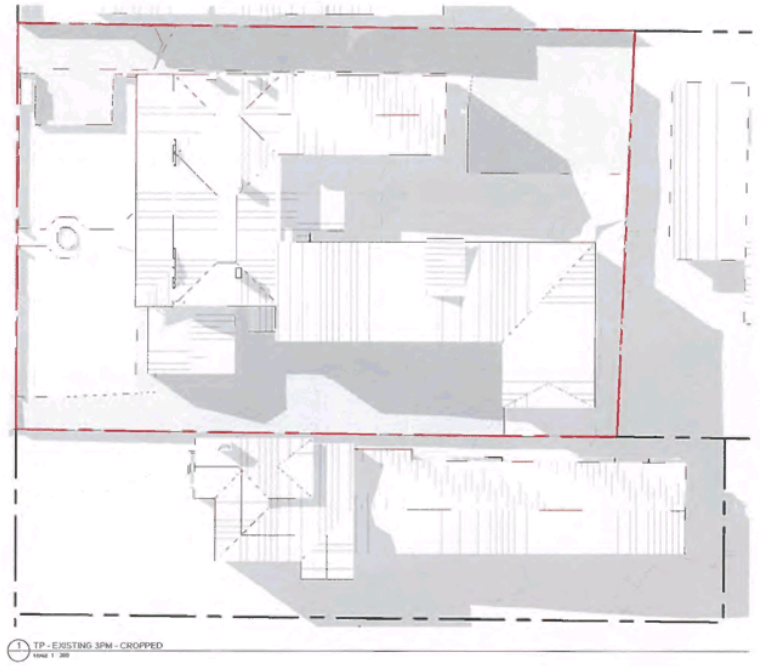
C

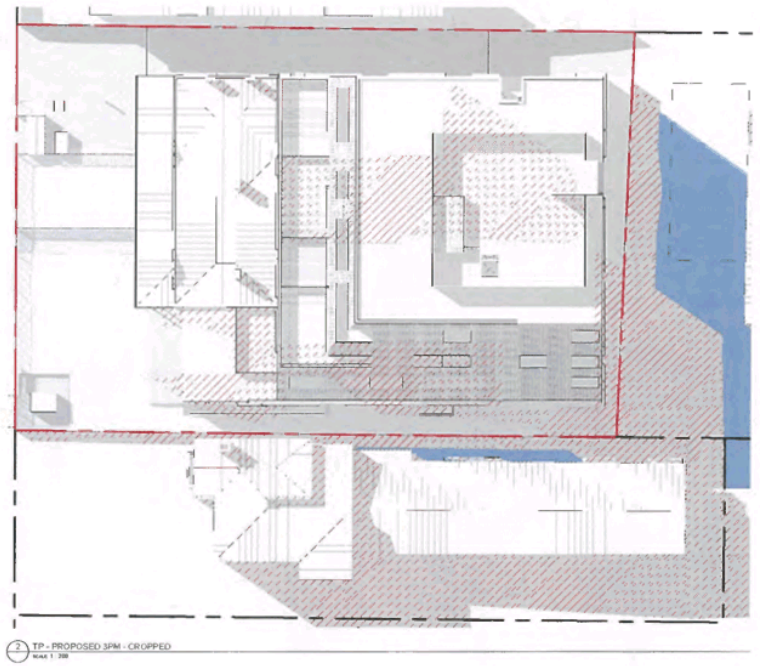
4 NOV 2017

16127

Attachment 2 - 351 Church Street Richmond - architectural plans

Attachment 2 - 351 Church Street Richmond - architectural plans





SHADOW LEGEND

CHT ARCHITECTS

MECWACARE
351 CHURCH ST, RICHMOND, VIC, 3121

mecwacare

SHADOW STUDY - 22ND SEPTEMBER

PRELIMINARY
NOT FOR CONSTRUCTION

TP36 C

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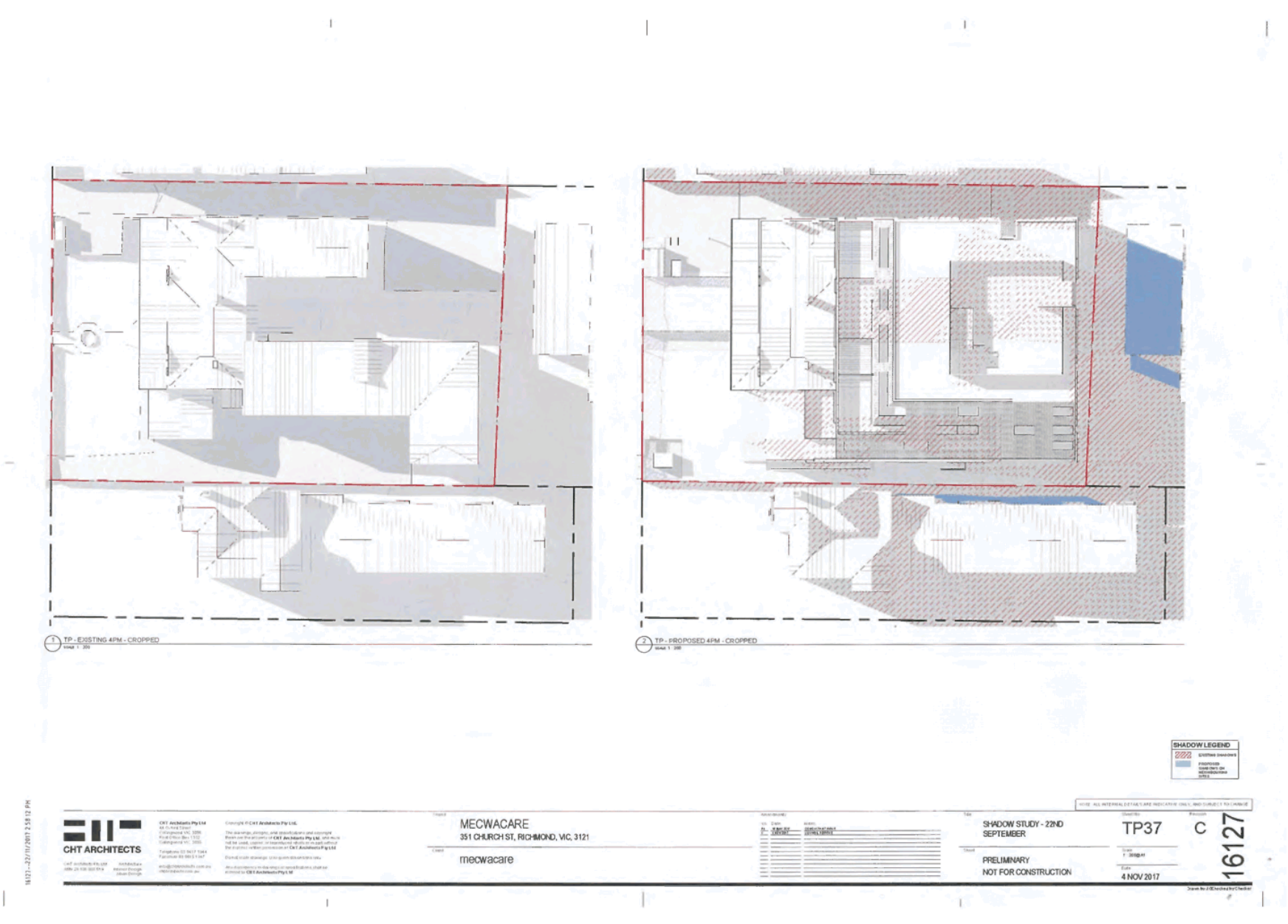
4 NOV 2017

16127

Attachment 2 - 351 Church Street Richmond - architectural plans



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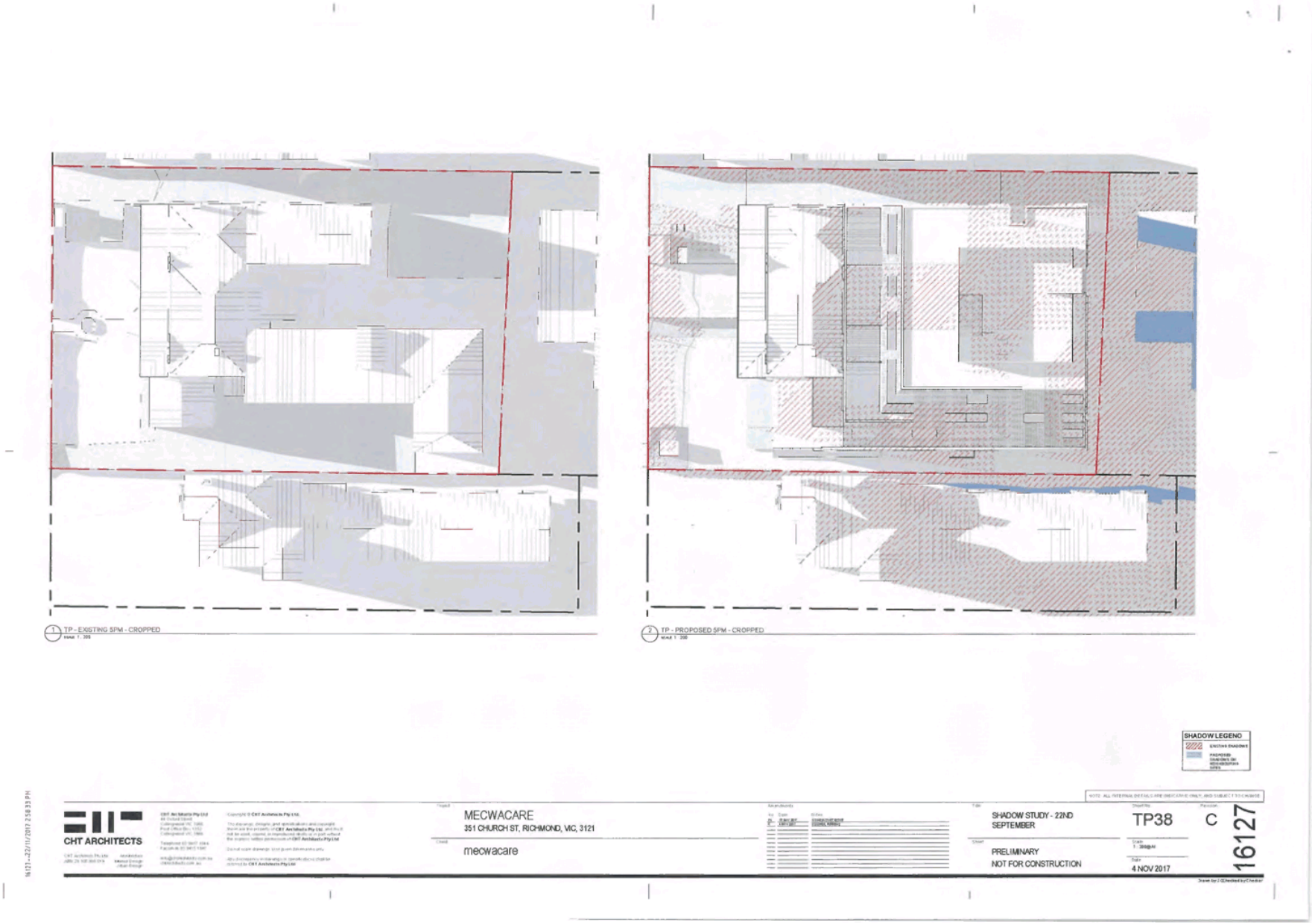

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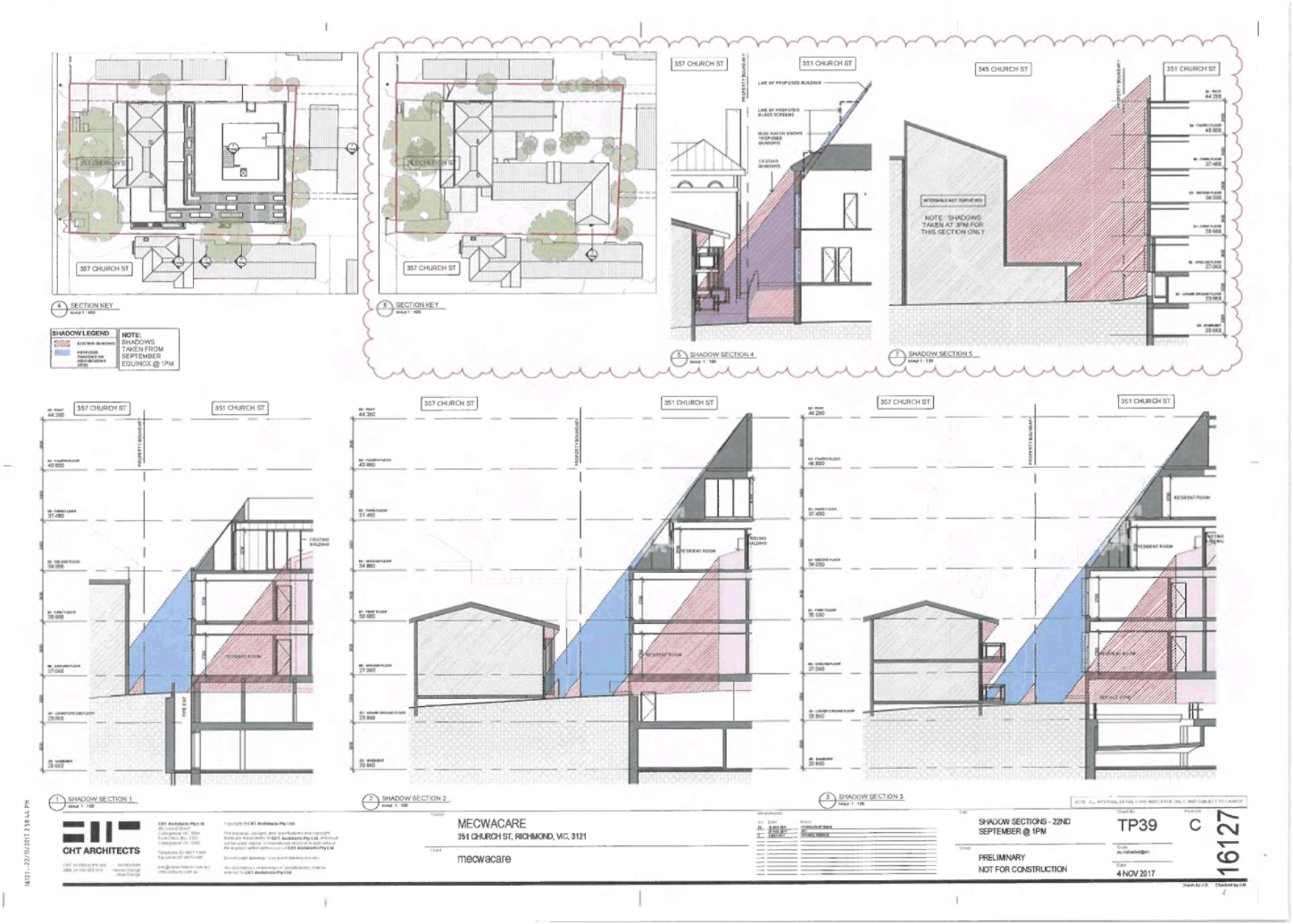

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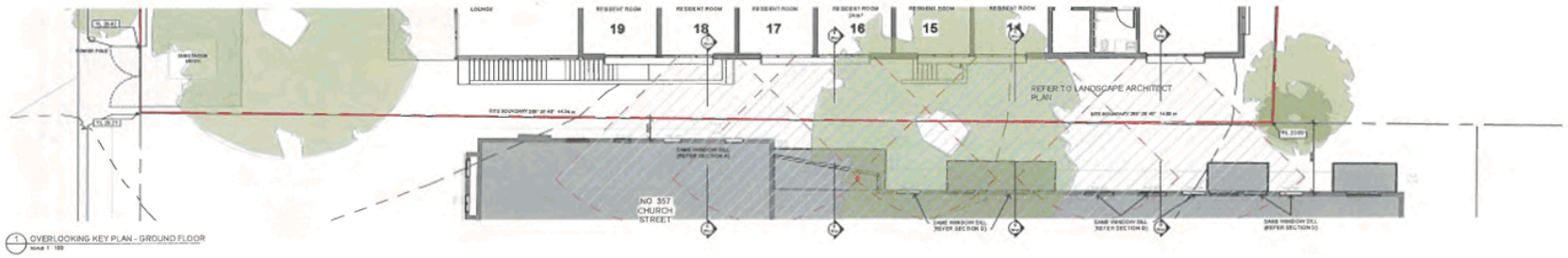
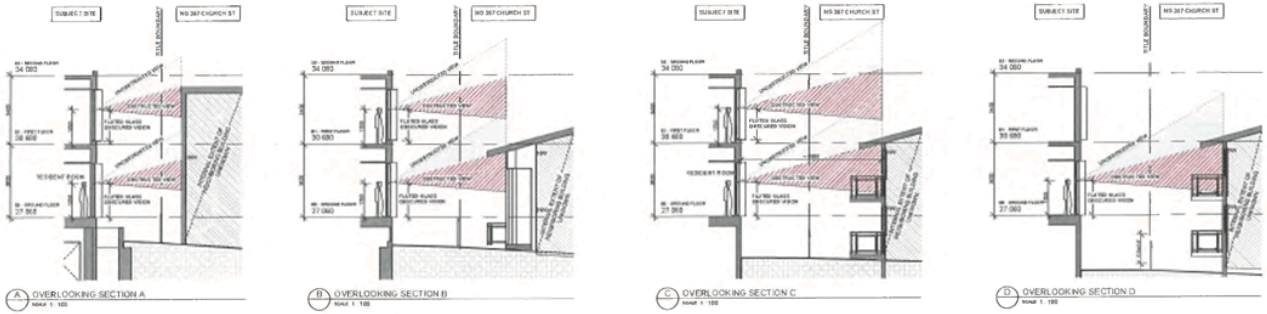




CHT ARCHITECTS

MECWACARE
351 CHURCH ST, RICHMOND, VIC, 3121

mecwacare

OVERLOOKING SECTIONS - SOUTH

PRELIMINARY
NOT FOR CONSTRUCTION

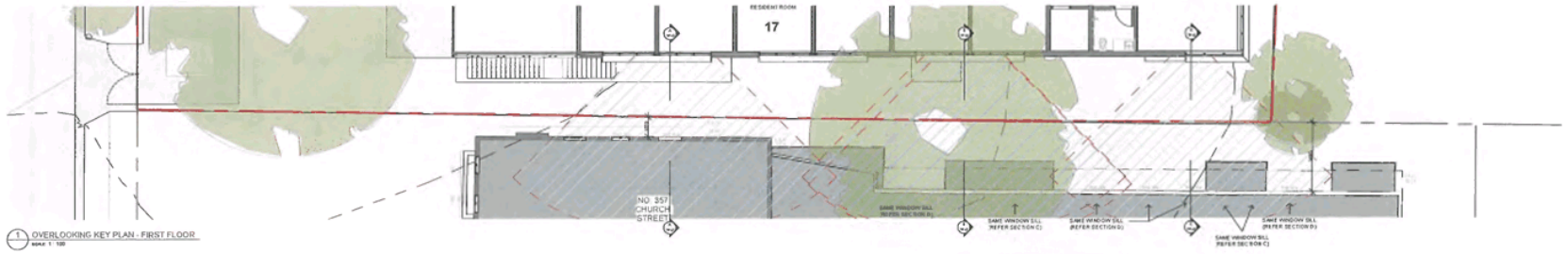
TP41

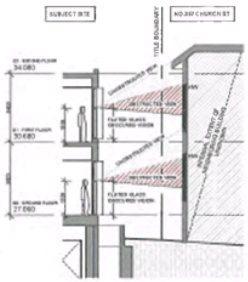
C

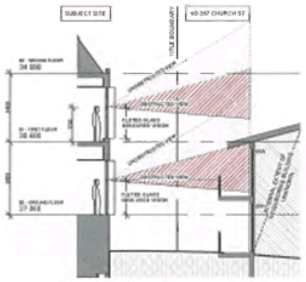
4 NOV 2017

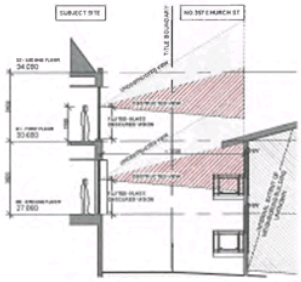
16127

Attachment 2 - 351 Church Street Richmond - architectural plans









CHT ARCHITECTS

MECWACARE
351 CHURCH ST, RICHMOND, VIC, 3121

mecwacare

OVERLOOKING SECTIONS - SOUTH

TP42

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PRELIMINARY
NOT FOR CONSTRUCTION

4 NOV 2017

16127

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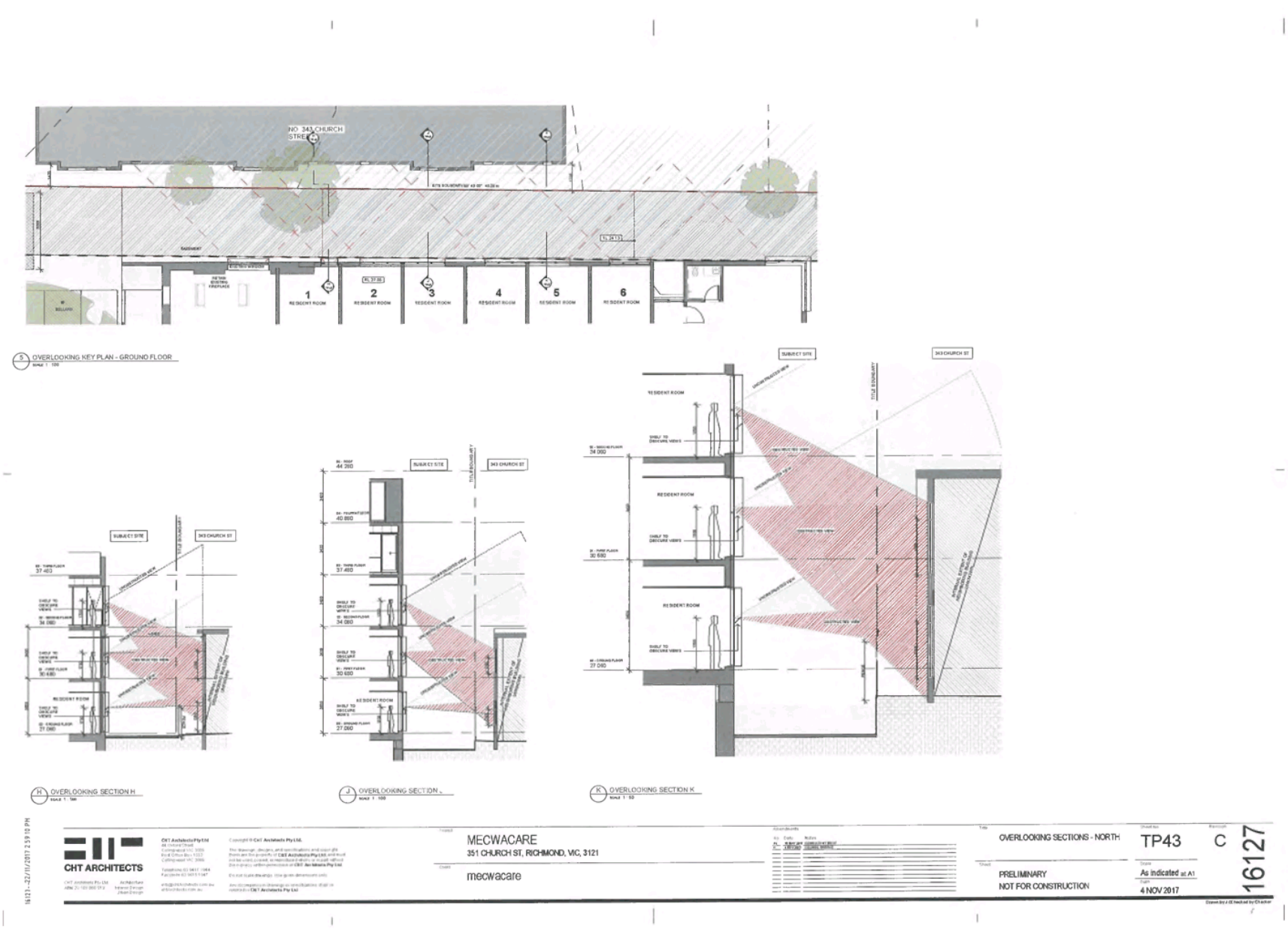
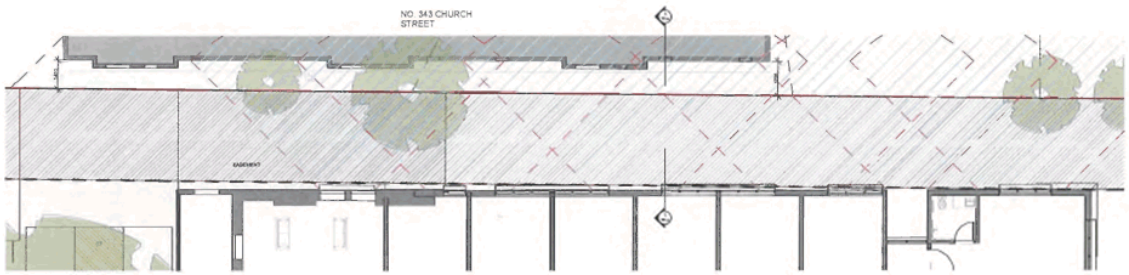


Attachment 2 - 351 Church Street Richmond - architectural plans



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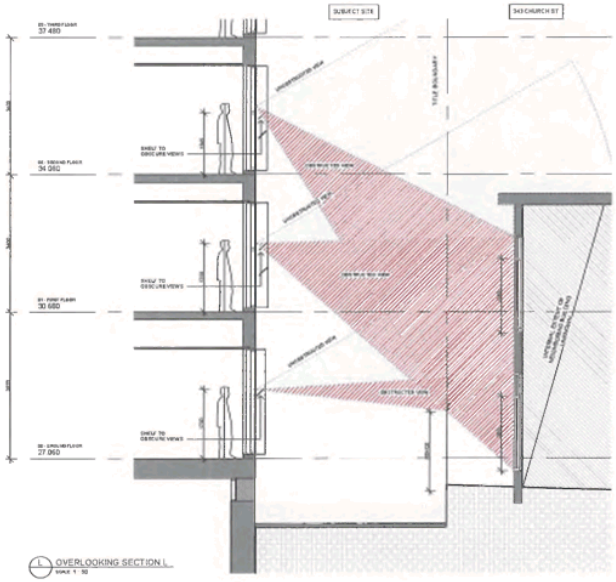

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SCALE 1 : 50

CHT ARCHITECTS

MECWACARE
351 CHURCH ST, RICHMOND, VIC, 3121

mecwacare

OVERLOOKING SECTIONS - NORTH

PRELIMINARY
NOT FOR CONSTRUCTION

TP44

C

4 NOV 2017

16127

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Planning and Environment Act 1987

YARRA PLANNING SCHEME

AMENDMENT C225

EXPLANATORY REPORT

Who is the planning authority?

This amendment has been prepared by the Yarra City Council, which is the planning authority for this amendment.

The Amendment has been made at the request of Urbis Pty Ltd on behalf of Mecwacare.

Land affected by the Amendment

The Amendment applies to 351-353 Church Street, Richmond (the **Land**).

What the amendment does

The Amendment changes the Schedules to Clauses 52.03 "Specific Sites and Exclusions", and Clause 81.01 "Documents Incorporated in this Scheme" to incorporate the site specific control "*351-353 Church Street, Richmond – Incorporated Document, November 2017*" which allows:

> Despite any provision to the contrary, or any inconsistent provision in the Scheme, no planning permit is required for, and no planning provision in the Scheme operates to prohibit, restrict or regulate the use or development of the Land for the purposes of, or related to, the use and development of the Land for the purposes of the Proposal.
>
> The Proposal includes:
>
> - the use of the Land for the purposes of a 'Residential aged care facility' as defined by Clause 74 of the Scheme;
> - partial demolition of the existing heritage building on the Land in accordance with the plans endorsed by Council under this Incorporated Document;
> - variation of the [##insert] easement on the Land to allow for [##insert – details to be confirmed by Council]; and
> - construction of a multi-storey Residential aged care facility on the Land in accordance with the plans endorsed by Council under this Incorporated Document, subject to the conditions in Clause 6 of this Incorporated Document.

Strategic assessment of the Amendment

Why is the Amendment required?

Mecwacare is a leading not-for-profit organization that has been providing care to the Victorian community for over 55 years, offering residential aged care, respite care, in-home support, community programs, and disability and nursing services.

According to targets set by the Department of Social Services for residential aged care beds, Yarra City Council is currently experiencing a lack of supply and is in need of more beds to meet the predicted population increase.

The Amendment will facilitate the redevelopment of the site at 351-353 Church Street, Richmond to allow for a Residential Aged Care facility and help address these shortcomings.

Specifically the amendment is required to enable buildings and works to exceed the mandatory height control of 9 metres (10 metres where the slope of the land exceeds a

gradient of 2.5 degrees over an 8m section) in the Neighbourhood Residential Zone (which applies to the Land) at Clause 32.09 of the Yarra Planning Scheme.

The amendment proposes a site-specific exclusion under Clause 52.03 and to insert an Incorporated Document at Clause 81 of the Yarra Planning Scheme to enable the development proposal. The Incorporated Document allows the proposed building plans subject to a range of conditions. If approved the Incorporated Document would operate like a permit for the proposed development.

How does the Amendment implement the objectives of planning in Victoria?

The amendment implements the following applicable objectives for planning in Victoria as outlined in Section 4 of the *Planning and Environment Act 1987* (the Act):

(a) provide for the fair, orderly, economic and sustainable use, and development of land

(c) secure a pleasant, efficient and safe working, living and recreational environment for all Victorians and visitors to Victoria

(d) conserve and enhance those buildings, areas or other places which are of scientific, aesthetic, architectural or historical interest, or otherwise of special cultural value

(g) balance the present and future interests of all Victorians..

The amendment will enable the use and development of the subject land in a way that better reflects these objectives and realises desired outcomes for the land.

The Amendment further implements the objectives of planning in Victoria by implementing the relevant parts of Plan Melbourne, the SPPF, LPPF and MSS of the Yarra Planning Scheme.

How does the Amendment address any environmental, social and economic effects?

The amendment has positive environmental, social and economic effects by enabling the redevelopment of a much needed facility in a strategic location.

An Arboricultural assessment and report has been prepared by Tree Logic in relation to the redevelopment of 351-353 Church Street, Richmond. This assessment found that the existing four significant trees on site, within the front setback of the existing buildings, have a high or moderate retention value. The proposal includes the retention and protection of these four trees.

A Heritage Impact Statement has also been prepared, which assesses the impact of the proposal, on the heritage place. In summary, the Statement supports the proposed partial demolition of the existing building fabric and the proposed redevelopment of the remainder of the site.

The amendment will result in a net community benefit in facilitating residential aged care development in an area that is readily serviced by existing infrastructure. Redevelopment of the land at 351-353 Church Street, Richmond will further stimulate regeneration of the local urban area. It will deliver environmental, social and economic benefits by enabling greater housing diversity and providing much needed aged care rooms within the municipality.

Does the Amendment address relevant bushfire risk?

The amendment will not have any significant impact on bushfire risk.

Does the Amendment comply with the requirements of any Minister's Direction applicable to the amendment?

The amendment is consistent with the Ministerial Direction on the Form and Content of Planning Schemes under section 7(5) of the Act. The following Directions under Section 12 of the Act are also relevant:

- Direction No. 9 – Metropolitan Strategy

The amendment has had proper regard to the Metropolitan Strategy and is closely aligned with the Key Directions for 'a more compact city' and 'a great place to be'.

The amendment is consistent with Policy 1.3 which seeks to 'Locate a substantial proportion of new housing in or close to activity centres and other strategic redevelopment sites that offer good access to services and transport.'.

The amendment is also consistent with Policy 5.1 which seeks to 'Promote good urban design to make the environment more liveable and attractive' and Policy 5.5 which seeks to 'Promote excellent neighbourhood design to create attractive, walkable and diverse communities'.

- Direction No. 11 – Strategic Assessment of Amendments

This direction ensures a comprehensive strategic evaluation of a planning scheme amendment and the outcomes it produces. The amendment complies with the requirements of Direction No. 11 as set out in the explanatory report.

How does the Amendment support or implement the State Planning Policy Framework and any adopted State policy?

The amendment to facilitate the development of the land for a residential aged care facility supports the State Planning Policy Framework, including:

Clause 9 – Plan Melbourne, as outlined above.

Clause 11 – Settlement, through anticipating and responding to the needs of existing and future communities.

Clause 15 – Built Form which seeks to achieve high quality architectural outcomes that contribute positively to the local urban environment, enhance the public realm, ensures development equity and limits detrimental impact.

Clause 16 – Housing, through providing for housing diversity and promoting a housing market that meets community needs.

Clause 19 – Infrastructure, which promotes the efficient use of existing infrastructure. The Amendment will facilitate new development which capitalises on existing community, social, cultural and development infrastructure and facilities.

How does the Amendment support or implement the Local Planning Policy Framework, and specifically the Municipal Strategic Statement?

Consistent with the policy objectives of the Municipal Strategic Statement and Local Planning Policy Framework, this Amendment will:

- Aid in accommodating 'a diverse range of people, including families, the aged, disabled, and those who are socially or economically disadvantaged' (Clause 21.03).
- Further 'encourage residential development which allows people to age in their existing homes and communities by supporting a range of housing types' and 'support the development of new residential care facilities' (Clause 21.04)
- Help 'conserve, protect and enhance identified sites and areas of heritage significance' (Clause 21.05).
- 'Ensure development is designed having particular regard to its urban context and specifically designed following a thorough analysis of the site, the neighbouring properties and its environs' (Clause 21.05)

Does the Amendment make proper use of the Victoria Planning Provisions?

The amendment makes proper use of the Victoria Planning Provisions by updating the schedules to Clauses 52.03 and 81.01 to insert '*351-353 Church Street, Richmond – Incorporated Document, November 2017'* into the Yarra Planning Scheme.

How does the Amendment address the views of any relevant agency?

The Amendment addresses the views of relevant agencies which have been sought during the preparation of the Amendment.

Does the Amendment address relevant requirements of the Transport Integration Act 2010?

The *Transport Integration Act 201*0 establishes a framework for the provision of an integrated and sustainable transport system in Victoria. While the proposed amendment is not considered to have a direct impact on the Transport Integration Act or transport system, it will facilitate a development which adjoins a tram route and is close to public transport systems and established road networks.

Resource and administrative costs

- **What impact will the new planning provisions have on the resource and administrative costs of the responsible authority?**

The amendment will not impose any significant impact on the administrative costs of the responsible authority

Where you may inspect this Amendment

The amendment is available for public inspection, free of charge, during office hours at the following places:

City of Yarra
Richmond Town Hall
333 Bridge Road, Richmond

The Amendment will also be available at the City of Yarra website at:

https://www.yarracity.vic.gov.au/the-area/yarras-future/yarra-planning-scheme

The amendment can also be inspected free of charge at the Department of Planning and Community Development website at:

www.dpcd.vic.gov.au/planning/publicinspection

11.7 Station Street, Carlton North - Temporary Road Closure

Trim Record Number: D17/189665
Responsible Officer: Group Manager Chief Executive's Office

Purpose

1. To, in the matter of the proposal to close Station Street, Carlton North at the intersection with Princes Street, on a temporary basis for the purposes of undertaking a traffic diversion experiment, now:
   (a) present to Council the outcomes of recent formal public consultation process; and
   (b) in accordance with Section 223 of the *Local Government Act* 1989, enable Council to hear verbal presentations by submitters, in support of their formal written submissions.

Background

2. Council on 3 October 2017, resolved:

   "That Council:

   (a) notes the response from VicRoads in its letter to Council dated 19 September 2017, that "VicRoads has no objection to the proposed trial closure of Station Street subject to the local community being supportive of the closure";

   (b) notes advice previously provided to the community indicating the opportunity to make submissions on the matter following receipt of the Vic Roads report, in accordance with Schedule 11, Section 10(1)(c) of the Local Government Act 1989 (the Act);

   (c) proceed with the consultation process for a trial road closure as outlined in the officers report; and

   (d) consider a further report on the matter, to include the assessment of submissions, and receive submissions for final determination at its first Council meeting in December 2017 and that all parties that have expressed interest in this matter be notified at least one week in advance of the report being presented to Council."

3. The primary purpose of the above resolution was to authorise the undertaking of a wide community consultation in respect of a proposal to implement a temporary closure of Station Street at Princess Street, Carlton North.

4. For context, a note of Council's previous determinations in respect of Station Street at the intersection of Prince's Street, is referenced in ***Attachment 1***.

5. The referenced Independent Traffic Impact Assessment (TIA) Study is appended for noting and context – refer ***Attachment 2.***

External Consultation

6. The consultation process and public notice thereof, was undertaken in line with Section 223 of the *Local Government Act* 1989 which requires that the public has at least 28 days' notice in which to provide a submission to Council on the proposal.

7. Consultation on the proposed temporary closure of Station Street was undertaken between 19 October 2017 and 22 November 2017.

8. A total of 2,744 letters were distributed by Council on 17 October 2017 to all properties and owner non-occupiers in the Carlton North Local Area Place Making area (LAPM 2).

9. This area is bounded by (and including) Park Street, Nicholson Street (west side of street), Princes Street (north side of street) and Lygon Street (east side of street).

10. The letter invited residents, property owners and business operators to provide a submission to Council on the proposal via either an online survey or a posted or emailed written submission.

11. The community were also advised of the public consultation process via a public notice in The Leader and the Council's Facebook page.

12. The invitation to present submissions, specifically requested respondents to indicate:

    (a) Do they support / do not support a trial road closure?

    (b) Some reasoning for their preference;

    (c) If they supported a trial road closure, did they prefer a:

        (i) 6 months trial;

        (ii) 12 months trial; or

        (iii) A different prescribed period of trial?

Individual Formal Submissions:

13. Overall, 136 individual responses were received for this consultation. These comprised:

    (a) 101 responses from residents or owner non-occupiers in Carlton North;

    (b) 17 responses from respondents who live outside of Carlton North but travel through the area by bicycle;

    (c) 9 responses from respondents who live outside of Carlton North but work or have children attend school in Carlton North; and

    (d) 9 responses from which the connection to Carlton North could not be identified from the response.

14. The submissions can be found in full in **Attachment 5** and has not been edited (aside from the redaction of personally identifying information in a small number of cases).

Consultation results – all respondents

15. Of **all** respondents:

    (a) 31 (23%) support the temporary closure; and

    (b) 105 (77%) oppose the temporary closure.

16. Responses to the preferred length of any temporary closure were:

    (a) 6 months (26%);

    (b) 12 months (18%); and

    (c) No Answer (56%).

17. Of the 31 respondents **who support** the temporary road closure:

    (a) 26 (84%) respondents live or own a property in Carlton North;

    (b) 2 (7%) respondents live outside Carlton North, but commute or regularly travel through the area by bicycle;

    (c) 1 (3%) respondent lives outside Carlton North, but either works or has children attend a school or child care in Carlton North; and

    (d) 2 (6%) respondents from which the connection to Carlton North could not be identified from the submission.

18. Of those respondents **who supported** the temporary road closure, the stated preferred length of any temporary closure was:

    (a) 6 months (19%);

    (b) 12 months (74%); and

    (c) No Answer (7%).

19. The key reasons given **for support** of the temporary road closure include:

Respondent feedback comments	Officer comment
Concerns regarding the number of vehicles which use Station Street **(36%)**	Independent TIA states that there is not an existing issue with regard to the level and speed of traffic on Station Street, between Lee Street and Princes Street, with 1,000 (daily), 100 (AM peak) and 12 (PM peak) vehicles using this section of the street and a recorded 85th percentile speed of 30.6km/hr.
A temporary closure is required to see if there are any issues or not **(21%)**	Noted.
It will reduce rat running in Carlton North **(16%)**	Independent TIA identified through an origin and destination survey that most traffic using Station Street between Lee Street and Princes Street is locally generated.
There were no apparent issues during the Melbourne Water closure **(14%)**	Acknowledged. However, it is noted that there were multiple other significant road closures at this time which impacted on traffic flows in a number of streets in Carlton North and North Fitzroy. The benefits or impacts of a Station Street closure cannot properly be isolated from these other closures.
Support the temporary closure, but issues resulting from more traffic on other streets need to be addressed **(11%)**	There are no current proposals being considered outside of the temporary closure of Station Street. Such a response may indicate the desirability of an overall LAPM study to effectively consult and establish an overall impact of a closure.
It will have a positive impact for children walking to school **(2%)**	Independent TIA identifies that the closure is unlikely to reduce vehicle movements in the area, rather redistribute existing vehicles onto Lee Street and Canning Street, which are also key routes to schools. It is noted though the level of traffic on Station Street is low.

20. Of the 105 respondents **who <u>oppose</u>** the temporary road closure:
    (a) 75 (71%) respondents live or own a property in Carlton North;
    (b) 15 (14%) respondents live outside Carlton North, but commute or regularly travel through the area by bicycle;
    (c) 8 (8%) respondents live outside Carlton North, but either work or have children attend a school or child care in Carlton North; and
    (d) 7 (7%) respondents where the connection to Carlton North could not be identified from submissions.
21. Of those respondents **who <u>opposed</u>** the temporary road closure, the stated preferred length of any temporary closure was:
    (a) 6 months (29%);
    (b) 12 months (1%); and
    (c) No Answer (70%).
22. The key reasons given for **<u>opposition</u>** to the temporary road closure:

Respondent feedback comments	Officer comment
It will reduce the amenity of Canning Street as a cycle route / less safe for cyclists and pedestrians / school children on Canning Street **(38%)**	Independent TIA states that increased traffic unlikely to have a significant impact on cyclists and pedestrians, although there is potential for occasional increased conflict between cyclists and vehicles at Canning / Davis intersection (cyclists) and vehicles / pedestrians at Canning / Princes intersection, with additional vehicles on Canning Street. Council's Strategic Transport Unit notes that additional vehicles on Canning Street will reduce the amenity of the cycle route in comparison to existing conditions.
It will result in more vehicles and increased congestion on other streets in Carlton North **(37%)**	Independent TIA states that there is not expected to be any significant impacts on the operation of other roads in Carlton North. The proposal will likely transfer the current low level impacts on Station Street onto other streets. Whilst the number of potentially redistributed vehicles from Station Street onto other streets is low (i.e. one to two additional vehicles per minute), it could increase current AM peak hour flows on sections of Lee Street (136 westbound vehicles), Canning Street (75 southbound vehicles) and Davis Street (27 westbound vehicles) by 74%, 72% and 152% respectively in comparison to existing conditions noted above. The independent TIA also notes that it is expected that queuing on Canning Street back from Princes Street may occasionally extend to Davis Street, which at times, may impede the right turn movement from Canning Street into Davis Street.
It has no wider benefit to Carlton North outside of the southern end of Station Street **(12%)**	Independent TIA identifies that the closure is unlikely to reduce vehicle movements in the area, rather redistribute existing vehicles onto Lee Street and Canning Street. It is noted though the level of traffic on Station Street is low.
It will result in longer travel times and inconvenience residents who require access to Princes Street **(7%)**	Independent TIA states that there is not expected to be any significant traffic impacts on other roads in Carlton North. Vehicle travel times will be longer given extra travel distance and delays with crossing pedestrians at Canning / Princes intersection.
There is no existing traffic issue on Station Street in comparison to other streets in Carlton North **(5%)**	Independent TIA states that there is not an existing issue with regard to level and speed of traffic on Station Street, between Lee Street and Princes Street, with 1,000 (daily), 100 (AM peak) and 12 (PM peak) vehicles using this section of the street and a recorded 85th percentile speed of 30.6km/hr. The number of vehicles using Station Street is lower than other north-south streets in the area, with the exception of Canning Street.
Proposal will not reduce traffic flows on Station Street to the north of Lee Street **(1%)**	Independent TIA notes that 58% of traffic that uses Station Street between Newry Street and Lee Street turns right into Lee Street, towards Canning Street. Proposal is unlikely to reduce the level of traffic using Station Street to the north of Lee Street.

Consultation results – Station Street residents

23. Of **Station Street** respondents, 14 (48%) support the temporary closure and 15 (52%) oppose the temporary closure.

24. The key reasons given by Station Street residents **who support** the temporary road closure include:

Respondent feedback comments	Officer comment
Concerns regarding the number of vehicles which use Station Street **(48%)**	Independent TIA states that there is not an existing issue with regard to level and speed of traffic on Station Street, between Lee Street and Princes Street, with 1,000 (daily), 100 (AM peak) and 12 (PM peak) vehicles using this section of the street and a recorded 85th percentile speed of 30.6km/hr.
There were no apparent issues during the Melbourne Water closure **(14%)**	Agreed. However, it is noted that there were multiple other road closures at this time which impacted on traffic flows in a number of streets in Carlton North and North Fitzroy. The benefits or impacts of a Station Street closure cannot properly be isolated from these other closures.
Support the temporary closure, but issues resulting from more traffic on other streets need to be addressed **(14%)**	There are no current proposals being considered outside of the temporary closure of Station Street. Such a response may indicate the desirability of an overall LAPM study to effectively consult and establish an overall impact of a closure.
A temporary closure is required to see if there are any issues or not **(14%)**	Noted.
It will reduce rat running in Carlton North **(5%)**	Independent TIA identified through an origin and destination survey that most traffic using Station Street between Lee Street and Princes Street is locally generated.
It will have a positive impact for children walking to school **(5%)**	Independent TIA identifies that the closure is unlikely to reduce vehicle movements in the area, rather redistribute existing vehicles onto Lee Street and Canning Street, which are also key routes to schools. Also Independent TIA notes that currently, 58% of traffic that uses Station Street between Newry Street and Lee Street turns right into Lee Street, towards Canning Street. Proposal is unlikely to reduce the level of traffic using Station Street to the north of Lee Street.

25. The key reasons given by Station Street residents **who <u>oppose</u>** the temporary road closure include:

Respondent feedback comments	Officer comment
It will result in more vehicles and increased congestion on other streets in Carlton North **(26%)**	Independent TIA states that there is not expected to be any significant impacts on the operation of other roads in Carlton North. The proposal will likely transfer the current low level impacts on Station Street onto other streets. Whilst the number of potentially redistributed vehicles from Station Street onto other streets is low (i.e. one to two additional vehicles per minute), it could increase current AM peak hour flows on sections of Lee Street (136 westbound vehicles), Canning Street (75 southbound vehicles) and Davis Street (27 westbound vehicles) by 74%, 72% and 152% respectively in comparison to existing conditions noted above. Independent TIA also notes that it is expected that queuing on Canning Street back from Princes Street may occasionally extend to Davis Street, which at times, may impede the right turn movement from Canning Street into Davis Street.

It will reduce the amenity of Canning Street as a cycle route / less safe for cyclists and pedestrians/school children on Canning Street **(26%)**	Independent TIA states that increased traffic unlikely to have a significant impact on cyclists and pedestrians, although there is potential for occasional increased conflict between cyclists and vehicles at Canning/Davis intersection (cyclists) and vehicles/pedestrians at Canning/Princes intersection, with additional vehicles on Canning Street. Council's Strategic Transport Unit considers that additional vehicles on Canning Street will reduce the amenity of the cycle route in comparison to existing conditions.
It has no wider benefit to Carlton North outside of the southern end of Station Street **(18%)**	Independent TIA identifies that the closure is unlikely to reduce vehicle movements in the area, rather redistribute existing vehicles onto Lee Street and Canning Street. It is noted though the level of traffic on Station Street is low.
There is no existing traffic issue on Station Street in comparison to other streets in Carlton North **(13%)**	Independent TIA states that there is not an existing issue with regard to level and speed of traffic on Station Street, between Lee Street and Princes Street, with 1,000 (daily), 100 (AM peak) and 12 (PM peak) vehicles using this section of the street and a recorded 85th percentile speed of 30.6km/hr. The number of vehicles using Station Street is lower than other north-south streets in the area, with the exception of Canning Street.
It will result in longer travel times and inconvenience residents who require access to Princes Street **(13%)**	Independent TIA states that there is not expected to be any significant traffic impacts on other roads in Carlton North. Vehicle travel times will be longer given extra travel distance and delays with crossing pedestrians at Canning/Princes intersection.
Proposal will not reduce traffic flows on Station Street to the north of Lee Street **(5%)**	Independent TIA notes that 58% of traffic that uses Station Street between Newry Street and Lee Street turns right into Lee Street, towards Canning Street. Proposal is unlikely to reduce the level of traffic using Station Street to the north of Lee Street.

Consultation results – respondents who live nearby on potentially impacted streets

26. The properties in the closest vicinity of Station Street / Princes Street are the streets where there is a higher likelihood of some traffic displacement with a Station Street closure in place.

27. This area is bound by Rathdowne Street (between Lee Street and Princes Street), Lee Street (between Rathdowne Street and Nicholson Street), Nicholson Street (between Lee Street and Princes Street) and Princes Street (between Rathdowne Street and Nicholson Street).

28. Of residents who live on these identified streets, 3 (13%) support the temporary closure and 20 (87%) oppose the temporary closure.

29. The key reasons given by these residents who **<u>support</u>** the temporary road closure include:

Respondent feedback comments	Officer comment
Concerns regarding the number of vehicles which use Station Street **(60%)**	Independent TIA states that there is not an existing issue with regard to level and speed of traffic on Station Street, between Lee Street and Princes Street, with 1,000 (daily), 100 (AM peak) and 12 (PM peak) vehicles using this section of the street and a recorded 85th percentile speed of 30.6km/hr.
There were no apparent issues during the Melbourne Water closure **(20%)**	Agreed. However, noted that there were multiple other road closures at this time which impacted on traffic flows in Carlton North and North Fitzroy. The benefits or impacts of Station Street closure cannot be isolated from these other closures.

It will have a positive impact for children walking to school **(20%)**	Independent TIA identifies that the closure is unlikely to reduce vehicle movements in the area, rather redistribute existing vehicles onto Lee Street and Canning Street, which are also key routes to schools. It is noted though the level of traffic on Station Street is low.

30. The key reasons given by these residents who **oppose** the temporary road closure include:

Respondent feedback comments	Officer comment
It will reduce the amenity of Canning Street as a cycle route / less safe for cyclists and pedestrians/school children on Canning Street **(36%)**	Independent TIA states that increased traffic unlikely to have a significant impact on cyclists and pedestrians, although there is potential for occasional increased conflict between cyclists and vehicles at Canning/Davis intersection (cyclists) and vehicles/pedestrians at Canning/Princes intersection, with additional vehicles on Canning Street. Council's Strategic Transport Unit considers that additional vehicles on Canning Street will reduce the amenity of the cycle route in comparison to existing conditions.
It will result in more vehicles and increased congestion on other streets in Carlton North **(24%)**	Independent TIA states that there is not expected to be any significant impacts on the operation of other roads in Carlton North. The proposal will likely transfer the current low level impacts on Station Street onto other streets. Whilst the number of potentially redistributed vehicles from Station Street onto other streets is low (i.e. one to two additional vehicles per minute), it could increase current AM peak hour flows on sections of Lee Street (136 westbound vehicles), Canning Street (75 southbound vehicles) and Davis Street (27 westbound vehicles) by 74%, 72% and 152% respectively in comparison to existing conditions noted above. Independent TIA also notes that it is expected that queuing on Canning Street back from Princes Street may occasionally extend to Davis Street, which at times, may impede the right turn movement from Canning Street into Davis Street.
There is no existing traffic issue on Station Street in comparison to other streets in Carlton North **(19%)**	Independent TIA states that there is not an existing issue with regard to level and speed of traffic on Station Street, between Lee Street and Princes Street, with 1,000 (daily), 100 (AM peak) and 12 (PM peak) vehicles using this section of the street and a recorded 85th percentile speed of 30.6km/hr. The number of vehicles using Station Street is lower than other north-south streets in the area, with the exception of Canning Street.
It has no wider benefit to Carlton North outside of the southern end of Station Street **(15%)**	Independent TIA identifies that the closure is unlikely to reduce vehicle movements in the area, rather redistribute existing vehicles onto Lee Street and Canning Street. It is noted though the level of traffic on Station Street is low.
It will result in longer travel times and inconvenience residents who require access to Princes Street **(4%)**	Independent TIA states that there is not expected to be any significant traffic impacts on other roads in Carlton North. Vehicle travel times will be longer given extra travel distance and delays with crossing pedestrians at Canning/Princes intersection.
Proposal will not reduce traffic flows on Station Street to the north of Lee Street **(2%)**	Independent TIA notes that 58% of traffic that uses Station Street between Newry Street and Lee Street turns right into Lee Street, towards Canning Street. Proposal is unlikely to reduce the level of traffic using Station Street to the north of Lee Street.

Summary of key findings

31. Overall, the majority of respondents 105 (77%) oppose the proposed temporary road closure. The opponents to the proposal are largely made up of Carlton North residents, but also include commuter cyclists and parents of children who attend schools in the area.

32. For Carlton North residents or property owners only, the majority of respondents 75 (74%) oppose the closure, whilst 26 (26%) support the closure.

33. Respondents **who support** the proposal consider that the temporary closure will assist with addressing traffic issues on Station Street and will reduce rat running, whilst some considered that there were no noticeable issues when the Melbourne Water temporary road closure was in place for 12 months.

34. Respondents **who oppose** the proposal generally consider there to be limited issues on Station Street and therefore consider the closure to be unnecessary as it would have limited wider benefit. These respondents are deeply concerned that the closure would impact negatively on access and safety given that traffic from Station Street will be transferred onto other streets. The safety and amenity of the key Canning Street cycling corridor is a key issue for these respondents.

35. Of all respondents, the stated preferred temporary closure length was 6 months.

Submissions tabled as Petitions (Two)

36. Two external petitions were tabled at the Council meeting of 21 November 2017.

37. One petition was tabled by a resident of Station Street which requests that Council abandon the proposal as it represents an ineffective, inequitable and costly attempt at traffic management in Station Street, one that will cause increased hazard for cyclists and pedestrians, as well as inconvenience for a majority of residents.

38. The petition stated:

    "We the undersigned residents, concerned citizens and parents of school children wish to inform the Yarra City Council of our concerns with the proposed closure of Station Street, Carlton North at the Princes St intersection. We ask that the Yarra City Council put our safety first and foremost* ***and CEASE THE PROJECT CONSIDERING THE PROPOSED TRIAL CLOSURE OF STATION STREET*** *which would divert up to 470 extra cars in front of cyclists and children commuting on Canning, Lee and Davis Streets. *The accompanying letter on the reverse of this page outlining our concerns forms part of this formal petition."*

    The referenced accompanying letter is noted as ***Attachment 3.***

39. This petition had 536 signatories including:

    (a) 187 Station Street residents;

    (b) 151 Canning Street residents;

    (c) 39 Davis Street residents;

    (d) 32 Lee Street residents;

    (e) 31 Rathdowne Street residents;

    (f) 46 residents from other streets in Carlton North; and

    (g) 50 signatories from outside Carlton North.

40. Another petition with a covering letter was tabled by a resident of Canning Street which requests that Council abandon the proposal due to the likely increased number of cars using Canning Street and the impact this will have on cyclists and pedestrians.

41. The covering letter states;

> "*Please find the following petition to the City of Yarra Council outlining our concerns with the proposed trial closure of Station Street at the Princes Street exit into Carlton North. We the signatories, request that Council immediately abandon this costly experiment that will potentially put an extra 470 cars in front of cyclists and pedestrians every day. As the organiser of this petition, I, xxxx xxxx, look forward to hearing that you have arranged for it to be tabled at a Council meeting by a Ward Councillors"*.

42. The actual petition wording is noted in ***Attachment 4.***

43. This petition had 279 signatories via the change.org website. Signatories include a mix of local residents and property owners, frequent users of the Canning Street cycle route and a small number of other respondents (approximately 8% of signatories) whose connection to Canning Street or Carlton North cannot be identified from the petition.

Internal Consultation (One Yarra)

44. Council's Strategic Transport officers have been consulted on this matter and contributed - - that any increase in traffic on or across Canning Street would result in a poorer outcome for cyclist amenity on the regionally significant Canning Street cycling corridor.

Financial Implications

45. Apart from the cost of mailing and advertising relating to the consultation process, there are no other costs associated with the consideration of submissions received and hearing of verbal presentations on the proposal.

Economic Implications

46. There are no economic issues arising from the recommendations contained in this report.

Sustainability Implications

47. There are no sustainability issues arising from the recommendations contained in this report.

Social Implications

48. There are no social issues arising from the recommendations contained in this report.

Human Rights Implications

49. There are no human rights issues arising from the recommendations contained in this report.

Communications with CALD Communities Implications

50. A language advisory panel was included in correspondence with residents and owner non-occupiers. This included contact details and a reference number to access Council's interpreter services.

Council Plan, Strategy and Policy Implications

51. The proposal to install a temporary road closure at Station Street for the purposes of undertaking a traffic diversion experiment is included as an action in Section 3.09 of Council's Annual Plan for 2016/17.

Legal Implications

52. There are no legal issues arising from the recommendations contained in this report, save that Council must hear any verbal presentations from persons who have submitted a written submission and expressed a wish to support same verbally.

Other Issues

53. None applicable to the consultation process.

Options

54. Not applicable to this report.

Conclusion

55. Council should note the report, note and acknowledge the formal submissions and the two petitions received, note the verbal presentations made in support thereof and refer the matter for formal consideration to the first Council Meeting cycles in 2018.

RECOMMENDATION

1. That Council:
   (a) note the report;
   (b) note and acknowledge the formal submissions and petitions received and the verbal presentations made at this meeting in support thereof; and
   (c) refer the matter of the proposal to close Station Street, Carlton North at the intersection with Princes Street, on a temporary basis for the purposes of undertaking a traffic diversion experiment, for formal consideration at the first cycle of Council meetings in 2018.

CONTACT OFFICER: **Ivan Gilbert**
TITLE: **Group Manager Chief Executive's Office**
TEL: **9205 5110**

Attachments
1 Previous Council Resolutions
2 Traffic Impact Assessment Study
3 Covering Letter of Petition
4 Wording of Online Petition
5 Station Street, Carlton North - Temporary Road Closure Submissions

Attachment 1 - Previous Council Resolutions

ATTACHMENT 1

Station Street Proposed Trial Closure

For context -- a note of Council's previous determinations in respect of Station Street at the intersection of Prince's Street is:-

1. ***10 November 2015 – Council Meeting***:

"That Council:

(a) notes the representations by some residents of Station Street, North Carlton:

(i) *claiming there is a bank up of arterial traffic using Station Street seeking to exit into Princes Street as an alternative to the Nicholson Street arterial road;*

(ii) *the previous consultations undertaken by Council on this matter in 2002/2003;*

(iii) *the worsening nature of traffic in this local residential street in the many years since; and*

(iv) *the considered effectiveness of the recent temporary closure at Station/ Princes Streets; and*

(b) resolves that Council receive a report by December 2015 on the continuation of the temporary closure of Station Street at Princes Street."

Notes:

- Following a submission made by a resident of Station Street, Council resolved to explore the possible continuation of a temporary closure of Station Street that was in place to facilitate Melbourne Water sewer upgrade works at the time.

- Melbourne Water required a permit to temporarily close Station Street for approximately 12 months to establish its worksite. This permit was only valid for the duration of works. As such, it was not possible to extend the temporary road closure beyond the duration of the Melbourne Water works outside of the requirements of the Local Government Act 1989.

- Schedule 11, Section 10, of the *Local Government Act* 1989 allows Council to block or restrict the passage or access of vehicles on a road, for as long as necessary, for the purposes of undertaking a genuine traffic diversion experiment. However, Council must not exercise this power unless it has considered a report from VicRoads on the matter.

- To allow VicRoads to prepare its report on the matter, Council was required to provide VicRoads with a range of information including a **Traffic Impact Assessment** (TIA).

- It was not possible to undertake the required TIA before Melbourne Water completed its works on Station Street as:

  (i) There were two further road closures associated with Melbourne Water works that significantly impacted on traffic patterns in the wider North Carlton and North Fitzroy area at that time.

Attachment 1 - Previous Council Resolutions

(ii) Any impacts associated with the Station Street closure at that time could not be isolated appropriately from the other road closures.

(iii) There was no budget allocated to fund the preparation of a TIA.

2. March 2016

- The Melbourne Water works were completed and Station Street was reopened to traffic following the completion of the works.

3. During February and March 2016

- Council undertook an initial public consultation exercise, to understand the level of support for a temporary closure on Station Street. All properties in North Carlton bound by (and including) Park Street, Nicholson Street (west side of street), Princes Street (north side of street) and Lygon Street (east side of street) were included in the community consultation.

- There were 354 responses during the February / March 2016 consultation. The results indicated that 50% of respondents supported the closure and 50% opposed.

4. 19 April 2016 – Council Meeting

"That Council resolves:

(a) to note the results of the community consultation undertaken for the Carlton North LATM area;

(b) to progress the process to install a temporary road closure at Station Street / Princes Street for the purposes of undertaking a traffic diversion experiment as per the Local Government Act 1989, Schedule 11, Section 10;

(c) to engage in further community consultation pursuant to the Local Government Act 1989, Section 223, for the installation of a temporary road closure for the purposes of undertaking a traffic diversion experiment;

(d) that the procedures outlined in (b) and (c) above be commenced in mid-2016 when it is considered traffic patterns will have returned to normal;

(e) to refer funding of up to $50,000 to the 2016/17 budget process to fund (b) and (c) above;

(f) to seek an immediate meeting with VicRoads, officers and ward councillors, to clarify additional traffic data and any other information required for a temporary road closure/traffic diversion experiment, in addition to the existing data (as set out in Paragraph 26 of the officer report) and information identified in Council's resolution on the matter on 15 December 2015; and

(g) *pending the above, to receive a further report from officers on the feedback from VicRoads and community submissions, for Council decision, and if approved, that Council receive an evaluation report on the results of the traffic diversion experiment 12 months after implementation."*

Attachment 1 - Previous Council Resolutions

Notes:

- Council allocated funds in the 2016/17 budget for officers to prepare the TIA and other supporting information to be provided to VicRoads to allow it to write its report on the matter.

5. 2 August 2016 – Council Meeting

"That Council:

(a) notes the Officer report suggesting abandoning the proposed temporary road closure at Station/Princes Streets as a trial traffic management experiment as inappropriate in light of the need for analysis of impacts of a trial based on up-to-date data analysis and other evidence;

(b) notes the representations by residents of Station Street, North Carlton on the officer report, and their own informative analysis of safety impacts and traffic flow based on local knowledge;

(c) determines in accordance with Council's three prior resolutions on this matter dated 10 November 2015, 15 December 2015, and 19 April 2016, and Council's 2016/17 Budget allocation for this trial to proceed, to continue the process to pursue the agreed trial closure at Station/Princes Streets as a traffic management experiment under the provisions of the Local Government Act; and

(d) receive a draft of the proposed report to VicRoads on this matter which is to incorporate photographic and other evidence on the former lengthy queuing on Station Street, submissions on amenity and safety issues impacting Station Street, safety impacts on Princes Street as determined to be included in Council's prior resolutions, together with fuller analysis of impacts on other streets based on up-to-data data analysis and other evidence. "

6. 20 December 2016 – Council Meeting

1. *"That Council notes:*
   (a) the reports on Station Street presented on traffic management issues proposed to be sent to VicRoads to allow it to write its report on the proposal to close Station Street, North Carlton at the intersection with Princes Street, on a temporary basis for the purposes of undertaking a traffic diversion experiment;
   (b) objectives applicable to VicRoads under the Transport Integration Act 2010 (Section 86) which include considerations of minimising adverse environmental impacts of the road system and contribution to social wellbeing and supporting liveable communities;
   (c) the Station Street resident submissions;
      (i) on Station Street as a neighbourhood residential street and the impacts of traffic queuing on Station Street to access the busy Princes Street arterial road, on families and children in Station Street, the Lady Gowrie Childcare Centre, and Carlton Neighbourhood Learning Centre;
      (ii) that the closing of Station St/Princes St intersection temporarily for the Melbourne Water works between January 2015 and March 2016 removed

Attachment 1 - Previous Council Resolutions

safety issues residents faced on a daily basis, from the numbers of cars using Station Street as a by-pass; and

(iii) seeking further consideration of safety and amenity concerns related to the above, including their close proximity to some 60,000 vehicles per day along Princes Street; and

(d) that the GTA report did not have access to information on traffic assessments undertaken during the 14 month period of the Melbourne Water sewer works temporary closure of Station Street;

(i) Melbourne Water commissioned traffic study by Cardno transport and traffic analysts;

(ii) Melbourne Water post-closure resident survey; and

(iii) City of Yarra traffic analysis contained in report to Council on 19 April 2016.

2. That Council resolves:

(a) to prepare a place-making assessment and proposal for the Southern end of Station Street adjacent to the Neighbourhood Learning Centre (assuming a road closure) and that it include;

(i) ways to engage the neighbourhood community with the activities and programs at the Neighbourhood Learning Centre;

(ii) public art and other outdoor activities that would be enabled by the road closure;

(iii) community garden projects that would be enabled by the road closure; and

(iv) other benefits to the community enabled by the road closure; and

(b) to undertake this place-making proposal using the remaining budget allocated for the temporary road closure;

(c) to defer submission of the report to VicRoads in order to enable place-making, environmental, social and amenity issues to be included, together with the opportunity for community input into those considerations; and

(d) *That a further report be presented to Council which includes this additional information prior to presentation to VicRoads."*

7. 5 September 2017 – Council Meeting

"That Council:

(e) notes:

(i) representations by Station Street residents since 2015, including several families with children seeking alleviation from the impacts of traffic accessing the Princes Street/Alexandra Parade major arterial in North Carlton;

(ii) Resident Submissions to VicRoads received September 2017, including analysis of traffic and safety, ongoing damage to road infrastructure at Station/Princes Streets from turning vehicles, and comparisons of Council

Attachment 1 - Previous Council Resolutions

traffic data before (Council surveys 2009, 2013), during (Council survey February 2015), and after (GTA survey September 2016) the previous 14 month temporary closure of Station/Princes Streets January 2015 to March 2016 required for Melbourne Water sewer works; and

(iii) the above Resident Submissions identifying that analysis of this comparative traffic data showed an overall reduction in traffic volumes in February 2015, during the period of that temporary closure, by a net 437 vehicles less per day for all streets affected by the temporary closure - Station, Davis, Canning Streets, and on Lee Street past the Carlton North Primary School; and

(f) notes Council's four prior resolutions on this matter dated 10 November 2015, 15 December 2015, 19 April 2016, and 20 December 2016, and the further information on these representations;

(g) now determines to refer to VicRoads, this Council resolution, the above Resident Submission to VicRoads, and material in Attachments 1 and 2, with a request that VicRoads consider this matter and provide a report in accordance with Schedule 11, Section 10(1)(c) of the Local Government Act 1989, on the proposed trial closure of Station Street North Carlton at its intersection with Princes Street, pending completion of a proposed North Carlton Local Area Traffic Management assessment of the area being undertaken; and

(h) resolves to receive a further report to Council within a month of receiving the report from VicRoads on this matter together with advice on further proposed consultation with the community on any proposed trial closure."

8. 3 October 2017 – Council Meeting

"That Council:

(i) notes the response from VicRoads in its letter to Council dated 19 September 2017, that "VicRoads has no objection to the proposed trial closure of Station Street subject to the local community being supportive of the closure";

(ii) notes advice previously provided to the community indicating the opportunity to make submissions on the matter following receipt of the Vic Roads report, in accordance with Schedule 11, Section 10(1)(c) of the Local Government Act 1989 (the Act);

(iii) proceed with the consultation process for a trial road closure as outlined in the officers report; and

(iv) *consider a further report on the matter, to include the assessment of submissions, and receive submissions for final determination at its first Council meeting in December 2017 and that all parties that have expressed interest in this matter be notified at least* one week in advance of the report being presented to Council."

Attachment 2 - Traffic Impact Assessment Study

GTAconsultants

Station Street Road Closure
Carlton North
Transport Impact Assessment

Client //	Yarra City Council
Office //	VIC
Reference //	V105900
Date //	25/11/16

Attachment 2 - Traffic Impact Assessment Study

Station Street Road Closure

Carlton North

Transport Impact Assessment

Issue: C 25/11/16

Client: Yarra City Council
Reference: V105900
GTA Consultants Office: VIC

Quality Record

Issue	Date	Description	Prepared By	Checked By	Approved By	Signed
A-Dr	25/10/16	Draft	Anna Scott / Jacquelyn Viti	Alex Blackett	Reece Humphreys	
A	14/11/16	Final	Anna Scott / Jacquelyn Viti	Alex Blackett	Reece Humphreys	
B	24/11/16	Revised Final	Anna Scott / Jacquelyn Viti	Alex Blackett	Reece Humphreys	
C	25/11/16	Revised Final	Anna Scott / Jacquelyn Viti	Alex Blackett	Reece Humphreys	

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Attachment 2 - Traffic Impact Assessment Study

Table of Contents

1.	**Introduction**	**1**
1.1	Background & Purpose	1
1.2	Proposed Closure	1
1.3	Methodology	2
1.4	References	2
2.	**Background**	**3**
2.1	Policy & Strategies	3
2.2	Temporary Closure (Water Main Upgrade)	5
3.	**Baseline Conditions**	**6**
3.1	Carlton North Demographics	6
3.2	Road Network	7
3.3	Crash Stats	9
3.4	Public Transport	11
3.5	Pedestrian Infrastructure	11
3.6	Cycle Infrastructure	11
4.	**Data Collection & Analysis**	**14**
4.1	Overview	14
4.2	Site Observations	14
4.3	Data Collection	15
4.4	Data Analysis	16
4.5	Station Street Traffic Generation	24
5.	**Traffic Impact**	**26**
5.1	Intersection Modelling	26
5.2	Network Implications	29
6.	**Conclusions & Recommendations**	**30**
6.1	Summary of Conclusions	30
6.2	Recommendation	31

Appendices

A:	SIDRA Intersection 7 Outputs

Figures

Figure 1.1:	Subject Site and its Environs	1
Figure 2.1:	VicRoads SmartRoads Network Operating Plan – City of Hobsons Bay	3

Figure 3.1:	Existing Mode Share for Trips to Work in Carlton North	6
Figure 3.2:	Existing Traffic Controls	9
Figure 3.3:	Crash Stats in the Vicinity of the Site	10
Figure 3.4:	Public Transport Map	11
Figure 4.1:	Existing AM Peak Hour Traffic Volumes (8:00am-9:00am)	17
Figure 4.3:	Morning Peak, Tuesday 13 September – 8:00am-9:00am	20
Figure 4.4:	School PM Peak, Tuesday 13 September – 3:00pm-4:00pm	21
Figure 4.5:	Commuter PM Peak, Tuesday 13 September – 5:00pm-6:00pm	22
Figure 4.6:	Origin and Destination of Vehicles using Station Street	23
Figure 4.7:	Map of Where 76 Vehicles Exiting Station Street is Generated From	25

Tables

Table 3.1:	Casualty Accident History	10
Table 4.1:	Super Tuesday Cycle Count Data (7:00am to 9:00am)	18
Table 4.2:	Existing Traffic Flows	19
Table 4.3:	Data Comparison 2009 and 2016	24
Table 5.1:	Existing Operating Conditions	26
Table 5.2:	Scenario 1 – Additional Volumes	28
Table 5.3:	Scenario 2 – Additional Volumes	28
Table 5.4:	Canning Street / Princes Street Intersection 8:00am-9:00am	29

1. Introduction

1.1 Background & Purpose

The City of Yarra is currently considering what the benefits and impacts are of temporarily closing Station Street, at its intersection with Princes Street in North Carlton, for the purposes of undertaking a traffic diversion experiment.

The closure has previously been suggested as part of a Local Area Traffic Management Study of the North Carlton area in 2003. This proposal only restricted egress (southbound) movements from Station Street to Princes Street due to expected redistributed rat-running traffic caused by other traffic management measures being implemented in the precinct.

Moreover, Station Street was temporarily closed at this location between January 2015 and March 2016 by Melbourne Water to enable them to upgrade the sewer network. Consequently, the City of Yarra has consulted with North Carlton residents about formally closing Station Street at Princes Street in February and March 2016. The responses from the community indicated that only half are supportive of the closure, with those not being supportive largely being concerned about traffic redistribution impacts onto surrounding streets.

GTA Consultants was commissioned by the City of Yarra in September 2016 to undertake a transport impact assessment of the proposed closure of Station Street at Princes Street. The assessment aims to provide an understanding of what the potential implications of the closure are, as well as whether the associated benefits and impacts are material.

1.2 Proposed Closure

The proposed road closure is located in North Carlton, at the intersection of Station Street and Princes Street. The location of the closure is shown in Figure 1.1.

Figure 1.1: Subject Site and its Environs

1.3 Methodology

The report is an assessment of the anticipated transport implications of the proposed closure of Station Street and has been completed through the following steps:

i A **policy review** of relevant transport documents affecting and guiding the development and management of the area
ii Setting out the **existing transport conditions** in the area to understand the current access arrangements and what impact the proposal will have of current users
iii The collection and analysis of **traffic data** to understand how motorists currently access and travel through the area
iv A **traffic assessment** of the proposal to identify how the resulting road network will operate in the future and what impacts there will be on current users
v Provide a **summary of conclusions** of what the potential implications of the closure are, as well as whether the associated benefits and impacts are material to current users
vi Provide a **recommendation** about whether the proposed closure of Station Street should be pursued further.

1.4 References

In preparing this report, reference has been made to the following:

- Documentation provided by the City of Yarra
- City of Yarra Local Area Traffic Management Plan
- VicRoads Traffic Engineering Manual Volume 1: Chapter 13
- traffic and car parking surveys undertaken by GTA Consultants as referenced in the context of this report
- an inspection of the site and its surrounds
- other documents as nominated.

2. Background

2.1 Policy & Strategies

There are a number of policies and strategies prepared by national, state and local government bodies that help inform how the North Carlton area should be developed and managed into the future. Those that have the highest relevance in regards to the proposed closure of Station Street is discussed below.

2.1.1 VicRoads SmartRoads Policy

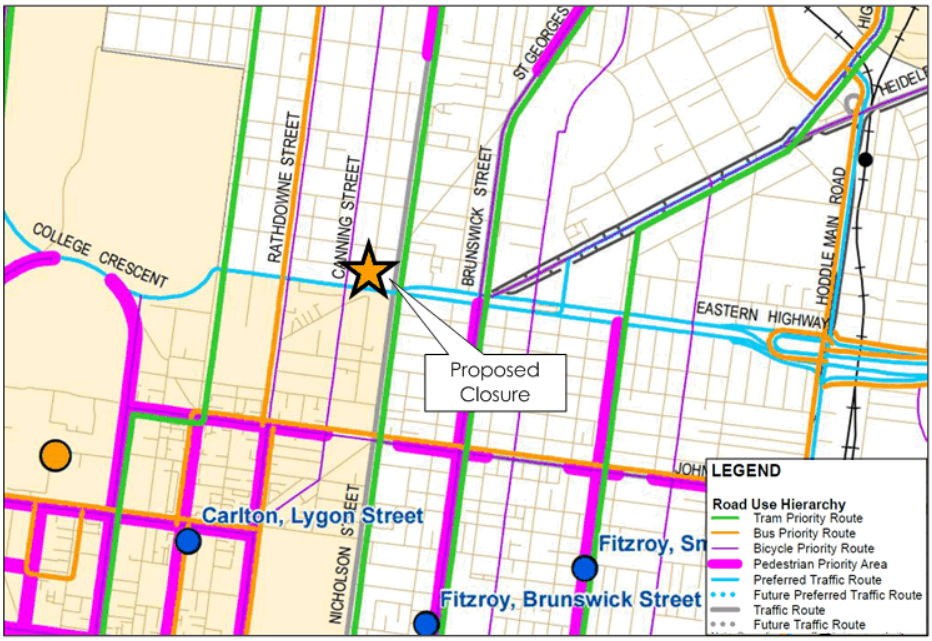
SmartRoads is a VicRoads policy which sets strategic 'modal' priorities on the road network and underpins many of the strategies significant to the operational directions that support broader strategies around land use and transport.

"There is no single solution to managing congestion on our roads. Sustainable management of congestion will require an integrated approach involving better management of the existing network, building new infrastructure, visionary land use planning, encouraging sustainable transport modes, and changes in behaviour by individuals, businesses and government."

All road users will continue to have access to all roads. However, certain routes will be managed to work better for cars while others for public transport, cyclists and pedestrians during the various peak and off-peak periods.

The VicRoads SmartRoads Network Operating Plan for the area surrounding the subject site has been reproduced in Figure 2.1.

Figure 2.1: VicRoads SmartRoads Network Operating Plan – City of Hobsons Bay



Source: VicRoads Website

Figure 2.1 illustrates the following arterial road network priorities in the area:

- Princes Street is nominated as a Preferred Traffic Route
- Nicholson Street is nominated as a Traffic Route and Tram Priority Route
- Rathdowne Street is a Bus and Bicycle Priority Route
- Canning Street is a Bicycle Priority Route.

On this basis, it can be understood that the proposed closure of Station Street will prevent vehicular access to a Preferred Traffic Route.

2.1.2 Yarra City Council Local Area Traffic Management Plan

The City of Yarra Council adopted its Local Area Traffic Management Policy in May 2014. The policy outlines Council's process for conducting Local Area Traffic Management (LATM) studies. The aim of the policy is to reduce traffic volume and speeds on local roads and provide a safer environment for cyclists and pedestrians. The LATM Policy separates the City of Yarra into 21 precincts, of which North Carlton is the #2 precinct, bound by Park Street to the north, Nicholson Street to the east, Princes Street to the south and Lygon Street to the west.

The LATM Policy sets out the following processes in undertaking studies:

- Upon receipt of requests, Council officers will make a preliminary site visit and review available traffic data to quantify if there is a readily apparent road safety problem.
- If the site is not considered a road safety issue, officers will refer this to the LATM priority ranking list.
- If a safety issue is identified from the analysis of site investigations, evidentiary traffic data and engineering assessment, officers may decide to bypass the LATM process and consider another appropriate course of action to address the issue.

The LATM priority ranking list is based on a warrants system to determine the priorities for competing LATM works. These precincts will be ranked according to the following criteria using available data:

- Casualty crashes – any reported fatalities, serious injuries and other injuries in the last five years on local streets or intersections;
- Traffic speed – any local street with an 85th percentile speed generally greater than 44km/h;
- Traffic volume – any local street with an average weekday traffic volume generally greater than 1,000 vehicles;
- Through traffic – any local street with a peak hour to 24-hour volume ratio generally in excess of 14%;
- Heavy vehicles – any local street with a proportion of commercial vehicles to all traffic generally in excess of 5%;
- Activity land use generators (e.g. hospitals and schools) – considered in terms of likely pedestrian and bicycle generation, especially by vulnerable road users; and
- Resident complaints – expressed by the number of received letters, petitions and notes to file from Council officers.

Each year Council officers review the LATM priority ranking list as part of developing budgets and traffic management programs for the subsequent financial year, and advise councillors accordingly.

It is unclear at this time how the proposed closure of Station Street ranks against other LATM issues within the municipality. However, as part of this study, consideration is being given to each the criteria used to assess such matters.

2.2 Temporary Closure (Water Main Upgrade)

Between January 2015 and March 2016, Melbourne Water closed various streets in North Carlton and North Fitzroy to enable them to replace a 114-year old section of the Carlton main sewer. These closures included Scotchmer Street, Amess Street and Station Street, and resulted in changes to traffic patterns through the wider North Carlton and North Fitzroy areas.

The works on Station Street required that it be closed at its intersection with Princes Street. During this time, pedestrian access was maintained along Station Street to Princes Street, but access by cars and cyclists was prevented, with detours in place.

Councillors and Council Officers met with VicRoads in mid-2016 about the potential of continuing the road closure on Station Street. While VicRoads noted that they had not observed any notable impact on the arterial road network with the closures in place, they did indicate they needed an assessment of the potential impacts in the area once traffic patterns returned to normal following the reopening of Scotchmer Street, Amess Street and Station Street.

3. Baseline Conditions

3.1 Carlton North Demographics

The North Carlton LATM Policy precinct (#2) is bound by Park Street to the north, Nicholson Street to the east, Princes Street to the south and Lygon Street to the west. Within this area, the below key demographics have been identified.

3.1.1 Population

According to data collected in 2015, Carlton North has a population of approximately 9,086 people. The area is highly accessible for tertiary institutions located in central Melbourne, with almost one in five residents studying at a university.

3.1.2 Land Use

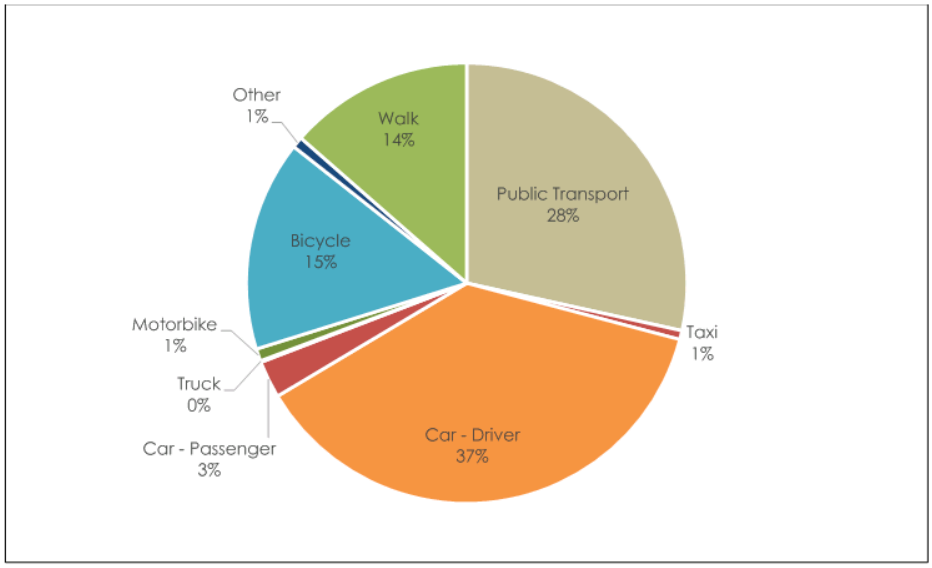
The area is predominantly residential; however, the following major non-residential land uses exist:

- Curtain Square, which is approx. 350m northwest of Station Street
- Carlton North Primary School, which is approx. 200m northwest of Station Street.

3.1.3 Mode Splits

Existing mode split data has been sourced from ABS 2011 Journey to Work Data, for residents of Carlton North. This data represents those who use one mode of transport only. Figure 3.1 shows that approximately 40% of those living in Carlton North drive their own car to get to work, almost 30% utilise public transport and 30% use some form of active travel (i.e. cycle or walk).

Figure 3.1: Existing Mode Share for Trips to Work in Carlton North



3.2 Road Network

3.2.1 Adjoining Roads

Station Street

Station Street functions as a local road in the Yarra Planning Scheme. It is a two-way road aligned in a north-south direction and configured with a two-lane, six-metre-wide carriageway set within a 20-metre-wide road reserve (approx.). Kerbside parking is permitted on both sides of the road, subject to time restrictions. There is an on-road bicycle lane in the northbound direction and a marked mixed traffic bicycle route in the southbound direction along Station Street.

Station Street carries approximately 1,000 vehicles per day[1].

Nicholson Street

Nicholson Street functions as a primary arterial road and is located within a Road Zone (Category 1) in the Yarra Planning Scheme. It is a two-way road aligned in a north-south direction and generally configured with a four-lane, 23-metre-wide carriageway set within a 31-metre-wide road reserve (approx.). Two tram lanes are located in the middle of the road. Some kerbside parking is permitted, subject to time restrictions. There are no bicycle lanes provided along Nicholson Street.

Nicholson Street carries approximately 10,000 vehicles per day.[2]

Canning Street

Station Street functions as a local road in the Yarra Planning Scheme. It is a two-way road aligned in a north-south direction and configured with a two-lane, 19-metre-wide carriageway set within a 29-metre-wide road reserve (approx.). Included within the carriageway are bicycle lanes in both directions, and an eight-metre-wide central median. Some kerbside parking is permitted, subject to time restrictions. Bicycles are catered for on Canning Street in on-road lanes in both directions.

Canning Street carries approximately 550 vehicles per day.[1]

Rathdowne Street

Rathdowne Street functions as a collector road in the Yarra Planning Scheme. It is a two-way road aligned in a north-south direction and is generally configured with a three-lane, 20-metre-wide carriageway set within a 31-metre-wide road reserve (approx.). Included within the carriageway are bicycle lanes in both directions, and a three-meter-wide central median. Some kerbside parking is permitted, subject to time restrictions.

Rathdowne carries approximately 10,500 vehicles per day.[1]

Lee Street

Lee Street functions as a local road in the Yarra Planning Scheme. It is a two-way road aligned in an east-west direction and configured with a two-lane, 14-metre-wide carriageway set within a 20-metre-wide road reserve (approx.). Included within the carriageway are bicycle lanes in both directions, and a three-metre-wide central median. Some kerbside parking is permitted, subject to time restrictions.

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[1] Based on traffic counts undertaken by GTA between 8 September 2016 and 14 September 2016.
[2] Source: VicRoads Traffic Profiler

Lee Street carries approximately 800 vehicles per day.[1]

Davis Street

Davis Street functions as a local road in the Yarra Planning Scheme. It is a two-way road aligned in an east-west direction and configured with a two-lane, 8-metre-wide carriageway set within a 20-metre-wide road reserve (approx.). Some kerbside parking is permitted, subject to time restrictions.

Davis Street carries approximately 700 vehicles per day.[1]

Princes Street

Princes Street functions as a primary arterial road and is located within a Road Zone (Category 1) in the Yarra Planning Scheme. It is a two-way road aligned in an east-west direction and is generally configured with a 6-lane, 22-metre-wide carriageway set within a 30-metre-wide road reserve (approx.), including a central median. Some kerbside parking is permitted, subject to time restrictions.

Princes Street carries approximately 29,000 vehicles per day.[2]

3.2.2 Surrounding Intersections

Key intersections in the vicinity of the site include:

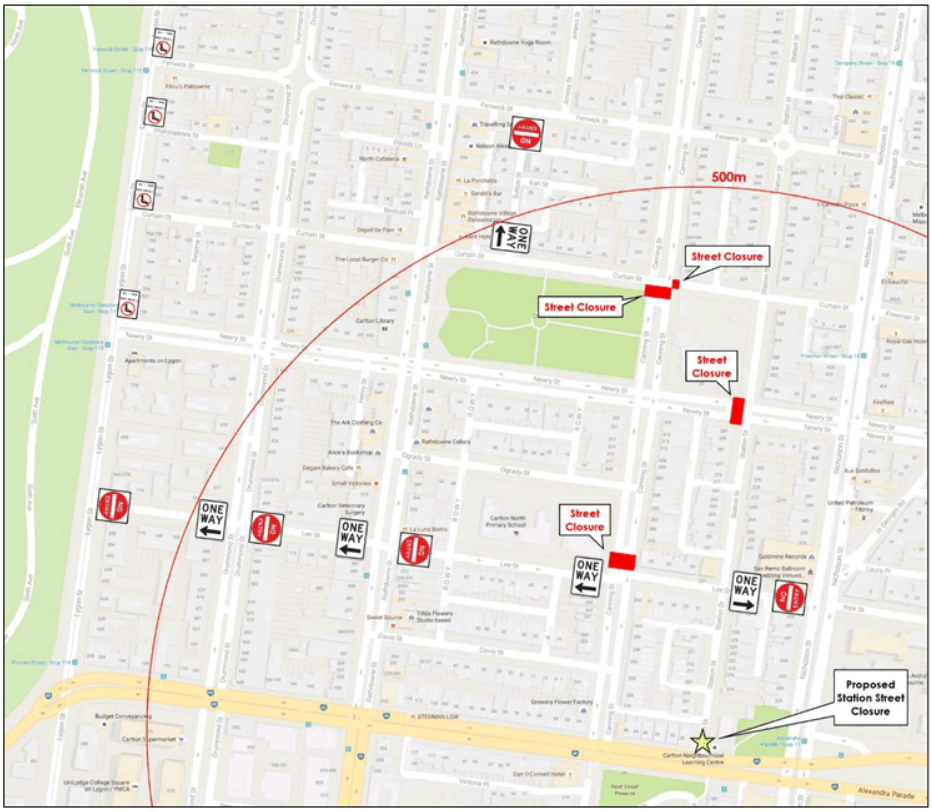
- Station Street / Princes Street (unsignalised T-intersection)
- Station Street / Lee Street (unsignalised X-intersection)
- Station Street / Newry Street (unsignalised X-intersection)
- Canning Street / Princes Street (signalised X-intersection)
- Canning Street / Davis Street (unsignalised T-intersection)
- Princes Street / Nicholson Street / Alexandra Parade (signalised X-intersection)
- Princes Street / Rathdowne Street (signalised X-intersection).

3.2.3 Existing Traffic Controls

A number of existing traffic controls and movement restrictions exist within the North Carlton area, proximate to Station Street. These include no entry, no right turn and various street closures.

These traffic controls within 500m of the proposed closure are shown in Figure 3.2.

Figure 3.2: Existing Traffic Controls



Base Map Source: Google Maps

3.3 Crash Stats

A review of the reported casualty accident history for the roads and intersections adjoining the subject site has been sourced from VicRoads CrashStats accident database. This database records all accidents causing injury that have occurred in Victoria since 1987 (as recorded by Victorian Police) and categorises these accidents as follows:

- Fatal injury: at least one person was killed in the accident or died within 30 days as a result of the accident.
- Serious injury: at least one person was sent to hospital as a result of the accident.
- Other injury: at least one person required medical treatment as a result of the accident.

A summary of accidents in the vicinity of the proposed closure is shown in Figure 3.3.

Figure 3.3: Crash Stats in the Vicinity of the Site

A summary of the accidents shown in Figure 3.3 in the vicinity of the site for the last available five-year period is presented in Table 3.1.

Table 3.1: Casualty Accident History

Location	Accident No.		
	Fatality crashes	Serious Injury crashes	Other Injury crashes
Roads in Vicinity of the Site			
Station Street	0	0	0
Canning Street	0	1	3
Rathdowne Street	0	3	1
Nearby Intersections			
Princes Street / Station Street	0	0	0
Princes Street / Canning Street	0	0	1
Princes Street / Rathdowne Street	0	0	13

Source: VicRoads

Figure 3.3 and Table 3.1 indicates that no injuries have been recorded as a result of vehicles travelling along and turning out of Station Street on Princes Street (noting the temporary closure for sewer works).

Moreover, the following accidents have been recorded in the area, which could be further impacted by displaced traffic that currently uses Station Street to access Princes Street:

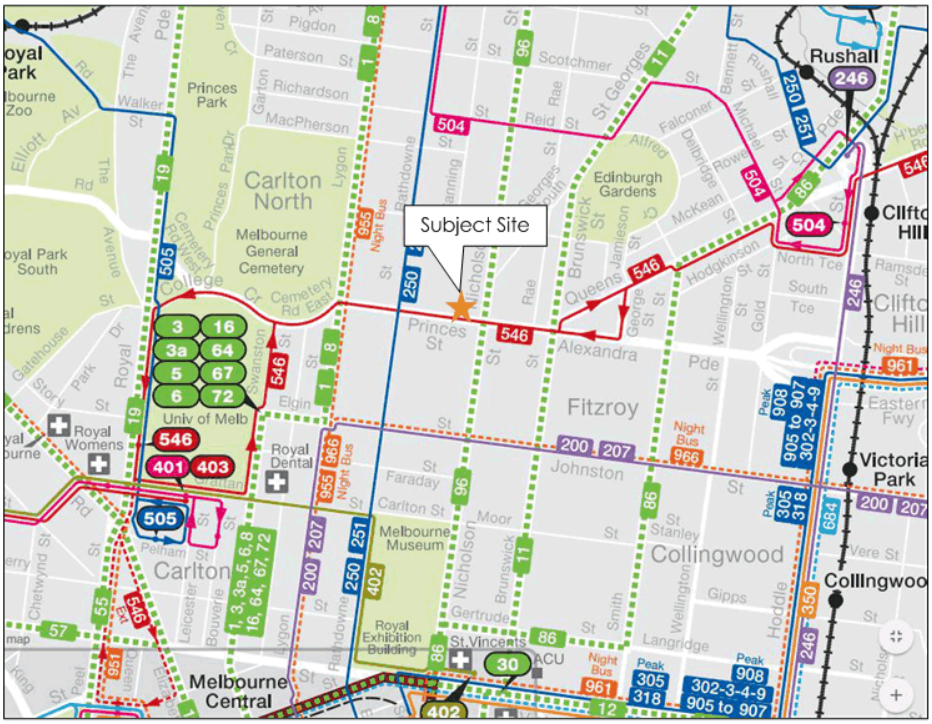
- There has been a total of 15 crashes at the Princes Street/Rathdowne Street, and one at the Princes Street/Canning Street intersection
- Along Rathdowne Street there have been a total four crashes recorded
- Along Canning Street there have been a total of four crashes, noting that the three at the Lee Street intersection all involving and resulting in injuries to cyclists (one serious).

Given the above there is no historic accident trend that exists with Station Street or its intersection with Princes Street.

3.4 Public Transport

Figure 3.4 shows the subject site in relation to existing public transport routes within its vicinity.

Figure 3.4: Public Transport Map



As indicated in Figure 3.4, the area surrounding Station Street is well serviced by public transport, with bus and tram routes operating frequently within the vicinity of the proposed closure. It is noted that the closure of Station Street would have no direct impact on existing public transport routes or stops. However, consideration of any traffic impacts from the re-routing of vehicles is considered further in Section 5.

3.5 Pedestrian Infrastructure

Pedestrian paths are located on all footpaths in the vicinity of the site, with signalised pedestrian crossings on Princes Street at the intersections with Rathdowne Street, Canning Street and Nicholson Street.

3.6 Cycle Infrastructure

The Principal Bicycle Network (PBN) is a network of on and off-road cycling corridors that have been identified to support cycling for transport and access major destinations in metropolitan Melbourne. The PBN was reviewed and updated in 2012 by VicRoads and all local Councils.

The PBN is also a 'bicycle infrastructure planning tool' to guide State investment in the planning and development of the future metropolitan Melbourne bicycle network. In this regard, a subset

of the PBN has been identified and elevated to a higher level of priority, mainly on the basis of potential for separation from motorised traffic, making these routes more attractive to less experienced bike riders. These cycling corridors are referred to as Bicycle Priority Routes (BPRs) and form part of the modal priorities for the road network set out in the VicRoads SmartRoads Network Operating Plans for each municipality (as shown in Figure 2.1 for the study area).

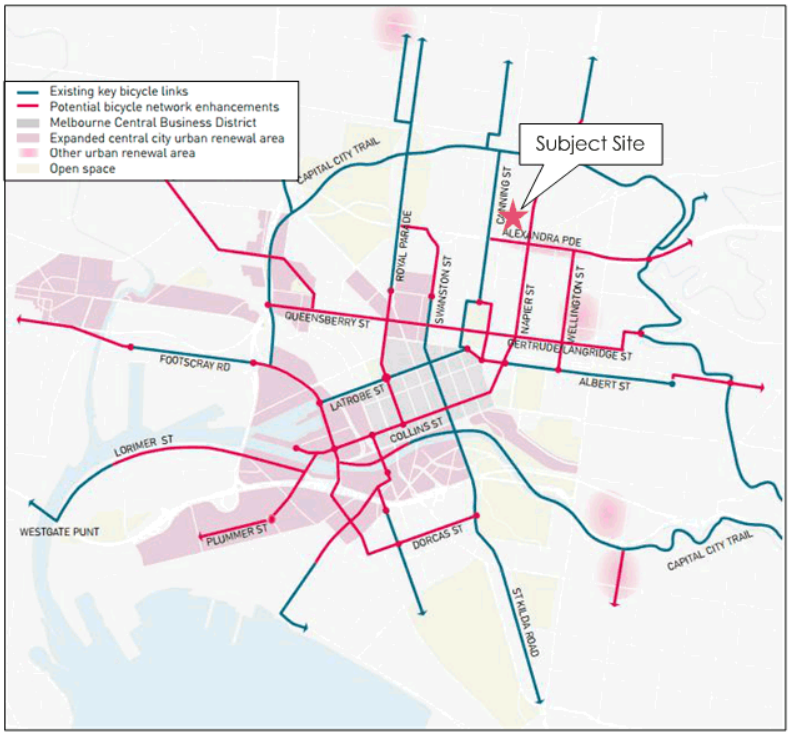
Proximate to Station Street, the following BPR's are indicated in Figure 2.1:

- Canning Street
- Rathdowne Street.

It is noted that the type of bicycle facility (i.e. on or off-road and separated or shared) has not been indicated as part of the PBN and BPRs. Rather, the PBN and BPRs show the proposed cycling network. The associated facilities should be delivered in accordance with the relevant standards and guidelines, such as the Australian Standards, Austroads Guides and VicRoads' Cycle Notes.

In addition, Strategic Cycle Corridors (SCC) form another subset of the PBN, and represent an initiative outlined in Plan Melbourne to support walking and cycling in Central Melbourne. SCCs are intended to be corridors designed to provide high quality bicycle infrastructure to, and around, major activity areas in metropolitan Melbourne. The SCC's are shown in Figure 3.5[3].

Figure 3.5: Strategic Cycle Corridors



Source: Plan Melbourne

[3] Further information regarding the PBN and BPRs is available at https://www.vicroads.vic.gov.au/traffic-and-road-use/cycling/bicycle-network-planning

Figure 3.5 indicates that Canning Street and Princes Street form part of the proposed SCC network.

It is also noted that the southbound bicycle lane on Canning Street between Lee Street and Princes Street has recently been relocated from being between the traffic and parking lanes, to the right-hand side of the traffic lane, up against the central median.

This arrangement has been implemented given the following benefits:

- Better connects with the signalised crossing facilities of Princes Street
- Locates cyclists away from the car door opening area of the kerbside parking lane
- Removes the conflict point between left-turning vehicles from Lee Street into Canning Street, and left-turning vehicles from Station Street into Princes Street.

While the above benefits have been achieved, this arrangement places cyclists on the right-hand side of traffic, which is not where motorists expect cyclists to be. This could potentially result in motorists that are turning right-in and out of Davis Street from Station Street to not be looking where the southbound cyclists are coming from.

However, this potential conflict issue is not considered significant given the signage and linemarking that is in place, especially the green bicycle lane pavement markings across the central medial break. To further improve the level of safety at this location, consideration could be given to raising the bicycle lane, even if only 50mm or similar (i.e. not kerb high) to further raise the awareness and slow down approaching motorists.

4. Data Collection & Analysis

4.1 Overview

A range of targeted site observations, surveys and analysis has been undertaken to understand how users currently access and travel through the area. The associated information has been used to inform what impacts and / or benefits the closure of Station Street will have in the area.

4.2 Site Observations

Site observations were undertaken on Tuesday 13 September 2016, which was at the same time as the majority of the survey data was collected. This was in an effort to verify the survey data, but also provide greater context and understanding of user behaviour in the area.

A summary of the key site observations during the AM and PM peak periods are set out below.

4.2.1 Princes Street / Station Street

On-site surveys of the Princes Street / Station Street intersection were undertaken during the peak periods to understand what proportion of vehicles exiting Station Street cross the adjacent three eastbound traffic lanes to access the right-turn lane and travel south on Nicholson Street. Concerns have been raised about this movement and that the potential closure of Station Street would eliminate this movement from occurring.

It is noted that should Station Street be closed that those currently completing this movement would most likely go to Canning Street instead. While they would still need to cross three traffic lanes to access the right-turn lane, they would be able to do it over a longer distance, so is likely to have some safety improvements.

However, it is noted that there are many other examples in the area where local roads intersect major roads, such as Drummond Street, Rae Street and Gore Street to the east and west along Princes Street and Alexander Parade, and they seem to genially operate satisfactorily.

Also, the intersection of Canning Street and Princes Street is a partially signalised intersection, with a dedicated bicycle lane on the right-hand side of the road. Vehicles making the left-hand turn movement from Canning Street to Princes Street are restricted by a left turn arrow. The arrow is red during the majority of the pedestrian and cyclist crossing phases, to separate vehicle and pedestrian movements, but does drop out before the end of pedestrian and cyclist crossing phases to provide opportunity for vehicles to exit Canning Street before traffic along Princes Street get the green phase.

From these on-site surveys the vehicles turning left out of Station Street did the following:

- In the AM period (7:45-8:15am), 48 vehicles turned left out of Station Street, with three (6%) vehicles moving to the right hand lane to turn right onto Nicholson Street.
- In the PM period (5:00-5:30pm), 6 vehicles turned left out of Station Street, with one (16%) vehicle moving to the right hand lane to turn right onto Nicholson Street.

These observations indicate that a very low proportion of vehicles in both peaks make this movement. However, it was also observed to be very difficult to complete this movement due to there being an almost continuous traffic stream and/or vehicles queued in the lanes they

needed to cross and finally enter. In fact, vehicles trying to complete this manoeuvre generally have to rely on other motorists to let them in.

As such, this movement was observed to occur at low speeds, which means the potential severity of any crash would be minor. However, those making these movements likely cause other motorists to become frustrated with them, such as those waiting to get out of Station Street.

4.2.2 Queuing

From our on-site observations in the peak periods, the following locations was observed.

- Vehicles turning out of Station Street onto Princes Street formed a queue in the morning of up to 35m (up to six cars), due to the limited opportunities to enter the Princes Street traffic lanes.
- Queuing on Rathdowne Street was observed to queue back from Princes Street to Curtain Square (i.e. approx. 350m) during the AM peak period. This queue typically did not clear each cycle, as the amount of green time given to the northern approach was fairly limited, but also due to there only being one approach lane serving the associated traffic volume. This issue was not observed in the PM peak period, including with the southern approach to Princes Street, which accommodates more vehicles.
- Canning Street experienced minimal queuing, with the maximum queue observed containing two vehicles.

4.2.3 Key Trip Attractors

The land uses surrounding the site are largely residential, with some exceptions, including the Carlton Neighbourhood Learning Centre (20 Princes St), local shops, restaurants and gyms.

The most significant nearby attraction is the Carlton North Primary School (60 Lee St). The school has approximately 275 pupils, between the years of Prep and Year 7. This site generates substantial traffic, particularly during morning and afternoon school peak periods when students are being picked-up and dropped-off.

Working with the schools to understand how the pick-off / drop-off arrangements and demands can be better managed is likely something already being undertaken, but if material changes can be achieved, it would likely result in significant congestion and amenity improvements in the area. Such activities are being completed around Australia, with one of the leading school travel plan guides / tool kits being available through the following link:

http://www.darwin.nt.gov.au/sites/default/files/Active_Schools_Toolkit_web.pdf

4.3 Data Collection

The following data was collected by GTA in order to assess the operation of the site and surrounding area:

i Weekday AM and PM (Tuesday 13 September) peak period movement counts (all modes) at the following intersections:
  - Station Street / Princes Street
  - Station Street / Lee Street
  - Station Street / Newry Street
  - Canning Street / Princes Street
  - Canning Street / Davis Street.

ii Weekend midday (Saturday 10 September) and weekday AM and PM (Tuesday 13 September) peak period movement counts (all modes) at the Canning Street / Lee Street intersection.

iii Obtain a typical weeks' worth of SCATS[4] data (from Friday 9 September until Thursday 15 September) and the operational sheets for the following signalised intersections:

- Princes Street / Nicholson Street / Alexandra Parade
- Princes Street / Canning Street
- Princes Street / Rathdowne Street.

iv Peak period sample surveys of the above signalised intersections to identify the following:

- turning splits from lanes that support more than one turning movement
- queue lengths for each turning movement.

v Undertake tube counts for a 7-day period (from Friday 9 September until Thursday 15 September) at 17 locations.

vi Numberplate surveys at 10 locations, to identify the access routes of those currently utilising Station Street during a typical weekday AM period (Tuesday 13 September).

4.4 Data Analysis

4.4.1 Intersection Summaries

GTA Consultants undertook traffic movement counts at the intersections listed above on Tuesday 13 September 2016.

The AM (8:00am-9:00am) and PM (5:00pm-6:00pm) peak hour traffic volumes are shown in Figure 4.1 and Figure 4.2, respectively.

[4] Sydney Coordinated Adaptive Traffic System (SCATS) is a sophisticated and dynamic intelligent transport system used for day to day monitoring and operation. It provides traffic signal coordination that improves both traffic flow and safety for all road users.

Figure 4.1: Existing AM Peak Hour Traffic Volumes (8:00am-9:00am)

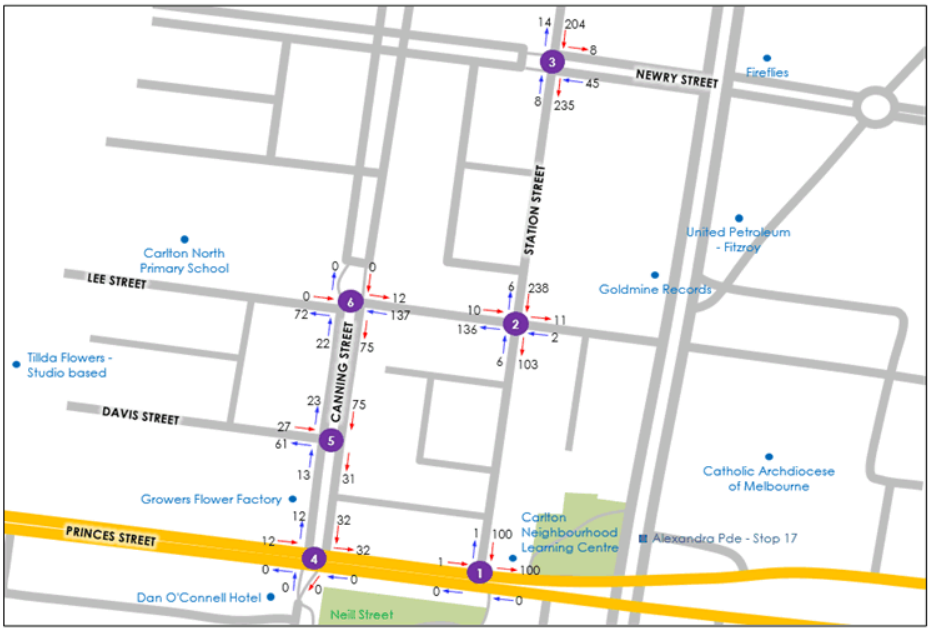


Figure 4.2: Existing PM Peak Hour Traffic Volumes (5:00pm-6:00pm)

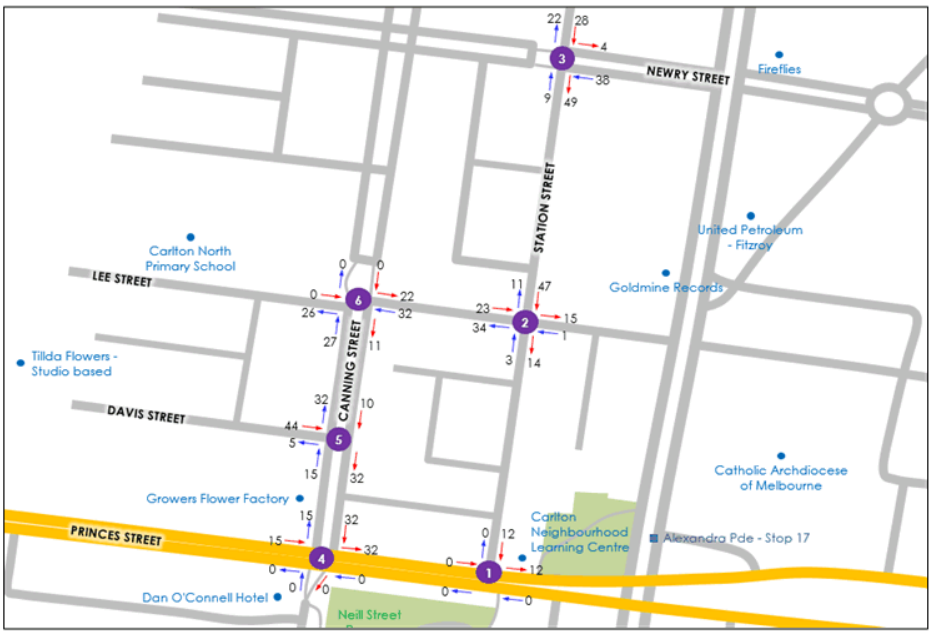


Figure 4.1 and Figure 4.2 indicates that along and proximate to Station Street the following key movements currently occur:

- In the AM peak period the main route through the local road network is via Station Street, Lee Street, Canning Street and Davis Street.
- In the AM peak 100 vehicles access Princes Street via Station Street, and only 32 vehicles access Princes Street via Canning Street.
- In the PM peak period the volumes within the local road network are quite low, with the highest movement being 42 vehicles travelling out of Davis Street onto Canning Street, of which approximately half turn left and the other half right towards Princes Street.
- In the PM peak 12 vehicles access Princes Street via Station Street, and again 32 vehicles access Princes Street via Canning Street.

4.4.2 Canning Street Bicycle Data

Super Tuesday

Data undertaken during the Super Tuesday Cycle Count (undertaken on 1 March 2015 from 7:00am-9:00am and 3 March 2016 from 7:00am-9:00am) indicates that there has been a 15% increase in cyclist traffic at the Canning Street/Princes Street intersection in the last year. Further, these counts have shown that Canning Street has the third highest on-road cyclist volumes travelling into the CBD (after St Kilda Road and Royal Parade).

Table 4.1: Super Tuesday Cycle Count Data (7:00am to 9:00am)

Intersection	2015 Volume	2016 Volume	Change	% Difference
Canning St/Princes Street (all directions)	1,054	1,215	+161	+15%

GTA Cycle Counts

GTA conducted counts of the Canning Street / Princes Street intersection in the AM (8-9) and PM (5-6) peak. A summary of the peak hour results is provided as follows:

- AM Volumes:
  - Southbound - 299 (i.e. toward the city)
  - Northbound - 22 (i.e. away from city)
- PM Volumes:
  - Southbound - 11 (i.e. toward the city)
  - Northbound - 223 (i.e. away from city)

4.4.3 Pneumatic Tube Counts

Pneumatic tubes were installed in a number of locations in the nearby area to determine the existing traffic flows through the road network over the course of the week starting Friday 9 September until Thursday 15 September.

During that week, the day during which the network accommodated the highest traffic demand was on Thursday, and the traffic conditions recorded on this day are summarised in Table 4.2.

Table 4.2: Existing Traffic Flows

Location	AM Peak (8:00-9:00)	PM Peak (3:00-4:00)	PM Peak (5:00-6:00)	Daily Total	85th %ile Speed
Pidgon Street between Drummond Street and Rathdowne Street	509	550	632	6,411	39.7
Rathdowne Street south of Pidgon Street	512	645	829	8,461	41.4
Richardson Street between Drummond Street and Rathdowne Street	181	146	173	1,828	27.6
Drummond Street between Richardson Street and Macpherson Street	255	57	67	1,123	39.9
Fenwick Street between Drummond Street and Rathdowne Street	97	33	36	626	36.1
Drummond Street between Newry Street and Lee Street	368	64	66	1,336	39.1
Rathdowne Street between Newry Street and Ogrady Street	574	931	1,148	12,506	40.7
Davis Street between ROWY and Canning Street	92	69	54	719	45.0
Canning Street between Princess Street and Davis Street	46	45	53	548	35.2
Station Street near #207	93	27	22	452	30.6
Lee Street between Station Street and Canning Street	131	55	52	799	34.8
Station Street near #258	208	60	65	1,028	37.5
Rae Street between Alexandra Parade and York Street	238	110	111	1,649	38.9
Fenwick Street between Station Street and Canning Street	90	80	87	1,115	37.7
Richardson Street between Station Street and Canning Street	358	292	334	3,691	37.7
Pidgon Street between Station Street and Canning Street	449	429	446	5,202	37.0

Table 4.2 indicates that the local roads in the area all generally operate as per their classification within the road network, as indicated through the traffic volume ranges and target speeds for each road type set out in Table C1 of Clause 56.06 of the Yarra Planning Scheme. The only roads that do not strictly comply with Table C1 of Clause 56.06 of the Yarra Planning Scheme are Rathdowne Street and Richardson Street, which exceed the upper daily traffic volume ranges of 7,000 and 3,000 vehicles per day, but significantly and only over specific blocks, which is common within built up areas like Carlton North.

While the Thursday was identified to accommodate the highest traffic volumes, the majority of the survey activities occurred on Tuesday 13 September, which based on the pneumatic tube counts accommodated traffic volumes that were 6% less than what was recorded on the Thursday, which is not considered to be material on the overall operation of the road network.

As such, and for comparison purposes, further detail relating to the AM and PM peak period results from the pneumatic tube counts are discussed based on what was recorded on the Tuesday below.

Morning Peak

Figure 4.3 shows the morning peak for vehicles travelling in the wider North Carlton neighbourhood that may be affected by the proposed closure of Station Street.

Figure 4.3: Morning Peak, Tuesday 13 September – 8:00am-9:00am

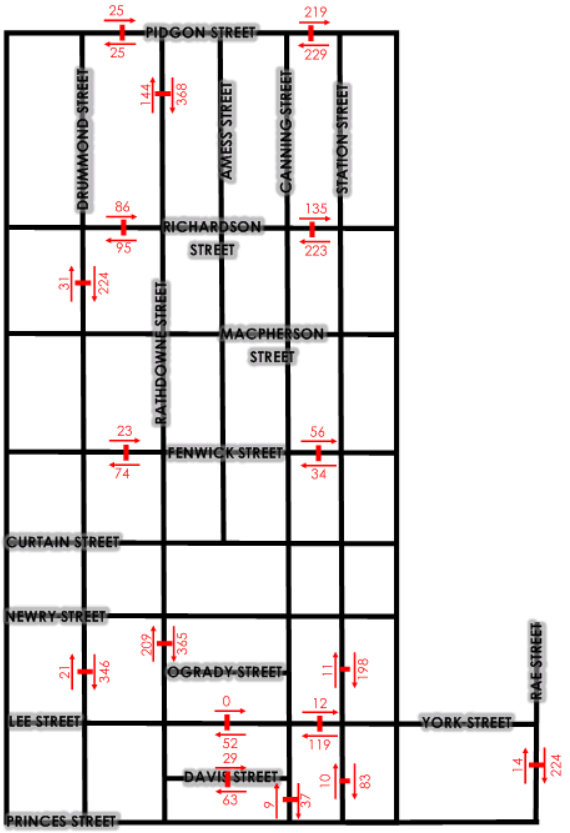


Figure 4.3 indicates that:

- Most (58%) vehicles that use Station Street north of Lee Street do not continue along Station Street between Lee Street and Princes Street
- Station Street is more utilised than Canning Street for vehicles wishing to travel eastbound onto Princes Street, with 99 vehicles exiting Station Street and 41 exiting Canning Street
- Station Street carries far fewer vehicles (198) than Rathdowne Street (365) or Drummond Street (346)between Lee Street and Newry Street.
- Vehicles travelling along Station Street north of Lee Street exiting the neighbourhood to the west are using Lee Street or Davis Street.

Afternoon Peak

In order to ensure that the peak traffic was recorded correctly, two afternoon peaks have been reported as follows to enable consideration of both the PM peak associated with the Carlton North Primary School and typical commuter peak period:

- School peak between 3:00pm and 4:00pm, as shown in Figure 4.4
- Commuter peak between 5:00pm and 6:00pm, as shown in Figure 4.5.

Figure 4.4: School PM Peak, Tuesday 13 September – 3:00pm-4:00pm

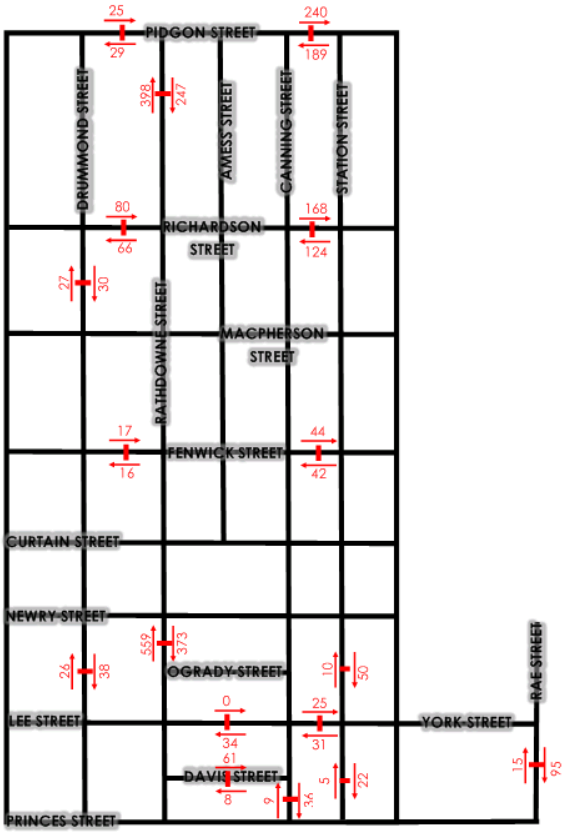


Figure 4.4 indicates the school PM peak occurs from 3:00pm to 4:00pm. During this time, the following has been identified from the recorded traffic:

- Station Street is less utilised than any of the surrounding streets for southbound movements
- Majority of vehicles travelling southbound along Station Street turn right into Lee Street rather than continuing along Station Street to Princes Street.

Figure 4.5: Commuter PM Peak, Tuesday 13 September – 5:00pm-6:00pm

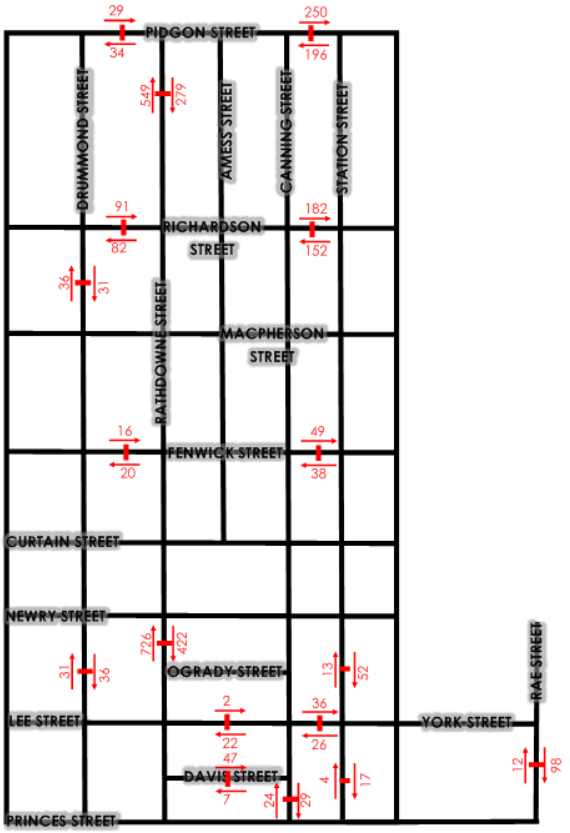


Figure 4.5 indicates the commuter PM peak occurs from 5:00pm to 6:00pm. During this time, the following has been identified from the recorded traffic:

- The commuter PM peak accommodates higher traffic volumes than the PM school peak.
- Local traffic movement patterns are generally the same in the commuter PM peak as the PM school peak.

It is noted that there are some minor discrepancies between the pneumatic tube counts and the intersection turning movement counts. These are largely explained due to the differences in the locations the associated surveys occurred (i.e. the pneumatic tube counts don't occur at the intersections), and that there are traffic activities occurring between them.

4.4.4 Origin – Destination Data

In order to determine the routes motorists utilise in accessing the Station Street / Princes Street intersection, origin destination data was collected for vehicles passing a number of checkpoints in the nearby local road network.

These surveys were completed on Tuesday 13 September 2016, between 7:30 and 9:30am, considered to be the peak time for vehicles to be accessing the network.

Sites that origin destination data has been collected are marked in blue and purple in Figure 4.6. The red numbers adjacent to each site show the number of trips that have originated from the said site and are recorded going through the Station Street / Princes Street intersection (total number is indicated in red next to the Station Street / Princes Street intersection.

Figure 4.6: Origin and Destination of Vehicles using Station Street

Figure 4.6 indicates the following in terms of the origin and destination of vehicles in the AM peak hour that used Station Street to access Princes Street:

- A total of 24 vehicles originated from one of the other nine sites
- No vehicles from Lee Street used Station Street to access Princes Street
- 14 vehicles originated from outside the North Carlton neighbourhood
- The remaining 10 vehicles that were only detected at Site #8 (Station Street / Newry Street intersection) are considered to originate from within the local neighbourhood.

4.4.5 Traffic Volume Comparisons

In order to confirm whether the traffic data collected in September 2016 was representative of typical conditions the following has been identified:

- Surveys occurred on a typical weekday, noting that it was during the school term and no public holidays occurred across the associated week
- SCATS data of the signalised intersections along Princes Street for a number of days throughout the year has found that the volumes recorded along Princes Street on Tuesday 13 September 2016 was around 85% of the peak volumes identified.
- A comparison between traffic volumes recorded in 2009 on a number of the local roads to those in September 2016 are presented in Table 4.3 and show they are generally similar, except along Station Street (between Princes Street and Lee Street), where volumes have decreased significantly.

Table 4.3: Data Comparison 2009 and 2016

Location (2009)	Daily Volume (2009)	Comparative Location (2016)	Daily Volume (2016)
Canning Street between Davis Street and Lee Street	559	Canning Street between Princess Street and Davis Street	620
Davis Street between Rathdowne Street and Canning Street	759	Davis Street between Rathdowne Street and Canning Street	719
Lee Street between Station Street and Canning Street	921	Lee Street between Station Street and Canning Street	854
Lee Street between Rathdowne Street and Canning Street	432	Lee Street between Rathdowne Street and Canning Street	391
Station Street between Lee Street and Newry Street	1,019	Station Street near #258	1,028
Station Street between Princes Street and Lee Street	747	Station Street near #207	473

It is also noted that since 2009 and 2016 there has not been any significant developments accommodated proximate to Station Street, except for general increased residential dwelling densities, which are gradual in this area. This is reflected in the traffic volume comparisons in Table 4.3, where the traffic volumes in 2009 are similar to those in 2016.

4.5 Station Street Traffic Generation

Only 24 of the 100 vehicles that use Station Street to access Princes Street in the AM peak hour come from a location to the north of Newry Street. This occurs even though the traffic volumes at each intersection along Station Street between Newry Street and Princes Street seem to be fairly balanced, i.e. the number of movements at each intersection are generally the same.

However, the other 76 vehicles are believed to be generated from the mostly residential dwellings accessed from the rear laneways that intersect Station Street and the many on-street car spaces along Station Street. In essence, there are vehicles starting and finishing their trips within each block, as well as some turning around, such as those accessing the Carlton Neighbourhood Learning Centre.

This can be seen through Figure 4.7, which shows on an aerial photograph the following being accommodated in the associated area that generates these vehicles movements:

- in the order of 100 kerbside car parking spaces
- in the order of 200 dwellings accessed from connecting rear laneways.

Figure 4.7: Map of Where 76 Vehicles Exiting Station Street is Generated From

Given the level of development and public on-street car parking that exists in the area identified in Figure 4.7, it is considered reasonable that some 76 vehicle movements could be generated along Station Street that want to access Princes Street at its southern end, as well as various other vehicles finishing their trips in this area of turning around after a short stop, such as the Carlton Neighbourhood Learning Centre.

5. Traffic Impact

5.1 Intersection Modelling

Intersection modelling has been undertaken using SIDRA Intersection 7[5], in order to understand how the following intersections currently operate, and how they might in the future should Station Street be closed.

- Nicholson Street / Princes Street signalised intersection
- Canning Street / Princes Street give-way controlled T-intersection
- Rathdowne Street /Princes Street signalised intersection.

The commonly used measure of intersection performance is referred to as the *Degree of Saturation (DOS)*. The DOS represents the flow-to-capacity ratio for the most critical movement on each leg of the intersection. For signalised intersections, a DOS of around 0.95 has been typically considered the 'ideal' limit, and for non-signalised intersections a DOS of 0.90 has been typically considered the 'ideal' limit[6], beyond which queues and delays increase disproportionately.

5.1.1 Existing Conditions

Table 5.1 presents a summary of the existing operation of the three intersections, with full results presented in Appendix A of this report.

Table 5.1: Existing Operating Conditions

Peak Hour	Approach	DOS	Average Delay (sec)	95th Percentile Queue (m)
Princes Street and Rathdowne Street				
AM	Rathdowne Street (South)	0.764	66 sec	60.2 m
	Princes Street (East)	0.789	25 sec	344.6 m
	Rathdowne Street (North)	0.562	57 sec	107.0 m
	Princes Street (West)	# 0.791	27 sec	180.1 m
PM	Rathdowne Street (South)	0.796	44 sec	259.7 m
	Princes Street (East)	# 0.798	36 sec	309.9 m
	Rathdowne Street (North)	0.527	66 sec	71.3 m
	Princes Street (West)	0.789	37 sec	296.1 m

[5] SIDRA is used for intersection and network capacity, level of service and performance analysis. Program used under license from Akcelik & Associates Pty Ltd.

[6] SIDRA INTERSECTION adopts the following criteria for Level of Service assessment:

Level of Service		Intersection Degree of Saturation (DOS)		
		Unsignalised Intersection	Signalised Intersection	Roundabout
A	Excellent	<=0.60	<=0.60	<=0.60
B	Very Good	0.60-0.70	0.60-0.70	0.60-0.70
C	Good	0.70-0.80	0.70-0.90	0.70-0.85
D	Acceptable	0.80-0.90	0.90-0.95	0.85-0.95
E	Poor	0.90-1.00	0.95-1.00	0.95-1.00
F	Very Poor	>=1.0	>=1.0	>=1.0

Peak Hour	Approach	DOS	Average Delay (sec)	95th Percentile Queue (m)
Princes Street and Canning Street				
AM	Canning Street (North)	0.023	7 sec	3.6 m
	Princes Street (West)	0.436	6 sec	108.0 m
PM	Canning Street (North)	0.023	7 sec	3.6 m
	Princes Street (West)	# 0.568	7 sec	167.4 m
Princes Street, Nicholson Street and Alexandra Parade				
AM	Nicholson Street (South)	# 0.802	64 sec	73.0 m
	Alexandra Parade (East)	0.754	29 sec	341.1 m
	Nicholson Street (North)	0.792	64 sec	172.7 m
	Princes Street (West)	0.681	27 sec	274.8 m
PM	Nicholson Street (South)	0.872	63 sec	284.0 m
	Alexandra Parade (East)	0.796	36 sec	293.7 m
	Nicholson Street (North)	0.677	60 sec	132.4 m
	Princes Street (West)	# 1.019	130 sec	708.5 m

DOS – Degree of Saturation, # - Intersection DOS

Table 5.1 indicates that the above intersections currently operates well with minimal queues and delays on all approaches, except for the Princes Street / Nicholson Street / Alexandra Parade intersection in the PM peak, whereby the western leg is at capacity during this period.

It is also noted that the existing conditions SIDRA Model in the AM for the Rathdowne Street / Princes Street intersection and Canning Street / Princes Street intersection indicates queue lengths on the north approaches that are less than what was observed on-site. This is due to the SIDRA Models considering the associated intersections in isolation. In reality they are impacted by constraints and queuing that occurs at adjacent intersections.

While these intersections have not been calibrated to try and reflect the queue lengths observed, the following should be noted and applied to the post-development models to better reflect what the actual queuing will be:

- Rathdowne Street / Princes Street intersection: North approach queue was 107m in the SIDRA Model but 350m observed due to congestion along Rathdowne Street to the south.
- Canning Street / Princes Street intersection: North approach queue was 4m in the SIDRA Model but 14m observed due to queuing back from Nicholson Street on Princes Street.

5.1.2 Post Road Closure

With the closure of Station Street, it is not clear exactly how motorists that currently use it to access Princes Street will change their behaviour. However, for assessment purposes the following two scenarios have been tested:

- **Scenario 1:** Closure of Station Street, with traffic redistributed throughout the local road network (i.e. Canning Street, Lee Street and Davis Street) in the same proportions as currently exhibited.
- **Scenario 2:** Closure of Station Street, with all vehicles using the Station Street / Princes Street intersection assumed to now use the Canning Street / Princes Street intersection.

Moreover, it is acknowledged that only in the AM peak period will there be a significant change in traffic patterns in the local road network with the closure of Station Street.

As such, Table 5.2 and Table 5.3 show the additional volumes in the AM peak of the local road network for both Scenario 1 and Scenario 2, respectively.

Table 5.2: Scenario 1 – Additional Volumes

Street	Mid-Block Location	Direction	Existing	Future		% Change
				Additional	Total	
Davis Street	between Canning Street and Rathdowne Street	Eastbound	27	41	68	152%
		Westbound	61	-	61	-
Lee Street	between Canning Street and Rathdowne Street	Westbound	72	47	119	65%
	between Station and Canning Street	Eastbound	9	-	9	-
		Westbound	136	101	237	74%
Canning Street	between Princes Street and Davis Street	Northbound	11	1	12	9%
		Southbound	29	11	40	38%
	between Davis Street and Lee Street	Northbound	10	2	12	20%
		Southbound	75	54	129	72%
Station Street	between Lee Street and Newry Street	Northbound	235	-	235	-
		Southbound	8	-	8	-

Table 5.2 shows that by using the existing distribution throughout the local road network, in the AM peak, 47 vehicles continue westbound along Lee Street, while 54 travel southbound along Canning Street, with only 11 continuing to the Princes Street intersection. These numbers are low, with no more than one additional vehicle movement per minute on each of these roads to what they currently accommodate. As such, is not expected to have a material impact on their current operations.

Table 5.3: Scenario 2 – Additional Volumes

Street	Mid-Block Location	Direction	Existing	Future		% Change
				Additional	Total	
Davis Street	between Canning Street and Rathdowne Street	Eastbound	27	-	27	-
		Westbound	61	-	61	-
Lee Street	between Canning Street and Rathdowne Street	Westbound	72	-	72	-
	between Station and Canning Street	Eastbound	9	-	9	-
		Westbound	136	101	237	74%
Canning Street	between Princes Street and Davis Street	Northbound	11	1	12	9%
		Southbound	29	101	130	348%
	between Davis Street and Lee Street	Northbound	10	-	10	-
		Southbound	75	101	176	135%
Station Street	between Lee Street and Newry Street	Northbound	235	-	235	-
		Southbound	8	-	8	-

Table 5.3 demonstrates that all 101 vehicles currently using Station Street will continue along Lee and Canning Street, to exit at Princes Street.

Furthermore, based on only the Canning Street / Princes Street intersection of those been modelled will experience significant changes to their future operation.

As such, Table 5.4 presents a summary of the future operation of the Canning Street / Princes Street intersection under the above two scenarios, with full results of the intersection modelling provided in Appendix A of this report.

Table 5.4: Canning Street / Princes Street Intersection 8:00am-9:00am

Scenario	Approach	DOS	Average Delay (sec)	95th Percentile Queue	
				veh	m
Scenario 1	Canning Street (North)	0.031	9 sec	0.7 veh	4.9 m
	Princes Street (West)	0.436	6 sec	15.4 veh	108.1 m
Scenario 2	Canning Street (North)	0.095	10 sec	2.3 veh	15.9 m
	Princes Street (West)	0.436	6 sec	15.4 veh	108.1 m

DOS – Degree of Saturation, # - Intersection DOS
Note: This model is based on an existing conditions model that has not been calibrated and is only appropriate for comparative purposes to understand the extent of changes that occur in the operation of the intersection.

Table 5.4 indicates the following with the future operation of the Canning Street / Princes Street intersection based on the above two scenarios with the closure of Station Street:

- **Scenario 1:** Based on traffic volumes generally changing based on the current distributions in the area, the intersection still operates well, but queuing along Canning Street increases from 3m to 5m. However, calibrating the results to reflect observed conditions indicates that the existing 14m (two vehicle) queue will increase to 21m (3 vehicles).
- **Scenario 2:** Based on all the traffic volumes from Station Street using Canning Street to access Princes Street, the intersection still operates well, but the queuing along Canning Street increases from 3m to 16m. Once these results have been calibrated, the existing 14m (two vehicle) queue will increase to 63m (9 vehicles).

5.2 Network Implications

Based on the access arrangements available with the local road network in this area and the existing traffic volumes that have been recorded, it is considered that the most likely of the two scenarios to occur with the closure of Station Street at Princes Street is Scenario 2. This is considered to be the case as you can only turn-left-out onto Princes Street, so these vehicles will be expected to continue to do this if forced to Canning Street instead of changing the direction they are travelling in a congested network, which is what is required based on Scenario 1.

On this basis, it is expected that queuing along Canning Street back from Princes Street will likely extend to Davis Street at times. This will in turn result in some of the 75 vehicles that turn right into Davis Street to be caught up in this queue, which will increase these users' delays.

In terms of the cyclists that currently use Canning Street, the increased southbound traffic volumes with the closure of Station Street is not considered to have a significant impact on them. However, it is considered to be safer that cars travelling along Canning Road in a southbound direction before turning right into Davis Street do so in free-flow conditions, because if queued they won't have as clear sight-lines of the approaching cyclists as drivers will need to look over their shoulders.

It is also noted that with increased left-turning volumes coming out of Canning Street that there is a potential for increased conflicts with pedestrians crossing Princes Street, as drivers will be looking to the west for a gap in the traffic stream, while the pedestrian crossing facility is on the left-hand side of Canning Street. It is noted that a left-turn red arrow is in place to help with this potential conflict, but is not held for the entire time pedestrians are crossing Princes Street.

Beyond this, there is not expected to be any significant impacts on the rest of the existing transport network (noting 14 vehicles use the local road network in this precinct), whether it is the road based public transport services / reliability, arterial road network or emergency vehicles access arrangements.

6. Conclusions & Recommendations

6.1 Summary of Conclusions

Based on the analysis and discussions presented within this report, the following conclusions have been made regarding the temporary closure of Station Street at Princes Street:

- The closure was previously suggested in a LATM Study in 2003 for Northern Carlton as a potential response to expected changes in local traffic movements.
- Station Street is a local road that, at its southern end, intersects with Princes Street, which is an arterial road managed by VicRoads, and based on their SmartRoads Policy is a Preferred Traffic Route
- Station Street carries approximately 1,000 vehicles per day, with 100 vehicles in the AM peak hour, 22 vehicles in the school PM peak hour and 12 vehicles in the commuter PM peak hour turn left-out and access Princes Street.
- There are no accidents that have been recorded along Station Street between and including Newry Street and Princes Street over the latest available five-year period, noting the Station Street / Princes Street intersection was closed over a 15 month period
- From on-site surveys the vehicles turning left out of Station Street did the following:
  - In the AM period (7:45-8:15am), 48 vehicles turned left out of Station Street, with three (6%) vehicles moving to the right hand lane to turn right onto Nicholson Street.
  - In the PM period (5:00-5:30pm), six vehicles turned left out of Station Street, with one (16%) vehicle moving to the right hand lane to turn right onto Nicholson Street.
- Of the 100 vehicles that turned out of Station Street in the AM peak hour, 24 vehicles were identified to have originated from a location north of Newry Street, with only 14 of these originated from outside the North Carlton neighbourhood, so the majority are considered to be local generated traffic.
- Should those that currently use Station Street to access Princes Street use Canning Street when it is closed, it is expected that queuing along Canning Street will occur, namely as queuing back from Princes Street will extend back past Davis Street at times and also prevent the 75 vehicles wanting to turn right into Davis Street.
- Potential safety impacts with pedestrians and cyclists as follows may occur with the additional traffic using Canning Street:
  - Queued vehicles waiting to turn right into Davis Street may not see the southbound cyclists as clearly as they would do in free flow conditions
  - When vehicles turn left out of Canning Street they are looking to the right, but the signalised crossing facility on Princes Street is on the left-hand side of Canning Street
- Broader impacts on the existing transport network are considered to be minimal with the closure of Station Street.

6.2 Recommendation

The analysis and discussions presented within this report has identified that there is not an existing issue with regard to the level of traffic and speed on Station Street for access to Princes Street. Those that currently use Station Street to access Princes Street have been found to mostly be generated by those that live on and proximate to Station Street (i.e. local trips).

However, there are a relatively small number of vehicles that cross the three through lanes to access the right-turn lane on Princes Street to travel southbound on Nicholson Street. While this is not ideal, and has some potential safety issues, there is no crash history and the manoeuvre occurs in a congested road environment, where vehicle speeds on Princes Street are low, at least in the peak commuter periods.

The analysis and on-site observations indicate that there is no existing significant queuing or delays in accessing Princes Street from Station Street or Canning Street. However, should Station Street be closed the majority of the traffic accessing Princes Street via Station Street will then use Canning Street, which is expected to see queues increase from 14m (two vehicle) to 63m (9 vehicles). With this increased queuing on Canning Street, it will at times extend back pass Davis Street and also prevent the 75 vehicles wanting to turn right into Davis Street, which will increase delays for all these vehicles.

The increased queuing on Canning Street following the closure of Station Street could be mitigated through modified signal coordination on Princes Street, but would require VicRoads support and ongoing monitoring.

In summary, there is not considered to be an existing issue with the operation and use of Station Street. It has low speeds, volumes and crash history with the majority of users being local generated trips. However, there is a potential for increased congestion on Canning Street with the closure of Station Street due to the redistributed traffic volumes still wanting to access Princes Street. As such, the closure of Station Street at Princes Street is not considered to achieve any major benefits to the network, and has the potential to result in more congestion on Canning Street.

Appendix A

SIDRA Intersection 7 Outputs

Attachment 2 - Traffic Impact Assessment Study

MOVEMENT SUMMARY

Site: 4381 [Princes /Rathdowne AM 8-9]

New Site
Signals - Fixed Time Isolated Cycle Time = 150 seconds (User-Given Cycle Time)
Variable Sequence Analysis applied. The results are given for the selected output sequence.

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Rathdowne Street											
1	L2	63	0.0	0.098	40.5	LOS D	3.1	21.5	0.71	0.72	35.4
2	T1	139	0.0	0.334	53.7	LOS D	8.6	60.2	0.89	0.73	32.1
3	R2	197	0.0	0.764	83.3	LOS F	7.7	53.7	1.00	0.89	20.0
Approach		399	0.0	0.764	66.2	LOS E	8.6	60.2	0.92	0.81	26.2
East: Princes Street											
4	L2	133	0.0	0.789	31.8	LOS C	48.9	342.1	0.85	0.80	35.6
5	T1	2306	0.0	0.789	25.0	LOS C	49.2	344.6	0.81	0.75	37.5
6	R2	40	0.0	0.149	21.5	LOS C	1.1	7.5	0.67	0.71	39.0
Approach		2479	0.0	0.789	25.3	LOS C	49.2	344.6	0.81	0.75	37.4
North: Rathdowne Street											
7	L2	4	0.0	0.562	62.4	LOS E	15.3	107.0	0.95	0.80	25.2
8	T1	447	0.0	0.562	56.6	LOS E	15.3	107.0	0.94	0.79	31.3
9	R2	26	0.0	0.128	65.5	LOS E	1.7	11.8	0.90	0.72	28.7
Approach		478	0.0	0.562	57.1	LOS E	15.3	107.0	0.94	0.79	31.1
West: Princes Street											
10	L2	549	0.0	0.526	26.1	LOS C	24.8	173.4	0.66	0.79	41.3
11	T1	1194	0.0	0.526	20.6	LOS C	25.7	180.1	0.66	0.60	40.3
12	R2	185	0.0	0.791	66.6	LOS E	10.9	76.0	1.00	1.01	28.6
Approach		1928	0.0	0.791	26.6	LOS C	25.7	180.1	0.69	0.69	38.7
All Vehicles		5284	0.0	0.791	31.8	LOS C	49.2	344.6	0.79	0.74	35.7

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Average Back of Queue Distance m	Prop. Queued	Effective Stop Rate per ped
P1	South Full Crossing	53	21.4	LOS C	0.1	0.1	0.53	0.53
P2	East Full Crossing	53	62.7	LOS F	0.2	0.2	0.92	0.92
P3	North Full Crossing	53	4.8	LOS A	0.1	0.1	0.25	0.25
P4	West Full Crossing	53	63.6	LOS F	0.2	0.2	0.92	0.92
All Pedestrians		211	38.1	LOS D			0.66	0.66

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

Attachment 2 - Traffic Impact Assessment Study

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Organisation: GTA CONSULTANTS | Processed: Thursday, 20 October 2016 10:27:18 AM
Project: P:\V10500-10599\V105900 - Station Street Road Closure, North Carlton\Modelling\4381 Princes Rathdowne\161020sidra-V105900-Princes Rathdowne.sip7

Attachment 2 - Traffic Impact Assessment Study

MOVEMENT SUMMARY

Site: 4381 [Princes /Rathdowne PM 5-6]

New Site
Signals - Fixed Time Isolated Cycle Time = 150 seconds (User-Given Cycle Time)
Variable Sequence Analysis applied. The results are given for the selected output sequence.

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	95% Back of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Rathdowne Street											
1	L2	156	0.0	0.188	31.9	LOS C	6.7	47.2	0.64	0.74	38.7
2	T1	569	0.0	0.796	45.6	LOS D	37.1	259.7	0.96	0.87	34.5
3	R2	581	0.0	0.686	45.7	LOS D	16.2	113.1	0.94	0.83	28.4
Approach		1306	0.0	0.796	44.0	LOS D	37.1	259.7	0.91	0.84	32.5
East: Princes Street											
4	L2	224	0.0	0.791	40.2	LOS D	43.9	307.5	0.91	0.85	31.5
5	T1	1603	0.0	0.791	34.2	LOS C	44.3	309.9	0.86	0.79	32.9
6	R2	153	0.0	0.798	51.0	LOS D	7.2	50.4	1.00	0.92	26.8
Approach		1980	0.0	0.798	36.2	LOS D	44.3	309.9	0.88	0.81	32.2
North: Rathdowne Street											
7	L2	46	0.0	0.527	70.1	LOS E	10.2	71.3	0.97	0.79	23.0
8	T1	246	0.0	0.527	64.4	LOS E	10.2	71.3	0.97	0.79	29.2
9	R2	29	0.0	0.258	75.0	LOS E	2.1	14.5	0.96	0.74	26.7
Approach		322	0.0	0.527	66.2	LOS E	10.2	71.3	0.97	0.79	28.1
West: Princes Street											
10	L2	25	0.0	0.789	43.1	LOS D	42.2	295.6	0.92	0.84	36.5
11	T1	1885	0.0	0.789	36.6	LOS D	42.3	296.1	0.90	0.81	32.1
12	R2	63	0.0	0.365	35.7	LOS D	2.3	16.2	0.89	0.75	37.6
Approach		1974	0.0	0.789	36.6	LOS D	42.3	296.1	0.90	0.81	32.4
All Vehicles		5582	0.0	0.798	39.9	LOS D	44.3	309.9	0.90	0.81	32.0

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Average Back of Queue Distance m	Prop. Queued	Effective Stop Rate per ped
P1	South Full Crossing	53	30.1	LOS D	0.1	0.1	0.63	0.63
P2	East Full Crossing	53	69.3	LOS F	0.2	0.2	0.96	0.96
P3	North Full Crossing	53	12.6	LOS B	0.1	0.1	0.57	0.57
P4	West Full Crossing	53	44.2	LOS E	0.2	0.2	0.77	0.77
All Pedestrians		211	39.0	LOS D			0.73	0.73

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

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Organisation: GTA CONSULTANTS | Processed: Thursday, 20 October 2016 10:27:20 AM
Project: P:\V10500-10599\V105900 - Station Street Road Closure, North Carlton\Modelling\4381 Princes Rathdowne\161020sidra-V105900-Princes Rathdowne.sip7

MOVEMENT SUMMARY

Site: 4415 [Princes Street/Canning Street AM - 8-9 - Existing]

Three-way intersection with "Seagull" treatment (Signals)
Signals - Fixed Time Isolated Cycle Time = 150 seconds (User-Given Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
East: Princes Street											
5	T1	2555	0.0	0.555	6.3	LOS A	22.9	160.6	0.41	0.38	45.8
Approach		2555	0.0	0.555	6.3	LOS A	22.9	160.6	0.41	0.38	45.8
North: Canning Street											
7	L2	34	0.0	0.023	7.0	LOS A	0.5	3.6	0.23	0.53	36.0
Approach		34	0.0	0.023	7.0	LOS A	0.5	3.6	0.23	0.53	36.0
West: Princes Street											
10	L2	13	0.0	0.436	11.0	LOS B	15.4	107.9	0.35	0.33	51.1
11	T1	1993	0.0	0.436	5.4	LOS A	15.4	108.0	0.35	0.33	47.3
Approach		2005	0.0	0.436	5.5	LOS A	15.4	108.0	0.35	0.33	47.4
All Vehicles		4594	0.0	0.555	6.0	LOS A	22.9	160.6	0.39	0.36	46.3

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Distance m	Prop. Queued	Effective Stop Rate per ped
P2	East Full Crossing	53	69.3	LOS F	0.2	0.2	0.96	0.96
P3	North Full Crossing	53	5.9	LOS A	0.1	0.1	0.28	0.28
P4	West Full Crossing	53	69.3	LOS F	0.2	0.2	0.96	0.96
All Pedestrians		158	48.1	LOS E			0.73	0.73

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

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Organisation: GTA CONSULTANTS | Processed: Tuesday, 25 October 2016 11:10:09 AM
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MOVEMENT SUMMARY

Site: 4415 [Princes Street/Canning Street PM - 5-6 - Existing]

Three-way intersection with "Seagull" treatment (Signals)
Signals - Fixed Time Isolated Cycle Time = 150 seconds (User-Given Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
East: Princes Street											
5	T1	2006	0.0	0.436	5.4	LOS A	15.4	108.0	0.35	0.32	47.4
Approach		2006	0.0	0.436	5.4	LOS A	15.4	108.0	0.35	0.32	47.4
North: Canning Street											
7	L2	34	0.0	0.023	7.0	LOS A	0.5	3.6	0.23	0.53	36.0
Approach		34	0.0	0.023	7.0	LOS A	0.5	3.6	0.23	0.53	36.0
West: Princes Street											
10	L2	16	0.0	0.568	12.0	LOS B	23.9	167.2	0.42	0.40	50.0
11	T1	2598	0.0	0.568	6.5	LOS A	23.9	167.4	0.42	0.39	45.5
Approach		2614	0.0	0.568	6.5	LOS A	23.9	167.4	0.42	0.39	45.6
All Vehicles		4654	0.0	0.568	6.0	LOS A	23.9	167.4	0.39	0.36	46.2

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Distance m	Prop. Queued	Effective Stop Rate per ped
P2	East Full Crossing	53	69.3	LOS F	0.2	0.2	0.96	0.96
P3	North Full Crossing	53	5.9	LOS A	0.1	0.1	0.28	0.28
P4	West Full Crossing	53	69.3	LOS F	0.2	0.2	0.96	0.96
All Pedestrians		158	48.1	LOS E			0.73	0.73

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

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Project: P:\V10500-10599\V105900 - Station Street Road Closure, North Carlton\Modelling\4415 Princes Canning\161019sidra-V105900 - Princes Canning 4415.sip7

MOVEMENT SUMMARY

Site: 1 [Existing AM Peak - Test - Updated Vols]

Alexandra Parade and Nicholson Street
Signals - Fixed Time Isolated Cycle Time = 160 seconds (User-Given Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Nicholson Street (South Approach)											
1	L2	91	0.0	0.438	62.7	LOS E	10.2	72.1	0.90	0.77	29.8
2	T1	217	3.0	0.438	57.1	LOS E	10.2	72.1	0.90	0.75	30.9
3	R2	153	0.0	0.802	74.8	LOS E	10.4	72.5	1.00	0.99	26.9
Approach		460	1.4	0.802	64.1	LOS E	10.4	73.0	0.93	0.83	29.3
East: Alexandra Parade (East Approach)											
4	L2	553	0.0	0.487	22.0	LOS C	22.4	156.8	0.58	0.77	43.2
5	T1	2393	5.0	0.754	28.6	LOS C	46.7	341.1	0.83	0.77	40.9
6	R2	89	0.0	0.723	89.5	LOS F	7.3	51.0	1.00	0.84	24.3
Approach		3035	3.9	0.754	29.2	LOS C	46.7	341.1	0.79	0.77	40.5
North: Nicholson Street (North Approach)											
7	L2	135	0.0	0.792	69.4	LOS E	24.3	172.7	1.00	1.00	28.8
8	T1	461	3.0	0.792	64.3	LOS E	24.3	172.7	0.97	0.93	29.2
9	R2	100	0.0	0.403	57.4	LOS E	6.3	44.2	0.90	0.78	30.8
Approach		696	2.0	0.792	64.3	LOS E	24.3	172.7	0.97	0.92	29.3
West: Alexandra Parade (West Approach)											
10	L2	35	0.0	0.681	30.7	LOS C	22.9	166.4	0.68	0.62	41.5
11	T1	2047	5.0	0.681	26.8	LOS C	37.6	274.8	0.72	0.65	41.7
12	R2	35	0.0	0.281	36.4	LOS D	1.4	9.8	0.87	0.74	37.2
Approach		2117	4.8	0.681	27.0	LOS C	37.6	274.8	0.72	0.65	41.6
All Vehicles		6307	3.8	0.802	34.9	LOS C	46.7	341.1	0.80	0.75	38.1

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Distance m	Prop. Queued	Effective Stop Rate per ped
P1	South Full Crossing	53	21.6	LOS C	0.1	0.1	0.52	0.52
P2	East Full Crossing	53	70.5	LOS F	0.2	0.2	0.94	0.94
P3	North Full Crossing	53	24.8	LOS C	0.1	0.1	0.56	0.56
P4	West Full Crossing	53	68.6	LOS F	0.2	0.2	0.93	0.93
All Pedestrians		211	46.4	LOS E			0.74	0.74

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

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Organisation: GTA CONSULTANTS | Processed: Wednesday, 19 October 2016 3:41:15 PM
Project: P:\V10500-10599\V105900 - Station Street Road Closure, North Carlton\Modelling\2100 Princes Nicholson\161019sidra-V105900-Nicholson Princes Alexandra.sip7

Attachment 2 - Traffic Impact Assessment Study

MOVEMENT SUMMARY

Site: 1 [Existing PM Peak - Test - 5-6 - Updated Vols]

Alexandra Parade and Nicholson Street
Signals - Fixed Time Isolated Cycle Time = 160 seconds (User-Given Phase Times)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Nicholson Street (South Approach)											
1	L2	35	0.0	0.872	70.5	LOS E	39.6	284.0	1.00	0.97	28.8
2	T1	771	3.0	0.872	64.7	LOS E	39.6	284.0	0.96	0.94	29.3
3	R2	252	0.0	0.824	54.9	LOS D	16.0	112.3	0.94	0.89	31.5
Approach		1057	2.2	0.872	62.5	LOS E	39.6	284.0	0.96	0.93	29.8
East: Alexandra Parade (East Approach)											
4	L2	321	0.0	0.297	19.2	LOS B	10.8	75.9	0.52	0.73	44.7
5	T1	1897	5.0	0.736	37.5	LOS D	40.2	293.7	0.88	0.80	37.2
6	R2	120	0.0	0.796	58.6	LOS E	6.0	41.8	1.00	0.87	30.5
Approach		2338	4.1	0.796	36.1	LOS D	40.2	293.7	0.84	0.79	37.6
North: Nicholson Street (North Approach)											
7	L2	205	0.0	0.677	61.0	LOS E	18.7	132.4	0.94	1.00	30.4
8	T1	321	3.0	0.677	60.4	LOS E	18.7	132.4	0.95	0.87	30.0
9	R2	48	0.0	0.355	50.5	LOS D	2.6	18.3	0.97	0.74	32.7
Approach		575	1.7	0.677	59.8	LOS E	18.7	132.4	0.95	0.91	30.4
West: Alexandra Parade (West Approach)											
10	L2	56	0.0	1.019	156.4	LOS F	64.7	470.4	1.00	1.28	17.1
11	T1	2564	5.0	1.019	131.0	LOS F	97.1	708.5	1.00	1.27	19.1
12	R2	40	0.0	0.248	34.6	LOS C	1.5	10.6	0.85	0.74	37.9
Approach		2660	4.8	1.019	130.1	LOS F	97.1	708.5	1.00	1.27	19.2
All Vehicles		6629	3.9	1.019	80.1	LOS F	97.1	708.5	0.93	1.01	26.0

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Distance m	Prop. Queued	Effective Stop Rate per ped
P1	South Full Crossing	53	30.7	LOS D	0.1	0.1	0.62	0.62
P2	East Full Crossing	53	74.3	LOS F	0.2	0.2	0.96	0.96
P3	North Full Crossing	53	31.3	LOS D	0.1	0.1	0.63	0.63
P4	West Full Crossing	53	57.1	LOS E	0.2	0.2	0.85	0.85
All Pedestrians		211	48.3	LOS E			0.76	0.76

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

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Organisation: GTA CONSULTANTS | Processed: Tuesday, 25 October 2016 9:57:21 AM
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MOVEMENT SUMMARY

Site: 4415 [Princes Street/Canning Street AM - 8-9 - Future - Scenario 1]

Three-way intersection with "Seagull" treatment (Signals)
Signals - Fixed Time Isolated Cycle Time = 150 seconds (User-Given Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
East: Princes Street											
5	T1	2555	0.0	0.555	6.3	LOS A	22.9	160.6	0.41	0.38	45.8
Approach		2555	0.0	0.555	6.3	LOS A	22.9	160.6	0.41	0.38	45.8
North: Canning Street											
7	L2	45	0.0	0.031	7.0	LOS A	0.7	4.9	0.23	0.53	36.0
Approach		45	0.0	0.031	7.0	LOS A	0.7	4.9	0.23	0.53	36.0
West: Princes Street											
10	L2	14	0.0	0.436	11.0	LOS B	15.4	108.0	0.35	0.33	51.1
11	T1	1993	0.0	0.436	5.4	LOS A	15.4	108.1	0.35	0.33	47.3
Approach		2006	0.0	0.436	5.5	LOS A	15.4	108.1	0.35	0.33	47.4
All Vehicles		4606	0.0	0.555	6.0	LOS A	22.9	160.6	0.38	0.36	46.2

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Distance m	Prop. Queued	Effective Stop Rate per ped
P2	East Full Crossing	53	69.3	LOS F	0.2	0.2	0.96	0.96
P3	North Full Crossing	53	5.9	LOS A	0.1	0.1	0.28	0.28
P4	West Full Crossing	53	69.3	LOS F	0.2	0.2	0.96	0.96
All Pedestrians		158	48.1	LOS E			0.73	0.73

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

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MOVEMENT SUMMARY

Site: 4415 [Princes Street/Canning Street AM - 8-9 - Future - Scenario 2]

Three-way intersection with "Seagull" treatment (Signals)
Signals - Fixed Time Isolated Cycle Time = 150 seconds (User-Given Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
East: Princes Street											
5	T1	2555	0.0	0.555	6.3	LOS A	22.9	160.6	0.41	0.38	45.8
Approach		2555	0.0	0.555	6.3	LOS A	22.9	160.6	0.41	0.38	45.8
North: Canning Street											
7	L2	139	0.0	0.095	7.2	LOS A	2.3	15.9	0.25	0.55	35.9
Approach		139	0.0	0.095	7.2	LOS A	2.3	15.9	0.25	0.55	35.9
West: Princes Street											
10	L2	14	0.0	0.436	11.0	LOS B	15.4	108.0	0.35	0.33	51.1
11	T1	1993	0.0	0.436	5.4	LOS A	15.4	108.1	0.35	0.33	47.3
Approach		2006	0.0	0.436	5.5	LOS A	15.4	108.1	0.35	0.33	47.4
All Vehicles		4700	0.0	0.555	6.0	LOS A	22.9	160.6	0.38	0.36	45.7

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
Vehicle movement LOS values are based on average delay per movement.
Intersection and Approach LOS values are based on average delay for all vehicle movements.
SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians

Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Distance m	Prop. Queued	Effective Stop Rate per ped
P2	East Full Crossing	53	69.3	LOS F	0.2	0.2	0.96	0.96
P3	North Full Crossing	53	5.9	LOS A	0.1	0.1	0.28	0.28
P4	West Full Crossing	53	69.3	LOS F	0.2	0.2	0.96	0.96
All Pedestrians		158	48.1	LOS E			0.73	0.73

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)
Pedestrian movement LOS values are based on average delay per pedestrian movement.
Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

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Attachment 2 - Traffic Impact Assessment Study

Melbourne
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Attachment 3 - Covering Letter of Petition

[redacted]

Yarra City Council
PO Box 128
Richmond VIC 3121

Attn: GOVERNANCE

21 November 2017

Dear Sir/Madam

PETITION TO THE MAYOR AND COUNCILLORS YARRA CITY COUNCIL:
PROPOSED TRIAL CLOSURE OF STATION STREET NORTH CARLTON

The attached Petition of 536 residents of the City of Yarra, residents mainly from, but not confined to, Station, Canning, Lee and Davis Streets North Carlton - draws the attention of Council to its decision to progress closure of Station Street North Carlton at its intersection with Princes Street.

Your Petitioners note that VicRoads, in a letter to Council of 19 September 2017, gave approval for the experiment to proceed **subject to the local community being supportive of the closure.**

Your Petitioners also note that an evidence-based recommendation by Council's officers to not proceed with closure of the intersection was rejected by Council.

The signatories to this Petition, set out hereunder, support the recommendation made by Council's officers.

Your Petitioners therefore request that Yarra Council immediately abandon this expensive experiment, as it represents an ineffective, inequitable and costly attempt at traffic management in Station Street, one that will cause increased hazard for cyclists and pedestrians, as well as inconvenience for a majority of residents.

Yours faithfully

[redacted]

[redacted]

Attachment 4 - Wording of Online Petition

change.org

Recipient: Yarra City Council

Letter: Greetings,

Dear Councillors of the Yarra City Council,

We, the signatories of the petition 'No Closure of Station Street Carlton North', ask that you, legal representatives of Residents of the City of Yarra and governors of local roads and bike paths used by residents of, visitors to and commuters through the City of Yarra, heed our concerns and, as recommended by council's own officers, "conclude the process of a temporary closure of Station Street at the intersection with Princes Street on the basis that evidence does not exist to support the proposal."

We, the signatories of the petition 'No Closure of Station Street Carlton North', have concerns with any closure of Station Street, temporary or permanent, given that the 'Traffic Impact Assessment Report for VicRoads' (GTC Consultants, 2016) commissioned by Yarra City Council does not recommend the closure of Station Street.

We, the signatories of the petition 'No Closure of Station Street Carlton North', object a daily increase of traffic on Canning Street in the order of up to 470 vehicles per day given that almost all vehicles presently accessing Princes Street via Station Street will then use Canning Street, which is expected to see queues increase from 14m (two vehicles) to 63m (9 vehicles), as identified in the GTC modelling.

We, the signatories of the petition 'No Closure of Station Street Carlton North', have concerns for the more than 1300 cyclists using the Canning Street bike path (as identified in the latest Bicycle Network's Super Tuesday bike count) as the assessment report by GTC Consultants identifies the aforementioned queues "will extend back past Davis Street at times and prevent vehicles wanting to turn right into Davis Street [which] has potential safety impacts for pedestrians and cyclists as queued vehicles waiting to turn right into Davis Street may not see the southbound cyclists as clearly as they would do in free flow conditions" (GTC Consultants, 2016).

We, the signatories of the petition 'No Closure of Station Street Carlton North', have concerns for the safety of young members of

Attachment 4 - Wording of Online Petition

our community walking to our local school, Carlton North Primary, who would need to contend with up to 500 extra vehicles accessing the intersection at Lee and Canning Street.

We, the signatories of the petition 'No Closure of Station Street Carlton North', object to any further spending on this proposed project, with $50,000 having already been spent "to cover the traffic impact assessment, place making assessment and a Transport Integration Act assessment of the proposed road closure."

We, the signatories of the petition 'No Closure of Station Street Carlton North', request that councillors give proper and due consideration to the lack of community support for this closure based on your own results from a community consultation that concluded "the majority of feedback (over 80% of responses) was opposed to the road closure." (YourSayYarra website, October 2017).

We, the signatories of the petition 'No Closure of Station Street Carlton North', wish to highlight to council that VicRoads in its letter to Council dated 19 September 2017, stated "VicRoads has no objection to the proposed trial closure of Station Street subject to the local community being supportive of the closure" and that further progression of this project is in defiance of this.

We, the signatories of the petition 'No Closure of Station Street Carlton North', oppose the closure of Station Street at the Princes Street exit as it represents an ineffective, inequitable and costly attempt at traffic management in Station Street, one that will cause increased hazard for cyclists and pedestrians, as well as inconvenience for a majority of residents.

We, the undersigned residents, concerned citizens, cyclists and parents of school children wish to inform the Yarra City Council of our concerns* with the proposed trial closure of Station Street, Carlton North at the Princes St intersection.

We ask that the Yarra City Council put our safety first and foremost and cease the project considering the proposed trial closure of Station Street in Carlton North which would divert up to 470 extra cars in front of cyclists and children commuting on Canning, Lee and Davis Streets.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

Station Street, Carlton North - Temporary Road Closure

Public Submissions

No.	Address	Support for trial	Trial Length	Submission
1	Station St (between Lee and Princes)	Yes	12 months	Well it is a disgrace that we remain the only street opened to rat runners, the majority of whom would not be City of Yarra residents, but they choose to use our street to access Princes Street on their way East. How did they cope during the street closure for the relining of the sewer mains? They coped very well indeed as they did not have the luxury of sailing down Station Street. Was there a spike in cycling accidents caused by vehicular traffic on Canning and Rathdowne Streets - I seriously doubt it. Residents in Canning Street are running there own scare campaign to keep Station Street open, what imperial facts are they basing same campaign on? When the inevitable serious accident happens to a child or adult pedestrian or cyclist on Station Street, if the street remains open, who will be ultimately responsible, the councillors that vote to keep the street open? I would be taking independent legal action if I was a councillor given the circumstances. Anyways, I remain gravely concerned. On many a morning I have had near misses due to speeding vehicles along our street and trust that common sense ought prevail with a view to permanently closing the street.
2	Cecil St	No	12 months	I strongly object to the closing of station st due to the increased traffic diversion down canning st
3	Cunningham St	No	No Answer	Any increase in traffic onto Canning Street wold be a disaster. Please reconsider. I have cycled it for 14 years and Canning is a delight. In June, I had to assist a young women cycling south on Canning hit by a car that was travelling east-west - the woman in the car was unfamiliar with the steady stream of bikes on Canning and while very distressed, she was driving a massive 4wd - against the cyclist who ended up with a smashed shoulder. It made me think that even in bike-friendly Yarra, we have still not quite got the message yet. Don't let the cars (or the vociferous residents of Station St) win. Here's blog I follow about the dreadful crossing just to the south http://crapcyclelanesofmelbourne.blogspot.com.au/2010/06/canning-street-and-alexandra-parade.html
4	Drummond St	Yes	6 months	Limit through traffic ('rat run') to make it quieter and safer. 6 months should be sufficient time to see if it is worthwhile to make the closer permanent.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
5	Drummond St	No	No Answer	I firmly believe, contrary to the views expressed by VicRoads, that the closure of Station Street will lead to through traffic being diverted to other N/S local roads, including Drummond Street. The "rat running" by the speeding vehicles of morning commuters is already bad enough on Drummond Street without making it worse by closure of an alternative route South to Princes Street. If Station Street is to be closed at Princes Street then Drummond Street should also be closed at Princes Street thereby forcing all Southbound commuter traffic on to Nicholdson or Rathdowne Streets.
6	Lee St (between Rathdowne and Canning)	No	6 months	Increase in traffic down other roads will have affect on kids, cyclists and the school So many Street blocks already and traffic is becoming more problematic. Leave station st as an exit point and if anything consider opening the lee st near Nicholson end so cars can go back down that way instead of past the school.
7	Canning St (south of Princes)	No	6 months	My children walk to school each day on Canning Street. I am concerned that the proposed change will push more cars onto Canning St and exacerbate existing safely issues i.e. speeding cars doing the rat run.
8	Drummond St	No	6 months	The trial closure of Station St would affect a much broader area. Cars currently using Station St will have to seek alternative routes which will experience increasing traffic. This area, which has many road diversions and closures can already be confusing for visitors to the area to negotiate. The proposed closure would further confuse visitor drivers to the area causing u-turns, reversing of cars, frustration etc, all of which would reduce safety in the area. Whilst this proposal may be attractive to residents of Station St, we are a densely-populated inner city area where all residents have a responsibility to share as many of the less desirable features of this otherwise very pleasant and liveable part of Yarra.
9	Curtain St (between Drummond and Rathdowne)	Yes	12 months	It takes time for people to adjust. I agree with the suggestion that there be a 'check in' at 6 months.
10	Station St (between Lee and Princes)	Yes	12 months	We have noticed a large amount of traffic on our street in the morning since the road was re-opened. All of this traffic seems to be going to Alexandra parade. With the increase in traffic I am concerned about the safety of my children when they walk to and from school. It has also become more difficult to park in front of our house, and getting in and out of our car park has been more difficult. Finally, the road noise has also increased since the road was re-opened. I fully support the road closure proposal that has been put forward in this submission.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
11	Canning St (between Lee and Princes)	No	No Answer	There are no safety issues on Station Street. This closure benefits a small number of households at the expense of the wider neighbourhood. Council officers do no recommend the closure. Diverting extra traffic will impact on pedestrians, families and cyclists in surrounding streets.
12	Fenwick St (between Canning and Station)	No	No Answer	I use that exit to travel to work. Travelling down Rathdowne Street adds an extra 20-25 minutes to my trip because of the congestion, traffic lights and cyclists.
13	Drummond St	No	No Answer	There is no benefit to the North Carlton community in the closure of this street, Station St. is an average street with no trees or median strip that is grassed or treed. If the council is looking at a street closure that the local community can enjoy, we should be looking at wide, treed streets with a grassed/treed median strip, where people can have picnics and games in safety. The closure will cause unnecessary increase in traffic, west of Station St. towards Lygon Street..Canning Street is already carved up in sections. This closure may benefit a few living in the street, but will disadvantage most of the North Carlton community.
14	Station St (between Lee and Princes)	No	No Answer	I am Writing in response to Councils request for submissions regarding the proposed trial closure to vehicular traffic of Station Street at Princes Street. After careful consideration I must advise council that I do not support the proposed closure. My reasons are as follows: Any benefits accruing from this closure append only to a handful of Station Street residents on the last block before Princes Street, It seems grossly unfair to inconvenience the bulk of Station Street Residents and the general motoring public, to satisfy the whims of a few. I am home based and make journeys by car at least 6 times a day. If this closure proceeds and I lose my access to Princes Street, I would have to travel a considerable distance North to enable a U turn on to Nicholson Street, whereupon I could join the traditional traffic jam, inching my way South until I eventually draw level with my own house. Hardly an appealing concept. I could, in the short term simply travel West on Lee Street and turn down Canning Street but that is not a long term solution as the following paragraphs will address. I have spoken with a residents group on Canning Street and they are less than thrilled at the prospect of significantly increased traffic flows on this last block before Princes Street. Also, Canning Street is a Major North/South Bicycle route and very well patronised ,particularly at rush hour. Now Cyclists, pedestrians and cars don't mix well. The Bicycle path on this stretch of road is oddly placed, making it difficult for motorists to maintain adequate separation from cyclists, My. Prediction is that commuters, anxious to catch the lights, will largely ignore the needs of cyclists, who are not a popular group with

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
				motorists at the best of times. The last Brick in the wall of this ill advised proposal is the peculiar timing of the Traffic Lights controlling the egress from Canning Street onto Princes Street. The lights simply show a red arrow until such time as the lights further West on Princes Street turn green, whereupon the red arrow extinguishes. This means that the motorist at the head of the queue needs the reflexes of a Formula One Race Car driver to accelerate around the corner ahead of the approaching tidal wave of traffic. I can only guess that the raison detre For this bizarre set up is to further inconvenience motorists for whatever reasons. I have a suspicion that there are elements in the City of Yarra that would like to use the coming mayhem on Canning Street as a justification for closing that off to cars as well, which of course would then turn Lee Street into a Parking Lot. I mentioned my fears to one of our Ward Councillors who gave the opinion that I might be suffering from mild Paranoia. Can Council give an Iron Clad assurance that Canning street will be left open to traffic? To put this in a larger context, Reservoir used to be more or less the Northern boundary of Greater Melbourne. A drive up the Hume Freeway will draw attention to a number of large Bill Boards advertising the imminent advent of a number of vast new suburbs, the inhabitants of which, like most of Melbourne, will of course, be totally car dependent. And the jobs are In Melbourne so a steady increase in traffic volume in The City of Yarra is inevitable. Traffic is a fluid system , it has both flow and pressure. The council can have a modicum of control over flow by means of road signs, one ways and closures, but local authorities can have not the slightest impact on the Pressure of Commuter Traffic. Council would be better advised to work with greater Melbourne on real long term solutions to traffic growth such as supporting the urgently required East /West link , Urban Fringe Parking and Public transport upgrades.
15	Station St (between Newry and Lee)	Yes	12 months	12 months would allow monitoring for the cycle of a year. At morning and afternoon peak times I have difficulty with both parking and backing out in the street. I have been caught up in cars banked back from Princes Street to almost Newry Street Cars travel too quickly down the street - with little regard for speed bumps. It is often difficult to cross road with toddlers
16	Station St (between Newry and Lee)	Yes	12 months	Less cut through traffic - greater safety.
17	Station St (between Newry and Lee)	Yes	12 months	It may reduce traffic travelling along Station St to Princess St. However, I am concerned that closing Station St at Princess St may just lead to cars travelling south on Station St turning right at the corner of Lee and Station Sts, and heading to Canning St. This will increase traffic in Lee St. Closure of Station St at Newry St would stop this.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
18	Station St (between Newry and Lee)	Yes	12 months	Too many people use Station Street as a shortcut or major thoroughfare and travel at excessive speeds. Also it would be prudent to limit traffic on Lee street at Station Street. This would deter tourists who use Station Street as part of their daily commute.
19	Lee St (between Rathdowne and Canning)	No	No Answer	This is a disgracefully poor and wasteful Council process, which is lacking any positive outcomes except for a few self-interested Station Street residents. It's bad for Lee and Canning Streets, bad for the school and children, bad for the cyclists on the bicycle 'highway' of Canning Street. It has been shown to be unpopular in the community, and Council's own traffic data shows no reason to proceed. Council should devote its limited resources elsewhere. Stop this circus now!
20	Canning St (between Lee and Princes)	No	No Answer	The closure of Station St will lead to an increase in vehicle and bicycle traffic in adjacent streets [namely Canning St]. This closure will advantage Station St residents but disadvantage Canning St residents
21	Station St (between Curtain and Newry)	Yes	No Answer	As a resident of Station Street for the last 8 years I am in favour of the proposed trial closure of Station Street at Princes Street. The traffic has increased noticeably since the Melbourne Water contractors have completed their work and the street re-opened. The first observation I make is the traffic around 8am with cars heading to the Eastern Freeway. We are always able to tell when the traffic is heavy on Rathdown or Lygon Streets or when there is a police operation on Rathdown Street because the traffic is greatly increased on this street. There has never been a police operation for speeding on Station Street, to my knowledge at least. I have been retired for some years and am home and tend to notice such things. My second observation about the increased traffic involves the early afternoon "rush" from about 3:30 pm on weekdays and sadly it mostly involves tradesmen's utility vehicles making a dash for the freeway and then home. As I have mentioned the rare occasion that a police car is observed on this street is a surprise for us all. I wonder if another rat run in this area should also be considered, that would be the traffic turning left from Nicholson Street into Newry street then left again, (mostly) to head south on Station street then right at Lee street to get to Rathdown Street or continue south to join Princes Street heading east.. These vehicles seldom observe the STOP sign at the Station/ Newry Street intersection and generally slow to about 30 kilometres per hour but never stop. A problem here is the location of the STOP sign which is about 5 or 6 metres from the corner where a driver would expect to see that sign. This situation might be more dangerous than that in Princes Street where there are 2 child care centres.
22	Amess St	No	No Answer	The closure of Station Street will put more traffic on to Canning Street which is used by me as a cyclist. More traffic will lead to more accidents.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
23	Curtain St (between Rathdowne and Canning)	Yes	12 months	Station Street was closed for a significant period during the recent sewer works and the lessening in motorised traffic was noticeable and beneficial to those trying to navigate around the area by walking and cycling. I'm surprised and disappointed that the actual experience and data available during the previous closure has not informed the review with regards the proposed trial which still references more recent traffic measurements and tries to guesstimate where that traffic will go if Station Street was to be closed again. As a daily user of Canning Street on bicycle I did not experience any safety or traffic problems during the previous closure and welcome this new trial. I do encourage Council to consider what other treatments may address any concerns over additional traffic on Lee and Canning Streets and the impact that may have on safety of people walking and cycling (not that this is necessary from my experience previously but I'm empathetic to more vulnerable people such as young students at the PS where I was on School Council for 6 years until recently). A suggerstion is the use of the red left turn arrow at Canning and Princes Streets be used to moderate mototrised trafffic on Canning Street if the closure does result in more people driving on the Southern section of Canning. Please proceed with the trial. It is only a trial after all and is totally reversible. If Council is to be true to implementing its Safe Travel Policy it will need to ameliorate motorised traffic and the filtered permeability closures such as this trial provides (and are already on Canning Street further north) will be a key factor in making this happen. More active travel and less driving is another goal of Council and will only occur if our streets are safer, slower and more inviting. this place making opportunity is another win for livability in our community. Please proceed.
24	Drummond St	No	No Answer	Drummond Street is not being considered at the same time. It is my understanding that previously the situation in Drummond St would considered at the same time as Station St. I have read the email from Jason Edwards (602 Drummond St) to COY Councillors. I found it interesting what he said about previous LATM.s. Drummond street is taking more traffic and sometimes at speed and I see that the Station street closure will only exacerbate this. The council should take opportunity to close Drummond St at Princess St and for safety to provide pedestrian / bike crossing lights on Princess St Additionally please do intersection works at Lee St so that south bound cars on Drummond St need to slow to turn left on Lee St - it can be dangerous in Lee St just around the corner when getting into ones car
25	Drummond St	No	6 months	**First Submission** This closure will negatively impact the quality of life, safety and residential property values of surrounding streets. The only people who will benefit are those residents in the southern end of Station Street. -Please clarify why the COY has completely reversed its' policy on road closures for Station and Drummond Streets after more than 15-years of refusing to close one street independently of another? -Please provide the names of all Councilors that supported this proposal, and exerted influence in seeing this actioned by Vic

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
				Roads? -Why did the COY not utilize the opportunity to monitor the impact of a Station Street closure on surrounding streets during the 18-month closure for pipe works, rather than forcing Drummond Street residents to go through the process a second time? -The Statements Council provided to VicRoads in support of this application are the complete antithesis of the arguments they used when refusing to close Drummond Street for more than 15-years. How has the COY completely reversed its' position on the impact of traffic, emergency vehicles and the arterial road network? -Please clarify what legal precedence COY has to alter infrastructure that impacts' negatively on the value of homes, the quality of life of residents, and the safety of families and children? -COY has already funded extensive speed and volume reducing measures in Station Street such as plantings and speed humps. Why is Council continuing to use funds and resources on this one street, when Drummond Street does not have a single traffic measure heading north-south? -For many years Station Street residents would only support the closure of Drummond Street if Station Street was closed simultaneously citing impact on property values and quality of life due to increased traffic flow. Council supported an all-or-nothing position on this issue therefore what has changed to allow one street to be closed at the expense of the other? This closure will make Drummond Street the ONLY residential street between Royal Parade and Nicholson Street that will be open to the flow of north-south traffic. It is also the ONLY street in North Carlton that has zero infrastructure to curb speed and volume. Legal, political, financial and ethical questions need to be asked as to how this proposal was approved by you and your staff given Councils' position on street closures in North Carlton for almost two-decades. Drummond Street residents fought for more than 10-years to close our street and the first COY LATM was developed by those residents. I was a part of that LATM committee for more than 5-years and saw firsthand the reasoning's behind closing only one of the two southbound streets. Councilors have been aware of this issue for many years and still supported a proposal that will make this situation exponentially worse **Second Submission** Just having returned from working internationally I was dismayed to see there is another push to close Station Street thereby leaving Drummond Street the only residential street in North Carlton exposed to rat-running, speeding and vehicular abuse. I have been involved in this issue and LATM development for more than 15-years and will make an online submission, however given this proposal is a complete reversal of Councils position on road closures in North Carlton I have several questions for you: -Please clarify why the COY has completely reversed its' policy on road closures for Station and Drummond Streets after more than 15-years of refusing to close one street independently of another?

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
				-Please provide the names of all Councilors that supported this proposal, and exerted influence in seeing this actioned by Vic Roads? -Why did the COY not utilize the opportunity to monitor the impact of a Station Street closure on surrounding streets during the 18-month closure for pipe works, rather than forcing Drummond Street residents to go through the process a second time? -The Statements Council provided to VicRoads in support of this application are the complete antithesis of the arguments they used when refusing to close Drummond Street for more than 15-years. How has the COY completely reversed its' position on the impact of traffic, emergency vehicles and the arterial road network? -Please clarify what legal precedence COY has to alter infrastructure that impacts' negatively on the value of homes, the quality of life of residents, and the safety of families and children? -COY has already funded extensive speed and volume reducing measures in Station Street such as plantings and speed humps. Why is Council continuing to use funds and resources on this one street, when Drummond Street does not have a single traffic measure heading north-south? -For many years Station Street residents would only support the closure of Drummond Street if Station Street was closed simultaneously citing impact on property values and quality of life due to increased traffic flow. Council supported an all-or-nothing position on this issue therefore what has changed to allow one street to be closed at the expense of the other? This closure will make Drummond Street the ONLY residential street between Royal Parade and Nicholson Street that will be open to the flow of north-south traffic. It is also the ONLY street in North Carlton that has zero infrastructure to curb speed and volume. Legal, political, financial and ethical questions need to be asked as to how this proposal was approved by you and your staff given Councils' position on street closures in North Carlton for almost two-decades. Drummond Street residents fought for more than 10-years to close our street and the first COY LATM was developed by those residents. I was a part of that LATM committee for more than 5-years and saw firsthand the reasoning's behind closing only one of the two southbound streets. I have attached photographs taken this week from my home in Drummond Street. This is a very regular occurrence as our street has become the preferred route for peak hour traffic heading to the city. Councilors have been aware of this issue for many years and still supported a proposal that will make this situation exponentially worse. I look forward to receiving your response to my queries.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
26	Drummond St	No	No Answer	I refer to the communication of 17th October 2017 as received from Yarra Council in relation to the proposed trial to close off Station St at Princess St. I am the owner/occupier of the property of **<redacted>** Drummond St., North Carlton and am completely opposed to this proposal. If considering this proposal you may also wish to consider doing the same at the end of Drummond Street and Princess St.. Our street is already used by rat runners to get onto Princess St. Closing off Station St will only mean increased traffic in our street. Our street of a morning between 7.30am and 8.30am is gridlocked and occasionally backed up from Princess St to Lee St and beyond with non-residents attempting to access Princess St.. The no left turn signs off Lygon St are ineffective. A continuous stream of traffic access our street via Pigdon St or the numerous right hand turns available to traffic off Rathdowne St.. If you are considering the closure of Station and Princess St., I would strongly recommend the same logic be applied the end of Drummond St and Princess St.. It makes no sense to close off this Street and have no proactive plan to keep traffic off surrounding residential (non arterial) streets.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
27	Herbert St	No	6 months	It has already been closed due to road works so local people have experienced the effects of its closure. It is dangerous for cyclists when cars divert from Station St via Lee St into Canning St, an important route for cyclists.
28	Hughes St	No	No Answer	Today I received a letter in the mail asking my thoughts on closure of Station St at Princes St. As a person who lives at the other end of North Carlton I do not think this is a good idea. Residents of North Carlton regularly attempt to stop motorists from the adjoining suburbs travelling through our streets. In my opinion we are all part of Melbourne and if people from Coburg, Brunswick and beyond want to come into North Carlton, why shouldn't they? Furthermore these smaller streets (Drummond, Station etc.) carry very little traffic. If Station St is closed off it will divert traffic to Canning St, where there is major bike traffic, and will have the secondary effect of disrupting a very good bike route. I feel that once again we have a small and powerful lobby group of local residents who put their own comfort ahead of that of their immediate neighbours and of fellow Melbournians who don't have the privilege to live so close to the city. I might add that about 20 years ago when some residents in Drummond St mounted a campaign to stop cars entering Drummond St from Park St. I opposed that for the same reasons. The restrictions on turning left from Lygon St into the streets between Park and Princes streets in the mornings is a constant annoyance to me. I think the existing semi-closures, speed limits etc. are perfectly adequate for managing the traffic in leafy and privileged North Carlton. I don't think Council should be wasting any more money on this issue.
29	Curtain St (between Rathdowne and Canning)	No	No Answer	It's already extremely difficult to travel south from this area. We can't turn right onto Nicholson St, and Rathdowne/Lygon are backed up during busy periods. Station St gives easy direct access to get onto the Eastern Freeway. It's not a dangerous or inconvenient intersection and I'm not sure why you feel the need to close it off. If you're going to block streets off, what solution are you offering as an alternative? You aren't going to reduce car use by doing so... the traffic still has to go somewhere. What plan do you have to redirect the flow?
30	Station St (between Curtain and Newry)	No	6 months	I live in Station St and feel the traffic problem is not significant enough to warrant the closure. The morning peak is the only period in which traffic flow is heavier. To close the road at Princes St may reduce some traffic along Station however I believe most traffic would continue to use Station St and turn right at Lee St. This would channel traffic onto Canning St or up Lee St to Rathdowne St. For me the safety of the cyclists using Canning St and the school community at Lee St would be affected by the traffic diverting onto these Streets as an alternative. As a Station Street resident, I find access to Princes St useful, and am not concerned by greater traffic using the street as it's confined to the morning peak. At those hours traffic is greater everywhere. That's just the reality.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
31	Hughes St	No	No Answer	I am in the area all the time and am concerned about the road closure forcing extra traffic through Lee Street to Canning Street which is dangerous in a school zone and for pedestrians and cyclists in the area.
32	Amess St	Yes	12 months	Station St has been a rat run for years and this appears a good opportunity to force through traffic onto the arterial roads where it belongs. In my opinion you should seek the longest possible trial closure, so this situation becomes the norm and doesn't have to be reviewed for a long time. Aim for four or five years.
33	Macpherson St (between Amess and Canning)	No	No Answer	Please note at this stage we are not supportive of using our rates to fund this proposal, which: • does not have a clear majority of directly impacted residents in its favour (appears equally split 50/50); • will likely have adverse consequences by pushing any "through/ non-local" traffic onto Canning Street, which is a bike and pedestrian friendly street so exposes these vulnerable transport groups to higher numbers of vehicular "rat racing" and possible accidents; • states that the proposed trial will be undertaken to "assess the traffic impacts of such a closure" yet your notice goes onto state that Vic Roads "is satisfied that the level of traffic diversion would have negligible impacts on the arterial road network"; and • doesn't provide a clear statement of what is the problem attempting to be solved and why it is a priority in a cost constrained environment. Equally details of costs (both incurred to date and projected throughout and post the proposed closure) and measures is not summarised in the resident notice, and the hand-off links require an undue time commitment to get through. Preferred contact = return email.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
34	Nicholson St (between Newry and Lee)	No	No Answer	I do not support the closure of Station Street because, as a local, I use Station Street to access the eastbound (i.e. outbound) lanes of Alexandra Parade on a weekly basis, because it is quick and safe. If Station Street were closed, then I would have to divert, out of my way, down Canning Street to access Alexandra Parade, but Canning Street is already a busy main thoroughfare for bicycles and for schoolchildren going to Lee Street School. Also, Canning Street has already been closed further up, specifically in order to increase the safety of schoolchildren going to Lee St School, therefore it makes no sense whatsoever to suddenly be diverting traffic *onto* Canning St near the school. Also, the lane markings on that section of Canning Street are confusing for drivers because the bicycle lane has been shifted to the wrong side of the lane, which means that any cars turning onto Canning Street have a high probability of accidentally driving down the bicycle lane (as a local, even I have done this accidentally). Therefore closing Station Street means that more cars will be driving down Canning St with the bicycles, and there will be more chance of an accident happening. The whole idea is stupid, dangerous, and a waste of money and resources. I also do not support the costs involved in the closure (up to $10,000); as a ratepayer I REFUSE to pay this! It is a massive waste of ratepayer funds!
35	Canning St (between Lee and Princes)	No	No Answer	Redirected traffic from Station St will diminish amenity in nearby streets [such as Canning] due to increased traffic flow in those streets which in turn represents an increased hazard to residents due to the higher numbers of vehicles and bicycles and associated traffic congestion. A benefit for the few residents in Station streets will represent a larger disadvantage to residents in nearby streets.
36	Nicolson St (between Church and Freeman)	No	6 months	Puts more pressure (traffic) on the surrounding streets that I ride on for work, walk through for school drop-off. We already have several one way streets, dead-ends and limit opportunities for u-turns on Nicholson Street that it makes navigating a mindfield.
37	Nicolson St	No	No Answer	Good Morning Thank you for your letter to residents regarding the proposed trial closure of Station St at Princes St, and the invitation to make written submissions. I am writing to advise that I do NOT support the proposed closure. My reasons are that I use this route to drive to work, and it would lengthen my commute to have to use another street. I live on Nicholson St and commute to work in Ringwood and Notting Hill via the eastern freeway. Station St is the fastest option; my others are Canning St or Rathdowne St, which are both quite busy. If Station St was closed I imagine everyone currently using it would simply shift to one of those, so it wouldn't ease traffic in the area. It would likely also create tension between drivers and cyclists, as cyclists mostly use Canning and Rathdowne, and wouldn't appreciate more cars. Nicholson St is very slow, and I definitely wouldn't switch to that. I would have to double back quite far and cross the tram tracks to be facing the right way. Thank you for taking the time to consider my submission. If you require further information, feel free to give me a call on **<redacted>**

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
38	Station St (between Fenwick and Curtain)	No	No Answer	As a resident of Station Street itself, and Carlton North as a whole - I have grave concerns for pedestrians, particularly school children who walk to Carlton North Primary School, as well as cyclists whose use of the designated bike path along Canning Street. Not only is the proposed closure of Station Street dangerous, it serves no-one other than a few select residents in the block of Lee Street & Princes Street. How can something with so many negative ramifications be even considered?
39	Park St	No	No Answer	I am very concerned that this closure will force more traffic onto/across Canning Street, negatively impacting on this popular and safe cycling route that I regularly use as a cyclist. I currently cross Princess St at Canning St as a pedestrian every day on my walk to work. Cars frequently ignore the red arrow and turn left from Canning St into Princes St across the pedestrian crossing placing pedestrian safety at risk. Many drivers appear to do this knowingly because the build up of traffic means they have to 'fight' their way onto Princes St. I fear that the closure of Station Street will result in more drivers ignoring that traffic signal, placing pedestrians in even more danger. A red light camera is needed on this intersection.
40	Davis St	No	No Answer	It will put more pressure on Davis Street, Canning, Lee and Rathdowne Streets in terms of traffic numbers. Davis Street is busy as it is given drivers avoid the Princes and Rathdowne intersections. As a community we all need to share the load in terms of traffic. Closing Station Street is very unreasonable and will cause other residents to take the additional traffic load in additional streets. This is extremely unfair! As I said Davis Street is busy enough with speeding drivers. Canning is closed off - that is enough in my opinion. Thank you.
41	Pigdon St	No	No Answer	I wish to indicate that I do not support the closure of Station Street. Notwithstanding VicRoads 'satisfaction', the plan causes problems for those Calton North residents trying to enter and then travel west-east along Princes St/Alexandra Parade without any counterbalancing relief. Presently, it is difficult to enter Princes St/Alexandra Parade from North Carlton due to sheer weight of traffic wishing to head south into CBD. The lanes on offer to left turners onto Lygon Street and Nicholson Street are not exclusive and are filled with those going straight. Even Rathdowne Street, which has a dedicated left lane, doesn't offer much relief as the left lane is so short. Closure of Station Street reduces options for those wishing to avoid this north-south traffic. Thank you.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
42	Station St (between Fenwick and Curtain)	Yes	12 months	To stop the large increase in through traffic...although they will simply turn right at Lee street and then into Rathdowne street so unsure what the benefit will be?
43	Princes St (between Rathdowne and Canning, north side)	No	No Answer	This will increase the traffic on neighbouring streets such as Canning, which is a major cycle path. Many families live in the neighbourhood and walk, cycle, scoot with young children who will be put in danger with the increase in traffic. If traffic from station were blocked from flowing onto the Princes st end of Canning I don't see an issue.
44	Station St (between Fenwick and Curtain)	No	No Answer	No I do not support the closure of Station street at Princess street , it will not change the traffic flow in the rest of station Street only the houses between Lee street and Princess Street. The other problem is that it will turn the peak hour traffic into Lee Street on to the bike path and down past Lee street school . I WOULD LIKE TO SEE BETTER TRAFFIC MANAGEMENT FOR THE FULL WHOLE OF STATION .
45	Canning St (between Lee and Princes)	No	6 months	As a resident of Canning St, A bike commuter, a driver and a parent of an almost 3 yo child, I spend vast amount of time on Canning St, more so then most. My grandmother owned the house prior to me, and some of my earliest memories are visiting Canning St in the early 80's. Back then you were able to turn right into Canning St from Alexander prd, crossing 3 lanes of traffic, and the centre verge was often used for parking. Seeing my grandparents hanging out in the front of the house with the other family's (mostly Italians) in the street imparted a sense of community into me, and I find myself to this day, living my life on the sidewalk or centre verge of Canning St with my friends, family and child. Because of this, I notice the changes that are happening in Canning St, the early morning traffic of people heading to work by bike or foot, the lines of kids heading into school, a lot coming from across Alexander Prd, young kids scooting or riding their bike, or walking in with friends and family members. I also see crowds of kids coming from their parents car, or walking from home, interacting around Lee and Canning St. I also see the endless stream of bikes, 1000's in the morning and the same again at night during peak hour, part of the daily bike commute that reduces the traffic burden throughout the City of Melbourne. All these commuters choose Canning St for a reason, hundreds of others choose Rathdowne St because it is a faster ride, but the majority choose Canning St, because it is safe, and for no other reason. As a commuter I am one of these bike riders, and the problems I face on an almost daily basis with the interactions with cars, makes me shake my head in frustration. Though Canning St is well known as the Bike Super Highway, it is also used by many pedestrians walking home from work, theses pedestrians are a constant flow and for each light set there is at least a few pedestrians crossing The issue being is that the pedestrian crossing creates a red turning arrow, so cars cannot turn left into Alexander Prd, this causes a backup of cars along Canning St, and creates frustration for people trying to rat race into Rathdowne St via Davis St. This is not just an issue in the afternoon but is also in the morning when the below photo was taken. One of the biggest issues is that people ignore

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
				the red arrow and turn anyway, endangering pedestrians. Another observation I make is that when cars hit a dead end trying to execute a short cut, they get extremely frustrated and drive faster and more recklessly trying to make up time, this is an observation I have made around Curtain Square with people cutting down Newry St from Rathdowne St only to find Newry St closed and Canning St closed on both ends, this is a daily observation. The closure of Station St seems to go against the whole reason of canning St. Canning St has always been the bike high way. It is a street of beauty, and is used by pedestrians and cyclists alike. The reason they do this is because it is a beautiful street, a safe street and one that cars do not overly use. To deliberately increase traffic on Canning St, for no really good reason but because another street is unhappy with the traffic conditions seems silly. The core issue is people rat racing, closing the end point of the rat race and diverting them down Canning St, a street which Council has been trying to make safer for cyclists for the last few years, consider how much bike infrastructure has been completed on that section of Canning to make it safer for cyclists, to undo it all doesn't seem justified. I understand that Station St has a traffic number problem, but why does Canning St have to shoulder the burden of Stations St's issues, why should Canning St have to take more traffic, it does not seem overly just. Canning St was created to be a wide boulevard, that when looked up, would end at the new Royal Exhibition Building and is a find example of a wide boulevards and must be one of the City of Yarra's premier streets. Upon it lawns, children play, dogs are walked, people walk barefooted, people sunbake, people stop and sit under the trees and chat and parties are held. For what reason would you want to destroy this community street? I have a proposal. How about we protect the 1000's upon 1000's of bike riders, pedestrians, kids, parents, dog walkers, dogs, teachers, families, sight seers, revellers, sunbakers, hipsters, couples and the multitude of other people who use Canning St and make it the safe and beautiful Street it is and close Canning St at Alexander Prd! Only then would everyone be safe from the cars that are slowly eating our beautiful street up. My Proposal, (only changes are listed) No exit from Canning St onto Alexander Prd (Canning St residents are required to exit via Lee St) No left turn into canning St From Lee St (Lee St traffic can only proceed straight ahead, as per current one way street) No exit from Davis St on to Rathdowne St (Traffic can only enter Davis St from Rathdowne St) Cars will be able to enter Canning St from either Rathdowne St or Alexander Prd.
46	Station St (between Fenwick and Curtain)	No	No Answer	It's inefficient - closing Station Street will not change traffic volume and behaviour in that part of the street stretching from Park Street to Lee Street. Council has not considered other options for traffic management in the street. It's disgracefully unfair: while a very small proportion of Station Street residents will benefit from the closure, the majority will be inconvenienced, cyclists in Canning Street will be exposed to increased hazard, as will children attending Lee Street Primary School.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
47	Princes St (between Canning and Station, north side)	No	No Answer	We being **<redacted>**, **<redacted>** and **<redacted>**, owners of **<redacted>** Princes Street ,North Carlton opppose the proposed trial closure of Station Street North Carlton . We feel the closure of the street serves no purpose other than making local traffic more conjested as there will be an increase of people driving through other local streets in which case they wouldn't even use if Station Street remains open . Residents of other local streets will see a surge in traffic numbers and they will not appreciate this especially local families which would put unnecessary strain on local streets and make them unsafe . The main idea of having local streets is so people can access their house. Having driven through Station Street , the only cars that are really driving through or out of Station Street via the Princes Street access is local residents accessing their properties. As we own a property that is on **<redacted>** Princes Street which is close to Station Street , it would affect access into our property and others who have garage access behind Princes Street as we then cannot use Station Street to turn into via Princes Street. It affects ease of access. Also I would probably see a rise in cars using the lane way access behind Princes Street to get into Station Street . The street closure does not make sense especially considering there are closed streets and one way streets already in the local vincinity. So I ask the question of how are people supposed to access their properties if the streets keep closing ? We appreciate an update on this matter .
48	Station St (between Fenwick and Curtain)	No	No Answer	Closure does not solve the traffic problems in Station Street. It's unfair. It will please a very small minority while making it inconvenient to a majority of Station Street residents. The situation should have been abandoned at the last meeting to save time and money. Also consideration does not seem to have been given to the impact of traffic on other streets in the area. Studies have been undertaken by independent traffic consultants hired by Council stating it is not viable. So why Yarra councillors want to pursue this project is incomprehensible. It's a waste of ratepayers' funds.
49	Station St (between MacPherson and Fenwick)	No	No Answer	I do not consider that there is an increased number of vehicles in Station Street since the reopening of the entry to Princes Street. I also think that closing Station Street will increase the congestion in Canning Street which is already servicing an increasing number of bikes and pedestrians. The Canning Street exit to Princes Street only allows 1 or 2 vehicles to enter Princes Street due to the left turn red arrow.
50	Canning St (between Lee and Princes)	No	No Answer	As a parent of young children and as a regular bike rider, I do not want the cars that usually exit via Station to Princes Street to be diverted down Lee Street and onto Canning Street in front of Carlton North Primary School and the state's third busiest bike arterial as it presents increased risk of traffic accidents involving children and cyclists.
51	Station St (between MacPherson and Fenwick)	Yes	12 months	The traffic in peak hour past my property is very noisy and unsafe for a residential area. It becomes a thoroughfare not a residential street during peak hour. Other streets in Carlton North have been blocked to stop through traffic and it is certainly time to do the same in Station Street. I wonder whether the barricade should be before Lee Street to stop traffic using it as a bypass to Rathdowne Street and preventing traffic from using Canning Street as another entry point to Princes Street.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
52	Lee St (between Rathdowne and Canning)	No	No Answer	Why is council still pursuing this mad idea when it is clearly so unpopular and dangerous to cyclists?? I hope Council is recognising the risk that a cyclist will sue them when they are hit due to this traffic diversion on what has become a bicycle highway.
53	Station St (between MacPherson and Fenwick)	Yes	12 months	Am increase im traffic
54	Station St (between Richardson and MacPherson)	No	6 months	Children and cyclists will get killed by your short sighted idea
55	Fenwick St (between Rathdowne and Canning)	Yes	6 months	You will need to improve / fix the layout of the Canning Street / Princes Street intersection to cope with more car traffic. Most cars turning onto Princes Street ignore the traffic lights due to the extremely poor design of this intersection.
56	Princes St (between Canning and Station, north side)	No	No Answer	I refer to your letter of 17 October inviting submissions on the proposed trial closure of Station Street, Carlton North at the intersection with Princes St, and submit as follows. My partner **<redacted>** and I own and occupy the premises as **<redacted>** Princes St Carlton North, **<redacted>**, and immediately adjacent to the proposed road closure. Vehicle access to our property is **<redacted>**. We strongly oppose the proposed closure of Station Street, whether temporary or permanent. This would have additional and ongoing detrimental impacts on our business, which we are just in the process of rebuilding following the completion of Melbourne Water works in June this year. Since 2003 we have run our small business from the premises (as permitted). Our business has been decimated by two-and-a half years of Melbourne Water works (since January 2015) including deafening noise and vibration, associated property damage, road closures, and endless disruptions immediately outside our workplace. (We are in the process of pursuing a compensation claim against Melbourne Water in relation to the losses we have incurred as a direct result of their activities.) Our objections include the following: There is no apparent evidence to suggest any problem exists in Station Street with traffic movement and parking that warrants the proposed road closure. It is apparent a very small handful of residents feel a road closure would be of benefit to them, but the majority of stakeholders who have responded to consultation about placemaking at the subject intersection are strongly opposed to the closure. Thus the proposed road closure would appear to be a solution to a non-existent problem.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
				I note Council has prepared a traffic study to assess the traffic impacts of closing the road. I have not had the benefit of seeing this study, but would argue that the proposed road closure would almost certainly create more traffic movement through Station Street and adjoin residential streets than is currently the case. Vehicle access to our premises is via Station St, so in order to access our off-street parking, we will be forced to access via the residential streets to the north. More importantly, visitors to our clinic who arrive by car all park in Station Street, where there is always ample short-term parking available during normal business hours. There is very limited parking available in Princes Street, and the constant flow of heavy, fast-moving traffic makes reverse parking impossible. (See below re safety). Therefore Station St represents the only viable on-street parking opportunity for visitors. Visiting vehicles typically arrive from the west on Princes Street, turn left into Station Street, parking within the first 20 metres or so of the intersection, and then depart with a left-hand turn into Princes Street. Our visitors therefore have no impact on Station St or other residential streets beyond the immediate vicinity of the proposed road closure. In the event of road closure, in order to access to parking in Station St, visitors will be forced to arrive from the north, weaving through the labyrinth of local residential streets that lead to Station Street. The same applies to visitors to the Carlton North Neighbourhood House, the clinic at 32 Princes Street, and other local visitors. Thus the proposed road closure will inevitably force these road-users away from the major arterial in Princes St into residential street, especially Canning, Lee and Newry Streets. This increased traffic movement will be exacerbated by the many other road closures, one-way streets, no-turn restrictions and other traffic calming measures in the immediate vicinity. Visitors unfamiliar with the indirect access routes to Station St, and will inevitably drive up and down many other streets unnecessarily while finding their way. By creating an incentive to reverse-park in Princes Street in order to avoid these movements, this also creates a safety hazard. Princes Street traffic is constant, especially during peak periods there is rarely a break in traffic. Reverse-parking is a serious hazard, avoided by all who know the area. The prior closure of Station Street for the purposes of accommodating Melb Water works anecdotally bore out all these difficulties and unintended consequences. All this seems unnecessary in response to a problem that does not currently exist. I further refer to and re-iterate the objections raised previously (dated 20 July 2017) in relation to placemaking proposals. I would be happy to expand on these concerns and others further as needed.
57	Rae St	Yes	12 months	I attend the CNLC and also sometimes walk or ride my bike down Station St. It is a very pleasant street which is well used by local residents - walking, cycling, taking their dogs out, taking their kids to daycare/school or on trips, etc. There are alternate routes for motor traffic and this closure will adversely inconvenience a few locals who care most about driving but benefit many more locals who care about the safety and amenity of the street and area.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
58	Rae St	Yes	6 months	I support the trial, with appropriate modifications to mitigate concerns about pedestrian and cyclist safety, so that council, residents, cyclists and school parents have better quantitative data on the impact of closing Station St. If the trial goes ahead, the Give Way signs at the Lee St/Canning St intersection should be converted to stop signs to improve safety to pedestrians and cyclists using Canning St, especially during school hours.
59	Station St (between Richardson and MacPherson)	No	No Answer	This part of the road was closed due to road works for the best part of a year last year. I did not see that it made any difference to the traffic in Station st. I suggest that the road remains open. Traffic will just divert down canning st. There is no benefit to this closure
60	Rathdowne St (between Macpherson and Fenwick)	No	No Answer	I strongly opposed the Road closure trail of Station Street. Traffic congestion within the Streets of Carlton North is already at capacity and with forecast population growth expected in Melbourne over the medium term, any closure of existing roads is frankly, crazy policy. Inner city locations must share the load on traffic flows, therefore to close any Road only places undue pressure on surrounding Roads. I suggest you take the time to look at the traffic flows on Rathdowne Street between 7.00am to 8.30am Monday to Friday, its a disaster. The same is true of evening peak times. Furthermore little regard is paid by Council and or Vic Police to the 40Kms per hour speed limit on Rathdowne Street - another area where Council should invest some of their time as opposed to ideas about Road closures. Yes, like residents of all Streets in Carlton North, residents of Station Street would be delighted to have the Street closed to thru traffic, but the point is traffic has to go somewhere and existing road infrastructure must share the load. I refer the Council to the article in The Age newspaper Oct 24 2017,headed "Cars continue to rule Melbourne". As much as Council's plan for bike routes etc car transport is here to stay. Again I state my very strong opposition to this Council proposal.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
61	Canning St (between Lee and Princes)	No	No Answer	The closure will push all of the Station street traffic onto Canning street at the corner of Lee St & Canning. There are thousands of bike commuters and hundreds of children walking to school using the same intersection and this closure will put hundreds of cars into the same bottle neck. My two children walk to school through this Lee St & Canning Street intersection and at least once a week we see an incident where a car nearly takes out a cyclist. Putting more cars through this are will only make the risk to my children and cyclist exponentially greater. Why would you push more commuting motorist closer to a school? Pushing more motoring traffic to interrupt one of the most used cyclist commuter routes of Canning street to me seems especially dangerous. Furthermore this additional traffic pushed onto Canning street will want to turn right onto Davis street to get to Rathdowne street crossing the bike lane of Canning street, I truly fear for the safety of all pedestrians and cyclist as a result. Don't do this, this closure benefits an handful of Station street residents whose sole motivation is to create a cul de sac to improve their own property value to the detriment to thousands who rely on the safety of Canning street as it is set up at the moment. Its a selfish motivation which has catastrophic effect on the suburb of Carlton North.
62	Rathdowne St (between Macpherson and Fenwick)	No	6 months	Having no vehicle access to Princes Street via Station Street, a route I regularly take to get access to the Eastern freeway with my car, will be a major inconvenience for me. Moreover, such a closure, albeit temporary, will act as a precedent resulting in further restrictions to vehicles in other streets in Carlton North.
63	Rathdowne St (between Fenwick and Curtain)	No	No Answer	I believe the closure of Station St in the area proposed will cause additional vehicle traffic to be diverted along Lee St to Canning St to reach Princes Street. In my view this will create an additional hazard for the children/pedestrians in that immediate area. I further understand that Council's own Traffic department has not supported the proposal, which should be enough reason to not proceed.
64	Canning St (between Lee and Princes)	Yes	12 months	Reduction of traffic through Station Street While recent works were being carried out traffic reduction was quite noticeable
65	Lee St (between Rathdowne and Canning)	No	6 months	A closure at Station Street would force drivers up Lee Street which is a narrow one way street that runs along Lee St Primary school. In the mornings I already see how many cars travel up the road and am concerned this will add to risks with the children crossing the road.
66	Canning St (south of Princes)	No	6 months	We have been through this at the start of the year. If you close Station Street it will increase traffic on Canning Street between Princes Street and Lee Street, particularly those drivers who also like to hoon down Davis Street as a short cut, making it even more dangerous for the vast number of children walking to Carlton North Primary School. The road closure is patently not needed, even as a temporary measure.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
67	Canning St (between Lee and Princes)	No	6 months	Closing Station st will increase the traffic past my home in **<redacted>** Canning St. It is unfair that the residents of Station st involved in the road closure have no traffic past their home and my home has increased traffic. The residents of Station st have no right to a quieter street when it results in other homes having more traffic noise. Closing Station st will also result in more traffic interacting with the cyclists in Canning St which will increase the risk of accidents and injury. My family strongly objects to the closure of Station St .
68	Canning St (between Newry and Lee)	No	No Answer	I strongly oppose the closure of Station Street. Not only will be closure increase traffic in several intersections heavily used by School children (including the street directly in front of Carlton North Primary School). But, traffic will be re-directed onto one of the busiest cycle lanes in Inner Melbourne. Adding more cars into an area which has been designated a safe and user friendly bike lane (and also safe school zone) seems rather crazy, and counter intuitive, does it not?
69	Rathdowne St (between Macpherson and Fenwick)	No	No Answer	The closure of Station Street for a prolonged period during major works shut off Station Street shut off Station Street as a means of accessing Princess Street, Alexandra Parade and the Eastern Freeway from the Rathdowne Street/Canning Street area in Carlton North. We live in Rathdowne Street and have a rental property in Canning Street. The closure had an impact by increasing south bound traffic in Rathdowne Street, particularly during the morning peak with all traffic being forced onto Rathdowne Street exacerbating an already bad situation. Being able to access Princess Street, Alexandra Parade and the Eastern Freeway from Station Street divert some of the Rathdowne Street traffic to use the Station Street access without creating significant problems with excessive traffic or disturbance in Station Street. Nicholson Street is not a ready alternative with access from the west of the street as there are only limited access points. Retaining access to Princess Street from Station Street has a considerable benefit to local residents without a significant disruption or impact on residents in the vicinity of the intersection, particularly as there is no actual crossing or access for a right hand turn.
70	Davis St	No	No Answer	diverted traffic danger to cyclists and school children & pedestrians in Caning and Davis Streets Closure is NOT supported by the local community There is more than enough space for community events on the grounds of the Neighbourhood house, the vacant lot adjacent extending to Nicholson St., the large grounds of North Carlton Primary School (Lee St.) 2 blocks away are available to the community out of school hours and Curtain Sq. Park is nearby. Council has wasted enough money exploring this project

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
71	Nicholson St (between Church and Freeman)	No	No Answer	Additional traffic will be pushed onto canning street to access Alexandra parade crossing a very busy bike path and school crossing area. My child attends nth Carlton primary and I have witnessed 3 incidents of cars hitting cyclists at the corner of canning street and Lee street in the past few years.
72	Amess St	No	No Answer	My wife & I do not want trial closure because it may lead to permanent closure. Closure will push more cars on to close by through streets which are getting fewer & fewer. Motorist speed down these through streets trying to make up time. They are dangerous to pedestrians of all ages. Living in Amess Street (a through street)we are witness to such speeding drivers. Each year there are more cars, therefore we need more roads not less. We DO NOT WANT closure,
73	Rathdowne St (Between Mary and Pigdon)	No	No Answer	I am emailing to express serious concerns about the proposed closure of Station Street near Princess Street -- I am firmly against the closure. As a resident of North Carlton living on Rathdowne St. and a professor at the University of Melbourne who studies inequality, my concerns are as follows: First, the initial closure of Station Street that motivated this entire debacle was meant to be temporary in order to complete road works. This undoubtedly increased traffic through other streets including my own, Rathdowne St. This increase is unfair to the extent that Station Street residents have been enjoying a quieter and less polluted environment at my and others' expense by increasing noise and pollution in our immediate environments -- more traffic for us as driver reroute their paths away from Station Street. Such an injustice is acceptable to the extent that road works are necessary, and temporary closures are ended once road words cease. Everyone in a city can be expected to endure some temporary hardships such as this because such ahrdships can be expected, in the long run, to be distributed equitably across locations and residents--at some point Rathdowne will need to be closed for roadworks and, during this time when I enjoy less traffic, residents who live elsewhere will have to endure increased traffic. Therefore, to make permanent the temporary closure is a slap in the face to all of us who have to deal with increased traffic -- it is quite unjust. Station street residents have already been provided with benefits at my and others' expense, and now that the road works are complete they should be expected to return to their previous state of having an open street in order to relieve the burden that has been laid on my and others' shoulders. To now suggest and entertain a permanent closure is simply unfair. Second, permanently closing Station street based on the precedent of a temporary closure is more than simply unjust, it is unreasonable because of how it promotes linking temporary closures to permanent closures. Consider what would happen if every temporary closure that was appreciated by very immediate local residents was turned into a permanent closure, or even just suggested as something to consider for a council and local population. Very quickly an entire city would have its traffic throughputs decimated by making permanent temporary closures, and tremendously valuable and scare resources would be squandered merely considering such closures. As a general rule, the status of temporary closures as temporary must be respected. To do otherwise is to misunderstand the crucial role that trust plays in tolerating the negative effects of such closures. If temporary closures may be made permanent or even just considered as being potentially permanent, then no one

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
				who is negatively affected by temporary closures is going to tolerate them, making it more difficult to simply plan and execute road works without civil protest. This is all to say that the City of Yarra and its residents may be understood as having a social contract wherein things like temporary road closures and their negative effects are tolerated only because they are understood as temporary. To allow this understanding to change, as Yarra City is doing in this case, is totally unacceptable, because it erodes trust and sets a very dangerous precedent for the permanency of what is and should be understood as temporary road closures. Third, there is a general point about injustice and inequality in traffic and its negative effects. The council is now describing the Station Street closure as "place making", but at what cost? By removing traffic from Station Street, traffic along my street and others will increase. This is to say that without increasing public transport resources, traffic is a zero sum game and less for some residents means more for others. Because the Station Street residents want less traffic, my children and other families will be exposed to more pollution, more traffic noise, and more danger in being hit by cars. Such negative effects of traffic should be fairly distributed across members of a society, and in this case the Station Street residents are attempting to make this distribution less equal, which is to say more unfair. All residents of any local area would want less traffic. Therefore, to favor this desire for the residents of any given location at the expense of other residents elsewhere is to create a grave injustice that should not be tolerated. Why must I and others who will be affected by the selfish desires of Station Street residents suffer because they desire greater inequality that will be to their benefit? This erodes trust in society and the Yarra City Council, and I strongly disagree with granting such selfish wishes at the expense of others and their families. The Yarra City Council and its residents, especially those in favor of closing Station Street at Princess Street, should get serious about injustice and inequality, as well as the trust and implicit social contract that we should maintain with one another. Without these things, society breaks down, as those with the ability to influence public policy and public institutions are allowed to hoard valued resources for themselves--whatever those resources may be, including less traffic in their area. The Yarra City Council and the residents of Station Street seem to misunderstand this very serious point. The world needs more equality, not less, and certainly not in the name of making permanent something that was mutually understood as being temporary. The Station Street residents (and others nearby who support the closure of Station Street) are being extremely selfish here to the detriment of others, including me, and this should not be condoned, supported, or otherwise countenanced by the Council.
74	Amess St	No	6 months	The closure would increase car traffic around Carlton North Primary School and cause unnecessary dangers to our young children.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
75	River St Richmond	No	No Answer	Please accept this email as a strong indication of my opposition towards the proposed closure of Station Street, which would most certainly have a detrimental impact on the current safety and bike-friendliness of Canning Street as a cycling route from central Melbourne through to the safety of Bike Paths in Carlton North. As a keen and regular commuting cyclist, I enjoy the massive benefits of commuting from the outer northern suburbs (Eltham North), including personal health and fitness, environmental and cost benefits. But as a father of 3 young children, each day that I cycle to work, my priority is still to ensure my children are still able to see their father return home safely each evening. To ensure this is possible, I do my best by cycling infrequently on roads and ride extended kilometres by sticking to designated paths and bike lanes as much as possible. My 30-km commute is at its most precarious as I exit the Melbourne CBD heading towards Carlton North for 4kms to join the bike path at the end of Canning Street on Park Road, Carlton North, where there is always a great relief to have made it to the relative safety of a car-free cycling route. Only occasionally have I continued my ride along Rathdowne Street, which provides a bike lane but also multiple lanes of cars, and unfortunately I was hit by one of these cars in April last year, requiring a knee operation and almost 12 months of rehabilitation before I could again start to cycle. Since this incident, I have ALWAYS used the option of Canning Street. If you take an evening or mid-morning cycle through the area, then it becomes abundantly clear why Canning Street and the traffic minimizing infrastructure that has been put in place, is crucial to the provision of a safe cycling option for commuters such as myself. If the closure of Station Street will create a greater number of cars across and along Canning Street, this can only increase the risk of cyclist-car incidents, which the cyclist will inevitably come off second-best. I can attest to this from personal experience, and I am one of the lucky ones, because although still a little impeded, my children still have their father. I would like you to seriously think about the impact of any decision to close streets in the area, that could put this family dynamic at risk, as it applies to me and many others like me. Please consider this as part of any road closure decision.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
76	Scotchmer St	No	No Answer	As a regular cyclist using Canning Street, I saw first hand the impact of the temporary closure of Station Street during the Melbourne Water works. Since those works have concluded (ie Station Street re-opened), car congestion in Canning Street south of Lee Street is much lighter. My objection is based on three points: 1. The expert report from GTA notes that there is no benefit to the closure and does not recommend it. 2. The resident's submission to Council is flawed: a) Empirical observation of the bike path south of Lee street shows that, due to the poor design of the kerb, cars almost always cross into the bicycle lane. b) Proposed traffic flows made by residents do not correlate with those from experts hired by Council. During the Melbourne Water disruption, it was not unusual for queuing of cars north of Davis Street, making the David Street / Canning Street crossover a congestion point for cars and dangerous for cyclists. 3. Lack of clear consensus from residents, and risk of Council appearing to favour one group of residents over another. Figure 2 in the Council document shows a 50-50 split, at 26 votes each, for the proposed closure. a) Westminster political guidelines always state that in a tied vote, the decision is affirmed in the negative (ie maintaining the status quo) b) Should Council approve this, it would appear that Council is favouring one group of residents (Station Street residents) over others. (The reverse argument, that rejecting it would be favouring another group of residents is invalid, because rejecting it would maintain the status quo.) It is worth noting that not all residents in Station Street between Lee and Princes Street support the closure... Finally, it is disappointing that Council refused to accept an online petition, signed by several hundred people, via change.org, against the proposed closure. I suspect that this petition shows that the wider group of stakeholders would find there was a net negative benefit from the closure, and it is unclear why those voices were not able to be heard.
77	St Georges Rd	No	No Answer	A boy from our school (Carlton North Primary) was nearly killed by a taxi going through the bike path on Canning street (coming up Newry). Any increase in traffic coming via this route is likely to be very dangerous for kids walking or riding to and from school
78	Station St (between Fenwick and Curtain)	Yes	12 months	Stop the morning 'rat run'

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
79	Station St (between Fenwick and Curtain)	No	No Answer	**First Submission** By closing this road all the through traffic will be re directed to canning st exit where many bikes travel. Why don't you spend money/time on policing why there are so many cars traveling down Station st. Ie stopping non residents using this street as a thoroughfare **Second Submission** I DO NOT support the proposed trial closure of Station Street, North Carlton for the following reasons: • this will only reduce traffic for that section of Station St. What about other sections in the street that will still get the traffic before this section? • this will only increase the number of cars exiting Canning St which will be more dangerous due to the bike traffic. • this will not solve the problem of cars using these streets, as quicker thoroughfares as they will just use the next alternative - Canning St • the whole traffic issue could be managed by policing the actual signs that stop cars from entering our neighborhood to cut traffic congestion. Why can't you make all of the signs (NO LEFT TURN BETWEEN 7-9AM excluding residents) • the pedestrian/bike lights at Canning Street do not work as efficiently, whilst the lights are on red for the bikes, most times there is a red arrow for cars trying to enter Alexandra Pde due to pedestrian traffic. At best only 1 or 2 cars can turn left I think the Traffic and Engineering department needs to think of other ways to address traffic issues in our Neighborhood, you can't just close off all the streets.
80	Station St (between Fenwick and Curtain)	No	6 months	I live on Station and turn off Station onto Princes St at 8am Monday - Friday. Traffic is never an issue and sometimes I am the only car banked up and waiting to turn. I think this is an unnecessary change and that if it goes through, people will just turn at Canning St instead which is a greater risk to cyclists and pedestrians than cars using Station St.
81	Station St (between Pigdon and Richardson)	Yes	12 months	Maybe assist in stopping rat run drivers through the suburb
82	Nicholson St (between Macpherson and Fenwick)	No	No Answer	There is no need for closure of the street. Closure would just send traffic through other areas/ streets and would create a snowball effect on other streets. I live in a very busy street and it is unimaginable that there would be a trial street closure in Nicholson Street. The only people it would benefit is a handful of residents at the southern end of the street. There are already traffic humps to limit the speed of traffic and having lived in the area for 40 years, I have never seen the street overwhelmed by through traffic.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
83	Sutton St North Carlton	No	6 months	Living within the enclosure of North Carlton already, access to the East or South East is very difficult, especially at peak times. Closing Station St heading South would only put further traffic into Lee and Canning Streets impacting those residents and users. If Vic Roads believes the traffic diversion would have negligible impacts then why is it being considered in the first place. I would agree that entering Station Street from Princess Street could be a consideration for closure but not the other way. Please consider the whole of North Carlton, not just a very small percentage.
84	John St (suburb unknown)	No	No Answer	Hello Re: Station Street proposed trial closure at Princes St I wish to advise that I do not support the above closure for the following reasons: - It will force more drivers to use Nicholson St - Nicholson St is already a very busy road - Keeping Station St opens allows traffic to move more freely - Keeping all nearby streets open allows traffic to move more freely
85	Canning St (between Newry and Lee)	Yes	12 months	

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
86	Wilson St Princes Hill	No	No Answer	I DO NOT support the proposed trial for the following reasons: 1) There is no significant issue with the traffic flow near the corner of Station and Princes Street and therefore this proposal would be a waste of council resources (time and money) in a period where the council is facing significant budget challenges. As per the traffic study undertaken by GTA Consultants, dated 25.11.2016, the morning peak hour traffic volume on Station Street near Princes Street is very low (93 vehicles) compared to other surrounding north/south roads (for example Drummond Street 368 vehicles, Rathdowne Street 574). In fact this area has the LIGHTEST DAILY FLOWS OF ALL MEASURED STREETS in the area (refer Table 4.2 page 19)! 2) The proposed closure would adversely impact the surrounding north/south roads by increasing their traffic flows. These surrounding roads all carry higher traffic volumes than Station Street near the corner of Princes Street. 3) The closure would likely result in more cars using Canning Street which has the third highest on-road cyclist volumes travelling into the CBD (after St Kilda Road and Royal Parade) (GTA report page 18) and therefore would increase safety risks to cyclists. 3) The proposed closure would inconvenience local residents who are already subject to a number of traffic controls in the area. This proposal should not ahead. The resources should be used for more important council priorities.
87	Address unknown	No	No Answer	I drop my son at Lady Gowrie of a morning and would find it really hard to get to the school in a timely matter, i have very strict work hours and already find it really hard getting near the school with all the road cut offs etc it would just add another difficulty driving in Carlton North. Also i imagine the cut off would make Canning St, Nicholson and Rathdowne even busier, risking the bike riders even more so
88	Address unknown	No	No Answer	It will make surrounding streets busier with traffic and increase danger to bike riders and pedestrians.
89	Canning St (between Newry and Lee)	No	No Answer	Too much traffic congestion where Canning meets Alexander Parade, also where Lee St meets Canning. - making it very dangerous not just for cars but also the considerable bike & foot traffic in that immediate vicinity. Secondly, it is bad enough with angry drivers speeding outside my house because they find themselves locked into the local streets and cannot get out. The Station Street closure will enhance this because as more cars will then go onto Rathdowne, out of frustration (as happens now) they try to turn left not knowing that the streets are blocked off; they already do this - this will just magnify. Remember there is a primary school opposite my house.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
90	Canning St (between Fenwick and Curtain)	No	No Answer	During peak hours in the morning, most residents like myself who live on canning st need to access princes street for our commute to work. We have to avoid rathdown st as there is very heavy traffic (waiting 15mins to travel only 200 metres!!), so most bypass this by going down station st in order to access princes st. If station st was blocked, this would then detour everyone to turn right at lee st, then left on canning st and then access princes st, however there is a busy bike lane on canning st so drivers would then have to spend alot of time waiting to give way to the cyclists which would then clog up lee st with traffic. Sure enough the residents on lee and canning st would then complain too. There are already some speed bumps on station st which encourages cars to go slow. If speeding was really the core issue for the station st residents, then more speed bumps should be installed. I personally think the residents on station st are over reacting and want to keep their street quiet because who doesn't want the luxury of a nice quiet street! Station st is already such a quiet street compared to the busy canning st which has all the traffic and an insane amount on cyclists. It is obvious that Installing more speed bumps on station st is the most cost effective, easy and no-brainer thing to do instead of detouring traffic to lee & canning st.
91	Address unknown	No	No Answer	I don't think there should be any trial. It's obvious that if you block off access to Station St then people will find alternate routes to get to their destination, including more traffic entering, crossing or travelling along Canning St. This is a key, safe, untroubled cycling thoroughfare, carrying thousands of cyclists in safety daily. Doing anything that disrupts that would be an incredibly damaging and a backward step. Lets think environmental and global but act local. I'm not on any 'activist' groups just a very concerned citizen who works in the area and cycles to work via Canning St and other local streets every day.
92	North Carlton (address unknown)	Yes	12 months	12 months is needed to include data from all seasons and all months.
93	North Carlton (address unknown)	No	No Answer	It is inconvenient.
94	North Carlton (address unknown)	No	6 months	Your own report says there is nothing to gain by closing the road, even detrimental to Canning Street volumes. Why spend any more money on this?
95	Canning St (between Fenwick and Curtain)	No	6 months	It would force cars wanting to turn left onto Princes Street to travel up to Canning Street, which would impact both cyclists and motorists. Cyclists do tend to take over the road whilst waiting for the lights to change at the intersection of Canning and Princes Streets. Also when pedestrians cross at these lights, there is little time to allow for cars to turn. If there is a queue along Canning Street then cars will travel up past the school along Lee Street to Rathdowne Street. Traffic is already unbearable along Rathdowne Street during the AM peak period.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
96	North Carlton (address unknown)	No	6 months	The closure limits access to Princess street which is the link to the freeway
97	Address unknown	No	No Answer	This will create more traffic on Canning st and put cyclists in danger.
98	Address unknown	No	No Answer	As an avid bike commuter from Preston into the CBD (via Canning St Carlton North) each day for work, I am strongly opposed to the planned closure of Station St. This will divert traffic onto Canning St and make cycling more dangerous. Canning St is already very busy with cyclists. I often see up to three or four riding abreast, which means riding or overtaking in the car lane. We do not need more cars in this road, it would only discourage cycling. Please consider an alternative solution for the Station St issue and leave us cyclists to ride safely.
99	Address unknown	No	6 months	The closure of station street will divert traffic onto canning street, which includes large numbers of cyclists (is a strategic cycling corridor) and has a school in the vicinity. This will likely increase the risk to cyclists and pedestrians in this area of injury through increased exposure to vehicular traffic.
100	Canning St (between MacPherson and Fenwick)	Yes	12 months	Traffic is rat running through Station Street. We want a great place to live and through traffic should be restricted. There are other ways for locals to get in and out of the area. The Melbourne Water closure proved that it can be closed and have no impact on access but will improve local amenity.
101	Address unknown	No	6 months	The closure will increase traffic around a school and designated bike zone thus compromising safety for both primary age children and cyclists.
102	Canning St (between MacPherson and Fenwick)	No	6 months	I do not agree with diverting extra traffic near a Primary School with young children walking & cycling to school and towards a Major bike artery going into the city. IF you want to create a green space close Canning Street and divert traffic away from potential accidents to children and cyclists.
103	Address unknown	No	No Answer	My child and I use the Canning Street bike path to access Carlton North Primary School. Closing Station St will force more cars onto Canning Street which should retain its bike/school children priority.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
104	Canning St (between Richardson and MacPherson)	No	6 months	To keep increased traffic flow away from the school and cycle riders, also absolutely unnecessary and yet another complete waste of rate-payers money.
105	Princes Hill (address unknown)	Yes	12 months	This area is commonly used for those that commute via bike and if car traffic can be kept to the main roads it will keep it safer for bikes and pedestrians.
106	Station St (between Pigdon and Richardson)	Yes	6 months	I believe it will directly benefit the residents of Station Street at the Princes street end, by reducing the flow of traffic out of Station Street into Princes Street in the mornings in particular. Since the temporary closure at Princes Street during sewerage works we have continued to avoid driving down that end of Station Street and this has not caused us any issues.
107	Canning St (between Pigdon and Richardson)	No	No Answer	I am writing to voice my opposition to the closure of station st at the princess st intersection. I live in Canning st north Carlton and often use station st to access princess st as an alternative to both rathdowne and Nicholson streets that can be busy and banked up at peak hour and other times of the day. I feel that the closure would increase my travel time and access to the eastern freeway.
108	Address unknown	No	6 months	Extending this farce any longer than necessary is a traversity

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
109	Amess St	No	No Answer	The proposal to remove access from Station Street to Princes st. serves only to benefit a very small number of residents and will cause severe impact on many more. Not only will it force residents seeking to travel east significant extra time by being forced to use an already over crowded Rathdowne St to gain access to Eastern travel it will place an enormous burden on Canning St and create a traffic bottleneck for residents of Canning South of Lee St. During the board of works sewer works on Station and the more recent gas works, my daily commute to the Eastern suburbs was increased by 20 Minutes by being forced to use Rathdowne. With Rathdowne now only one lane due to the bike lane and the imbalance of the green light given to Alexandra Parade, the extra time was significant. Additional traffic being forced to use Rathdowne will only add to this time. Whilst I appreciate non resident drivers using Station Street as a "rat run", other measures such as local traffic only signage can assist in reducing this occurrence. Imagine how many cars would seek to travel east by using Canning Street. This is totally unfair on residents in this Street and merely shifts the problem as opposed to sharing it. It will as well impact on the large number of cyclists who use Canning St. A compromise position could see vehicles blocked from entering Station from Princes and installing speed humps in the section of Station between Lee and Princes. This would significantly reduce traffic and also have a major impact on speed and traffic noise. In summary your own report from 2016 (point 6.2) clearly demonstrates that this proposal does not have merit given the disruption it will cause particularly on the residents of Canning, Lee and Davis in addition to dangers of people taking children to the primary school and bicycle riders using the Canning bike lane. Given the nature of this report I am surprised that the council is still considering the proposal. Not to mention the cost this has already incurred!

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
110	Lee St (between Rathdowne and Canning)	No	No Answer	I reside at **<redacted>** Lee Street Carlton North. I strongly opposed a trial and possible implementation of this street closure. • I am concerned it will increase traffic significantly in both Lee St and Canning St impacting residents amenity. • As you would be aware there is a bike trail along Canning Street, used by thousands of cyclists each day. The closure would create possible hazards etc for all road users including cyclists. Perhaps what is forgotten is that these cyclists still come down Lee Street, footpaths etc creating more hazards and possible incidents. • Lee St Primary School- I am concerned increased traffic would pose a threat to kids, parents who drop off and pick up and mums with prams who use all streets around the school. • Cars and Cyclists who go down Lee Street- one way street in wrong direction. I work from home and the number of cars and especially cyclists, who come down Lee St- a one way street- THE WRONG WAY is very high especially between 3-6pm. It is already a dangerous situation, which will only increase with the closure of Station St. • I have also raised the above issue with Fitzroy Police, who have advised me to send an e-mail stating my concerns. They have advised they will then look into this issue. • I am also concerned of an increase in rat runners, already a major issue in the area. • I do not see any advantage, whatsoever, in closing Station Street. It will only create more issues, dangerous situations and possible hazards as outlined in my above email. No doubt more issues, would also arise as the population increases in the area, including increased traffic congestion in the coming years and decades.
111	Address unknown	Yes	12 months	Safer cycling
112	North Fitzroy (address unknown)	No	6 months	Changing the roads would put cyclists and school children at risk of danger
113	Address unknown	No	6 months	Will increase traffic in Canning St where my son (and many young kids) attending Carlton North Primary rides his bike to school.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
114	Amess St	No	No Answer	I am a local resident, living in Amess St, North Carlton and working in Croydon. I commute between 5 and 6 days a week by car and use Station Street to access the east. I need a car for my work as home visits are a necessary service in my industry. I have a number of comments and opinions about this proposal. This street was closed for a period of 15 months in 2015 already. Why repeat this? A traffic impact assessment was carried out not 12 months ago. The cost of this report would have been considerable. Why repeat this? The report summary assesses the closure of Station Street to have adversely increased car density and queuing times along Canning and Davis streets. It also increased car and cycle proximity along Canning St as well as car and pedestrian contact on Princes St. The likelihood of collisions involving cyclists and pedestrians with cars increased also. Davis and Canning street residents will have further increased traffic levels as a result. Traffic density, congestion and impact on residents in Station street is low and is constituted by mainly locally generated traffic as reported in the 2016 report, section 6.2. The closure will not benefit the overall traffic network and will increase congestion in areas where cars, pedestrians and cyclists must mix. The closure will not benefit residents on Davis or Canning streets as it will increase traffic flow and queuing. The closure will benefit the of residents in Station street only. This is done at the expense of locally generated traffic, cyclists, pedestrians both on Princes St and towards school and preschool, residents of Davis St and residents of Canning St. What I do not understand is the reason this closure is being pursued at all. A handful of residents are wasting council time and ratepayers money for their own gain. This proposal has a negative impact on thousands of cyclists, pedestrians and other local residents who live on surrounding roads or use the local roads on a daily basis. The traffic impact assessment has been done already and the findings indicate these negative effects and with no effective gain. I do not support the closure of Station Street on a trial or permanent basis as it will have a negative impact on a huge number of locals. There are only 27 homes in the this already quiet part of Station Street by contrast to the thousands in the area that will be affected by the proposed closure.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
115	Address unknown	No	No Answer	Blocking Station St to vehicles will likely encourage greater vehicle traffic on Canning St, which is a very popular and very safe cycling corridor. More traffic in Canning St would diminish its use as an ideal north-south route for cyclists through Carlton. I don't necessarily object to Station St being closed to traffic - this sort of thing should be welcomed - but not at the expense of a likely impact on greater vehicle numbers on Canning St which ought to be protected as an ideal cycling route.
116	Address unknown	No	6 months	diverting the traffic that currently accesses this exit to Princes Street onto Lee, Canning and Davis Streets poses serious SAFETY RISKS FOR CYCLISTS AND PEDESTRIANS
117	Address unknown	No	6 months	The closure of Station St will divert hundreds of cars a day onto nearby Canning St. Canning St is a very successful corridor for cycling and the extra cars will be very dangerous for the cyclists. Canning St is also used for pedestrian access for the primary school. This is an ill conceived and poorly thought out proposal. If you block Station St you need to block Canning St as well to Alexandra Parade.
118	Station St (between Pigdon and Richardson)	No	No Answer	Re: Proposal to have a trial road closure of Station Street North Carlton at Princes Street I object to the proposal, and indeed to ever have a closure of Station Street at Princes Street, for the following reasons 1. I agree with the report commissioned by Yarra City council 'Traffic Impact Assessment Report for VicRoads' that the closure is not justified based on traffic impact assessment. As a local resident I use that entrance onto Princes Street a few times a week at various times of the day or night, and except for a brief period during the morning peak the traffic is negligible, if not nonexistent. 2. I think it is unfair on residents in Canning Street for them to have extra traffic being diverted along their street as a result of this closure, particularly as there are many cyclists (myself included a few times a week) who use Canning Street as a designated bicycle route. 3. I strongly object to the amount of money that has already been spent on this project- ($50 000 already I believe)- when the case for the closure has I believe been flimsy, but also when the Yarra City Council is so much in debt and I am sure has much higher priorities for spending our precious rate income, notwithstanding the Greens councillors' push on calming and greening on this issue. (I encourage those councillors to find a worthier place for their schemes if they must.) I think it is a travesty to spend any more Council funds on this issue. 4. Community support for the proposal has been poor - I believe over 80% of feedback so far has been against the proposal.
119	Canning St (between Pigdon and Richardson)	No	No Answer	As a cyclist who uses Canning St 3 or 4 times a week, I do not support the proposal because it would cause an increase in car traffic

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
120	Canning St (between Mary and Pigdon)	Yes	12 months	**First Submission** Station Street needs to be permanently closed to vehicles at Princes Street. This measure I believe will reduce the number of vehicles travelling through Station Street as an alternative to the nearby arterial Nicholson or Rathdowne Streets. Station Street is a narrow residential street. There are families with young children that live on the street. I am concerned by the volume of and speed at which vehicles travel along Station Street. A lot of this traffic does not seem to be local. I am supportive of any traffic restrictive measure on our streets, as this can only improve the safety and liveability of residents in the area. Melbourne's traffic volumes have grown significantly in recent years and continue to do so. The Counsel needs to implement more traffic restrictions to protect its residents from this onslaught. **Second Submission** STATION STREET PROPOSED CLOSURE AT PRINCESS STREET http://www.yoursayyarra.com.au/station-street-closure I DO support a 12 month trial closure of Station Street for the following reasons: Station Street needs to be permanently closed to vehicles at Princes Street. This measure I believe will reduce the number of vehicles travelling through Station Street as an alternative to the nearby arterial Nicholson or Rathdowne Streets. Station Street is a narrow residential street. There are families with young children that live on the street. I am concerned by the volume of and speed at which vehicles travel along Station Street. A lot of this traffic does not seem to be local. I am supportive of any traffic restrictive measure on our streets, as this can only improve the safety and liveability of residents in the area. Melbourne's traffic volumes have grown significantly in recent years and continue to do so. The Counsel needs to implement more traffic restrictions to protect its residents from this onslaught.
121	Station St (between Pigdon and Richardson)	Yes	6 months	Think the closure is a good idea for the community and it worked well when closed a for the drainage works. 6 months is probably enough time for the trial and a shorter trial means a shorter timeframe towards permanent closure.
122	Station St (between Mary and Pigdon)	No	No Answer	I have already made a submission to Yarracity to install humps in Station Street up to Park Street because of speeding drivers and large trucks through Station Street (between Richardson and Park) who are avoiding Nicholson Street traffic lights. The closure of Station Street at Princes Street will not address this issue and possibly make it worse through the effects of even more drivers using the northern part of Station Street to enter and exit onto Nicholson.
123	Address unknown	No	6 months	I am concerned about the impact of redirecting motor vehicle traffic onto Canning St.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
124	Address unknown	Yes	12 months	Streets Alive Yarra supports a 12-month trial of the closure of Station Street, for the simple reason that Yarra should gradually move toward calmer, safer streets. Traffic should be pushed to nominated arterials, surrounding “superblocks” of calm, quiet, safe residential streets. The proposed trial closure of Station Street will add to the evidence base, providing key data to better understand how to manage Yarra’s long term transition to increased levels of active transport. As Yarra’s population continues to increase, enabling active transport is the only effective way to manage congestion. The primary argument against the trial - that it will divert traffic to the bicycle priority route on Canning Street - is absurdly reductionist and suggests the absence of a long term plan. The solution is to apply traffic mitigation strategies to Canning Street as well. Both Station and Canning streets should be calm, slow and safe, with low levels of through traffic. A holistic LAPM approach is required, borrowing ideas from the Barcelona “superblock” concept. As part of a street hierarchy (https://www.streets-alive-yarra.org/street-hierarchy) both Station and Canning streets should be “green” streets - i.e. quiet, calm and with low levels of traffic. Traffic should be diverted onto nominated arterials such as Nicholson and Rathdowne. The image is an example, showing the street hiearchy in LAPM 20. The same concept can be applied to the LAPM area for Station Street.
125	Station St (between Mary and Pigdon)	No	No Answer	"I do not support proposed closure" I am registering my displeasure at the proposed closure of Station St at Princes Street. I feel there is enough speed humps along that end of Station St to discourage through traffic using it apart from the residents who live in the street. I did a survey last year re speed humps between Pigdon and Park Sts of residents here but to date I have heard of no follow up results. I have also signed a survey stating that I do not want the road closed. I am also very upset about Council's decision to not celebrate Australia Day on 26th January. Why weren't the residents consulted first. I have not heard of anyone in agreement with your decision and I have spoken to a lot of people. Another thing I am upset with is all the graffiti that is on every coner, house, empty shop, laneway, sign post, brick fence and even on top of the street verandahs in Nth Fitzroy shopping centre, The brick fence around the Catholic Church corner of Alexandra Parade and Nicholson St is a disgrace.
126	Address unknown	No	No Answer	We don't need extra cars on Canning St
127	Address unknown	No	No Answer	Canning street is a bike friendly street. We should be using it as an example, encouraging more peole to get on thier bikes and out of thier cars.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
128	Lee St (between Drummond and Rathdowne)	Yes	6 months	I think that it is preferable to keep streets local. It is also assists to keep traffic running more smoothly on Princess Street.
129	Station St (between Mary and Pigdon)	No	6 months	Easy access to and from Princes Street from Station Street With the Station Street closure, traffic will be redirected to Canning Street
130	North Fitzroy (address unknown)	No	No Answer	The number of cyclists traveling down Canning St is already enough for young children to deal with let alone increasing the traffic onto Canning St. A child was recently hit by a taxi on Canning/Lee Sts. Squeezing more traffic down Canning is not a solution. Surely if residents want Station St blocked off it can be done before Lee St. There must be alternative solutions. Increasing traffic directly into the surrounding streets of a primary school is madness.
131	Address unknown	No	No Answer	Re the proposed trial for the closure of Station St. Nth Carlton. We are totally opposed to this closure as it simply means more traffic will be directed to other streets, Nicholson, Drummond, Rathdowne and Lygon. We have fought other proposed closures for the past 50 years. Such closures mean that a group of residents selfishly want to change the neighbourhood traffic for their benefit only. it also means that the value of their properties increases at the expense of properties where the traffic will increase. We have seen similar proposals over many years and are determined to oppose them. The Vic Roads report is only about arterial roads, that does NOT include for example Drummond st. Many years ago there was the same proposal to close Drummond St. and this was overwhelmingly defeated by the community. Please inform us as to where this proposal originated from as we suspect it comes from lobbying residents who do not consider the other residents. Often these are relatively new residents.
132	Address unknown	Yes	No Answer	I do support the proposed trial closure as it will assist with motor vehicle traffic management along Station St.
133	Address unknown	No	No Answer	Hello, I am writing to say that I do not support the trial closure of Station St, as I use the Canning St bike path four days a week to ride to work and I think that it will increase traffic on Canning St, and therefore increase the likelihood of traffic/bike accidents on Canning St.

Attachment 5 - Station Street, Carlton North - Temporary Road Closure Submissions

No.	Address	Support for trial	Trial Length	Submission
134	Address unknown	No	No Answer	I do not support the trial closure for Station Street at Princess Street: • it will bottle neck traffic into the main arteries (Nicholson and Rathdowne Streets) for residents living in Station and Canning Streets going both into and out of Princess Streets (when coming from the West)
135	Address unknown	No	No Answer	I have heard that the council is investigating closing Station St Carlton North at Princes St. I am concerned that this will push cars onto Canning St. I frequently ride my bicycle along Canning St, choosing this route as there are few cars and it feels very safe. Having more cars on the street (even for a small part of the street) will reduce the feeling of safety and decrease this great asset to the area. Please add my details to any consultation that the council does on this matter, as I would like to stay informed.
136	North Fitzroy (address unknown)	No	No Answer	I am a resident of Fitzroy North and regularly commute via Canning St. I am concerned that plans to close Station St at Princes St will direct increased vehicle traffic to Canning St. Information obtained by Bicycle Network indicates that an independent traffic assessment has also found that the change would achieve no major benefits, and clog up the nearby streets. (https://www.bicyclenetwork.com.au/newsroom/2017/10/12/help-can-the-planned-station-street-closure). Canning St is a fantastic asset to cyclists and I believe its retention as a cycling corridor should be prioritised. I look forward to your reply.

11.8 PLN16/0085 - 115 Victoria Parade and 28 - 42 Young Street, Fitzroy - Australian Catholic University: Endorsement of Amended Development Plan.

Executive Summary

Purpose

To assess the revised Development Plan submitted for the Australian Catholic University Site (**ACU**) and against its compliance with the Order of the Victorian Civil and Administrative Tribunal (**VCAT**) approving the Development Plan.

Key Issues

Compliance with the VCAT Order dated 4 October 2017.

Financial Implications

None

PROPOSAL

Endorsement of the revised Development Plan submitted for the Australian Catholic University Site.

11.8 PLN16/0085 - 115 Victoria Parade and 28 - 42 Young Street, Fitzroy - Australian Catholic University: Endorsement of Amended Development Plan.

Trim Record Number: D17/188667
Responsible Officer: Director Planning and Place Making

Purpose

1. This report provides an overview of the revised Development Plan submitted for the Australian Catholic University Site (**ACU**) and assesses its compliance with the Order of the Victorian Civil and Administrative Tribunal (**VCAT**) approving the Development Plan subject to conditions.

Background

2. The subject site for the purpose of this report is defined by the area of the Development Plan Overlay - Schedule 2 (**DPO**) being land known as 115 Victoria Parade (Mary Glowery building), 28 Young Street (Car park) and 20 – 26 Brunswick Street (Cathedral Hall and adjacent building).

Figure 1: Aerial photo with DPO area overlayed.

3. An approved Development Plan for the portion of the Australian Catholic University land was endorsed by Council on 8 January 1999. On 9 February 2016 the University submitted changes to that Development Plan to Council for consideration.

4. Council determined at the Council meeting of 21 March 2017 that were it in a position to approve the amended Development Plan for the ACU site it would have subject to a number of conditions outlined below.

Element	Changed conditions sought by Council
Victoria Parade Building	<u>New Building</u> 1. Reduction in height of the proposed new building to have a maximum height of RL 86.5. 2. Proposed setbacks of level 1 – 3 to be increased to mirror those of levels 4 – 5. 3. Maintain the tripartite (podium, middle, top) composition of the building form as depicted in the DP. 4. Revised façade detail to the central bridging element introducing clear glazing and amended stair treatment. <u>Addition to Mary Glowery Building</u> 5. Setback of the addition to the Mary Glowery Building to Napier Street to be increased from 31.9m to a minimum of 42 metres. 6. The addition to the Mary Glowery building to be set in from the existing external façade of the heritage building. 7. Deletion of the external structural support columns. <u>General</u> 8. Reduction in height and extent of the two plant areas to not be visible from the street and designed to be integrated into the building. 9. No signage (lit or otherwise) to be visible from residential areas (including Young Street, Little Victoria, Napier Street, George Street, Little George Street and Gore Street.
Young Street Building	10. Retention of existing northern on boundary wall associated with the car park structure adjacent to 44 Young Street. 11. Eastern wall of the Young Street Hub to either comply with the B17 ResCode height envelope or alternatively, be no higher than the existing conditions with any higher structure to be setback to limit off site amenity impacts. 12. Reduction in height and extent of the plant area to not be visible from the street and designed to be integrated into the building.
Cars and Vehicular Access	13. A maximum of 50 car parking space located within a basement level utilising access and egress from the existing Victoria Parade crossover. 14. A minimum of 12 disabled car parking spaces accessible by staff and students.
Sustainable Transport	15. A minimum of 160 new bike spaces to be provided with the Development Plan area. 16. Retention of the Melbourne Bike Share facility in its current location. 17. A Green Travel Plan for the University examining implementation of more sustainable transport measures.
Landscaping	18. Additional information regarding lighting of the Hub landscape areas to ensure pedestrian safety while controlling light spill to adjoining residential areas.

Use	19. Accurate information applicable only to the Development Plan area regarding increased student and staff numbers reflecting the existing figures being 2,125 full time students and 180 staff.
Other	20. Any other changes as a consequence of the above.

5. The ACU had lodged declaration proceedings with the VCAT and a 4 day hearing was heard in June of this year.

6. The VCAT determined on 4 October that the Development Plan for the ACU should be approved subject to changes. The VCAT more specifically determined:

   (a) *The following is approved at the Australian Catholic University St Partrick's Campus Development Plan pursuant to clause 43.04 of the Yarra Planning Scheme: Australian Catholic University St Patrick's Campus Development Plan October 2016 with the following modifications:*

	Conditions
(a)	The variations recorded within the document prepared by Lyons Architects and titles Description of Proposed Design Changes (dated May 2017 other than those variations shown in respect of Council's conditions 6 and 7 on pages 6, 7, and 8.
(b)	Creation of a wall/fence along the northern boundary wall adjacent to 44 Young Street, to infill the gap between the dwelling fronting Young Street and the garage facing little Napier Street to ensure privacy within the secluded private open space and habitable room windows of 44 Young Street.
(c)	A minimum of 160 new bicycle spaces to be provided within the Development Plan area.
(d)	A Green Travel for the University examining the implementation of sustainable transport measures.
(e)	Any references to 270 car spaces in section 4.6, in Appendix A and other parts of the Development Plan where car parking number are mentioned must refer to a 'maximum of 270 car spaces'.
(f)	Additional information regarding lighting of the Hub landscape area to ensure pedestrian safety whilst avoiding light spill to adjoining residential properties. `

7. The above table outlines the six changes required to be made to the Development Plan and outlines the scope of assessment for this report.

8. The ACU lodged with Council on 25 October and 4 December 2017 the Development Plan which will be assessed by this report.

Planning Applications

9. A planning application for part of the ACU site being 115 and 115B Victoria Parade, Fitzroy or more particularly known as the Mary Glowery Building was lodged with Council on 20 November.

10. Council cannot determine that planning application until an amended Development Plan has been approved.

11. Concurrently, an application has been made to Heritage Victoria for the same site seeking heritage approval. That application is currently being advertised and a referral from Heritage Victoria to Council has been made.

12. The referral was received by the Planning and is currently being assessed by an external consultant heritage expert. Council officers have sought an extension of time to enable firstly, a detailed review of the application to be made by Council's external Heritage advisor, and then a report prepared to a formal meeting. If an extension of time to 6 February is not consented to by Heritage Victoria, an extension to 16 January 2018 will be sought so that it could be considered by the Internal Development Approvals Committee (IDAC).

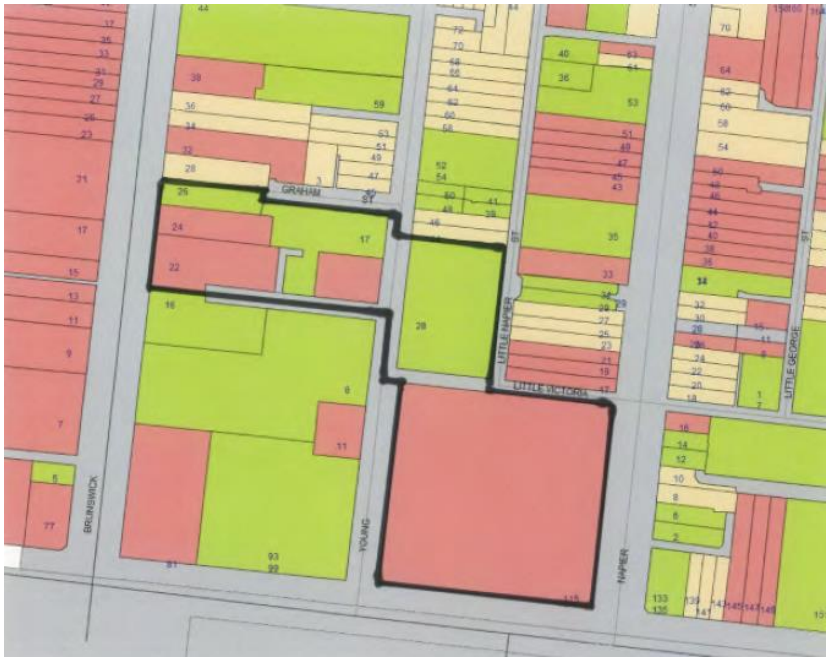
Planning Scheme Provisions

13. The site is included in two zones, Mixed Use Zone and the Commercial 1 Zone.

14. Both the Young Street precinct and the Victoria Parade precinct have interfaces with Residentially Zoned land (Neighbourhood Residential Zone 1) separated by either Little Napier/Victoria Street or Napier Street.

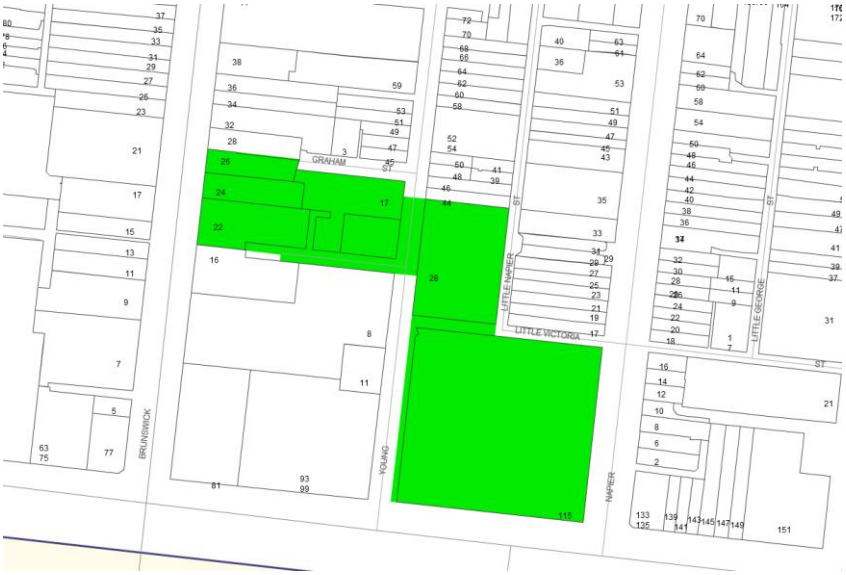
15. The site is also included within Heritage Overlay area 334 and 347 with buildings either identified as individually significant (red) or non-contributory (green). For the purpose of this report the car park at 28 Young Street is identified as non-contributory and the Mary Glowery building to Victoria Parade being Individually significant.

16. More recently the Mary Glowery building and its surrounding land have been included on the State Heritage Register.



17. An Environmental Audit Overlay and Design and Development Overlay – Schedule 2 – Main Roads and Boulevards also applies to the site.

18. The Development Plan Overlay - Schedule 2 Australian Catholic University Development Plan, applies to the site and is a site specific control which '*provides criteria for any future use and development'* of the site.



Extent of DPO outlined in Green

19. At Clause 3, the DPO outlines what is required to be shown in a Development Plan which includes requirements relevant to the use of the site, development and traffic and landscape management.

Planning Assessment

20. As outlined above, the VCAT decision required 6 points to be addressed in the revised Development Plan. The table below outlines where within the amended Development Plan the required changes have been made.

Conditions	Page reference showing changes.
The variations recorded within the document prepared by Lyons Architects and titles Description of Proposed Design Changes (dated May 2017 other than those variations shown in respect of Council's conditions 6 and 7 on pages 6, 7, and 8.	The Lyons Architects and titles Description of Proposed Design Changes (dated May 2017) show changes being the addition of an outdoor activity space above the Mary Glowery, an articulated clear glazing section; roof plant being set in from the building, signage removal, screening to the Young Street building, disable spaces and relocation of the bike share facility; compliance with each of these requirements is outlined below: <u>outdoor activity space above the Mary Glowery:</u> pages 36, 37, 40, 41, 43, 44, 48 and 128 <u>articulated clear glazing section:</u> pages 40 <u>roof plant being set in from the building:</u> pages 37 <u>signage removal:</u> pages 41and 42 <u>screening to the Young Street building:</u> pages 57 and 58 <u>disable spaces:</u> pages 62 <u>relocation of the bike share facility</u> page 63 and 113

Creation of a wall/fence along the northern boundary wall adjacent to 44 Young Street, to infill the gap between the dwelling fronting Young Street and the garage facing little Napier Street to ensure privacy within the secluded private open space and habitable room windows of 44 Young Street.	Pages 57 and 58
A minimum of 160 new bicycle spaces to be provided within the Development Plan area.	Pages 62, 105, 106, 110, 148 and 150
A Green Travel for the University examining the implementation of sustainable transport measures.	Pages 109, 147 – 155.
Any references to 270 car spaces in section 4.6, in Appendix A and other parts of the Development Plan where car parking number are mentioned must refer to a 'maximum of 270 car spaces'.	Pages 62, 104, 105, 106, 107, 110, 148 and 152
Additional information regarding lighting of the Hub landscape area to ensure pedestrian safety whilst avoiding light spill to adjoining residential properties. `	Pages 76 and 77

Other changes

21. The Development Plan while not required to has been updated to reflect the Mary Glowery Buildings inclusion on the State Heritage Register.

External Consultation

22. No external consultation required. The Council must endorse the amended Development Plan with the changes as directed by VCAT once it is satisfied those requirements have been met.

Internal Consultation (One Yarra)

23. No internal consultation required.

Financial Implications

24. No financial implications associated with the endorsement of the Development Plan.

Economic Implications

25. The approval of the Development Plan is the first step in the proposed expansion by the ACU and an increase in the number of students and employees within this area.

Sustainability Implications

26. Sustainability has been included within the Development Plan.

Social Implications

27. No particular social implications are known. The planning matters and heritage matters are being progressively assessed through the respective processes.

Human Rights Implications

28. No Human Rights Implications are known.

Communications with CALD Communities Implications

29. No CALD community implications are known.

Council Plan, Strategy and Policy Implications

30. Not specifically relevant to the considerations of this report. Normal town planning and heritage aspects are, however, important and require assessment via the due processes.

Legal Implications

31. None.

Conclusion

32. The amended Development Plan has included all the required changes outlined within the VCAT Order dated 4 October 2017.

33. Accordingly, the amended Development Plan is now recommended to be endorsed.

RECOMMENDATION

1. That Council:

   (a) note the report of officers regarding the amended Development Plan in compliance with the VCAT Order 4 October, 2017;

   (b) note the detailing of all the required changes to the Development Plan for the Australian Catholic University; and

   (c) authorise the Manager, Statutory Planning to endorse the Development Plan for the Australian Catholic University as outlined in this report and as shown in the attachments.

CONTACT OFFICER: **Mary Osman**
TITLE: **Manager Statutory Planning**
TEL: **92055334**

Attachments

1 PLN16/0085 - 115 Victoria Parade & 28 - 42 Young Street Fitzroy - Final Order
2 PLN16/0085 - 115 Victoria Parade & 28 - 42 Young Street Fitzroy - ACU Development Plan Part 1
3 PLN16/0085 - Public Notification ACU Development Plan Part 2
4 PLN16/0085 - Public Notification ACU Development Plan Part 3
5 PLN16/0085 - Public Notification ACU Development Plan Part 4
6 PLN16/0085 - Public Notification ACU Development Plan Part 5
7 PLN16/0085 - Public Notification ACU Development Plan Part 6
8 PLN16/0085 - Public Notification ACU Development Plan Part 7
9 PLN16/0085 - Public Notification ACU Development Plan Part 8
10 PLN16/0085 - Public Notification ACU Development Plan Part 9
11 PLN16/0085 - Public Notification ACU Development Plan Part 10
12 PLN16/0085 - Public Notification ACU Development Plan Part 11
13 PLN16/0085 - Public Notification ACU Development Plan Part 12

VICTORIAN CIVIL AND ADMINISTRATIVE TRIBUNAL

ADMINISTRATIVE DIVISION

PLANNING AND ENVIRONMENT LIST

VCAT REFERENCE NO. P9/2017

CATCHWORDS

Yarra Planning Scheme; Application pursuant to Section 149(1)(d) of the *Planning and Environment Act 1987*; Commercial 1 Zone; Mixed Use Zone; Development Plan Overlay Schedule 2 (DPO2); Heritage Overlay (HO334); State Significant Health and Education Precinct; Amendment to approved Development Plan; Australian Catholic University; St Patrick's Campus; Melbourne Bike Share facility; Daniel Mannix Building; Height; Setbacks; Integration with heritage fabric; Vehicle access; Parking.

APPLICANT	Australian Catholic University
RESPONSIBLE AUTHORITY	Yarra City Council
OTHER	South Fitzroy Protection Group Inc.
SUBJECT LAND	115B Victoria Parade 28-42 Young Street FITZROY VIC 3065
WHERE HELD	Melbourne
BEFORE	J A Bennett, Presiding Senior Member Alison Slattery, Member
HEARING TYPE	Hearing
DATE OF HEARING	27, 28, 29 and 30 June 2017
DATE OF INTERIM ORDER	1 September 2017
DATE OF ORDER	4 October 2017
CITATION	Australian Catholic University v Yarra CC [2017] VCAT 1387

ORDER

1 In application P9/2017 the decision of the Responsible Authority is set aside.

2 The *Australian Catholic University St Patrick's Campus Development Plan* dated January 1999 is amended.

3 The following is approved as the *Australian Catholic University St Patrick's Campus Development Plan* pursuant to Clause 43.04 of the Yarra Planning Scheme: *Australian Catholic University St Patrick's Campus Development Plan October 2016* with the following modifications:

(a) The variations recorded within the document prepared by Lyons Architects and titled Description of Proposed Design Changes (dated May 2017 other than those variations shown in respect of Council's conditions 6 and 7 on pages 6, 7 and 8.

(b) Creation of a wall/fence along the northern boundary wall adjacent to 44 Young Street to infill the gap between the dwelling fronting Young Street and the garage facing little Napier Street to ensure privacy within the secluded private open space and habitable room windows of 44 Young Street.

(c) A minimum of 160 new bicycle spaces to be provided within the Development Plan area.

(d) A Green Travel Plan for the University examining the implementation of sustainable transport measures.

(e) Any references to 270 car spaces in section 4.6, in Appendix A and other parts of the Development Plan where car parking numbers are mentioned must refer to a 'maximum of 270 car spaces'.

(f) Additional information regarding lighting of the Hub landscape area to ensure pedestrian safety whilst avoiding light spill to adjoining residential properties.

4 The Responsible Authority is directed to endorse the amended *Australian Catholic University St Patrick's Campus Development Plan October 2016* with the modifications set out in order 3 above.

J A Bennett
Presiding Senior Member

Alison Slattery
Member

APPEARANCES

For Australian Catholic University	Mr Stuart Morris, QC and Mr Barnaby Chessell, Barrister instructed by Norton Rose Fulbright Australia. They called expert evidence from the following witnesses: • Dr Stephen Weller, Chief Operating Officer and Deputy Vice-Chancellor of ACU. • Mr Andrew Biacsi, Town Planner of Contour Consultants Aust Pty Ltd. • Prof Mark Jacques, Landscape Architect and Urban Designer of Openwork Pty Ltd and Professor of Architecture (Urbanism) Industry Fellow at RMIT University. • Mr Mark O'Dwyer, Architect of H2o Architects Pty Ltd. • Mr Peter Barrett, Architectural Conservation Consultant. • Mr Stephen Hunt, Traffic Engineer of Traffix Group Pty Ltd. Mr Ross Leo, Acoustic Consultant of Marshall Day Acoustics prepared a witness statement but with the agreement of parties was not required to attend the hearing to present evidence and answer questions. Mr Morris also tabled an affidavit signed by Gregory Joseph Craven, Vice-Chancellor and President of the ACU.
For Yarra City Council	Ms Mimi Marcus, Solicitor of Maddocks Lawyers. She called expert evidence from the following witnesses: • Mr Bruce Trethowan, Architect and Heritage Consultant of Trethowan Architecture and Interior Design. • Mr Tim Biles, Town Planner and Urban Designer of Message Consultants Australia Pty Ltd.
For South Fitzroy Protection Group Inc.	Mr Phillip Campbell, Ms Jennie Morrison and Mr Christopher Balmford.

INFORMATION

Description of Proposal	In summary, the proposed amendments to the approved Development Plan seek to: • Demolish the four level car park in Young Street and replace it with a triangular shaped, three level, 16 metre high building to be used as a hub for performance and sports activities. The area around the building is to be developed and landscaped for communal open space. • Construct a 12 storey, 60 metre high building over the existing at-grade car park. Seven levels of basement for 270 cars are also to be provided with access off Napier Street. • Relocation of Melbourne Bike Share parking from Napier Street to Victoria Parade. • Construct a four storey addition to the northern wing of the Mary Glowery Building. • Demolish the existing glass façade and ramp attached to the front of the Mary Glowery Building facing Victoria Parade. In a more general sense, amendments also seek to formalise the strategic directions of the campus including consolidating the campus from a number of satellite sites, increasing student numbers to 12,900, increasing staff numbers to 1,200 and increasing the typical operating hours to 8.00am to 9.30pm Monday to Thursday, 8.00am to 6.00pm on Fridays with the library accessible 24 hours per day.
Nature of Proceeding	Application under section 149(1)(d) of the *Planning and Environment Act 1987* – to review the failure to determine an application to amend an approved Development Plan. [1]

[1] Section 4(2)(d) of the *Victorian Civil and Administrative Tribunal Act 1998* states a failure to make a decision is deemed to be a decision to refuse to make the decision.

Zones and Overlays	Commercial 1 Zone (**C1Z**). Mixed Use Zone (**MUZ**). Heritage Overlay Schedule 334 (**HO334**). Design and Development Overlay Schedule 2 (**DDO2**). Development Plan Overlay Schedule 2 (**DPO2**).
Key Scheme policies and provisions	Clauses 9, 10, 11, 15, 17, 18, 19, 21.03, 21.04, 21.05, 21.06, 21.07, 21.08, 21.09, 21.11, 22.02, 22.03, 22.05, 22.12, 22.16, 22.17, 32.04, 34.01, 43.01, 43.02, 43.04, 52.06, 52.07, 52.34 and 65.
Land Description	The section of the Australian Catholic University, St Patrick's Campus directly affected by the proposed amendments to the approved Development Plan comprises two parcels of land. The first, known as 115B Victoria Parade, is bounded by Victoria Parade, Young Street, Little Victoria Street and Napier Street and is occupied by the four and six storey Mary Glowery Building and an at-grade car park on the corner of Victoria Parade and Napier Street. The second, known as 28-42 Young Street, is an existing four level car park on the east side of Young Street, north of Little Victoria Street and west of Little Napier Street.
Tribunal inspection	An accompanied inspection took place in the middle of the last day of the hearing. The Tribunal also undertook two unaccompanied inspections of the locality prior to finalising its determination.

REASONS[2]

DELAY CAUSED BY THE GAZETTAL OF AMENDMENT VC139

1 Just prior to finalising our decision in late August 2017, Amendment VC139 of the Yarra Planning Scheme was gazetted on 29 August 2017. The Amendment includes modifications to State planning policy including to Clauses 11, 15 and 16, removes redundant references to the *Guidelines for Higher Density Residential Development* (Department of Sustainability and Environment, 2005), *Design Guidelines for Higher Density Residential Development* (Department of Sustainability and Environment, 2004), *Safer Design Guidelines for Victoria* (Crime Prevention Victoria and Department of Sustainability and Environment, 2005) and *Activity Centre Design Guidelines* (Department of Sustainability and Environment, 2005) in the State Planning Policy Framework (SPPF) and zones, and inserts references to the *Urban Design Guidelines for Victoria* (Department of Environment, Land, Water and Planning, 2017) in the SPPF.

2 In the interests of procedural fairness, parties were invited, should they wish, to make further submissions addressing any implications of Amendment VC139 relating to the proceeding. Parties were given a timetable which concluded on 29 September 2017.

3 By that date, written submissions had been received from Council, the South Fitzroy Protection Group Inc and Norton Rose Fulbright on behalf of the Australian Catholic University (ACU).

4 Council made general reference to the changes introduced by Amendment VC139 and made specific comment about height and scale, access arrangements and Council's condition/variation 18 (lighting of the Hub landscape area). Council noted that the *Urban Design Guidelines for Victoria* (Urban Design Guidelines) identify the importance of built form and massing adjacent to residential uses. Specifically, it noted that objective 1.3.3 seeks to ensure that large development sites maintain the amenity of adjacent residential areas and that objective 1.3.3a seeks to create a transition from large development sites to adjacent residential neighbourhoods using scale, built form and uses. Council reiterates that the proposal fails to respond to and transition appropriately at its interface with the low scale, fine grain and largely residential neighbourhood to the east and north, all within a Heritage Overlay. In terms of access, Council reiterates that providing access from Napier Street will unreasonably impact on the amenity of nearby properties where currently it is a reasonably quiet residential street. Council also states that access from Napier Street will likely impact on the operation of cyclists in Napier Street. Reference was made to objectives 1.1.3, 1.13c, 2.1.1 and 2.3.1 in the Urban Design

[2] We have considered the submissions of all the parties that appeared, all the written and oral evidence, all the exhibits tendered by the parties, and all the statements of grounds filed. We do not recite or refer to all of the contents of those documents in these reasons.

Guidelines to support these comments. Council noted that Clause 15.01-2 and objectives 3.1.4, 6.5.3 and 6.5.4 of the Urban Design Guidelines support Council's condition/variation 18.

5 The South Fitzroy Protection Group Inc made a detailed submission on the impact of changes introduced by Amendment VC139. Specifically comment was made about Clause 15 including policy to ensure that development appropriately responds to and protects places and sites with significant heritage and other listed values. The submission highlighted phrases such as *development planning must support* and *planning should achieve high quality urban design and architecture that contributes positively to local urban character and sense of place,* and *minimise detrimental impact on neighbouring properties*. The changes introduced by Amendment VC139 reaffirms the previous opposition by the South Fitzroy Protection Group Inc to the amendments sought to the approved Development Plan. Specifically these include a reduction in the height of the new building, increasing the setback from Napier Street of the new extension to the Mary Glowery Building, avoiding vehicle access from Napier Street and reducing the height or setting back the new Hub Building from Little Napier Street.

6 The ACU made reference to the changes made by Amendment VC139 and submitted that the planning policy context has not materially changed from that considered at the hearing. The submission notes that Council's requirement for additional information about Hub landscaping and lighting (condition 18) has already been agreed by the ACU. Issues about built form and vehicular and pedestrian access were discussed at the hearing and the changes introduced by Amendment VC139 do not introduce new considerations that were not comprehensively addressed at that time.

A BRIEF OVERVIEW OF THE ST PATRICK'S CAMPUS OF THE AUSTRALIAN CATHOLIC UNIVERSITY

7 The St Patrick's campus of the ACU in Fitzroy was established in 2000 following amalgamation of two existing campuses known as Mercy and Christ. The Catholic Church has had a long association with the Fitzroy site which in a physical sense dates back to the purchase of the Exhibition Boot Factory in 1903 for meeting rooms, and the later construction of Cathedral Hall in 1904. Even earlier, in 1842, it was the birthplace of Mary MacKillop, St Mary of the Cross.

8 Over subsequent decades, additional properties have been purchased and/or developed since 2000 as part of the expansion of the ACU campus. These notably include the Daniel Mannix Building and St Mary of the Cross Square, the Mary Glowery Building, the Young Street multi-level car park, a modular building containing tutorial rooms, and refurbishment of Cathedral Hall and other nearby buildings.

9 Although the amendments being sought to the approved Development Plan go beyond changes to built form, the focus of the hearing was about the

built form changes proposed for two of the sites forming part of the St Patrick's Campus. These can be most conveniently described as the Mary Glowery Building and associated at-grade car park at 115B Victoria Parade and the multi-level car park at 28-42 Young Street.

10 The following plan depicts the boundaries of the whole campus with a dotted line and identifies the two sites we are primarily concerned with being numbers 1 and 5, located east of Young Street.

11 The plan also shows the location of Napier Street which is of relevance for our discussion about car parking and access. The plan also depicts the overall layout of the campus and the physical connectedness of the various parts of the campus.

12 Although not named on the plan, Little Victoria Street links Young and Napier Streets between sites 1 and 5 and Little Napier Street is a north-south laneway separating site 5 from the residential properties to the east.

WHAT AMENDMENTS ARE BEING SOUGHT TO THE APPROVED DEVELOPMENT PLAN?

13 The existing Development Plan (*Australian Catholic University St Patrick's Campus Development Plan October 1999*) was endorsed by Council in January 1999. It followed the gazettal of a Development Plan Overlay (DPO) in October 1998. Importantly, the DPO did not cover the whole of the St Patrick's Campus but was confined to the two sites identified as 1 and 5 on the campus plan, as well as 20-26 Brunswick Street (identified as 3, 7, 8, 9, 12, 13 and 10 on the campus plan, and which are principally the Cathedral Hall and associated buildings).

14 A planning permit was concurrently issued which allowed for the use and development of the land for the purposes of education establishment (university) and associated buildings and works, car parking, landscaping and signage. Plans endorsed under the permit have subsequently been amended in June 1999, September 2000 and June 2016.

15 The application to amend the approved Development Plan was made in February 2016. Despite the notice exemption provisions contained in the DPO, Council gave notice of the amendment application and received 123 submissions. Council also obtained internal and external advice and undertook a consultation forum.

16 In response to these inputs, in October 2016 ACU lodged an amended Development Plan. The main changes involved reducing the height of the new building and extensions to the Mary Glowery Building, changing the design of the new building on the Young Street car park, redesigning the access from Napier Street including retention of all on-street parking on the east side of the street and removing 2 street trees, deleting the Young Street road closure and air bridges, removing signage to Napier Street, relocating the bike share facility to Victoria Parade and providing screening to protect the secluded open space of 44 Young Street.

17 The amended Development Plan incorporating the modifications referred to above comprises:

- Demolition of the three/four storey car parking structure in Young Street and replacing it with a three storey building with a triangular footprint which is positioned at an angle to Little Napier Street. The building is to be used for performance and sports activities and the land around the building is to be developed for outdoor spaces. The building has a height of 16.2 metres (approximate RL of 61.23 metres).
- Construction of a 60 metre high building over 7 levels of basement parking in place of the at-grade car park in the south eastern corner of the site. The building has an approximate RL of 100.4 metres and the car park a capacity of 270 vehicles. The upper levels of the building will partly extend over the northern wing of the Mary Glowery

Building. The new building adds an additional three storeys plus plant to the existing building (total RL of 85.9 metres) along part of the Young and Little Victoria Street frontages. Setbacks from Napier Street and the extent of overhang of the new extension over the Mary Glowery Building have been varied.

- Removal of the glazed entrance and ramp from Victoria Parade and restoration of the façade. The main entrance will then be via the new 60 metre high building.
- Providing access to the basement parking from Napier Street with changes to kerb side parking, street trees, bicycle parking and the bicycle lane to accommodate traffic movements to and from the basement.
- Student numbers to increase from 10,000 to 12,900 and staff numbers from 800 to 1,200.

WHAT POSITION HAS COUNCIL TAKEN IN RESPECT OF THE PROPOSED AMENDMENTS?

18 Subsequent to the lodgement of the application for review, Council in March 2017 determined that it supports the amended Development Plan, subject to 20 changes or variations.

19 Council' submission and the evidence of witnesses called by Council commented about each of these changes but, as we later discuss, we have grouped these changes into four main points of contention or issues to be determined. However, in our discussion of each of the issues, we have included reference to Council's conditions to ensure that we have considered each of the changes Council is wishing to make before the amended Development Plan is approved.

WHAT CONSIDERATION HAVE WE TAKEN OF THE SUBMISSION MADE BY THE SOUTH FITZROY PROTECTION GROUP INC.?

20 We start by recording that by order dated 2 May 2017, Deputy President Gibson determined that the application by the South Fitzroy Protection Group Inc. to be joined as a party to the proceeding pursuant to section 60 of the *Victorian Civil and Administrative Tribunal Act 1998* be refused.

21 However Deputy President Gibson also determined that pursuant to section 98(1)(c) of the *Victorian Civil and Administrative Tribunal Act 1998*, leave was given to South Fitzroy Protection Group Inc to make a submission at the hearing of this proceeding which does not exceed one hour in length of time subject to the discretion of the Tribunal at the hearing. She also determined that making a submission at the hearing did not include a right to call witnesses, or to ask questions of witnesses called by the parties.

22 Clauses 43.04-1 and 43.04-3 of the DPO provides that a Development Plan only has to be prepared or amended to the satisfaction of the responsible authority. This circumstance can be distinguished from most planning

permit applications where there are specific notification and review rights by third parties. However the decision guidelines for DPO2 do provide for public inspection of the Development Plan for 14 days day prior to its consideration by the responsible authority and consideration of any written comments received in response to the display of the Development Plan.

23 The South Fitzroy Protection Group Inc. presented a comprehensive written submission which we do not recite in detail in these reasons. In summary, the South Fitzroy Protection Group Inc. supports an amendment to the approved Development Plan provided it is subject to strict adherence with the 20 changes sought by Council. However, it seems to us that the South Fitzroy Protection Group Inc. has some major concerns about the ongoing expansion of the St Patrick's Campus and the adverse impacts it is having on the residential areas in Napier and other nearby streets.

24 Although the South Fitzroy Protection Group Inc. submitted that the proposal represents an attempted misuse of the planning system, we do not agree with that proposition. We acknowledge that the ongoing expansion of institutional uses can have adverse impacts on surrounding neighbourhoods, but would not confine that to the inner suburbs of Australian cities. It can affect any urban area and includes both a geographic expansion of an institution as well as an intensification of activity on a site or sites.

25 Conflict arising from the expansion of schools is a relatively common example, although it tends to be more on the basis of applications for planning permits rather than Development Plans. The specific list of misuses on pages 1 and 2 of the executive summary of the submission presented by the South Fitzroy Protection Group Inc. neatly encapsulates the key concerns about the proposed amendments to the Development Plan. Some of these, such as the loss of revenue from nine car spaces and ignoring the commitments given in the MOU signed on 19 June 2015, are not matters which are germane to our consideration of the planning merits of the amendments to the approved Development Plan. Nor do we comment on the fairness or otherwise of the DPO and the way it is structured to remove third party notification and review rights.

26 We emphasise that we are not assessing a planning permit application and that further detailed approvals will be required. It is correct that the Development Plan that has been approved, and which we are being asked to amend, does contain a great deal of detail. Despite that detail, we need to be mindful that any later approvals only have to be generally in accordance with the approved Development Plan. It is important therefore, that we do not get overly focussed on the minutia of building design, such as whether windows should be screened, what signage should be allowed or what should be the colour and materials of buildings. In our opinion that is best left to the permit application stage.

WHAT ARE THE KEY POINTS IN CONTENTION REQUIRING OUR DECISION?

27 We have previously recorded that Council has decided to support the amendments to the approved Development Plan subject to 20 conditions or variations to different elements of the plan. For completeness, we record these as follows:

Element	Conditions/variation required
Victoria Parade Building	<u>New Building</u> 1. Reduction in height of the proposed new building to have a maximum height of RL 86.5 (approximately 14 metres less than proposed). 2. Proposed setbacks of levels 1-3 to be increased to mirror those of levels 4-5. 3. Maintain the tripartite (podium, middle, top) composition of the building form as depicted in the Development Plan. 4. Revised façade detail to the central bridging element introducing clear glazing and amended stair treatment. <u>Addition to Mary Glowery Building</u> 5. Setback of the addition to the Mary Glowery Building to Napier Street to be increased from 31.9 metres to a minimum of 42 metres. 6. The addition to the Mary Glowery Building to be set in from the existing external façade of the heritage building. 7. Deletion of the external structural support columns. <u>General</u> 8. Reduction in height and extent of the two plant areas to not be

	visible from the street and designed to be integrated into the building. 9. No signage (lit or otherwise) to be visible from residential areas (including Young Street, Little Victoria, Napier Street, George Street, Little George Street and Gore Street).
Young Street Building	10.Retention of existing northern on boundary wall associated with car park structure adjacent to 44 Young Street. 11.Eastern wall of the Young Street Hub to either comply with the B17 height envelope of Clause 55 (ResCode) or alternatively, to be no higher than the existing conditions with any higher structure to be set back to limit off site amenity impacts. 12.Reduction in height and extent of the plant area to not be visible from the street and designed to be integrated into the building.
Cars and Vehicular Access	13.A maximum of 50 car parking spaces located within a basement level utilising access and egress from the existing Victoria Parade crossover. 14.A minimum of 12 disabled car parking spaces accessible by staff and students.
Sustainable Transport	15.A minimum of 160 new bike spaces to be provided with(in) the Development Plan area. 16.Retention of the Melbourne Bike Share facility in its current location. 17.A Green Travel Plan for the University examining

	implementation of more sustainable transport measures.
Landscaping	18.Additional information regarding lighting of the Hub landscape areas to ensure pedestrian safety while controlling light spill to adjoining residential areas.
Use	19.Accurate information applicable only to the Development Plan area regarding increased student and staff numbers reflecting the existing figures being 2,125 full time students and 180 staff.
Other	20.Any other changes as a consequence of the above.

28 Of these 20 changes, the ACU has agreed to modify the Development Plan in response to conditions 3, 4, 6, 7 and 8.

29 Mr Biles in giving evidence on behalf of Council does not consider condition 2 vital to the success of the DPO and is unnecessary. He supports the changes made to the Development Plan in response to conditions 3, 4, 6 and 7. He considers that conditions 8, 9, 10, 11 and 12 should be refined/reworked and supports conditions 18 and 19. He did not review conditions 13 to 17 concerning cars and vehicular access and sustainable transport.

30 Mr Trethowan was called to give heritage evidence on behalf of Council. He made reference to conditions 1 to 12 in part 1 of his statement but did not make specific recommendations in relation to each individual condition. Instead he undertook a holistic assessment of the proposed amendments to the approved Development Plan and of the built form impacts. In response to questions in cross-examination he acknowledged that he had not reviewed the May 2017 plans showing design changes. He did, however, provide verbal comment on each of the design changes made in response to conditions/variations relevant to heritage issues.

31 On the basis of Mr Biles' evidence we can confidently limit the points in dispute between Council and the ACU. We are less certain about limiting the issues in dispute based on Mr Trethowan's written evidence. However, our written notes of his answers to questions has enabled us to better understand his response to each of the Council's conditions/variations that have implications from a heritage perspective.

32 Given these circumstances, we have therefore found it necessary to comment on each of the conditions as points in contention or dispute, whilst also having regard to Mr Biles' specific commentary about each of the conditions he was asked to review, together with the verbal comments made by Mr Trethowan.

OUR ASSESSMENT OF EACH OF THE POINTS IN CONTENTION

33 Before discussing each of these matters, we firstly comment about the strategic planning context and what the Planning Scheme has to say about development for this major institution and for land in this section of the municipality.

The Yarra Planning Scheme

34 We start by recording that the recently released *Plan Melbourne 2017-2050: Metropolitan Planning Strategy* (**Metropolitan Strategy**) identifies the St Vincent's Hospital and the ACU Precinct in East Melbourne/Fitzroy as a place of state significance that will be the focus for investment and growth.[3] The purpose of these health and education precincts is:

> *To support health and education services that are well served by public transport in a range of locations around Melbourne.*
>
> *Their specialised economic functions will be reinforced, and they should provide opportunity for ancillary health and education services, retail, commercial and accommodation uses.*

35 Map 4 in the Metropolitan Strategy depicts key features in and around Melbourne's central city and identifies the ACU as an Education facility.[4] It also identifies St Vincent's Hospital and the Royal Victorian Eye and Ear Hospital, which in effect create a cluster of health and educational facilities in this location.

36 Policy at 1.1.4 in the Metropolitan Strategy notes that Victoria has an international reputation in health and medical research and is a leading provider of high quality international education. It records that almost 90,000 international students are linked along the listed corridors (including the City) and observes that planning for the growth of these precincts will need to focus on improving access and diversifying job choices. Importantly for our assessment of the amended Development Plan, it is policy that co-location of facilities (for example, a university and a hospital) will make better use of existing infrastructure and support the growth of associated businesses and industries. Specialised economic functions should be reinforced.[5]

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3 Metropolitan Strategy - pages 14 and 15.
4 Metropolitan Strategy - page 24.
5 Metropolitan Strategy – page 34.

37 These broad, metropolitan level strategic aims are reinforced in State planning policies in the Yarra Planning Scheme. For example, Clause 11.06-01 includes a strategy to:

> *Support the significant employment and servicing role of Health and Education Precincts by:*
>
> - *focussing on improving access, particularly public transport access.*
> - *encouraging co-location of facilities to better utilise existing infrastructure.*
> - *supporting and facilitating growth of associated businesses and industries.*
> - *reinforcing their specialised economic functions while also providing opportunities for ancillary retail, commercial, accommodation and supporting services.*

38 Clause 19.02-1 for health facilities includes a strategy to locate hospitals and other large health service facilities in designated health precincts and areas highly accessible to public and private transport. Similarly, policy for education facilities at Clause 19.02-2 includes a strategy to locate secondary and tertiary education facilities in designated education precincts and areas which are highly accessible to public transport.

39 Local policy is largely silent about the ACU campus, although land along Brunswick Street, including part of the campus, has been identified as being in a Major Activity Centre on the strategic framework plan in Clause 21.03 (the plan includes a note that the map is only indicative).

40 Victoria Parade is also identified as a Boulevard in Clause 21.04.

41 There is acknowledgement in Clause 21.04-4 that while large regional facilities such as the Epworth and St Vincent's hospitals are important to the community, their operation and expansion must respond to the local context. The two hospital facilities are simply used as examples, and it is reasonable to presume that the ACU campus is also a large regional facility that must respond to local context. There is also an objective to limit the impact of large regional facilities and to require the preparation of master plans for such facilities. Although the approved Development Plan is a form of master plan, it does not cover the whole ACU campus but only that part within DPO2.

42 We record that much of local planning policy is framed to guide consideration of permit applications, not Development Plans or amendments to an approved Development Plan. Council fairly concedes in its submission (paragraph 73) that the primary issue for Council is the way in which the proposed built form responds to its physical context, there being largely no dispute about the strategic context.

43 Whilst that may be Council's position about strategic context, we consider it is too qualified. Based on our understanding of the Metropolitan Strategy,

together with relevant State and local planning policies, we consider that there can be no dispute and no uncertainty about the state significance of this particular health and education precinct and the support being given for it to grow and for the specialised economic functions to be reinforced.

44 Although planning policies and other documents frequently ask for high quality urban design and architecture (sometimes even excellence or exemplary design), Clause 65 of the Planning Scheme seeks an acceptable outcome, not an ideal or perfect outcome. We agree with Council that how the built form responds to its physical context is the key issue in dispute, but observe that the designation of the precinct as one of state significance carries great weight in terms of assessing the physical context.

45 For example, we would suggest that a high rise apartment building, with a similar building envelope, but without the support of being a use or activity consistent with the reason for it being designated as a precinct of state significance, may mean that such a proposal would not be acceptable in this physical context. To suggest otherwise downplays or diminishes the role of higher level strategic policy in balancing potentially conflicting objectives to achieve a net community benefit.[6]

Visual bulk of the main building - height and setbacks, impact on heritage fabric within the site and neighbourhood, and providing an appropriate transition to the lower scale, fine grain and largely residential neighbourhood to the north and east (conditions 1 to 9 and 12)

46 The proposal to construct a 60 metre high building on the existing at-grade car park on the corner of Victoria Parade and Napier Street and to extend the new building to the northwest as an upper level addition to the Mary Glowery Building has generated most opposition from nearby residents and from Council. There are related concerns about setbacks and the way in which the new upper level addition to the Mary Glowery Building has been integrated into the heritage fabric of that building.

47 We consider that it is more useful to refer to building heights in metres rather than storeys, as the number of storeys will vary depending on floor to floor heights. We therefore use metres in the following discussion of building heights.

48 We have earlier in our reasons recorded that the ACU has modified the Development Plan to respond to Council's conditions 3, 4, 6, 7 and 8 and that Mr Biles supports the modifications made in response to conditions 2, 3, 4 and 7. These modifications are included in a document *'Description of Proposed Design Changes'*, dated 26 May 2017. Mr Biles agrees with the thrust of condition 8 concerning the two plant areas and considers that the issue of signage referred to in condition 9 should be assessed separately at the permit application stage.

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6 Clause 10.04 of the Yarra Planning Scheme.

49 We record that Mr Trethowan still supports the changes required by condition 2 (Napier Street setbacks), condition 5 (increased setback from 31.9 to 42 metres in Napier Street) and conditions 6 and 7 (setbacks and changes to the additions to the Mary Glowery Building). He considers the changes to the Young Street building are appropriate subject to the base of the triangle aligning with Little Napier Street. In particular, he does not accept the changes made in response to conditions 6 and 7 and considers the new addition is 'still oozing over the existing building'. He considers that the height of the new building should not exceed that of the Daniel Mannix Building (RL 89.7 metres). Although not subject to a Council condition/variation, Mr Trethowan also seeks to preserve the remnant section of the original perimeter wall of the Note and Stamp Printing Department, which is positioned along Napier and Little Victoria Streets.

50 The approximate 14 metre reduction in height of the new building required by Condition 1 (from RL 100.4 metres to RL 86.5 metres) represents a significant change to the building envelope. We note that the upper level extension to the Mary Glowery Building is to a height of RL 85.9 metres and that the reduction in height, in effect, only affects the top four levels of the new building (plus plant). In considering whether the height should be reduced we have had regard to Mr Biles notion of a zone of transition to the north, north east and east of the ACU site. We also note that the officer's report to Council recommended that the building be reduced in height to RL 92 metres, or about half the reduction to RL 86.5 metres decided upon by Council at its meeting on 21 March 2017. We have taken into account Mr Trethowan's opinion that the building should not exceed the height of the Daniel Mannix Building (RL 89.7 metres).

Condition 1

51 We have also had regard to the expert evidence of Mr O'Dwyer and Professor Jacques who were called on behalf of the ACU to support the 60 metre high building and the upper level 31.9 metre setback from Napier Street. Of relevance to our discussion about heights and setbacks, Mr O'Dwyer provided an analysis of neighbourhood character, site context and built form and noted the precedents for higher built form in the neighbourhood.

52 Whilst we acknowledge the existence of higher buildings in the wider precinct, including the public housing towers at Atherton Gardens and the taller buildings of the Royal Victorian Eye and Ear Hospital, St Vincent's Hospital and the Daniel Mannix Building, we are not persuaded that the existence of these taller buildings can be reasonably used to justify a 60 metre high building at 115B Victoria Parade, given the specific and different physical context of each site. Of the examples given, we consider that the Daniel Mannix Building is most relevant given it has been approved and constructed very recently, is in the same precinct of

recognised stage significance and with a somewhat similar physical relationship to the lower scale residential areas to the north and east.

53 Professor Jacques gave evidence about urban design issues and in paragraphs 19 to 57 made specific comment about the height and setbacks of the new Victoria Parade building. He acknowledges that the proposed building massing is substantial, but that when seen in the context of others in the precinct, does not appear to be demonstrably taller or more massive. He considers that the massing acknowledges the lower height condition in Napier Street through a steep 15 metre wide transition at its eastern end and that this steep transition of emphatically different heights characterises other precincts across Melbourne.

54 We find Professor Jacques' reference to a steep transition a more accurate description than the concept of a zone of transition, which is depicted on Figure 3 of Mr Biles' evidence statement as being within the residential area to the north, northeast and east. In our minds this zone of transition implies a graduation or stepping down in height away from ACU campus. Rather than a zone of transition, we consider there is an abrupt edge, perhaps it could be described as an escarpment, along Little Napier Street, Little Victoria Street and the southern section of Napier Street, where there is distinct difference in built form and activity on either side of the boundary.

55 At the hearing there was considerable discussion about the relative heights of buildings along the north side of Victoria Parade and whether an appropriate transition in scale could or should be achieved with a building of RL 100.4 metres or at RL 86.5 metres. At RL 100.4 metres the proposed 60 metre high building would be the highest building between Nicholson Street to the west and Smith Street to the east. The streetscape diagram (Diagram 6) in Mr Biles' evidence statement suggests that the proposed building would exceed the height of the Daniel Mannix Building and the St Vincent's Private Hospital Building by approximately 10 metres. There is a building within the St Vincent's Public Hospital which exceeds the height of the proposed building by approximately one metre, but it is located well north of Victoria Parade and is viewed within the context of mostly higher built form.

56 We consider a very important factor in determining whether a 60 metre high building is acceptable is that it is primarily positioned within, and has a presence as part of, the linear development along Victoria Parade. Whilst future heights to the east may be tempered by the Heritage Overlay, the existence of the overlay does not necessarily prevent some form of redevelopment of the mostly low rise buildings fronting Victoria Parade. The new building does not require demolition of dwellings, is positioned outside the residential neighbourhood and separated from the dwellings further north and north east in Napier Street by the retained Mary Glowery Building.

57 A reduction in height to RL 86.5 metres as sought by Council would result in the building having a height approximately 4 metres less than the Daniel Mannix Building. Mr Trethowan, in answer to questions, has suggested that the new building should be no higher than the Daniel Mannix Building. We note that a previous permit for a RL 92.4 metre high building on the corner of Brunswick Street has now lapsed, although we would expect that at some point the site will be redeveloped for a taller building than now exists. But even if a building is eventually constructed on that site of the same or greater height, it can arguably sustain a higher building because of its location on a prominent corner, remoteness to low scale residential areas and proximity to existing higher buildings, including those to the west and south west.

58 Despite significant increases in height, we do not find the height of the building unacceptable when viewed from the north in Napier Street. We consider that because of the way in which the highest part of the new building is positioned south of the northern wing of the Mary Glowery Building, a visual transition has been achieved from the north in Napier Street. The northern wing of the Mary Glowery Building, with a height of RL 61.3 metres, provides physical separation between the residential areas to the north and the highest part of the new building, and acts as a podium for the higher tower form beyond. The viewline south along Napier Street also includes a backdrop of higher buildings on the south side of Victoria Parade. In making these comments we acknowledge that the new building will be markedly taller than those buildings and there is no hiding the fact that it represents a significant increase in height over any existing buildings along Napier Street, south of Gertrude Street.

59 More problematic is the appearance of the building from the east side of Napier Street in the residential section between Victoria Parade and Little Victoria Street. We are less concerned about the properties facing Victoria Parade that are zoned Commercial 1 Zone and, to a lesser extent, those properties opposite the eastern end of the northern wing of the Mary Glowery Building which already face the eastern end of a taller non-residential building. The intermediate built form of the first 6 levels (ground through to level 5) will largely obscure views of the building below the height nominated by Council. As height increases above that point more of the building will be visible although at a very steep angle from the viewing point on the opposite, eastern side of Napier Street.

60 The question for us is whether the height at RL 100.4 metres (60 metres) is sufficiently respectful of the lower scale residential properties to the east or whether the height should be reduced to that nominated by Council. Alternatively we could nominate a height that was suggested by Mr Trethowan at the hearing (rounded to RL 90 metres or a 50 metre high building similar to the Daniel Mannix Building).

61 Earlier in our reasons we made reference to the site being part of a precinct of state significance and observed that such a designation must have great

weight in deciding whether a building of the proposed height should be allowed on this site. During the hearing we also received evidence about the ACU's desire to consolidate a number of separate facilities onto the St Patrick's campus and of the need to provide a certain amount of floor space to meet the needs of students and staff from those consolidated facilities. In essence, it was put to us that lowering the new building by 14 metres would have a significant impact on the floor space yield and hence the ability to consolidate students and staff in the manner described to us by expert witnesses at the hearing and in the amended Development Plan documents.

62 In weighing up whether the new building should be reduced in height, we acknowledge that unlike many taller buildings we are asked to assess, there are no direct adverse amenity impacts to residential neighbours caused by overlooking, overshadowing, high walls on boundaries and so forth. In this proposal, the height of the building is about potential detriment to visual amenity and the character of the nearby residential area.

63 In effect, Council is asking that a proposed 60 metre building be reduced in height by approximately 25%. The alternative suggestion by Mr Trethowan to match the height of the Daniel Mannix Building would reduce it by approximately 15%.

64 We consider that Council has gone too far in asking for the building to be reduced in height by 14 metres. The decision was not consistent with the officer recommendation to reduce the height by 8 metres to RL 92 metres. Neither of these heights has direct reference to the height of existing buildings.

65 Ultimately, we have come to the conclusion that a building with a height of 60 metres is acceptable despite its location proximate to a low scale, residential precinct. We acknowledge that the ACU requires a certain floor space to accommodate students and staff as a result of consolidation of a number of remote facilities, and that the ACU is part of a health and education precinct of state significance. We agree with Professor Jacques that the building has substantial massing and that there is a steep transition to Napier Street. That will remain the case even if the building is lowered in height due to the impact of diminishing perspective.

Condition 2

66 Condition 2 requires the proposed setbacks of levels 1-3 to be increased to mirror those of levels 4-5. Mr Biles does not support the condition and does not consider it is vital to the success of the DPO. We go further and consider that the articulation of the new building and the way in which it has been set back, is vital to the successful composition of the building and the way in which it integrates with the retained northern and western wings of the Mary Glowery Building.

67 The proposed setbacks open up views to the ends of the wings and lessen visual bulk by stepping back the levels to a point until the topmost levels

(Levels 8 and above) cantilever towards Napier Street but still at a setback of over 15 metres.

68 We agree with Mr Biles and do not support the change required by condition 2.

Condition 3

69 Condition 3 requires that the tripartite (podium, middle, top) composition of the building form, as depicted in the Development Plan, be maintained. We agree with Mr Biles and other expert witnesses that this tripartite form has been maintained and confirm that we support the composition as shown in the amended Development Plan.

Condition 4

70 Condition 4 requires a revised façade detail to the central bridging element to introduce a clear glazing and amended stair treatment. The 26 May 2017 proposed design changes incorporate a glazed element and we support the modification made on the basis that it provides visual separation between the new building and the upper level extension to the Mary Glowery Building.

Condition 5

71 Condition 5 requires the setback of the addition to the Mary Glowery Building to Napier Street to be increased from 31.9 metres to a minimum of 42 metres.

72 We do not support Council's suggested changes.

73 Various expert witnesses discussed the additional setback. Mr Barrett addressed it from a heritage perspective and in his evidence included a sight line diagram showing that, at a setback of 31.9 metres, only the lightweight netting and a glass balustrade in the activity space would be visible from the opposite side of Napier Street over top of the northern wing of the Mary Glowery Building.

74 Professor Jacques made reference to models he asked to be prepared for setbacks at 31.9 and 42 metres. Based on the models he considers that the greater setback would result in a blander, sheer five storey volume presenting to the east and north. In his opinion the 42 metre setback would dissolve the tripartite massing, with the bridging element becoming a material treatment on a flat façade rather than a void and/or mass that creates a shadow. Mr O'Dwyer considers that the setback at approximately 31 metres assists in maintaining the Napier Street presence and representation of the 1930s northern wing of the Mary Glowery Building.

75 Mr Biles has a different opinion. He suggests that the additional setback will remove a major element of mass, bulk and weight at this most critical interface with mainly residential neighbours. He accepts that with a setback of 31.5 metres it *is an architectural statement of drama which can be*

beneficial in reading the building from a distance and from the experience within the ACU itself, but at close residential quarters it can be, and is overwhelming. He acknowledges that the May 2017 design plans improve the situation, but he still supports Council's additional setback.

76 Whilst we understand Mr Biles' point about the impact at close residential quarters, we are not persuaded that an additional setback is required. The nearest dwelling at 17 Napier Street, on the north side of Little Victoria Street, comprises a largely blank wall along Little Victoria Street with two relatively small windows towards the front nearest Napier Street. There is a small yard at the rear facing Little Napier Street which is largely roofed except for a central section which is open to the sky. The proposed setback at 31.9 metres results in the upper level extension to the Mary Glowery Building being positioned about midway along the open section of yard, whereas Council's variation to 42 metres would result in the extension being positioned in the vicinity of Little Napier Street.

77 The rear yards of the properties to the north of 17, 19 and 21 Napier Street are more open and therefore more exposed to views of the upper level extension. We have undertaken two unaccompanied inspections since the hearing to specifically look at the impact of the proposal, particularly on the nearest properties in Napier Street.

78 Whilst we acknowledge that the new building will be much taller and have much greater visual presence than the existing Mary Glowery Building, we are not persuaded that the extension should be set back further from Napier Street to improve the relationship with the residential properties to the north in Napier Street. We are concerned that the additional setback will result in a very abrupt and bland face to the building and one which will open up views to the highest part of the new building to the south east. We also agree with Professor Jacques that it would dissolve the tripartite massing which we consider is an attractive feature of the proposal.

Conditions 6 and 7

79 Conditions 6 and 7 require the addition to the Mary Glowery Building to be set in from the existing external façade of the heritage building and for deletion of the external structural support columns. Mr Biles supports the modifications shown on the 26 May 2017 design changes. We note that Professor Jacques supported the modifications shown 26 May 2017 design changes. Mr Trethowan did not support the changes shown in response to condition 6 but did support the changes to condition 7.

80 We consider that the latest changes to position the support columns away from the existing fabric of the Mary Glowery Building, rather than abutting the façade has benefits in avoiding structural or other damage to the building as explained to us at the hearing. It also has the benefit of creating a distinct element that does not detract from the heritage fabric and value of the Mary Glowery Building.

81 We therefore support the latest changes, although we accept that it means the new upper levels will visibly extend beyond the façade of the Mary Glowery Building.

Conditions 8 and 12

82 Conditions 8 and 12 are very similar in their intent and require a reduction in height and extent of the two plant areas to not be visible from the street and designed to be integrated into the building. We agree with both these conditions and observe that the 26 May 2017 proposed design changes demonstrate that the plant on both the new building and the upper level extension to the Mary Glowery Building will be integrated into the design of the built form and will not be visible from adjacent streets. We consider that the east-west section on page 9 of the design changes demonstrates that the plant can be successfully integrated into the building.

Condition 9

83 Condition 9 requires that no signage (lit or otherwise) is to be visible from residential areas (including Young Street, Little Victoria, Napier Street, George Street, Little George Street and Gore Street). We agree with Mr Biles that it is more appropriate that advertising signs be subject to a separate permit approval at a later stage in the process.

The student facility and communal space in Young Street (conditions 10 and 11)

84 We start by saying that we support the demolition of the car parking structure and its replacement with a performance and student facility Hub building. The triangular footprint with a base that is slightly skewed off the north-south alignment of Little Napier Street creates an interesting and appealing built form composition. The ground level spaces to the north and south of the building provide outdoor areas to be used by students and staff. The triangular shape of these two spaces, which results from the triangular footprint of the building, opens up the site to the west towards Young Street and to the south towards Little Victoria Street. They provide linkages through the site and between different buildings within the campus.

85 We do not agree with any suggestion by Mr Trethowan or Council that the base of the triangular should be aligned along the western side of Little Napier Street. The building is physically separated from the residential properties to the north and east and when approaching the new building from the north along Little Napier Street, it will be clear that a person is entering a different, non-residential precinct. We consider that the new building and surrounding open spaces will represent a significant and positive improvement to the physical environment that exists at present.

86 Not only will the built form be an improvement, but the activity within the building and the spaces around it will enhance the sense of activation and help overcome what is currently an uninviting and potentially unsafe

environment, particularly along Little Napier Street where the existing car park presents as a 'blank' wall.

Condition 10

87 Condition 10 requires retention of the existing northern wall of the car park where it adjoins 44 Young Street. Whilst we understand and agree with Council's desire to protect the amenity of the dwelling to the north, we do not support such a prescriptive condition. Although Mr Biles suggested that a 7.5 high infill wall should be provided in the gap between the dwelling fronting Young Street and the garage facing Little Napier Street, we consider that a performance based requirement would offer a more flexible but equally satisfactory method of protecting amenity. We have included a modification to the amended Development Plan to achieve this outcome.

Condition 11

88 Although Council's condition 11 seeks that the standard B17 envelope in Clause 55 (ResCode) be applied along the eastern wall adjacent Little Napier Street, we do not agree with either that suggestion, or Mr Biles' alternative suggestion of raking the wall at an angle of 65 to 80 degrees 10 metres above ground level. We are not persuaded that any of these suggested changes are necessary to protect residential amenity of the rear yards of properties on the east side of Little Napier Street. In making these comments we recognise that the existing car park wall has a height of approximately 10 metres and that the new building will have a height of approximately 16 metres. We consider that the skewing of the building away from Little Napier Street, from zero metres at the south end to 4.4 metres at the north end, will serve to lessen visual bulk and is acceptable. We therefore do not support Council's condition 11.

Car parking, traffic arrangements and alternative travel modes (conditions 13, 14, 15, 16 and 17)

89 We confirm our comments made at the conclusion of the hearing that we support the proposal in terms of the number and location of car spaces and bicycle spaces, access arrangements off Napier Street and the modifications required to retain the continuity and functionality of the priority bicycle route adjacent the site. We therefore do not support any of the modifications required by conditions 13, 14 and 16.

90 We note that the ACU has agreed to provide 160 bicycle spaces within the Development Plan area (condition 15) and to prepare a Green Travel Plan examining implementation of more sustainable transport measures (condition 17). We note that the ACU has drafted wording for these two variations and we have included them in our order.

91 Conditions 13 and 14 required a minimum of 50 car spaces to be accessed from the existing Victoria Parade crossover and a minimum of 12 disabled

spaces accessible by staff and students. Condition 16 required retention of the Melbourne Bike Share facility in its current location.

92 Neighbours are concerned about the amenity impact of directing all parking access to and from Napier Street. They also consider that less parking should be provided. We record that from a heritage perspective, Mr Trethowan agreed, in answer to questions, that although both Victoria Parade and Napier Street are sensitive to car access, it is better to provide access off Napier Street. Mr Biles did not comment on any of these conditions.

93 Mr O'Dwyer considers that locating the basement ramp in Napier Street means that car movements do not interfere with the pedestrian movements near the entrance to the building and it provides a 'spacer' or separation that disconnects the new building from the northern wing of the Mary Glowery Building.

94 Mr Hunt was the only expert witness called to give evidence about parking and traffic related issues. Appendix A to the Development Plan includes the *Integrated Transport and Access Plan – Cardno.* This provides a detailed analysis of parking and in part 6.3 has calculated that the proposed provision of 270 car spaces represents a net decrease of 30 spaces over the 300 currently provided by way of the multi-deck car park at 28-42 Young Street and the 50 at-grade spaces provided at 115B Victoria Parade. Cardno has also calculated that compliance with the rates specified in Clause 52.06 of the Planning Scheme would require the provision of 280 spaces.

95 In principle, we agree with the desirability of providing less rather than more parking on the ACU campus. To the extent that we are assessing an amendment to a Development Plan and not a planning permit application, we consider there is scope for the ACU to take a more proactive and leadership role in addressing the growing problem of traffic congestion and the desirability of encouraging travel by other transport modes.

96 In *Ronge v Moreland CC* (Red Dot)[7] the Tribunal made the following observations about car parking and traffic, which are just as relevant to the here as to a residential development in Brunswick:

> *[62] We lastly deal with car parking and traffic, matters commonly raised in objections by neighbours, even when car parking complies with the rates set out in Clause 52.06. Responsible Authorities on the other hand more often accept the advice of their professional staff that on-site car parking is adequate and traffic impacts acceptable, despite not meeting the rates in Clause 52.06.*
>
> *[63] Such is the situation here, where Council's professional staff gave conditional support for the parking and access arrangements of the proposal. We agree with that assessment*

[7] *Ronge v Moreland CC* (Red Dot) [2017] VCAT 550.

> *and support the car parking and access arrangements and are not persuaded that the traffic impacts are unacceptable.*
>
> *[64] We have already referred to what can only be described as the massive increase in Melbourne's population projected through until 2050. Our roads are already congested and will be unimaginably so if a 'business-as-usual' approach is accepted through until 2050. The stark reality is that the way people move around Melbourne will have to radically change, particularly in suburbs so well served by different modes of public transport and where cycling and walking are practical alternatives to car based travel.*
>
> *[65] State and Local planning policies are already acknowledging the change that is required with Plan Melbourne 2017-2050 and State policies referring to 20-minute neighbourhoods. At the municipal level, Moreland has long been recognised as being at the forefront of encouraging less reliance on car based transport. For example, the Moreland Integrated Transport Strategy 2010 includes a key principle that walking and cycling are the preferred modes of transport.*
>
> ...
>
> *[70] Although Ms Dunstan undertook a car parking demand assessment, as called for by Clause 52.06-6 when there is an intention to provide less car parking than that required by Clause 52.06-5, we found the whole discussion around car parking of marginal value given the strong policy imperatives about relying less on motor vehicles and more on public transport, walking and cycling. Census data from 2011 or 2016 is simply a snapshot in time, a base point, but we are not persuaded that such data should be given much weight in determining what number of car spaces should be provided in future, for dwellings with different bedroom numbers. Policy tells us the future must be different. We consider that oversupplying parking, whether or not to comply with Clause 52.06, has the real potential to undermine the encouragement being given to reduce car based travel in favour of public transport, walking and cycling.*

97 In considering whether car parking should be less than that proposed, it is important to recall the previously cited State planning policy at Clause 19.02-1 which seeks *to locate secondary and tertiary education facilities in designated education precincts which are highly accessible by public transport*. Policy 1.1.4 in the Metropolitan Strategy recognises that the growth of these precincts will need to focus on improving access, particularly via public transport.

98 In our opinion, the combined message of these higher level policies is that one of the reasons the ACU campus has been recognised of state significance is its high accessibility to public transport. We are therefore

reluctant to agree to an amended Development Plan which so specifically refers to the provision of 270 car spaces.

99 We again emphasise this is not a planning permit application where we would be constrained from mandating fewer car spaces than that proposed, or that did not meet the rates in Clause 52.06 or in a Parking Overlay. Instead a Development Plan sets the broad parameters for any later planning permit application and it provides an opportunity for the exact number of car spaces to be revisited at the later planning permit stage. We therefore consider that the Development Plan should be reworded to refer to a maximum of 270 car spaces, and specifically record that the number will be subject to further assessment when an application is made for a planning permit.

100 Although we understand why Council is seeking to have a minimum of 12 disabled spaces to carry forward a requirement in a separate permit (PL09/0533), we consider that the number (and location) of disabled spaces should be determined at the permit application stage rather than set out as a definite requirement under the Development Plan.

101 It follows from the above discussion that if the number of on-site spaces is reduced, then the impacts on the road network will be less. However, for the purposes of assessing Council's conditions 13, 14 and 16 and the desire to avoid vehicular access to Napier Street, we have made our assessment on the basis of the proposals contained in the Integrated Transport Access Plan and the evidence of Mr Hunt.

102 We consider that all vehicular access should be via Napier Street. We understand that Council's engineering department did not object to the access in Napier Street, although a number of design suggestions were made to minimise the risk of vehicles utilising the northern end of Napier Street.

103 We reject any suggestion that the car park access to 50 basement spaces should be to Victoria Parade. We agree with Mr O'Dwyer that it is preferable to remove all cars from the main pedestrian entrance. Not only does it improve the appearance of the forecourt and entrance of the building by avoiding crossovers and a basement ramp, but it perhaps most importantly removes a potential major point of conflict between vehicles and pedestrians. That potential conflict is exacerbated because of the amount of pedestrian traffic along Victoria Parade.

104 We also do not understand the submission that providing vehicular access from Victoria Parade will avoid cars crossing the bicycle path. That is simply not correct. We have inspected the existing arrangements very closely and both the eastern and western bicycle lanes crossing Victoria Parade occur west of the existing vehicle entrance to the at-grade car park. As a consequence all bicycle traffic has to cross over the existing vehicular entrance. Whilst we acknowledge that the traffic volumes from the basement will be much greater than with the existing car park, we are not

persuaded that the new arrangement is so dangerous or so inconvenient that it should be rejected in favour of retaining access from Victoria Parade.

105 VicRoads access management policies strongly seek to avoid access to a main road when alternative access is available from a lower order road, including in many instances, rear laneways. Given the traffic volumes along Victoria Parade and the location of a bus lane along Victoria Parade outside the site, we consider that retaining vehicular access from Victoria Parade is less desirable than providing well managed access along the southern end of Napier Street.

106 The Functional Layout Plan (Revision 4) attached as Appendix A to the Integrated Transport Access Plan demonstrates to our satisfaction that traffic arriving and leaving the basement car park can be directed to and from Victoria Parade without utilising the northern end of Napier Street. In supporting the access arrangements, we acknowledge that it can never be ruled out that some motorists will try and access the site from north in Napier Street. Detailed design of the traffic islands opposite the entrance can largely eliminate traffic entering from the north, but anyone who has viewed reality car crash programs, would understand that illegal behaviour can never be avoided.

107 Concerns were also raised about the loss of on-street parking spaces along the frontage of the site in Napier Street and the impact on the bicycle path and Melbourne Bike share facility. We do not share those concerns. The relocated bicycle path does require removal of nine on-street car spaces and the relocation of the Melbourne Bike Share facility to a position in front of the Mary Glowery Building in Victoria Parade.

108 We observe that the bicycle lane crosses Victoria Parade about midway along the frontage between Napier and Young Streets. The relocated Melbourne Bike Share facility will be located west of where the bicycle path crosses Victoria Parade and in that sense is not on or beside the bicycle path. However, because of the removal of the glass façade and ramp at the front of the western wing of the Mary Glowery Building the footpath is widened so there is more room to position the Melbourne Bike Share Facility without completely blocking the footpath. It will be adjacent to bus stops in Victoria Parade and in a visually prominent position rather than around the corner in a side street.

109 For those reasons, we would support relocation even if it wasn't required to allow access to the basement.

Other changes including landscaping, student and staff numbers (conditions 18, 19 and 20)

Condition 18

110 We note that the ACU supports condition 18 requiring additional information for lighting of the Hub landscaping area and we have included that variation in our order.

Condition 19

111 Condition 19 requires accurate information about the total number of students and staff who will occupy the whole Development Plan area. ACU says that it is inappropriate and impractical because student and staff are continually moving the campus. We note that Clause 3.0 of the DPO2 requires that a Development Plan must show a description of the proposed use including the proposed hours of operation (has been done), full-time and part-time student numbers and staff numbers.

112 We record that the amended Development Plan already includes existing and projected student and staff numbers in section 1.5 and that these figures have been used in the Integrated Transport and Access Plan at Appendix A. These project 10,700 students (EFT) and 1,200 staff by 2020 for the whole campus. We accept that students and staff move around the campus in a fluid way and that not all of them are within the area affected by DPO2. We accept that it is not possible to be more prescriptive about the numbers within different parts of the campus and that a total number is just as useful in terms of gauging the intensity of student and staff activity across the whole campus.

Condition 20

113 Condition 20 concerns consequential changes and does not require comment other than that such changes will need to be made.

Off-site amenity impacts caused by noise to residential properties

114 Concerns were raised about noise from the upper level sports facilities and a suggestion that we require them to close at 9pm, increase the sound barrier wall and require continuous acoustic testing. We understand the concerns but as with other aspects of the amended Development Plan, these suggestions involve a level of detail which would be more appropriately addressed as part of any future planning permit application.

CONCLUSION

115 We agree that the amended Development Plan dated October 2016 should be approved, but with variations and additions to address shortcomings or changes we have discussed earlier in our reasons. We acknowledge that Council has agreed to support the amended Development Plan subject to 20 conditions/variations.

116 We also acknowledge that the South Fitzroy Protection Group Inc. supports Council's position, although it is evident from the submissions made at the hearing that residents are concerned at the way in which the ACU is adversely impacting on their neighbourhood and on their lives. Those impacts, such as students smoking in local streets and laneways, along with littering from cigarette butts and coffee cups, are beyond the scope of our considerations on the planning merits, or otherwise of the amendments proposed to the approved Development Plan. However, they serve to highlight the conflicts that can, and do, arise when institutions are located in and adjacent to residential neighbourhoods.

117 Our consideration of the amended Development Plan and the variations asked for by Council, has required us to balance the very strong policy support for reinforcing a health and education precinct of state significance against policy that requires a response to local context, which in this instance includes the lower scale residential neighbourhood to the north, north east and east.

118 We have also taken into consideration the changes introduced by Amendment VC139, including objectives in the Urban Design Guidelines that seek to ensure that large development sites maintain the amenity of adjacent residential areas and to create a transition from large development sites adjacent residential neighbourhoods using scale, built form and mass.

119 Although planning policies and other documents frequently ask for high quality urban design and architecture (sometimes even excellence or exemplary design), Clause 65 of the Planning Scheme seeks an acceptable outcome, not an ideal or perfect outcome. In addition, Clause 10.04 of the Planning Scheme requires us to:

> …endeavour to integrate the range of policies relevant to the issues to be determined and balance conflicting objectives in favour of net community benefit and sustainable development for the benefit of present and future generations.

120 We acknowledge that the changes we have approved to the existing Development Plan will bring result in both positive and negative outcomes and that there will be different opinions about what those outcomes may be.

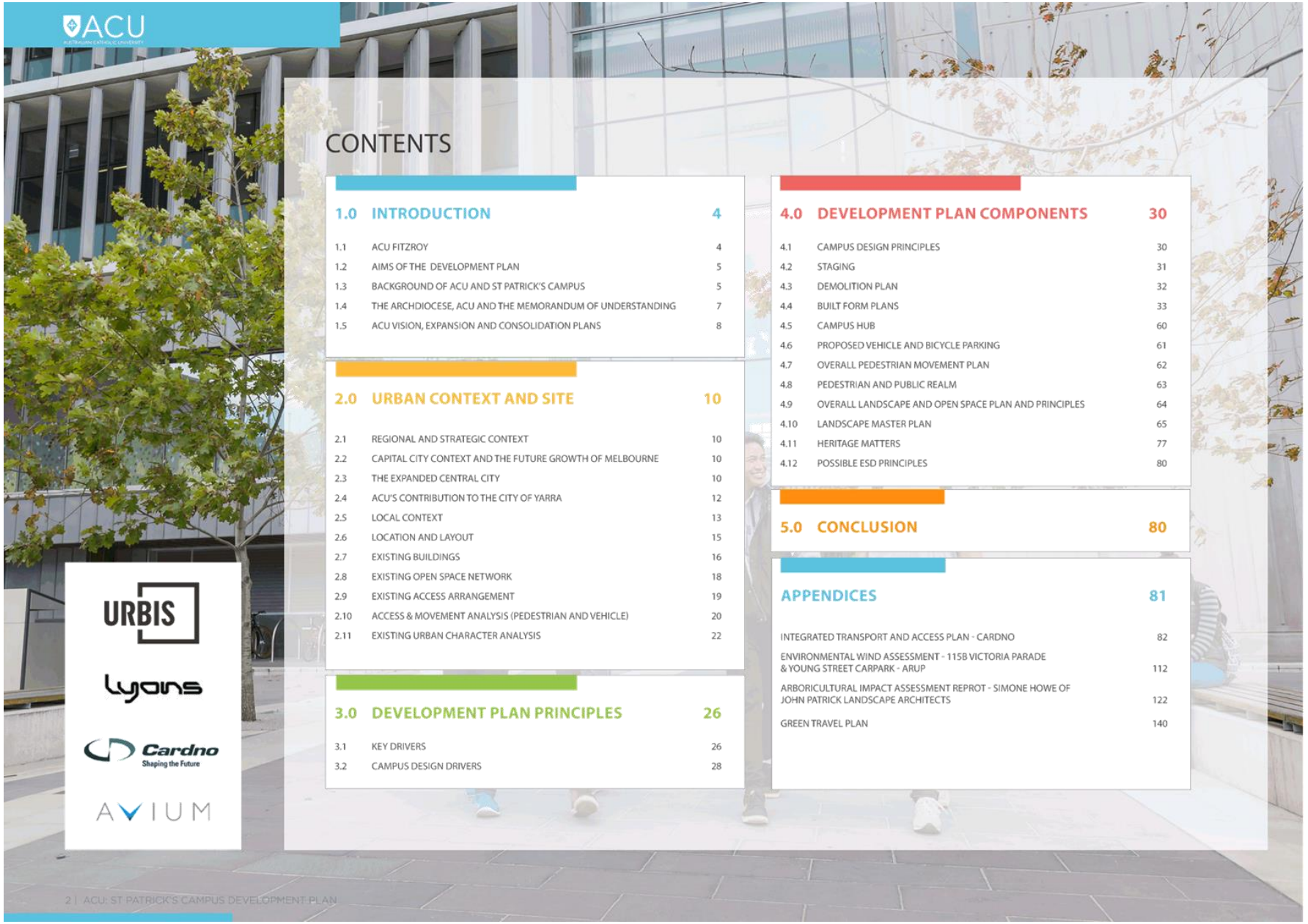
121 We consider that, on balance, there is a net community benefit in approving the amended Development Plan subject to the changes shown on Description of Proposed Design Changes (dated 26 May 2017) together with the other modifications referred to in our order.

J A Bennett
Presiding Senior Member

Alison Slattery
Member

Attachment 2 - PLN16/0085 - 115 Victoria Parade & 28 - 42 Young Street Fitzroy - ACU Development Plan Part 1

Attachment 2 - PLN16/0085 - 115 Victoria Parade & 28 - 42 Young Street Fitzroy - ACU Development Plan Part 1

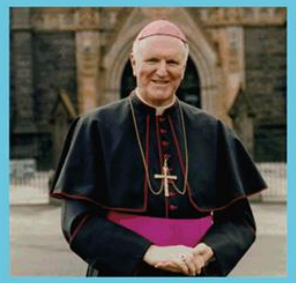


Attachment 2 - PLN16/0085 - 115 Victoria Parade & 28 - 42 Young Street Fitzroy - ACU Development Plan Part 1

ACU

AUSTRALIAN CATHOLIC UNIVERSITY

TESTIMONIALS



Message from the Archbishop Denis Hart

It is with great pleasure that I introduce the ACU Development Plan.
It is my privilege as Archbishop of Melbourne and President of ACU Limited to have the opportunity to reflect on what has been achieved by the Vice-Chancellor Professor Greg Craven, and the University staff under the wise guidance of the Chancellors General Sir Peter Cosgrove AK, MC, and the Hon. John Fahey AC with the support and encouragement of the Archdiocese of Melbourne at the Australian Catholic University's St Patrick Campus.

It is a particular source of pride for me with St Patrick's Campus located nearby to St Patrick's Cathedral and the administration of the Archdiocese to observe its development and witness the active engagement of the University with the Church and community in Melbourne. I welcome the opportunity for your comments and input to the further development of the Campus to further enhance this growing educational environment in Fitzroy.

Integral, as indeed primary for this whole activity, is the work done for the welfare of the students. We hope to provide our students with areas which will open out and provide the space for recreation, for the important dialogues which are part of University life, for friendship and for mutual understanding. The further expansion of the Melbourne Campus of the University I hope, will be a long-standing inspiration to the young people who come here, to look at the possibilities that we can all do for other people and to carry the work of the University further into the community, so that the contribution of all made here will be something which is remarkably authentic, human, guided by the respect for the dignity of the human person with the unique gifts and abilities of each young person placed at the service of those around them.

- Archbishop Denis Hart

Message from the Vice-Chancellor Professor Greg Craven

It is with great pleasure that I introduce the ACU Development Plan
Australian Catholic University (ACU) has developed and grown substantially over the last five years and its strategic positioning has now entered a new phase. ACU has transitioned from a University striving towards ambitious targets to one that is comprehensively embedding excellence.

The University is ambitious in achieving its goals and vision whilst at the same time is clear about its Catholic Identity and Mission. The University continues a legacy that spans over 2,000 years of the Catholic intellectual tradition's engagement in faith and reason. The ACU Development Plan will ensure we continue to work together towards the shared goals of academic excellence and as a provider of innovative and quality higher education courses; delivered by leading academics, to meet the needs of a diverse student body in conjunction with our partners and the community.

I look forward to working with the ACU community and our stakeholders to achieve these goals through the next phase of the University's development.

- Professor Greg Craven - Vice-Chancellor

1.0 INTRODUCTION

1.1 ACU Fitzroy

This Development Plan has been prepared to guide future planning permit applications for development at St Patrick's Campus of the Australian Catholic University (ACU) in Fitzroy.

The main campus is located in the block bounded by Brunswick Street, Napier Street, Victoria Parade and Gertrude Street. The properties owned by ACU within this block are shown on the aerial image.

This Development Plan applies to the ACU land within the Development Plan Overlay (DPO2) in the Yarra Planning Scheme.

Other areas of the campus will be subject to a separate planning approvals process.

1.2 Aims of the Development Plan

ACU's Development Plan is to deliver a landmark mixed use development at the St Patrick's Campus in Fitzroy, Melbourne to accommodate the planned growth in students, teachers and research activities, improve the quality of teaching and learning environments and improve the pedestrian and public environment for students and the wider community.

The existing Australian Catholic University Development Plan was approved over 16 years ago (in 1999) and the works proposed under this Development Plan have since been completed.

This Development Plan replaces the 1999 Development with a contemporary document that formalises the strategic direction of the campus's next phase of development over the coming decade.

1.3 Background of ACU and St Patrick's Campus

The Melbourne Campus at Fitzroy, incorporating Cathedral Hall, was established in 2000 after the ACU's existing two Melbourne campuses (Mercy and Christ) were formally amalgamated. Mercy and Christ had both been colleges of the Victorian Institute of Catholic Education between 1975 and 1990.

At the heart of the Fitzroy neighbourhood is the historic Cathedral Hall, at 20 Brunswick Street, near the Victoria Parade corner. Connected to the Hall is the former Exhibition Boot Factory which, having provided the Hall with a foyer, cloakroom, ticket office and club rooms, was refurbished in the early 2000s.

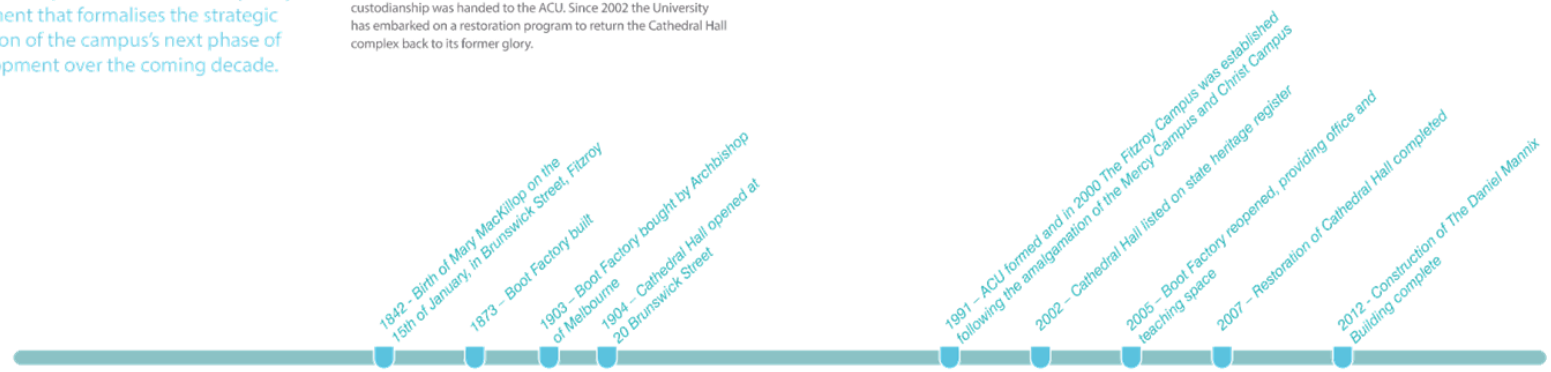
Built in 1873, the Boot Factory turned out footwear for the Exhibition Boot Company for nearly 30 years before it was bought by the then Archbishop of Melbourne, Dr Thomas Joseph Carr in 1903.

Archbishop Carr set about refurbishing the Boot Factory to provide the Catholic community with its own class, club and meeting rooms. The facility was opened in 1903 when Archbishop Carr also bought two adjacent properties and it is here where Cathedral Hall was built and opened in 1904, followed by a supper room, opened in 1908. Since then, this complex of buildings has served the local Catholic and wider community as the venue for congresses, concerts, lectures, meetings and socials, and provided nearby Catholic schools with an assembly hall.

Cathedral Hall was showing real signs of wear and tear when custodianship was handed to the ACU. Since 2002 the University has embarked on a restoration program to return the Cathedral Hall complex back to its former glory.

The Supper Room was the first part of the Cathedral Hall complex to be restored, with a contribution to the project from the National Institute of Youth Performing Arts. A fully restored and renovated Boot Factory, providing office and teaching space, was reopened in November 2005. In October 2007, following its restoration, Cathedral Hall was reopened. This was made possible through generous contributions from the Ian Potter Foundation and the Archdiocese of Melbourne. The Hall now provides space for lectures, with music and drama students using the Supper Room. The complex provides a focal point for the intellectual and cultural life of the University.

St Mary of the Cross, Australian Catholic University, St Patrick's Campus, Fitzroy



Attachment 2 - PLN16/0085 - 115 Victoria Parade & 28 - 42 Young Street Fitzroy - ACU Development Plan Part 1

ACU
AUSTRALIAN CATHOLIC UNIVERSITY

1.3 Background of ACU and St Patrick's Campus

The University chooses to focus on areas of higher education and research that are closely connected with its particular character as a University that is Catholic, public and national. The focus areas are Education and Arts, Health Sciences, Law and Business and Theology and Philosophy.

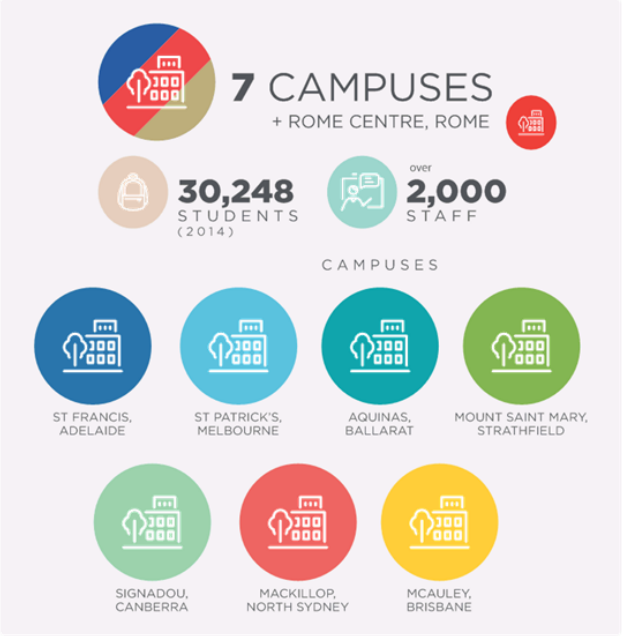
The University has the following Research Institutes, several of these have substantial teams based in Melbourne:

- Institute for Religious and Critical Inquiry (Melbourne)
- Learning Sciences Institute Australia (Melbourne)
- Mary MacKillop Institute for Health Research (Melbourne)
- Institute for Positive Psychology and Education
- Institute for Social Justice
- Institute for Health and Ageing (Melbourne)
- Institute for Religion, Politics, and Society (Melbourne)

As a Catholic University, the ACU is grounded in the Catholic intellectual tradition, affirming the value of rational inquiry and the human search for meaning, the combination of faith and reason. Knowledge is cultivated in an environment that fosters intellectual freedom, personal development and equity for all. The ACU's Catholic identity is animated through a commitment to serving others, developing an active learning community and fostering a just society.

ACU has a long tradition of nurturing the intellectual and spiritual life of students and staff. This is grounded in a commitment to caring for the whole person – mind, body and spirit, to support staff and students integrate learning, work, faith and community engagement.

ACU's Institute for Advancing Community Engagement (IACE), facilitates University-community partnerships that respects the wellbeing and dignity of people and through collaboration, seeks to enhance wellbeing of individuals and their communities locally, nationally and internationally. Australian Catholic University's (ACU) community engagement capacity has grown through its strategic partnerships with more than 170 organisations, including Catholic and other faith-based community groups and schools, as well as professions and corporations. Respect for each person, appreciation of the sacred in life and commitment to the common good are core values within the ACU's community engagement, which draws upon the capacities and scholarship of staff and students in its collaboration with people and communities here and overseas.



FACULTIES, INSTITUTES AND CENTRES

Australian Catholic University (ACU) has a wide range of institutes and centres as well as four faculties:

Education and Arts – areas include education, arts and humanities, global studies and international development studies, media communications, social science, youth work, and creative arts, visual arts and design.

Health Sciences – areas include counselling, environmental science, exercise physiology, exercise science, mental health, midwifery, nursing, occupational therapy, paramedicine, physiotherapy, public health, psychology, social work and speech pathology.

Law and Business – areas include commerce, business administration, accounting and finance, information technology, human resource management, law, marketing, supply chain management, occupational health and safety, and sustainability.

Theology and Philosophy – both these disciplines encourage students to search for wisdom, knowledge and truth.

Attachment 3 - PLN16/0085 - Public Notification ACU Development Plan Part 2

ACU

1.5 ACU Vision, Expansion and Consolidation Plans

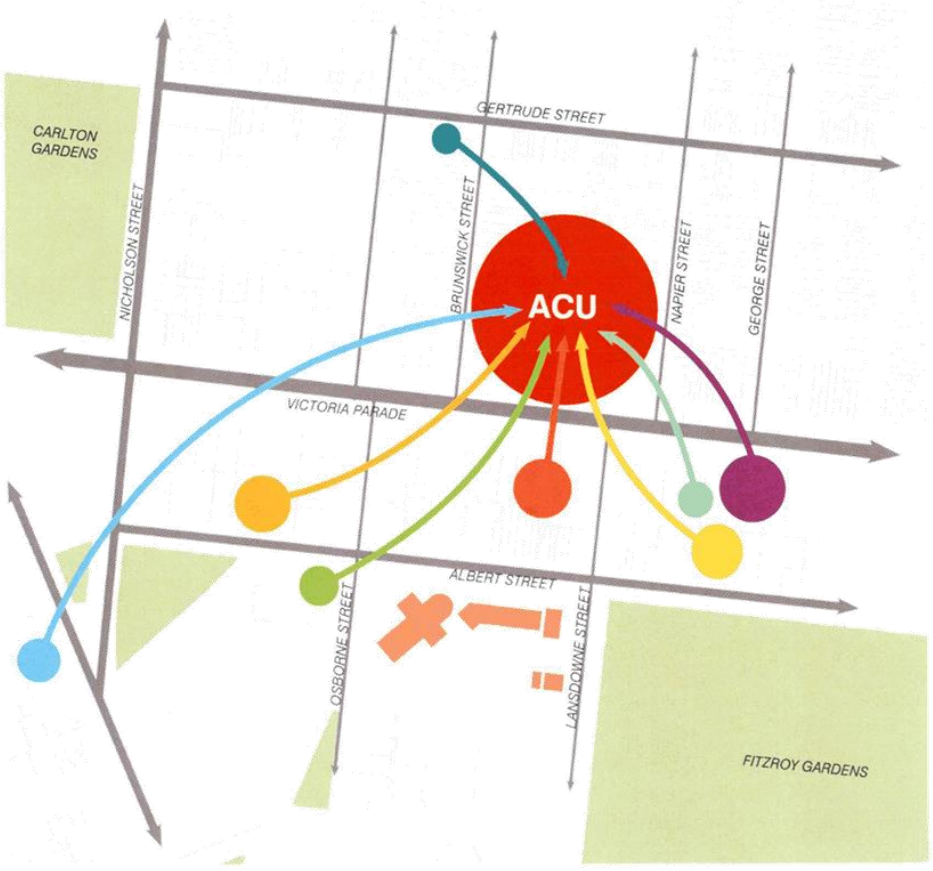
The ACU provides a diverse range of subjects including Accounting and Finance, Commerce, Business, Marketing, IT, Law, Arts, Education and Teaching, Theology, Exercise Science and Allied Health, Philosophy, Paramedicine, Psychology, Nursing, Midwifery and Science. 10,000 students and approximately 800 staff attend the campus throughout the academic year. Since the ACU was formed on 1 January 1991, it has seen continued growth in the demand for its tertiary education student placements.

St Patrick's Campus will need to grow to accommodate approximately 12,900 students and 1,200 staff by 2020. The growth of the research functions will also continue in response to emerging opportunities.

The Development Plan has been developed to accommodate the ACU's required growth in students, teaching staff and research from 2015 to 2025. It has been developed to enable the ACU to further consolidate its teaching and research activities onto the Campus.

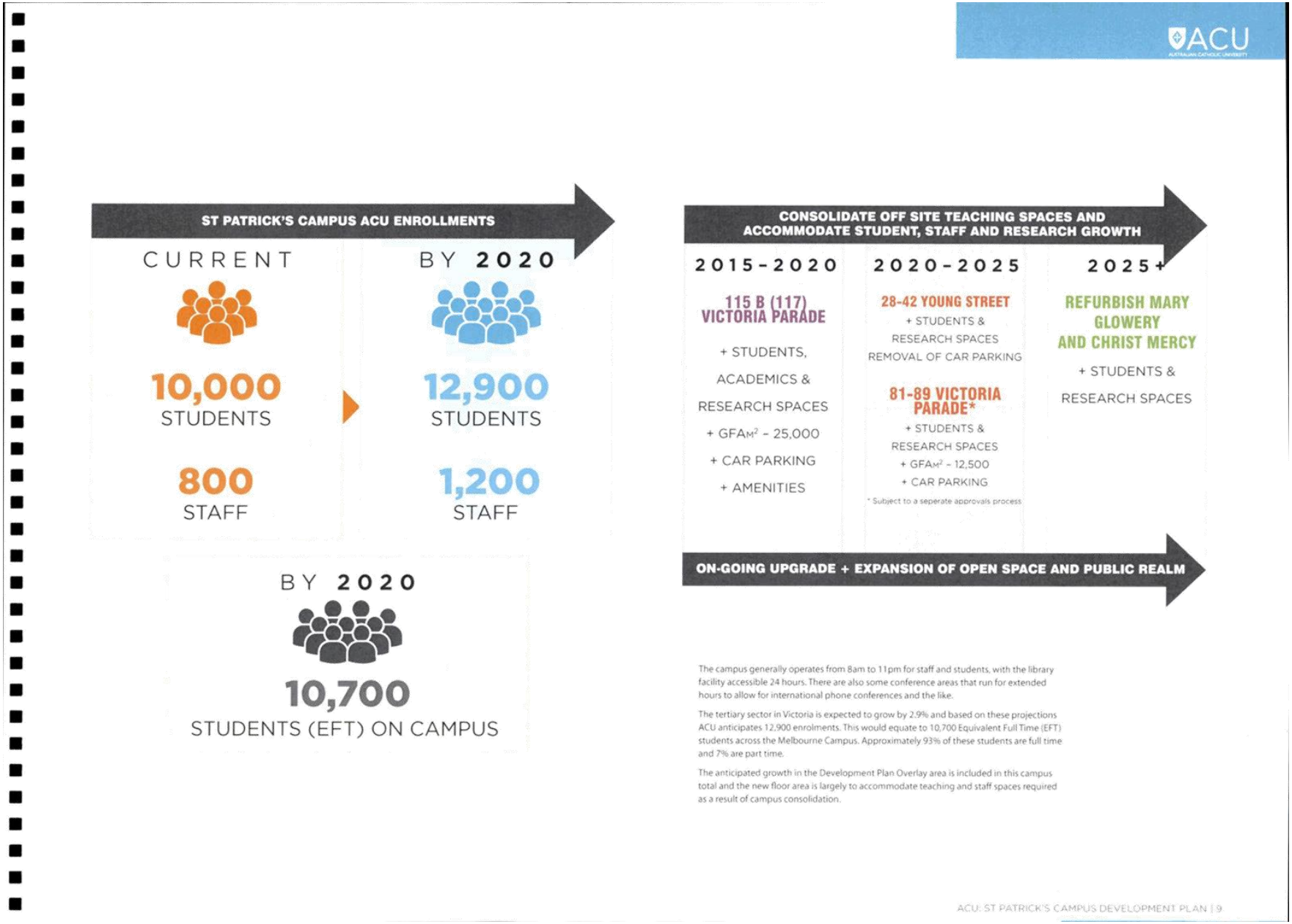
To enable the expected growth to be accommodated and campus consolidation objectives to be achieved, additional floor area will be required on the main campus.

The majority of new floor area will be located at the Victoria Parade frontage of the site which is the more robust interface, away from residential properties further north. New buildings will be constructed at 115B Victoria Parade (corner of Napier Street) and 81-89 Victoria Parade (corner of Brunswick Street) – not part of the Development Plan Overlay. The Mary Glowrey building will also be refurbished and extended.



Intended campus consolidation

Attachment 3 - PLN16/0085 - Public Notification ACU Development Plan Part 2



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2.0 URBAN CONTEXT AND SITE

2.1 Regional and Strategic Context

Within the wider strategic document, Plan Melbourne, it is recognised that Melbourne is home to a range of world ranked universities and that the knowledge based sector is fundamental to the city's ability to compete within the global economy and provide a quality skills base for future growth and prosperity.

2.2 Capital City Context and the Future Growth of Melbourne

Population growth and the changing economy are increasing the importance of cities and changing the way they function. Cities will continue to grow and shoulder a higher proportion of population growth. As cradles of innovation and creativity they are the major wealth generators and centres of opportunity, attracting people, business and investment from around the world.

Melbourne is growing rapidly, with a projected population of up to 7.7 million by 2051. Over the same period an additional 1.7 million jobs are forecast. A large percentage of this growth is to be accommodated in the expanded central city.

The 'Knowledge Economy' has been identified as a crucial economic development goal for Australian cities and specifically, Melbourne. Knowledge based industries typically cluster in defined locations, indicating there will be continued demand for significant growth in the central city, including the internationally recognised hospital, health and medical research precincts on Victoria Parade. In June 2014, the Education and Training and Health Care industries contributed $19 billion (5.5%) and $24 billion (6.8%) per annum, respectively, to Victoria's Gross State Product.

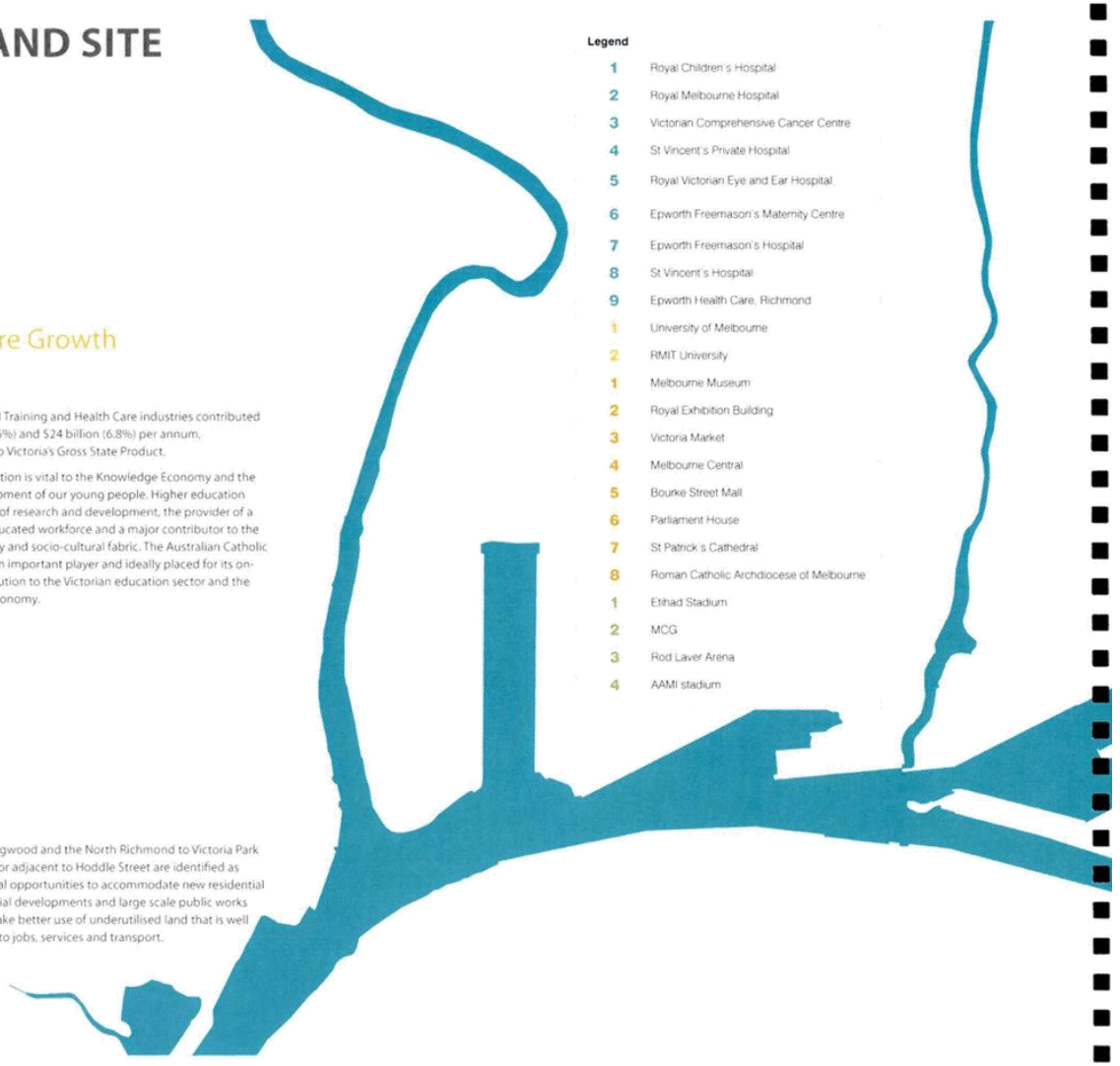
Tertiary education is vital to the Knowledge Economy and the future development of our young people. Higher education is a key driver of research and development, the provider of a skilled and educated workforce and a major contributor to the city's liveability and socio-cultural fabric. The Australian Catholic University is an important player and ideally placed for its on-going contribution to the Victorian education sector and the knowledge economy.

2.3 The Expanded Central City

The City of Yarra is part of the expanded central city. The expanded central city is forecast to become Australia's largest business centre with an additional 260,000 - 320,000 jobs to create a total of 1 million jobs by 2031.

The Fitzroy/East Melbourne Health and Education precinct is a significant State cluster of health and education services. It includes the ACU, St Vincent's Pubic and Private Hospitals, The Eye and Ear Hospital, Epworth Freemason's Hospital and Medical Centre on Victoria Parade, Epworth Freemasons Hospital on Albert Street and the St Vincent's Private Hospital on Grey Street.

Nearby, Collingwood and the North Richmond to Victoria Park Station corridor adjacent to Hoddle Street are identified as Urban Renewal opportunities to accommodate new residential and commercial developments and large scale public works projects to make better use of underutilised land that is well located close to jobs, services and transport.



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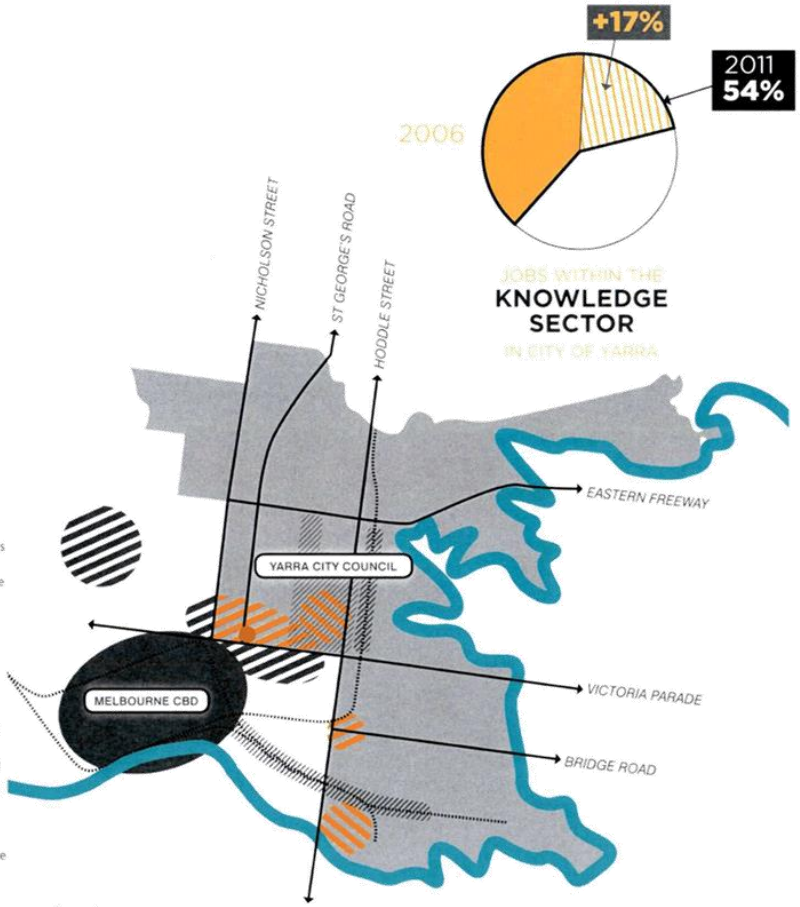
2.4 ACU'S Contribution To The City Of Yarra

The City of Yarra's Economic Strategy 2015-2020 (the Strategy) reinforces the importance of the knowledge economy to the City. In 2011, 30,950 or 46% of workers in the City of Yarra were employed in the Knowledge Sector. Between 2006 and 2011, knowledge sector jobs grew by 17%, or 5,279 jobs, representing 54% of employment growth across all industries over this period.

The Strategy identifies the importance of the 'smart city' concept to the local economy. The 'smart city' concept recognises and supports the growth of urban areas where the key drivers of innovation, productivity and growth in the knowledge economy occur. Typically they are areas where research institutions, medical complexes, universities and clusters of high-tech and creative firms are located and attract the co-location of other businesses, entrepreneurs and professionals. Mapping businesses in the City of Yarra illustrates a number of distinct geographical clusters where businesses and institutions have clustered to provide economic advantage from collocation and collaboration. These include the Fitzroy Health and Education Cluster and the nearby Collingwood Creative Cluster.

In 2011, the Fitzroy Health and Education Cluster featured the greatest concentration of employment in the municipality with over 5000 employees. The area functions as a significant research and medical education cluster. Melbourne's growing and ageing population is expected to drive increased demand for these services in the future. Consultation with St Vincent's by the City of Yarra has indicated that major growth is required over the next 5 years. It is anticipated there will be significant flow on effects to surrounding businesses and opportunities for co-location of other activities, particularly in the education and research sectors. The City of Yarra recognises that these activities have the potential to dramatically influence the future urban and economic growth of the municipality.



Yarra City Council Clusters

In recognition of these important drivers of change, the ACU and City of Yarra have entered into a Memorandum of Understanding (MOU) which identifies areas for collaborative work. This includes:

Place Making and Urban Design

- collaborate on placemaking initiatives in the Education Precinct by ensuring that preliminary consultations between Council and the ACU take place at the conceptual stage concerning the ACU capital works program
- ensure that there are best outcomes for the broader community through the design of urban spaces and the interface between the ACU and Council land
- ensure that the integrity of the site's heritage features are maintained.

Education and Professional Development

- provide opportunities for the training and professional development of the employees of the Council by the ACU
- provide opportunities for relevant employees of the Council to assist as guest lecturers/tutors at the ACU
- collaborate in relation to research and learning opportunities that might arise from time to time, including research into relevant portfolio area.

Health and Wellbeing

- improve the health and well-being of the Yarra community through creating a stronger relationship between the ACU's public health expertise and the work of Council.

Economic Development

- work together to expand the benefit of the ACU's activities to a wider local community.

Research

- develop cooperative research projects across a range of portfolio areas which harness the strong research expertise of the ACU in a way that assists Council to improve its evidence base and advocate for better social outcomes.

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2.5 Local Context

The ACU is strategically located on Victoria Parade, one of the Melbourne's most important entrance boulevards to the CBD, alongside St Kilda Road and Royal Parade. Melbourne's entrance Boulevards are very distinctive features of the city's urban fabric. Characterised by a number of large institutional and commercial buildings, Victoria Parade is a very wide, generous, tree lined space where relatively tall buildings are both suitable and can be accommodated given the width of the road, the size of many of the lots and the dominance of the Boulevard's tree canopy.

The Campus sits at the north east entrance of the Melbourne CBD and the northern entrance to the Brunswick Street Activity Centre. The local area includes the Gertrude Street shopping precinct and the Smith Street Activity Centre.

The ACU is in walking distance or a 5 minute tram ride to the CBD and Parliament Station. It sits within the expanded Central City zone which will continue to grow to accommodate health, education and knowledge intensive activities. A number of prominent landmarks can be seen from or are in walking access from the campus including, St Patrick's Cathedral, the Carlton Gardens, Royal Exhibition Buildings, the Melbourne Museum, Parliament House and the Fitzroy Gardens.

Victoria Parade is the central spine of the Fitzroy/East Melbourne Health and Education Precinct. It accommodates a cluster of capital city health and knowledge economy activities including the Epworth Freemason's Hospital, the Eye and Ear Hospital and St Vincents' Public and Private Hospitals and the Catholic Theological College. There are also a number of large tenancies along the Boulevard accommodating a mixed use of medical, government and business services.

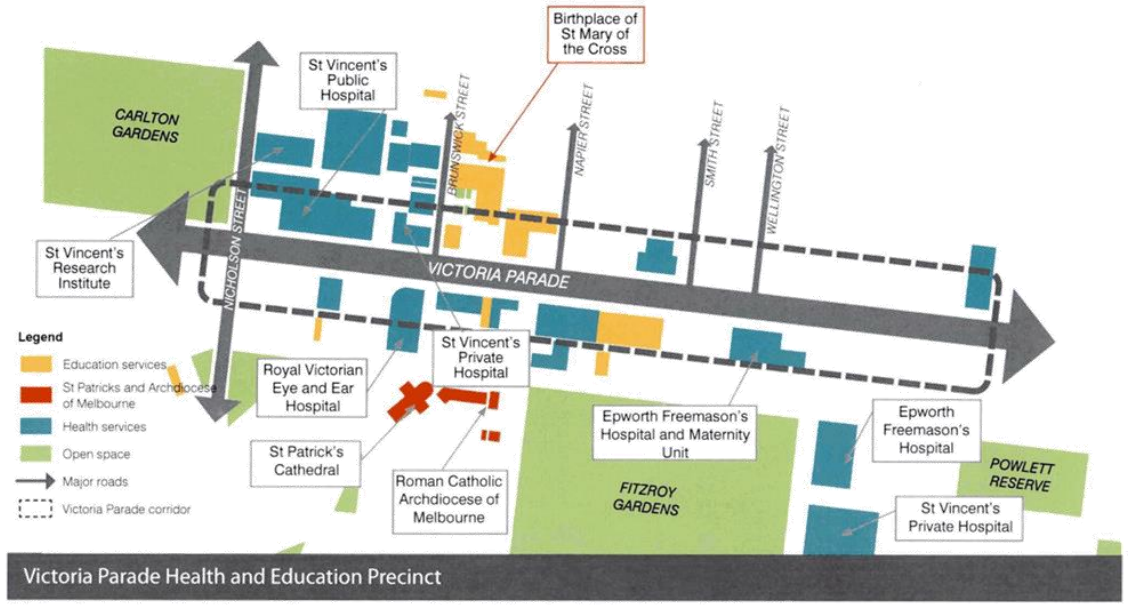
The geographic location of the campus provides a landmark opportunity to signify both the location of the St Patrick's Campus and a key component of the health and education cluster located along the length of Victoria Parade with higher buildings clustered at the western end of Victoria Parade including the Eye and Ear Hospital and St Vincents' Public and Private Hospitals.

Victoria Parade contains a number of sites that can accommodate significant growth, including the ACU St Patrick's Campus. The ACU Campus is ideally located to accommodate growth given:

- the relatively large lot size
- the sites location on a major civic boulevard which provides the opportunity to accommodate greater scale and height similar to St Kilda Road
- the scale and orientation of the ACU Campus which is to the south of the fine grain residential precincts in Fitzroy, meaning any overshadowing from high scale development will be minimal
- the size of the Campus provides the opportunity for higher scale development to be located on the Victoria Parade frontage with height transitioning down to the north to assist in minimising the impact of bulk on the sensitive residential uses to the north of the Campus

As identified in the Yarra City Council Economic Strategy 2015- 2020, the Australian Catholic University and St Vincent's Hospital's need to accommodate major growth over the next 5 years given increased demand for Education, Medical and Research activities and to capitalise on the productivity and job growth which will drive development investment within the education and health precincts in both the City of Melbourne and the City of Yarra. Growth of jobs in these sectors is of State importance.

To the north, east and west of the site, the Campus abuts a mix of development types including larger lots accommodating St Vincent's Hospital and a mix of one, two and three storey finer grain terraces and warehouse conversions. To the north of Little Victoria Street, Napier Street comprises primarily one and two storey heritage terraces with future development managed by the South Fitzroy Heritage Overlay. The interface to the south and east of the Campus is particularly sensitive given the intact nature of much of the Napier Street heritage properties.



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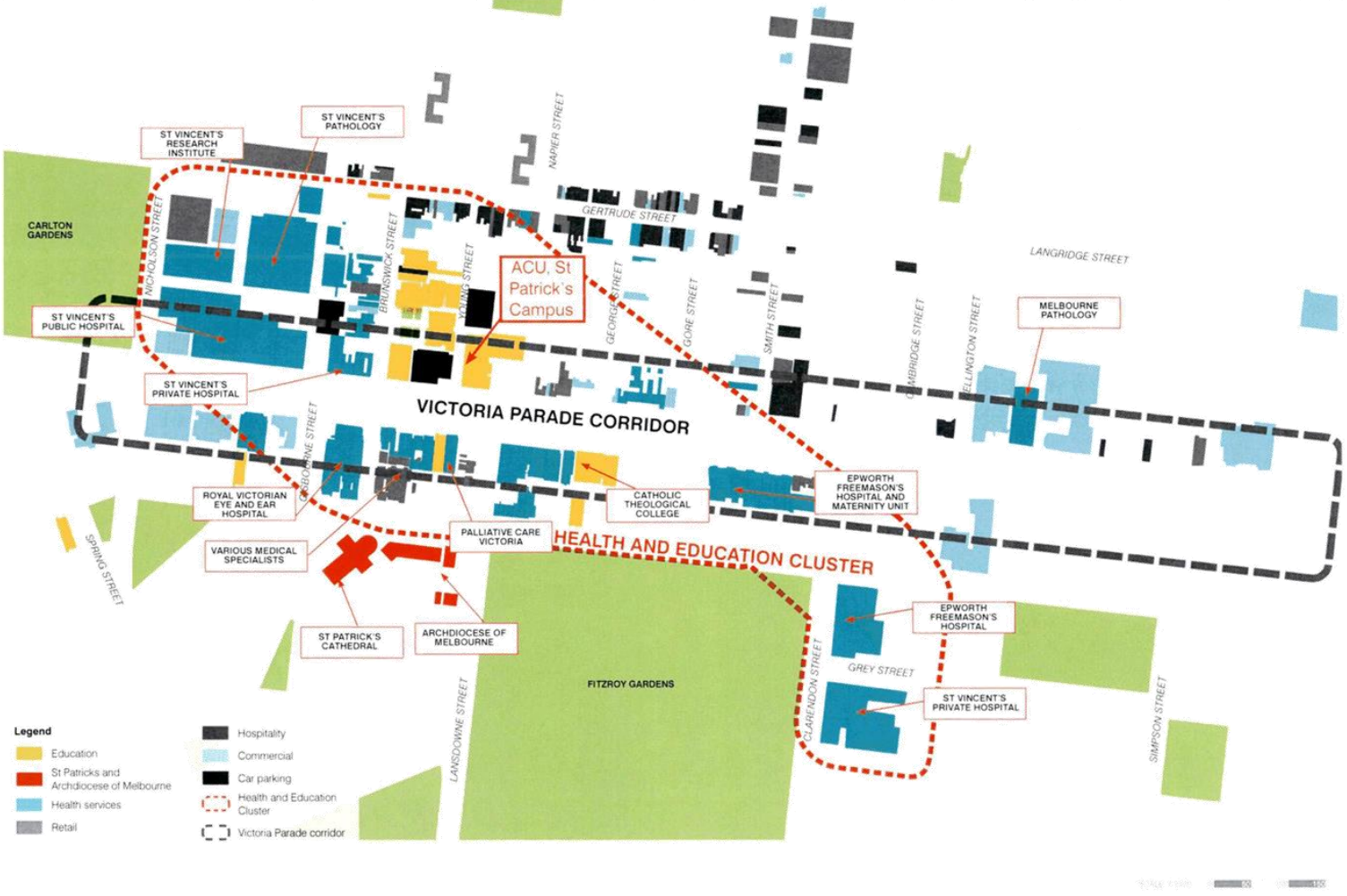
ACU

FIGURE 1.1. Local Context



14 | ACU ST PATRICK'S CAMPUS DEVELOPMENT PLAN

Attachment 3 - PLN16/0085 - Public Notification ACU Development Plan Part 2

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2.6 Location and Layout

The ACU St Patrick's Campus comprises a number of historic and contemporary buildings, the Campus has the following buildings and facilities which are mapped on the adjacent plan, noting that the car park (No. 6) is not part of the ACU campus:

1. The Mary Glowery Building (115 Victoria Parade): Main teaching space, reception, library, student facilities, the Office of Student Success, specialist laboratories for Speech Pathology, Biology, Occupational Therapy, Media Labs, gymnasium and Moot Court (5 storeys)
2. The Daniel Mannix Building: Philippa Brazill rsm Lecture Theatre, rooftop garden, St. Mary of the Cross Chapel, Nursing, Paramedics, Exercise Science, Midwifery, Occupational Therapy and Psychology, specialist laboratories. (7 storeys + 2 plant)
3. Cathedral Hall: Auditorium and tutorial rooms (3 storeys)
4. Modular Buildings (81-89 Victoria Parade) (2 storeys): Tutorial rooms
5. Young Street multi-level carpark (4 storeys)
6. Victoria Parade multi-level carpark
7. Christ Lecture Theatre
8. Mercy Lecture Theatre
9. Drake House
10. Visual Arts Building
11. Early Childhood Centre
12. Recital Room
13. Mac Labs
14. St Mary of the Cross Square
15. 3 Graham Street
16. 32 Brunswick Street

Campus Plan

Attachment 3 - PLN16/0085 - Public Notification ACU Development Plan Part 2

ACU

2.7 Existing Buildings

The main buildings on the campus that are subject of redevelopment under this Development Plan are:

- 115B Victoria which is currently used as an at grade car park
- The Mary Glowrey Building
- 28 - 42 Young Street

It is noted that 81 - 89 Victoria Parade is subject to a seperate approvals process.

Attachment 3 - PLN16/0085 - Public Notification ACU Development Plan Part 2

Attachment 3 - PLN16/0085 - Public Notification ACU Development Plan Part 2

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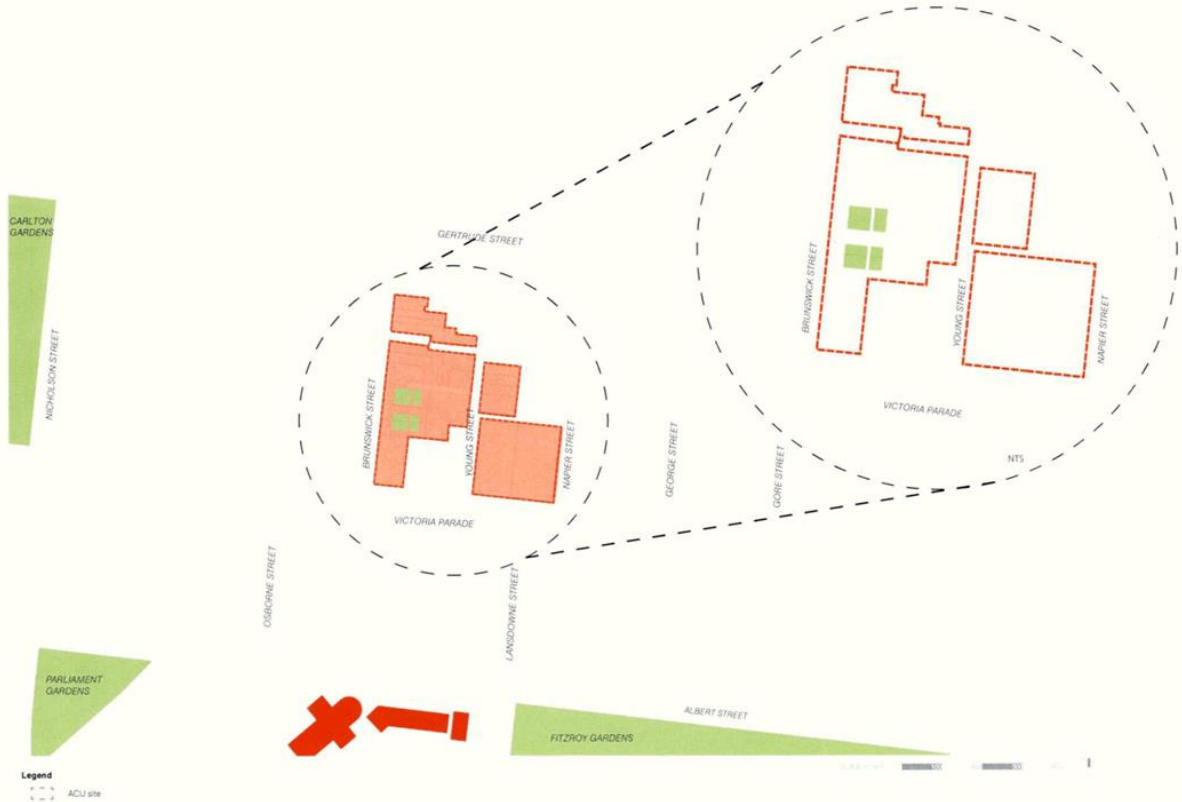
2.8 Existing open space network

The existing open space areas are generally limited to use by ACU students and staff and the Development Plan seeks improvements to open up the campus and open space areas to the wider community.



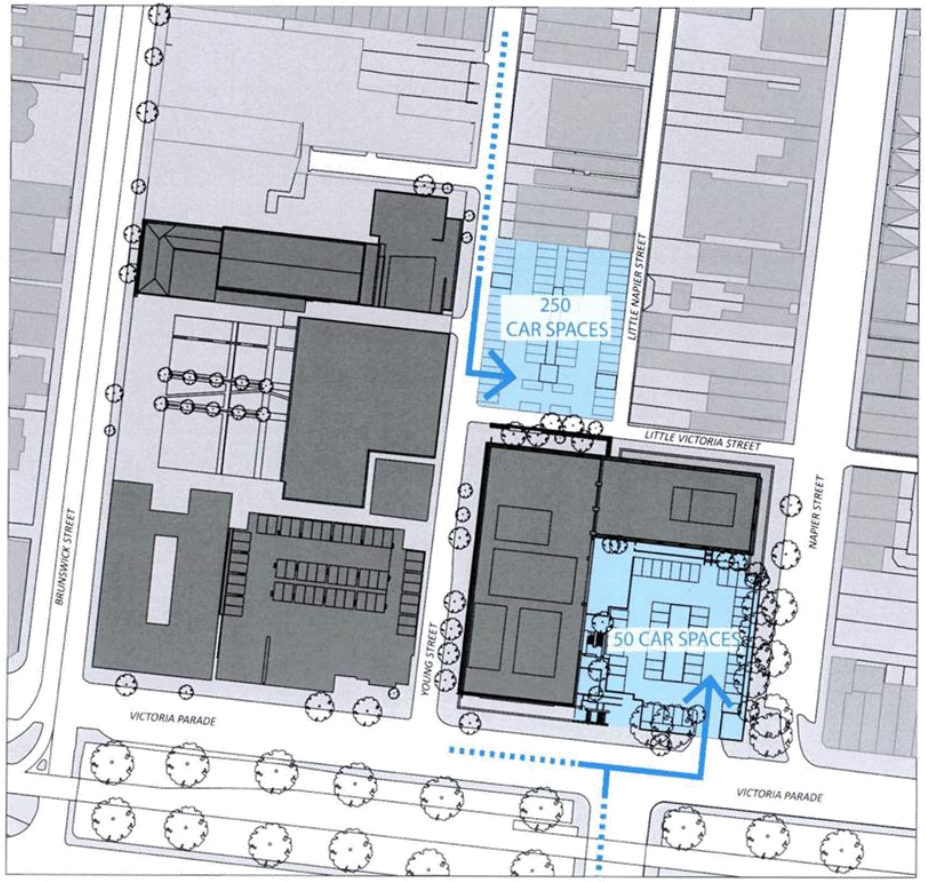
18 | ACU: ST PATRICK'S CAMPUS DEVELOPMENT PLAN

Attachment 4 - PLN16/0085 - Public Notification ACU Development Plan Part 3

ACU

2.9 Existing access arrangement

Existing access arrangements are provided to the at grade car park on the corner of Victoria Street and Napier Street and to a multi level car park in Young Street.



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2.10 Access & Movement Analysis (pedestrian and vehicle)

The main buildings on campus are proximate to a wide array of transport options including major vehicular routes, all types of public transport, cycling routes and walking linkages. The intended campus consolidation and eventual growth will harness the excellent access available to future staff, students and visitors while also seeking to prioritise sustainable modes of transport by improving access to public transport and integrating pedestrian linkages into the wider pedestrian network.

Victoria Parade

Victoria Parade is one of the major thoroughfares of inner Melbourne. Between Spring Street and Hoddle Street it is a divided road with a wide reservation down the middle. Victoria Street is the extension of the Parade to the east and west that runs for over six kms, between North Melbourne and the Yarra River.

Victoria Street and Parade form a part of the borders of several inner Melbourne suburbs including West Melbourne, North Melbourne, Melbourne, Carlton, East Melbourne, Fitzroy, Collingwood, Richmond and Abbotsford. In addition to the Carlton Gardens, several other Melbourne landmarks are located on Victoria Parade, including RMIT University, the Royal Exhibition Building, St Vincent's Hospital, Queen Victoria Market, the Eastern Hill Fire Station and the Victorian Trades Hall.

Public Transport

The site is very well serviced by trams, trains and buses. Diagonally opposite the Campus is the St Vincent's Plaza super stop which is on the corner of Victoria Parade and Brunswick Street. Trams from this stop service the CBD and travel to North Balwyn, Docklands, Box Hill, Port Melbourne, Victoria Gardens, St Kilda and West Preston. The Nicholson Street Tram is approximately 250m from the Campus servicing Bundoora and East Brunswick.

Parliament Station can be accessed via a short tram ride and it is within a 10 minute walk from the campus for an able bodied person.

Victoria Parade is a priority Bus Route with buses servicing the Northern and Eastern suburbs including Bundoora, Kew, Doncaster, Ringwood and Healesville.

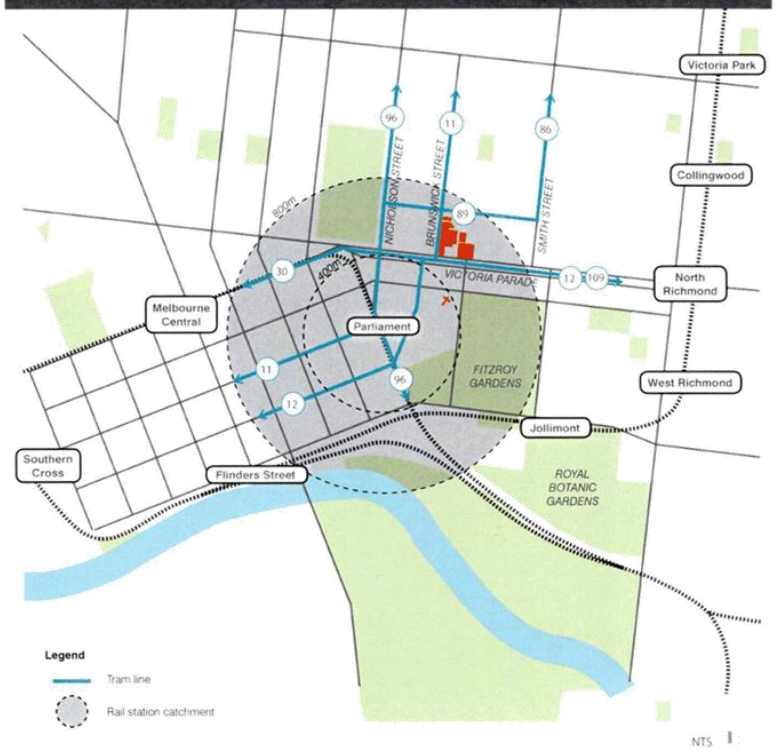
Cycling Access

The ACU campus has good access to the adjacent Napier Street priority cycling route and the recently completed Copenhagen style cycling link to the east of the site along Wellington Street.

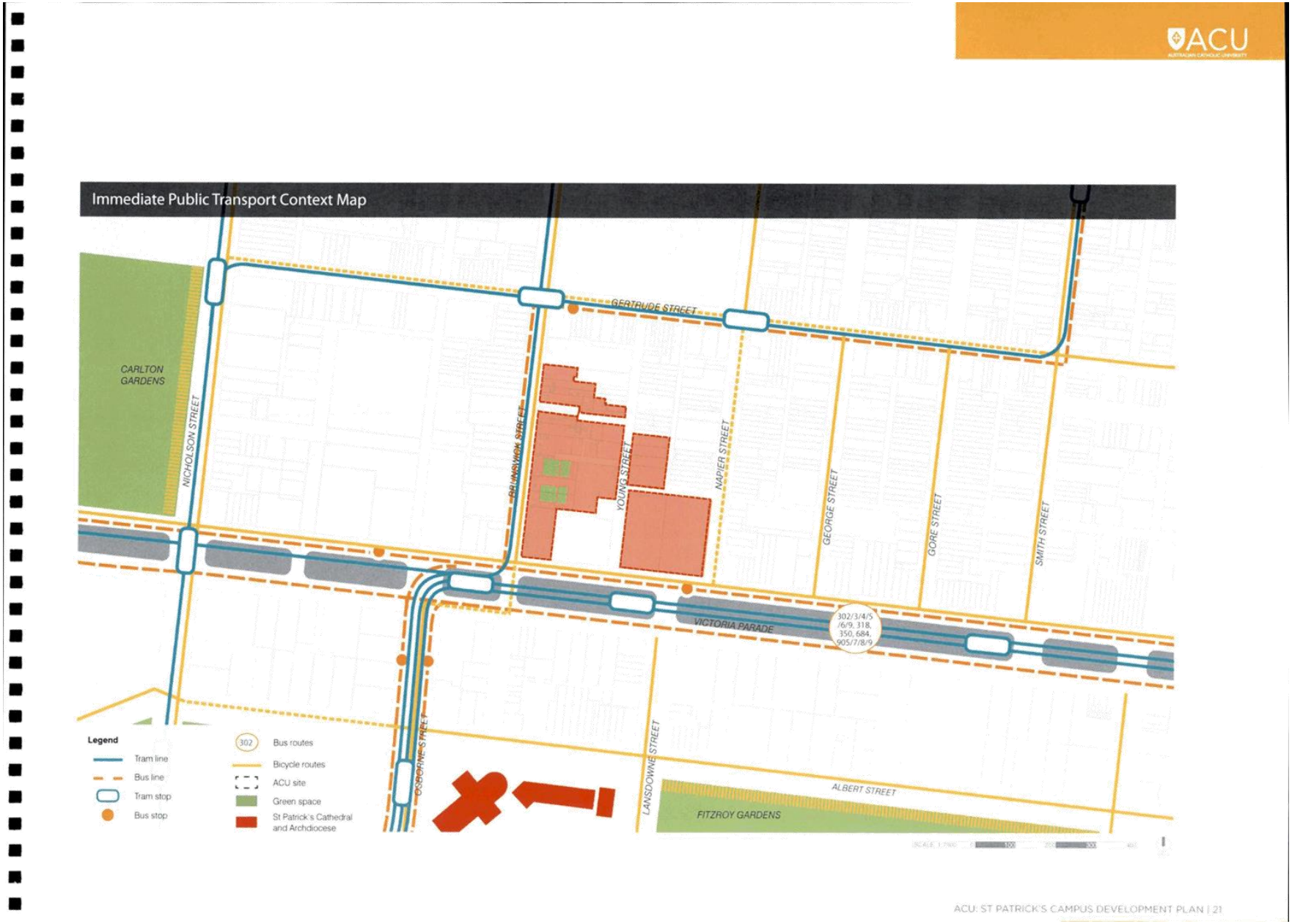
Campus Pedestrian Access

The Campus surrounds has a high number of pedestrians with people moving between the nearby bus and tram stops, large car parks and street car parking areas.

Public Transport Context Overview Map



Attachment 4 - PLN16/0085 - Public Notification ACU Development Plan Part 3



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2.11 Existing Urban Character Analysis

The ACU main campus is located within the block bounded by Brunswick Street, Napier Street, Victoria Parade and Gertrude Street, with Young Street running north-east through this wider block. ACU manages 15 buildings within this space as previously demonstrated in Section 2.6.

Victoria Parade is a major vehicular boulevard with distinctive trees creating a central avenue. Victoria Parade plays host to a range of larger buildings, more suitable to face this robust physical interface than the low scale residential areas beyond. Such buildings include various health and research facilities such as St Vincent's Private Hospital, Centre for Eye Research Australia, Epworth Freemason's Maternity Unit, and numerous others. Built form ranges widely in height from single and double storey heritage or period homes, to multi storey commercial facilities. Located within the blocks behind Victoria Parade (to both north and south) are a wide range of fine grain heritage or period dwellings interspersed with other commercial or larger residential developments.

The lower end of Brunswick Street exhibits a wide array of built form including the large social housing developments located between Gertrude and Duke Streets, with further single to triple storey developments of varying heritage or modern value between the social housing and the university.

Within Young Street, which itself is a narrow local access street, beyond the existing ACU campus buildings and the two car parks, are varying single to triple storey housing in period to modern architectural styles.

Napier Street, like Young Street, a local access street, is largely low-rise with single to triple storey residential and small scale commercial uses. The street is similar to many others in the wider area, demonstrating a fine-grain heritage streetscape interspersed with some more modern development.

The interface between the university buildings and the lower scale built form to the north and east is softened at present through the use of varying architectural treatments and design styles, designed to complement the heritage nature of some of the building stock, softened further by landscaping which fits the treed avenues.

Attachment 4 - PLN16/0085 - Public Notification ACU Development Plan Part 3

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ACU

Victoria Street, south view away from the Australian Catholic University

Victoria Street, north view towards Australian Catholic University

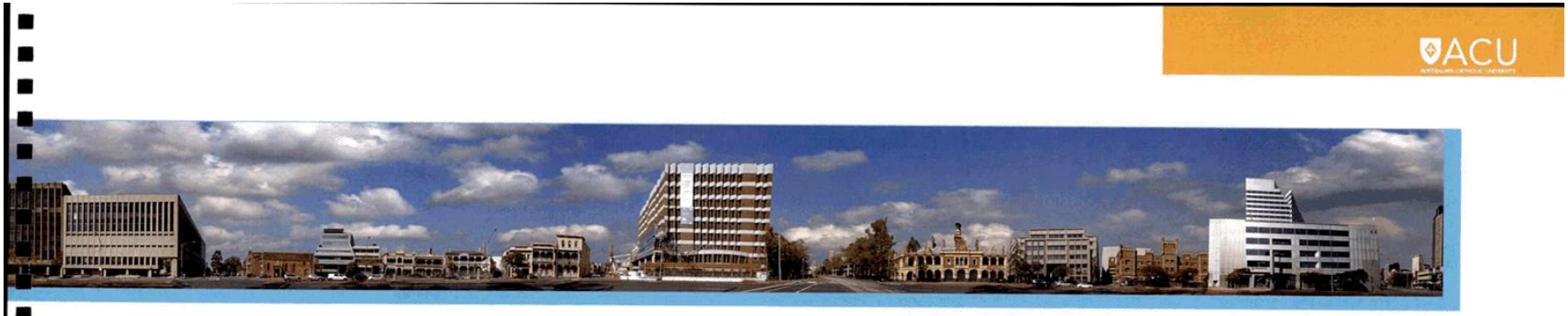
View 1 - Hard commercial edge along Victoria Street opposite to ACU

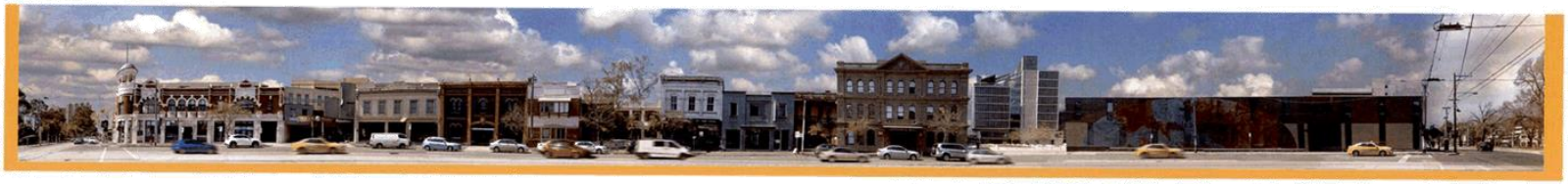
View 2 - Mixed use Victoria Street facade adjacent the ACU

View 3 - Finer grain heritage protected built form joining the ACU along Brunswick Street

24 | ACU: ST PATRICK'S CAMPUS DEVELOPMENT PLAN

Attachment 4 - PLN16/0085 - Public Notification ACU Development Plan Part 3





Brunswick Street, view to the left of the Australian Catholic University

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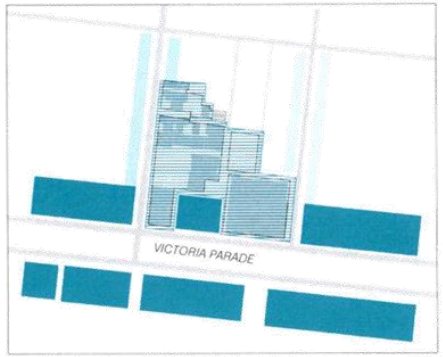
3.0 DEVELOPMENT PLAN PRINCIPLES

3.1 Key Drivers

The following section sets out the key drivers of the Development Plan, campus design drivers and ideas and design principles for new buildings.

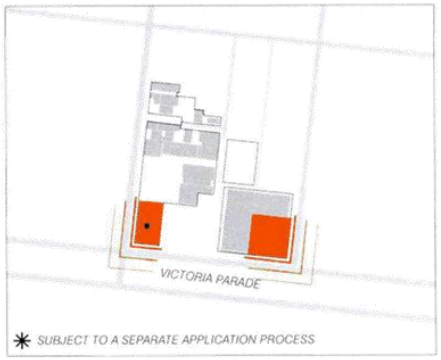
Responding to Victoria Parade through appropriate scale and mass.

Responding to the established and future character of the Victoria Parade Health and Education Corridor as a higher density mixed use urban zone, the sites location on a major civic boulevard provides the opportunity to accommodate greater scale and height similar to buildings on St Kilda Road



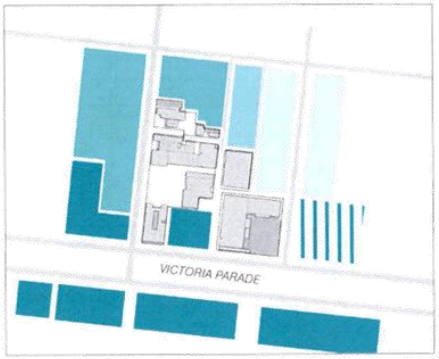
Improving the legibility of the Campus through the development of well designed larger buildings for visitors to the site and as the north east entrance to the CBD

Higher Buildings will be oriented toward Victoria Parade to strengthen the presence of the University as part of this important gateway to the central city. New well designed buildings on Victoria Parade will improve the presentation of the Campus when viewed from its surrounds.



Responding to the range of local scale and heritage character conditions to the north and east of the Campus.

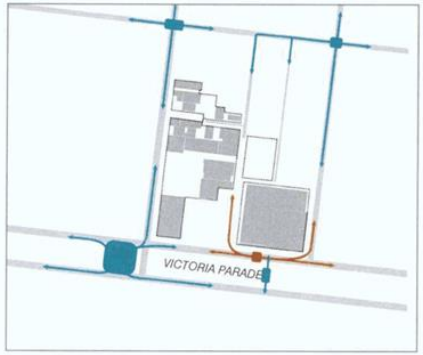
The size and orientation of the Campus provides the opportunity for higher scale development to be located on the Victoria Parade frontage with height transitioning down to the north to assist in reducing the impact of bulk on the sensitive residential uses to the north of the Campus



Attachment 4 - PLN16/0085 - Public Notification ACU Development Plan Part 3

Improving access to public transport along Victoria Parade, Brunswick Street and Gertrude Street from and to the Campus.

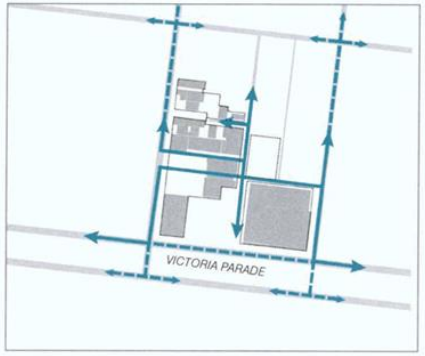
The development proposes overall improvements to the quality and safety of the public realm by improving the integration of pedestrian access and the surrounding local precinct with a priority to improve safety and accessibility to public transport stops adjacent to the Campus



St Vincent's Plaza tram stop, Victoria Parade

Ensuring pedestrian links from the Campus integrate and form part of the wider pedestrian network.

St Patrick's Campus will become a pedestrian oriented space that is safe for students and prioritises sustainable modes of transport. The Campus will be developed to improve the quality of walking infrastructure on Campus and better integrate these pathways with the pedestrian routes adjacent to the Campus



Pedestrian crossing along Victoria Parade

Improving the provision and access to additional and upgraded open space for the ACU Campus community, visitors to the site and the local community

Incrementally, the Campus will have a network of different kinds of interlinked open spaces that serve different functions and offer flexible spaces for break out, social and structured interaction and rest and relaxation for both the University and local community. Green landscape elements will be incorporated throughout the Campus where ever possible

Pedestrian access in and around the campus

Attachment 4 - PLN16/0085 - Public Notification ACU Development Plan Part 3

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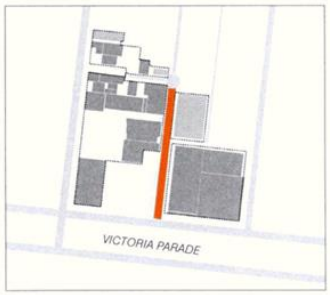
3.2 Campus Design Drivers

The Campus design drivers reflect the ACU's vision to improve the quality and accessibility of the Campus' public realm and as a well-designed city campus, to better integrate the Campus within its local environment.

The Campus open space and pedestrian networks will be improved and expanded to better connect the Campus to its local surroundings, increase access to open space in the South Fitzroy area and provide greater accessibility and sense of safety for all students, academic staff and visitors to the Campus.

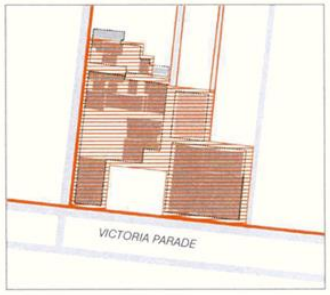
The planning and design principles have been developed in accordance with the City of Yarra's commitment to reduce car dependence by promoting walking, cycling and public transport use and improving the quality of walking and cycling infrastructure within and adjacent to the St Patrick's Campus.

1 Road closure or a shared zone along **Young Street** will create a **pedestrian environment** that contributes to public open space and amenity.

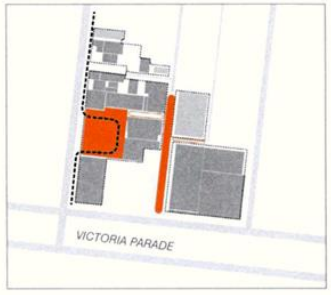


2 Two **Campus spines** provides access for pedestrians through and around the Campus and opens up the Campus for improved public access.

3 A **pedestrian focused** Campus provides a safer environment for students and strengthens connections and relationships between campus buildings and the adjacent campus uses and activities.

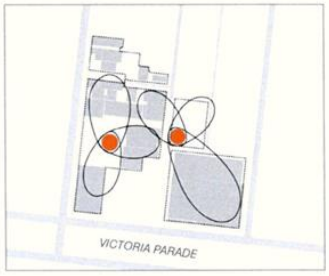


4 A **series of open spaces** will increase public realm access and Campus amenity.



5 A **hierarchy** of open spaces will serve a multitude of passive and active functions.

6 Establishing **Campus hearts**, that work together as communal spaces will serve the various buildings within the Campus.



7 An **activated building edge** on the ground level allows for a connection between the built form and open spaces.

8 The Campus green is both a **public and Campus** zone, connecting the Campus back to the wider community.

Attachment 5 - PLN16/0085 - Public Notification ACU Development Plan Part 4

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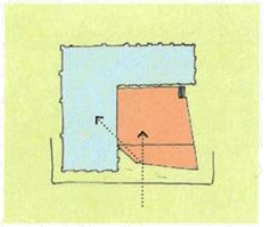
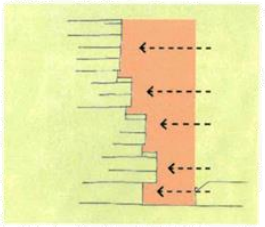
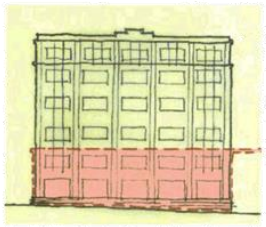
Design Ideas – New Buildings



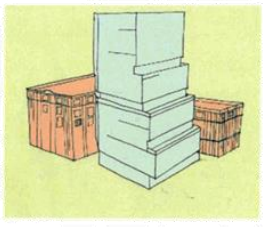
Consolidates a new entry for the both the Mary Glowrey Building and the new 117 Victoria Parade building;



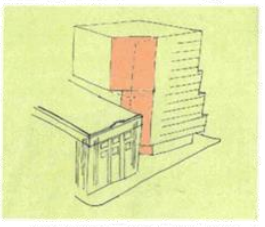
The new building consciously steps back from Napier Street, keeping the primary height and mass of the new buildings recessed away from Napier Street where the context is more of a residential scale;



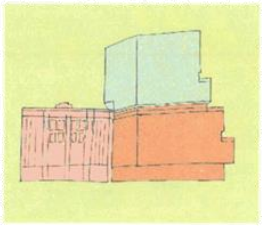
The existing glass facade and ramp arrangement of the Mary Glowrey Building on Victoria Pde is removed to re-establish the heritage facade;



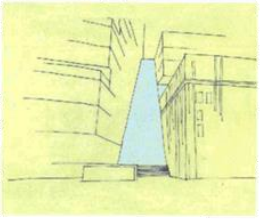
The new building has been formally composed such that it allows the two primary ends of the Mary Glowrey Building to be fully expressed in the primary streetscapes. This is achieved by revealing and preserving corners of the Mary Glowrey Building;



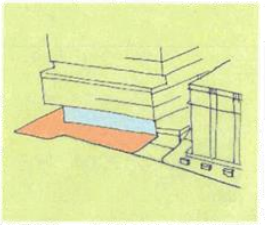
The building form of 115B is champhered to reveal a greater portion of the Mary Glowrey facade as well as softening the impact of the 115B building on the Victoria Parade streetscape.



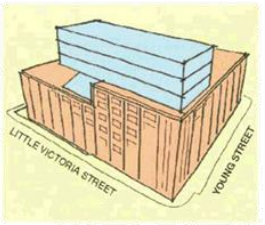
The form of the new building defines two distinct bands dictated by the relationship with the Mary Glowrey building. This modulation presents opportunities for specific architectural design details that respond to each unique relationship.



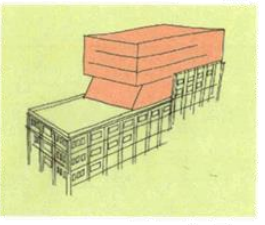
An atrium between Mary Glowrey and 115B has been established to provide connections between the two buildings as well as preserving access to natural light for both buildings; The existing glass pop-out of the Mary Glowrey is removed to restore the heritage facade.



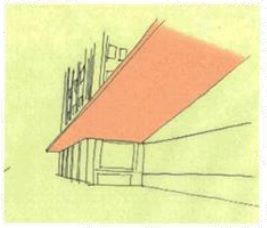
The ground floor is undercut away from the site boundary on Victoria Pde and Napier Street, revealing valuable urban space at the ground plane as well as providing generous means of circulation around the building from Napier Street and Victoria Parade;



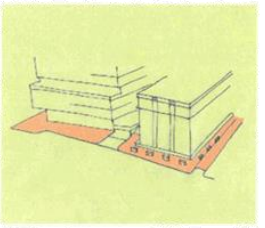
The addition of three levels above the Mary Glowrey building adds valuable floor space at high levels of the building, and by elevating it off the western wing of the Mary Glowrey it creates a valuable double height space on top of the Mary Glowrey building accessible from the new building;



The addition above the Mary Glowrey building is set back from the line of the new 115B building as well as from Napier Street so as to minimise its visual impact on the Mary Glowrey building and on the residential context of Napier Street;

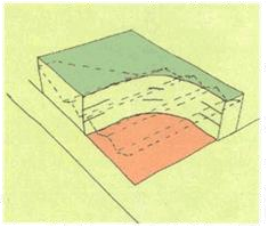


A new canopy will extend beyond the entrance to 115B along the face of the Mary Glowrey Building on Victoria Parade continuing into Young street. The canopy will improve the pedestrian experience for the new 115B inhabitants and link the new building with the heart of the campus.

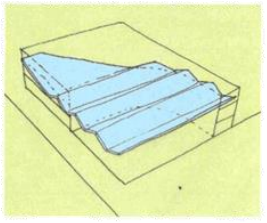


An inclusive ground plane strategy around 115B seeks to activate the zone around the Mary Glowrey and Litte Victoria Street through the breaking down of the edge wall of the Mary Glowrey. Elevating a majority of the light court to street level widens the streetscape providing opportunities to give back to the urban realm.

The car park entry and exit have been planned to be located on Napier Street which ensures the preservation of a strong active urban corner on Victoria Parade and enables a more harmonious relationship between cars, cyclists and pedestrians around 115B;



The built form at ground level has been restrained to create a shaped outdoor campus hub/ community space that is covered with a glazed roof; A green roof will be provided to the top level.



The open space is sheltered and shaded by a large transparent roof that extends over the space. The roof will create a high level amenity all year round to protect the use and functionality of this space;

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4.0 DEVELOPMENT PLAN COMPONENTS

4.1 Campus Design Principles

The Campus Design Principles have been developed in response to the important role and location of the St Patrick's Campus and to improve the Campus amenity and accessibility for students, academic staff, visitors to the Campus and the local community. The proposed developments provide the opportunity for the Campus to fulfil its role as part of the capital city knowledge economy and reflect its location at the north east entrance to the CBD that is part of the cluster of higher health and education buildings including the Eye and Ear Hospital and St Vincent's Public and Private Hospitals.

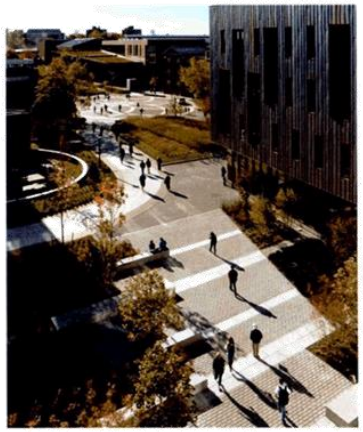
The vision of the ACU is for the St Patrick's Campus to optimise its place making potential through the development of well designed higher buildings that improve the legibility of the Campus while respecting the important interface it has with the finer grain heritage buildings and streetscapes.

As the Campus is developed, the open space and pedestrian networks will be improved and expanded to better connect the Campus to its local surroundings, increase open space in the South Fitzroy area and provide greater accessibility for students, academic staff, visitors to the Campus and the local community.

LAND USE

The Development Plan is required to enable the ACU to accommodate growing community educational and research needs and to consolidate all of the ACU's teaching, academic and research activities to the St Patrick's Campus.

The Development Plan has been developed for delivery over a fifteen year period.



BUILT FORM

The ACU St Patrick's Campus will deliver built form that relates to the surrounding context and creates an optimal environment for the sustainable operation of the University. This will include:

- height and mass that relates to the established and future character of the Victoria Parade corridor as a health and education focused mixed use urban zone.
- buildings with identity oriented toward Victoria Parade and the city to serve a marker to strengthen the presence of the University within the city.
- transitions in height and intensity to respond to the character of the inner Fitzroy precinct.
- the ground level of buildings interact with the public realm and help activate and enliven open spaces within the Campus.

THE CAMPUS PUBLIC REALM

The Campus public realm will provide a hierarchy of spaces that are activated and well used by the University and the wider community. This will be achieved through the following key initiatives:

- St Patrick's Campus will become a pedestrian oriented space that is safe for students and prioritises sustainable modes of transport.
- the closure of Young Street to through traffic will remove pedestrian vehicle conflicts and provide additional open space and improve amenity for pedestrians.
- the creation of a pedestrian focussed zone that encompasses University buildings will create strong pedestrian linkages through the Campus and to adjacent uses, activities and transport networks.
- a number of different kinds of open spaces that serve different functions and offer flexible spaces for break out, social and structured interaction for both the University and the community and places for rest and relaxation.
- green landscape elements will be incorporated where ever possible including enhancing any blank walls.
- the public realm will be activated by built form boundaries where inside and outside activities cross over with uses that extend into open spaces from within buildings.

ENVIRONMENTAL SUSTAINABILITY

The built form and public realm is designed to maximise environmental sustainable development (ESD) initiatives and showcase sustainability to the wider community. This includes:

- ensure efficient use of energy and reduce operating greenhouse gas emissions.
- ensure efficient use of water, reducing potable water use and encourage the collection and reuse of stormwater and alternative water sources.
- improve indoor environment quality including by providing fresh air intake and cross ventilation, natural and appropriate levels of lighting and external views, and thermal comfort.
- reduce the impact and improve the quality of stormwater runoff through water sensitive urban design.
- promote sustainable transport including walking, cycling and public transport to minimise car dependency.
- manage waste through waste avoidance, reuse and recycling and the creation of adaptable spaces and buildings formed with durable materials.
- enhance biodiversity and natural habitats and reduce the urban heat island effect through the use of indigenous vegetation.

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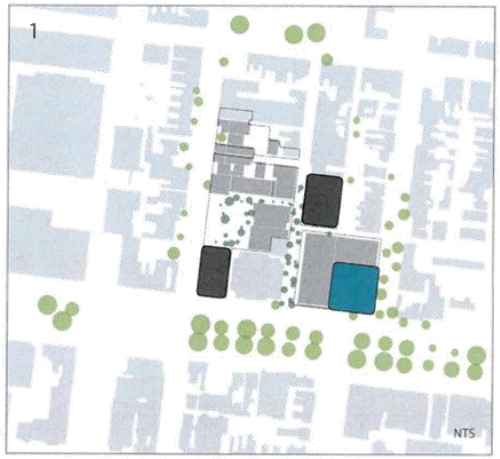
4.2 Staging

The campus will be gradually consolidated allowing the leases to be released and a consolidated campus achieved.

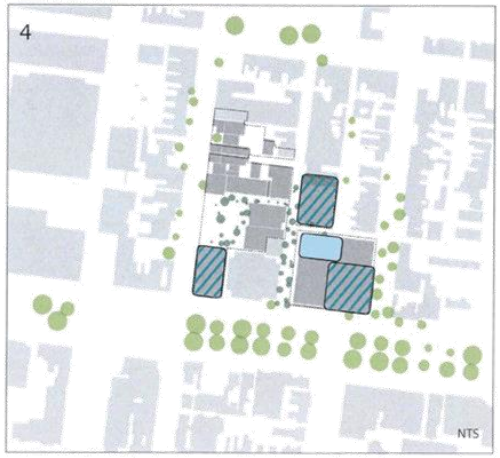
The indicative staging is indicated in the diagrams below with the new building at 115B Victoria Parade being the first priority project. Future stages will depend on future education needs, growth and funding availability, as the university continues to plan for evolving educational trends.



Stage 1- Development of 115B Victoria Parade

Stage 2 - Development of 81-89 Victoria Parade
*subject to a separate application process

Stage 3 - Development of 28-42 Young Street



Additional floor space to Mary Glowrey Building

Legend

- Campus building for development
- Current development within stage
- Completed development
- Additional floor space to existing buildings

Attachment 5 - PLN16/0085 - Public Notification ACU Development Plan Part 4

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4.3 Demolition Plan

The following diagram indicates areas and buildings requiring demolition to allow for the new buildings sought to achieve the campus objectives.

Demolition Plan

Attachment 5 - PLN16/0085 - Public Notification ACU Development Plan Part 4

4.4 Built Form Plans

- The likely building envelopes of the key development projects are set out in this section and include:
- 115B (117) Victoria Parade with underground car parking
- 28-42 Young Street and campus hub

4.4.1 115B (117) VICTORIA PARADE

The first stage of development is to construct a new building on the corner of Napier Street and Victoria Parade. The existing car park will be replaced with new underground car parking and a new building constructed. This will be the first important step in achieving the campus consolidation objectives sought. The following drawings indicate the indicative layout and building envelope.

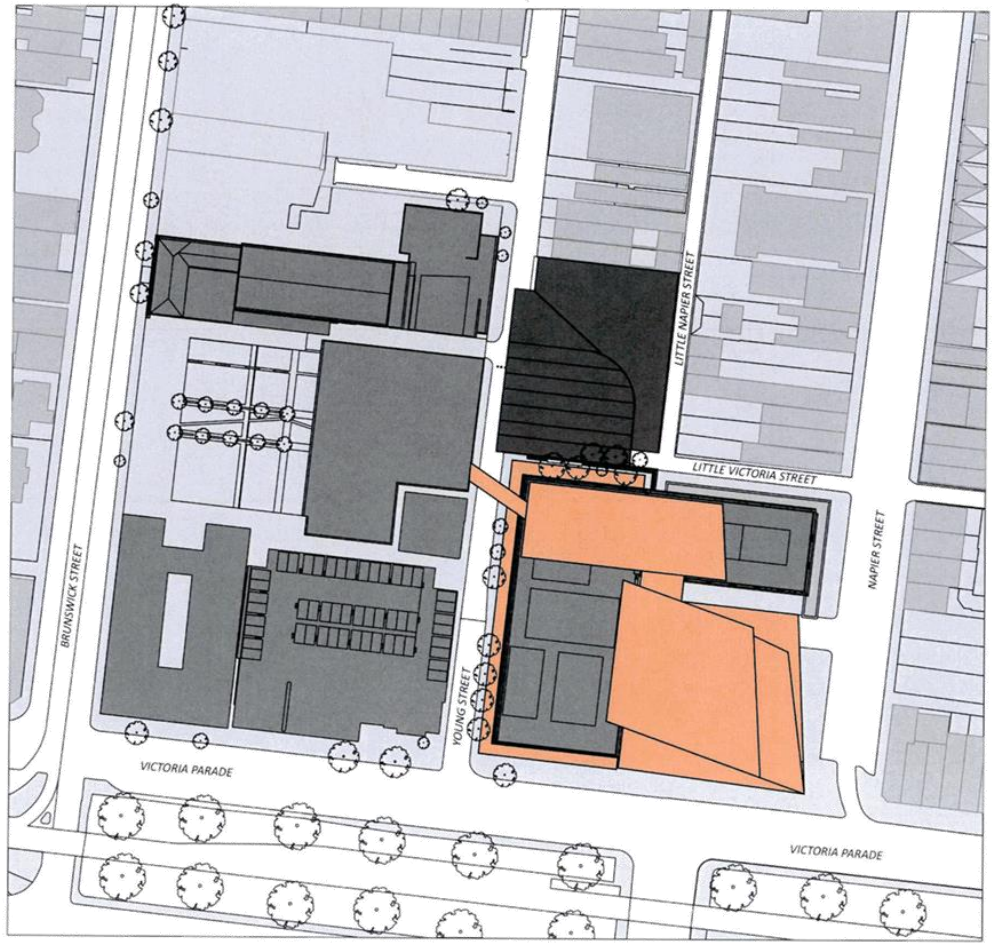
The extension to the Mary Glowrey building will be a later stage of development and provide a connection to the new building at 115B and additional learning space on the campus.

In the long term and subject to Council approval, an air bridge will be constructed between the Mary Glowrey building and the Daniel Mannix Building to provide a connection over Young Street.

4.4.1.1 Potential air bridges

The proposed air bridge(s) would provide an upper level connection between the Daniel Mannix building and the Mary Glowery building. This would improve student and staff movement through the campus and provide an alternative crossing over Young Street.

This will be subject to consultation with and approval from various Council departments for building over the Council road reserve.



Proposed Plan

Attachment 5 - PLN16/0085 - Public Notification ACU Development Plan Part 4

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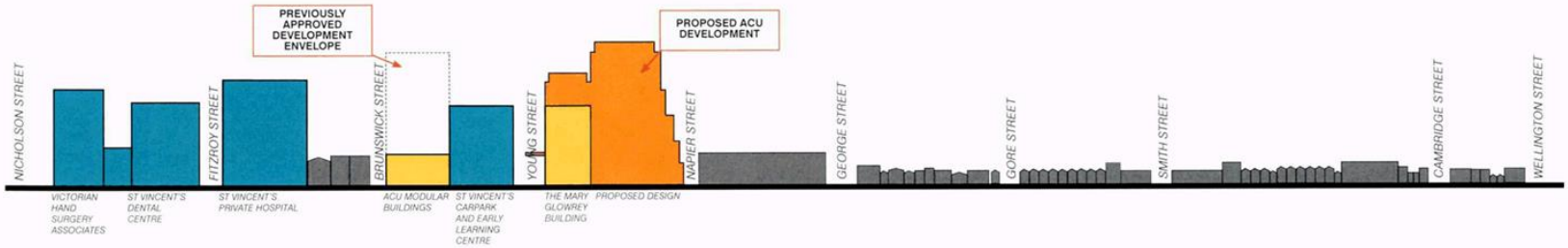
4.4.2 PROPOSED STREETSCAPE



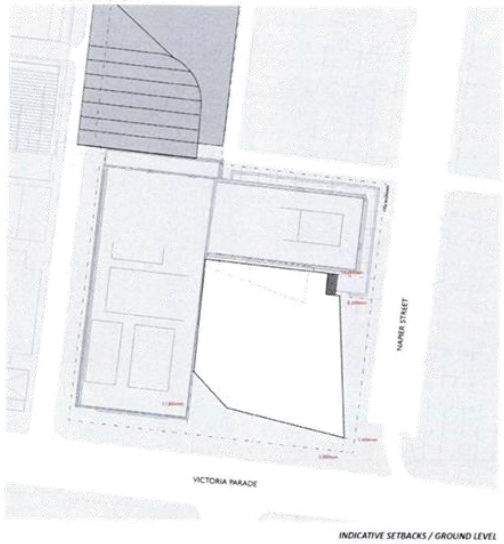
Victoria Street facade adjacent ACU

Legend

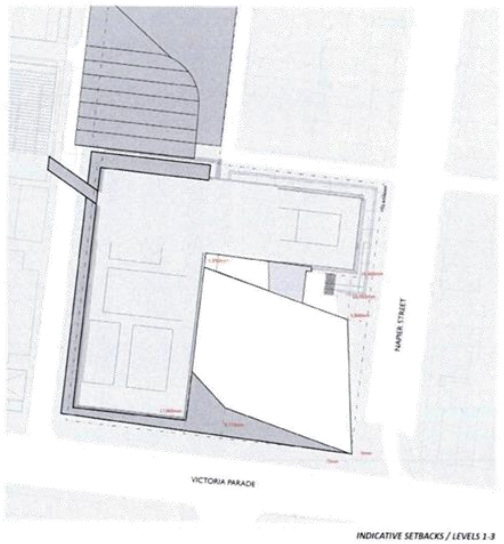
- Education services
- Proposed ACU building
- Health services
- Previously approved development envelope

Attachment 5 - PLN16/0085 - Public Notification ACU Development Plan Part 4

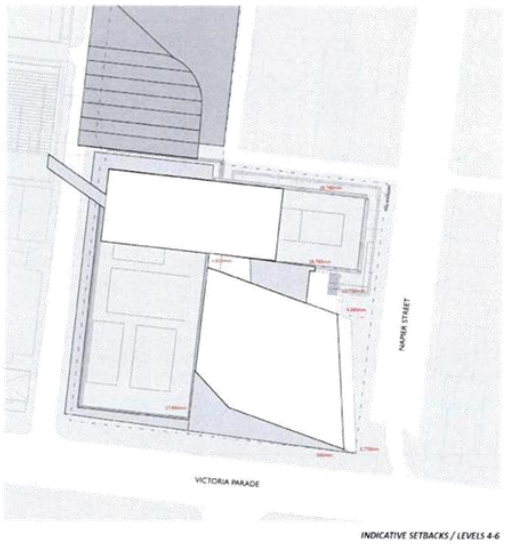
4.4.3 INDICATIVE SETBACKS AND FLOOR LAYOUT



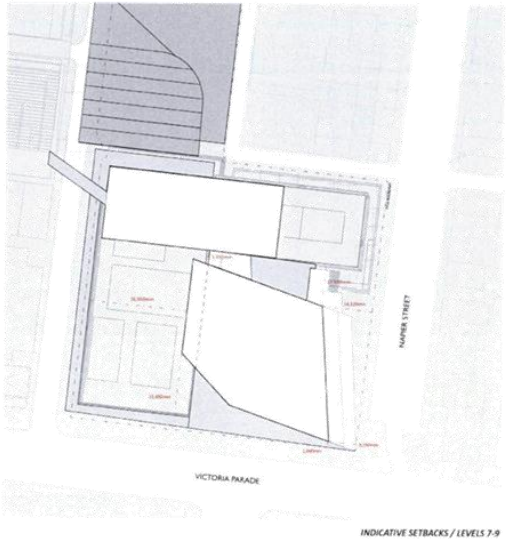
INDICATIVE SETBACKS / GROUND LEVEL



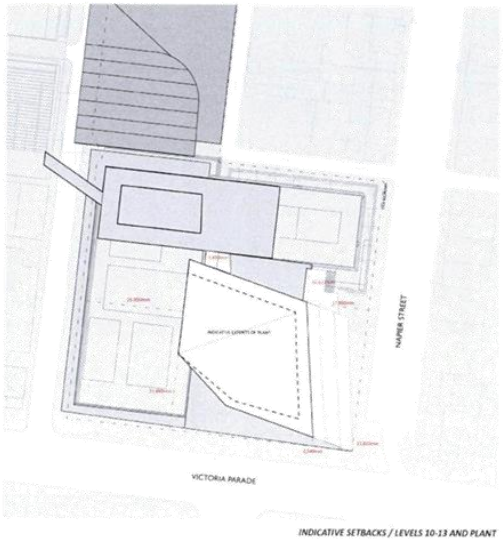
INDICATIVE SETBACKS / LEVELS 1-3



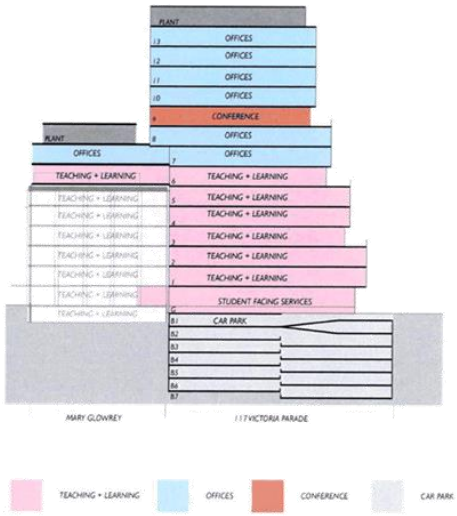
INDICATIVE SETBACKS / LEVELS 4-6



INDICATIVE SETBACKS / LEVELS 7-9



INDICATIVE SETBACKS / LEVELS 10-13 AND PLANT



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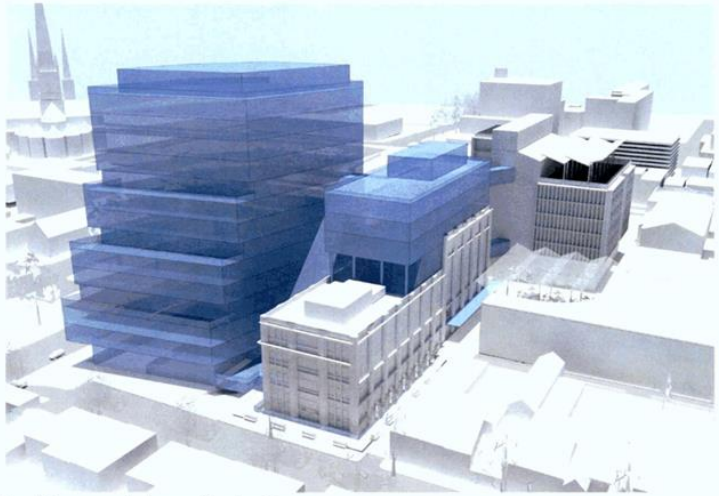
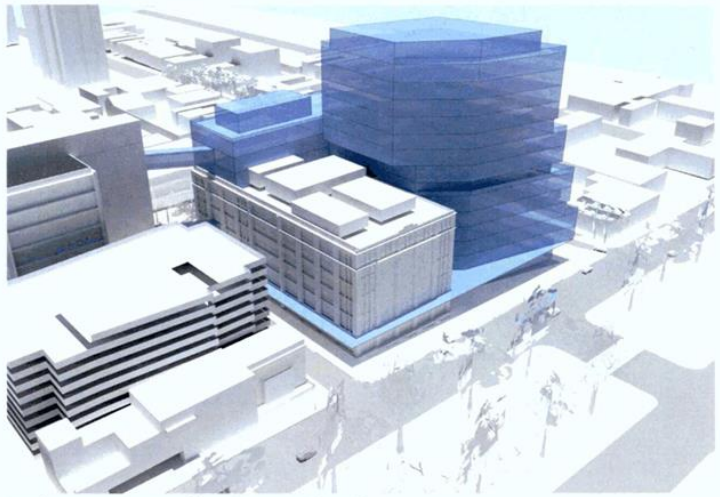
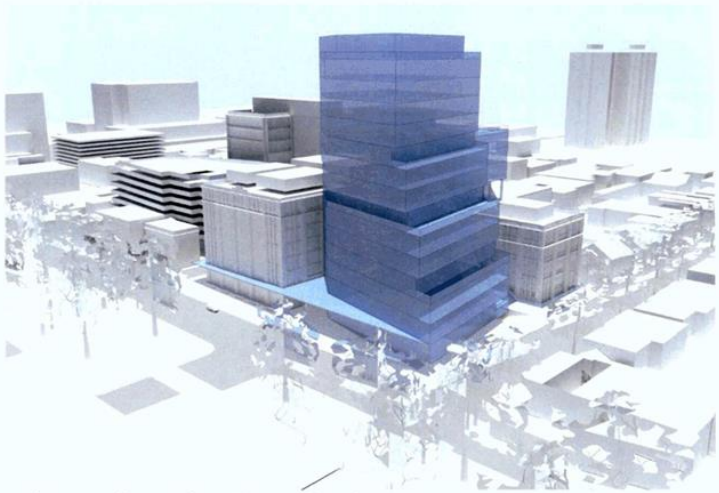
4.4.4 INDICATIVE BUILDING ENVELOPES



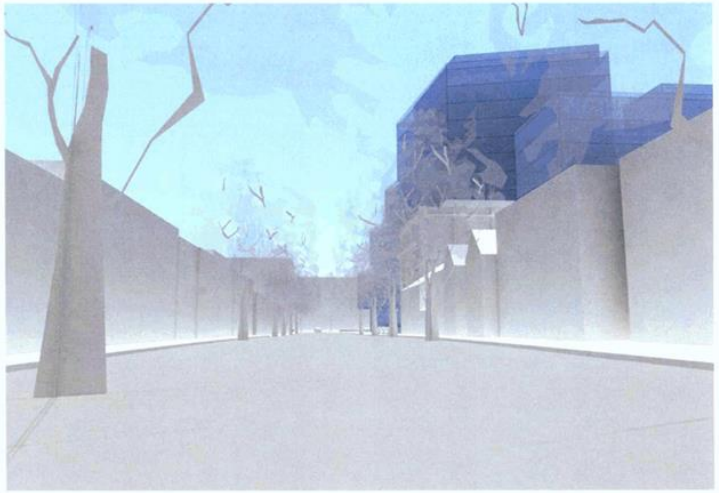
Looking south down Napier Street



Looking north-east from Victoria Parade



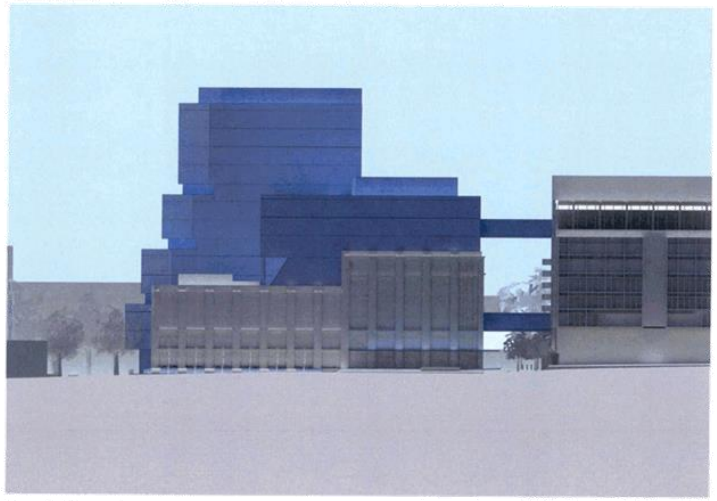
Looking north-west from Victoria Parade



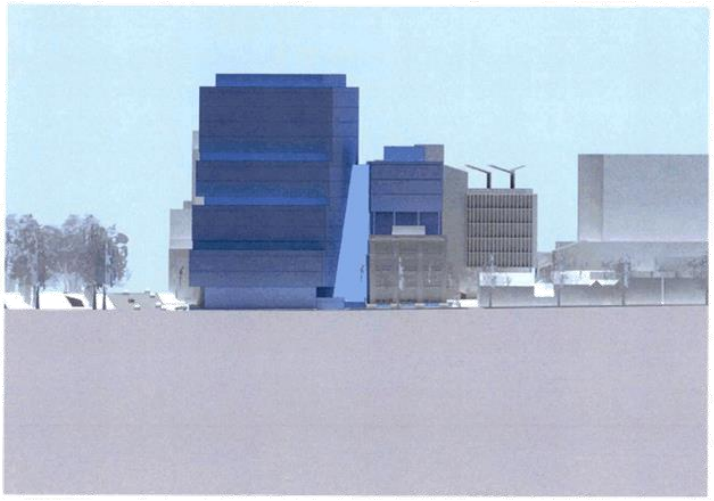
Looking north up Young Street

36 | ACU: ST PATRICK'S CAMPUS DEVELOPMENT PLAN

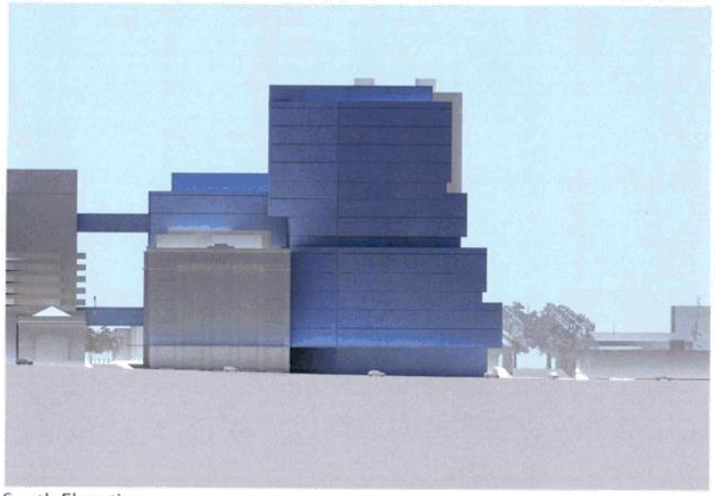
Attachment 5 - PLN16/0085 - Public Notification ACU Development Plan Part 4



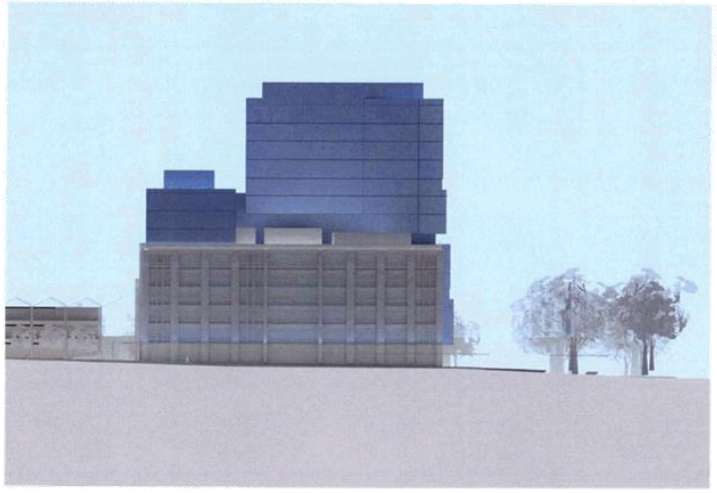
North Elevation



East Elevation



South Elevation



West Elevation

Attachment 6 - PLN16/0085 - Public Notification ACU Development Plan Part 5

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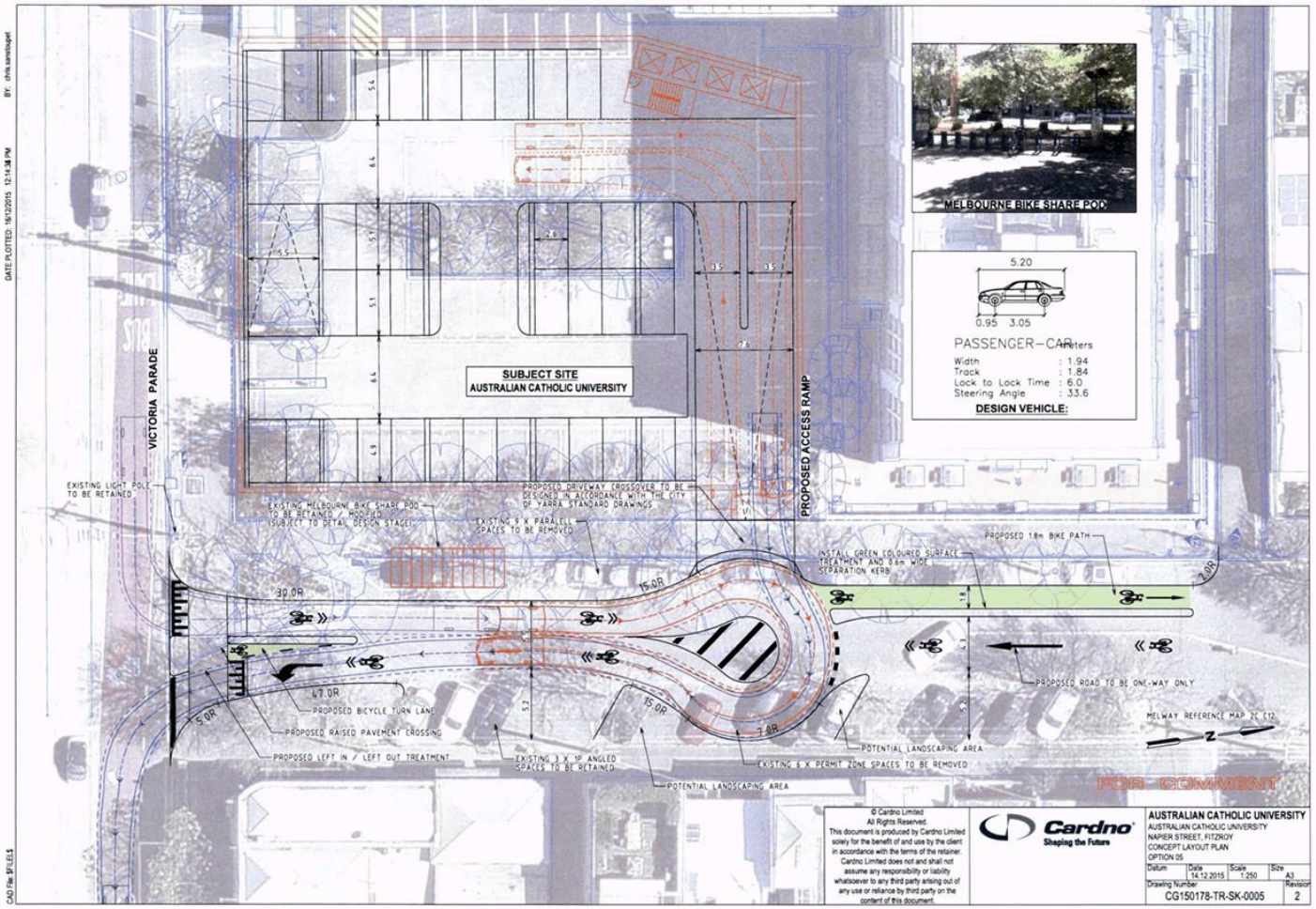
4.4.5 Proposed Access 115B Victoria Parade



38 | ACU: ST PATRICK'S CAMPUS DEVELOPMENT PLAN

Attachment 6 - PLN16/0085 - Public Notification ACU Development Plan Part 5

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4.4.6 POSSIBLE MATERIALS PALETTE

The new building designed by Lyons will be constructed of high quality materials and finishes to provide an appropriate contemporary response to the surrounding area. Some examples of the possible materials are provided below.

UPPER LEVELS

GLAZING
SUNSHADES
METAL PANELS
PRE-CAST CONCRETE
FRIT
BALUSTRADES
BALCONY PAVING
STEEL STRUCTURE

LINK

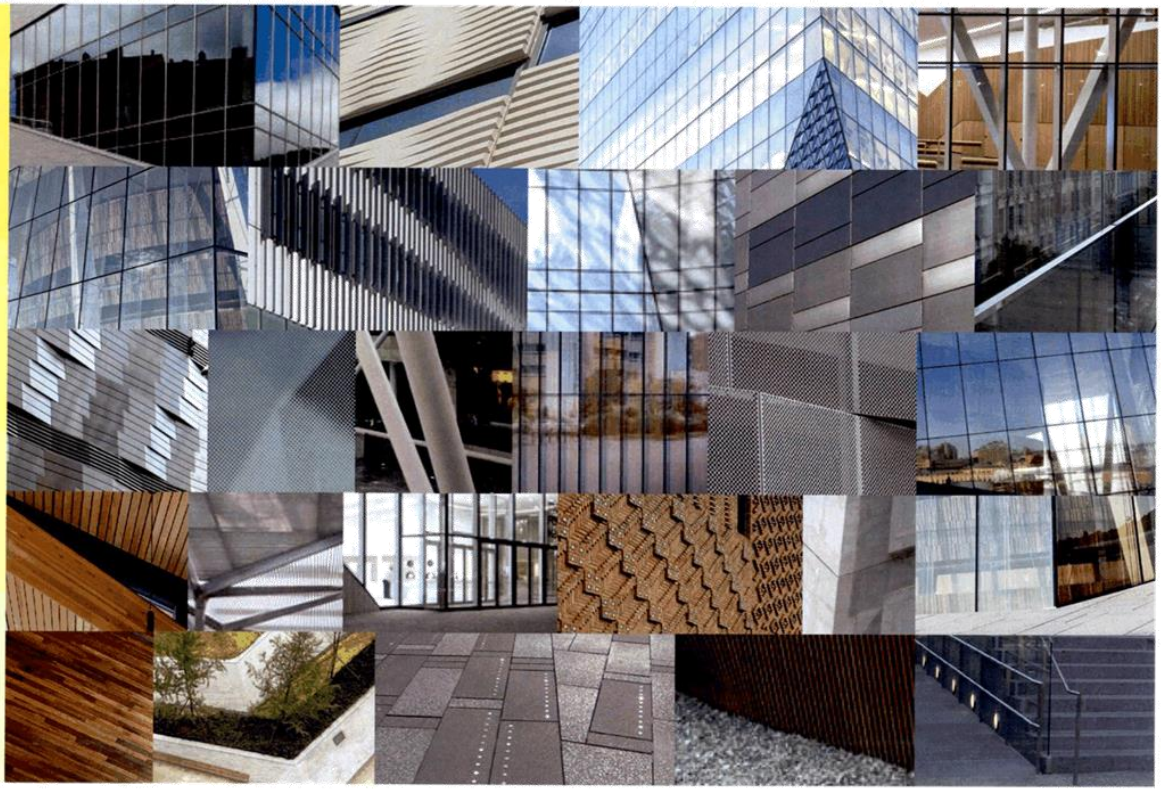
GLAZING
SUNSHADES
METAL PANELS
FRIT

LOWER LEVELS

CANOPY
GLAZING
METAL PANELS
BRICKWORK
FRIT
STEEL STRUCTURE
CONCRETE
TIMBER STRUCTURE

GROUND PLANE TREATMENT

PAVING
OUTDOOR SEATING
PLANTING
DECKING
LIGHTING



INDICATIVE MATERIAL PALETTE

Attachment 6 - PLN16/0085 - Public Notification ACU Development Plan Part 5

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4.4.7 DESIGN GUIDELINES - 115B VICTORIA PARADE

The proposed building form for 115B provides the following key design outcomes:

- Consolidates a new entry for the both the Mary Glowrey Building and the new 117 Victoria Parade building;
- Pushing the main building entry circulation within the Ground Floor glazing line enables a distinct interface with the public realm, whilst also ensuring a high-level of visibility through the corner of the new building.
- The new building consciously steps back from Napier Street, keeping the primary height and mass of the new buildings recessed away from Napier Street where the context is more of a residential scale;
- The existing glass facade and ramp arrangement of the Mary Glowrey Building on Victoria Parade is removed to re-establish the heritage facade;
- The new building has been formally composed such that it allows the two primary ends of the Mary Glowrey Building to be fully expressed in the primary streetscapes. This is achieved by revealing and preserving corners of the Mary Glowrey Building;
- The building form of 115B is champhered to reveal a greater portion of the Mary Glowrey facade as well as softening the impact of the 115B building on the Victoria Parade streetscape.
- The form of the new building defines two distinct bands dictated by the relationship with the Mary Glowrey building. This modulation presents opportunities for specific architectural design details that respond to each unique relationship.
- An atrium between Mary Glowrey and 115B has been established to provide connections between the two buildings as well as preserving access to natural light for both buildings; The existing glass pop-out of the Mary Glowrey is removed to restore the heritage facade.
- The ground floor is undercut away from the site boundary on Victoria Parade and Napier Street, revealing valuable urban space at the ground plane as well as providing generous means of circulation around the building from Napier Street and Victoria Parade;
- The south-east corner of the new building is intended as an urban gesture that demarks the corner of Napier Street and Victoria Parade. The location of the corner is crucial as the anchor point that enables the desired floor area to be achieved whilst revealing the two primary ends of the Mary Glowrey building.
- The addition of three levels above the Mary Glowrey building adds valuable floor space at high levels of the building, and by elevating it off the western wing of the Mary Glowrey it creates a valuable double height space on top of the Mary Glowrey building accessible from the new building;
- The addition above the Mary Glowrey building is set back from the line of the new 115B building as well as from Napier Street so as to minimise its visual impact on the Mary Glowrey building and on the residential context of Napier Street;

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- A new canopy will extend beyond the entrance to 115B along the face of the Mary Glowrey Building on Victoria Parade continuing into Young street. The canopy will improve the pedestrian experience for the new 115B inhabitants and link the new building with the heart of the campus.
- An inclusive ground plane strategy around 115B seeks to activate the zone around the Mary Glowrey and Little Victoria Street through the breaking down of the edge wall of the Mary Glowrey. Elevating a majority of the light court to street level widens the streetscape providing opportunities to give back to the urban realm.
- The car park entry and exit have been planned to be located on Napier Street which ensures the preservation of a strong active urban corner on Victoria Parade and enables a more harmonious relationship between cars, cyclists and pedestrians around 115B;
- The built form at ground level has been restrained to create a shaped outdoor campus hub/ community space that is covered with a glazed roof; A green roof will be provided to the top level.
- The open space is sheltered and shaded by a large transparent roof that extends over the space. The roof will create a high level amenity all year round to protect the use and functionality of this space;

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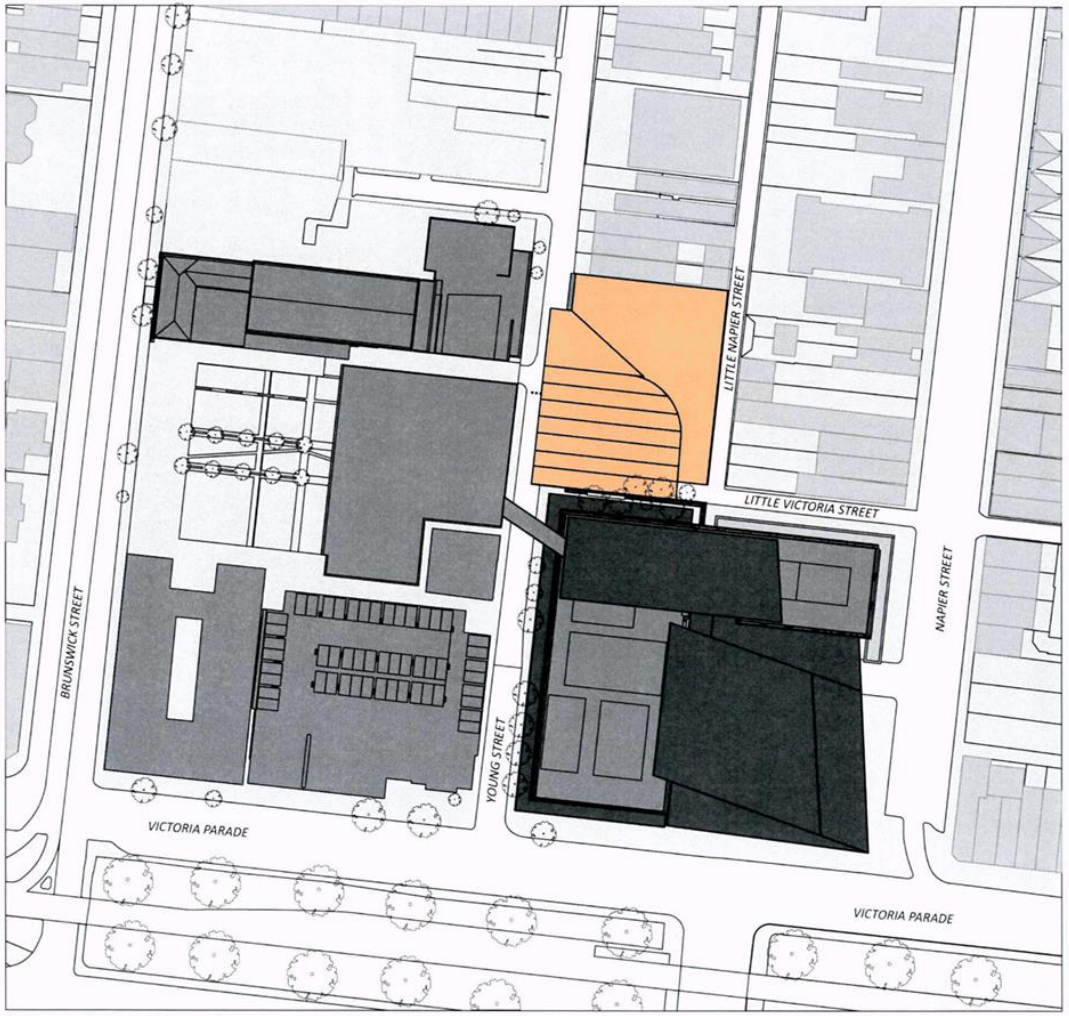
4.4.8 28 - 42 YOUNG STREET

The existing multi level car park will be removed to make way for a new student facility and community space with public open space. The following drawings indicate the indicative layout and building envelopes.

4.4.9 DESIGN GUIDELINES - 28-42 YOUNG STREET

The proposed building form for the Young Street Building provides the following key design outcomes:

- The built form at ground level has been restrained to create a shaped outdoor campus hub/ community space that is covered with a glazed roof;
- the open campus hub connects with the Mary Glowrey Building forming a singular consolidated space for students and staff and the centre of the campus;
- the open space is sheltered and shaded by a large transparent roof that extends over the space. The roof will create a high level amenity all year round to protect the use and functionality of this space;
- the building's primary frontages are to the hub space, Young Street and Little Victoria Streets
- the new outdoor open space is intended to emphatically establish a central student focussed area which has access to north sun and is largely protected from vehicular traffic;
- a green roof will be provided to the top level.



Proposed Plan - Young Street

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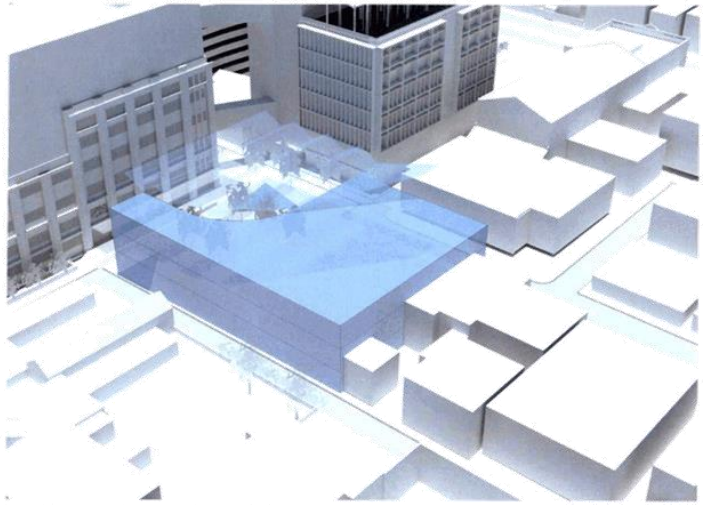
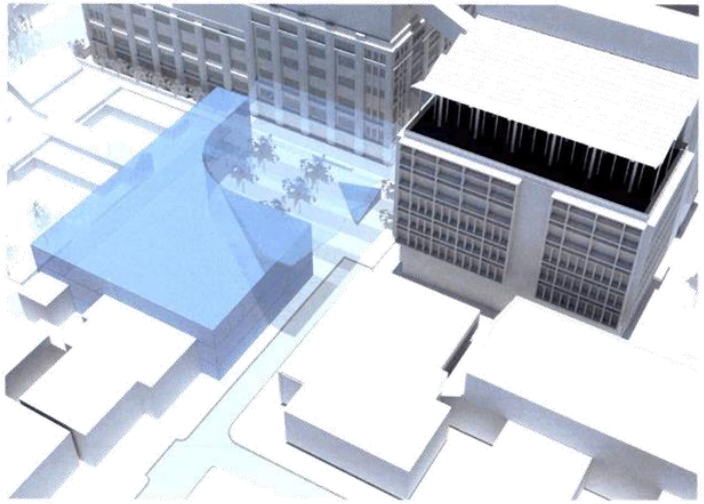
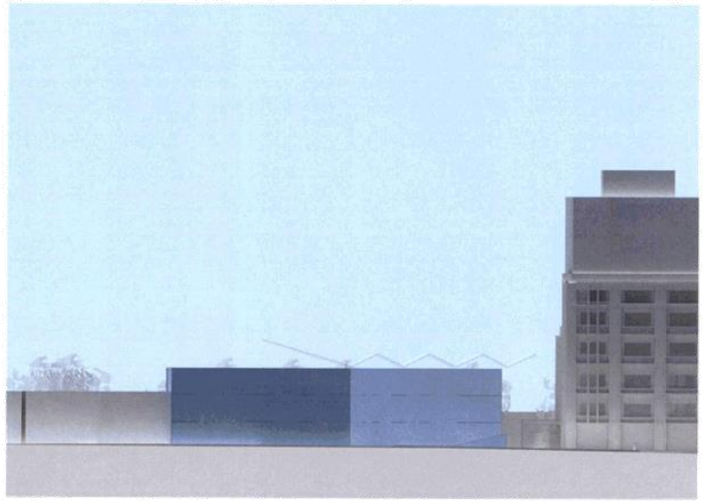


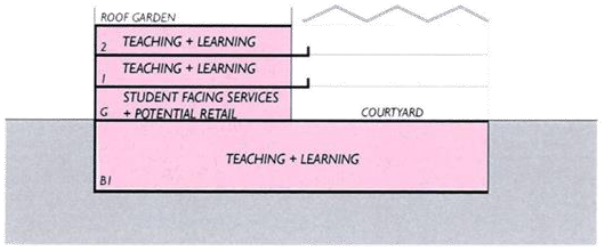
Looking south-west from Little Napier Street



Looking south-east from Young Street



West Elevation



YOUNG STREET BUILDING

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Attachment 6 - PLN16/0085 - Public Notification ACU Development Plan Part 5

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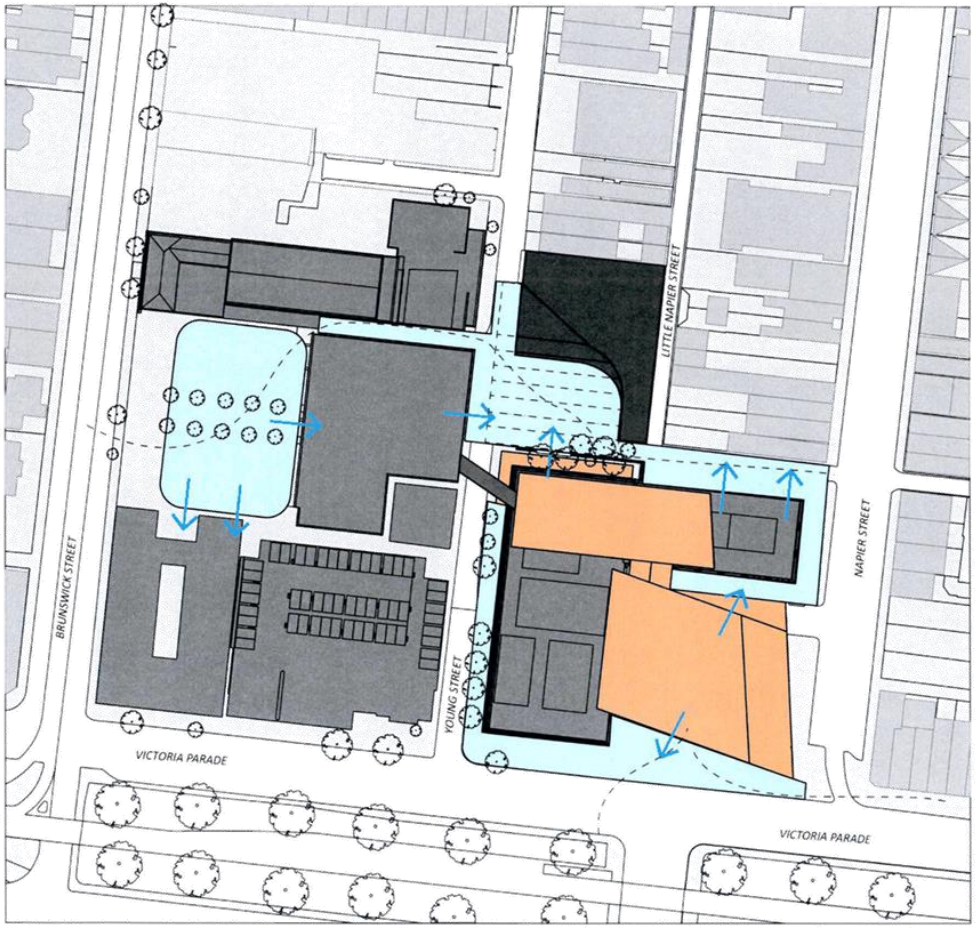
4.5 Campus Hub

The new buildings will create a new campus hub which will provide an attractive and active space, bringing the campus together.



Proposed Plan - Hub Diagram

Attachment 6 - PLN16/0085 - Public Notification ACU Development Plan Part 5

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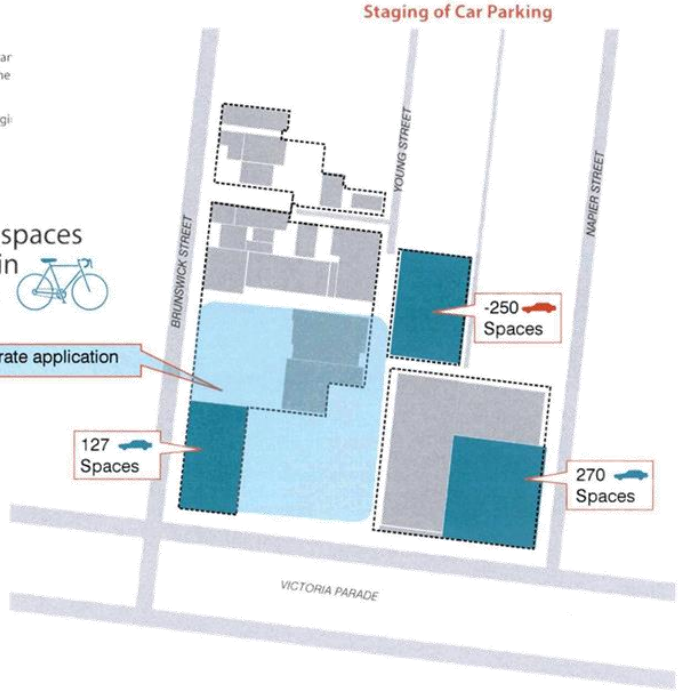
4.6 Proposed Vehicle and Bicycle Parking

The removal of the Young Street Car Park and creation of new underground car parking will allow for more active frontages and ar improved urban design outcome across the ACU campus and in the public realm.

New bicycle facilities will be provided across the campus, encouragi students and staff to cycle.

38 bike spaces within DPO area

Subject to separate application



Development of 115 B (117) Victoria Parade with 270 car parking spaces

Demolition of existing car park, -250 car parking spaces

60 bike spaces in total on campus

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4.7 Overall Pedestrian Movement Plan

The new campus hub and new buildings with access edges will improve pedestrian movements around the campus and link the campus together and to the surrounding street network.

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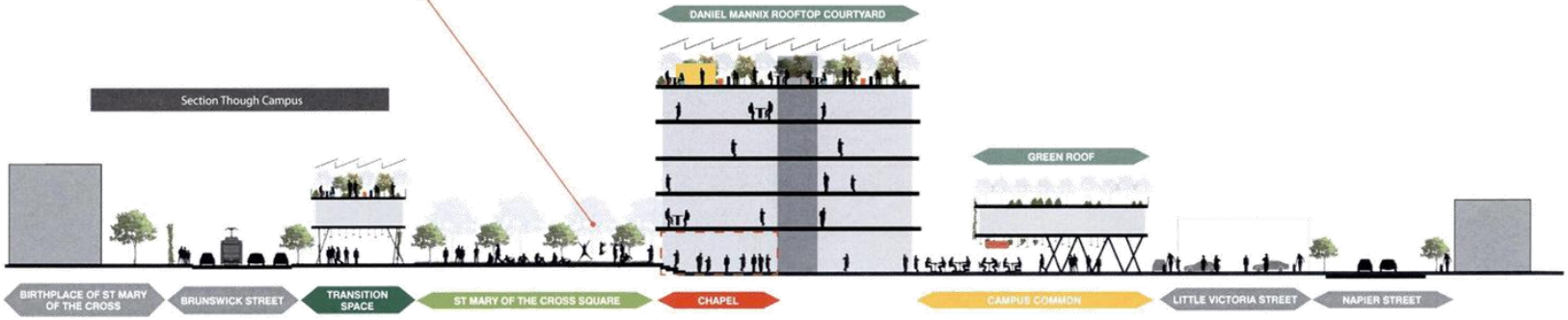
4.8 Pedestrian and Public Realm

It is proposed to make consequential improvements to pedestrian access throughout the Campus as well as the quality of the public realm to provide an attractive and safe environment that will integrate with the surrounding local precinct.



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4.9 Overall Landscape and Open Space Plan and Principles

The proposed landscaping will connect the campus and meet safety principles to provide areas that are well used, have passive surveillance and provide attractive spaces for the public.

It is noted that any future public realm works will be undertaken in consultation with Council and that additional approvals from relevant authorities, including Council department outside of planning are required for public realm works. ACU acknowledges that public realm works associated with each future planning permit are likely to be subject to a condition that requires the works to be undertaken at the University's cost in consultation with Council.

48 | ACU: ST PATRICK'S CAMPUS DEVELOPMENT PLAN

4.10 Landscape Master Plan

CLIMATE AND SUSTAINABLITY FRAMEWORK

There are numerous linear corridors leading from the surrounding streets to the ACU Campus that create wind tunnels. Along with the over shadowing created by the surrounding buildings can result in uncomfortable urban environments.

Provide opportunities for sheltering from cool or hot winds such as:

- Planting screens.
- Landscape walling and artistic screens.
- Purpose designed shelters that combine with other functions such as water run-off storage and lighting.

There is also extensive hardscaping typical of university environments. Future design should minimise stormwater run-off and moderate discharge flows through on-site absorption and retention.

Provide opportunities for Water Sensitive Urban Design (WSUD) such as:

- Use of permeable channels and paving
- decorative garden beds that act as rain gardens
- collection and retention of roof rainwater run-off and re-use on canopy trees

ACCESS AND MOBILITY

The ACU campus will become a vibrant part of the local urban realm. It is presently fragmented from the surrounding road network with sections of the campus isolated and 'off limits'. The future design will have a high level of accessibility and mobility, and encourage pedestrian use through prominent and cohesive visual way-finding opportunities.

Primary Pedestrian Promenade

Young Street, Duke Street and St. Mary of the Cross Square will become the primary pedestrian area that will have provision for maintenance and service vehicle access. This promenade will act as the main circulation spine through the campus.

Design opportunities include:

- Medium canopy deciduous trees
- Feature paving
- Feature lighting including canopy lighting
- Short term seating areas
- WSUD opportunities including permeable paving and raingardens

Secondary Pedestrian Links

The north end of Young Street and the west half of Little Victoria Street will become the secondary pedestrian area that will also have provision for maintenance and service vehicle access. These routes will act as links to the main circulation spine through the campus.

Design opportunities include:

- Planter boxes
- Vertical green walls
- Feature paving
- Feature lighting in pavement

Shared Access

Little Victoria Street, Little Napier Street and the section of Young Street intersecting with Graham Street will allow for vehicle and pedestrian access that link Gertrude Street and Napier Street to the campus.

Design opportunities include:

- Vertical green walls
- Creative use of urban wall art such as murals and vertical sculptures

Transition Zones

Transition zones are pedestrian cross over entry points that signify a change in urban neighbourhood such as from street realm to campus environment which could be either pedestrian only or shared conditions.

Design opportunities include:

- Use of pavement material to produce change
- Signage
- Colour
- Lighting

Viewlines

There are long narrow vistas to the campus from surrounding main roads such as Victoria Parade, Napier Street and Gertrude Street. ACU identity and way finding should be the primary design feature to make the campus legible and accessible.

Design opportunities include:

- Innovative features on campus building walls such as murals, vertical sculptures and green walls
- Use of signature red and vertical linear signage design
- Lighting
- Consolidation of wall materials used at pedestrian level to uniform space that act as a visual symbol representing the identity of the ACU campus
- Signage opportunities on the corner of Brunswick Street and Victoria Parade

OPEN SPACE AND LANDSCAPE FRAMEWORK

ACU Forecourt

An extensive covered public 'room' that will become the key social hub for the campus connected by pedestrianised green links in collaboration with urban art.

Design opportunities include:

- Active café precinct
- Creative use of paving lighting
- Landscape details such as seats, bins, lights and feature paving
- Canopy trees
- Urban art such as sculptures
- The forecourt will be accessible to the public outside of University operating hours.

Victoria Parade Entry Area

At the corner of Victoria Parade and Napier Street there is an urban edge that will reinforce the presence of the campus from commuters travelling from the east via a creative entry design.

Design opportunities include:

- Integrated seating areas that cater for short stops/meeting points
- Canopy trees
- Feature paving
- Signage
- Lighting

The legibility of the Campus will be further enhanced with a hierarchy of landscaped precincts defined as:

- Primary Linear Green Corridors
- Secondary Linear Green Corridors

Primary Linear Green Corridor

Young Street and St. Mary of the Cross Square will become the primary linear green corridors and will have its own theme planting particular to this precinct.

Design opportunities include:

- Medium to large deciduous canopy trees
- Ground cover plantings
- WSUD opportunities including raingardens

Secondary Linear Green Corridor

Little Napier Street, Little Victoria Street, Duke Street and Young Street to the north will become the secondary linear green corridors and will have its own theme planting particular to this precinct.

Design opportunities include:

- Vertical green walls
- Planter boxes
-

SAFER SPACES

The landscape design is based on the standard CPTED (Crime prevention through environmental design) principles, to ensure increased activity and maximise visual and physical connectivity. Lighting will provide an inviting after hours environment and the structure of planting will ensure direct sight lines, through low ground level planting and high canopy trees.

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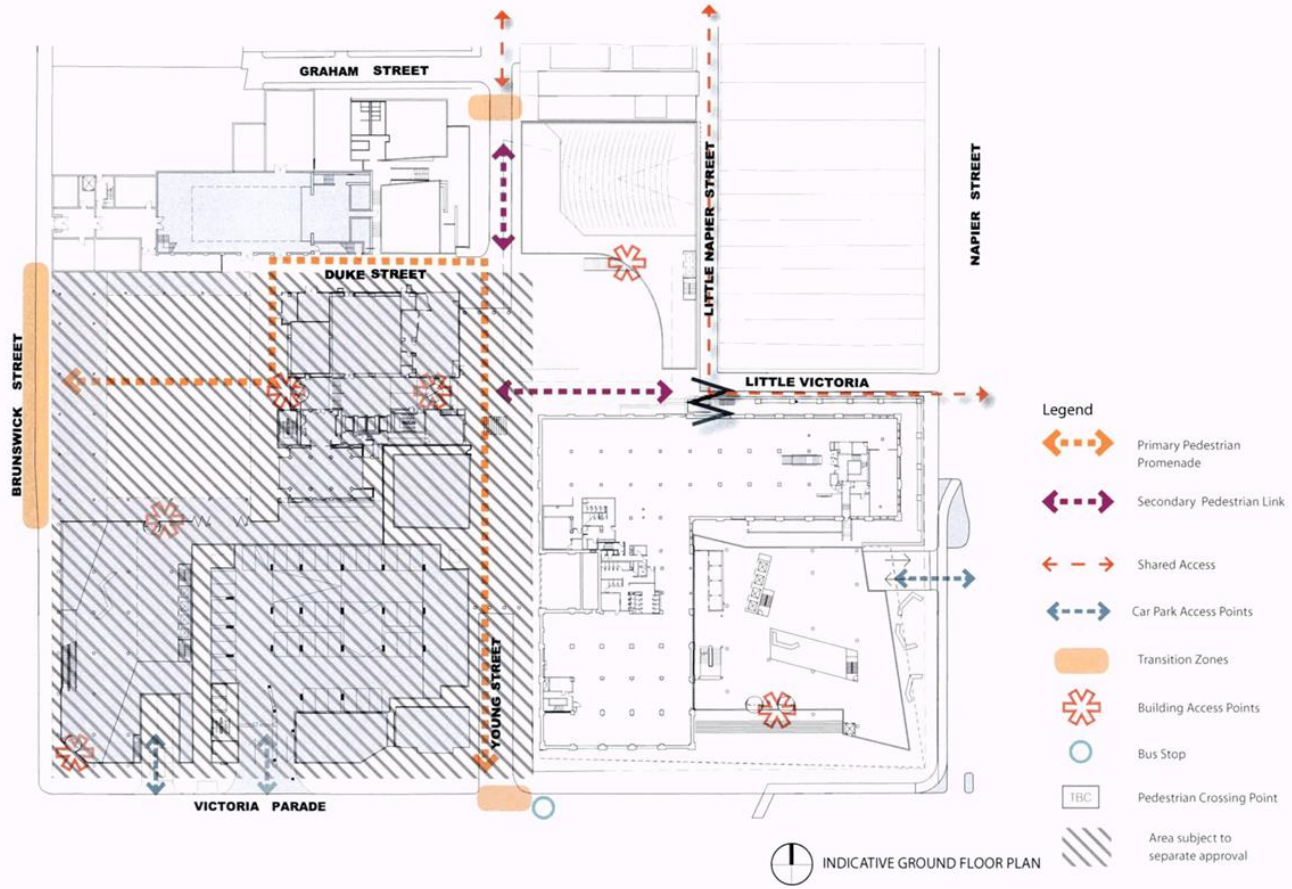
4.10.1 ANALYSIS PLAN 1 : ACCESS AND MOBILITY PLAN



Legend

- Primary Pedestrian Promenade
- Secondary Pedestrian Link
- Shared Access
- Car Park Access Points
- Transition Zones
- Building Access Points
- Bus Stop
- TBC Pedestrian Crossing Point
- Area subject to separate approval

50 | ACU: ST PATRICK'S CAMPUS DEVELOPMENT PLAN

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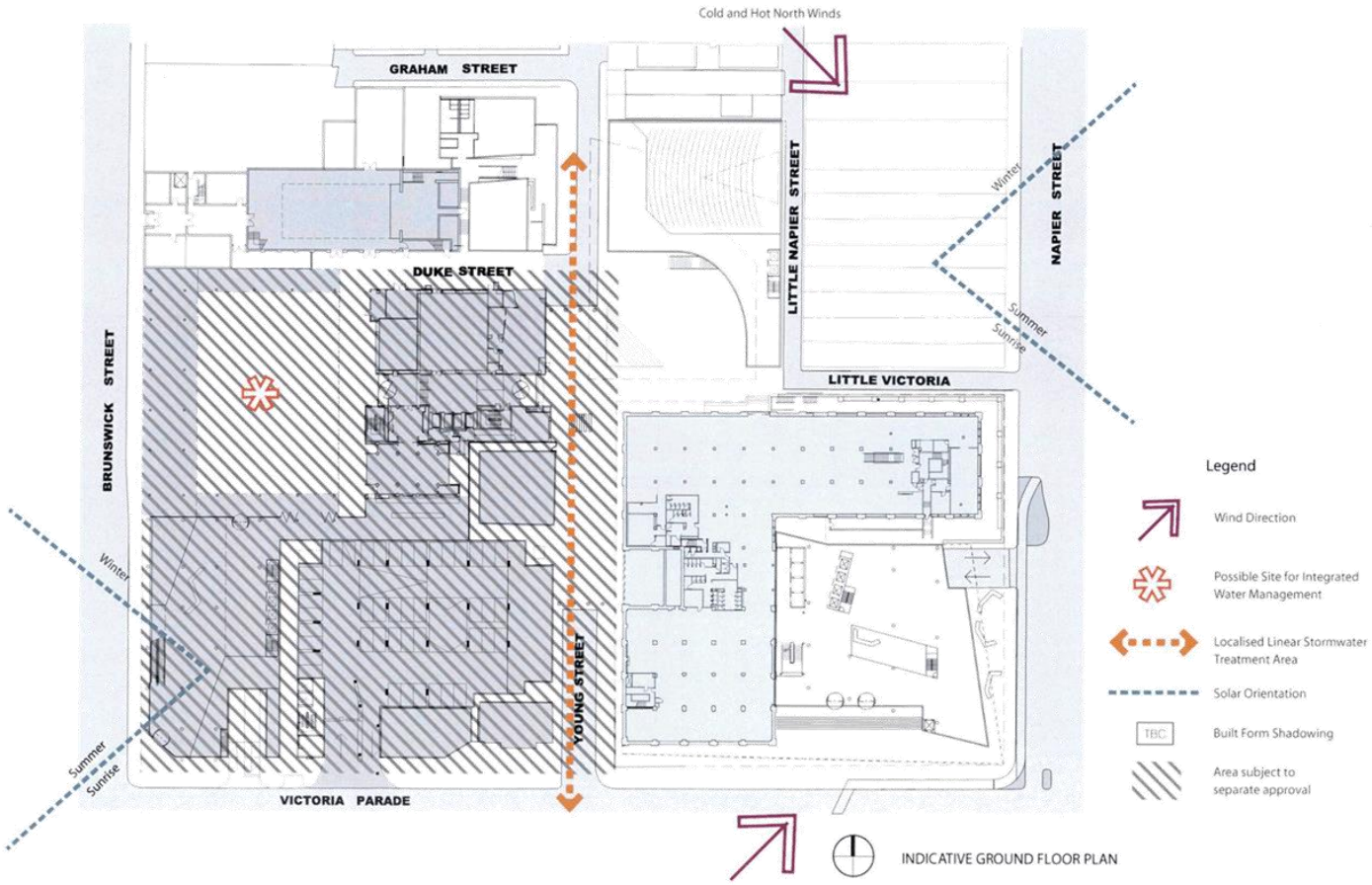
4.10.2 ANALYSIS PLAN 2 : CLIMATE AND SUSTAINABLE FRAMEWORK PLAN



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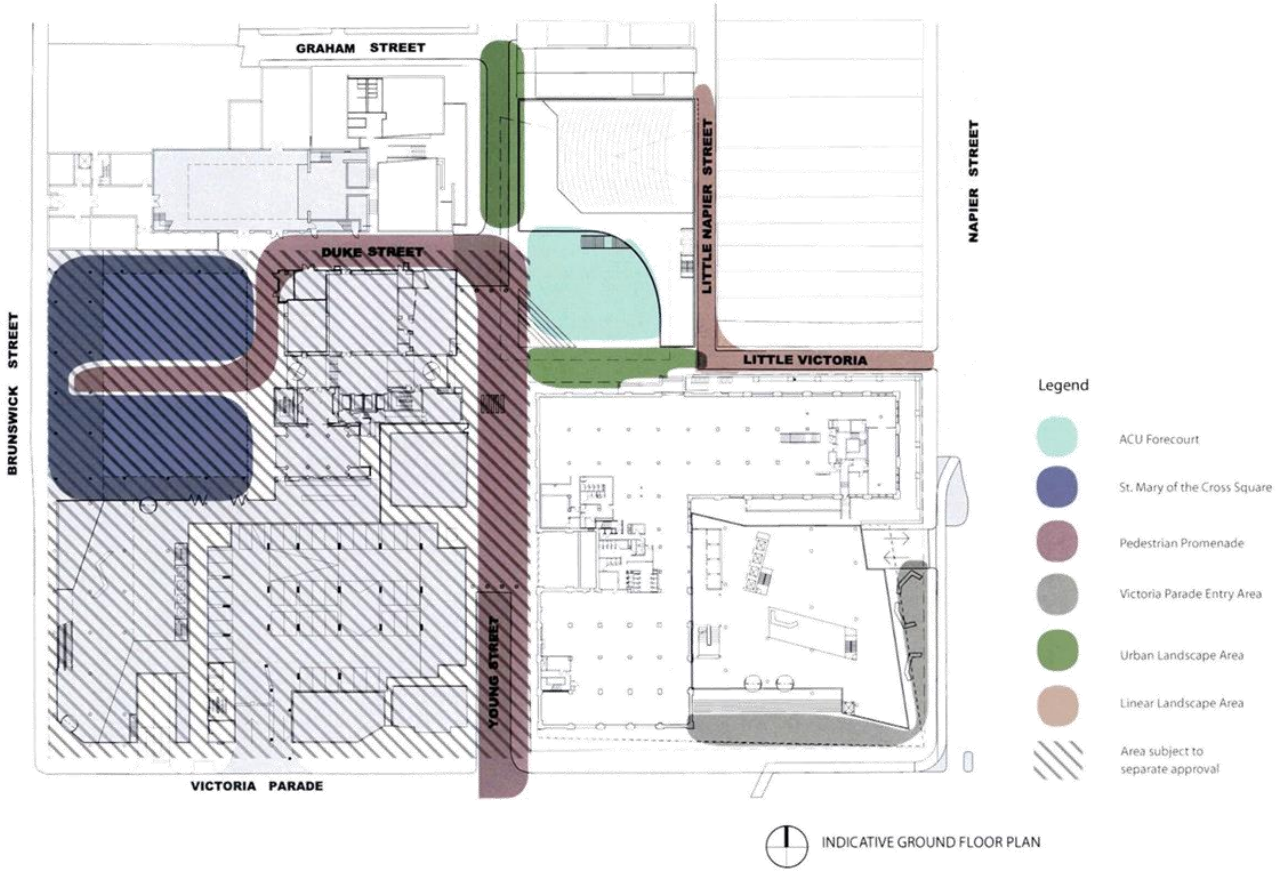
4.10.3 ANALYSIS PLAN 3 : INDICATIVE LANDSCAPE PLAN



INDICATIVE GROUND FLOOR PLAN

52 | ACU: ST PATRICK'S CAMPUS DEVELOPMENT PLAN

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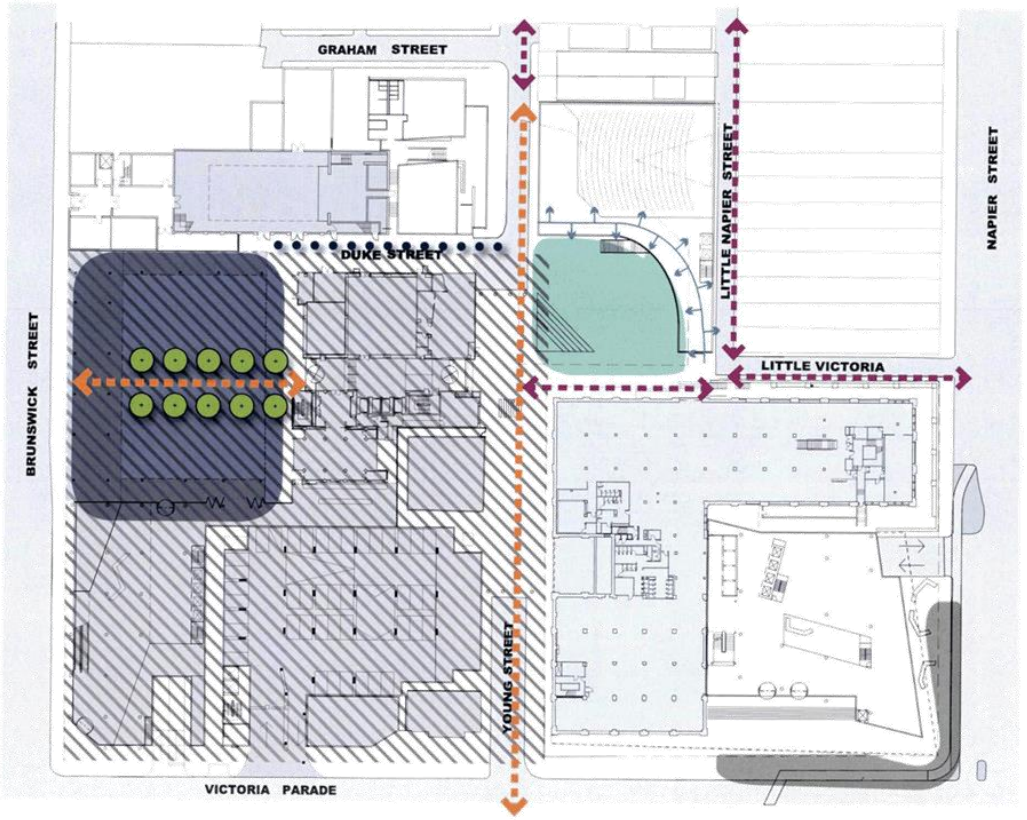
4.10.4 ANALYSIS PLAN 4 : OPEN SPACE AND LANDSCAPE FRAMEWORK PLAN



INDICATIVE GROUND FLOOR PLAN

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4.10.5 LANDSCAPE CHARACTER

ACU FORECOURT

Intent

- To provide for a high technology hub in a campus style environment supported by café precinct and distinctive landscaping.

Objectives

- To compliment the active edge with distinctive landscape features such as feature paving, urban art and thematic landscaping.
- To provide good, clear access from surrounding links.
- To provide views to the hub from the surrounding streets to reinforce its presence.

PEDESTRIAN PROMENADE

Intent

To provide for a clear way finding identity through the campus set in a well landscaped environment and prominent connections to the surrounding streets.

Objectives

- To provide distinctive landscape features such as feature paving and thematic vertical landscaping including deciduous canopy trees.
- To incorporate WSUD opportunities including raingardens.
- To provide good, clear access from surrounding links.
- Provide views to the hub from the surrounding streets to reinforce its presence

Attachment 7 - PLN16/0085 - Public Notification ACU Development Plan Part 6

VICTORIA PARADE ENTRY AREA

Intent

To enhance the existing entry to the campus from Victoria Parade

Objectives

- To provide integrated seating areas that caters for short stops/meeting points.
- To provide distinctive landscape features such as feature paving and deciduous canopy trees.

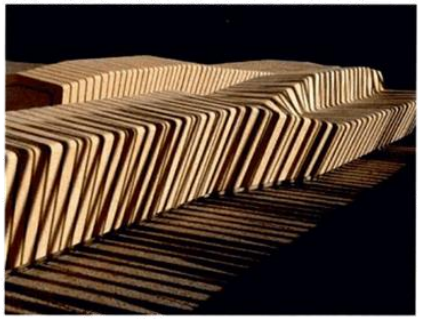
URBAN LANDSCAPE AREA

Intent

To provide for a clear way finding identity to and from the ACU Forecourt and Pedestrian Promenade set in a vertical landscaped environment.

Objectives

- To provide distinctive landscape features such as feature paving, vertical green walls and planter boxes.
- To provide good, clear access from surrounding links using signage and lighting.
- Provide views to the campus hub to reinforce its presence.



LINEAR LANDSCAPE AREA

Intent

- To provide for a clear way finding identity through the campus using urban art to create prominent connections to the surrounding streets.

Objectives

- To reinforce the campus identity using urban art elements such as vertical sculptures and colour
- To provide good, clear access from surrounding links

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4.10.6 INDICATIVE LANDSCAPE MASTER PLAN

Attachment 8 - PLN16/0085 - Public Notification ACU Development Plan Part 7

4.10.7 ACU FORECOURT / STUDENT HUB DETAIL PLAN

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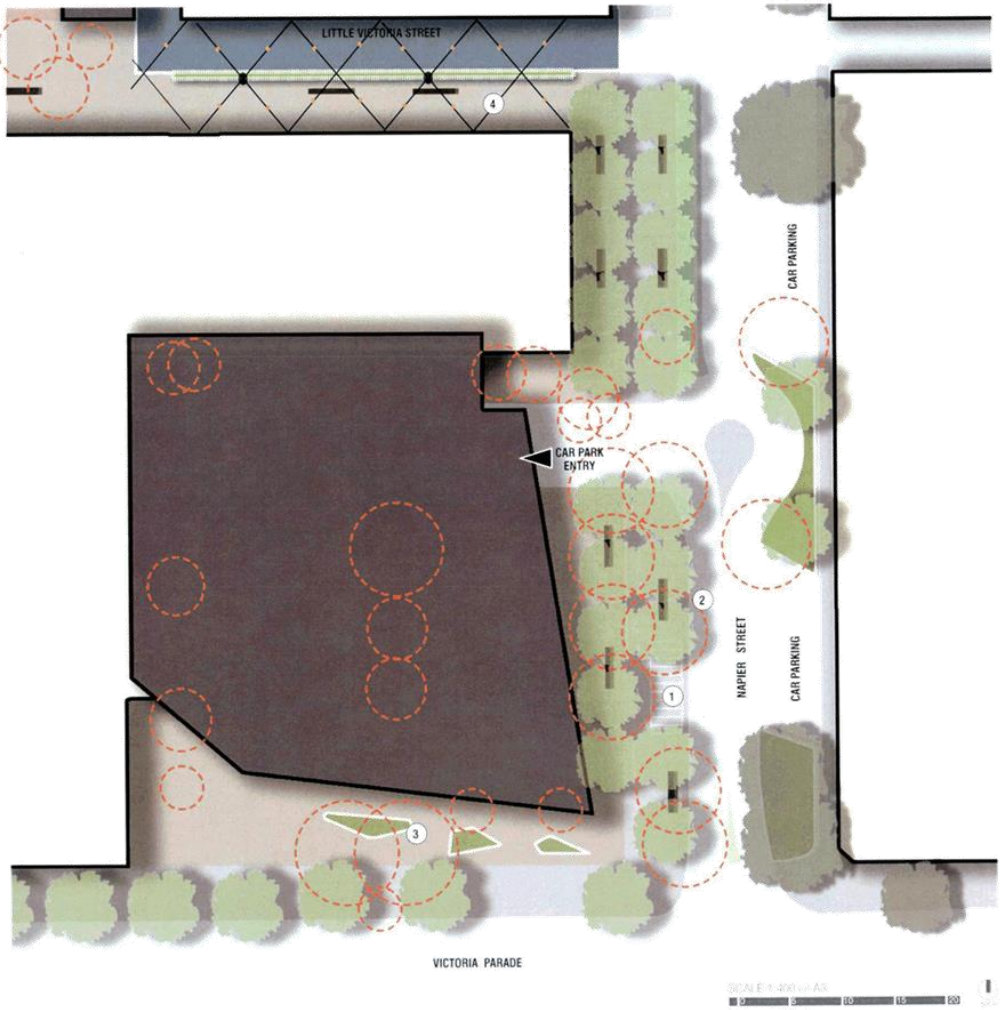

Legend

- Existing trees to be retained
- Proposed trees
- Proposed planting
- Bench Seats
- Trellis screen & flat sculptural elements
- Existing Trees to be removed
- Main building entries
- (1) Existing Bike Share Pod
- (2) Proposed Bike Lane
- (3) Raised Planters / Seating Pods
- (4) Laneway Lighting
- Area subject to separate approval

58 | ACU: ST PATRICK'S CAMPUS DEVELOPMENT PLAN

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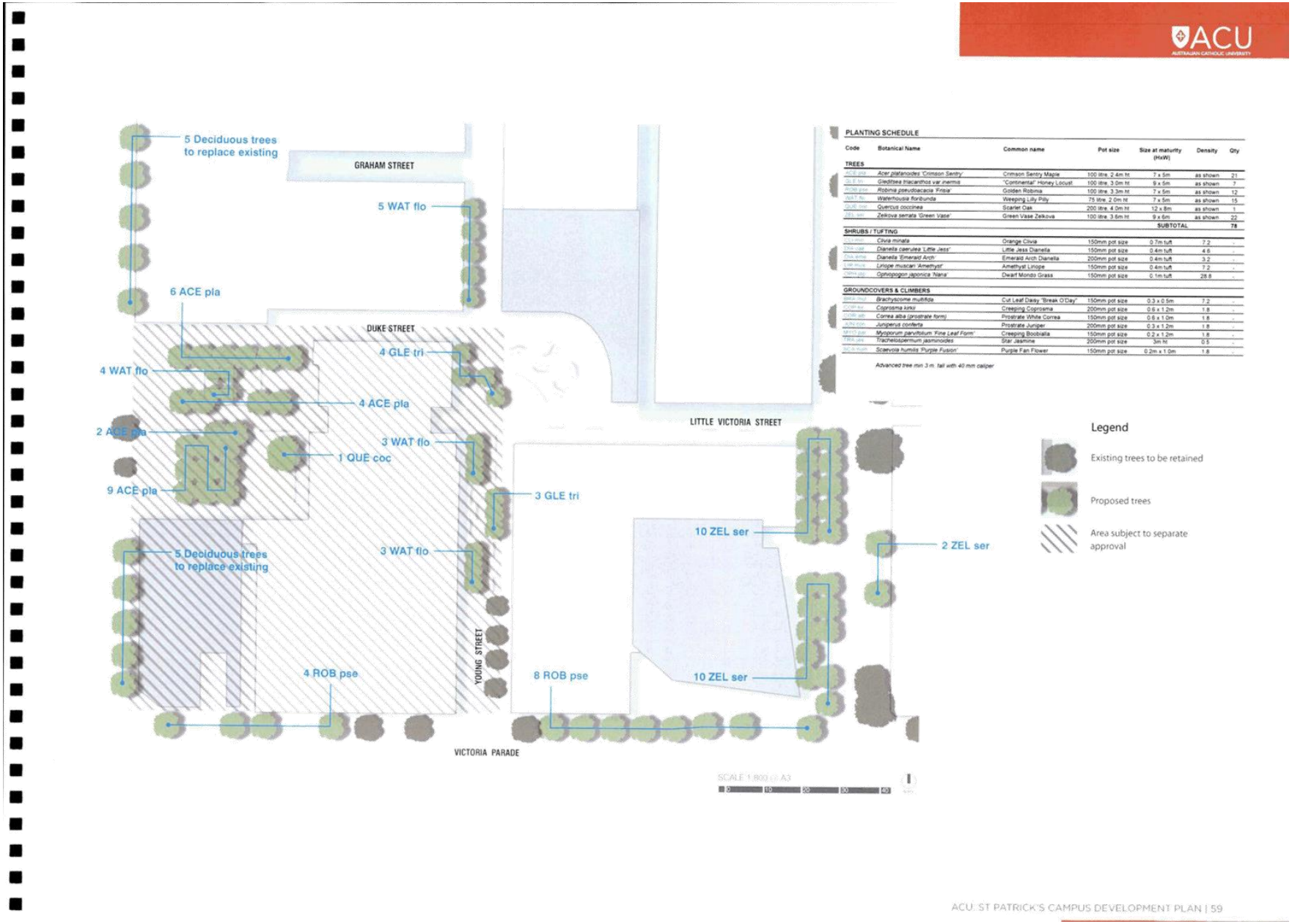

Attachment 8 - PLN16/0085 - Public Notification ACU Development Plan Part 7

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4.11 Heritage Matters

The heritage nature of particular built form both on campus and within the surrounding streetscape is an important consideration. Appropriate integration, design characteristics, and the interface to neighbouring heritage built form and streetscapes will be integral to the development.

The proposed building envelopes have been reviewed by heritage consultant Bryce Raworth who has provided the following assessment.

Bryce Raworth
conservation • heritage

4 December 2015

Re: *Australian Catholic University – Development Plan*

The following discussion has been prepared at the request of Urbis on behalf of the Australian Catholic University. It comments upon heritage considerations in relation to the proposed development plan for the St Patrick's Campus of the Australian Catholic University (ACU) in Fitzroy, and in particular sets out relevant heritage considerations that have informed the design and location of new works anticipated through the Development Plan. The Development Plan has been prepared to guide future planning permit applications for development at this campus, which is located in the block bounded by Brunswick Street, Napier Street, Victoria Parade and Gertrude Street.

The focus of future change is to be located at the Victoria Parade frontage of the site, away from residential properties further north. New buildings are anticipated at 115B Victoria Parade (corner of Napier Street) and 81-89 Victoria Parade (corner of Brunswick Street) – not part of the Development Plan Overlay. The Mary Glowrey building, the former Commonwealth Note and Stamp Printing department, will be refurbished and extended. The site of the current car park site at 28-42 Young Street will also be redeveloped.

This commentary draws upon inspections of the site and its surrounds and a review of relevant heritage studies and planning scheme documents including the City of Yarra's heritage policy at clause 22.02. In addition, regard has been had for the buildings and streetscapes adjacent this site, which is within the South Fitzroy Heritage Overlay precinct, HO334, and for the building gradings found in Council's heritage database.

The heritage overlay precinct that includes the subject land and the land to the north, east and west, the South Fitzroy precinct, is a key consideration in the development of the ACU land, and it is important that new built form acknowledge the sensitivity of the South Fitzroy precinct and nearby heritage buildings. Development of the ACU land is subject to the heritage overlay and will be assessed against the relevant heritage policies.

The statement of significance for HO334 in the *City of Yarra Heritage Review* (2007) states that:

The South Fitzroy Heritage Overlay Area is significant:

- *As the earliest urban area outside the Melbourne City grid to be settled in the Melbourne municipality, with several buildings from the mid nineteenth century surviving as testimony to its early establishment;*
- *For the unusually high number of early Victorian-era and some Regency period buildings, being generally simply detailed and a clear reflection of the early date of Fitzroy's settlement.*
- *As evidence of early government planning controls or Acts of Parliament, from the 1850s, that aimed to solve street alignment problems in this privately planned suburb, arising from a hitherto lack of co-ordination between neighbouring allotment owners;*
- *As a good example of the successful application of the Act for Regulating Buildings and Party Walls, and for Preventing Mischiefs by Fire in the City of Melbourne (Melbourne Building Act 1849), which forced the use of fireproof construction and gave South Fitzroy a character distinct from other inner suburbs such as Richmond and Collingwood, that have a greater proportion of Victorian-era timber buildings.*
- *As a substantially intact collection of predominantly mid to late nineteenth and early twentieth century building stock, interspersed with well preserved inter-war residential, commercial, retail and industrial buildings that contribute to the historical character of the area.*
- *For the relatively large number of individually significant buildings, being predominantly solid masonry rather than clad with timber, largely as a result of the Melbourne Building Act, 1849.*
- *For the ornate and exuberant detail of many late nineteenth and early twentieth century buildings in the suburb, reflecting the affluence of many of the inhabitants of this area, particularly in the late 19th century.*
- *For the early street, lane and allotment layouts, some original bluestone kerbs, paving and guttering, and some mature exotic street trees, providing an appropriate setting for this collection of residential, retail, commercial and industrial buildings.*
- *For the landmark qualities of some large factory and warehouse buildings from the late 19th and earlier 20th century, such as the MacRobertson confectionary complex which are significant features in the skyline of this predominantly low rise suburbs and*
- *For the major early institutions that developed on its fringes, in particular, St Vincent's Hospital and The Convent of Mercy, as closely linked with the area's history, education and welfare within the metropolitan area.*

The statement of significance for the former Commonwealth Note and Stamp Printing department at 115 Victoria Parade (now known as the Mary Glowrey building) is as follows:

What is significant?

The Commonwealth Note and Stamp Printing department … was created in 1929-1931 for the Australian Commonwealth Government and has close historical associations with that important function.

The place has a fair integrity to its creation date (now altered cosmetically, as conversion to university; car park at rear and side).

Fabric from the creation date at the Commonwealth Note and Stamp Printing department, former is locally significant within the City of Yarra, compared to other similar places from a similar era.

How is it significant?

The Commonwealth Note and Stamp Printing department, former at 115 Victoria Parade, Fitzroy is historically significant to the locality of Fitzroy and the City of Yarra.

Why is it significant?

The Commonwealth Note and Stamp Printing department is significant as the former origin of all legal tender in Australia, receiving extensive publicity on its completion and with suitably monumental Greek Revival styling.

It is to be noted that alterations to the former Commonwealth Note and Stamp Printing department building are somewhat more than cosmetic – this statement of significance predates a number of works that have been completed within the site, the additions to the southern facade and to the east elevation in particular. In addition, most of the windows have been replaced with simple modern glazing in place of the original steel framed windows.

Attachment 8 - PLN16/0085 - Public Notification ACU Development Plan Part 7

Yarra's heritage policy at Clause 22.02-5.7.1 provides detailed guidelines for new development and alterations and additions heritage overlay areas, with key policy including the following:

Encourage the design of new development and alterations and additions to a heritage place or a contributory element to a heritage place to:

- *Respect the pattern, rhythm, orientation to the street, spatial characteristics, fenestration, roof form, materials and heritage character of the surrounding historic streetscape.*
- *Be articulated and massed to correspond with the prevailing building form of the heritage place or contributory elements to the heritage place.*
- *Be visually recessive and not dominate the heritage place.*
- *Be distinguishable from the original historic fabric.*
- *Not remove, cover, damage or change original historic fabric.*
- *Not obscure views of principle facades.*
- *Consider the architectural integrity and context of the heritage place or contributory element.*

Encourage setbacks from the principal street frontage to be similar to those of adjoining contributory buildings; where there are differing adjoining setbacks, the greater setback will apply.

Encourage similar façade heights to the adjoining contributory elements in the street. Where there are differing façade heights, the design should adopt the lesser height.

Clause 22.02-5.7.2 encourages the following specific requirements with regard to corner sites and sites with dual frontages:

Encourage new building and additions on a site with frontages to two streets, being either a corner site or a site with dual street frontages, to respect the built form and character of the heritage place and adjoining or adjacent contributory elements to the heritage place.

Encourage new buildings on corner sites to reflect the setbacks of buildings that occupy other corners of the intersection.

This Clause also includes specific requirements for upper level additions to Industrial, Commercial and Retail Heritage Places or Contributory Elements:

Encourage new upper level additions and works to:

- *Respect the scale and form of the existing heritage place or contributory elements to the heritage place by being set back from the lower built form elements. Each higher element should be set further back from lower heritage built forms.*
- *Incorporate treatments which make them less apparent.*

The extent of development proposed under the Development Plan is substantial and will lead to appreciable change in the built form of the campus and associated change to views and character within adjoining streets. This said, the Development Plan seeks to realise this substantial change in such a manner as to achieve an acceptable level of compliance with the heritage policy.

The VCAT approval for a fourteen storey development at the corner of Brunswick Street and Victoria Parade, ie 81-89 Victoria Parade (VCAT reference no. P2540/2006), establishes a relevant benchmark for redevelopment along this part of Victoria Parade and within the ACU campus. While there are buildings along Victoria Parade within the context of the site that are of individual heritage significance, the streetscape to this part of Victoria Parade is of mixed character, is not of particular significance, and lends itself to higher development. The scale of current and proposed development to the west of ACU along Victoria Parade, at the St Vincent hospital sites, and the scale of buildings to the south side of Victoria Parade, also suggest that built form of substantial scale may be considered in the Victoria Parade context.

The Development Plan contemplates a new building of approximately 15 storeys on the carpark site at the corner of Victoria Parade and Napier Street, known as 115B Victoria Parade. This is to be attached to the east side of the former Commonwealth Note and Stamp Printing Department at 115 Victoria Parade. The new building is to provide a new major point of entry to the campus, and will utilize the lift and stair 'core' attached to the east side of the heritage building. The existing glazed entry and disabled access ramp addition to the front of 115 Victoria Parade will be removed and that part of the facade reinstated.

The residential streetscapes to Napier Street to the east and north of the site at 115B Victoria Parade include heritage buildings of one and two storeys, with two storeys being predominant. It is a fine heritage street, particularly to the north of ACU. On this basis it is appropriate that new built form be very carefully designed to limit impact upon the character, appearance and significance of the streetscapes to Napier Street.

Young Street, located more centrally in relation to the ACU land holdings, is less significant at its southern end, already containing built form of scale, but includes a heritage building at the Victoria Parade corner, west side, and the former Commonwealth Note and Stamp Printing Department on the east corner.

The development concept for 115B Victoria Parade proposes to:

- Remove the existing glazed entry and disabled access ramp from the front facade of 115 Victoria Parade.
- Consolidate the entry for both 115 and 117 Victoria Parade to create an active and prominent entry.
- Angle the entry and south elevation to 115B Victoria Parade back from the alignment of 115 Victoria Parade in order to reveal the robust corner detailing of 115 Victoria Parade. This corner will remain visible in views from Victoria Parade, with the angled form of the new works providing a 'setback' against the heritage building.
- Use a similar strategy to provide views to the eastern wing of the Mary Glowrey building (former Commonwealth Note and Stamp Printing Department).
- Integrate the floor plans of 115 Victoria Parade and 115B Victoria Parade, the new building being attached to a side of 115 Victoria Parade that has already been the subject of considerable modification and additions.
- The new built form will in part overhang 115 Victoria Parade, albeit with a degree of vertical separation that will enable the two forms to read separately.
- The building adopts a stepped profile to create a height relationship with the built form along Napier Street.
- An addition of three levels to the rear part of the northern wing of the Mary Glowrey building (former Commonwealth Note and Stamp Printing Department), well set back from Napier Street in accordance with heritage policy for upper level additions to industrial buildings.

The existing car park building at 28-42 Young Street will be demolished and replaced with a low rise building that incorporates an open plaza or community space at ground level. The development of this part of the campus is not seen to raise heritage issues insofar as it is in a highly altered environment with negligible heritage fabric to its streetscape.

Having regard for the Development Plan, it is apparent that the anticipated works will result in substantial change and to some extent impacts upon heritage values and views, relative to both the former Commonwealth Note and Stamp Printing Department building and the streetscapes extending to the north along Napier Street and to the east along Victoria Parade. This said, the Development Plan seeks to limit these impacts through focusing change within the already altered environment of Victoria Parade, at the very southern edge of the South Fitzroy precinct.

The associated impacts upon the setting of the former Commonwealth Note and Stamp Printing Department and upon views within Napier Street and Victoria Parade are within the range that can reasonably be considered having regard for the location at Victoria Parade and taking a whole of planning view that accepts the importance of ACU within this precinct and within the City of Yarra. In modelling the form and scale of the built form anticipated by the Development Plan, an attempt has been made to limit impacts upon buildings, views and the broader heritage overlay precinct, while still seeking to achieve ACU's requirements.

Bryce Raworth and Carolynne Baker

ACU
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4.12 Possible ESD Principles

The following principles have been developed by Arup consulting.

ACU promotes a holistic view of sustainability and embraces the need to be a leader, a good neighbour and a global citizen. The following principles summarise the way that ACU and their project team will progress the projects within the Development Plan.

PURSUING THE COMMON GOOD

Part of ACU's mission is to pursue the common good. Within the context of this Development Plan, this is translated as the following:

- Demonstrating environmental stewardship through the application of recognised frameworks, such as the City of Yarra's Built Environment Sustainability Scorecard (BESS) and Green Star.
- Being a place-maker by providing spaces for community use
- Protecting heritage value through considered and sensitive design

ENHANCING USER EXPERIENCE

The success of education buildings relies in part on the user experience they provide. In this case, users includes students, academic and administrative staff, and the community. Research has shown that our local environment can influence our health, wellbeing and productivity. As such, the Development Plan will enhance the user experience through:

- High quality daylight and artificial lighting
- Good indoor air quality
- Good acoustics
- High levels of thermal comfort
- Desirable external spaces and microclimates (wind, rain, sun, heat island etc)
- Opportunities to connect to nature
- Cyclist facilities

PROGRESSING WITH CONFIDENCE

We live in an uncertain world, with a changing climate, increasing rate of change in pedagogy and student needs, and rising utility prices. Through design and project processes such as those listed below, the projects within the Development Plan will give ACU, the City of Yarra and surrounding community confidence.

- Design for future climate
- Flexible and adaptable buildings and spaces
- Efficient consumption, onsite generation of energy, and capture and reuse of water
- Quality control in design, construction and operation (e.g. planning and building code compliance, as-built green star rating, commissioning, tuning, metering and monitoring)

SAVING MONEY

Sustainability initiatives provide an opportunity for ACU to save money upfront and in the long term. The projects within the Development Plan will be vigilant for opportunities to reduce costs, enabling ACU to focus its spending in higher value areas.

- Energy efficiency leading to smaller plant (chiller, substation etc) size
- Durable materials and finishes
- Appropriate access for maintenance
- Efficient consumption, onsite generation of energy, and capture and reuse of water

BESS CATEGORY	CREDIT	CREDIT SUMMARY	DEVELOPMENT PLAN RESPONSE
Management	Pre-application meeting		To be part of building design stage.
	Thermal performance modelling	Preliminary Section J glazing assessment undertaken.	To be part of building design stage.
	Building users guide	A building users' guide be produced and issued to occupants.	To be part of building design stage.
Energy	Greenhouse Gas Emissions	Reduction in emissions compared to a code compliant reference building	To be part of building design stage. The development plan notes that appropriate solar controls, such as external shading and screens, expect to be added to the façade as part of design optimisation.
	Peak Demand	Has the instantaneous (peak-hour) demand been reduced by >25%	As for Greenhouse Gas Emissions above.
	Energy consumption (Electricity and Gas)	Is the annual electricity consumption >10% below the benchmark Is the annual gas consumption >10% below the benchmark?	As for Greenhouse Gas Emissions above.
	Car Park Ventilation	If you have a basement carpark, is it either: (a) fully naturally ventilated (no mechanical ventilation system), or (b) use Carbon Monoxide monitoring to control the operation and speed of the ventilation fans?	To be part of building design stage. Car park ventilation will use carbon monoxide control.
	Domestic Hot Water	Does the hot water system use >10% less energy (gas and electricity) than the reference case?	To be part of building design stage.
	External lighting	The external lighting is controlled by a motion detector.	To be part of building design stage.
	Internal lighting	The maximum illumination power density (W/m2) in at least 90% of the relevant Building Class is at least 20% lower than required by Table J6.2a of the NCC BCA (2013) Volume 1 Section J (Class 2 to 9).	To be part of building design stage.
Water	Rainwater capture and reuse.	Installation of rainwater tanks and reticulation of captured rainwater for reuse (e.g. toilet flushing, irrigation).	To be part of building design stage. Space for rain water storage has been allowed for within the basement as part of the building massing.
	Fixtures and fittings	WELS rating of water fixtures, fittings and connections.	To be part of building design stage.
	Water efficient landscaping	Water efficient landscaping is installed.	To be part of building design stage.
	Fire test system water	Measures have been taken to reduce water consumption when testing fire safety systems.	To be part of building design stage. Space for capture and reuse of fire system test water has been allowed for within the basement as part of the building massing.
Stormwater	Stormwater treatment	Best practice stormwater management is demonstrated.	As for Rainwater capture and reuse above. The overflow from the rainwater tank will have appropriate treatment to improve the quality of discharged stormwater quality.
IEQ	Daylight access - non-residential	Percentage of the nominated area has at least 2% daylight factor.	Arup can provide a desktop assessment as part of Development Plan

Attachment 8 - PLN16/0085 - Public Notification ACU Development Plan Part 7

BESS CATEGORY	CREDIT	CREDIT SUMMARY	DEVELOPMENT PLAN RESPONSE
Transport	Bicycle parking – non-residential	The planning scheme requirements for employee bicycle parking been exceeded by at least 50%.	Need to check with Cardno
	Bicycle parking – non-residential visitor	The planning scheme requirements for visitor bicycle parking been exceeded by at least 50%.	Need to check with Cardno
	End of trip facilities - non-residential	End of trip facilities provided with: 1 shower for the first 5 bicycle spaces plus 1 for each 10 bicycles spaces thereafter. Changing facilities adjacent to showers. One secure locker per required bicycle space in the vicinity of the changing / shower facilities.	Need to check with Lyons / ACU about whether this can be incorporated within the existing spatial allowances.
	Electric vehicle infrastructure	Facilities for the charging of electric vehicles are provided.	Need to check with Cardno
	Car share scheme	A formal car sharing scheme has been integrated into the development.	Need to check with Cardno
	Motorbikes/mopeds	A minimum of 5% of vehicle parking spaces are designed and labelled for motorbikes (must be at least one motorbike space).	Need to check with Cardno
Waste	Construction waste management	There is a commitment to re-use and recycle construction & demolition waste.	To be part of building design stage.
	Building re-use	If the development is on a site that has been previously developed, at least 30% of the existing building has been re-used.	Existing building is being retained in full.
	Food & garden waste	Facilities are provided for on-site management of food and garden waste.	To be considered as part of building design stage.
	Convenience of recycling	The recycling facilities are at least as convenient for occupants as facilities for general waste.	Spatial allowance and loading dock access should be considered as part of Development Plan
Urban Ecology	Communal spaces	There is at least the following amount of common space (m2): 1m² for each of the first 50 occupants, an additional 0.5m² for each occupant between 51 and 250, and an additional 0.25m² for each occupant above 251.	To be part of building design stage.
	Vegetation	Percentage of the ground area of the site that is covered with vegetation.	Need to check with Lyons / ACU
	Green roof	The development incorporates a green roof.	Need to confirm what we want to say regarding consideration of green roof for car park.
	Green wall or façade	The development incorporates a green wall or façade.	To be considered as part of building design stage.
	Food production - non-residential	There is at least 0.25m² of space per occupant dedicated to food production.	To be considered as part of building design stage.

Attachment 8 - PLN16/0085 - Public Notification ACU Development Plan Part 7

5.0 CONCLUSION

The Development Plan has been developed in line with ACUs Vision for the St Patrick's Campus and will guide key developments within the Development Plan Overlay area for the coming decade.

The Development Plan will guide development of the parts of ACU subject to the Development Plan Overlay in accordance with the Yarra Planning Scheme.

Through the Development Plan the projects will improve the public realm through providing new landscaping opportunities, open space and pedestrian connectivity through the campus. The new building projects will continue ACUs tradition of providing excellent teaching and learning spaces.

Attachment 8 - PLN16/0085 - Public Notification ACU Development Plan Part 7

Attachment 9 - PLN16/0085 - Public Notification ACU Development Plan Part 8

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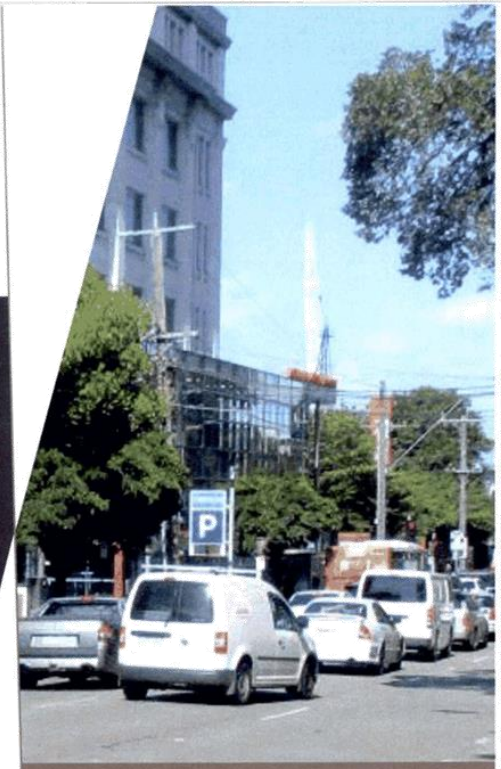
Integrated Transport and Access Plan

Australian Catholic University, St. Patricks Campus, Fitzroy



Prepared for Australian Catholic University
16 March 2016

66 | ACU: ST PATRICK'S CAMPUS DEVELOPMENT PLAN

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Attachment 9 - PLN16/0085 - Public Notification ACU Development Plan Part 8

Table of Contents

1	Introduction	5
1.1	Background	5
1.2	Purpose	6
2	Existing Conditions	7
2.1	Population and Employment	7
2.2	Mode Share and Journey to Work	7
2.3	Pedestrian Network	8
2.4	Bicycle Network	10
2.5	Public Transport	11
2.6	Traffic Network	14
2.7	Road Network Safety	21
2.8	Car Parking	22
3	State and Local Policies	23
3.1	State Policies	23
3.2	Local Policy	25
4	Development Proposal	27
4.1	Development Plan	27
4.2	Individual Projects	27
5	Transport Network Interventions	28
5.1	Pedestrian Network	28
5.2	Bicycle Network	28
5.3	Local Area Traffic Management	29
6	Statutory Considerations	30
6.1	Schedule 2 to the Development Plan Overlay	30
6.2	Design Response	30
6.3	Car Parking - Clause 52.06	32
6.4	Bicycle Parking – Clause 52.34	32
7	Traffic Impact	34
7.1	Projected Traffic Volumes	34
7.2	Traffic Distribution	36
7.3	Post Development Traffic Flows	36
7.4	Intersection Analysis	37
8	Green Travel Initiatives	38
8.1	Preamble	38
8.2	Objective	38
8.3	Action Plan (Guide Only)	38
8.4	Monitoring and Review	39
9	Conclusions	41

Appendices

Appendix A Concept Functional Layout Plans

Appendix B Bicycle Parking Products

Attachment 9 - PLN16/0085 - Public Notification ACU Development Plan Part 8

1 Introduction

1.1 Background

A Development Plan is being prepared for the Australian Catholic University (ACU) St Patrick's Campus to deliver a mixed use development to accommodate the planned growth in students, teachers and research activities, improve the quality of teaching and learning environments and improve the pedestrian and public environments for students and the wider community.

To accompany the Development Plan, Cardno was engaged by Australian Catholic University to prepare an Integrated Transport and Access Plan (ITAP). The subject site comprises a number of buildings generally located to the north of Victoria Street between Brunswick Street and Napier Street, as shown in Figure 1-1.

The site is located within the suburb of Fitzroy and is situated approximately 500 metres to the east of the Melbourne Central Activities District (CAD).

Figure 1-1 Site Location

Schedule 2 to the Development Plan Overlay (DPO) (or Schedule 2 to Clause 43.04 of the Yarra Planning Scheme) applies to the subject site and provides criteria for any future use and development of the land at 115 Victoria Parade and 20-23 Brunswick Street, Fitzroy.

Schedule 2 to the DPO is dated 19 January 2006 and provides the following requirements for the Development Plan which are considered relevant to transport:

> The development plan must show:

- The number, location, dimensions and layout of all car parks and access ways to and from them;
- A management plan for the operation and maintenance of the car park areas;
- The location and dimensions of all bicycle, vehicle and pedestrian ways;
- A traffic management plan which must show any traffic management and traffic control works considered necessary in adjoining and nearby roads when the development is completed; and
- The means of vehicular and pedestrian ingress to and egress from the land.

1.2 Purpose

The purpose of this report is to provide an integrated transport approach to any future use and development at ACU.

This report considers all modes of transport, with a view to updating and incorporating a more integrated transport approach to the transport related requirements in Schedule 2 to the DPO.

Attachment 9 - PLN16/0085 - Public Notification ACU Development Plan Part 8

2 Existing Conditions

2.1 Population and Employment

ACU currently operates with 10,000 effective full time (EFT) students and approximately 800 staff.

2.2 Mode Share and Journey to Work

Relevant journey to work and journey to education data has been sourced from the 2011 ABS Census and the Victorian Integrated Survey of Travel and Activity (VISTA) which is an ongoing survey of travel and activity that helps the government make better transport and land-use planning decisions.

Table 2-1 shows journey to work data based on work destinations for the suburb of Fitzroy from the 2011 ABS Census and for the City of Yarra from the VISTA 09 surveys.

Table 2-1 Journey to Work Data Based on Work Destination

Travel Mode	2011 ABS Census Suburb of Fitzroy	VISTA 09 City of Yarra
Car Based	53%	57%
Public Transport	32%	30%
Bicycle/Walking	15%	13%

Table 2-2 shows journey to education data based on education destinations for RMIT in Melbourne and the Kangan Institute of TAFE in Cremorne. The results are noticeably different, with significantly higher public transport use at RMIT and higher car and bicycle/walking use at Kangan Institute of TAFE.

Table 2-2 Journey to Education Data Based on Education Destination

Travel Mode	VISTA09 RMIT, Melbourne City of Melbourne	VISTA 09 Kangan Institute of TAFE, Cremorne City of Yarra
Car Based	10%	37%
Public Transport	67%	30%
Bicycle/Walking	23%	33%

The site is more similarly located to RMIT with respect to its public transport accessibility and proximity to the CAD, however on-site parking at ACU is more readily available.

In this respect, it is anticipated that car use would be in between the results for RMIT and Kangan Institute of TAFE.

This is confirmed by results from a questionnaire survey that was distributed by ACU staff in May and August 2009.

Approximately 800 surveys were completed and returned, with the results of the survey summarised in Table 2-3, Table 2-4 and Table 2-5 overleaf.

The data collected by the University reveals that a large contingent of the existing student and staff population either catch public transport or walk or cycle to the facility.

Table 2-3 Student Travel Modes

Travel Mode	Number	Percentage
Car (Driver)	122	19%
Car (Passenger)	38	6%
Public Transport	445	68%
Walked	31	5%
Cycled	17	2%
Total	653	100%

Table 2-4 Staff Travel Modes

Travel Mode	Number	Percentage
Car (Driver)	62	34%
Car (Passenger)	12	7%
Public Transport	80	44%
Walked	15	8%
Cycled	8	4%
Other	3	2%
Total	180	100%

Table 2-5 University Travel Modes

Travel Mode	Number	Percentage
Car (Driver)	184	22%
Car (Passenger)	50	6%
Public Transport	525	63%
Walked	46	6%
Cycled	25	3%
Other	3	0%
Total	833	100%

2.3 Pedestrian Network

The existing pedestrian network in the vicinity of the site includes footpaths on both sides of Brunswick Street, Young Street and Napier Street providing access towards Gertrude Street and Victoria Street to the north and south respectively, both of which are also provided with footpaths along both sides.

Victoria Street is also provided with footpaths along both sides of the central median.

Pedestrian crossings are provided at all signalised intersections, including:

> Brunswick Street / Gertrude Street;

> Brunswick Street / Victoria Street / Gisborne Street; and

> Victoria Street / Lansdowne Street.

At all other intersections, pedestrian crossings are facilitated by pram ramps.

Tram stops associated with tram routes 11, 30, 86, 109 and 120 are readily accessible from the subject site via the pedestrian network, whilst Parliament Train Station is also readily accessible via footpaths along both sides of Gisborne Street.

The pedestrian network in the vicinity of the site also provides access towards neighbouring suburbs, including Collingwood, Fitzroy North, Carlton, East Melbourne and the Central Activities District (CAD).

Key pedestrian links in the vicinity of the site is shown graphically in Figure 2-1, whilst Figure 2-2 shows the pedestrian links within the campus and their connections to the key pedestrian links.

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Figure 2-1 Key Pedestrian Links

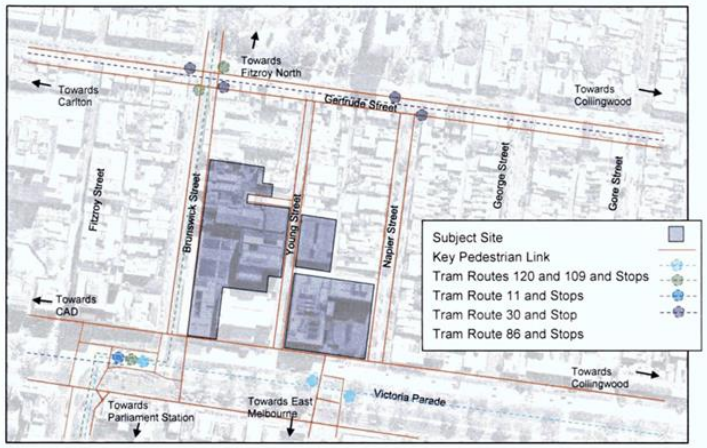

Figure 2-2 Campus Pedestrian Links

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2.4 Bicycle Network

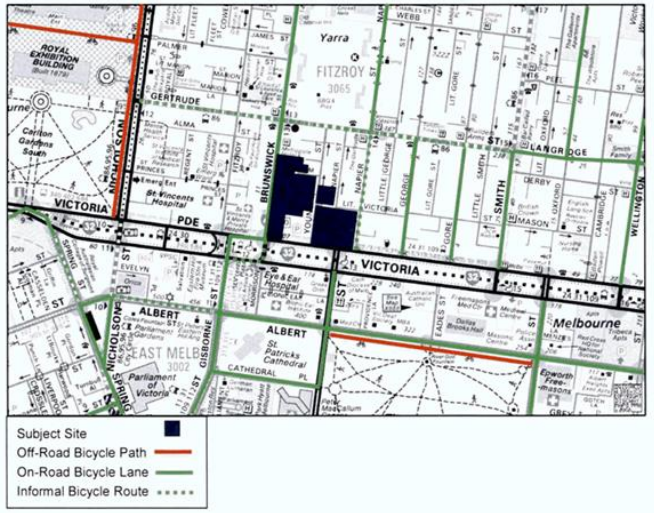
The subject site is well serviced by Melbourne's extensive bicycle network, with numerous off-road paths, on-road bicycle lanes and informal bicycle routes (generally designated by painted bicycle symbols and the provision of bicycle storage boxes at signalised intersections).

As shown in Figure 2-3, on-road bicycle lanes on Brunswick Street provide access towards Fitzroy North to the north and towards the CAD to the south via Gisborne Street.

The informal route along Gertrude Street provides access towards Collingwood to the east and towards Carlton to the west via the off-road path between the Royal Exhibition Building and the Melbourne Museum.

Additional on-road bicycle lanes along key streets, including Napier Street, Albert Street, Clarendon Street and Wellington Street provide further connections to surrounding suburbs, including East Melbourne and Richmond.

Figure 2-3 Existing Bicycle Network



It is noted that to improve safety for cyclists that traverse between Napier Street and Lansdowne Street, new traffic signals (yet to be commissioned) have been installed on the departure side (eastbound) of the Victoria Parade / Lansdowne Street intersection.

Council have also recently installed bike share pods along Napier Street, proximate to its intersection with Victoria Parade, and we understand that they also plan to install an additional bike share pod at the Fitzroy Town Hall.

Attachment 9 - PLN16/0085 - Public Notification ACU Development Plan Part 8

2.5 Public Transport

The subject site has excellent access to public transport, with all train lines accessible via Parliament Station approximately 550 metres walking distance from the subject site and numerous bus and tram routes operating either along the site frontage or in close proximity to the subject site.

The St Vincents Plaza / Victoria Parade tram stop and the Australian Catholic University / Victoria Parade bus stop are located directly adjacent to the site and provide access to a total of 4 tram routes and 11 bus routes respectively. Parliament Station is also accessible via tram routes 11, 12 and 109 which are serviced by the abovementioned St Vincents Plaza / Victoria Parade tram stop.

Tram lines along Victoria Street, Nicholson Street and Gisborne Street are separated from the traffic lanes, whilst the tram lines along Gertrude Street, Brunswick Street and Smith Street are shared with a traffic lane.

Notably, bus lanes have recently been installed along Victoria Parade to give buses priority through this corridor. It is also understood that plans are being progressed to upgrade the Brunswick Street tram corridor, with the works including the provision of DDA compliant stops.

The public transport services in the vicinity of the site are summarised in Table 2-6 and illustrated graphically in Figure 2-4.

Figure 2-4 Public Transport Map

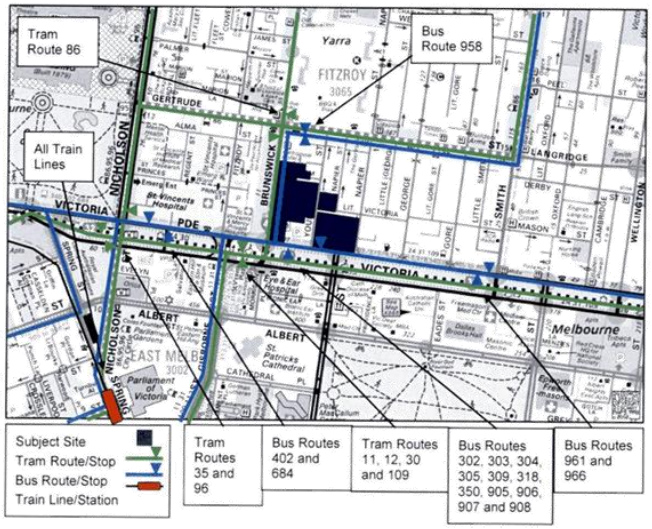


Table 2-6 Public Transport Services

Service	Route No's	Route	Nearest Stop	Approximate Walking Distance
Tram	11	West Preston – Victoria Harbour Docklands	St Vincents Plaza/Victoria Pde	Adjacent Site
	12	Victoria Gardens – St Kilda		
	30	St Vincents Plaza – Docklands via La Trobe St		
	109	Box Hill – Port Melbourne		
	86	Bundoora RMIT – Waterfront City Docklands	Brunswick St/Gertrude St	100 m
	35	City Circle (Free Tourist Tram)	Nicholson St/Victoria Pde	300 m
	96	East Brunswick – St Kilda Beach		
Bus	302	Box Hill via Belmore Rd and Eastern Fwy	Australian Catholic University / Victoria Pde	Adjacent Site
	303	City – Ringwood North via Park Rd		
	304	City – Doncaster SC via Belmore Rd and Eastern Fwy		
	305	City – The Pines SC via Eastern Fwy		
	309	City – Donvale via Reynolds Rd		
	318	City – Deep Creek		
	350	City – La Trobe University via Eastern Fwy		
	905	City – The Pines SC via Eastern Fwy, Templestowe (SMARTBUS Service)		
	906	City – Warrandyte via The Pines SC (SMARTBUS Service)		
	907	City – Mitcham via Doncaster Road (SMARTBUS Service)		
	908	City – The Pines SC via Eastern Fwy (SMARTBUS Service)		
	402	Footscray – East Melbourne via North Melbourne	St Vincents Hospital / Victoria Pde	200 m
	684	Eildon – Melbourne via Lilydale Station		
	958	NightRider – City – Eltham via Smith Street, Darebin Road	Brunswick St / Gertrude St	100 m
	961	NightRider – City – Doncaster via Eastern Freeway, James Street	Smith St / Victoria Pde	300 m
	966	NightRider – City – Croydon – Lilydale via Victoria Street, Maroondah Highway		
Train	All Train Lines		Parliament Station	550 m

A consolidated public transport and bicycle network plan is provided as Figure 2-5 overleaf.

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Figure 2-5 Public Transport and Bicycle Network

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2.6 Traffic Network

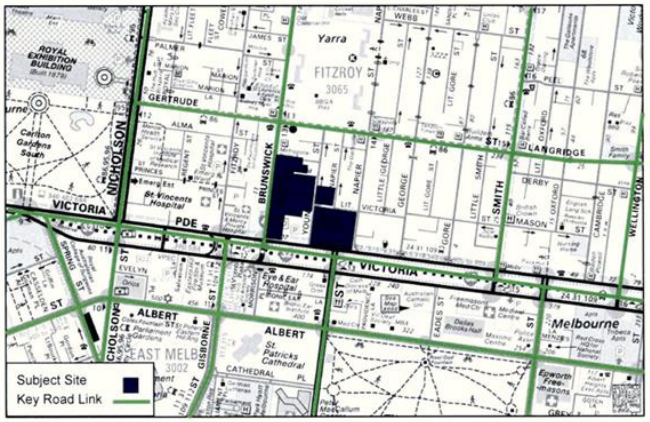
2.6.1 General

The subject site is primarily accessible via Victoria Parade and Young Street. Various additional crossovers and roller doors provide secondary access, generally for loading and other authorised vehicles.

The key road links in the vicinity of the site are shown in Figure 2-6, providing access to surrounding suburbs including Collingwood and Richmond to the east, Fitzroy North to the north, Carlton and the CAD to the west and East Melbourne to the south.

Both the Eastern Freeway and CityLink are readily accessible from Hoddle Street via Victoria Parade. A detailed review of the streets in the immediate vicinity of the site is provided in the following sections.

Figure 2-6 Key Road Links in the Vicinity of the Site



2.6.2 Brunswick Street

Brunswick Street is generally aligned in a north-south direction from St Georges Road in Fitzroy North to Victoria Parade in Fitzroy. North of Alexandra Parade, Brunswick Street is a Declared Main Road. ACU is located on the east side of Brunswick Street.

Adjacent the subject site, Brunswick Street operates with two lanes in each direction, with the central lanes shared between tram lines and traffic and the kerbside lanes generally operate as shared parking and bicycle lanes, with the exception of between 7:00am – 9:00am Monday to Friday when 'No Stopping' restrictions apply on the eastern kerb.

Both the Brunswick Street / Gertrude Street and Brunswick Street / Victoria Parade intersections are controlled by traffic signals.

In the vicinity of the site, Brunswick Street operates with a posted speed limit of 40 kilometres per hour. A view of Brunswick Street in the vicinity of the site is provided in Figure 2-7.

Attachment 10 - PLN16/0085 - Public Notification ACU Development Plan Part 9

Figure 2-7 Cross Section of Brunswick Street in the Vicinity of the Site

2.6.3 Young Street

Young Street is a local road aligned in a north-south direction from Gertrude Street to Victoria Parade. Young Street primarily serves an access function, with speed humps used along its length for traffic calming purposes. ACU is located on both the east and west sides of Young Street, with vehicle access provided to an ACU car park and an ACU private road named Duke Street.

Along its length, Young Street operates with a varying cross section. Between Victoria Parade and the entrance to the ACU car park, Young Street operates with a single traffic lane in each direction, albeit a kerbside waiting zone is proposed along the western kerbside to accommodate vehicles waiting to access the deck car park during peak periods. This section of Young Street operates with a posted speed limit of 40 kilometres per hour. Beyond this point, Young Street operates as one way, northbound only.

Between the Little Victoria Street and Duke Street, Young Street operates as a shared zone, within a carriageway which allows for a single lane of northbound traffic. As required for shared zones, this section of Young Street operates with a speed limit of 10 kilometres per hour. There is generally no provision for kerbside parking, with the exception of a single indented parallel disabled car space.

Between Duke Street and Graham Street, Young Street operates with a single northbound traffic lane, with no provision for kerbside parking. North of the shared zone, Young Street operates with a default speed limit of 50 kilometres per hour.

Between Graham Street and Gertrude Street, Young Street allows for a single lane of northbound traffic, with kerbside parallel parking permitted on the west kerb clear of traffic.

The Young Street / Victoria Parade intersection operates as a left in/left out 'Give Way' controlled T-intersection, with priority given to Victoria Parade.

The Young Street / Gertrude Street intersection operates as a 'Stop' controlled T-intersection, with priority given to Gertrude Street and allows for outbound movements out of Young Street only. Right turns are banned between 7:00am – 9:00am Monday to Friday.

The various cross sections of Young Street are shown in Figure 2-8 through to Figure 2-11.

Figure 2-8 Cross Section of Young Street Between Victoria Parade and ACU Car Park Entrance

Figure 2-9 Cross Section of Young Street Between ACU Car Park Entrance and Duke Street

Attachment 10 - PLN16/0085 - Public Notification ACU Development Plan Part 9

Figure 2-10 Cross Section of Young Street Between Duke Street and Graham Street

Figure 2-11 Cross Section of Young Street Between Graham Street and Gertrude Street

2.6.4 Napier Street

Napier Street is a local road aligned in a north-south direction from Cecil Street to Victoria Parade. Various traffic calming treatments are used along the length of Napier Street, including speed humps and no through roads. ACU is located on the west side of Napier Street. No vehicle access to ACU is provided via Napier Street.

In the vicinity of the site, Napier Street allows for a single lane of traffic in each direction, with angle parking permitted alongside one kerb and parallel parking permitted on the other kerb and a bicycle symbol identifying Napier Street in the vicinity of the site as an informal bicycle route.

The Napier Street / Gertrude Street intersection operates as a 'Stop' controlled cross intersection, with priority given to Gertrude Street. Right turns out of the southern Napier Street approach are not permitted between 7:30am – 9:00am Monday to Friday.

The Napier Street / Victoria Parade intersection is a left out only 'Stop' controlled intersection, with priority given to Victoria Parade. Inbound movements into Napier Street are not permitted.

In the vicinity of the site, Napier Street operates with a posted speed limit of 40 kilometres per hour.

A view of Napier Street in the vicinity of the site is provided in Figure 2-12.

Figure 2-12 Cross Section of Napier Street in the Vicinity of the Site

2.6.5 Victoria Parade

Victoria Parade is a Declared Main Road which extends generally in an east-west direction from Hoddle Street to La Trobe Street. At both ends, Victoria Parade continues as Victoria Street. ACU is located on the north side of Victoria Parade, with access provided to an on-site car park.

In the vicinity of the site, Victoria Parade operates with three traffic lanes and a bus lane in each direction, separated by a wide central median which accommodates tram lines, tram stops and footpaths. Left and right turn deceleration lanes are provided at key intersections.

Along its length, parallel parking is intermittently permitted on both the north and south kerbs and as well as both kerbs of the central median, however in the vicinity of the site, parking is only permitted on the south kerb.

Victoria Parade operates with a posted speed limit of 60 kilometres per hour. A view of Victoria Parade in the vicinity of the site is provided in Figure 2-13.

Figure 2-13 View of Victoria Parade in the Vicinity of the Site

Attachment 10 - PLN16/0085 - Public Notification ACU Development Plan Part 9

2.6.6 Little Victoria Street

Little Victoria Street is a right of way (ROW) which extends in an east-west direction from George Street to Little Napier Street. Little Victoria Street terminates at the eastern boundary of ACU, however does not provide vehicle access to the ACU.

In the vicinity of the site, Little Victoria Street operates as one way westbound only within a single traffic lane and provides access to Little Napier Street which services dwellings fronting Napier Street. Kerbside parking is not permitted.

A view of Little Victoria Street in the vicinity of the site is provided in Figure 2-14.

Figure 2-14 Cross Section of Little Victoria Street in the Vicinity of the Site

2.6.7 Little Napier Street

Little Napier Street is a right of way (ROW) which extends in a north-south direction from Gertrude Street to Little Victoria Street. Little Napier Street is aligned along part of ACU's eastern boundary, however does not provide vehicle access.

Little Napier Street operates as one way northbound only within a single traffic lane and services dwellings fronting Napier Street.

A view of Little Napier Street is provided in Figure 2-15.

Figure 2-15 Cross Section of Little Napier Street in the Vicinity of the Site

2.6.8 Graham Street

Graham Street is a local road which extends west from Young Street for approximately 45 metres before terminating. Graham Street extends to the boundary of ACU.

Graham Street operates as two way, however allows for a single direction of travel at any one time. Parallel parking is permitted on the northern kerb, as shown in Figure 2-16.

Figure 2-16 Cross Section of Graham Street in the Vicinity of the Site

2.6.9 Duke Street

Duke Street is a private road within ACU which extends west from Young Street for approximately 40 metres.

Duke Street operates as a shared zone, within a carriageway which allows for a single direction of travel at any one time. As required for shared zones, this section of Young Street operates with a speed limit of 10 kilometres per hour with a speed limit of 10 kilometres per hour.

No entry to Duke Street is permitted with the exception of authorised vehicles. The cross section of Duke Street in the vicinity of the site is provided in Figure 2-17.

Figure 2-17 Cross Section of Duke Street in the Vicinity of the Site

Attachment 10 - PLN16/0085 - Public Notification ACU Development Plan Part 9

ACU

Cardno

2.7 Road Network Safety

Casualty accident history for the roads and intersections adjoining the ACU, specifically, roads bound by Victoria Parade to the south, Gertrude Street to the north, Brunswick Street to the west and Napier Street to the east have been sourced from VicRoads CrashStats accident database.

A summary of the accidents in the area defined above for the last available five year period (1 July 2008 to 30 June 2013) are summarised in Figure 2-18.

Figure 2-18 Causality Accident History - 1 July 2008 to 30 June 2013

The CrashStats review indicates that a total of 33 casualty accidents have been reported within the nominated area within the five year period. Table 2-7 provides a broad summary of the accident categories.

Table 2-7 Accident Categories

Accident Type	Number
Involving Bicyclists	16
Involving Motor Cyclists	8
Involving Pedestrian	5
Involving Motorists Only	4
Total	**33**

Cardno Integrated Transport and Access Plan: Australian Catholic University, St. Patricks Campus, Fitzroy

2.8 Car Parking

The site is located in an area with constrained long term and short term on street parking opportunities with observations revealing that on street parking opportunities in the vicinity of the site are generally ticketed and or marked as permit zones with the limited number of long term (4 hours or greater) parking opportunities observed to be well utilised.

Notwithstanding the above, commercial off-street parking facilities are available and include the onsite parking spaces provided by the University, specifically, a 250 space deck car park accessible via Young Street, and a 50 space at grade car park accessible via Victoria Parade.

The location of these onsite parking areas are illustrated in Figure 2-19.

Figure 2-19 ACU Onsite Car Parking Spaces

Attachment 10 - PLN16/0085 - Public Notification ACU Development Plan Part 9

3 State and Local Policies

3.1 State Policies

3.1.1 Plan Melbourne

Plan Melbourne outlines the vision for Melbourne's growth to the year 2050. The document was developed through extensive collaboration across government and a sustained dialogue with professional, industry and community stakeholders. In October 2013, Plan Melbourne was released for comment, with the final Plan Melbourne released in May 2014 following submissions.

Plan Melbourne specifies seven outcomes and objectives for Melbourne, summarised as follows:

- **Delivering jobs and investment:** Create a city structure that drives productivity, supports investment through certainty and creates more jobs;
- **Housing choice and affordability:** Provide a diversity of housing in defined locations that cater for different households and are close to jobs and services;
- **A more connected Melbourne:** Provide an integrated transport system connecting people to jobs and services, and goods to market;
- **Liveable communities and neighbourhoods:** Create healthy and active neighbourhoods and maintain Melbourne's identity as one of the world's most liveable cities;
- **Environment and water:** Protect our natural assets and better plan our water, energy and waste management systems to create a sustainable city;
- **A state of cities:** Maximise the growth potential of Victoria by developing a state of cities which delivers choice, opportunity and global competitiveness; and
- **Implementation:** Delivering better governance: Achieve clear results and deliver outcomes through better governance, planning, regulation and funding mechanisms.

Plan Melbourne provides a number of directions to address the abovementioned outcomes and objectives. Those considered relevant to this report include:

- **Direction 1.1:** Define a new city structure to deliver an integrated land use and transport strategy for Melbourne's changing economy;
- **Direction 3.1:** Transform the transport system to support a more productive central city;
- **Direction 3.2:** Improve access to job-rich areas across Melbourne and strengthen transport networks in existing suburbs; and
- **Direction 3.4:** Improve local travel options to increase social and economic participation.

A discussion paper called Plan Melbourne Refresh was prepared in October 2015 to build upon the work and consultation undertaken in preparing Plan Melbourne, with Plan Melbourne 2016 to be released in the first half of 2016.

It is understood that Plan Melbourne 2016 will maintain the key priorities of Plan Melbourne 2014 but strengthen its focus on a number of areas.

3.1.2 Transport Integration Act

The Transport Integration Act 2010 creates a new framework for the provision of an integrated and sustainable transport system in Victoria. The Act provides a vision statement, reproduced below:

> *"The Parliament recognises the aspirations of Victorians for an integrated and sustainable transport system that contributes to an inclusive, prosperous and environmentally responsible State."*

The Transport Integration Act recognises that the transport system should be considered as a single system and ensures that all transport agencies work together.

3.1.3 VicRoads' SmartRoads

SmartRoads is a policy implemented by VicRoads to manage competing interests for limited road space by giving priority use of the road to different transport modes at particular times of the day. Road Use Hierarchy Maps show the priority modes on each road for each Council area and form the foundation for the network operating plan.

The Road Use Hierarchy Map in the vicinity of the site is provided in Figure 3-1 and shows Victoria Parade as a tram priority, bus priority and traffic route, whilst Brunswick Street is shown as a tram priority and bicycle priority route as well as a pedestrian priority area.

Figure 3-1 SmartRoads Road Use Hierarchy Map

3.1.4 Victorian Cycling Action Plan 2013 & 2014

The Victorian Cycling Action Plan 2013 & 2014 was prepared for the Victorian Government and was released in December 2012. The document aims to grow and support cycling in Victoria by making it easier for more people to cycle and to make it safer for people who already ride as well as improving the cycling experience for all types of bike riders.

Six directions are identified within the Victorian Cycling Action Plan to achieve the above aims, reproduced as follows:

- **Build evidence:** build a stronger evidence base for the Victorian Government to make more informed decisions;
- **Enhance governance and streamline processes:** clarify accountability and improve co-ordination, planning and delivery;
- **Reduce safety risks:** reduce conflicts and risks to make cycling safer;
- **Encourage cycling:** help Victorians feel more confident about cycling and make cycling more attractive;
- **Grow the cycling economy:** support opportunities to grow and diversify Victoria's economy through cycling; and
- **Plan networks and prioritise investment:** plan urban cycling networks to improve connectivity and better target infrastructure investment for urban networks, regional trails and specialist cycle sport infrastructure.

Attachment 10 - PLN16/0085 - Public Notification ACU Development Plan Part 9

3.1.5 Pedestrian Access Strategy

The Pedestrian Access Strategy was prepared in 2010 and aims to increase walking for transport in Victoria and notes that more people walking has the potential to help ease congestion, reduce greenhouse emissions, improve the health of Victorians and promote social connections.

The document lists five strategic directions for walking, reproduced as follows:

- **Encourage people to walk by changing attitudes and behaviour:** This aims to make walking the top-of-mind choice for Victorians – especially for short trips – by making walking for transport a visible and valued part of daily life;
- **Collaborate to improve provision for walking:** This aims to clarify the roles and responsibilities of both state and local governments in providing for walking. The Victorian Government will work with local governments to ensure they have the capacity and information they need to provide better pedestrian facilities;
- **Create pedestrian-friendly built environments, streets and public spaces:** This aims to ensure built environments across Victoria facilitate easy and efficient pedestrian movements;
- **Increase the safety of walking:** This will identify and address risks to pedestrians across the transport system and give pedestrians the skills to negotiate road environments; and
- **Continue integrating walking with public transport:** This aims to ensure more Victorians walk in combination with public transport. Walkers need to find it easy to get to major public transport hubs across Victoria and easy walking access should be provided at public transport stops.

3.2 Local Policy

3.2.1 Clause 21.06 of the Yarra Planning Scheme

Clause 21.06 of the Yarra Planning Scheme details the City of Yarra's transport related objectives and strategies.

Clause 21.06 acknowledges that the City of Yarra needs to reduce car dependence by promoting walking, cycling and public transport use as viable and preferable alternatives. Clause 21.06 lists four objectives as follows:

- **Objective 30:** To provide safe and convenient pedestrian and bicycle environments;
- **Objective 31:** To facilitate public transport usage;
- **Objective 32:** To reduce the reliance on the private motor car; and
- **Objective 33:** To reduce the impact of traffic.

Of particular relevance to this report is Strategy 32.2 in response to objective 32, reproduced below:

- **Strategy 32.2:** Require all new large developments to prepare and implement integrated transport plans to reduce the use of private cars and to encourage walking, cycling and public transport.

3.2.2 Strategic Transport Statement 2006 - City of Yarra

The Strategic Transport Statement is a document prepared by the City of Yarra in 2006 which addresses the access needs of Yarra's community whilst minimising the impact of cars on Yarra's community. The Strategic Transport Statement has a stated vision as follows:

> *"To create a city which is accessible to everyone irrespective of levels of personal mobility and where a fulfilling life can be had without the need for a car."*

The Strategic Transport Statement lists seven key Strategic Transport Objectives to achieve the above vision as follows:

- Create a city which is a great and safe place to walk and increase the numbers of those walking in Yarra.
- Create the most bicycle friendly city in Australia and increase the numbers of those cycling in Yarra.
- Advocate for increased performance of public transport across Melbourne and thereby reduce the number of car trips and through traffic by both Yarra and non-Yarra residents.
- Ensure that any new road construction is not in conflict with encouraging more sustainable transport use.
- Ensure Council's response to parking demand is based on Yarra's parking hierarchy and sustainable transport principles.

- Work to limit freight movement to arterial roads and freeways within Yarra and work to reduce freight movement through Yarra.
- Encourage Council staff to use more sustainable transport for their travel and increase the capacity of the Council as a whole to respond to and initiate positive actions to further strategic transport objectives 1 to 6.

3.2.3 City of Yarra Bicycle Strategy 2010 – 2015

The City of Yarra Bicycle Strategy 2010 – 2015 details the status on bicycle initiatives and lays out future plans for bicycle facilities, with an aim to establish cycling as a legitimate first choice of transport by people of all ages and cycling abilities.

The document lists 11 strategies to achieve the above, listed as follows:

- **Strategy 1:** Better on-road bicycle network;
- **Strategy 2:** Better local streets for cycling;
- **Strategy 3:** Better off-road bicycle network;
- **Strategy 4:** Better bicycle network maintenance;
- **Strategy 5:** Better end of trip facilities – bicycle parking;
- **Strategy 6:** Better bicycle network accountability;
- **Strategy 7:** Better bicycle safety by reducing conflict;
- **Strategy 8:** Better Council use of bicycles;
- **Strategy 9:** Better recruitment and retention of cyclists;
- **Strategy 10:** Better policies; and
- **Strategy 11:** Better innovation and relationships.

3.2.4 Inner Melbourne Action Plan (IMAP)

The Inner Melbourne Action Plan (IMAP) was prepared by the Cities of Melbourne, Yarra, Port Phillip, and Stonnington in partnership with VicUrban and was adopted in December 2005. Maribyrnong City Council also became a member of the IMAP in 2013.

The IMAP lists a number of strategies to achieve its vision, including the following which are considered relevant to this report:

- **Strategy 2:** Effectively link transport routes so that the Inner Melbourne Region is accessible throughout by walking, cycling and public transport;
- **Strategy 3:** Minimise the growing impact of traffic congestion;
- **Strategy 4:** Increase public transport use.

Attachment 11 - PLN16/0085 - Public Notification ACU Development Plan Part 10

4 Development Proposal

4.1 Development Plan

The Development Plan has been developed to accommodate the ACU's projected growth in students, teaching staff and research from 2015 to 2025, whilst also allowing the consolidation of teaching and research activities onto the Campus.

Specifically it is anticipated that by 2020, ACU will increase their EFT student numbers to 10,700 and staff numbers to 850. A summary of existing and proposed student and staff numbers is provided in Table 4-1.

Table 4-1 Student and Staff Numbers

Existing	Proposed	Change
10,000 EFT Students	10,700 EFT Students	+700 EFT Students
800 Staff	850 Staff	+50 Staff

The key principle that will inform the Development Plan from a transport perspective is based on the City of Yarra's commitment to reduce car dependency by promoting walking, cycling and public transport use.

Specifically, the St Patrick's Campus Development Plan seeks to become a pedestrian oriented space that is safe for students and prioritises sustainable modes of transport by:

> Improving access to public transport along Victoria Parade, Brunswick Street and Gertrude Street;
> Ensuring pedestrian links from the Campus integrate and form part of the wider pedestrian network;

The above will be achieved through the following initiatives:

- The closure of Young Street to through traffic. This will contribute towards a safer environment for students by removing pedestrian vehicle conflicts, whilst providing additional open space and improved amenity for pedestrians. This initiative will strengthen connections between campus buildings and the adjacent campus uses and activities;
- The creation of strong pedestrian linkages through the Campus and to adjacent uses, activities and transport networks. Notably, the facade treatment to Victoria Parade on the Mary Glowrey Building is to be removed and by so doing, substantially increasing the footpath width along Victoria Parade in the section between Young Street and Napier Street. This will be particularly beneficial at the interface of the Mary Glowrey Building and the existing bus stop.

4.2 Individual Projects

The individual projects within the Development Plan will include:

4.2.1 115 B Victoria Parade

This project will comprise of the subdivision of 115 Victoria Parade to create a new building site. Within this site, a multi storey building is contemplated, and will include the provision of basement level car parking for 270 spaces.

Vehicular access to this building is planned via Napier Street. Mitigation works will be required at the intersection of Victoria Parade and Napier Street to accommodate direct access via Victoria Parade.

4.2.2 28 - 42 Young Street (Existing Deck Car Park)

The existing 250 space deck car park is to be demolished and in its place a new building is proposed.

This new building will have no car parking spaces provided, instead, the development of this site will provide the opportunity to make substantial improvements to pedestrian access and connectivity between the campus buildings, and the adjacent campus uses and activities.

5 Transport Network Interventions

5.1 Pedestrian Network

The Development Plan contemplates improvements to pedestrian access throughout the Campus, with the aim of providing an attractive and safe pedestrian environment that will integrate with the surrounding local precinct.

The key initiatives in this regard will be:

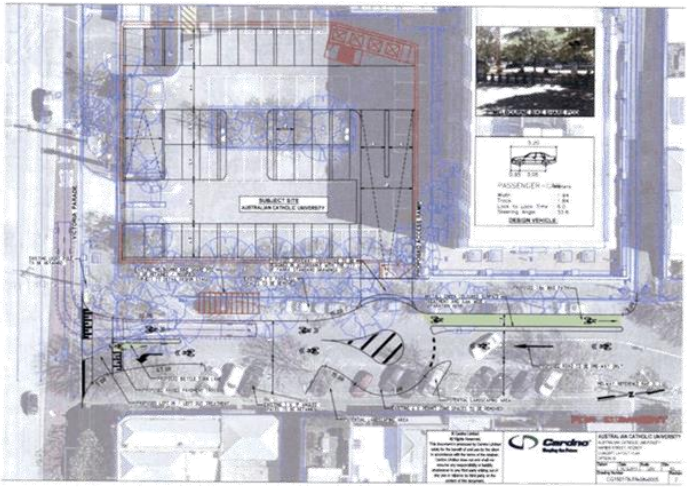
- The closure of Young Street to through traffic at a location south of Little Victoria Street, and the subsequent introduction of open space in the area between Duke Street and a location south of Little Victoria Street; and
- The creation of strong pedestrian linkages through the Campus and to adjacent uses, activities and transport networks. Notably, the facade treatment to Victoria Parade on the Mary Glowrey Building is to be removed and by so doing, substantially increasing the footpath width along Victoria Parade in the section between Young Street and Napier Street. This will be particularly beneficial at the interface of the Mary Glowrey Building and the existing bus stop.

5.2 Bicycle Network

The mitigation works required at the intersection of Victoria Parade and Napier Street to accommodate vehicular access to the 115 B Victoria Parade project, provides the opportunity to improve cyclist amenity at this intersection.

The concept plan shown as Figure 5-1 illustrates the proposed mitigation works and improvements to cyclist amenity.

Figure 5-1 Victoria Parade / Napier Street – Concept Design



It is noted that the plan has been presented to both VicRoads and Council, with both Authorities supportive of the proposed changes.

Attachment 11 - PLN16/0085 - Public Notification ACU Development Plan Part 10

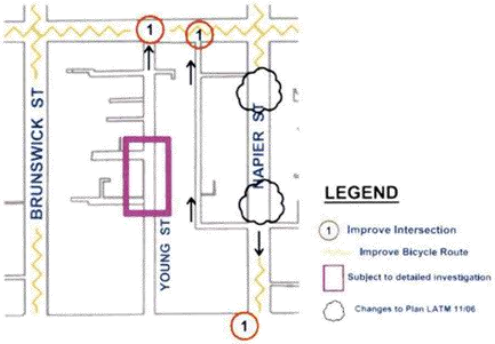
5.3 Local Area Traffic Management

The City of Yarra have over the years undertaken Local Area Traffic Management (LATM) studies to improve traffic conditions and road safety in local streets.

The studies looked at issues such as traffic speed and volume, pedestrian safety and comfort, and how to calm traffic so that neighbourhoods are more liveable. Notably, as part of these studies residents and businesses were surveyed about the traffic issues in the area and proposed treatments.

The ACU St Patrick's Campus is located within the bounds of Council's LATMS11 – Fitzroy area. An extract of LATMS11 – Fitzroy is provided as Figure 5-2.

Figure 5-2 Extract of LATMS 11 – Fitzroy



LATMS11 identifies the intersection of Victoria Parade and Napier Street as an intersection that requires improvement, furthermore, the section of Young Street between Little Victoria Street and Duke Street has also been identified as an area that is subject to detailed investigation.

Noting the foregoing, the proposed pedestrian and bicycle network interventions could be considered as options consistent with the aspirations of LATMS 11 – Fitzroy.

6 Statutory Considerations

6.1 Schedule 2 to the Development Plan Overlay

The statutory requirements applicable to the Development Plan are contained within Schedule 2 to the Development Plan Overlay.

The traffic specific requirements are reproduced below.

The development plan must show:

- The number, location, dimensions, and layout of all car parks and access ways to and from them;
- A management plan for the operation and maintenance of the car park areas;
- The location and dimensions of all bicycle, vehicle and pedestrian ways;
- A traffic management plan which must show any traffic management and traffic control works considered necessary in adjoining and nearby roads when the development is completed; and
- The means of vehicular and pedestrian ingress to and egress from the land.

An assessment of the Development Plans against the above noted statutory requirements follows:

6.2 Design Response

6.2.1 Car Parking and Accessways

A total of 270 car spaces are proposed. These spaces are to be provided within basement levels on the 115 B Victoria Parade project.

Specific to the DPO Requirements:

- Parking spaces are to be provided at a minimum 4.9m long and 2.6m wide;
- Accessways within the car park are to be provided at a minimum 6.4m wide where they serve car parking spaces and a minimum 5.5m wide where they do not abut parking. The main accessway off Napier Street is provided at a width of 7.6m;
- A minimum head height of 2.1 metres is proposed within the basement car park, increasing to 2.5m where provision is made for disabled spaces and Small Rigid Vehicles.

These dimensions are consistent with the requirements contained within Clause 52.06 of the City of Yarra Planning Scheme.

6.2.2 Bicycle Ways

An additional 60 bicycle spaces are to be provided as part of the Development Plan.

These bicycles are to be spread throughout the campus and designed to meet the dimension requirements set out by Bicycle Network and or AS2890.3.2015, and would comprise a mix as appropriate of the products as per the spec sheets provided as Appendix B.

6.2.3 Pedestrian Ways

These pedestrian linkages through the site are to be designed to meet and or exceed the relevant standards.

6.2.4 Means of Vehicular and Pedestrian Access

For the 115B Victoria Parade project, vehicular access is to be facilitated via Napier Street, as illustrated in the accompanying concept functional layout plan, Cardno drawing number CG150178-TR-SK-0005 provided as Appendix A.

Pedestrian access is to be provided via Victoria Parade, and Napier Street. The new building will also be accessible via existing pedestrian access points provided for the adjacent Mary Glowrey Building.

With regard to the 28 - 42 Young Street project, no car parking is proposed. Accordingly no vehicular access is contemplated. Pedestrian access will be via Little Victoria Street and Young Street.

Attachment 11 - PLN16/0085 - Public Notification ACU Development Plan Part 10

6.2.5 Traffic Management Plan

Victoria Parade / Napier Street

Vehicular access to the 115B Victoria Parade project will be via Napier Street. Ingress and egress will be restricted to the intersection of Victoria Parade and Napier Street.

This intersection will require mitigation works to allow access via Victoria Parade. The traffic management and traffic control works considered necessary to mitigate the impact of the development at this intersection are illustrated in Cardno drawing number CG150178-TR-SK-0005, provided as Appendix A.

Young Street

The closure of Young Street to through traffic at a location south of Little Victoria Street is contemplated as part of the 28 - 42 Young Street project.

This proposed closure of Young Street will require the northern section of Young Street to operate as a two way road section providing local access to residential properties located to the north of Duke Street. Currently this section of Young Street operates one way, northbound.

The southern section of Young Street will continue to operate two way, and will provide access to loading areas associated with the ACU Campus buildings and emergency vehicle access as required.

We have assessed the implications of truncating Young Street, and the capacity of these truncated road sections to adequately cater for traffic flows follows, our findings are that the roads

> Young Street can be classified as an Access Place.
  - An Access Place as defined by Clause 56.06 of the City of Yarra Planning Scheme, is a minor street providing local residential access with shared traffic, pedestrian and recreation use, but with pedestrian priority;
  - This road can accommodate an indicative maximum traffic volume of between 300 – 1000 vehicles over a 24-hour period, within a 5.5m wide carriageway with parking on one side of the carriageway.

> Tube count surveys undertaken on Young Street in October 2015,at a location north of the existing access to the deck car park indicates that on average Young Street carries about 550 vehicles per day northbound, with the car park contributing about 100 vehicles per day to this total figure. On weekends northbound traffic flows reduce to 250 vehicles per day.

> The proposed closure of Young Street, and the demolition of the existing deck car park will remove traffic currently generated by the car park. Based on the October 2015 surveys and excluding the car park generated traffic, Young Street carries in the order of 450 vehicle movements per day at the mid-block location.

  Noting the one way operation of this road, it is reasonable to draw the conclusion that a similar quantum is registered at the northern end of the road. This traffic would be representative of residential traffic departing the area and the recorded mid-block values representative of residents returning to the area.

  It is likely that this quantum may include rat running traffic, however for the purpose of this assessment this likelihood will not be factored into our deliberations.

  Noting the above, a truncated Young Street would be expected to record about 900 vehicles per day at the northern end. This quantum is within the indicative maximum range set out in Clause 56.06 and as such the proposal to truncate Young Street is supportable, and would not be expected to have an impact to access for residential properties to the north of the ACU campus.

> At the southern end of Young street, traffic volumes will be substantially lower as there will be no capacity to accommodate through traffic, nor any high traffic generating facilities. Access will be limited to heavy vehicles, garage collection vehicles and emergency vehicles. Accordingly, the quantum of traffic at this road section is not expected to reach or exceed the indicative maximum traffic volume of between 300 – 1000 vehicles over a 24-hour period.

6.2.6 Car Park Management Plan

The following conditions detail the operation and management of the car parking spaces proposed within the 115B Victoria Parade project.

> The parking area will be secured by boom gates, activated by card readers and ticket machine on entry and departure during the facility's operating hours, anticipated to be between 7:00am – 10:30pm Weekdays and 8:00am – 10:30pm weekends consistent with current operational practises for the existing car park. During afterhours the car park will be secured by roller doors.

> To alert casual parkers entering the car park and ensure ease of use, clearly visible advisory signs will be posted on the car park entry and also within the car park. Additionally to avoid conflict between long term (staff) and short term (students) users, spaces will be designated to individual staff members once they are determined, with signage posted at the end of each bay.

> Operation of the public car park will be limited to the hours listed above, with its fee structure expected to be based on an hourly charge rate in-line with other commercial car park facilities in the vicinity of the site. The actual charge rates will however be determined once an operator is appointed. The car park is to be operated by ticket collection on entry, and then payment at a pay station likely to be located adjacent to the lifts.

6.3 Car Parking - Clause 52.06

6.3.1 Car Parking Requirement

Table 1 to Clause 52.06 of the City of Yarra Planning Scheme sets out the car parking provision rates that apply to the Development Plan.

Specifically, Clause 52.06 requires that parking for the University is provided at a minimum rate of:

> 0.4 spaces to each student that is part of the maximum number of students on site at any one time.

The student population on the campus is projected to increase by 700 students, in the period between 2015 – 2020.

Application of this rate to the proposed increase in students reveals a statutory requirement to provide a minimum 280 spaces.

6.3.2 Car Parking Provision

A total of 270 spaces are proposed as part of the 115B Victoria Parade Project, whilst the 28 - 42 Young Street project contemplates the demolition of the 250 space deck car park, with no additional parking provided on this site.

Accordingly the Development Plan seeks to provide a net increase of 20 spaces, against a statutory requirements of 280 spaces.

This provision is considerably lower than the statutory requirements, and as such is in line with the sustainable transport objectives set out in local and state level policies.

6.4 Bicycle Parking – Clause 52.34

6.4.1 Bicycle Parking Requirement

Table 1 to Clause 52.34 of the City of Yarra Planning Scheme sets out the bicycle parking provision rates that apply to the Development Plan.

Specifically, Clause 52.34 requires that bicycle parking for the University is provided at a minimum rate of:

> 1 space to each 20 employees; plus
> 1 space to each 20 full time students.

The student population on the campus is projected to increase by 700 students, with a corresponding increase in staff numbers of 50 in the period between 2015 – 2020.

Application of these rates to the proposed increase in staff and students reveals a statutory requirement to provide a minimum 38 bicycle spaces comprising, 3 staff spaces and 35 student spaces.

Attachment 11 - PLN16/0085 - Public Notification ACU Development Plan Part 10

6.4.2 Bicycle Parking Provision

It is intended to provide 38 bicycle spaces in accordance with the statutory requirements within the DPO area. These spaces will be supplemented by an additional 22 spaces that will be provided across the balance of the campus, not contained within the DPO area.

Accordingly, a total of 60 bicycle parking spaces are proposed

This provision is higher than the statutory requirements, and as such is in line with the sustainable transport objectives set out in local and state level policies.

7 Traffic Impact

7.1 Projected Traffic Volumes

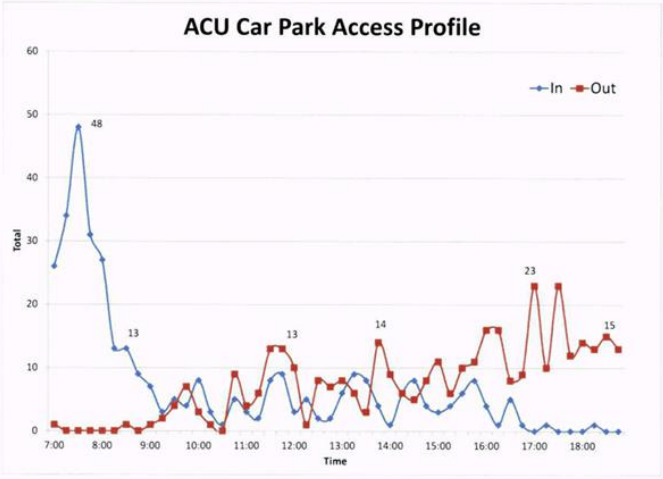
The existing ACU deck car park with 250 spaces used by both staff and students provides a suitable reference to inform consideration about projected traffic volumes at the proposed 270 space car park at the 115B Victoria Parade project.

The existing car park was surveyed on

- Wednesday 21st May 2014 between 7am to 7pm; and
- Wednesday 7th October 2015 between 7am to 7pm

Figure 7-1 and Figure 7-2 provide a summary of the traffic profile at the ACU car park access points.

Figure 7-1 ACU Car Parking Access Profile – Wednesday 21st May 2014



The key findings were as follows:

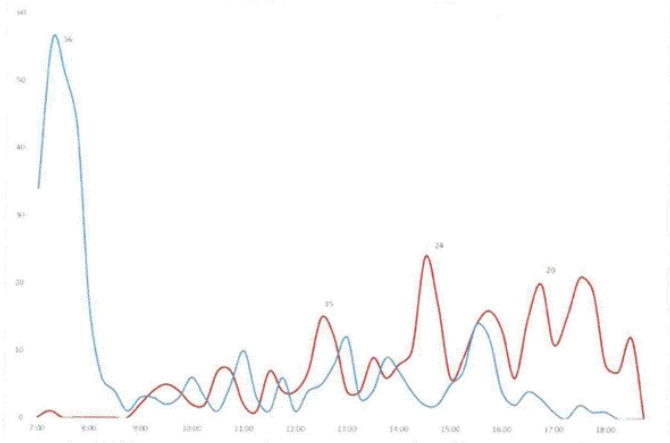
> There were a total of 355 inbound movements and 360 outbound movements; and

> The AM and PM peak hour was found to occur between 7:15am - 8:15am and 5:00pm-6:00pm respectively, when a total of 140 and 68 vehicle movements were recorded at the site access points respectively;

The above equates to about 60 % of the car parking spaces filling during the AM peak period, and about 30% of the spaces emptying during the PM peak. Traffic flows during the peak periods was observed as being heavily biased towards the peak direction.

Attachment 11 - PLN16/0085 - Public Notification ACU Development Plan Part 10

Figure 7-2 ACU Car Parking Access Profile – Wednesday 7th October 2015



The key findings were as follows:

> There were a total of 375 inbound movements and 360 outbound movements; and

> The AM and PM peak hour was found to occur between 7:00am - 8:00am and 3:15pm-4:15pm respectively, when a total of 185 and 45 vehicle movements were recorded at the site access points respectively.

The above equates to about 75 % of the car parking spaces filling during the AM peak period, and about 20% of the spaces emptying during the PM peak. Traffic flows during the peak periods was observed as being heavily biased towards the peak direction.

Conservatively adopting a rate of 75 % of the car spaces filling during the AM peak and 30% emptying during the PM peak, the proposed 270 space car park would be projected to generate:

> 203 vehicle movements during the AM peak period; and
> 81 vehicle movements during the PM peak period.

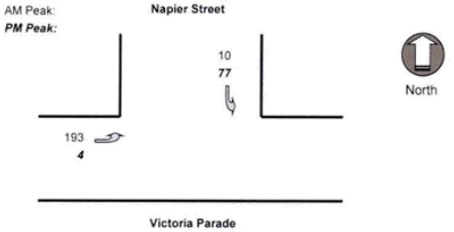
For the purpose of this assessment, traffic flows will be biased 95% in the peak direction and 5% in the non-peak direction.

7.2 Traffic Distribution

Traffic to and from the proposed car park will be restricted to the Victoria Parade / Napier Street intersection.

This intersection will operate as a left in / left out, with all traffic arriving from the west and departing towards the east. Figure 7-3 illustrates the projected additional traffic flows at this intersection.

Figure 7-3 Projected Additional Traffic Flows – Victoria Parade / Napier Street



Based on historic traffic data collected on Napier Street in 2010, Napier Street at a location north of Little Victoria Street carries on average about 700 southbound vehicles per day, inclusive of about 100 vehicle movements during the AM peak period and 50 vehicle movements during the PM peak period.

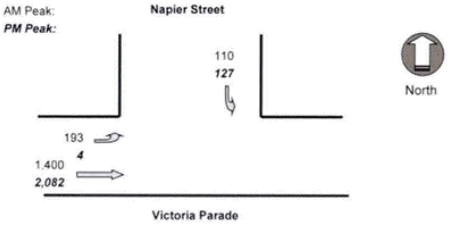
7.3 Post Development Traffic Flows

SCATS data sourced from VicRoads for Thursday 8th October 2015 at the intersection of Victoria Parade / Lansdowne Street, show that during the AM peak periods occurs between 11:00am – 12:00pm, when 1,400 vehicles are recorded past Napier Street. The PM peak period occurs between 5:00pm – 6:00pm, with 2,082 vehicles recorded past this intersection.

Noting that Napier Street is located a short distance downstream of the signalised intersection of Victoria Parade / Lansdowne Street, platooning of traffic during the peak periods has been observed which assists motorists seeking to depart from Napier Street.

Allowing for the estimated existing traffic flows on Napier Street and the recorded traffic flows along Victoria Parade, the anticipated post development traffic flows are illustrated as Figure 7-4.

Figure 7-4 Anticipated Post Development Traffic Flows – Victoria Parade / Napier Street



Attachment 11 - PLN16/0085 - Public Notification ACU Development Plan Part 10

7.4 Intersection Analysis

The operation of the Victoria Parade / Napier Street intersection was analysed using SIDRA Intersection. This computer package, originally developed by the Australian Road Research Board, provides information about the capacity of an intersection in terms of a range of parameters, as described below:

Degree of Saturation (D.O.S.) is the ratio of the volume of traffic observed making a particular movement compared to the maximum capacity for that movement. Various values of degree of saturation and their rating are shown in Table 7-1.

Table 7-1 Rating of Degrees of Saturation

D.O.S.	Rating
Up to 0.6	Excellent
0.6 to 0.7	Very Good
0.7 to 0.8	Good
0.8 to 0.9	Fair
0.9 to 1.0	Poor
Above 1.0	Very Poor

It is considered acceptable for some critical movements in an intersection to operate in the range of 0.9 to 1.0 during the high peak periods, reflecting actual conditions in a significant proportion of suburban signalised intersections.

The ***95th Percentile (95%ile) Queue*** represents the maximum queue length, in metres, that can be expected in 95% of observed queue lengths in the peak hour; and

Average Delay is the delay time, in seconds, which can be expected over all vehicles making a particular movement in the peak hour.

The results of the SIDRA Intersection analysis are summarised in Table 7-2.

Table 7-2 SIDRA Intersection Analysis Summary

	Approach	Degree of Saturation	95th%ile Queue	Average Delay
AM Peak	Napier Street (North)	0.174	5 metres	12 seconds
	Victoria Parade (West)	0.252	0 metres	1 second
PM Peak	Napier Street (North)	0.298	8 metres	16 seconds
	Victoria Parade (West)	0.375	0 metres	0 seconds

Based on the foregoing the intersection of Victoria Parade and Napier Street is expected to operate under excellent conditions, with motorists experiencing limited queues and delays.

7.4.2 Operation of Site Access

An assessment of the operation of the site access has been undertaken to determine likely queues at the control point, especially during the AM peak period.

The analysis shows that during the AM peak period when 193 ingress vehicle movements are projected, 95th %ile queues of 4 vehicles are anticipated.

The boom gates are to be located at the bottom of the ramp, and will therefore afford motorists a queuing distance of about 24 metres, measured from the title boundary.

This setback can accommodate about 4 vehicles and as such queues on to Napier Street are not anticipated. The design and location of the proposed car park access is therefore not expected to result in obstruction of traffic.

8 Green Travel Initiatives

8.1 Preamble

Green Travel initiatives are aimed at encouraging people to choose sustainable travel alternatives such as cycling, walking or public transport, over private car usage. It is the view of the State Government that tertiary institutions are typically considered positive environments for the application of travel behaviour change programs as many students and staff already support sustainable transport for environmental, financial and social reasons.

Accordingly, and in consultation with Council, the University could develop a Green Travel Plan that would seek to encourage staff and students to choose sustainable travel alternatives such as public transport, cycling and walking.

8.2 Objective

As a guide, the objective of this travel plan would be to increase the proportion of alternative modes of transport users to 90% within 5 years.

The co-ordination and implementation of the Green Travel Plan would be the responsibility of the University in consultation with Council to:

- Increase awareness of sustainable transport options to new and ongoing staff and students at the Campus;
- Encourage behavioural change in both new and on-going staff and students from single-occupant motor vehicle use, to alternative methods of transport i.e. trams, buses and bicycle/walking paths;
- Implement travel/behaviour change strategies and use pre/post surveys to monitor changes in attitudes and reported behaviour;
- Identify infrastructure changes / improvements to alternative travel mode infrastructure within the locality that will facilitate take-up of sustainable transport behaviours by the University community.

8.3 Action Plan (Guide Only)

Various strategies can be adopted to encourage the use of non-private motor vehicles, as described in action plans below.

8.3.1 General Actions

Action	Timeline	By whom	Approx cost
Strategic Marketing			
Promotion of Green travel initiatives during orientation and enrolment & re enrolment periods.	Ongoing - from time of occupation	University	
Toilet talk – a series of posters behind toilet doors where people can read them in private!			
An events calendar: 3-4 events per year. Best in conjunction with state wide events such as Ride to Work Day, World Environment Day, National Walk to Work Day. Plan for lunch, morning teas or breakfasts, guest speakers, demonstrations etc.			
Display boards in prominent locations to showcase green travel initiatives.			

8.3.2 Walking

Action	Timeline	By whom	Approx cost
Produce a map showing safe walking routes to and from the University with indicative walking times, not distances, to local facilities, such as shops and bus stops (people often have an unrealistic idea of how long it takes to walk).	On Occupation	University	
Provide umbrellas in staff rooms to encourage staff members to consider walking during adverse weather periods.			
Create a sustainable travel tab on the University web page with links to appropriate external and internal websites.			
Negotiate with council for improvements to footpaths in the vicinity of the site.	Ongoing	University & Council	

Attachment 11 - PLN16/0085 - Public Notification ACU Development Plan Part 10

8.3.3 Cycling

Action	Timeline	By whom	Approx cost
Establish a University "Bicycle Users Group" (BUG). BUGs are formed by people who want to work together to improve facilities for cyclists and encourage cycling.	On Occupation	University	
Participate in annual events such as 'Ride to Work Day.'	Ongoing		
Provide sufficient bicycle parking and end of trip facilities (lockers / showers) to meet staff and student needs, which is easily accessible and secure.	On Occupation		
Ensure bicycle parking is clearly visible or provide signage to direct people to cycle bays.			
Produce a map showing quiet cycle routes to the University.			
Create a sustainable travel tab on the University web page with links to appropriate external and internal websites			
Improvements to Bicycle Infrastructure as identified in Yarra City Council's Draft Bicycle Strategy, such as modifications to the adjacent traffic signals along Victoria Parade to facilitate connections to the bicycle routes in the City of Melbourne.		Council	

8.3.4 Public Transport

Action	Timeline	By whom	Approx cost
Develop a map showing public transport routes to the University.	On occupation	University	
Put up a noticeboard with leaflets and maps showing the main public transport routes to and from the University.	On occupation		
Create a sustainable travel tab on the University web page with links to appropriate external and internal websites.	On occupation		
Encourage public transport use for business travel. This could be done by ensuring shared tickets are available at the University for work travel during the day.	Ongoing		
Participate in the Public Transport Victoria (PTV), 'Commuter Club' scheme, which allows employers to purchase discounted yearly MYKI cards on behalf of their employees.			

8.3.5 Car Parking / Car Pooling

Action	Timeline	By whom	Approx cost
Set up a car pooling database.	Within 12 Months of Occupation	University	
Allocate priority parking spaces for car poolers.			
Provide sustainable transport allowances for staff who surrender car parking permits.			
Review pricing policy for existing ACU off street car park.			

The above draft action plans are an outline of the actions and incentives that could be undertaken / provided and is subject to agreement between the University and Council.

8.4 Monitoring and Review

In order to monitor the success of the aforementioned Green Travel Initiatives a 3 stage monitoring system could be implemented by the University.

Elected University representatives, both staff and students would be assigned as Travel Co-ordinators of the Travel Plan. The co-ordinators would be responsible for:

> Organising the monitoring of the performance of the Travel Plan against the targets that have been agreed.
> Reviewing the occupancy / use (and abuse) of the facilities that are provided on site, for example, cycle racks, motorcycle and motorcar parking spaces, clothes lockers, and travel information etc.

> Reviewing the operational success or failure of the plan and produce a review report highlighting successful initiatives and also areas that require improvement. This review report would be issued to Council on an annual basis and would form the platform of an updated Travel Plan, with sign off/ input from Council sought on a regular basis, the duration of which would be determined by the relevant stake holders.

The suggested 3 stage monitoring program is outlined below:

Stage 1

> Stage 1, would involve a questionnaire survey of students and staff during the enrolment process. The survey will be useful to collect information on the travel characteristics of the staff and students and assist in gauging interest in the various Green Travel initiatives and to seek ideas for other Green Travel initiatives.

Stage 2

> Stage 2, would involve a questionnaire and feedback form to be filled out by Students and Staff 3 months after implementation, in order to determine what Green Travel initiatives are working and which are not. A pro forma for the Stage 2 questionnaire is shown attached as Annex 1.

Stage 3

> Stage 3, would be the monitoring component of the plan which would be undertaken 6 months after occupation. This questionnaire would test the success rate of the various initiatives and help rework programs to suit the needs of staff and students. A pro forma for the Stage 3 questionnaire is shown attached as Appendix 1.

It is expected that the above stages would form the basis of a continuous monitoring program to gauge the effectiveness of the travel plan.

Attachment 12 - PLN16/0085 - Public Notification ACU Development Plan Part 11

ACU
AUSTRALIAN CATHOLIC UNIVERSITY

9 Conclusions

> The Development Plan has been developed to accommodate the ACU's projected growth in students, teaching staff and research from 2015 to 2025, whilst also allowing the consolidation of teaching and research activities onto the Campus.

Specifically it is anticipated that by 2020, ACU will increase their EFT student numbers to 10,700 and staff numbers to 850

> The individual projects within the Development Plan will include:

115 B Victoria Parade

This project will comprise of the subdivision of 115 Victoria Parade to create a new building site. Within this site, a multi storey building is contemplated, and will include the provision of basement level car parking for 270 spaces.

Vehicular access to this building is planned via Napier Street. Mitigation works will be required at the intersection of Victoria Parade and Napier Street to accommodate direct access via Victoria Parade.

In principle approval has been secured from VicRoads and Council to the suite of mitigation works proposed.

28 - 42 Young Street (Existing Deck Car Park)

The existing 250 space deck car park is to be demolished and in its place a new building is proposed.

This new building will have no car parking spaces provided, instead, the development of this site will provide the opportunity to make substantial improvements to pedestrian access and connectivity between the campus buildings, and the adjacent campus uses and activities.

This proposed closure of Young Street will require the northern section of Young Street to operate as a two way road section providing local access to residential properties located to the north of Duke Street. Currently this section of Young Street operates one way, northbound.

The southern section of Young Street will continue to operate two way, and will provide access to loading areas associated with the ACU Campus buildings and emergency vehicle access as required.

A truncated Young Street would be expected to adequately carry traffic volumes within the indicative maximum range set out in Clause 56.06. As such the proposal to truncate Young Street is supportable and would not be expected to have an impact to access for residential properties to the north of the ACU campus.

> The key principle that will inform the Development Plan from a transport perspective is based on the City of Yarra's commitment to reduce car dependency by promoting walking, cycling and public transport use.

Specifically, the St Patrick's Campus Development Plan seeks to become a pedestrian oriented space that is safe for students and prioritises sustainable modes of transport by:

- Improving access to public transport along Victoria Parade, Brunswick Street and Gertrude Street;
- Ensuring pedestrian links from the Campus integrate and form part of the wider pedestrian network;

The above will be achieved through the following initiatives:

- The closure of Young Street to through traffic. This will contribute towards a safer environment for students by removing pedestrian vehicle conflicts, whilst providing additional open space and improved amenity for pedestrians. This initiative will strengthen connections between campus buildings and the adjacent campus uses and activities;
- The creation of strong pedestrian linkages through the Campus and to adjacent uses, activities and transport networks. Notably, the facade treatment to Victoria Parade on the Mary Glowrey Building is to be removed and by so doing, substantially increasing the footpath width along Victoria Parade in the section between Young Street and Napier Street. This will be particularly beneficial at the interface of the Mary Glowrey Building and the existing bus stop

> A total of 270 spaces are proposed as part of the 115B Victoria Parade Project, whilst the 28 - 42 Young Street project contemplates the demolition of the 250 space deck car park, with no additional parking provided on this site. Accordingly the Development Plan seeks to provide a net increase of 20 spaces, against a statutory requirements of 280 spaces.

> The proposed 115B Victoria Parade Car Park will be operated and managed as follows:

- The parking area will be secured by boom gates, activated by card readers and ticket machine on entry and departure during the facility's operating hours, anticipated to be between 7:00am – 10:30pm Weekdays and 8:00am – 10:30pm weekends consistent with current operational practises for the existing car park. During afterhours the car park will be secured by roller doors.
- To alert casual parkers entering the car park and ensure ease of use, clearly visible advisory signs will be posted on the car park entry and also within the car park. Additionally to avoid conflict between long term (staff) and short term (students) users, spaces will be designated to individual staff members once they are determined, with signage posted at the end of each bay.
- Operation of the public car park will be limited to the hours listed above, with its fee structure expected to be based on an hourly charge rate in-line with other commercial car park facilities in the vicinity of the site. The actual charge rates will however be determined once an operator is appointed. The car park is to be operated by ticket collection on entry, and then payment at a pay station likely to be located adjacent to the lifts.

> The proposal triggers a statutory requirement to provide 38 bicycle spaces. A total of 60 bicycle parking spaces are proposed, with 38 spaces to be provided within the DPO area and the balance, 22 spaces provided across the campus in locations outside the DPO area. This provision is higher than the statutory requirements, and as such is in line with the sustainable transport objectives set out in local and state level policies.

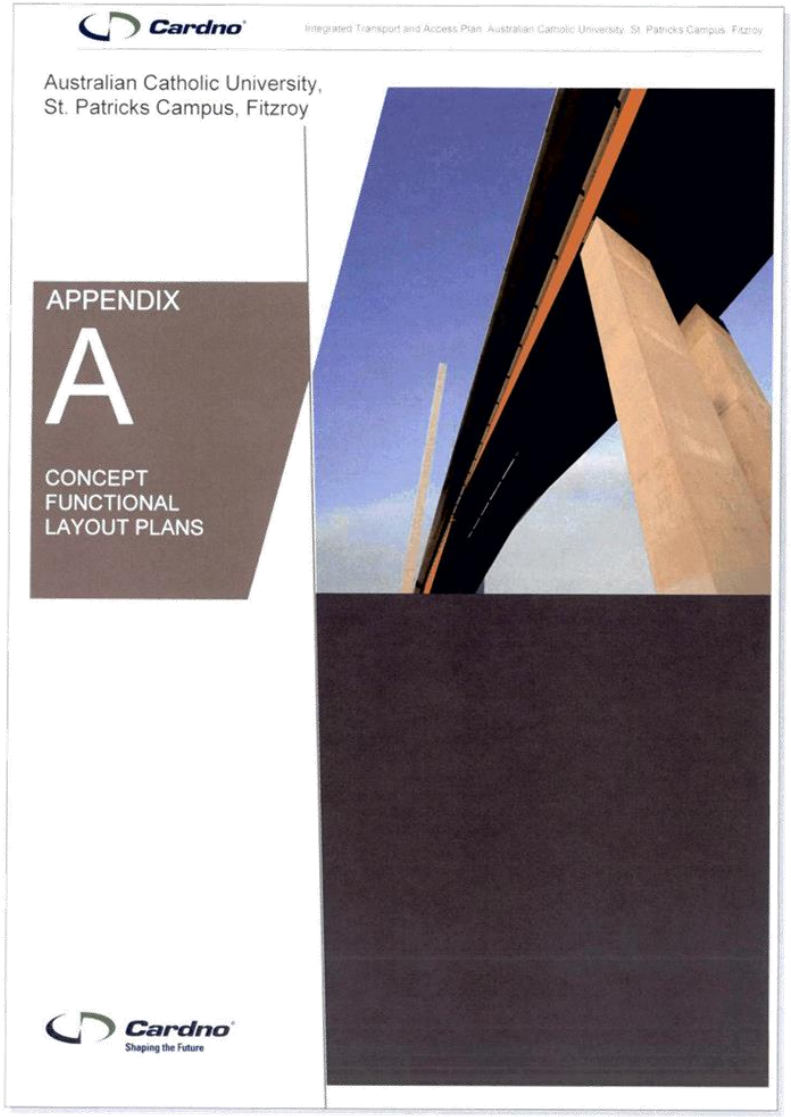
> An assessment of the post development intersection operating conditions shows that with the mitigation works proposed, the Victoria Parade and Napier Street intersection will to operate under excellent conditions, with motorists experiencing limited queues and delays.

Additionally, an assessment of the operation of the site access has revealed 95th %ile queues of 4 vehicles are anticipated during the critical AM peak period. The design of the car park access and proposed location of the control points is such that motorists will be afforded a queuing distance of about 24 metres, measured from the title boundary.

This setback can accommodate about 4 vehicles and as such queues on to Napier Street are not anticipated. The design and location of the proposed car park access is therefore not expected to result in obstruction of traffic.

> ACU will also actively pursue Green Travel initiatives aimed at encouraging people to choose sustainable travel alternatives such as cycling, walking or public transport, over private car usage.

Attachment 12 - PLN16/0085 - Public Notification ACU Development Plan Part 11



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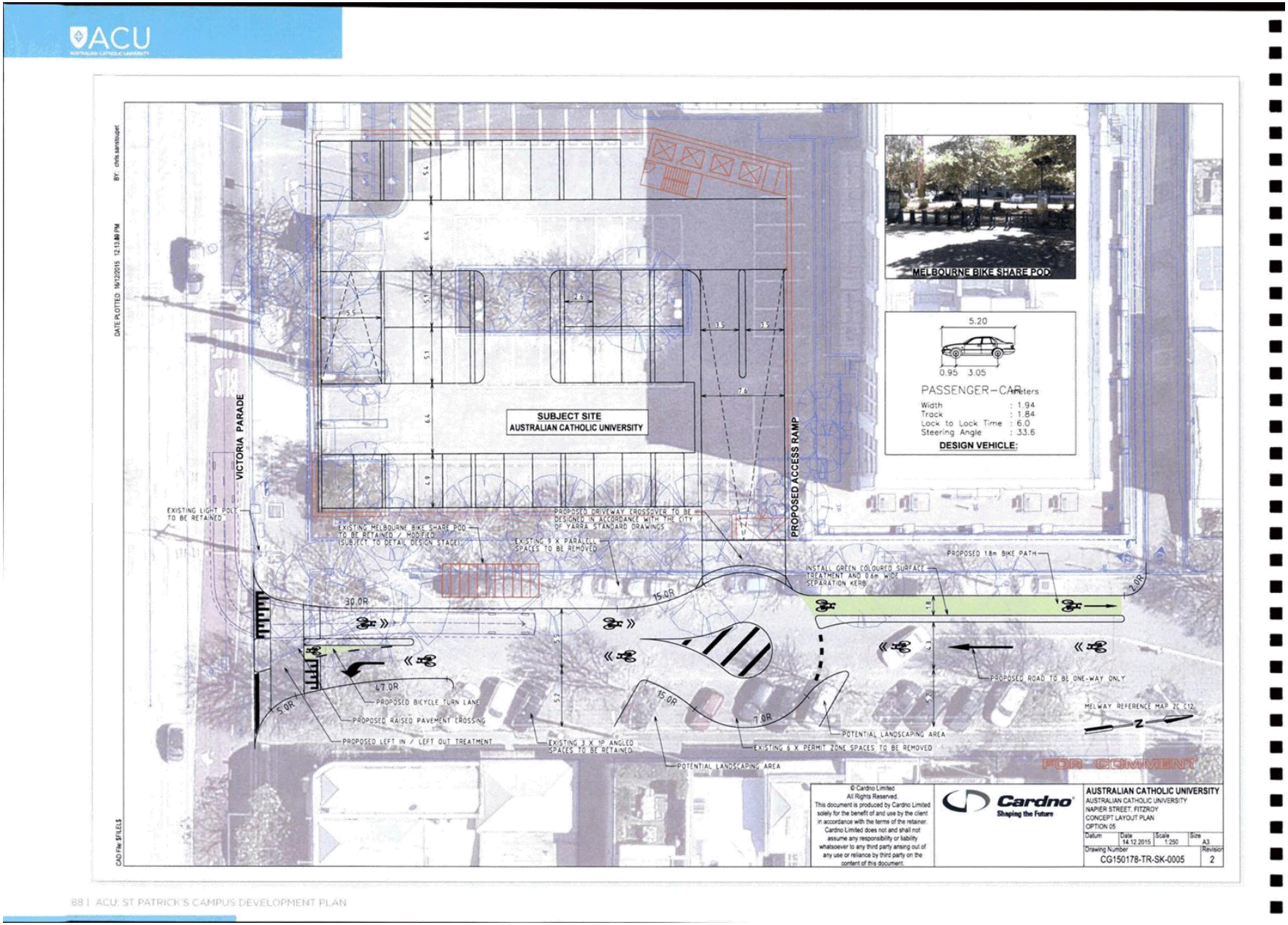

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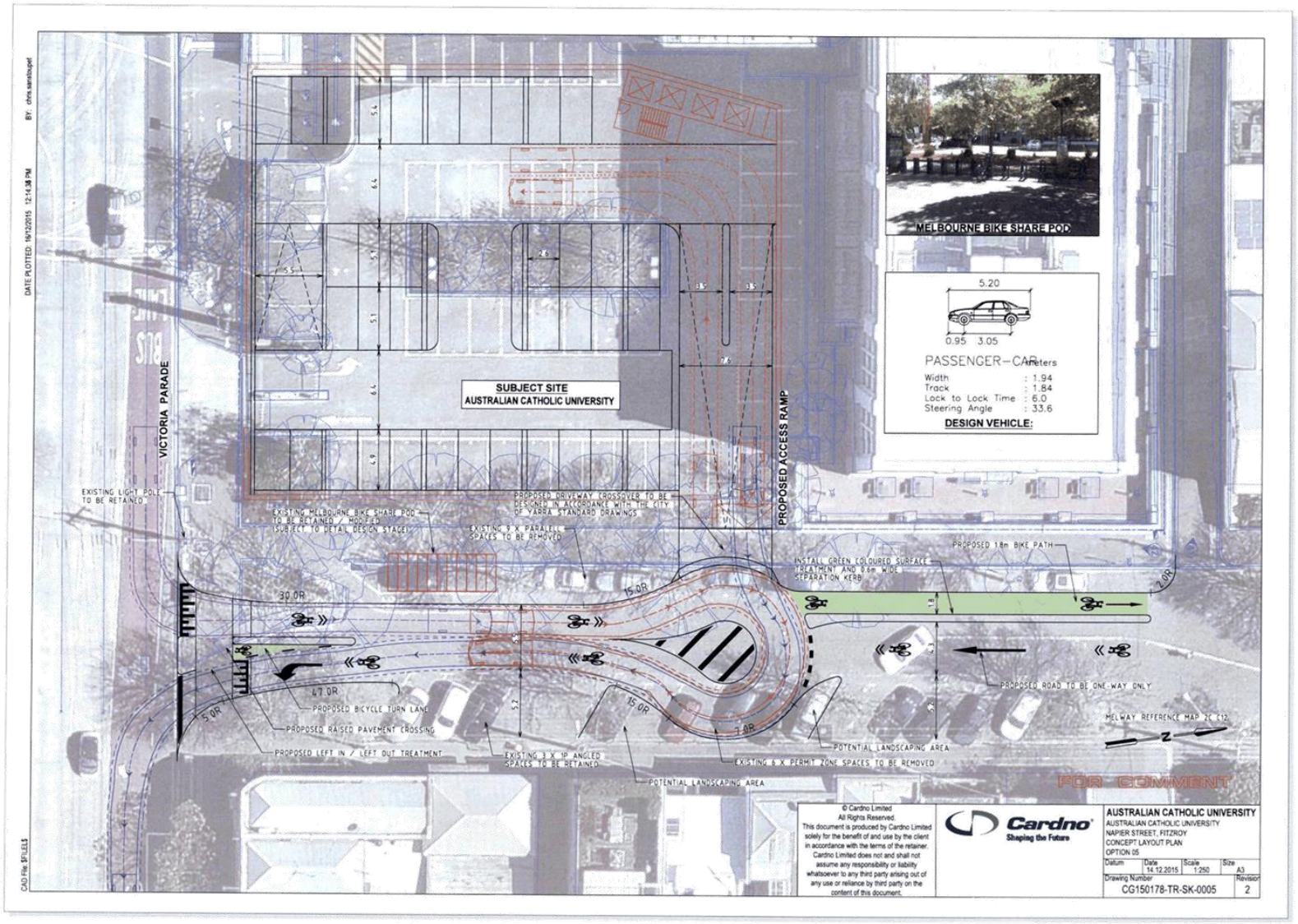

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ACU

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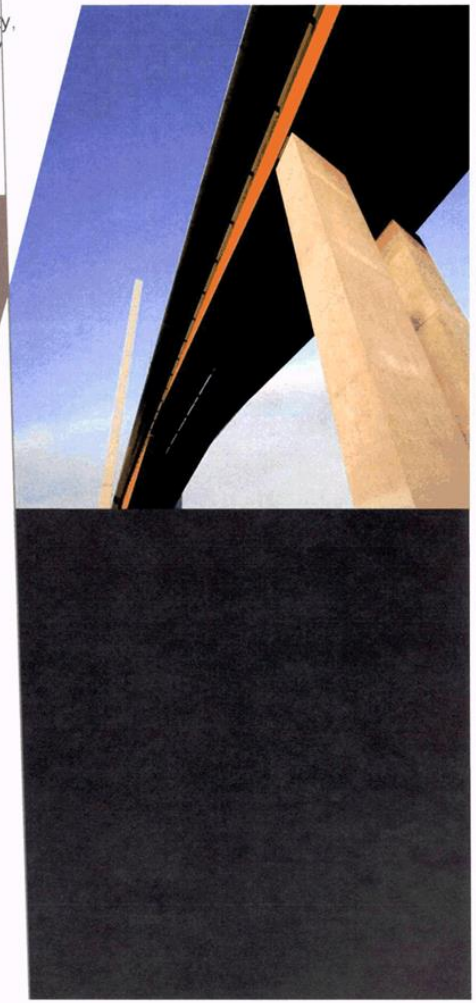
Integrated Transport and Access Plan: Australian Catholic University, St. Patricks Campus, Fitzroy

Australian Catholic University,
St. Patricks Campus, Fitzroy



APPENDIX

B

BICYCLE PARKING PRODUCTS

Cardno
Shaping the Future

BICYCLE

Anaconda™

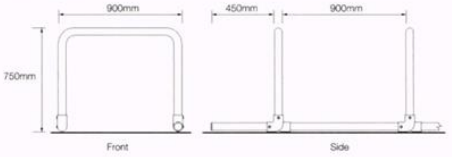
Galvanised finish

Features

Anaconda rails are a freestanding version of the Flat Top. These are an excellent solution for use on asphalt or for temporary event parking. Available in a variety of materials and finishes in both adult and junior sizes.

- Easy to use with any bike lock
- Freestanding, self-supporting and can be fastened to all surfaces
- Using clamp-on elbows to join the rails
- Powder coated, galvanised, alloy or stainless steel rail tubing available
- Parks two bicycles per rail in either parallel or diagonal set up
- Supports all styles of bicycles in an upright position

Dimensions



Specifications

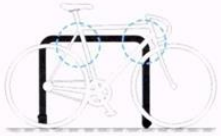
Material options
- 316 Marine grade stainless steel
- Galvanised
- Powder coated
- Alloy

Fixing options
- Clamp on

Recommended fasteners
- Zinc plated Dynabolts (M10 x 65mm)
- Stainless Dynabolts (M10 x 65mm)

Dimensions
900mm [w] x 750mm [h]

Locking points



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Design. Supply. Install.

Bicycle Network ABN 41 026 835 903
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TAS 210 Collins St, Hobart 7000

BIKE PARKING EXPERTS®

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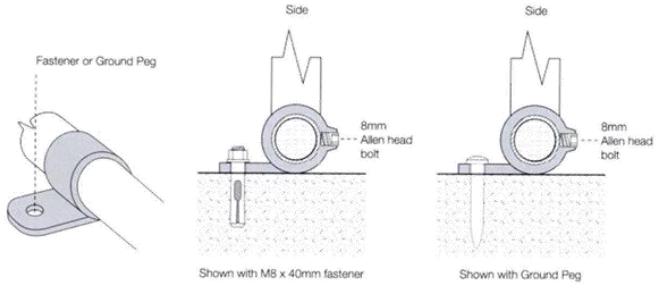
Fixing options

Fix to the ground with Ground Tabs fitted over the piping at each end and secured with 8mm Allen bolts.
Use either a Fastener or Ground Peg to secure the Anaconda to the ground.

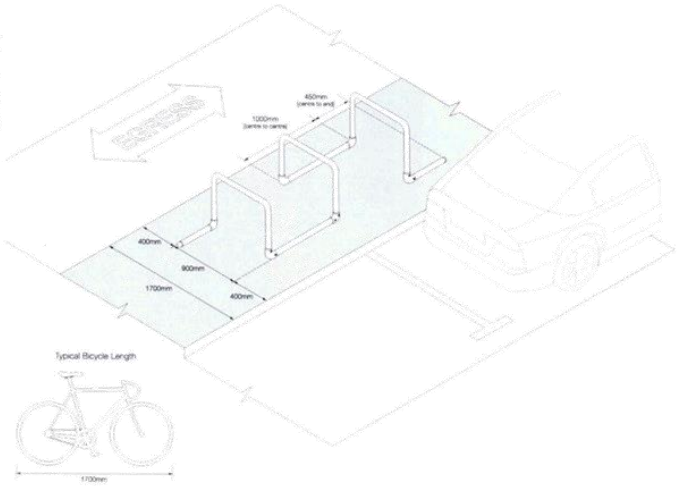


Shown with M8 x 40mm fastener

Shown with Ground Peg

Bike Parking Experts | Anaconda™

Layout guidelines



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BIKE PARKING EXPERTS®

BICYCLE NETWORK

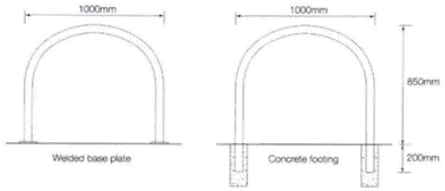
Arc de Triomphe™

Stainless steel finish

Features

- Each rail supports two adult bikes in an upright position
- Can be either bolted to a concrete slab or concreted in situ
- Available in stainless steel or galvanised steel
- Provides the ability to lock both wheels and frame
- Suitable for foyers and entry areas

Dimensions



Specifications

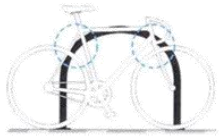
Material options
- 316 Marine grade stainless steel
- Galvanised

Fixing options
- Welded flange
- In situ

Recommended fasteners
- Galvanised Dynabolts (M10 x 65mm)
- Stainless Dynabolts (M10 x 65mm)
- Shear Nut security fasteners

Dimensions
1000mm [w] x 850mm [h]

Locking points



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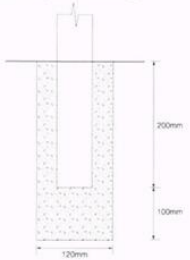
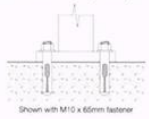
ACU
AUSTRALIAN CATHOLIC UNIVERSITY

Fixing options

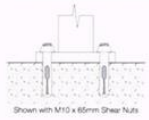
In situ (Concrete footing)



Welded flange (Bolt on)



Welded flange (Security heads)



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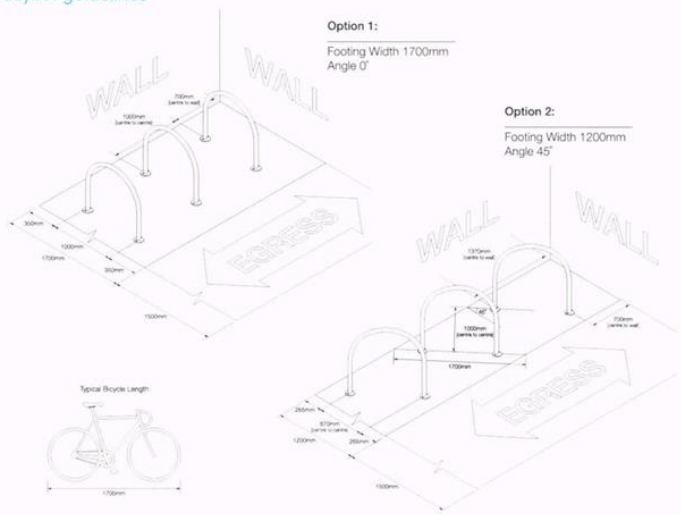
Layout guidelines



Option 1:
Footing Width 1700mm
Angle 0°

Option 2:
Footing Width 1200mm
Angle 45°

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BIKE PARKING EXPERTS®

BICYCLE NETWORK

Flat Top™

Stainless steel finish

Features

- Each rail supports two adult bikes in an upright position
- Can be either bolted to a concrete slab or concreted in situ
- Available in stainless steel or galvanised steel
- Provides the ability to lock both wheels and frame
- Suitable for interior use including storage cages

Specifications

Material options
- 316 Marine grade stainless steel
- Galvanised

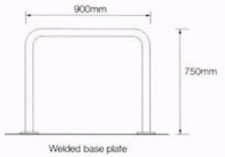
Fixing options
- Welded flange
- In situ

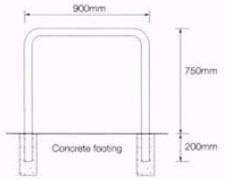
Recommended fasteners
- Galvanised Dynabolts (M10 x 65mm)
- Stainless Dynabolts (M10 x 65mm)
- Shear Nut security fasteners

Dimensions
950mm [w] x 750mm [h]

Locking points

Dimensions





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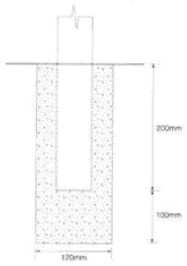
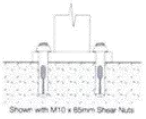
Fixing options

In situ (Concrete footing)



Welded flange (Bolt on)

Welded flange (Security heads)

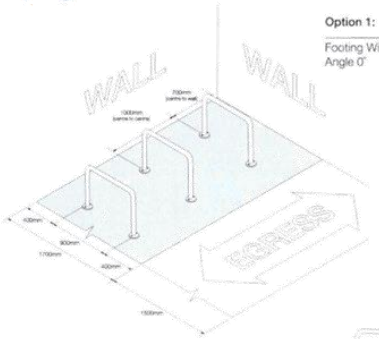


Bike Parking Experts | Flat Top™

Layout guidelines

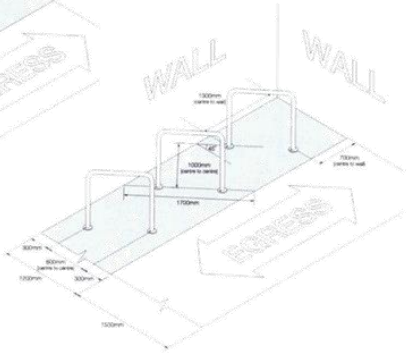
Option 1:

Footing Width 1700mm
Angle 0°



Option 2:

Footing Width 1200mm
Angle 45°



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BIKE PARKING EXPERTS®

BICYCLE

Ned Kelly®™

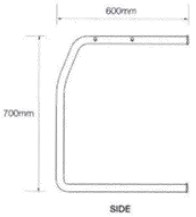
Galvanised finish

Black powder coat finish

Features

- Each rail provides storage for a single bike
- Suits bikes with full length mud guards
- Available in galvanised or powder coat over mild steel
- Provides the ability to lock the main frame and one wheel
- Support prongs with protective coating prevent damage to rim
- Can be used with custom framing – no wall needed

Dimensions



Specifications

Material options
- Galvanised
- Powder coat over mild steel
- Stainless steel*

Fixing options
- Bolt on to wall
- Fixed to support framing

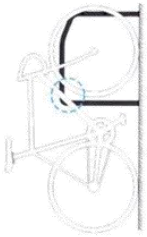
Recommended fasteners – wall
- Dynabolts (M8 x 40mm)
- Shear Nut security fasteners

Recommended fasteners – framing
- Bolt and nut (M10 x 60mm)
- Tek screws

Dimensions
125mm [w] x 700mm [h] x 600mm [d]

* Pre-order only

Locking points



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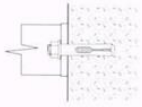
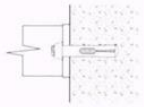
Bike Parking Experts | Ned Kelly™

Fixing options

Fix to a wall using 4x fasteners or Shear Nuts

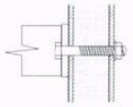


Shown with M8 x 40mm fastener

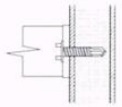


Shown with M8 x 40mm Shear Nuts

Fix to a frame using 4x bolts or Tek Screws

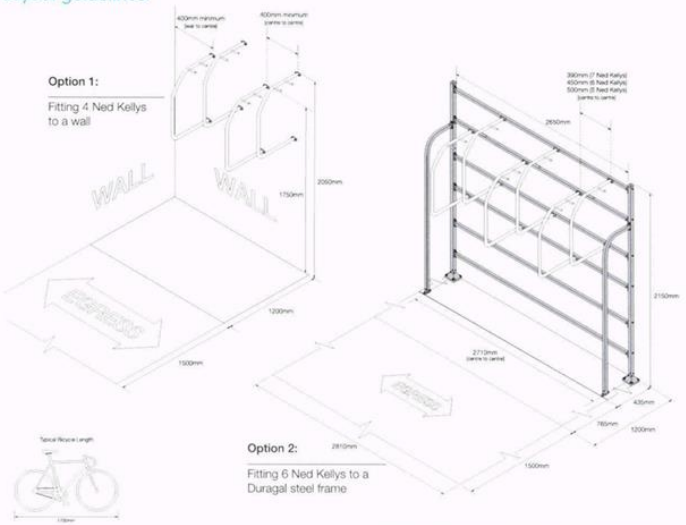


Shown with M10 x 60mm Bolt, Washer & Nut



Shown with Tek Screw

Layout guidelines



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BIKE PARKING EXPERTS®

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BICYCLE NETWORK

Pole Vault™

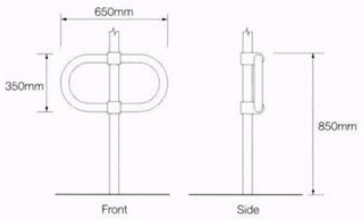
Stainless finish

Features

The Pole Vault is an innovative street sign mounted bike parking rail that can be fitted in minutes with no drilling or concrete required.

- Can be retro-fitted to existing sign poles
- Accommodates a wide range of sizes and styles of bikes
- Can be moved and re-fitted to different locations
- Comes in standard galvanised, powdercoat or stainless material
- Easy to use with any bike lock
- Supports the entire bike so it won't slip or fall over

Dimensions



Specifications

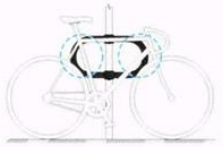
Material options
- 316 Marine grade stainless steel
- Galvanised

Fixing options
- Clamp on

Recommended fasteners
- 8mm Allen head bolts

Dimensions
650mm [w] x 350mm [h]

Locking points



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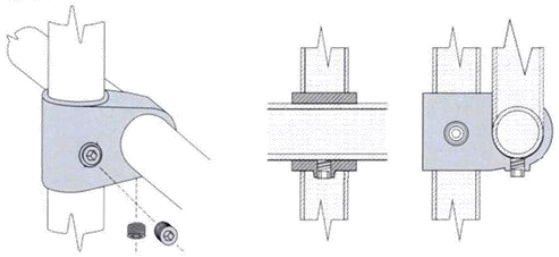
Fixing options

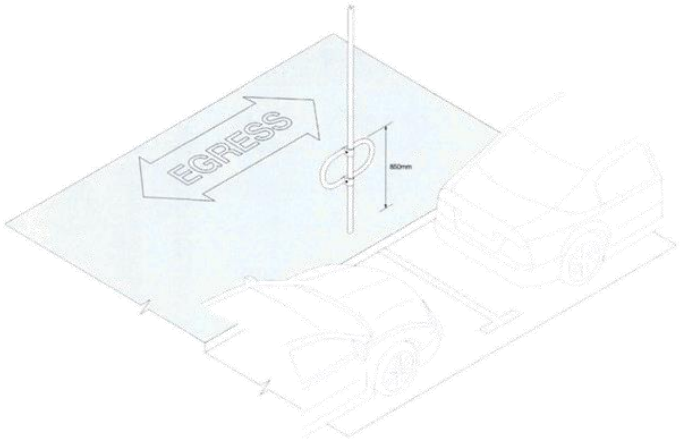


2 x 8mm Allen head bolts per clamp

Front

Side

Layout guidelines



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BIKE PARKING EXPERTS®

BICYCLE NETWORK

Towel Hitching™

Zinc treated finish

Features

The Towel Rail is a space-efficient solution for parking single bikes against a wall, useful in narrow corridors.

- Space-efficient, can be fitted to narrow corridors
- Can be supplied in powdercoat or galvanised finish
- Accommodates all types and sizes of bicycle
- Is easy to use with any bike lock
- Requires no lifting

Dimensions

TOP

SIDE

Specifications

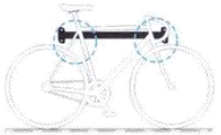
Material options
- Powder coated
- Stainless steel
- Zinc treated
- Galvanised

Fixing options
- Welded flange - Bolt On

Recommended fasteners
- Zinc plated dynabolts (M8 x 40mm)

Dimensions
900mm [w] x 150mm [d]

Locking points



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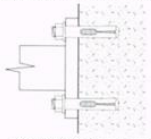
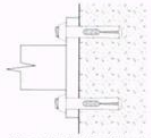
Fixing options

Fix to a wall using 4x fasteners or Shear Nuts



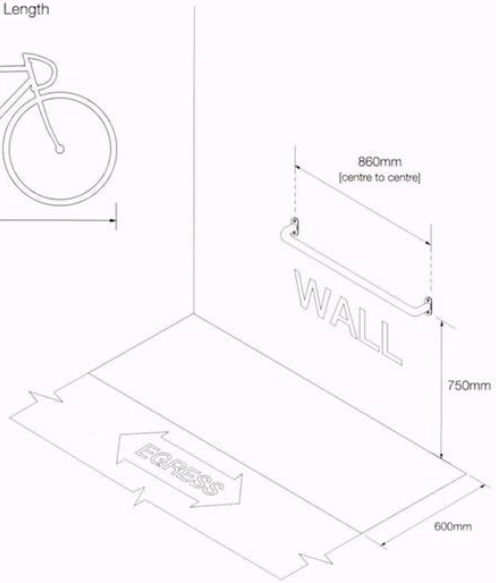
Shown with M8 x 40mm fastener



Shown with M8 x 40mm Shear Nuts

Layout guidelines

Typical Bicycle Length



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Australian Catholic University, Melbourne Campus

Environmental Wind Assessment - 115B Victoria Parade & Young Street Carpark

Job number 601875-09
Issue 01 | 14 January 2015

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East Melbourne
VIC 3002
Australia
www.arup.com

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It is not intended for and should not be relied upon by any third party and no responsibility is undertaken to any third party.

ARUP

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ACU

Figure 1.1 - Existing site conditions - view from south

Figure 1.2 - Location plan

1.0 Introduction

The following sketchbook pertains to the proposed development at the Australian Catholic University (ACU) Melbourne Campus, located in Fitzroy. Arup have been engaged to undertake an environmental wind assessment in the townplanning stage in relation to the likely wind conditions around the proposed development sites at 115B Victoria Parade and Young Street Carpark (refer to Figs. 1.1 & 1.2).

The study has been undertaken to provide an assessment of the environmental wind conditions and their relationship to the anticipated functional uses of public spaces surrounding the development.

It is to be noted that the following review is based on preliminary site massing. While computational studies have been undertaken at this stage, it is recommended that a full wind tunnel assessment is undertaken in later design stages to further inform the design.

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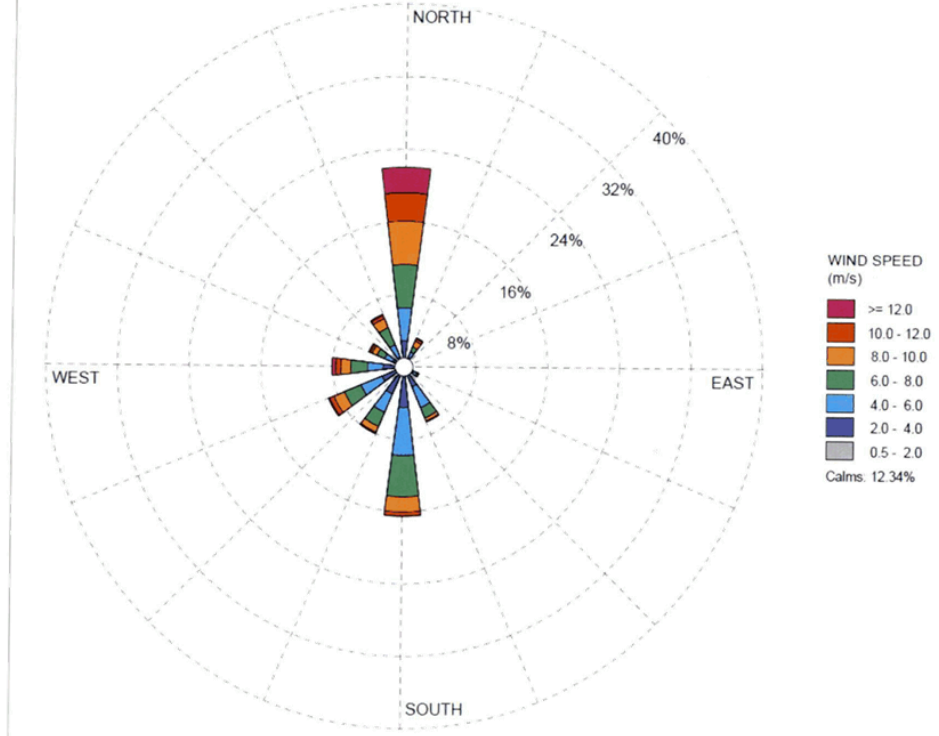

Figure 2.1 - Annual wind rose for Melbourne

2.0 Basis of Assessment

Melbourne Wind Data and Climate

The statistical wind assessment is based on long-term Bureau of Meteorology weather data recorded at Tullamarine Airport, Melbourne for the period 1970 to 2009. Figure 2.1 provides a summary of all measured data for this period in the form of a wind-rose.

The local wind environment in Melbourne is generally characterised by moderately high wind speeds. The directionality of the wind changes with the seasons, although there is a consistently high occurrence of quite strong winds from the north in all seasons. These northerly winds will approach the site from the northern suburbs. A secondary prevailing wind comes from the south, with increasing frequency during the summer and spring months. These are the two key wind conditions that dominate on the pedestrian level wind environment. Afternoon wind speeds are typically higher than in the morning.

A review of the historical wind data indicates that the mean wind speed at Melbourne Airport is 5.1m/s. This is measured at 10m above ground level as per the Bureau of Meteorology standard practice. Wind conditions in Fitzroy are quite different to the airport because of differences in surrounding terrain. The passage of wind across built-up areas to the east, north and west would likely have the effect of slowing down the wind at pedestrian height. Wind from the south would travel across Port Phillip Bay, before reaching the bayside suburbs. This would still likely result in lower wind speeds at pedestrian height, compared to at the airport.

Site Wind Transformation

The measured wind data has been processed using an industry-standard approach assuming a Weibull approximation to the statistical wind distribution for each wind direction.

The Weibull wind distributions have been transformed from the airport to the site to account for the effects of terrain roughness. This wind transformation has been done in accordance with a well-established approached developed by the UK-based engineering advisory organisation, ESDU.

The methodology for generation wind statistics based on long-term measured data and then transforming this information from the meteorological site to the reference location near the building is identical to the approach used when undertaking a wind-tunnel assessment.

Table 2.1 - Lawson criteria for wind comfort

Annual mean hourly wind speed exceeded 5% of the time [m/s]	
< 2	Outdoor fine dining
2 - 4	Long periods of standing or sitting
4 - 6	Short periods of sitting or standing
6 - 8	Leisurely walking or window shopping
8 - 10	Fast or business walking
> 10	Uncomfortable for all uses

2.0 Basis of Assessment

Environmental Wind Speed Criteria

The wind environment becomes less comfortable as the mean hourly wind speed increases; particular mean hourly wind speed ranges are considered appropriate for different uses. Above a certain threshold, higher mean hourly wind speeds start to cause distress and can potentially create unacceptable wind conditions; these are assessed under distress criteria. Please note that the purpose of this report is to assess pedestrian comfort, not distress.

The choice of wind speed criteria tends to vary from one region to the next. However, the generally accepted industry approach for comfort, the Lawson comfort criteria, is outlined in Table 2.1. This approach is based on the 95th percentile annual mean hourly winds speed and has formed the basis of the assessment.

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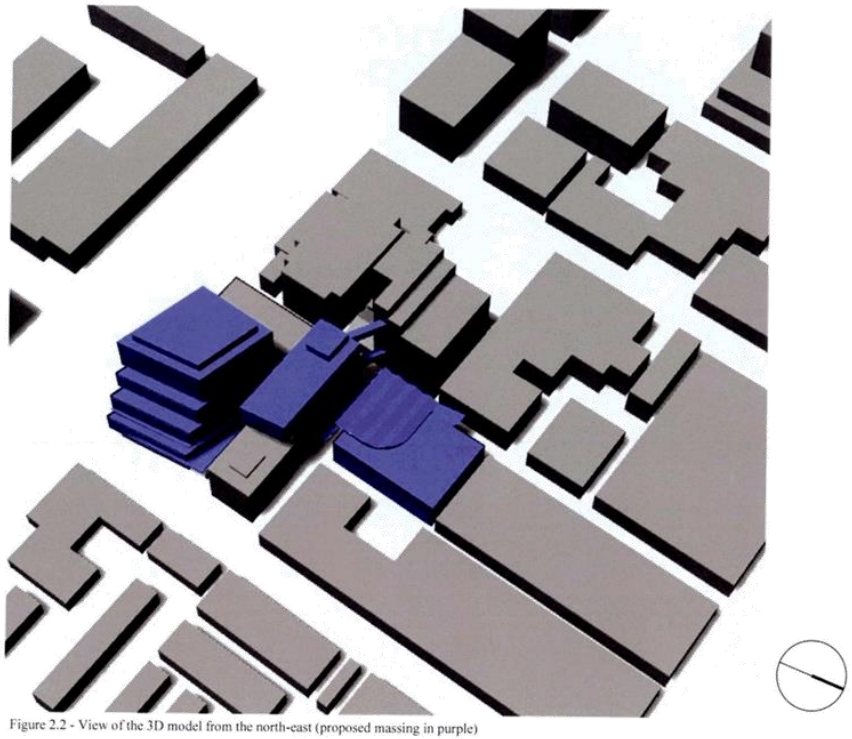

Figure 2.2 - View of the 3D model from the north-east (proposed massing in purple)

2.0 Basis of Assessment

Numerical Wind

The assessment provided in this report is based on computer simulation of wind flow through the proposed development and surrounding buildings using computational fluid dynamics (CFD).

Arup have used the recommended best practice approach to wind modelling described in *Best Practice Guideline for the CFD simulation of flows in the urban environment* produced as part of European intergovernmental research organisation COST action 732 (Franke, et al., 2007) as a guide. A steady state, Reynolds-averaged Navier-Stokes (RANS) approach is used for the assessment, which is compatible with the best practice guidelines and in accordance with the methods used by Blocken et al (2011 and 2012) for environmental wind assessment.

Similarly to the Lawson criteria, the RANS approach is based on hourly averaged mean wind speeds and as such, is compatible for use with the Lawson criteria, as applied in this assessment.

Vegetation in the streetscape and surrounding parks has been excluded from the analysis due to limitations with CFD being able to accurately model their effect. In general, vegetation has a beneficial effect by reducing wind speeds and consequently some conservatism is built into the CFD assessment.

Figure 2.2 shows an image of 3D model, which is a simplified version (for the purposes of the simulation) of the 3D model provided by Lyons on 21 December 2015, which includes the proposed site massing and surrounding buildings. Based on our experience, the simplifications made to the 3D model will not impact on the accuracy of the results and have been undertaken in order to reduce the computational and overall time to complete this assessment.

In general, it is expected that this analysis will be more conservative than wind tunnel testing and appropriate for this stage in the development process. In subsequent design phases, it is recommended that wind tunnel testing is undertaken to confirm environmental wind patterns and the specific environmental conditions, as well as specific wind mitigation measures.

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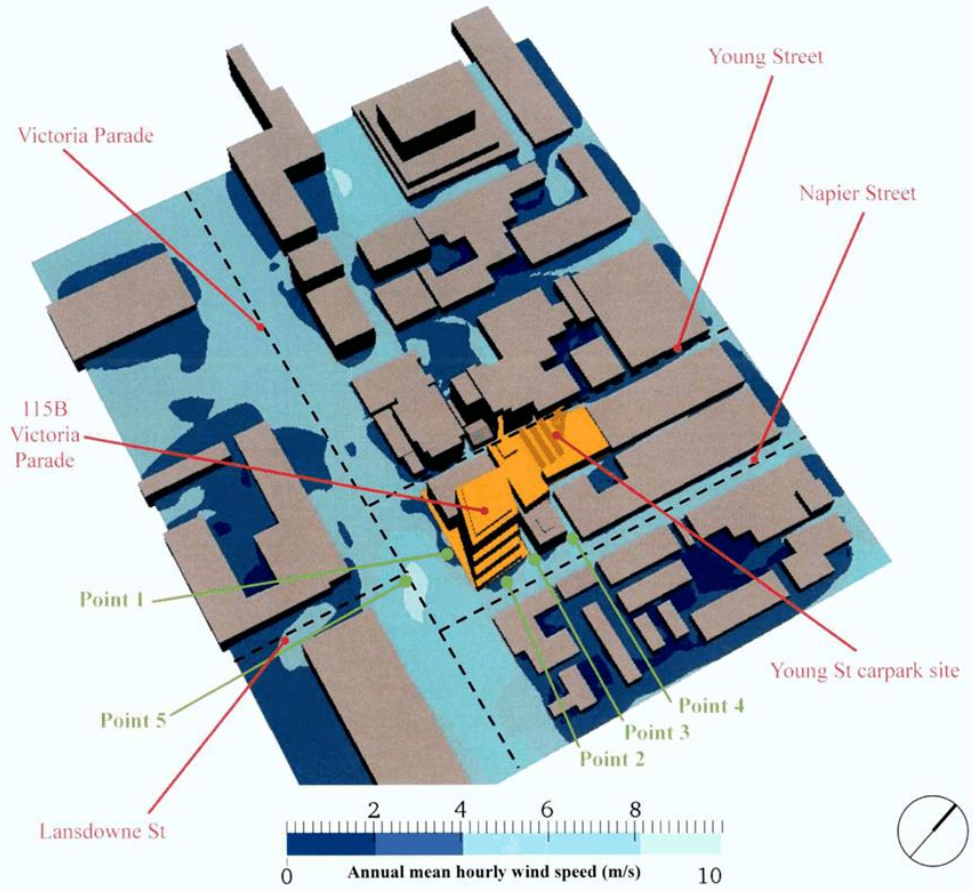

Figure 3.1 - Lawson criteria for wind comfort, viewed from the south-east (proposed sites shown in in orange)

3.0 Assessment Results

Figure 3.1 provides a contour plot of the annual mean hourly wind speeds as applicable to the Lawson comfort criteria. The section plane is at pedestrian height (1.5m above the ground) through the area of interest around the site. The lower annual mean hourly wind speeds (2-8 m/s) are acceptable for sitting and standing activities and the higher speeds (8-10 m/s) indicate areas that are more suited as a general thoroughfare where pedestrians have a clear objective in walking from one place to another, such as business walking.

In general, all pedestrian areas immediately adjacent to the proposed sites are likely suitable for pedestrian standing (or sitting for a short time) or walking, with some areas suitable for pedestrian sitting for a longer duration. This is summarised in Table 3.1 below.

Table 3.1 - Summary of average comfort ratings

Point	Location	Average comfort rating
1	Victoria Parade side of 115B Victoria Parade	suitable for short to long periods of sitting or standing
2	Napier St side of 115B Victoria Parade	suitable for short to long periods of sitting or standing
3	Laneway between 115 & 115B Victoria Parade	suitable for short periods of sitting or standing or leisurely walking or window shopping
4	Little Victoria St (north side of existing 115 Victoria Parade)	suitable for short periods of sitting or standing or leisurely walking or window shopping
5	Victoria Parade, in the vicinity of Lansdowne St intersection	suitable for leisurely or business walking

Whilst the 115B Victoria Parade building is relatively tall in nature, in comparison to adjacent buildings, the staggered nature of the massing and canopies/overahangs on Napier Street (point 2) and Victoria Parade (point 1) help with mitigating the likelihood of downdraughts impacting on pedestrian comfort. The canopy on the south side of the building (point 1) will likely provide adequate protection to the entrance indicated at this point.

There are some higher wind speeds on Victoria Parade in the vicinity of the Victoria Parade / Lansdowne St intersection (point 5). This location is likely only suitable for walking but this is deemed to be acceptable given that it is an intersection and people are likely not to be spending periods of time at this location.

Attachment 13 - PLN16/0085 - Public Notification ACU Development Plan Part 12

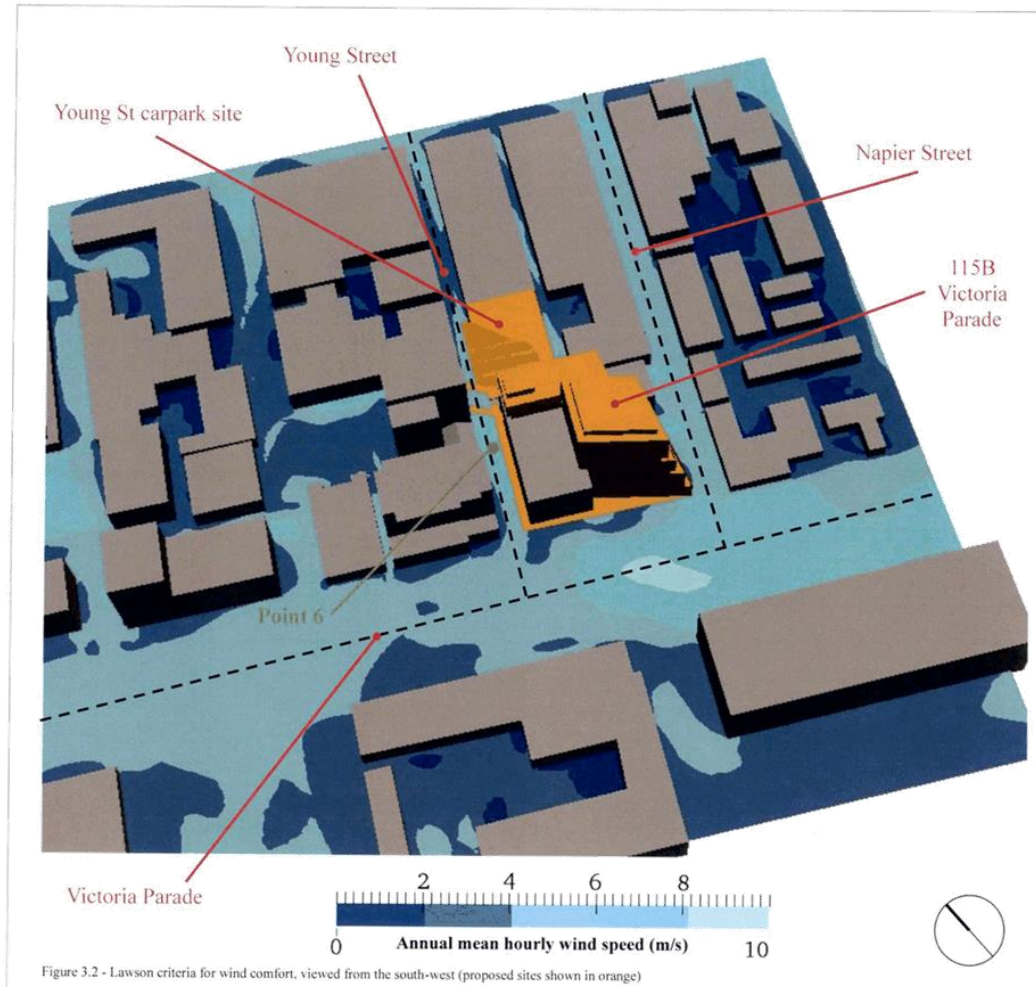

Figure 3.2 - Lawson criteria for wind comfort, viewed from the south-west (proposed sites shown in orange)

3.0 Assessment Results

Figure 3.2 shows a view from the south-west, providing a better view of the Young St likely comfort levels adjacent to 115/115B Victoria Parade sites. There are slightly raised wind speeds at this location (point 6), relative to the surrounding areas. This location is still suitable for sitting for short periods or leisurely walking (as per Table 3.2) and given that it is a thoroughfare, the likely conditions are considered to be comfortable for such activities.

Table 3.2 - Summary of average comfort ratings

Point	Location	Average comfort rating
6	Young St side of 115/115B Victoria Parade	suitable for short periods of sitting or standing or leisurely walking or window shopping

Attachment 13 - PLN16/0085 - Public Notification ACU Development Plan Part 12

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4.0 Summary

In summary, an environmental wind assessment has been undertaken in relation to the likely wind conditions around the proposed development sites at 115B Victoria Parade and Young Street.

A statistical wind assessment was undertaken based on long-term Bureau of Meteorology weather data, which has been transformed from the meteorological site to the reference location using industry standard methodologies. A computer simulation of wind flow through the proposed development and surrounding buildings was subsequently undertaken using CFD.

The results have been assessed against the generally accepted industry approach for comfort, the Lawson comfort criteria. This approach is based on the 95th percentile annual mean hourly winds speed and has formed the basis of the assessment.

In general, all pedestrian areas immediately adjacent to the proposed sites are likely suitable for pedestrian standing (or sitting for a short time) or walking, with some areas suitable for pedestrian sitting for a longer duration. There are some locations in the surrounding public domain of the proposed sites that are only suitable for walking but given that these areas are likely thoroughfares, the conditions are deemed to be acceptable.

5.0 References

Blocken, B., Stathopoulos, T., Carmeliet, J. & Hensen, J., 2011. Application of CFD in building performance simulation for the outdoor environment: an overview. *Building Performance Simulation,* Volume 4, pp. 157-184.

Blocken, B., Janssen, W. D. & van Hooff, T., 2012. CFD simulation for pedestrian wind comfort and wind safety in urban areas: General decision framework and case study for the Eindhoven University Campus. *Environmental Modelling & Software,* Volume 30, pp. 15-34.

Franke, et al., 2007. *Best Practice Guideline for the CFD Simulation of Flows in the Urban Environment,* s.l.: COST 732: Quality Assurance and Improvement of Microscale Meteorological Models.

Attachment 13 - PLN16/0085 - Public Notification ACU Development Plan Part 12

Attachment 13 - PLN16/0085 - Public Notification ACU Development Plan Part 12

11.9 Amendment C231 Queens Parade Clifton Hill

Executive Summary

Purpose

The purpose of the report is for Council to consider:

(a) seeking authorisation to exhibit Amendment C231 in accordance with the *Planning and Environment Act 1987,* which implements the Clifton Hill Built Form Review; and

(b) requesting the Minister for Planning to introduce interim controls for additional areas of Queens Parade.

Key Issues

Interim built form controls (Design and Development Overlay Schedule 16) currently only apply to the western end of Queens Parade and expire on 30 March 2019.

Amendment C231 proposes to apply permanent built form controls to the length of the Queens Parade, excluding residentially zoned land, which would provide clear guidance for future built form along Queens Parade. Amendment C231 has been informed by the Clifton Hill Built Form Review, which has undertaken considerable testing of potential future buildings looking at various street wall heights, building heights and setbacks. The testing ensures that views to significant landmarks are retained as well as important heritage streetscapes.

It is proposed to seek Interim DDO control over Precincts 2C, 2D, 3A, 4 and 5 which form the remainder of the Queens Parade area.

The Amendment also proposes to rezone two parcels of land from Commercial 2 to General Residential (in one case) and to Commercial 1 (in the other case).

The Amendment also proposes to implement the recommendations of the Queens Parade Built Form and Heritage Analysis & Recommendations by GJM Heritage, which includes re-grading some heritage properties.

Financial Implications

Other than officer time and the administration fee to the Department of Environment, Land, Water and Planning (DELWP) there are no financial costs associated with requesting the Minister for Planning to authorise an amendment.

If the full amendment is advanced and exhibited as recommended, the cost of the exhibiting and progressing the amendment through a public consultation and planning panel process is estimated to be in the order of $100,000 - $150,000 (assuming a 6 day planning panel).

PROPOSAL

In summary that Council:

(a) endorse the Queens Parade, Clifton Hill Built Form Review as a basis for Amendment C231;

(b) request the Minister for Planning in accordance with Section 8 (1) (b) and 20 (4) of the *Planning and Environment Act 1987*, to introduce interim Design and Development Overlay Schedule to the areas of Queens Parade within Precincts 3A, 4 and 5; and

(c) seek authorisation from the Minister for Planning to prepare Amendment C231 to the Yarra Planning Scheme in accordance with section 8A of the *Planning and Environment Act 1987*.

11.9 Amendment C231 Queens Parade Clifton Hill

Trim Record Number: D17/198394
Responsible Officer: Director Planning and Place Making

Purpose

1. The purpose of this report is for Council to consider:

   (a) seeking authorisation to exhibit Amendment C231 in accordance with the *Planning and Environment Act 1987,* which implements the Clifton Hill Built Form Review; and

   (b) requesting the Minister for Planning to replace the existing interim controls (which only refer to two precincts) with interim controls that cover the length of Queens Parade.

Background

Interim controls - current

2. On 22 November 2016 Council authorised officers to undertake a three stage urban design and heritage assessment of Queens Parade which would provide the basis for future planning controls.

3. Stages 1 and 2 of the assessment are complete. They saw the introduction of an interim Design and Development Overlay (DDO 16) at the western end Queens Parade including the site at 26-56 Queens Parade, Fitzroy North. On 7 February 2017, Council requested the Minister to use his powers under Section 20(4) of the *Planning and Environment Act 1987* to introduce the interim control by way of a Ministerial amendment. The amendment was gazetted on 20 March 2017 and the interim DDO16 was introduced into the planning scheme on that date.

4. The interim DDO provides mandatory height controls and preferred front, side and rear setbacks at 26-56 Queens Parade, although Council had sought mandatory setbacks, especially for the front setback. The overall maximum building height sought by Council was 8 storeys, however the interim control by the Minister for Planning provided a maximum building height of 31m or 10 storeys. The interim DDO requirements are being used in the current VCAT appeal that is underway.

5. The interim controls were introduced for a period of two years while an amendment to introduce a permanent DDO for Queens Parade is prepared, considered and finalised.

Discussion

26-52 Queens Parade VCAT case

6. VCAT heard this case in September and handed down an Interim Order which raised areas of concern the Tribunal had with the proposal. The Order gives the proponent the opportunity to address the matters raised by the Tribunal through submitting amended plans. The issues are:

   (a) whether the development responds appropriately to the significance of the heritage place; and

   (b) whether the presentation to Queens Parade responds to the (interim) DDO.

7. The proponent will prepare a response to the Order and the Tribunal will make a final decision on the application.

8. The lesson from the VCAT interim Order for Queens Parade was that the Tribunal found that the heights were acceptable and that it was a workable control.

Queens Parade Built Form Review and Heritage Assessment

9. This report presents the findings of Stage 3 of the urban design and heritage assessment of Queens Parade and includes a draft DDO which would introduce permanent controls along the majority of Queens Parade from Alexandra Parade to Hoddle Street (NB. it excludes residential zoned areas that already have height limitation through the zone requirements).

10. Hansen Partnership, in association with GJM Heritage Consultants, has undertaken the Queens Parade Built Form Review which provides the detailed analysis and a thorough and strategic basis for the future planning of the area. This work provides the strategic justification and evidence to support and justify the DDO requirements and would be relied upon at a Planning Panel.

11. Part of the review has seen considerable testing of potential future building forms, looking at various street wall heights, building heights and setbacks and their impact on views to significant landmarks and significant heritage streetscapes. The testing has formed the basis of the controls in the DDO which are designed to ensure that the prominence of the heritage streetscape and heritage buildings is retained whilst allowing for capacity for new development which responds to the existing and future character.

12. The analysis has indicated that more intensive development is likely to occur at the western and eastern ends of Queens Parade, based on the large size of properties and existing planning approvals. The middle section of the centre (Precinct 4) is likely to have less intensive development in order to protect the heritage street scapes, views to key heritage buildings and to protect the amenity of adjacent residential areas. This is indicated in the diagram below.

13. The key landmarks are St John the Baptist Church at 61 Queens Parade and the former ANZ Bank Building at 370 Queens Parade. Protecting the significant heritage strip shopping centre streetscape in Precinct four has also been a strong consideration in the Built Form Review.

14. The Review recommends detailed controls which include:

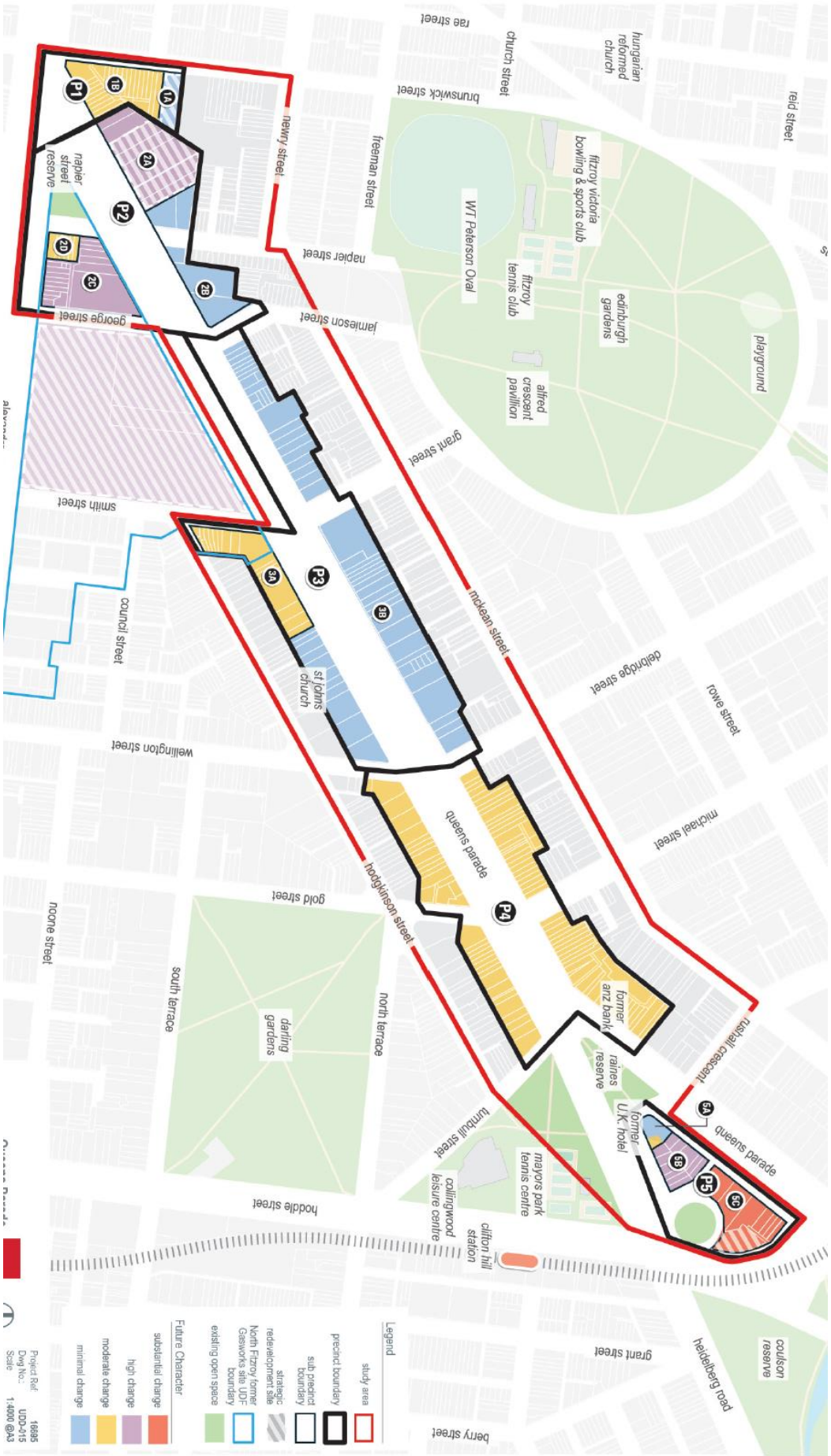
    (a) mandatory controls to maintain views to St John's and the former ANZ Bank building;

    (b) mandatory controls in Precinct 4 to protect the heritage streetscape; and

    (c) preferred controls in other heritage and non-heritage areas to manage the street wall height of infill development and a minimum setback of all new development above the heritage street wall.

15. These controls reflect advice prepared by GJM Heritage contained in the Queens Parade Built Form Review. This advice would be a supporting document for the amendment when it is exhibited.

16. The map below illustrates the various precincts in Queens Parade and the table below sets out the preferred built form for each precinct and provides a summary of the proposed controls. The mandatory controls are highlighted in red type for emphasis. Full details of the requirements are contained in the draft DDO for the permanent controls.



Map outlining the various precincts in Queens Parade

Precinct	Sub precinct	Planning controls	
Precinct 1 **Brunswick Street Precinct** **Low rise rear extensions behind fine grain heritage frontages**	1B	Building heights	9m (3 storey) preferred maximum building heights. 9m Mandatory for Lot 1 on Title Plan TP806921
		Front street wall height	Retain existing heritage façade. To match existing parapet height of 460 Brunswick
		Upper level setback	Minimum 6m preferred setback except mandatory 5m on corner (460 Brunswick Street)
Precinct 2 Boulevard Precinct **Mid-rise infill addressing the boulevard**	2A	Building heights	31m (10 storeys) mandatory maximum
		Front street wall height	Match height of existing heritage street wall or 10m (3 storey) mandatory
		Upper level setback	For heritage buildings: 8m setback between 3 and 5 storeys, 11m above 5 storeys For non-heritage buildings: 5m setback between 3 and 5 storeys, 8m above 5 storeys
	2C	Building heights	28m (8 storeys) maximum
		Front street wall height	18m (5 storeys) on Queens Parade and George Street On Napier St, match parapet height of terraces
		Upper level setback	6m
	2D	Building height	Prefer two additional storeys at rear to 18m
Precinct 3 St John's Precinct **Renewal infill at corner of Smith/Queens which protects view of St John's belfry and spire**	3A	Building height	18m (5 storey) preferred
		Front street wall height	Retain heritage façade height
		Upper level setback	6m mix preferred and mandatory.
Precinct 4 Activity Centre Precinct **Recessive upper level development behind a uniform street wall**	4	Building height	21.5m (6 storey) mandatory maximum
		Front street wall height	Retain height of existing heritage façade. Minimum 8m (2 storey) where heritage façade. Maximum 11m or matching where adjacent heritage building is taller
		Upper level setback	Minimum 6m mandatory in heritage 5m preferred in side street Minimum 6m preferred in non-heritage.
Precinct 5 North Eastern Precinct **Mid-rise form near heritage buildings rising to high rise form on this island precinct**	5A	Building height	18m (5 storeys) preferred
		Front street wall height	Retain height of existing heritage façade mandatory
		Upper level setback	5m where adjoins heritage building preferred
	5B	Building height	28m (8 storey)
		Front street wall height	11m (3 storey) but mandatory at either side of former Clifton Motor Garage (205-211 Queens Parade)

Precinct	Sub precinct	Planning controls	
		Upper level setback	6m but mandatory at former Clifton Motor Garage (205-211 Queens Parade). Minimum 6m preferred elsewhere
	5C	Building height	Preferred maximum 49m (14 storey)
		Front street wall height	Preferred 35m (10 storey)
		Upper level setback	Preferred 10m

17. The Heritage assessment also supports additional properties to be included within a Heritage Overlay, the removal of some properties and changes to the gradings of specific buildings and updates to Statements of Significance for some overlays. These changes are outlined in paragraph 24 below.

Mandatory vs Preferred controls

18. The mix of mandatory and preferred requirements reflects the differences in the level of heritage significance and intactness between the various heritage areas along Queens Parade in the activity centre. The State Planning Practice Notes 59 and 60 provide clear guidance about the current circumstances in which mandatory controls can be applied and advises that *mandatory provisions will only be considered in circumstances where it can be clearly demonstrated that preferred provisions are insufficient to achieve desired outcomes*. The Practice Notes set out a series of questions that Councils must consider when deciding if controls should be mandatory.

19. The Practice Note sets a very high threshold to justify mandatory controls, especially for building heights within activity centres, and has been used by Planning Panels as a reason to not support mandatory controls. Council's advocacy to the State Government is seeking a less restrictive threshold in the Practice Note that has greater regard to the circumstances of the local area and the preferred level of change. Under the current Practice Note it is uncertain whether mandatory requirements would be accepted. The work of the consultants has informed the locations and circumstances where mandatory controls are being sought.

20. In the case of Queens Parade, a mix of **mandatory** provisions (street wall heights, upper level setbacks and overall building heights) are proposed on heritage grounds to:

    (a) protect the view to the spire of St John's Church from the corner of Smith Street and Queens Parade;

    (b) to protect the view to the former ANZ bank building from key vantage points along Queens Parade;

    (c) to retain the highly intact streetscape of parts of the retail core which have a consistent street wall height and which includes a building on the Victorian Heritage Register which is of recognised State significance;

    (d) to protect the individually significant building at 460 Brunswick Street (corner Queens Parade); and

    (e) to protect the distant city views from the Edinburgh Gardens.

21. In relation to Precinct 4, officers are recommending mandatory maximum building heights of 21.5m (6 storey) on the basis that Queens Parade is a neighbourhood centre, that more significant growth will be accommodated by those areas at the western (Precinct 2) and eastern (Precinct 5) ends of Queens Parade and to further protect the scale and views of the heritage street scapes.

22. **Preferred** controls are proposed in other areas.

Amendment C231

23. Amendment C231 proposes the following:

    (a) introduce a new permanent Design and Development Overlay (DDO16) along the length of Queens Parade (excluding residential areas) into the Yarra Planning Scheme;

    (b) rezone land at 660-668 Smith Street and 1-41 Queens Parade from C2Z to C1Z;

    (c) introduce a new HO (HO498) to the row of terraces at 472-484 Napier Street and give them an individually significant grading;

    (d) introduce a site specific heritage overlay (HO496) to 61-87 Queens Parade (the St John's Church complex) to provide for internal heritage controls on the organ;

    (e) extend HO327 over the full extent of the Modern façade of the former K.G. Luke factory site at 26 Queens Parade;

    (f) extend HO330 to include all of Raines Reserve (currently only partially in HO);

    (g) remove 201-217 Queens Parade and 10-12 Dummett Crescent from HO330;

    (h) remove the following properties from HO327 (Fitzroy North Precinct) and move to HO330 (Queens Parade Precinct) – an administrative fix up to better reflect that properties in Queens Parade should be included in the Queens Parade heritage precinct:

        (i) 390A Queens Parade; and

        (ii) Rears of 304, 312, 316 and 380 Queens Parade; and

    (i) change the heritage grading of the following properties:

        (i) 662 Smith Street (former Fire Station) from contributory to individually significant;

        (ii) 7-11 Queens Parade from contributory to not contributory;

        (iii) 137 Queens Parade from contributory to not contributory;

        (iv) 316 Queens Parade from contributory to not contributory;

        (v) 350 Queens Parade from ungraded to contributory;

        (vi) 390A Queens Parade (two storey building in north-east corner) from ungraded to contributory;

        (vii) 390A Queens Parade (all other buildings except two storey building in NE corner) from ungraded to not contributory; and

        (viii) 402 Queens Parade from contributory to not contributory; and

    (j) update Appendix 8 to make the various changes outlined above.

Design and Development Overlay

24. The proposed DDO16 includes general and specific precinct controls which respond to the varied context along the length of Queens Parade.

25. Details of the mandatory provisions designed to protect key views are outlined at paragraph 15 above consistent with the Practice Notes. Preferred controls have been applied elsewhere to provide flexibility and accommodate variations and innovative designs. Part of the work undertaken by Hansen Partnership has looked at current planning permit applications that have either been approved or are being considered by Council to test the proposed DDO heights and setbacks. Hansen Partnership has found that the heights and setbacks being proposed accord with the scale of development that has already been approved. It is important that the DDO reflects the reality of development decisions approved by Council or VCAT.

26. Where preferred building heights are proposed in the DDO, a range of performance based controls would also need to be met to ensure appropriate development.

Interim controls

27. The current interim controls apply only to precincts 1 and 2. Officers propose that these interim controls remain in place and that Council seek the introduction of additional interim controls over the remainder of Queens Parade – Precincts 3, 4 and 5.

28. The justification for seeking revised interim controls is that there are development pressures in Queens Parade and Council wants to be able to determine the scale of the development while the permanent controls are being considered. Interim controls will also provide greater certainty about the preferred development outcomes for Queens Parade and will assist in preventing planning applications that might try to pre-empt the permanent controls.

29. The statutory process for considering permanent controls will take a year at least, possibly more, which is why in the context of development pressures it makes sense to seek interim controls.

Current DDO vs proposed DDO

30. The current DDO 16 establishes controls for Precincts 1 and 2. There are some changes between the two controls which are explained below. These changes follow the more in depth study of the area by the urban design and heritage consultants.

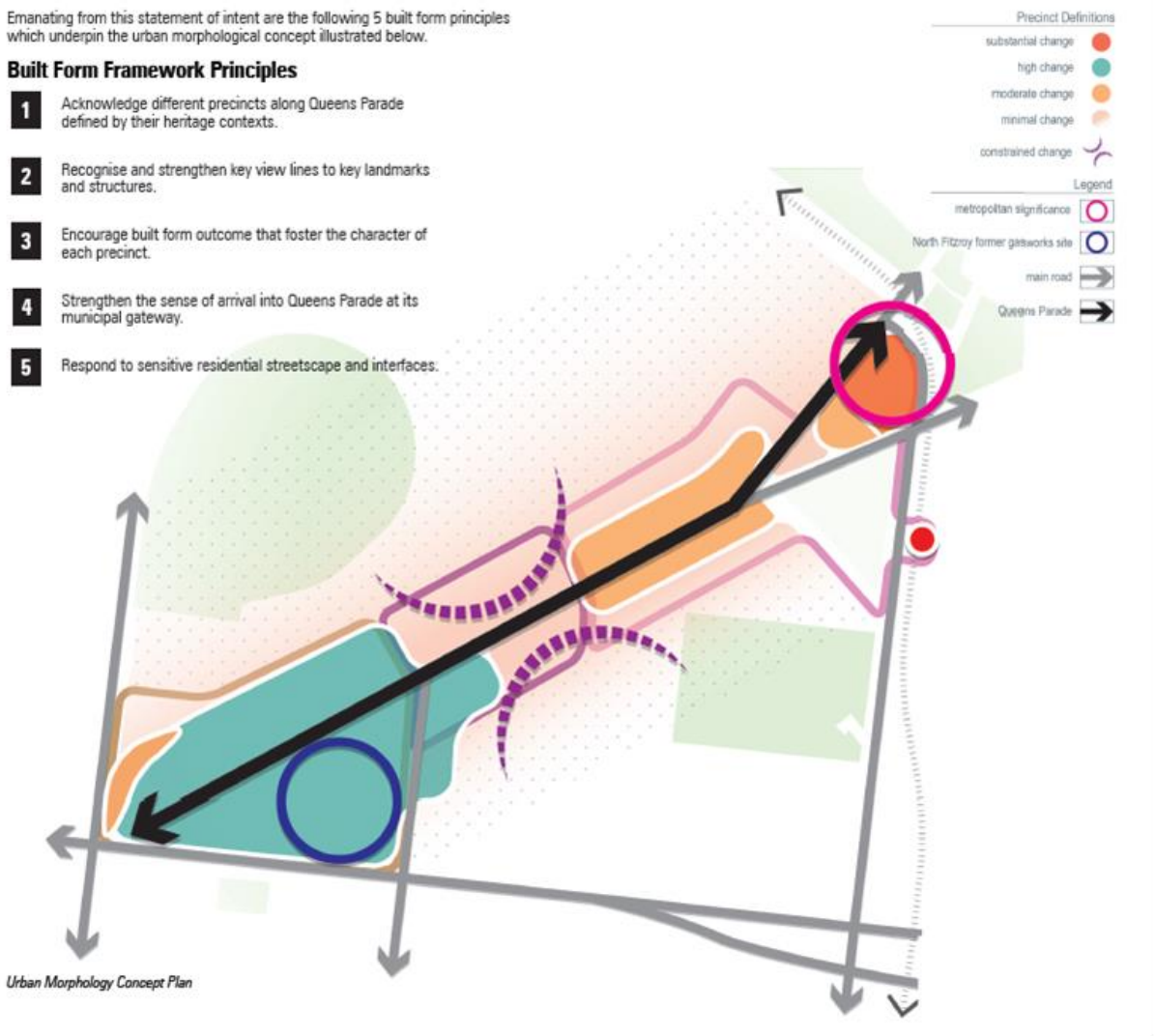
Control	Approved interim control	Proposed DDO	Reason for change
Precinct 1			
Building height	9m mandatory	9m preferred 9m mandatory for Lot 1 on Title Plan TP806921	A preferred height will enable flexibility to respond to the varied existing building forms on Brunswick Street and Queens Parade.
Setbacks from side and rear boundary	Res Code B17	Res Code B17	Unchanged
Upper level setback	8 metres preferred	Minimum 6m preferred on all sites except 460 Brunswick Street	The built form testing conducted by Hansen and GJM showed that the reduced setbacks were sufficient to protect 460 Brunswick Street and heritage streetscapes.
Development on and adjoining 460 Brunswick Street	Built to boundary at ground level – preferred. Retain existing front setback. Match existing street wall height – must not exceed the parapet height of that building mandatory	Built to boundary at ground level - preferred Match existing street wall height – mandatory 5m upper level setback mandatory	Unchanged Made mandatory Unchanged
Setbacks from side and rear boundary	ResCode Standard B17	ResCode Standard 17	Unchanged

Control	Approved interim control	Proposed DDO	Reason for change
Precinct 2A			
Building height	31 metres mandatory	31 metres mandatory	Unchanged
Street wall height	Retain height of existing heritage façade - mandatory. Where no heritage façade, mandatory 10m Street wall of development that adjoin the individually significant building should be below the parapet height of the heritage building.	Retain height of existing heritage façade - mandatory. Where no heritage façade, mandatory maximum 10m Street wall of development that adjoin the individually significant building should be below the parapet height of the heritage building.	Unchanged
Front setback	Built to boundary to a maximum height of 10 metres - preferred	Built to boundary to a maximum height of 10 metres - preferred	Unchanged
Upper level setback Above heritage façade	Minimum 8m preferred setback from 10 m to 16m. Minimum 11m setback above 16m.	Minimum 8m preferred setback from 10 m to 16m. Minimum 11m setback above 16m.	Unchanged
Upper level setback Above new street wall where no existing heritage façade	Minimum 5m preferred setback from 10 m to 16m. Minimum 8m setback above 16m.	5m preferred setback from 10 m to 16m. 8m setback above 16m.	
Setbacks from rear boundaries North and North West adjacent to NRZ and GRZ.	Res Code B17 measured from the rear boundary of adjoining properties up to 10m (3 storeys). Setback within a 45 degree angle line measured from 10m up to 25 m.	Res Code B17 measured from the rear boundary of adjoining properties up to 10m (3 storeys). Setback within a 45 degree angle line measured from 10m up to 25 m.	Unchanged

Control	Approved interim control	Proposed DDO	Reason for change
Setbacks from side boundary – east adjacent to NRZ	0m setback to match party wall of existing adjoining development up to 10m. Setback within a 45 degree angle line measured from 10m up to 25m	0m setback to match party wall of existing adjoining development up to 10m. Setback within a 45 degree angle line measured from 10m up to 25m	Unchanged
Setback from side boundary – east adjacent to MUZ	0m setback to match party wall of existing adjoining development or 10m where no party wall exists. Minimum of 9m setback from the windows/balconies of adjoining apartments up to 16m. Minimum 15m setback above 16m.	0m setback to match party wall of existing adjoining development or 10m where no party wall exists. Minimum of 9m setback from the windows/balconies of adjoining apartments up to 16m. Minimum 15m setback above 16m.	Unchanged
Setbacks from side/rear boundaries – west and north west adjacent to MUZ	0m setback to match party wall of existing adjoining development to the west or 10m where no party wall exists. Setback within a 45 degree angle line measured from 10m.	0m setback to match party wall of existing adjoining development to the west or 10m where no party wall exists. Setback within a 45 degree angle line measured from 10m.	Unchanged

Rezoning

31. The amendment proposes to **rezone** two parcels of land at 660-668 Smith Street and 1-41 Queens Parade from Commercial 2 Zone (C2Z) to Commercial 1 Zone (C1Z). Refer to the plan below.



32. This area was identified in Council's adopted Business Industrial Land Strategy for potential rezoning to a mixed use zone. Given the commercial nature of this small precinct a Commercial 1 zone is considered more appropriate and would support the provision of ground floor commercial uses in any redevelopment.

33. The properties at **660-668 Smith Street** are two double storey warehouses dating from approximately the 1970s. The properties at **1-41 Queens Parade** comprise the following mix of buildings and all are currently zoned Commercial 2 (C2Z):

    (a) 1 Queens Parade is currently occupied by a single storey shop which sits on a larger block and is currently occupied by a bicycle store;

    (b) 7 Queens Parade is a single storey warehouse with a tall parapet with a construction date estimated to be in the 1930s;

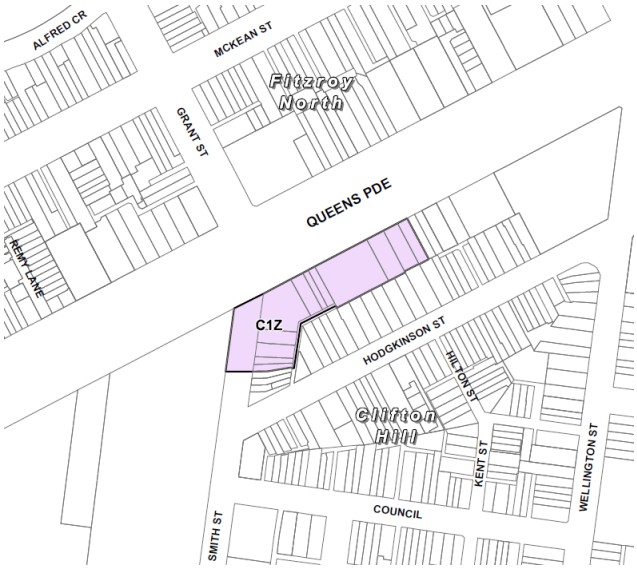
    (c) 9-11 Queens Parade is a pair of two storey, simple Victorian commercial buildings with shops at ground level and dwellings above. (NB. There is no number 13 Queens Parade);

    (d) 15 Queens Parade is a large site currently occupied by a single storey hardware and garden centre built in the 1970s or 1980s;

    (e) 35 Queens Parade is occupied by a single storey, double fronted Victorian dwelling which is currently used as an office building;

    (f) 37 Queens Parade is a single storey, Victorian era shop built to the boundary and currently used as a bicycle shop. (NB. There is no 39 Queens Parade); and

(g) 41 Queens Parade is a narrow, single fronted dwelling with a pitched roof which has undergone alteration and is currently used as a medical suite.



Plan of land proposed for rezoning to Commercial 1 Zone

34. It is proposed to rezone these above properties to a Commercial 1 Zone (C1Z) which would allow the development of dwellings above ground level. The properties to the immediate south of these properties in Smith Street are zoned C1Z and it makes sense in this location to rezone the remainder of the properties to C1Z as the C1Z is the most appropriate commercial zone for an activity centre. An Environmental Audit Overlay will also need to be applied to the properties.

<u>New heritage overlay</u>

35. **472-484 Napier** Street comprises Elizabeth Terrace, a row of five double story Victorian terraces with an elaborate central parapet (476-484 Napier Street) and a freestanding double storey terrace at 472 Napier Street which is on the corner of Alexandra Parade. The photo below illustrates the terraces. GJM Heritage Consultants have identified them as a highly intact example of Victorian terraced housing. They are currently in the Commercial 2 Zone and it is not proposed to rezone them as part of this amendment. It is proposed, however, to add them to the Heritage Overlay (HO498) and give them an individually significant grading as recommended by GJM.

Photo of 472-484 Napier Street

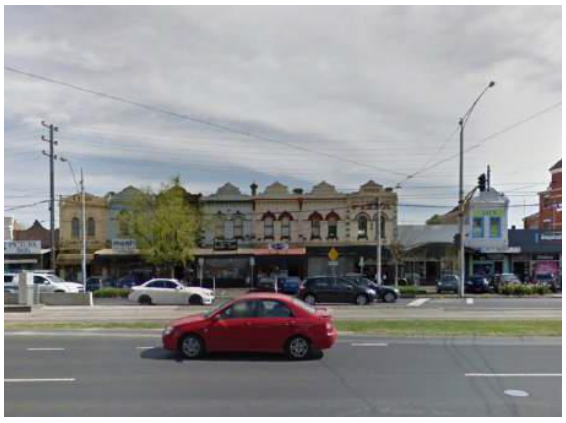


Photo of 350 Queens Parade (in centre of row)

36. **350 Queens Parade** is included in a row of highly intact shops. The remaining properties in the row at 340-356 are currently all graded contributory, except for the one at 350. GJM recommends that 350 Queens Parade be included in the heritage overlay and given the same contributory grading.

Removed from Heritage Overlay

37. **201-217 Queens Parade** and **10-12 Dummett Crescent** lie at the northern end of Queens Parade and neither parcel has a heritage building on it. They sit in a precinct that includes two properties on the Victorian Heritage Register (VHR) - the former United Kingdom Hotel (now McDonalds) and the former Clifton Motors garage which is being considered for inclusion on the VHR. GJM recommends that as there is no other heritage fabric in the precinct, the boundary of HO330 should be redrawn to exclude these properties.

Heritage re-gradings

38. The heritage consultants have reviewed the existing heritage overlays and heritage gradings of all buildings within the study area. This has identified several properties that should have their heritage gradings changed to ensure the heritage overlay correctly reflects their current level of significance.

39. Consequently the amendment proposes to regrade the following buildings in the heritage overlay:

    (a) 662 Smith Street (former Fire Station) from contributory to individually significant;

    (b) 7-11 Queens Parade from contributory to not contributory;

    (c) 137 Queens Parade from contributory to not contributory;

    (d) 316 Queens Parade from contributory to not contributory;

    (e) 350 Queens Parade from ungraded to contributory;

    (f) 390A Queens Parade (two storey building in north-east corner) from ungraded to contributory;

    (g) 390A Queens Parade (all other buildings except two storey building in NE corner) from ungraded to not contributory; and

    (h) 402 Queens Parade from contributory to not contributory.

40. **7 Queens Parade** is a single storey warehouse with a construction date estimated to be in the 1930s although it has been substantially altered since then. Currently graded contributory but GJM advises that the building is not representative of the era of the Queens Parade precinct and should be regraded “not contributory” within heritage overlay.

41. **9-11 Queens Parade** is a pair of two storey, simple Victorian commercial buildings with shops at ground level and dwellings above. They are currently graded contributory but GJM advises *that they broadly reflect the key development period and architectural form of the precinct’s identified significance; however, they are isolated Victorian buildings within an immediate context of ‘not-contributory’ buildings. It is our view that while these two buildings would be identified as ‘contributory’ within a reasonably intact streetscape, this section of HO317 fronting Queens Parade is heavily dominated by buildings identified as ‘not-contributory’ and the contribution nos. 9 and 11 make to the Clifton Hill Western Precinct is very marginal. The pair does not read as part of heritage precinct*. Accordingly, GJM recommends they be regraded “not contributory”.

42. **137 Queens Parade** is occupied by a vacant site following a fire which damaged the building and was subsequently demolished.

43. The redrawing of the boundary of HO330 would also include the full extent of **Raines Reserve**, only half of which is currently included in the heritage overlay. It would also include the northern extent of the Queens Parade carriageway to enable heritage considerations to be given to tram stops, street furniture and the like.

44. The rear of **316 Queens Parade** contains an unremarkable utilitarian building with no heritage features.

45. **390A Queens Parade** is located to the north of the shops (at the rear) and contains four buildings, all of which are currently in the heritage overlay but are ungraded. GJM has advised that only one of the buildings in the north-east corner should be included in the heritage overlay and be given a contributory grading. The remaining three buildings comprise a variety of utilitarian brick and timber structures of no heritage value. This land is currently included in HO327 North Fitzroy precinct and it is proposed to move it to HO330 Queens Parade. This is an administrative change.

46. **402 Queens Parade** is a double storey commercial building in the retail precinct which has a shop at ground level and a residence above. The upper level façade has no decorative features or a parapet.

47. It is proposed to seek interim heritage controls for properties within proposed new heritage overlays including 472-484 Napier Street and 350 Queens Parade, Clifton Hill.

Extension of heritage overlay

48. An error in the mapping of the extent of the heritage overlay affecting the K.G Luke building at 26-56 Queens Parade was revealed during the course of considering the planning application for the development of this site. The error results in the eastern most part of the façade and the buildings behind the facade at 42 Queens Parade not being within the overlay. This is also inconsistent with the statement of significance for the site which refers to the significance of the full extent of the Art Deco facade. It is therefore recommended that this error should be corrected as part of Amendment C231 and that HO 327 be extended to include the full extent of the façade of the K.G. Luke building at 26-56 Queens Parade and that the place be identified as individually significant within the 'City of Yarra: Review of Heritage Overlays Appendix 8'.

Timing

49. The interim controls in Precinct 1 and 2 were approved by the Minister in March 2017 for a period of two years while the permanent controls were developed. These interim controls will remain in place.

50. If Council agrees to seek authorisation from the Minister, public exhibition of the amendment will commence once the Minister has authorised it in the new year (assuming no further changes are required). The amendment then has to run through the statutory process which includes exhibition, consultation and recept of submissions that is reported to Council. Most likely a panel hearing will be needed to provide a report and recommendations on the amendment to Council. The panel's recommendations would then be considered by Council to assist it with its final decision in relation to the amendment. It is then sent to the Minister for Planning seeking final approval. This process takes around a year.

51. It is also proposed to seek approval from the Minister for Planning for the introduction of additional interim controls for those areas with Precincts 2C, 2D, 3A, 4 and 5 whilst the amendment proposing permanent controls runs through the assessment and approval process. Consideration of this request is a matter for the Minister and DELWP. If the request is acceptable a decision is likely in the first part of 2018 at the earliest.

External Consultation

52. The statutory provisions in the *Planning and Environment Act 1987* provide for extensive external consultation. If Council resolves to seek authorisation for the amendment as recommended and it is approved by DELWP for formal exhibition, the amendment would be exhibited for a minimum period of one month in accordance with the requirements of the *Planning and Environment Act 1987*.

53. Council would write to all affected parties and seek comments on the amendment. Fact sheets summarising the key information would be sent to affected parties and one on one meetings would be arranged for the community and stakeholders to meet with officers. These would run by prior appointment which ensures that the community can obtain information and have any questions answered.

54. The community would be advised that they can make submissions if they support or object to the amendment. These submissions would be considered by officers and reported to Council in accordance with the requirements of the *Planning and Environment Act 1987*. If submissions cannot be resolved by changes to the amendment, Council would have the opportunity to refer the submissions to a Planning Panel or to abandon the amendment. If the amendment is referred to the Planning Panel, all submitters would be given the opportunity to outline their submission to the Planning Panel who would make recommendations in a report to Council.

55. Officers will report all submissions to Council and any submissions that cannot be resolved by making changes to the amendment would be referred to an independent planning panel where all submitters would be given the opportunity to speak to their submission. The panel considers all submissions when making recommendations to Council about whether Council should approve or abandon the amendment.

56. The Panel report would be presented to Council for consideration for adoption (with appropriate changes) or abandonment.

Internal Consultation (One Yarra)

57. The strategic and statutory planning teams in Council have been involved in the preparation of the Queens Parade Built Form Review and the proposed planning controls.

Financial Implications

58. Other than officer time and the administration fee to DEWLP there are no financial costs associated with requesting the Minister for Planning to authorise an amendment.

59. If the full amendment is advanced and exhibited as recommended, the cost of exhibiting and progressing the amendment through a public consultation and planning panel process is estimated to be in the order of $100,000 -$150,000 (assuming a 6 day planning panel).

Economic Implications

60. There are no known economic implications associated with exhibiting and progressing the amendment.

Sustainability Implications

61. There are no known sustainability implications associated with exhibiting and progressing the amendment. ESD provisions would be considered in any planning application.

Social Implications

62. There are no known social implications associated with exhibiting and progressing the amendment.

Human Rights Implications

63. There are no known human rights implications associated with exhibiting and progressing the amendment.

Communications with CALD Communities Implications

64. Notification and consultation about the amendment would include advice about the use of interpreter service by residents. It would be available to help affected parties understand the proposal and associated processes. The amendment process would involve the steps outlined in Council's strategy to engage and assist CALD communities.

Council Plan, Strategy and Policy Implications

65. The DDO supports the following strategy in the Council Plan:

> *Manage change in Yarra's built form and activity centres through community engagement, land use planning and appropriate structure planning processes.*

Legal Implications

66. The approach outlined in this report is in accordance with the requirements of the *Planning and Environment Act 1987.*

Other Issues

67. There are no other issues associated with this proposal.

Options

68. Council can either agree to proceed with the amendment with or without changes and request that the Minister for Planning authorises it for exhibition to the community, or the Council can decide not to proceed with the amendment.

Conclusion

69. Amendment C231 proposes to introduce a new Design and Development Overlay for Queens Parade into the Yarra Planning Scheme which would provide much sought after guidance for future built form along Queens Parade.

70. It is proposed to seek the introduction of additional interim controls for Precincts 2C, 2D, 3, 4 and 5 whilst Amendment C231 is exhibited and assessed for approval.

71. The amendment also proposes to rezone two parcels of land from Commercial 2 to Commercial 1 (in the other case).

72. The amendment also proposes re-grading of some heritage properties which have been recommended after detailed investigation by Council's heritage consultant.

73. The amendment has been informed by a Built Form Review and the Queens Parade Heritage analysis and recommendations, which has undertaken considerable testing of potential future buildings looking at various street wall heights, building heights and setbacks. The testing ensures that views to significant landmarks are retained and the important heritage streetscapes are retained.

74. The exhibition process of the amendment would provide an opportunity for the community to consider the proposed planning controls and associated documents.

RECOMMENDATION

1. That Council:

   (a) note the officer report on the Queens Parade Clifton Hill Built Form Review and Queens Parade Built Form and Heritage Analysis & Recommendations by GJM Heritage;

   (b) note the supporting Queens Parade, Clifton Hill Built Form Review, prepared by Hansen partnership and the Queens Parade Built Form Heritage Analysis & Recommendations by GJM Heritage;

   (c) endorse the Queens Parade, Clifton Hill Built Form Review as a basis for Amendment C231;

   (d) request the Minister for Planning in accordance with Section 8 (1) (b) and 20 (4) of the Planning and Environment Act 1987, to introduce a Design and Development Overlay Schedule 20 on an interim basis to Precincts 2C, 2D, 3A,4 and 5 along Queens Parade neighbourhood activity centre;

   (e) request the Minister for Planning in accordance with Section 8 (1) (b) and 20 (4) of the Planning and Environment Act 1987, to introduce an interim heritage protection for those properties proposed as new Heritage Overlays (472-484 Napier Street and 350 Queens Parade); and

   (f) seek authorisation from the Minister for Planning to prepare Amendment C231 to the Yarra Planning Scheme in accordance with section 8A of the Planning and Environment Act 1987.

2. That if authorisation is received from the Minister for Planning, Council exhibit Amendment C231 in accordance with Section 19 of the *Planning and Environment Act 1987*.

3. That subject to clause 2 above, note that the community consultation during the exhibition and consideration of the proposed amendment will include:

(a) public exhibition of the proposed amendment for one calendar month, in accordance with the requirements of the *Planning and Environment Act 1987*;

(b) notification letters detailing information about the proposed amendment and how to make a submission sent to each affected owner and occupier;

(c) provision of fact sheets with information about the amendment and the consideration process;

(d) community consultation sessions facilitated by Council officers with ward Councillors invited;

(e) consideration of community submissions with a report provided to Council;

(f) hearing community submissions and consideration of any recommended changes at a Council meeting; and

(g) should Council resolve to have the proposed amendment considered by a planning panel, submitters having the opportunity to present to the panel and finally to Council on the panel's report and recommendations.

CONTACT OFFICER: **David Walmsley**
TITLE: **Manager City Strategy**
TEL: **9205 5350**

Attachments

1 Amendment C231 - GJM Heritage Analysis and Recommendations
2 Amendment C231 - DDO16
3 Amendment C231 - Hansen Built Form Review

GJM Heritage

Queens Parade Built Form Heritage Analysis & Recommendations

Figure 1: Queens Parade, Fitzroy North and Clifton Hill (©nearmap, 7 Feb 2017)

11 December 2017

Prepared for the City of Yarra

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All photos were taken by GJM Heritage unless otherwise stated.

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Table of Contents

Executive Summary **v**
Presentation of the Report v
Part I: The Project and the Study Area **6**
1. Introduction **7**
1.1 Queens Parade, Clifton Hill/Fitzroy North 7
1.1.1 Brief History of Queens Parade 7
1.1.2 Brief Description of the Queens Parade Study Area 8
1.2 Scope of the Heritage Analysis & Recommendations Report 9
1.3 Methodology 9
2. Planning Context **12**
2.1 Activity Centre Planning and Heritage 12
2.2 Yarra Planning Scheme – Heritage Provisions 13
2.2.1 Heritage policy 13
2.2.2 Landmarks and Tall Structures 15
2.2.3 Heritage Overlay 17
2.3 Significant Streetscapes 19
2.3.1 Criteria and Threshold for a 'Significant Streetscape' 19
2.3.2 Addressing 'Significant Streetscapes' in Queens Parade 20
3. Heritage in Design and Development Overlays – Panel Findings **21**
3.1 Moreland Amendment C134 21
3.2 Boroondara Amendment C108 22
4. Mandatory and Discretionary Height and Setback Controls **24**
Part II: Heritage Analysis **25**
5. Precinct Boundaries **26**
6. Precinct 1 – Brunswick Street Precinct **27**
6.1 Description 27
6.2 Heritage Status 29
6.2.1 Existing Conditions 29
6.2.2 Recommended Changes 29
6.3 Zoning 30
6.3.1 Existing Conditions 30
6.3.2 Recommended Changes 30
6.4 Key views 30
6.5 Significant Streetscapes 31
6.6 Potential Future Character Considerations 31

6.7 Recommended Built Form Parameters 31
7. Precinct 2 – Boulevard Precinct 33
7.1 Description 33
7.2 Heritage Status 35
7.2.1 Existing Conditions 35
7.2.2 Recommended Changes 36
7.3 Zoning 37
7.3.1 Existing Conditions 37
7.3.2 Recommended Changes 37
7.4 Key views 37
7.5 Significant Streetscapes 37
7.6 Potential Future Character Considerations 37
7.7 Recommended Built Form Parameters 38
8. Precinct 3 – St John's Precinct 40
8.1 Description 40
8.2 Heritage Status 42
8.2.1 Existing Conditions 42
8.2.2 Recommended Changes 43
8.3 Zoning 44
8.3.1 Existing Conditions 44
8.3.2 Recommended Changes 44
8.4 Key views 44
8.5 Significant Streetscapes 45
8.6 Potential Future Character Considerations 45
8.7 Recommended Built Form Parameters 46
9. Precinct 4 – Neighbourhood Activity Centre Precinct 47
9.1 Description 47
9.2 Heritage Status 48
9.2.1 Existing Conditions 48
9.3 Zoning 52
9.3.1 Existing Conditions 52
9.3.2 Recommended Changes 52
9.4 Key views 52
9.5 Significant Streetscapes 54
9.6 Potential Future Character Considerations 55
9.7 Recommended Built Form Parameters 57
10. Precinct 5 - North Eastern Precinct 59
10.1 Description 59

10.2 Heritage Status 60
10.2.1 Existing Conditions 60
10.3 Zoning 62
10.3.1 Existing Conditions 62
10.3.2 Recommended Changes 62
10.4 Key views 62
10.5 Significant Streetscapes 63
10.6 Potential Future Character Considerations 63
10.7 Recommended Built Form Parameters 64
Section III – Built Form Testing and Recommendations 65
11. Built Form Testing 66
11.1 Purpose and Scope of Built Form Testing 66
11.2 Massing Model 66
11.3 Eye Level Views 67
11.4 Permit Application Review 67
12. Built Form Recommendations 68
12.1 Precinct 1 68
12.2 Precinct 2 69
12.3 Precinct 3 71
12.4 Precinct 4 73
12.5 Precinct 5 74
12.6 Additional guidance 75
Appendix A: Former KG Luke Pty Ltd Factory & Showroom Heritage Citation
Appendix B: Row of Houses, Elizabeth Terrace Heritage Citation
Appendix C: St John the Baptist Roman Catholic Church Complex Heritage Citation
Appendix D: Former Fire Station (No.11) Heritage Citation
Appendix E: Former Doctor's Surgery & House Heritage Citation
Appendix F: Former Clifton Motors Garage, Showroom & Residence Heritage Citation
Appendix G: Recommendations for amendments to Heritage Gradings & Heritage Overlay Boundaries
Appendix H: Yarra High Streets: Statements of Significance (Reference Document)

Executive Summary

Queens Parade, straddling the suburbs of Fitzroy North and Clifton Hill in the City of Yarra, was originally a track shown on Robert Hoddle's Crown survey of 1837, linking Melbourne to Heidelberg. Established by 1839, it was Melbourne's first major road[1]. The track originally extended from the top of Bourke Street and followed an irregular course to the northeast, following the top of the Collingwood escarpment and running along the current alignments of Queens Parade, Heidelberg Road, Upper Heidelberg Road and Lower Plenty Road[2]. Queens Parade was surveyed by Hoddle as a three chain (60m) wide boulevard and reserved in 1853. It was one of Melbourne's earliest surveyed boulevards[3].

Today Queens Parade is an area where more intensive development is starting to occur, particularly in the former light industrial areas to the south-west and north-east, but also within the commercial Neighbourhood Activity Centre.

Hansen Partnership, on behalf of the City of Yarra (Council), is completing a Built Form Review of Queens Parade (the Review) to determine where and how new development can occur. The desired built form outcomes will be translated into Design and Development Overlay (DDO) controls for the Parade.

This heritage advice will help ensure that the Review and the subsequent DDO appropriately responds to the heritage fabric and values of Queens Parade, leading to fully integrated decision-making when considering new development within the Queens Parade corridor.

This heritage advice analyses the existing heritage values and qualities along Queens Parade. It identifies any gaps, inconsistencies or inaccuracies with the current heritage controls and provides recommendations for addressing these issues. The advice then considers the built form parameters that are needed to ensure the heritage values of the area are appropriately managed in any proposal for more intensive development. This includes a consideration of whether mandatory or discretionary controls are appropriate to achieve the particular parameter.

Presentation of the Report

This following Heritage Report is presented in three parts:

Part I: The Project and Planning Framework
Part I introduces the project, the methodology applied to the project and the planning framework in which the project is occurring.

Part II: Heritage Analysis
Part II contains a heritage analysis of each Study Area precinct. It details the heritage qualities and values of each precinct, identifies any gaps or issues with the existing heritage framework and provides recommendations for appropriately managing heritage places within the study area.

Part III: Heritage Recommendations
Part III contains specific built form recommendations to ensure heritage places and values are appropriately managed within a changing Activity Centre. The specific recommendations are informed by modelling prepared by Hansen Partnership.

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[1] Maxwell Lay, 'Roads', *eMelbourne:* The *City Past and Present* (www.emelbourne.net.au/biogs/EM01255b.htm). Accessed 13 June 2017. Lay also states that the road was first surveyed to Eltham in 1840 and officially opened in 1841. Hoddle's survey in 1853 may have been a re-survey of the road.
[2] Maxwell Lay, 'Roads', *eMelbourne.*
Lewis, Miles, 'The First Suburb' in *Fitzroy: Melbourne's First Suburb (*1989), 7.
3 Graeme Butler and Associates, *City of Yarra Review of Heritage Overlay Areas* (2007), 120.

Part I: The Project and the Study Area

1. Introduction

1.1 Queens Parade, Clifton Hill/Fitzroy North

The City of Yarra (Council) is endowed with one of the largest and most highly intact collections of turn of the century 'High Streets' in the State of Victoria. These High Streets include the four Major Activity Centres of Swan Street and Bridge Road in Richmond, Brunswick Street in Fitzroy, and Smith Street straddling the suburbs of Fitzroy and Collingwood. They also include a number of Neighbourhood Activity Centres, including Rathdowne Street and Nicholson Street in Carlton North, St Georges Road in Fitzroy North, Queens Parade in Fitzroy North/Clifton Hill, Gertrude Street in Fitzroy, and Johnston Street in Fitzroy and Collingwood.

These High Streets help to define the character of the municipality and their value to the community is recognised by their inclusion in the Heritage Overlay of the Yarra Planning Scheme. However, the Activity Centre status of these High Streets presents a difficult challenge: how do we balance the retention of these highly valued streets with the need to ensure the long-term sustainability of these centres and meet the growth objectives of the Yarra Planning Scheme?

1.1.1 Brief History of Queens Parade

The following brief history is drawn from the Statement of Significance for HO330 Queens Parade Precinct and supplemented with additional research.

First established as a track linking Melbourne and Heidelberg in the late 1830s, Queens Parade was one of Melbourne's first major roads. Originally extending from the top of Bourke Street, the route followed an irregular course to the northeast and continued across Merri Creek. The road was surveyed by Hoddle as a substantial three chain (60 metre) wide boulevard, reserved in 1853, and known as Heidelberg Road at the southwest end and Plenty Road further to the northeast and crossing Merri Creek. This thoroughfare to Northcote and Heidelberg formed part of the boundary between Clifton Hill and North Fitzroy and was renamed Queens Parade in the early twentieth century.

Allotments were sold in Queens Parade from 1853, on the northwest side, south of Merri Creek as part of the Northcote township reserve. The Scotch Thistle Hotel, the present Terminus Hotel, had been built as part of this township by 1854 and some early bluestone terrace houses remain nearby at the corner of Rushall Crescent. Allotments fronting both sides of Queens Parade sold from 1864, however the street remained largely undeveloped until the construction of buildings such as the Daintons Family Hotel at the corner of Gold Street in 1875 and St John's Roman Catholic Church in 1876.

This thoroughfare between Melbourne and the northern districts experienced a development boom in the 1880s, serving local residents as well as those travelling through, and much of the present building stock in the precinct was constructed at this time. Development on the North Fitzroy (north) side was predominantly two-storey, including shops with residences above, such as the corner building at 336-338 Queens Parade (1870-1890) and banks, including the National Bank of Australasia (1886) and the three-storey landmark building, the former London Chartered Bank (later the ANZ Bank, 1889). The Clifton Hill (south) side of the street developed with some shops, large commercial enterprises and some public buildings, including the Post and Telegraph Office (1893), a row of seven shops and residences known as Campi's Buildings (1880s), the State Savings Bank (1889), and the Clifton Hill Hotel (1884) on the south-east corner of Wellington Street.

Public transport was established in the area during the Boom years of the 1880s. A cable tram service was provided along the developing Queens Parade commercial strip in 1887, terminating at the Merri Creek and this was extended to Northcote in 1889. Increasing demand for public transport resulted in the opening of the Royal Park to Clifton Hill railway in 1888.

Development in Queens Parade decreased during the depression years of the 1890s however a 1904 Melbourne and Metropolitan Board of Works detail plan indicates it was a tree lined commercial street with verandahs over the footpaths on both sides of Queens Parade. Commercial development re-commenced in the early twentieth century, adding an Edwardian presence to the existing Victorian character of the precinct.

Edwardian buildings include the Clifton Hill & Northcote United Friendly Societies Dispensary (1914), the Clifton Hill Post Office (c1915-20) and the former doctor's surgery and residence (c1915). Additionally, a substantial group of detached Edwardian houses were built on the North Fitzroy side of Queens Parade, at the north-east end.

By the close of the Edwardian period (c1918) the streetscape, that continues today to mark the entrance to Melbourne's northern suburbs, had largely been established. Notable Interwar buildings were added to the precinct in the 1930s including a pair of landmark Moderne buildings at the north-east end - the former United Kingdom Hotel (1937-38), located on the prominent intersection of Queens Parade and Heidelberg Road, and the nearby Clifton Motors Garage (1938 façade).

Extensive beautification took place in the Interwar period in the form of landscaping and further street tree plantings. Grassed median strips were planted with plane and elm trees at the west end and a triangular garden (Raines Reserve) was created at the junction with Heidelberg Road.

1.1.2 Brief Description of the Queens Parade Study Area

Queens Parade, straddling the suburbs of Clifton Hill and Fitzroy North, is a corridor of diverse functions and character and is lined with a mixture of residential, industrial and commercial buildings dating from all eras of its development. Within the Queens Parade Study Area (Figure 2), Queens Parade includes a highly intact, predominantly nineteenth century, shopping strip and important local landmarks in the form of the Church of St John the Baptist and the former ANZ Bank Building at 370-374 Queens Parade. This central part of Queens Parade, in particular, is highly valued by local residents and its heritage buildings provide a rich environment for retail, hospitality, commercial and community uses.

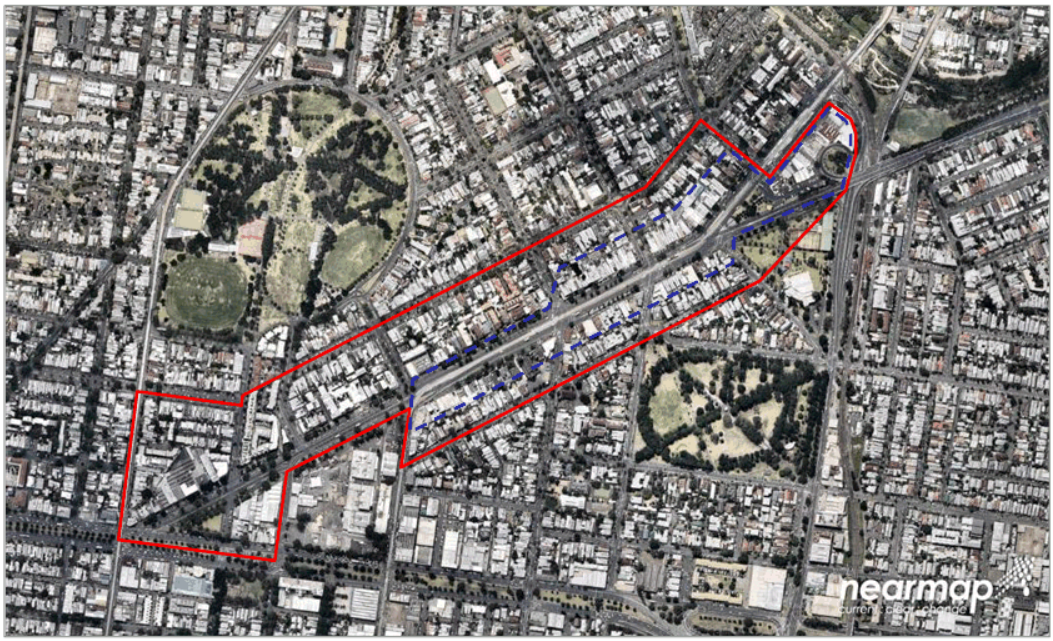

Figure 2: Extent of Study Area shown in red and Neighbourhood Activity Centre dashed in blue (base image ©nearmap, 7 Feb 2017)

The Study Area extends along either side of Queens Parade between the intersection of Brunswick Street and Alexandra Parade to the southwest and Coulson Reserve in the north-east. It generally extends one block either side of Queens Parade to Newry and McKean Streets in Fitzroy North and Hodgkinson Street in Clifton Hill. The Study Area excludes the former Fitzroy Gasworks site (bounded by Queens Parade, Alexandra Parade, George Street and Smith Street), which is identified as a 'strategic redevelopment site' and is managed

through a separate Urban Design Framework. The Study Area is approximately 1.6 kilometres in length and 250 metres in width.

The character of the Study Area varies along the length of Queens Parade, moving from the mixed residential and industrial character of the southwestern portion, which transitions to a mix of commercial and residential buildings in the centre of the Study Area with the prominent Church of St John the Baptist. North of this landmark the Study Area is characterised by a highly intact turn of the century retail and commercial Neighbourhood Activity Centre. The triangle of land bounded by Queens Parade, Heidelberg Road and Hoddle Street at the northeastern end of the Study Area returns to more industrial character.

Part II of this report describes the Study Area in greater detail.

1.2 Scope of the Heritage Analysis & Recommendations Report

In December 2016 GJM was engaged to provide high-level advice into initial work for the Review. The input constituted verbal advice on the adequacy of the Statements of Significance for Heritage Overlays within the Study Area and the appropriateness of the heritage gradings identified in the Incorporated Document *City of Yarra Review of Heritage Overlay Areas 2007: Appendix 8* (as amended) (Appendix 8). In the early stages of this work, and in response to a development application for the former KG Luke site at 26 – 56 Queens Parade, GJM provided verbal heritage advice on Schedule 16 to the DDO (Queens Parade West) (DDO16) and the supporting urban design analysis.

In April 2017 GJM was commissioned to provide detailed analysis of the heritage considerations for the Queens Parade Study Area (Figure 2) and to detail recommendations for its future management in the context of new development.

The purpose of our advice is to ensure that DDO controls arising from the Built Form Review take proper account of the heritage values of the precincts and individual buildings within the Study Area, leading to fully integrated decision-making when considering new development within the Queens Parade corridor.

The analysis within this report considers:

- The suitability of the extent of the Heritage Overlays for places and precincts within the Study Area.
- The heritage grading of each property within the Heritage Overlay in Appendix 8.
- The currency of the existing Statements of Significance for places and/or precincts to ensure they provide adequate guidance for the management of important heritage features.
- Places not currently included in the Heritage Overlay but which warrant further consideration for inclusion in the Heritage Overlay.
- Built form parameters necessary to appropriately manage increased commercial and residential development within the context of the existing heritage place and/or precinct.

Within this report, the term 'Study Area' or 'Queens Parade' refers to the area identified in red in Figure 2.

1.3 Methodology

The key background documents on which the heritage analysis is based are:

- Yarra Planning Scheme Heritage Overlay Maps 1 and 2
- Relevant Statements of Significance for heritage places and precincts within the Study Area and associated heritage studies
- Heritage Overlay 'Heritage Grading' data provided by Council in plan form
- Appendix 8.

The above documents have been reviewed in the context of the following clauses from the Yarra Planning Scheme and the relevant Planning Practice Notes (PPNs) published by the Department of Environment, Land, Water and Planning:

- The relevant provisions of the Yarra Planning Scheme in particular:

  - Clause 15.03 'Heritage'
  - Clause 21.05-1 'Heritage'
  - Clause 22.02 'Development Guidelines for Sites Subject to the Heritage Overlay'
  - Clause 22.03 'Landmarks and Tall Structures'
  - Clause 43.01 'Heritage Overlay'
  - Clause 43.01 'Schedule to the Heritage Overlay'
  - Clause 55.07 'Apartment Developments'
- PPN 1: *Applying the Heritage Overlay* (July 2015) (PPN1)
- PPN 59: *The role of mandatory provisions in the planning schemes* (June 2015) (PPN59)
- PPN 60: *Height and setback controls for activity centres* (June 2015) (PPN60).

The following Planning Panels Victoria (Panel) reports are relevant to the implementation of the Review as they consider the appropriateness of DDOs (containing both mandatory and discretionary provisions) within activity centres (or in the case of Melbourne Amendment C240 the Capital City Zone) that are also subject, in part, to the Heritage Overlay:

- Boroondara C108 'Neighbourhood Centres and Commercial Corridors' (26 February 2014)
- Banyule Planning Scheme Amendment C93 'Implementation of the Ivanhoe Structure Plan' (1 July 2014)
- Moreland Planning Scheme Amendment C134 'Brunswick Activity Centre' (15 May 2015)
- Melbourne Planning Scheme Amendment C240 'Bourke Hill' (4 May 2015)
- Bayside Planning Scheme Amendments C113, C114 and C115 'Mandatory provisions for the Sandringham Village, Bay Street and Church Street Activity Centres' (14 January 2015).

We have approached the preparation of this analysis as follows:

1. Completion of a desktop review of the above listed documents, Panel Reports, heritage mapping and grading information, and the Statements of Significance for heritage precincts and individually significant buildings, including those places registered on the VHR. The extent of the Heritage Overlays were cross-checked against Google Streetview. This preliminary review familiarised the project team with the heritage fabric of the Study Area prior to fieldwork being undertaken.
2. Completion of fieldwork by Jim Gard'ner and Renae Jarman. All buildings and structures within the Study Area were inspected from the public realm with particular attention paid to the presentation of heritage buildings to Queens Parade (and intersecting streets for corner sites), views along Queens Parade, views towards the spire of the Church of St John the Baptist, and views towards the local landmarks of the former ANZ Bank (370-374 Queens Parade, VHR H0892) and the former United Kingdom Hotel (199 Queens Parade, VHR H0684). The rear and side interfaces to the neighbouring residential areas subject to the Heritage Overlay were also considered. The purpose of the fieldwork was to:
   - Review the suitability of the extent of the existing Heritage Overlays and to identify if gaps existed.
   - Review the suitability of the existing Statements of Significance for heritage places and precincts against the extant heritage fabric and to identify where the statements required updating for the purposes of properly considering built form recommendations.
   - Review the extant heritage fabric against the heritage gradings contained within Appendix 8 and to identify any inconsistencies and inaccuracies.
   - Review the heritage buildings and streetscapes within the Study Area to identify the architectural and streetscape heritage features (e.g. parapets, roof forms, view lines, corner sites) that are relevant to a consideration of built form recommendations.
3. Participation in multiple workshops with Council and Hansen Partnership. The workshops:
   - Established potential 'precincts' within the Study Area that were driven by existing built form characteristics.

- Identified the desired future character of the Study Area precincts against heritage analysis and State and local planning policy drivers.
- Tested built form parameters for new development against the existing fabric of Queens Parade through modelling prepared by Hansen Partnership.

4. Finalisation of heritage recommendations for new built form parameters having considered the above.

2. Planning Context

2.1 Activity Centre Planning and Heritage

The *Planning & Environment Act 1987* and the Victoria Planning Provisions (VPP) requires planning and responsible authorities to take a balanced approach to strategic and statutory planning functions that consider potentially competing objectives in an integrated manner to deliver a net community benefit for current and future generations.

The objectives of planning in Victoria as set out in Section 4(1) of the Planning and Environment Act are:

- *To provide for the fair, orderly, economic and sustainable use and development of land.*
- *To provide for the protection of natural and man-made resources and the maintenance of ecological processes and genetic diversity.*
- *To secure a pleasant, efficient and safe working, living and recreational environment for all Victorians and visitors to Victoria.*
- *To conserve and enhance those buildings, areas or other places which are of scientific, aesthetic, architectural or historical interest, or otherwise of special cultural value.*
- *To protect public utilities and other assets and enable the orderly provision and coordination of public utilities and other facilities for the benefit of the community.*
- *To facilitate development in accordance with the objectives set out in the points above.*
- *To balance the present and future interests of all Victorians.*

Clause 10.04 of the VPP addresses 'integrated decision making', and states:

> *Society has various needs and expectations such as land for settlement, protection of the environment, economic well-being, various social needs, proper management of resources and infrastructure. Planning aims to meet these by addressing aspects of economic, environmental and social well-being affected by land use and development.*
>
> *Planning authorities and responsible authorities should endeavour to integrate the range of policies relevant to the issues to be determined and balance conflicting objectives in favour of net community benefit and sustainable development for the benefit of present and future generations.*

Activity Centres that are also subject to extensive Heritage Overlay controls (like Queens Parade) are examples of where the tension between competing planning objectives must be resolved in a balanced way. Queens Parade has excellent public transport connections, vibrant retail, commercial and hospitality uses and a high demand for housing choice. It also contains a highly intact, turn of the century commercial precinct containing heritage fabric that is highly valued by the local community. A balance between the demand for more intensive development with the protection of the heritage buildings and precincts is therefore required. To achieve this, it is considered necessary that the DDO – and the background work that underpins it – specifically includes heritage considerations which frames the design objectives in terms of mandatory and discretionary controls.

Figure 3: Existing zones covering the Study Area (outlined in black) and surrounds (© Hansen Partnership, 2017)

2.2 Yarra Planning Scheme – Heritage Provisions

Council has well-established heritage provisions within its planning scheme at Clauses 21.05-1 and 22.02. Also of relevance to the protection of the heritage values of Queens Parade is Clause 22.03, which includes policy to protect the visual prominence of the St John the Baptist spire in addition to other landmarks within the municipality.

2.2.1 Heritage policy

The relevant objective within Clause 21.05-1 'Heritage' of the Municipal Strategic Statement (MSS) is *Objective 14: To protect and enhance Yarra's heritage places.* The relevant strategies to implement this objective are:

- *Strategy 14.1 - Conserve, protect and enhance identified sites and areas of heritage significance including pre-settlement ecological heritage.*
- *Strategy 14.3 - Protect the heritage skyline of heritage precincts.*
- *Strategy 14.4 - Protect the subdivision pattern within heritage places.*
- *Strategy 14.6 - Protect buildings, streetscapes and precincts of heritage significance from the visual intrusion of built form both within places and from adjoining areas.*

Objective 14 and its associated strategies are considered to be compatible with appropriately sited and scaled higher density development on Queens Parade, although it would need to be accepted that in most cases Strategy 14.3 'Protect the heritage skyline of heritage precincts' would not be effectively achieved.

Clause 22.02 'Development Guidelines for Sites Subject to the Heritage Overlay' provides detailed guidance within the Local Planning Policy Framework (LPPF) on development within the Heritage Overlay, including demolition. The relevant objectives of Clause 22.02 are:

- *To conserve Yarra's natural and cultural heritage.*

- *To conserve the historic fabric and maintain the integrity of places of cultural heritage significance.*
- *To retain significant view lines to, and vistas of, heritage places.*
- *To preserve the scale and pattern of streetscapes in heritage places.*
- *To encourage the preservation, maintenance, restoration and where appropriate, reconstruction of heritage places.*
- *To ensure the adaptation of heritage places is consistent with the principles of good conservation practice.*
- *To ensure that additions and new works to a heritage place respect the significance of the place.*
- *To encourage the retention of 'individually significant' and 'contributory' heritage places.*
- *To protect archaeological sites of cultural heritage significance.*

Again, these objectives do not preclude higher density development along Queens Parade with the possible exception of '*To preserve the scale … of streetscapes in heritage places*'.

The demolition policy provided at Clause 22.02-5.1 encourages the retention of 'individually significant' and 'contributory' buildings within a heritage precinct. Removal of part of a heritage place or contributory element is contemplated if (in general terms) it can be demonstrated that the removal of the part will not adversely affect the significance of the building, or – for a contributory building – the part is not visible from the street, abutting a park or public open space.

Given that the significance of most heritage places along Queens Parade lies predominantly in their streetscape presentation, facade detailing and fine-grained pattern of subdivision, it is considered that a certain amount of rear demolition and redevelopment can be contemplated under the existing heritage policy.

Further, with the exception of those heritage places included on the VHR – and therefore regulated under the *Heritage Act 1995* – internal controls do not apply to the Study Area.

In most circumstances, the planning scheme effectively limits the control of heritage fabric within the Study Area to that which is visible from the public realm, including street facades, rear laneway views (where they exist) and visible roof and chimney elements.

In relation to 'New Development, Alterations and Additions', Clause 22.02-5.7.1 sets out the following policy:

> ***General***
>
> *Encourage the design of new development and alterations and additions to a heritage place or a contributory element to a heritage place to:*
>
> - *Respect the pattern, rhythm, orientation to the street, spatial characteristics, fenestration, roof form, materials and heritage character of the surrounding historic streetscape.*
> - *Be articulated and massed to correspond with the prevailing building form of the heritage place or contributory elements to the heritage place.*
> - *Be visually recessive and not dominate the heritage place.*
> - *Be distinguishable from the original historic fabric.*
> - *Not remove, cover, damage or change original historic fabric.*
> - *Not obscure views of principle façades.*
> - *Consider the architectural integrity and context of the heritage place or contributory element.*
>
> *Encourage setbacks from the principal street frontage to be similar to those of adjoining contributory buildings; where there are differing adjoining setbacks, the greater setback will apply.*
>
> *Encourage similar façade heights to the adjoining contributory elements in the street. Where there are differing façade heights, the design should adopt the lesser height.*
>
> *Minimise the visibility of new additions by:*
>
> - *Locating ground level additions and any higher elements towards the rear of the site.*
> - *Encouraging ground level additions to contributory buildings to be sited within the 'envelope'*

created by projected sight lines (see Figure 1).

- *Encouraging upper level additions to heritage places to be sited within the 'envelope' created by projected sight lines (for Contributory buildings refer to Figure 2 and for Individually significant buildings refer to Figure 3).*
- *Encouraging additions to individually significant places to, as far as possible, be concealed by existing heritage fabric when viewed from the front street and to read as secondary elements when viewed from any other adjoining street.*

Discourage elements which detract from the heritage fabric or are not contemporary with the era of the building such as unroofed or open upper level decks or balconies, reflective glass, glass balustrades and pedestrian entrance canopies.

To achieve a reasonable level of development capacity, discretion would need to be applied in relation to the requirements for the full or partial concealment of rear additions described in Figures 2 and 3 of Clause 22.02 (refer Appendix 1). This sightline-based guidance in the heritage policy is designed to preserve and enhance the character and appearance of predominantly one and two-storey dwellings within more typical narrow residential streets and cannot be readily applied to multi-storey development within an Activity Centre that has a 60 metre wide carriageway without unreasonably constraining development.

Likewise, the policy to '*Discourage elements which … are not contemporary with the era of the building such as unroofed or open upper level decks or balconies, reflective glass, glass balustrades and pedestrian entrance canopies*' may not achieve appropriate urban design and architectural outcomes in areas such as Queens Parade. In such areas a 'contrasting' or 'interpretative' design approach for new taller development is likely to be more recessive than a 'respectful' or 'historicist' one that would lead to the new additions inappropriately mimicking the historic form and potentially being more visually intrusive.

Clause 22.02-5.7.2 provides the following specific policy that applies to buildings within the Commercial Zone along Queens Parade that are subject to the Heritage Overlay:

Corner Sites and Sites with Dual Frontages

Encourage new building and additions on a site with frontages to two streets, being either a corner site or a site with dual street frontages, to respect the built form and character of the heritage place and adjoining or adjacent contributory elements to the heritage place.

Encourage new buildings on corner sites to reflect the setbacks of buildings that occupy other corners of the intersection.

…

Industrial, Commercial and Retail Heritage Place or Contributory Elements

Encourage new upper level additions and works to:

- *Respect the scale and form of the existing heritage place or contributory elements to the heritage place by being set back from the lower built form elements. Each higher element should be set further back from lower heritage built forms.*
- *Incorporate treatments which make them less apparent.*

The above two specific policies will help to effectively inform development proposals and statutory planning decision-making to ensure that the heritage values along Queens Parade are maintained while accommodating more intensive levels of development.

2.2.2 Landmarks and Tall Structures

Clause 22.03 – 'Landmarks and Tall Structures' identifies a number of landmark buildings and advertising signs to which views should be protected. The 'spire' of St John the Baptist Church is one such landmark.

The relevant policies include:

- *Maintain the prominence of Yarra's valued landmark signs.*
- *Protect views to the silhouette and profile of Yarra's valued landmarks to ensure they remain as the principal built form reference.*
- *Ensure the profile and silhouette of new tall structures adds to the interest of Yarra's urban form and skyline.*

The policy seeks to ensure new buildings within the vicinity of St John the Baptist Church are designed to ensure the landmark remains the principal built form reference in the area.

While Clause 22.03 identifies the 'spire' of the Church only, it is our view that it is appropriate that views of the belfry and spire in combination should be considered.

The belfry has openings (often with louvres) to allow the sound of the bells to travel. The spire of the church sits above the belfry stage and was designed to both symbolically reach towards the heavens and signal the church's presence as a landmark in the local area.

The belfry provides a visual anchor to the spire of the church and the keys views of these structures should include both the belfry of the belfry and the spire. The key views to be managed within the Study Area are identified in the anlaysis for each Study Area precinct in this report.

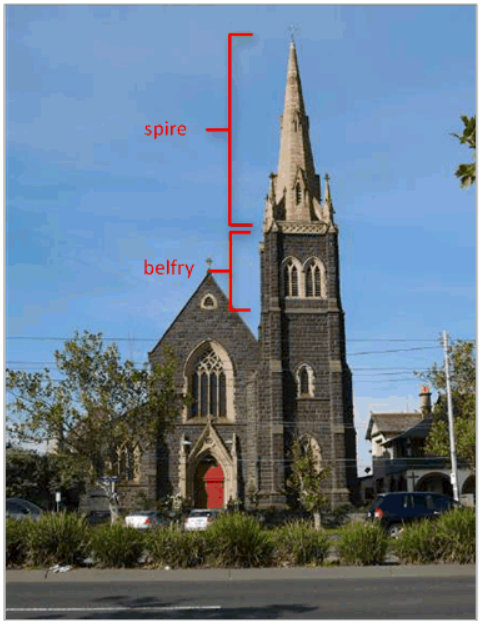

Figure 4: Elements of St John's Church to be captured in key views.

In addition to the belfry and spire of St John the Baptist Church, it is considered that views to the State-significant former ANZ Bank at 370-374 Queens Parade are important within the context of the Activity Centre. The former ANZ Bank is an imposing three-storey Queen Anne Revival style commercial building constructed of red brick with a steeply pitched slate roof and corner tower. The building occupies a prominent corner on Queens Parade where the road angles towards the north. Its landmark quality is derived from its tall three-storey scale within a lower context, the slate roofs and corner tower, the tall chimneys and the majority of the upper floor level that visually anchors the chimneys and roofs.

Figure 5: Elements of the former ANZ Bank to be captured in key views.

Occupying the key corner site at the fork of Queens Parade and Heidelberg Road, the former United Kingdom Hotel at 199 Queens Parade is also a visually prominent State-significant building in the Activity Centre. Given its siting and surrounding road reserve and public open space, it is not necessary to manage views to the building.

From within the Queens Parade Activity Centre – and particularly at the intersection of Queens Parade and Delbridge/Gold Street – a distant view of the State-significant Shot Tower on Alexandra Parade is available. This "glimpse" of the Shot Tower is not considered to be a key view to the Shot Tower and is not one that requires management through built form controls within the Queens Parade Study Area.

2.2.3 Heritage Overlay

The head heritage provision of the VPP, Clause 43.01 'Heritage Overlay', has the following purpose:

- *To implement the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.*
- *To conserve and enhance heritage places of natural or cultural significance.*
- *To conserve and enhance those elements which contribute to the significance of heritage places.*
- *To ensure that development does not adversely affect the significance of heritage places.*
- *To conserve specifically identified heritage places by allowing a use that would otherwise be prohibited if this will demonstrably assist with the conservation of the significance of the heritage place.*

Clause 43.01-4 sets out decision guidelines - in addition to those included in Clause 65 - that the Responsible Authority must consider before determining a permit application. These are:

- *The State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.*
- *The significance of the heritage place and whether the proposal will adversely affect the natural or cultural significance of the place.*
- *Any applicable statement of significance, heritage study and any applicable conservation policy.*
- *Whether the location, bulk, form or appearance of the proposed building will adversely affect the*

significance of the heritage place.

- *Whether the location, bulk, form and appearance of the proposed building is in keeping with the character and appearance of adjacent buildings and the heritage place.*
- *Whether the demolition, removal or external alteration will adversely affect the significance of the heritage place.*
- *Whether the proposed works will adversely affect the significance, character or appearance of the heritage place.*
- *Whether the proposed subdivision will adversely affect the significance of the heritage place.*
- *Whether the proposed subdivision may result in development which will adversely affect the significance, character or appearance of the heritage place.*
- *Whether the proposed sign will adversely affect the significance, character or appearance of the heritage place.*
- *Whether the lopping or development will adversely affect the health, appearance or significance of the tree.*

While some of these considerations are not obviously consistent with the addition of higher density development behind heritage buildings, the first purpose and first decision guideline encompasses the whole SPPF and LPPF (integrated decision-making). Therefore a balance must be struck between achieving the objectives of the Heritage Overlay and meeting the objectives of other parts of the VPPs including Activity Centre policy and commercial zoning. It is noted that new rear development can often be accommodated behind heritage buildings in commercial precincts without substantially compromising the identified significant values of these heritage places.

Figure 6: Heritage Overlays covering the Study Area and surrounds (© Hansen Partnership, 2017)

2.3 Significant Streetscapes

After completing a detailed heritage review of Queens Parade and its historical development, it became evident that the heritage significance of parts of the corridor extended beyond being a collection of 'individually significant' and 'contributory' buildings, but that the significance was more fine-grained than a precinct-wide consideration. In effect, within specific sections of Queens Parade, the significance of those sections is greater than the sum of their parts.

This is not true of the entire length of Queens Parade, but rather those highly intact streetscapes in which there is homogenous built form, high quality architectural design and a consistent period of development.

2.3.1 Criteria and Threshold for a 'Significant Streetscape'

PPN1 does not contain guidance with respect to significant streetscapes (nor does it provide guidance on the grading of individual buildings within a precinct); however, the City of Melbourne utilises a streetscape grading – proposed to be simplified through Melbourne Amendment C258 – which may have some utility in considering the significance of Queens Parade and other of Yarra's historically significant High Streets. In addition to grading individual buildings within the Heritage Overlay, the City of Melbourne has long defined the quality of streetscape to inform the application of its Heritage Policy outside the Capital City Zone contained at Clause 22.05.

The current Clause 22.05 of the Melbourne Planning Scheme defines the Streetscape Gradings as:

> ***Grading of Buildings and Streetscape Levels***
>
> *Every building of cultural significance has been assessed and graded according to its importance. Streetscapes, that is complete collections of buildings along a street frontage, have also been graded for planning control purposes. The individual buildings are grade A to D, the streetscapes from Level 1 to 3, both in descending order of significance. The grade of every building and streetscape is identified in the incorporated document Heritage Places Inventory 2000.*
>
> ...
>
> *Level 1 Streetscapes*
>
> *Level 1 streetscapes are collections of buildings outstanding either because they are a particularly well preserved group from a similar period or style, or because they are highly significant buildings in their own right.*
>
> *Level 2 Streetscapes*
>
> *Level 2 streetscapes are of significance either because they still retain the predominant character and scale of a similar period or style, or because they contain individually significant buildings.*
>
> *Level 3 Streetscapes*
>
> *Level 3 streetscapes may contain significant buildings, but they will be from diverse periods or styles, and of low individual significance or integrity.*

The City of Melbourne is currently progressing Amendment C258 which, amongst other things, includes an amended Heritage Policy for places outside the Capital City Zone at Clause 22.05 which defines a "Streetscape" as:

> *A streetscape is a collection of buildings along a street frontage. When referred to in relation to a precinct, a streetscape typically contains a majority of buildings which are graded significant or contributory.*

Clause 22.05 (as proposed to be amended) defines a "Significant streetscape" as:

> *Significant streetscapes are collections of buildings outstanding either because they are a particularly well preserved group from a similar period or style, or because they are highly significant.*

In relation to Queens Parade and other Yarra High Streets, a similarly high threshold for identifying what would constitute a 'significant streetscape' could be applied and emphasis should be placed on streetscapes that demonstrate the following characteristics:

- The vast majority of buildings are 'contributory' or 'individually significant' with few 'not-contributory' buildings or intrusive developments.
- There is a high degree of consistency in terms of:
  - scale
  - setbacks
  - street wall height
  - architectural form and style.
- The streetscape consists of development from a similar construction period.
- The streetscape demonstrates a high level of intactness.
- There is a recognised high level of architectural/aesthetic significance and/or architectural quality.

2.3.2 Addressing 'Significant Streetscapes' in Queens Parade

While desirable, we do not consider it necessary for a 'streetscape' designation to be identified in Council's Heritage Policy or Appendix 8 (as occurs in the City of Melbourne). In our view, it is appropriate for the significant streetscapes to be identified in the broader relevant precinct Statement of Significance and for built form controls proposed through the DDO to specifically address these considerations.

Having completed the detailed heritage analysis of Queens Parade, it is considered that significant streetscapes exist in the Study Area and these are discussed in greater detail within the precinct-by-precinct analysis in Part II of this report.

3. Heritage in Design and Development Overlays – Panel Findings

Planning Panels Victoria has recently considered four Planning Scheme Amendments that are of relevance to this project: Banyule Amendment C93, Boroondara C108, Moreland Amendment C134, and Melbourne Amendment C240.

All four amendments considered the appropriateness of mandatory controls in the context of PPN59 and provide guidance in which circumstances they should be applied. In response to submissions they also considered the issue of whether or not the DDO control should include objectives to protect heritage or whether this should be the sole domain of the Heritage Overlay provisions.

These reports provide useful guidance on the form and wording of DDO controls. In summary, Panel has concluded that:

- The Heritage Overlay identifies what is significant within an Activity Centre.
- Heritage is an appropriate issue for DDOs to provide guidance on to inform future development.
- Mandatory controls should be used in exceptional circumstances and their application should be guided by PPN59 and PPN60.
- Formulae defining the proportion of new built form that can be viewed above the street wall is an appropriate mechanism for informing the design and massing of new built form.

A discussion of the most relevant of these Panel reports, Moreland Amendment C134 and Boroondara C108, is provided below.

3.1 Moreland Amendment C134

Sydney Road, Brunswick is a Major Activity Centre with highly intact, predominantly Victorian streetscapes covered by the Heritage Overlay. Sydney Road and Queens Parade Activity Centres share common traits within their commercial cores including a tram route, a similar mix of historic and current uses, similar historic built form (predominantly two-storey Victorian-era or early twentieth-century commercial buildings), a high proportion of 'contributory' and 'individually significant' buildings with a high level of intactness, and similar existing street wall heights (generally between 8m and 11m). However, it is noted that Sydney Road is a Major Activity Centre with a carriageway width of approximately 20m while Queens Parade is a Neighbourhood Activity Centre with a boulevard width of approximately 60m.

The Statement of Significance for Moreland HO149 – Sydney Road Precinct[4] notes the precinct is of historical, architectural and social significance to the City of Moreland. It includes a number of close similarities with the Statement of Significance for HO330 – Queens Parade Precinct, including:

- Importance as a predominantly intact late 19th and early 20th century retail and commercial strip;
- Reflecting the considerable expansion and growth of the streets, and the suburbs as a whole, during the 19th century Boom period; and
- Largely intact upper floor facades of the shops.

Gazetted on 11 August 2016, Moreland Amendment C134 introduced DDO18, DDO19 and DDO20. DDO18 set mandatory street wall heights on Sydney Road north of Brunswick Road of between 8m and 11m, which would be an appropriate precedent for the Queens Parade Activity Centre given the similarities in their architectural form.

DDO18 also provides a preferred minimum 5m setback for development above the street wall and to establish a preferred ratio of ¾ : ¼ street wall to new built form through the following design objective:

- *Be designed to ensure that it occupies no more than one quarter of the vertical angle defined by the*

[4] Retreived from Victorian Heritage Database, 16 June 2017 (http://vhd.heritagecouncil.vic.gov.au/places/56076#sthash.7LcbbSIM.dpuf)

whole building in the view from an eye-level of 1.7 metres on the opposite side of the street, as illustrated in Figure 1 below.

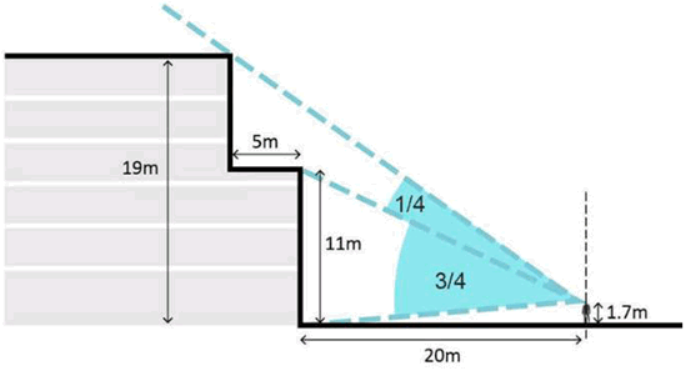

Figure 7: Upper level setbacks along Sydney Road (Figure 1 in Moreland DDO18, retrieved 2 June 2017).

As Queens Parade is a boulevard of approximately three times the width of Sydney Road, the ¾ : ¼ guidance in DDO18 is not considered to be a useful model. From the opposite side of the boulevard, the street wall appears in close to true elevation, making upper level "concealment" of new built form from this distance extremely challenging. It is therefore necessary to establish an alternative framework for new upper level development that does not dominate or visually overwhelm heritage buildings and the streetscape when viewed from within Queens Parade and relevant side streets. An appropriate framework is considered further in Parts II and III of this report.

DDO18 does provide a useful model for dealing with upper level development where the existing heritage building has a street wall height of less than the 11m street wall height provided in that control:

- *Where an existing building with a street wall height of less than 11 metres is to be retained for heritage reasons new development may occupy more than one quarter of the vertical angle defined by the whole building outlined in Figure 1 above.*

A similar approach can be adopted in the Queens Parade context, which will allow for a greater degree of visibility of new built form above single-storey heritage buildings. This will provide an appropriate balance between heritage retention and the creation of a reasonable development envelope.

3.2 Boroondara Amendment C108

The Panel considering Boroondara Amendment C108 discussed the use of mandatory street wall height, upper level setbacks and overall heights across 31 Neighbourhood Activity Centres and three commercial corridors (Camberwell Road/Burwood Road and Canterbury Road).

In relation to heritage the Panel noted its strong support for the protection of heritage assets in Boroondara and recommended reinstatement of policy in the exhibited Amendment that encouraged new development on or adjoining a heritage place to be moderated. In particular, the Panel recommended that policy guidance be included that:

> *The combination of the height, setbacks and design treatment of new buildings should ensure a heritage place on or adjoining the site is not overwhelmed or dominated.*

The Panel also considered the use of mandatory height and setback controls, and recognised that the version of *Plan Melbourne* at that time foreshadowed stronger policy support for the use of mandatory provisions in neighbourhood centres (and residential areas) to increase certainty.

The Panel report recognised that mandatory provisions that prescribed standards without a capacity for

departures have been supported in areas of consistently high heritage value with consistent character. While acknowledging the heritage values and 'main street' character of the Neighbourhood Activity Centres subject to C108, the Panel also recognised that new development will be visible behind the retained façades – particularly from oblique views – and that invisibility of upper level development is either unreasonable or not necessary to achieve the primacy of the street wall.

In conclusion, the Panel accepted some use of mandatory controls within Boroondara's neighbourhood centres, but not in the commercial corridors:

> *The Panel recognises that Plan Melbourne foreshadows stronger policy support for the use of mandatory provisions in neighbourhood centres (and residential areas) to increase certainty. The Panel considers the combination of the street wall and upper level setbacks is critical in neighbourhood centres to maintain the established main street character and in these situations mandatory controls can be justified. However, we consider development with elements that exceed the nominated height and/or adopt alternative setbacks should not be precluded as they may produce better outcomes in some circumstances. The overall maximum height limits should therefore remain discretionary to allow for such circumstances.*

It was the Panel's conclusion mandatory street wall heights that reflected the dominant character of the neighbourhood centres were acceptable (either 8m or 11m, depending on the context). It also found that if a mandatory upper level setbacks were to be adopted, they should be sufficient to ensure that in most cases the upper storey will be clearly distinguishable from the street wall of the heritage building and be a recessive element in neighbourhood centre streetscapes. To achieve this Panel identified 5m as being an appropriate mandatory minimum setback for upper level development in the context of Boroondara's neighbourhood activity centres.

4. Mandatory and Discretionary Height and Setback Controls

PPN59 notes that the VPPs are predominantly performance-based and that mandatory provisions are the exception. The PPN sets out a series of five criteria against which to test proposed mandatory provisions, being:

- Is the mandatory provision strategically supported?
- Is the mandatory provision appropriate to the majority of proposals?
- Does the mandatory provision provide for the preferred outcome?
- Will the majority of proposals not in accordance with the mandatory provision be clearly unacceptable?
- Will the mandatory provision reduce administrative costs?

While the Study Area is zoned C1Z and C2Z rather than Activity Centre Zone (ACZ), PPN60 still provides useful guidance on the application of height and setback controls within the Queens Parade corridor and particularly the use of discretionary and mandatory controls. PPN60 establishes a threshold of 'exceptional circumstances' that may justify the use of mandatory height and setback controls, and notes that a number of Panels have confirmed that discretionary is the preferred form of control in an Activity Centre. The PPN identifies that:

> *When appropriate maximum and minimum height and setback controls are identified, they should be included in the relevant planning scheme as discretionary controls with clear design objectives.*
>
> *Mandatory height and setback controls (that is, controls that cannot be exceeded under any circumstance) will only be considered in exceptional circumstances.*

PPN60 identifies the following criteria for 'exceptional circumstances' that *"...may be identified for individual locations or specific and confined precincts"*. These are (as relevant):

- *significant heritage places where other controls are demonstrated to be inadequate to protect unique heritage values*
- *sites of recognised State significance where building heights can be shown to add to the significance of the place, for example views to the Shrine of Remembrance...*

The Panels that considered Moreland Amendment C134 and Boroondara C108, provide further guidance on the application of mandatory street wall height and upper level setback controls along Queens Parade. The Panels concluded that mandatory controls were appropriate for both street wall heights and upper level setbacks along Sydney Road and in 31 neighbourhood centres in Boroondara. It is our view that Sydney Road and a number of the Boroondara neighbourhood centres share similarities with Queens Parade in terms of heritage fabric and streetscape characteristics.

Further discussion around where mandatory and discretionary controls are considered appropriate in the context of the Queens Parade Study Area can be found in Part II and Part III of this report.

Part II: Heritage Analysis

5. Precinct Boundaries

For the purposes of this project, the Study Area has been divided into five (5) precincts:

Precinct 1: Brunswick Street Precinct

Precinct 2: Boulevard Precinct

Precinct 3: St John's Precinct

Precinct 4: Activity Centre Precinct

Precinct 5: North Eastern Precinct

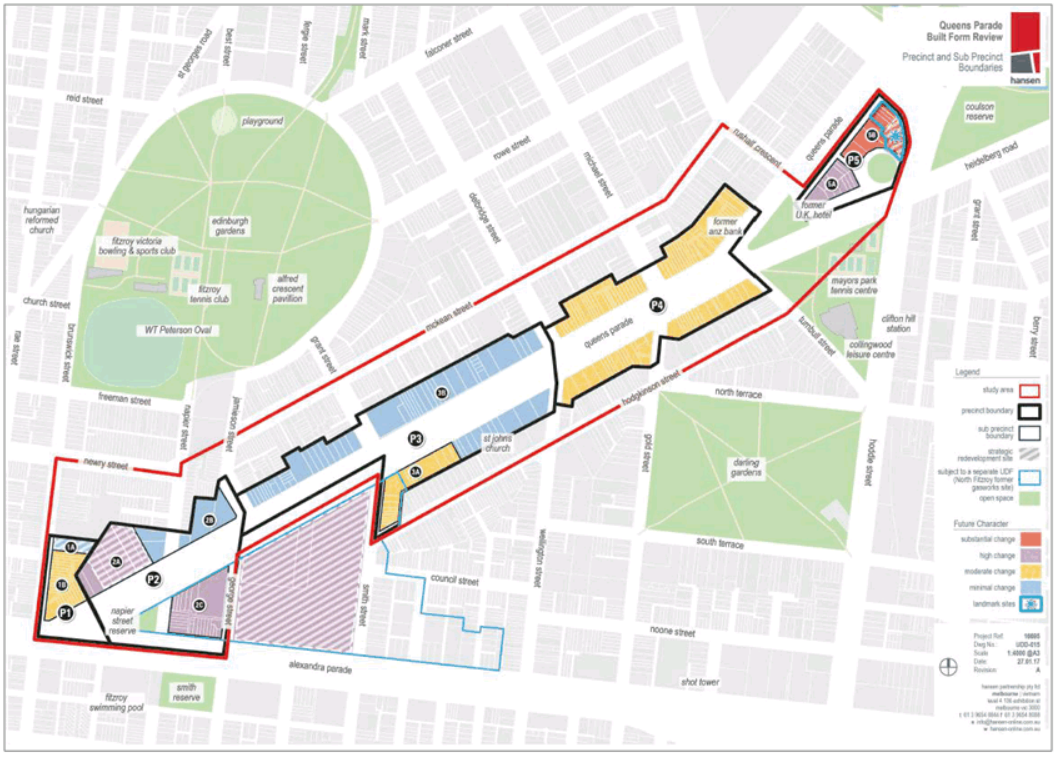

Figure 8: Study Area Precincts (© Hansen Partnership, 2017)

Following is an analysis of the heritage components and qualities of each of the Study Area Precincts, including significant views and streetscapes found within the precincts. An analysis of future built form character considerations has been provided along with recommended built form parameters to appropriately manage heritage values.

6. Precinct 1 – Brunswick Street Precinct

6.1 Description

Precinct 1 (Brunswick Street Precinct) is located at the northeast corner of Brunswick Street and Alexandra Parade/Queens Parade.

Precinct 1 is made up of two sub-precincts:

- Sub-precinct 1A – a strategic development site occupied by a recently constructed six-storey apartment development with no heritage values or issues associated with this site.
- Sub-precinct 1B – which is dominated by intact single-storey terraced houses along Brunswick Street and intact single and two-storey terraced houses along Queens Parade. The two-storey former National Bank is located prominently at 460 Brunswick Street, forms a southern gateway to Queens Parade and to Fitzroy North more broadly. There is a vacant site at the rear of the terraced houses at 484 to 490 Brunswick Street (known as Lot 1 on Title Plan TP806921).

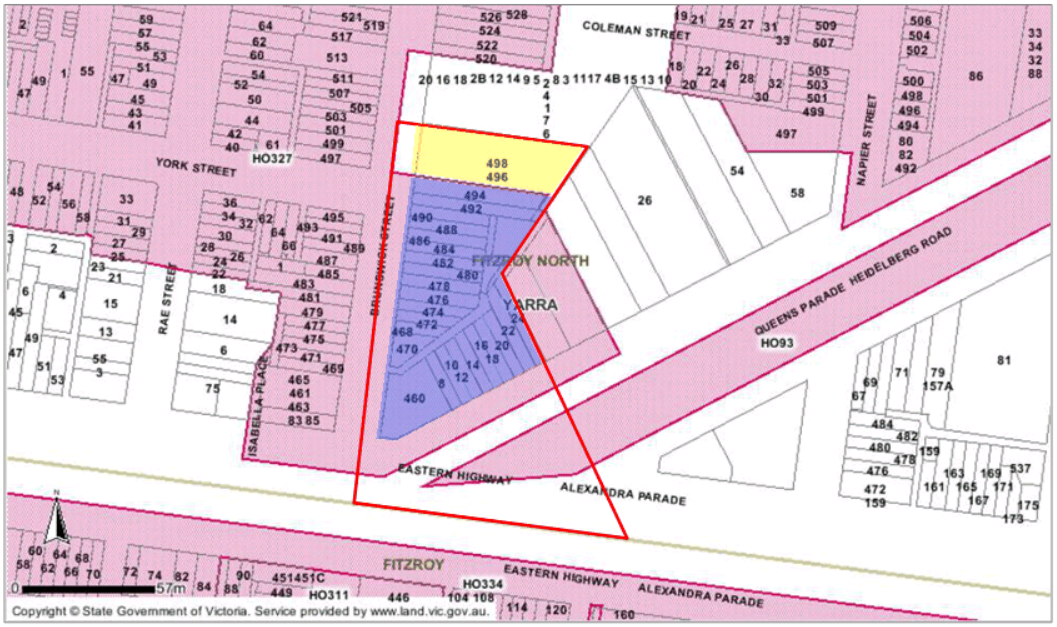

Figure 9: Heritage Overlay map – Precinct 1 outlined in red (sub-precinct 1A shaded in yellow and sub-precinct 1B in blue) (Planning Maps Online, accessed 14 August 2017)

Figure 10: Recent development at 496-498 Brunswick Street (sub-precinct 1A)

Figure 11: 460 Brunswick Street (sub-precinct 1B)

Figure 12: Single storey terraced houses Brunswick (sub-precinct 1B)

Figure 13: Double storey terraced houses on Queens Street Parade (sub-precinct 1B)

Figure 14: Vacant land (Lot 1 on Title Plan TP806921) (sub-precinct 1B)

6.2 Heritage Status

6.2.1 Existing Conditions

Sub-precinct 1A

Sub-precinct 1A falls outside of the Heritage Overlay and does not contain heritage fabric. It is noted that the site is identified in Appendix 8 as being 'not contributory' to the North Fitzroy Precinct (HO327) despite not being included within the Heritage Overlay precinct boundary.

Sub-precinct 1B

All the buildings within sub-precinct 1B are included within the North Fitzroy Precinct (HO327). The buildings are predominantly of Victorian-era construction and architectural style. With the exception of the corner former National Bank building at 460 Brunswick Street (Figure 11), they are one or two-storey residential or former commercial/residential buildings of the Victorian and Edwardian eras that generally demonstrate the following characteristics:

- Attached or terraced construction with no side yards
- Masonry construction with less than 40% of the street wall face comprised with openings such as windows and doors
- Painted render or face brick façades
- Parapeted front facades with some pitched and hipped roofs to single-storey buildings
- Small setbacks from Brunswick Street within gardens
- Small or no setback from Queens Parade
- Early or altered shop fronts to the ground floor of two-storey buildings on Queens Parade
- Verandahs to single-storey buildings
- Visible chimneys to single-storey buildings

The former National Bank building is a two-storey Neo-Classical commercial building that occupies the prominent corner of Brunswick Street and Alexandra Parade/Queens Parade. It is rendered with a parapeted form built to the street boundary. The hipped roof is clad in corrugated steel and there are five prominent rendered chimneys. The two-storey building is flanked by less adorned lower wings, and there is an at-grade car park behind to the building, which is suitable for redevelopment.

Appendix 8 identifies all buildings as being 'contributory' to the precinct, with the exception of the former National Bank Building at 460 Brunswick Street, which is identified as being 'individually significant'.

Precinct 1

The street trees within the central part of the Queens Parade road reserve are included within HO93. This Heritage Overlay falls partially within Precinct 1.

In summary, the existing heritage status for Precinct 1 is:

Victorian Heritage Register
None

Individual Heritage Overlays
HO93 – Queens Parade, Between Alexandra Parade & Delbridge Street Clifton Hill/ North Fitzroy - Street Trees (part)

Precinct Heritage Overlays
HO327 – North Fitzroy Precinct (part)

6.2.2 Recommended Changes

The following changes to the heritage controls are recommended for Precinct 1:

1. 496-500 and 498 Brunswick Street are identified as 'not contributory' in Appendix 8. The property (identified as 496-498 in Planning Maps Online) is not in the Heritage Overlay and is not a heritage place. Reference to '496-500 Brunswick Street' and '498 Brunswick Street' should be removed from Appendix 8.
2. The site known as Lot 1 on Title Plan TP806921 is vacant and should be identified in Appendix 8 as 'not contributory'.

6.3 Zoning

6.3.1 Existing Conditions

All land within Precinct 1 is included within the Mixed Use Zone (MUZ).

6.3.2 Recommended Changes

Given the heritage status and intact heritage fabric within sub-precinct 1B, the current use and built form character of this sub-precinct is considered to more readily accord with either the Neighbourhood Residential Zone (NRZ) (consistent with the zoning of the land generally to the north and west) or, alternatively, the General Residential Zone (GRZ). Consideration should be given to the merits of rezoning the land to NRZ or GRZ to reflect that use and character.

The 'individually significant' former National Bank at the corner of Brunswick Street and Queens Parade is not residential in character and has development opportunities on the vacant land to the north and east of it. It is considered to be appropriately zoned MUZ to enable a wider range of uses than NRZ or GRZ would allow. Likewise, the vacant land at the rear of 478-494 Brunswick Street – known as Lot 1 on Title Plan TP806921 – transitions to the MUZ-zoned development site at 26 Queens Parade and the MUZ is therefore considered appropriate in this location.

6.4 Key views

Distant views of the spire of St John the Baptist Church are afforded from the intersection of Alexandra and Queens Parades, although the heritage street trees largely obscure this view. This is not considered a key view of the Church belfry and spire and management of this view is not necessary.

Figure 15: Distant view of the spire of St John's Church (blue arrow) – note it is largely obscured by trees (View 1)

6.5 Significant Streetscapes

The rows of Victorian terraced houses at 470-494 Brunswick Street and 8-24 Queens Parade are highly intact and warrant their 'contributory' status in the context of the North Fitzroy Precinct. While they demonstrate a high degree of built form consistency, they are not considered to display the level of architectural and aesthetic quality necessary to be categorised as a 'significant streetscape' that would warrant additional built form considerations as part of this project.

While the buildings are not considered to create a significant streetscape, the mature double avenue of trees planted throughout the wide Queens Parade boulevard are significant (HO93) and new development should be scaled to avoid diminishing the boulevard quality of Queens Parade.

6.6 Potential Future Character Considerations

Sub-precinct 1A

The future character of sub-precinct 1A has been established by the recent development constructed on the site.

Sub-precinct 1B

With the exception of 460 Brunswick Street, sub-precinct 1B is residential in character with predominantly single-storey terraced houses on Brunswick Street and one and two-storey terraced houses to Queens Parade, set within the broader context of the wide Queens Parade boulevard. The heritage status of these properties will see the heritage fabric largely retained. The predominantly low scale of these residential properties should be retained. Rear development of two to three-storeys in height consistent with the provisions of Clause 22.02 relating to new development, additions and alterations would be appropriate.

The former National Bank building and the vacant site at the rear of 484-490 Brunswick Street are not residential in character and can accommodate more substantial development. Redevelopment of the former National Bank and land to the immediate north and west should respect the height of the existing building and any additional levels should be set back behind the parapet. Development on the vacant site at the rear of 478-494 Brunswick Street should ensure its scale protects the setting of the single-storey houses fronting Brunswick Street.

6.7 Recommended Built Form Parameters

Sub-precinct 1A

Sub-precinct 1A has recently been developed to a height of six-storeys and is unlikely to undergo further redevelopment for many years. It is considered that additional controls are not required for this site.

Sub-precinct 1B

Should the residential properties within sub-precinct 1B be rezoned GRZ or NRZ, a DDO is not considered necessary for 8-24 Queens Parade and 468-494 Brunswick Street. Their development would instead be guided by Clause 22.02 of the Yarra Planning Scheme and the underlying zoning.

If the land is not rezoned, it would be appropriate to introduce a DDO with height and setback controls consistent with the application of the Heritage Policy at Clause 22.02 and the intent of the residential zones to ensure the heritage fabric is appropriately managed.

Additional built form controls are required to inform development of the former National Bank building, the adjacent at-grade car parking at 460 Brunswick Street and the vacant land behind 484-490 Brunswick Street. These controls should:

- Ensure that any new development in the car park adjacent to the former National Bank building adopts the parapet height of the heritage building for the street wall.

- Ensure that any upper level additions to the former National Bank building and infill development adjacent to former National Bank building and at the rear of 484-490 Brunswick Street are subservient to the heritage fabric and are visually recessive in mass, scale and materiality.
- Ensure that any upper level additions to the former National Bank building are set back from the street wall to ensure the prominence of the heritage fabric is retained and the three-dimensional form of the heritage building is preserved.
- For the former National Bank building, require the retention of chimneys and encourage the retention of roof form that is visible from the public realm.

7. Precinct 2 – Boulevard Precinct

7.1 Description

Precinct 2 is located northeast of the intersection of Brunswick Street and Alexandra Parade. All buildings north of Queens Parade within this precinct are included within the North Fitzroy Precinct (HO327), with the exception of part of the former K.G. Luke site at 26-58 Queens Parade, which is not included in the Heritage Overlay.

Precinct 2 is made up of three sub-precincts:

- Sub-precinct 2A – primarily made up of the strategic development site at 26-54 Queens Parade which is currently occupied by the former K.G. Luke Pty Ltd factory.
- Sub-precinct 2B – comprising recently constructed three to five-storey apartment buildings and townhouses.
- Sub-precinct 2C – contains a mix of altered nineteenth century houses and later light industrial buildings. This sub-precinct is subject to a separate Urban Development Framework (UDF).
- Sub-precinct 2D – contains row of intact two-storey nineteenth century terraced houses at 472-484 Napier Street.

The Napier Street Reserve lies outside the sub-precincts and contains an electrical substation.

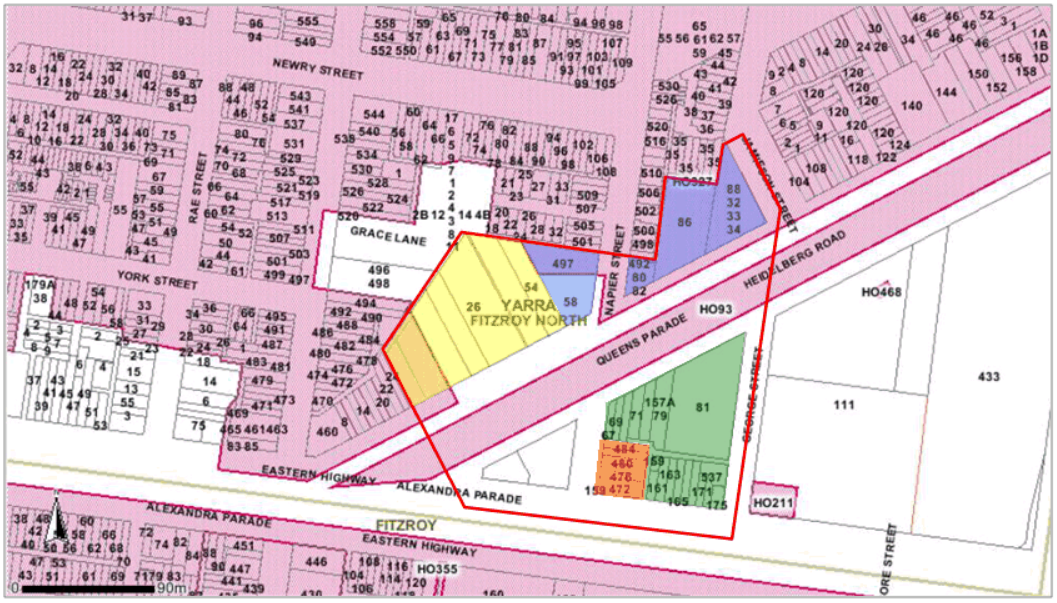

Figure 16: Heritage Overlay map – Precinct 2 outlined in red (sub-precinct 2A shaded in yellow, sub-precinct 2B in blue, sub-precinct 2C in green and sub-precinct 2D in orange) (Planning Maps Online, accessed 14 August 2017)

Figure 17: Former K.G. Luke Pty Ltd factory, 26-58 Queens Parade (sub-precinct 2A)

Figure 18: Contemporary apartments and townhouses (sub-precinct 2B)

Figures 19 and 20: Terraces houses at 472-484 Napier Street (sub-precinct 2D)

Figure 21: Light industrial buildings (sub-precinct 2C)

Figure 22: Electrical substation on Alexandra Parade in Napier Street Reserve

7.2 Heritage Status

7.2.1 Existing Conditions

Sub-precinct 2A

The former K.G. Luke Pty Ltd factory in sub-precinct 2A is partly covered by the North Fitzroy Precinct Heritage Overlay (HO327) and is partly outside the Heritage Overlay. At present approximately two-thirds of the Art Deco façade and associated factory buildings are included in the Heritage Overlay. Appendix 8 currently grades 26 Queens Parade and 28-58 Queens Parade as 'individually significant' within HO327, despite only part of the site being captured by the overlay.

The K.G. Luke Pty Ltd factory comprises a series of buildings with front facades to Queens Parade constructed in the 1930s and 1960s. The western part of the site appears to have been fully developed by the late 1930s (or early 1940s) and additional buildings were constructed in the 1960s. All buildings are of two-storeys and are painted in a consistent colour across the extensive Queens Parade façade.

The 1930s Art Deco facade is of brick and steel-frame construction with a render finish applied to the body of the main façade and piers that divide the façade into nine unevenly spaced bays. Spandrels below the first floor windows are of face brickwork that has been overpainted. The highly distinctive parapet consists of a series of uneven overpainted brick and cement rendered vertical elements which rise and fall across the façade, creating a castellated appearance. Openings are set between the piers and some original steel-framed windows remain at first floor level, however the majority of openings have been modified. A pyramidal hood, supported by heavy corbels, remains over a secondary entrance at street level.

The contrasting highly restrained façade of the 1960s building to the east is of seven bays with simple strips of high windows to the ground floor, larger windows to the first floor and a plain parapet above. Two bays at the eastern end are recessed at ground floor level and the upper floor is supported on two plain, square columns.

The showroom and factory buildings behind the 1930s façade and 1960s building are a mix of saw-tooth and pitched roofed structures that occupy the majority of the site.

Sub-precinct 2B

The buildings within sub-precinct 2B are all affected by HO327. Planning Maps Online identifies these properties (from west to east) as:

- 497 Napier Street
- 492 Napier Street & 80-82 Queens Parade
- 86 Queens Parade
- 88 Queens Parade & 32, 33 and 34 Jamieson Street.

All sites within this sub-precinct contain contemporary apartment/townhouse complexes that do not contribute to the heritage values of HO327. Appendix 8 identifies that 497 Napier Street and 80 & 86 Queens Parade are 'not contributory' to HO327; however other addresses are not listed.

Sub-precinct 2C

Sub-precinct 2C is not covered by the Heritage Overlay. The sub-precinct comprises a row of single-storey Victorian terraced houses facing Alexandra Parade flanked by two timber Edwardian-era houses to the west and a post-war two-storey shop to the east. The terraced houses are built to the street and are of rendered brick construction with decorative parapets and later timber and steel fences.

The balance of sub-precinct 2C contains a mix of twentieth century single-storey light industrial buildings and two highly altered Edwardian-era houses at the corner of Napier Street and Queens Parade.

Sub-precinct 2D

Sub-precinct 2D is not covered by the Heritage Overlay.

The sub-precinct contains an intact row of five double-storey Victorian terrace houses – 'Elizabeth Terrace' – at 472-484 Napier Street. These polychromatic brick terrace houses have double-storey front verandahs, rendered and balustraded parapeted facades that conceal hipped roofs, and rectangular window openings. The terraces are designed as a group with a large and elaborate central arched pediment containing an acroterion ornament, a central name plate displaying the name 'Elizabeth Terraces' and rows of rosettes across all facades. Dividing wing walls, which display consoles and masks, rise to plinths and urns at parapet level. Roofed with corrugated iron, the verandahs are decorated with cast iron friezes and balustrading, and cast iron palisade fences run between wing walls at street level.

The house at 472 Napier Street is a large two-storey polychromatic brick terrace house which is located on the corner of Alexandra Parade. The front elevation to Napier Street has a double storey verandah with decorative cast iron friezes and balustrade, an exposed hipped roof, wing walls and a cast iron palisade fence. The side façade to Alexandra Parade is rendered and painted and contains simple rectangular window openings. A large rendered chimney towards the front of the house is visible from this southern view.

Precinct 2

The street trees within the central part of the Queens Parade road reserve are included within HO93. This Heritage Overlay falls partially within Precinct 2.

In summary, the existing heritage status for Precinct 2 is:

Victorian Heritage Register
None

Individual Heritage Overlays
HO93 – Queens Parade, Between Alexandra Parade & Delbridge Street Clifton Hill/ North Fitzroy - Street Trees (part)

Precinct Heritage Overlays
HO327 – North Fitzroy Precinct (part)

7.2.2 Recommended Changes

The following changes to the heritage controls are recommended for Precinct 2:

1. The heritage significance of the former K.G. Luke Pty Ltd factory site has been reassessed as part of this review (see Appendix A). The reassessment has found that the Heritage Overlay (HO327) should cover the former K.G. Luke Pty Ltd factory site to the extent of the Art Deco façade and associated showroom and factory buildings (refer to Appendix A for details). It is recommended that the citation prepared for the place be included within the *Yarra High Streets: Statements of Significance* reference document at Clause 22.02 of the Yarra Planning Scheme (see Appendix H). Appendix 8 should be updated to reflect this change.
2. 58 Queens Parade is not included within the Heritage Overlay but is currently identified as part of the former K.G. Luke Pty Ltd factory and graded as 'individually significant' in Appendix 8. This site has been redeveloped as a five-storey apartment building and reference to it should be removed from Appendix 8.
3. 80, 86 and 88 Queens Parade, 32, 33 and 34 Jamieson Street and 497 Napier Street are occupied by contemporary multi-unit residential development. Appendix 8 should be updated to clearly identify these properties as 'not contributory' to HO327.
4. The row of terraced houses at 472-484 Napier Street in sub-precinct 2D are highly intact and their heritage significance has been assessed as part of this review (see Appendix B). The assessment has found that the row warrants inclusion on the Heritage Overlay. The citation prepared for the place should be included within the *Yarra High Streets: Statements of Significance* reference document at Clause 22.02 of the Yarra Planning Scheme (Appendix H). Appendix 8 should be updated to reflect this change.

7.3 Zoning

7.3.1 Existing Conditions

With the exception of 497 Napier Street (which is zoned NRZ), all land within sub-precincts 2A and 2B are included within the MUZ. The properties in sub-precincts 2C and 2D are all included in the Commercial 2 Zone (C2Z).

7.3.2 Recommended Changes

The current zoning for sub-precincts 2A, 2B and 2C is considered appropriate in heritage terms.

As the terraced houses at 472-484 Napier Street (in sub-precinct 2D) are considered to warrant inclusion within the Heritage Overlay, consideration should be given to applying the GRZ or NRZ to these properties in recognition of their current and historic residential use and low-scale residential character, rather than the current C2Z.

7.4 Key views

Distant views of the St John's Church spire are afforded from the north side of Queens Parade and the intersections of Napier and Jamieson Streets, although the heritage street trees largely obscure this view. These views are not considered key views of the Church belfry and spire and management of the views is not necessary.

7.5 Significant Streetscapes

The mixed nature of Precinct 2 and the amount of recent development and non-contributory buildings means that there are no streetscapes that are considered to be significant.

While the buildings are not considered to create a significant streetscape, the mature double avenue of trees planted throughout the wide Queens Parade boulevard are significant (HO93) and new development should be scaled to avoid diminishing the boulevard quality of Queens Parade.

7.6 Potential Future Character Considerations

Sub-precinct 2A

Sub-precinct 2A is a large former factory site that can accommodate higher built form; however, the site exists within a rich and fine-grained heritage context to the west and north, and contains an elaborate Art Deco façade of aesthetic significance. Any new development should be moderated so as to not visually dominate the Art Deco façade and to respect the two-storey scale of the Victorian-era terraces to the west and the predominantly single-storey scale of the Victorian-era streetscapes on Coleman and Newry Streets to the north of the site. The Victorian Civil and Administrative Tribunal (VCAT) recently considered an application for review (Gurner 26-56 Queens Parade Pty Ltd v Yarra CC [2017] VCAT 1745) in relation to planning permit applications (PLN16/0434 & PLN16/0732) for the redevelopment of the former K.G. Luke Pty Ltd factory. VCAT granted leave to prepare amended plans incorporating the changes referred to in the reasons for this order.

Sub-precinct 2B

The future character of sub-precinct 2B has been established by the three to five-storey apartment and townhouse developments. These sites are unlikely to undergo further redevelopment for many years.

Sub-precinct 2C

The absence of heritage buildings within sub-precinct 2C means greater height can be accommodated. An appropriate street wall height should be established that retains the openness and boulevard quality of both Alexandra and Queens Parades. The Napier Street and Alexandra Parade street wall should respect the two-storey Victorian-era street wall established by the parapets of the terraced houses in sub-precinct 2D where it

adjoins these buildings, with higher development set back to allow the heritage fabric to remain prominent in the streetscape and the Napier Street Reserve.

Sub-precinct 2D

The row of terraced houses at 472-484 Napier Street should be conserved. If included in the Heritage Overlay, the heritage status of these properties will see the heritage fabric largely retained. Rear extensions and additions that are consistent with the provisions of Clause 22.02 of the Yarra Planning Scheme will ensure the future character of these residential properties remains predominantly low-scale.

7.7 Recommended Built Form Parameters

Sub-precinct 2A

A DDO should apply to sub-precinct 2A to inform the redevelopment of the former K.G. Luke Pty Ltd factory. The DDO should focus on retaining the prominence of the aesthetically significant Art Deco facade within the streetscape and providing a sensitive interface with adjacent heritage buildings including those to the west and on Coleman and Newry Streets. The controls should:

- Require the retention, conservation and incorporation of the existing Art Deco façade into the redevelopment of the site. This includes ensuring that internal alterations respect the exterior form of the building, its fenestration pattern and existing inter-floor height.
- Adopt a street wall height for infill development on the site that is no higher than the top of the existing parapet at the former factory entrance to create a consistent parapet height along this portion of Queens Parade.
- Ensure that the heritage building retains its prominence within the streetscape and its three-dimensional form as viewed from the public realm to avoid 'facadism'. This will require new upper-level development to be set back from the street wall.
- Ensure that new development does not visually dominate the existing heritage fabric by requiring that new upper level built form is no greater than the volume of the heritage façade when the site is viewed from within the Queens Parade boulevard.
- Ensure new development does not diminish, or detract from the heritage values of the boulevard streetscape and the heritage trees along Queens Parade.
- Recognise the low-scale, fine grain heritage form of existing surrounding development and ensuring appropriate transitions to the one to two-storey adjacent heritage fabric. New development should not dominate views to the south over the rooftops of the terraced houses on Coleman and Newry Streets.

Sub-precinct 2B

Sub-precinct 2B has recently been developed to heights of three to five-storeys and additional controls are not required for this sub-precinct.

Sub-precinct 2C

For sub-precinct 2C, either the UDF should be modified to require consideration of the impact of development on the setting of 472-484 Napier Street, particularly when viewed from Napier Street, or a DDO be applied with the objective of protecting the visual prominence of the heritage buildings. A DDO control should:

- Ensure that any new development adjacent to the heritage buildings on Napier Street and Alexandra Parade adopts the parapet height of the heritage buildings for the street wall.
- Recognise the low-scale, fine grain heritage form of the heritage buildings and ensure appropriate transitions to the two-storey adjacent heritage fabric. New development should not dominate views over the rooftops of the two-storey heritage buildings on Napier Street when viewed from the opposite side of Napier Street.
- Ensure that any development adjacent to the heritage buildings are subservient to the heritage fabric and are visually recessive in mass, scale and materiality.

- Ensure new development does not diminish, or detract from the heritage values of the boulevard streetscape of Queens Parade.

Sub-precinct 2D

Should 472-484 Napier Street be rezoned GRZ or NRZ, a DDO is not considered necessary for those properties as their development would instead be guided by Clause 22.02 of the Yarra Planning Scheme and the underlying zoning.

If 472-484 Napier Street is not rezoned, it would be appropriate to introduce a DDO with height and setback controls consistent with the application of the Heritage Policy at Clause 22.02 to ensure the heritage fabric is appropriately managed.

8. Precinct 3 – St John's Precinct

8.1 Description

Precinct 3 extends along the north side of Queens Parade between Jamieson and Delbridge Streets to the depth of a single property. It extends along the south side of Queens Parade between Smith and Wellington Street to the depth of a single property and includes the properties on the east side of Smith Street between Hodgkinson Street and Queens Parade.

The southern side of Queens Parade falls within the extent of the Neighbourhood Activity Centre. All properties in Precinct 3 are included in the Heritage Overlay.

Precinct 3 is made up of two sub-precincts:

- Sub-precinct 3A – is primarily made up of the commercially zoned land on the east side of Smith Street and the south side of Queens Parade, which is occupied by single and two-storey commercial buildings.
- Sub-precinct 3B – comprises primarily residential properties on the north side of Queens Parade that range from single and two-storey Victorian and Edwardian-era houses to recent multi-unit apartments and townhouses. On the south side of Queens Parade, sub-precinct 3B includes a small number of two-storey Victorian-era houses and is dominated by the St John the Baptist Church complex (including the church, school, hall, residence and presbytery) at the eastern end of the precinct.

The former Fitzroy Gas Works site, which is bound by Queens Parade, Alexandra Parade, George Street and Smith Street, is excluded from the study area and is subject to a UDF. The site includes a 'Heritage Object' included on the VHR (Porter Prefabricated Iron Store, VHR H2243) – also included in the Heritage Overlay as HO467 – and the Former Fitzroy Gas Works Valve House and Store (HO211); however, the heritage impacts on this site have not been considered as part of this review as it is outside the study area.

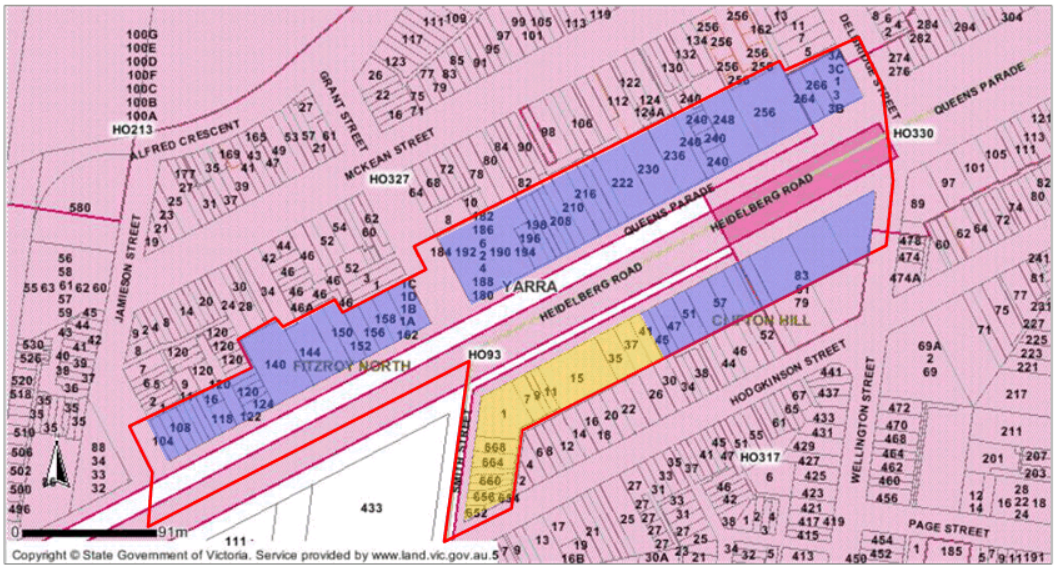

Figure 23: Heritage Overlay map – Precinct 3 outlined in red (sub-precinct 3A shaded in yellow and sub-precinct 3B in blue) (Planning Maps Online, accessed 28 August 2017)

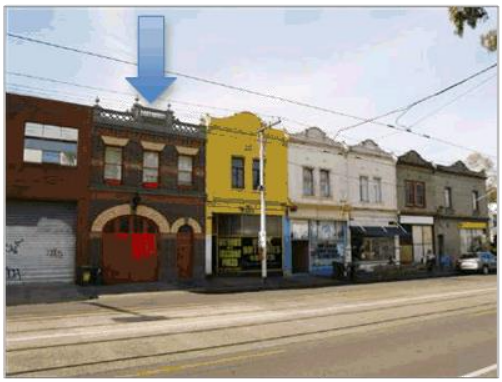

Figure 24: East side of Smith Street (sub-precinct 3A) former Fire Station, 662 Smith Street identified with arrow

Figure 25: 11A, 11 and 7 Queens Parade (sub-precinct 3A)

Figure 26: Commercial buildings on the south side of Queens Parade (sub-precinct 3A)

Figure 27: Recent townhouse development on the north side of Queen Parade (sub-precinct 3B)

Figure 28: Detached Victorian-era houses on the south side of Queens Parade (sub-precinct 3B)

Figure 29: St John the Baptist Church (sub-precinct 3B)

8.2 Heritage Status

8.2.1 Existing Conditions

Sub-precinct 3A

Sub-precinct 3A extends down Smith Street to Hodgkinson Street and includes the commercial strip from 1 to 41 Queens Parade. The whole of sub-precinct 3A falls within HO317 – Clifton Hill West Precinct.

Along Smith Street, sub-precinct 3A has a relatively consistent two-storey street wall derived from the smaller-scale, two-storey Victorian-era commercial buildings with a parapet height of approx. 9m. These shop/residences are built to Smith Street, constructed of rendered brick and have retained their first floor windows and early timber shopfronts on the ground floor

Of particular note within this group is the former Fire Station at 652-662 Smith Street. The facade of the Fire Station is of tuckpointed, polychromatic brickwork with the main body of brown brick, bands of red brick and cream brick window voussoirs. An elaborate rendered parapet above contains a central raised balustraded section and flanking sections comprised of circular motifs. Four urns sit atop dividing pedestals and the whole parapet is supported on pairs of rendered console brackets. The upper façade contains three rectangular double hung windows with chamfered reveals and a row of decorative tiles laid within a red brick band.

The façade at street level contains two arch-headed openings of unequal size, breaking the symmetry of the upper façade. The large flat-arched opening contains four doors with glazed top panels and the adjoining semi-circular arched opening contains a single panelled door with glazed fanlight. A tiled red brick band, matching that of the upper façade, forms an impost for cream brick voussoir arches which frame both openings.

The row of two-storey Victorian-era commercial buildings and the small former Fire Station are identified in Appendix 8 as 'contributory'.

To the north of the Fire Station are two storey brown brick post-war warehouse buildings and a single-storey showroom building.

Near the corner of Smith Street and Queens Parade are three altered commercial buildings at 7, 9 and 11 Queens Parade. The building at 7 Queens Parade is single-storey with a tall parapet with projecting brick panels at the centre and each end. There is no decorative architectural detail below the parapet and the ground floor façade has been heavily altered with no original openings evident. The two-storey pair of terraced shop residences at 9 and 11 Queens Parade have a relatively simple rendered form with a single pair of plain window openings with timber sash windows to the first floor. These three building are identified in Appendix 8 as 'contributory'.

North-east of 11 Queens Parade, at 15-33 Queens Parade is a post-war single-storey showroom/warehouse building along with the double-fronted, single-storey Victorian-era former house at 35 Queens Parade and a single-storey Victorian commercial building at 37 Queens Parade. 41 Queens Parade is a heavily altered late-nineteenth or early twentieth century single-storey house.

Nos 15-33 and 41 Queens Parade are identified in Appendix 8 as being 'not contributory' to the Clifton Hill West Precinct while nos 35 and 37 are identified as 'contributory'.

Sub-precinct 3B

North of Queens Parade, sub-precinct 3B is subject to either HO327 – North Fitzroy Precinct, or HO330 – Queens Parade Precinct, North Fitzroy/Clifton Hill. This portion of sub-precinct 3B has a mixed character with single and two-storey Victorian terraced houses and freestanding Edwardian-era houses interspersed with Inter-war period and 1960s blocks of flats, a former hotel and a bank as well as recent townhouse and apartment developments.

Buildings identified as 'Individually Significant' include:

- The bluestone house at 118 Queens Parade

- The former Selotta Shoes Pty Ltd Factory, 122 Queens Parade
- The house at 216 Queens Parade
- The Clifton Court Flats, 230 Queens Parade
- The front part of 240 Queens Parade
- The former Christian Brothers Monastery, 256-260 Queens Parade
- Residence and Surgery, 264 Queens Parade
- The former National Bank of Australasia, 270 Queens Parade.

On the south side of Queens Parade, sub-precinct 3B is subject to either HO317 – Clifton Hill Western Precinct or HO330 – Queens Parade Precinct, and is dominated by the 'Individually Significant' St John the Baptist Church complex, comprising the church with its prominent belfry and spire, the school, presbytery, former hall/shops, former presbytery and the Queens Parade boundary fence to the church and presbytery.

To the west of the St John's Church complex are four 'contributory' Victorian-era houses comprising a pair of rendered two-storey terraced houses built to the street boundary at nos 43 and 45, a free-standing polychromatic brick two-storey terraced house set back from the street at no. 47, and a double-fronted brick single-storey house at no. 51.

Precinct 3

The street trees within the central part of the Queens Parade road reserve are included within HO93. This Heritage Overlay falls partially within Precinct 3.

In summary, the existing heritage status for Precinct 3 is:

Victorian Heritage Register
None

Individual Heritage Overlays
HO93 – Queens Parade, Between Alexandra Parade & Delbridge Street Clifton Hill/ North Fitzroy - Street Trees (part)

Precinct Heritage Overlays
HO317 – Clifton Hill Western Precinct
HO327 – North Fitzroy Precinct
HO330 - Queens Parade Precinct, North Fitzroy/Clifton Hill

8.2.2 Recommended Changes

The following changes to the heritage controls are recommended for Precinct 3:

- The St John the Baptist Church Complex is a highly significant complex within the locality. The landmark status of the Church belfry and spire within the municipality is recognised in Yarra's 'Landmarks and Tall Structures' policy at Clause 22.03. It is recommended that the complex be included within its own Heritage Overlay in recognition of its significance within the City of Yarra and the need to trigger internal controls to manage the significant organ. See Appendix C for the heritage citation and detailed recommendations for the site. The citation should be included within the *Yarra High Streets: Statements of Significance* reference document at Clause 22.02 of the Yarra Planning Scheme (Appendix H). Appendix 8 should be updated to reflect this change.
- The former Fire Station at 662 Smith Street is currently identified as a 'contributory' building within HO317, however, it is an early and rare example of a suburban fire station which pre-dated the establishment of the Metropolitan Fire Brigade (see Heritage Citation at Appendix D). It is recommended that the property be re-graded within Appendix 8 to 'Individually Significant' and that the citation should be included within the *Yarra High Streets: Statements of Significance* reference document at Clause 22.02 of the Yarra Planning Scheme (Appendix H).
- The commercial Victorian-era buildings at 7, 9 and 11 Queens Parade are currently identified as 'contributory' within HO317. These simple and altered buildings are bookended by two large 'not

contributory' sites at 1-5 Queens Parade and 15-33 Queens Parade and their contribution to the HO317 is considered to be minimal. Appendix G to this report provides a detailed rationale for regrading these three properties to 'not contributory' in Appendix 8.

8.3 Zoning

8.3.1 Existing Conditions

Most of sub-precinct 3A is zoned C2Z with 652-658 Smith Street zoned Commercial 1 Zone (C1Z). The vast majority of sub-precinct 3B is zoned NRZ with isolated sites zoned GRZ at 120 Queen Street, 256 Queen Street and the St John's Parish Hall on the corner of Queens Parade and Wellington Street.

8.3.2 Recommended Changes

The use of a commercial zone for sub-precincts 3A and 3B is appropriate in heritage terms, and the predominately residential character of sub-precinct 3B is appropriately recognised through the existing GRZ and NRZ zoning.

8.4 Key views

A number of key views of the St John the Baptist Church belfry and spire are provided from within Precinct 3.

Figure 30: Location of key views (blue arrows) of St John's Church belfry and spire (red star) (©nearmap, 17 Sept 2017)

The principal view of St John's Church is from the opposite (northern) side of Queens Parade, particularly between Grant Street to the west and Delbridge Street to the east (Figure 31). These views are protected by the road reserve and HO93, which will trigger a consideration of heritage issues if development of intrusive street furniture, including tram infrastructure, is proposed.

Key oblique views of St John the Baptist Church belfry and spire from the west are provided from the intersection of Queens Parade and Delbridge Street (Figure 32), the northwestern intersection of Queens Parade and Grant Street (Figure 33) and from the southwestern side of Queens Parade and Smith Street (Figure 34). These views are where the belfry and spire of St John the Baptist Church become a prominent landmark element within the streetscape. Similar to the view directly opposite the Church, the view from the intersection of Queens Parade and Grant Street is protected by the road reserve and HO93. However, built

form controls will be required along the south side of Queens Parade to protect the key view of the landmark form from the intersection of Queens Parade and Smith Street.

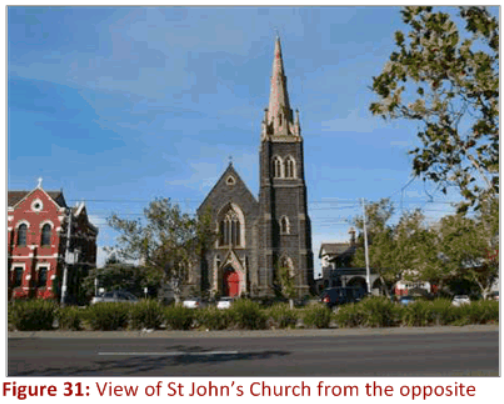

Figure 31: View of St John's Church from the opposite (northern) side of Queens Parade

Figure 32: View of St John's Church from intersection of Delbridge Street

Figure 33: View of St John's spire from intersection of Grant Street

Figure 34: View of St John's spire from the intersection of Smith Street

8.5 Significant Streetscapes

The mixed character of Precinct 3 and the amount of recent development and not-contributory buildings means that there are no streetscapes that are considered to be significant. While the two-storey Victorian-era shops and small former Fire Station at 652-662 Smith Street are highly intact and are consistently two-storeys, they comprise a short row (six buildings) that are relative simple in their architectural expression and quality. Within the wider context of the City of Yarra, they are not considered to form a significant streetscape.

While the buildings are not considered to create a significant streetscape, there are a small number of mature trees planted throughout the wide Queens Parade boulevard that are significant (HO93). New development should be scaled to avoid diminishing the boulevard quality of Queens Parade.

8.6 Potential Future Character Considerations

Sub-precinct 3A

Sub-precinct 3A has two distinct sections: the consistent and intact two-storey Victorian-era commercial buildings and former Fire Station from 652 to 662 Smith Street, and the mixed and less intact section on the northeast corner of Queens Parade and Smith Street and along Queens Parade itself.

The intact streetscape to Smith Street should retain the existing buildings and the existing street wall height with new development set back from the façade and designed to not dominate the Victorian-era buildings. The remainder of sub-precinct 3A has fewer site-specific heritage issues but will need to be massed to retain key views to the belfry and spire of St John's Church and to provide a sensitive transition to the predominantly single-storey Victorian-era character of Hodgkinson Street to the south.

Sub-precinct 3B

The residential component of sub-precinct 3B on the north side of Queens Parade, although predominantly single or two-storey, has accommodated apartment or townhouse development of up to four-storeys in recent years. The current zoning will limit future infill or rear development to two or three-storeys in height and the application of Clause 22.02 of the Yarra Planning Scheme will ensure the future character of this area remains predominantly low-scale.

The St John the Baptist Church complex and the Victorian-era houses to the west are unlikely to undergo significant change as there is only limited opportunity for redevelopment (with the exception of the 'not-contributory' vacant site on the grounds of St John the Baptist Church at 55-57 Queens Parade). The existing GRZ and NRZ zoning will ensure the future character of these properties remains predominantly low-scale and the prominence of the Church complex is maintained. Clause 22.02 will appropriately manage the infill development on the vacant site and rear additions to the existing buildings.

8.7 Recommended Built Form Parameters

Sub-precinct 3A

A DDO should apply to sub-precinct 3A to inform the redevelopment of this commercial strip both in terms of new development and rear additions behind graded buildings. The DDO should moderate new built form along Queens Parade to protect key views of the belfry and spire of St John the Baptist Church, ensure set back of new built form from the intact heritage street wall along Smith Street, and avoid new rear development dominating the single-storey Victorian-era streetscape on Hodgkinson Street.

This should occur through controls for the sub-precinct that:

- Adopts a street wall height and setback for infill development that protects the views of the belfry and spire of St John the Baptist Church along Queens Parade from the south-west corner of Smith Street, allowing the belfry and spire to be appreciated with clear sky around these features.
- Adopts a street wall height along Smith Street that respects the existing façade heights of 652-662 Smith Street.
- Ensures that new development does not visually dominate the existing heritage fabric and that heritage buildings – particularly those on Smith Street - retain their three-dimensional form as viewed from the public realm to avoid 'facadism'. This will require new upper level built form to be set back from the heritage façades.
- Ensures that any upper level or infill development adjacent to the heritage buildings are subservient to the heritage fabric and are visually recessive in mass, scale and materiality.
- Recognises the low-scale, fine grain heritage form of existing surrounding development and ensure appropriate transitions to the one to two-storey adjacent heritage fabric. New development should not dominate views to the northwest over the rooftops of the single-storey houses on Hodgkinson Street.
- Ensures new development does not diminish, or detract from the heritage values of the boulevard streetscape of Queens Parade.

Sub-precinct 3B

Sub-precinct 3B is either zoned GRZ or NRZ and therefore additional built form controls are not required to protect the heritage values of these areas.

9. Precinct 4 – Neighbourhood Activity Centre Precinct

9.1 Description

Precinct 4 contains the core of the retail high street. On the north side of Queens Parade it extends between Delbridge Street and number 410 Queens Parade to the depth of a single property, including the properties known as 304, 312 and 390A Queens Parade at the rear of the buildings fronting Queens Parade. On the south side of Queens Parade, Precinct 4 extends from Wellington Street to Turnbull Street to the depth of a single property (but also including 245 Gold Street near the corner of Queens Parade and Gold Street).

The precinct is characterised by highly intact, predominantly two-storey nineteenth and early twentieth century commercial buildings, with a single three-storey building (the former ANZ Bank, 370 Queens Parade, VHR H0892).

The whole of Precinct 4 falls within the extent of the Neighbourhood Activity Centre and is subject to the Heritage Overlay.

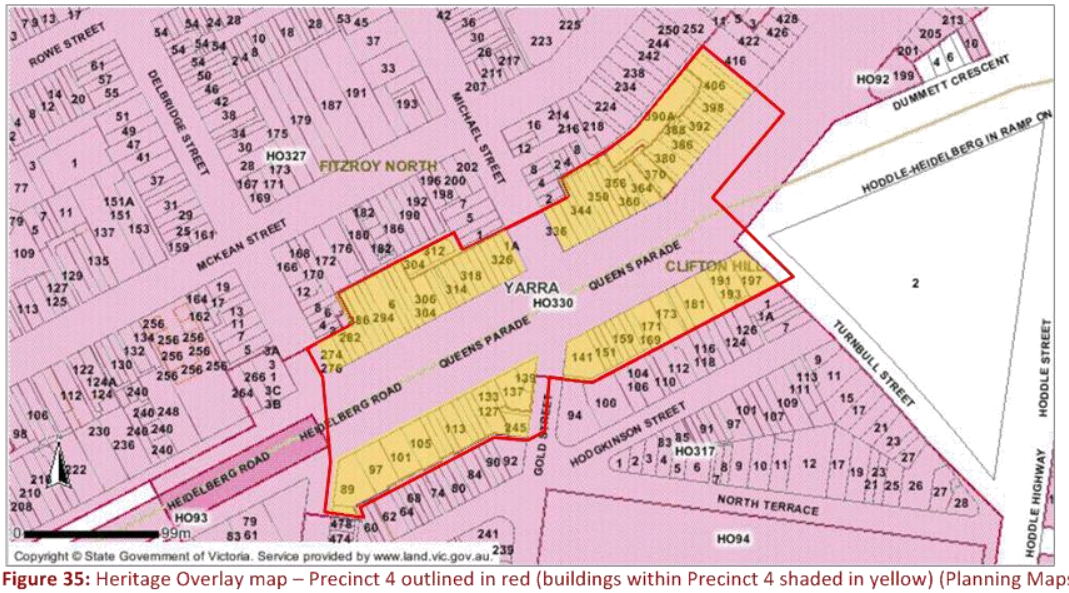

Figure 35: Heritage Overlay map – Precinct 4 outlined in red (buildings within Precinct 4 shaded in yellow) (Planning Maps Online, accessed 29 August 2017)

Figure 36: South side of Queens Parade

Figure 37: North side of Queens Parade

9.2 Heritage Status

9.2.1 Existing Conditions

Precinct 4 is predominantly made up of intact rows of two-storey shop-residences dating from the late nineteenth and early twentieth century with a small number of later single and two-storey infill buildings. The three-storey former ANZ Bank at 370 Queens Parade is a local landmark and is included on the VHR (H0892).

The buildings along Queens Parade within the precinct generally share a similar parapeted building form with zero street setback. They are of rendered brick construction with roofs that are generally hidden but chimneys are often visible above the decorative parapets. The parapet heights of the consistent rows of two-storey shop and residences – such as 149-167 Queens Street (Campi's Buildings) (Figure 38) and 344-356 Queens Parade (Figure 39) – establish a consistent street wall height of 8m-11m. The roof forms of the front two rooms of the nineteenth century shops and residences are generally between 8m and 12m deep. The chimneys common to most party walls and visible from the public realm are generally located approximately 2-3m from the building's façade, that is, about half the depth of the front room of the building.

Grander commercial buildings such as former hotels, a bank and post office, generally occupy the intersections of Queens Parade and other streets within Precinct 4. The facades of these buildings at the intersection of Gold Street and Queens Parade continue on the side street, as do those on the buildings on the eastern side of the intersection of Delbridge, Michael, and Wellington Streets. The building on the western side of the intersection of Michael Street and Queens Parade has a plain elevation to Michael Street with no return façade (Figure 42). The intersection of Turnball Street and Queens Parade is not occupied by a building.

The south side of Queens Parade contains some atypical heritage buildings that do not share the predominant two-storey parapeted form built to the street boundary. These buildings include the 'individually significant' Edwardian-era Doctor's Surgery & Residence at 105 Queens Street, the 'contributory' Clifton Hill Post Office at 181 Queens Parade, and the 'contributory' houses at 191 and 193 Queens Parade.

The whole of Precinct 4 is included within the HO330 - Queens Parade Precinct, North Fitzroy/Clifton Hill with the exception of 390A Queens Parade and the rear parts of 304, 312 and 316 Queens Parade, which are included within HO327 – North Fitzroy Precinct (with no identified grading in Appendix 8). These buildings are a variety of utilitarian brick and timber structures including remnants of an early former stables, garages of more recent origins and a substation that possibly previously functioned as a warehouse. 245 Gold Street also sits outside HO330 and is a 'contributory' building within HO317 – Clifton Hill Western Precinct.

A very high proportion of the buildings within Precinct 4 are identified in Appendix 8 as 'contributory' or 'individually significant'.

The former ANZ Bank, an impressive example of English Queen Anne Revival architecture designed by Oakden, Addison and Kemp, occupies a prominent location at the point at which Queens Parade bends into Heidelberg Road. It is of State-level heritage significance for the following reasons:

> *The ANZ Bank building in Queens Parade Fitzroy North is of architectural significance as one of the best examples of English Queen Anne Revival style commercial architecture in Melbourne. The building is prominent in the Queens Parade/Heidelberg Road intersection and its importance is emphasized by the corner tower, steep pitched roof and tall chimneys. The decorative brick and render bands give contrast to the precise red brick work. The building's architect, Henry Kemp, was one of Melbourne's best exponents of the style. Kemp was also responsible for the Australian Building in Elizabeth Street, Melbourne, which, when built around the same time as the ANZ Bank building, was reputed to be Australia's tallest building. The demolition of the Australian Building in 1980 gives the ANZ Bank building added significance, given the latter appears to have been a scaled down version of the former. The North Fitzroy bank is the only metropolitan example of Queen Anne style bank buildings in Victoria, the other seven being country buildings. The influence of the important English architect, Richard Shaw, the leading architect of the Queen Anne Revival style in London, is clearly evident in the ANZ Bank building.*

The ANZ Bank building in Queens Parade, Fitzroy North is of historical and social significance for what it reveals about the banking industry in late 19th century Melbourne. The importance of the bank in the local community was emphasised by its prominent street corner location and its distinctive and attractive architecture. The manager's residence above the banking chamber is a reminder of the different organisational and working arrangements that characterised 19th century cities. The building continues to have social significance due to its landmark qualities.

HO93 - Queens Parade, Between Alexandra Parade & Delbridge Street Clifton Hill/ North Fitzroy - Street Trees does not extend into Precinct 4, however the carriageway retains a boulevard character albeit with less mature specimens of trees and less consistency of planting.

In summary, the existing heritage status for Precinct 4 is:

Victorian Heritage Register
VHR H0892 – Former ANZ Bank, 370-374 Queens Parade, North Fitzroy (HO221)

Individual Heritage Overlays
None

Precinct Heritage Overlays
HO327 – North Fitzroy Precinct
HO330 – Queens Parade Precinct, North Fitzroy/Clifton Hill
HO317 – Clifton Hill Western Precinct

9.2.2 Recommended Changes

The following changes to the heritage controls are recommended for Precinct 4:

- 350 Queens Parade (Figure 39) is part of a highly intact row of shops and should be identified as 'contributory' in Appendix 8 (currently the neighbouring properties at 346-348 & 352-356 are all identified as 'contributory') (see Appendix G for further information).
- The 'individually significant' Doctor's Surgery & Residence at 105 Queens Street is an atypical heritage building in the context of HO330. A site-specific Statement of Significance has been prepared to assist in guiding appropriate management of the site (see Appendix E). The citation should be included within the *Yarra High Streets: Statements of Significance* reference document at Clause 22.02 of the Yarra Planning Scheme (Appendix H).
- 137 Queens Parade (Figure 41) is a vacant site following fire damage to the building. It is currently not graded in Appendix 8 but should be identified as 'not contributory'.
- 390A Queens Parade and the rear parts of 304, 312 and 316 Queens Parade (Figures 44-51) contain a variety of utilitarian structures including former stables, garages and a substation. The contribution these structures make to HO327 has been reviewed as part of this project (see Appendix G for a detailed analysis). As a result of this review it is recommended that:
  (i) The properties at 390A and the rear of 304, 312, 316 and 380 be removed from HO327 – Fitzroy North Precinct and include within the boundary of HO330 – Queens Parade Precinct.
  (ii) The entry in Appendix 8 for 'Shops & residences' at '380-378 Queens Parade' within HO327 – North Fitzroy Precinct be removed and that 380 Queens Parade be identified as a 'contributory' building within HO330 – Queens Parade Precinct.
  (iii) The following sites and associated gradings be indentified in Appendix 8 under HO330 – Queens Parade Precinct:
    - 304 Queens Parade (rear, off Koormiel Lane) – 'not contributory'
    - 312 Queens Parade (rear, off Koormiel Lane) – 'contributory'
    - 316 Queens Parade (rear, off Koormiel Lane) – 'not contributory'
    - 380 Queens Parade (rear, behind 360-378) – 'not contributory'
    - 390A Queens Parade (two-storey building in north-east corner) – 'contributory'
    - 390A Queens Parade (all buildings excluding two-storey building) – 'not contributory'

Figure 38: 149-167 Queens Parade (Campi's Buildings) (©Google)

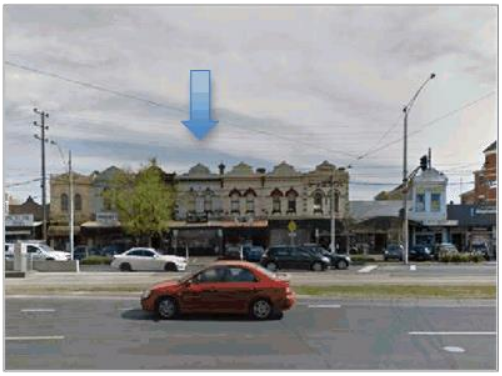

Figure 39: 342-364 Queens Parade – no. 350 indicated by arrow (©Google)

Figure 40: 400-410 Queens Parade

Figure 41: 139 (left), 137 (centre) and 133 (right) Queens Parade

Figure 42: 330 Queens Parade (from Michael Street) (©Google)

Figure 43: 245 Gold Street, Clifton Hill (©Google)

Figure 44: The southern elevations of the outbuildings at (left to right) 304, 312 & 316 (rear of all) Queens Parade

Figure 45: The northern elevations of the outbuildings at (right to left) 304, 312 & 316 (rear) Queens Parade

Figure 46: The western elevation of the outbuilding at 304 (rear) Queens Parade

Figure 47: The southern elevation of the outbuilding at 312 (rear) Queens Parade

Figure 48: The south-west elevation of the outbuilding at 390A Queens Parade (painted red). In the foreground is the vacant lot at 380 (rear) Queens Parade

Figure 49: The south-east elevations of the outbuildings at 390A Queens Parade

Figure 50: The north-east elevations of the outbuildings at 390A Queens Parade

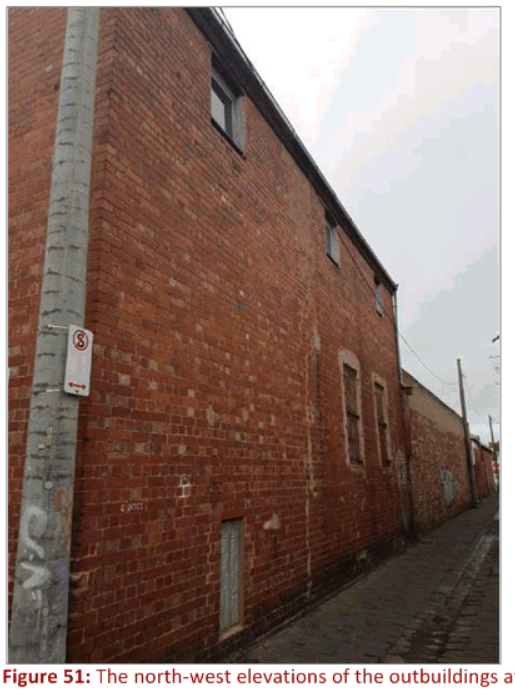

Figure 51: The north-west elevations of the outbuildings at 390A Queens Parade

9.3 Zoning

9.3.1 Existing Conditions

All of the properties within Precinct 4 are zoned C1Z which is appropriate for the historic and current use of this area, with the exception of 245 Gold Street, Clifton Hill (Figure 43). This single-storey late-Victorian residence is highly intact from the street and has been included in C1Z but its character and historic use is more aligned to a residential zone (GRZ or NRZ).

9.3.2 Recommended Changes

Consideration should be given to rezoning 245 Gold Street, Clifton Hill from C1Z to GRZ or NRZ.

9.4 Key views

Some of the key views of the belfry and spire of St John the Baptist Church are from within Precinct 4 and from Raines Reserve (including at the pedestrian crossing) looking southwest, through Precinct 4. Views to the local landmark of the state-significant former ANZ Bank at 370 Queens Parade, are also afforded from within Precinct 4, from Raines Reserve, and from Precinct 5.

The boundary of Precincts 3 and 4 at the intersection of Queens Parade and Delbridge Street provides a key view of St John's Church and spire (see 'Key Views' discussion for Precinct 3). Other key views occur from Raines Reserve within the fork of Queens Parade and Heidelberg Road. This view is protected by the road reserve and the street wall heights of the heritage buildings and do not require management through additional built form controls.

Key views of the former ANZ Bank are those where the three-storey form, roof and chimneys of the former ANZ Bank building becomes a prominent landmark within the streetscape. These views are provided from the south-eastern intersection of Queens Parade and Gold Street; from the south-western intersection of Queens Parade and Turnbull Street; from the western end of in Raines Reserve within Precinct 5; and from the

pedestrian traffic islands at the southwest end of Precinct 5 (Figure 56). Additional built form controls are required to protect the landmark quality and visual prominence of the upper floor, roof form and chimneys of the former ANZ Bank when viewed from the intersection with Gold Street (Figure 53) and from Raines Reserve (Figure 55). The views from the intersection with Turnbull Street and the traffic island (Figure 54) is protected by the road reserve.

Figure 52: Location of key views (blue arrows) of the former ANZ Bank (red star) (©nearmap, 17 Sept 2017)

Oblique views of the former United Kingdom Hotel (VHR H0892) are provided from the northern end of Precinct 4, but these are protected by the road reserve and do not require management through additional built form controls.

Glimpses of the Alexandra Parade Shot Tower are visible from the intersection of Queens Parade and Delbridge and Gold Streets, however these are not considered to be key views of the shot tower that should be protected or require additional built form controls.

Figure 53: Former ANZ Bank from the intersection of Queens Parade and Gold Street

Figure 54: Former ANZ Bank from the intersection of Queens Parade and Turnbull Street (©Google)

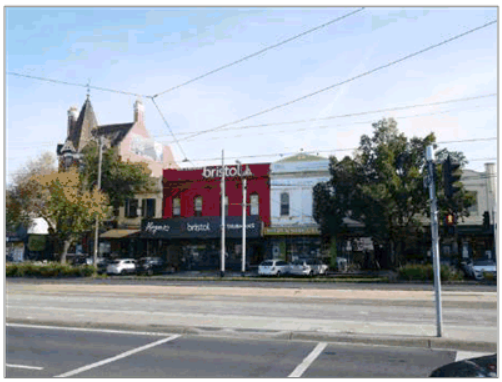

Figure 55: Former ANZ Bank from Raines Reserve

Figure 56: View of the belfry and spire of St John's Church from the traffic island

9.5 Significant Streetscapes

The following lengths of Queens Parade within Precinct are considered to have 'significant' streetscape value:

- The northern side of Queens Parade between Delbridge Street and Michael Street.
- The northern side of Queens Parade between Michael Street and number 410 Queens Parade.
- The southern side of Queens Parade between Wellington Street and Gold Street.
- The southern side of Queens Parade between Gold Street and number 167 Queens Parade.

These streetscapes:

- Consist predominantly of buildings graded 'contributory' or 'individually significant' in Appendix 8 (approximately 90%) with few 'not-contributory' buildings or intrusive developments.
- Have a high degree of consistency in terms of their:
  - Overall scale
  - Zero street setback
  - Predominantly (Victorian) two-storey street wall height
  - Predominantly Victorian and early Edwardian architectural form and style.
- Date predominantly from the Victorian and early Edwardian period with few later infill developments.
- Demonstrate a high level of intactness to their turn of the century period of construction.
- Are recognised for their aesthetic significance in the existing Statement of Significance for HO330 as follows:

> *For the late 19th and early 20th century shops, hotels, banks, and public buildings, often with intact street verandahs, lining both sides of a wide boulevard that provides for exceptional vistas to fine commercial rows, and demonstrates the major role of Queens Parade as a commercial, banking, accommodation and public transport nucleus for the district.*

As a result, these streetscapes are considered to satisfy the test for being 'significant streetscapes'. It is recommended that the Statement of Significance for HO330 be updated to ensure the intactness and quality of these identified streetscapes is appropriately recognised and the elements of the streetscape that contribute to its significance are clearly defined (see updated Statement of Significance in Appendix H).

The setbacks and form of the buildings from 169 Queens Parade to Turnbull Street (197 Queens Parade) are less consistent and there is a much higher proportion (approx. 45%) of 'not contributory' graded buildings. This section of the Queens Parade streetscape is therefore not considered significant.

While HO93 – Queens Parade does not extend into Precinct 4, the Statement of Significance for HO330 captures the significance of the boulevard as follows under 'Why is it significant?':

As one of Melbourne's early surveyed boulevards (1853) and one of the few associated with the prominent surveyor Robert Hoddle, enhanced by mature street trees (elms) and extensive beautification in the inter-war period in the form of plane trees and rockeries.

New development should be scaled to avoid diminishing the boulevard quality of Queens Parade.

Queens Parade
Queens Parade
Queens Parade

(Victorian-era) storeys. Single-storey development should be discouraged. Infill facades should respect the materiality and relationship between solid and void established by the 'contributory' and 'individually significant' buildings.

Figure 58: 97 Queens Parade

Figure 59: 141-147 Queens Parade

Figure 60: 171-175 Queens Parade

Figure 61: 197 Queens Parade (left)

Figure 62: 300-306 Queens Parade

Figure 63: 324-328 Queens Parade

Figure 64: 360 (left) and 364-366 (centre right) Queens

Figure 65: 408 Queens Parade (centre) (©Google)

Any new upper-level development should be set back from the street wall to retain the low-scale, neighbourhood character of the commercial strip and to retain the prominence of the heritage fabric in the streetscape. New upper-level development should be designed so as not to dominate the decorative façades of the Victorian-era buildings when viewed from the opposite side of Queens Parade. New development should also provide a sensitive transition to the predominantly single-storey Victorian-era character of Hodgkinson Street to the south and the one and two-storey Victorian-era character of McKean Street to the north.

Any new built form adjacent to the former ANZ Bank should be massed to retain key views to the upper level, highly decorative and prominent roof form and chimneys of the former bank.

9.7 Recommended Built Form Parameters

A DDO should apply to Precinct 4 to inform development within the Queens Parade Neighbourhood Activity Centre both in terms of new infill development and rear additions behind heritage buildings. The DDO should ensure new built form protects the strong two-storey character of the highly intact Queens Parade streetscape, to protect views to the local landmark of the former ANZ Bank building, and to avoid new rear development dominating the single-storey streetscapes on Hodgkinson Street and the single and two-storey streetscapes on McKean Street through controls which:

- Adopts a street wall height for infill development that reflects the established two (Victorian-era) storey scale of the precinct and discourages single-storey infill development.
- Ensures zero setback from the Queens Parade boundary for infill development.
- Adopts a street wall height and upper-level setbacks for infill development that protects the key views to the former ANZ Bank, 370 Queens Parade, from the south-west and north-east, in particular to the upper floor, roof form and chimneys.
- Ensures that the heritage buildings remain prominent within the Queens Parade streetscape and retain their three-dimensional form as viewed from the public realm to avoid 'facadism'. This will require new upper-level development to be set back from the street wall and for redevelopment to respect the existing inter-floor heights of the heritage fabric.
- Retains the visual prominence of the return façades of buildings that address both Queens Parade and Delbridge, Gold, and Michael Streets.
- Requires the retention of chimneys visible from the public realm.
- Ensures that new development does not visually dominate the existing heritage fabric by requiring that new upper level built form is no greater than the volume of the heritage façade when the site is viewed from the opposite side of the Queens Parade boulevard.
- Ensures that any upper level or infill development is subservient to the heritage fabric and is visually recessive in mass, scale and materiality.

- Ensures new development does not diminish, or detract from the heritage values of the boulevard streetscape of Queens Parade.
- Recognises the low-scale, fine grain heritage form of existing surrounding development and ensures appropriate transitions to the one to two-storey adjacent heritage fabric. New development should not dominate views over the rooftops of the single and two-storey houses on Hodgkinson and McKean Streets.

10. Precinct 5 - North Eastern Precinct

10.1 Description

The North Eastern Precinct is not contiguous with the rest of the main retail area (Precinct 4) and is a triangular shaped block bound by Raines Reserve (originally named Rain's Reserve) to the west, Queens Parade to the north, Dummett Crescent/Heidelberg Road on-ramp to the south, and Hoddle Street to the east. Dummett Crescent loops to the north to bisect the precinct. Precinct 5 is entirely commercial or industrial in its current use and is an island of land encircled by major thoroughfares.

Precinct 5 is made up of two sub precincts:

- Sub-precinct 5A – comprises the two-storey former United Kingdom Hotel (VHR H0892) and associated land (see Figure 67 & 68).
- Sub-precinct 5B – is made up of the land between the former United Kingdom Hotel and Dummett Crescent and is occupied by single-storey commercial buildings including the former Clifton Motors building at 205-211 Queens Parade, which is currently being considered for inclusion in the VHR (Figure 69). A single heavily altered Edwardian-era house is located at 4 Dummett Crescent (Figure 71) is the only domestic-scale building with the precinct.
- Sub-precinct 5C – is made up of land northeast of Dummett Crescent and comprises single-storey warehouse/factory buildings or land currently under redevelopment (Figure 70). A c.1980s office building is located at 501-513 Hoddle Street.

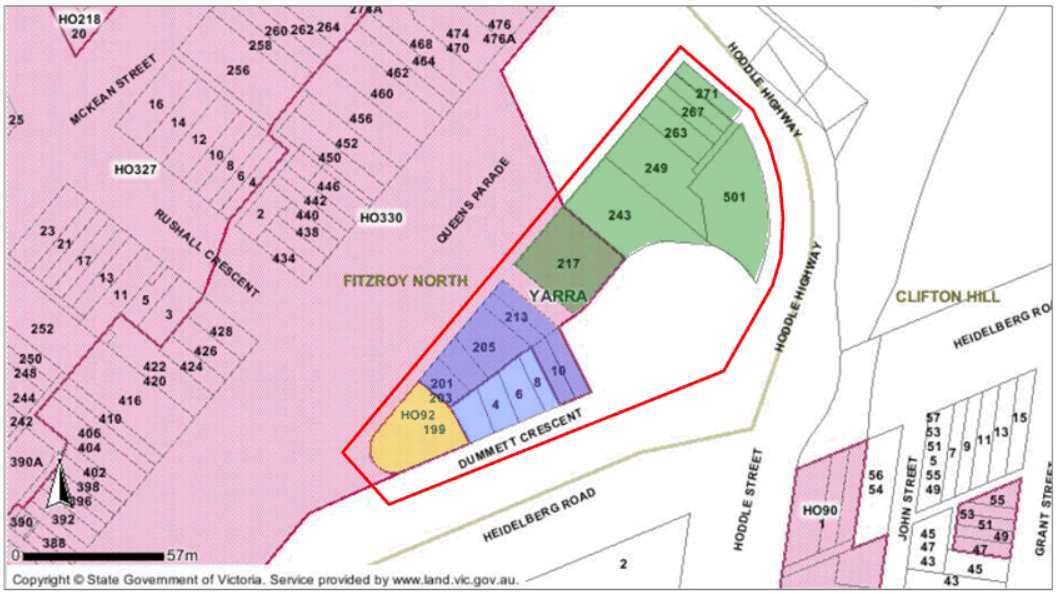

Figure 66: Heritage Overlay map – Precinct 5 outlined in red (sub-precinct 5A shaded in yellow, sub-precinct 5B in blue and sub-precinct 5C) (Planning Maps Online, accessed 30 August 2017)

Figure 67: Former United Kingdom Hotel from Queens Parade (sub-precinct 5A)

Figure 68: Former United Kingdom Hotel from Heidelberg Road (©Google) (sub-precinct 5A)

Figure 69: 199-213 Queens Parade (sub-precincts 5A & 5B)

Figure 70: 241-241 Queens Parade (sub-precinct 5C)

Figure 71: 4 Dummett Crescent (sub-precinct 5B) (©Google)

10.2 Heritage Status

10.2.1 Existing Conditions

Sub-precinct 5A

Sub-precinct 5A contains the two-storey Moderne former United Kingdom Hotel (now MacDonald's), at 199 Queens Parade (HO92), which is included on the VHR (H0892). The building was constructed in 1937-38, replacing a hotel of the same name that occupied the site. This two-storey brick building has cantilevered

concrete balconies with slim metal railings and light cantilevered canopies that accentuate the rounded horizontal mass of the ground and first floors. Manganese, tapestry and cream bricks provide polychrome and textural contrast, and the massive vertical fins rising through the centre of the western facade are striking features.

The building is of State-level significance for the following reasons:

> *The Former United Kingdom Hotel is of architectural significance as possibly Victoria's most exquisite and intact example of the Jazz Moderne style of architecture designed by one of the most notable proponents of the style, J. H. Wardrop. The building is greatly enhanced by its near-island siting and is externally and internally almost original, with the exception of post-war rear additions. It provides an interesting three-dimensional comparison with Wardrop's slightly earlier Alkira House in Queen Street, Melbourne.*
>
> *The Former United Kingdom Hotel is of historical significance as an exemplar of the material culture of the late 1930s and of prevailing social customs. The survival of much of the hotel's interior provides important evidence of hotel design and usage in the pre-WW2 period.*

Sub-precinct 5B

Sub-precinct 5B (with the exception of 2-8 Dummett Street) falls within HO330 - Queens Parade Precinct, North Fitzroy/Clifton Hill. The former Clifton Motors Pty Ltd Building (later Bayford Motors), 205-211 Queens Parade with its elaborate single-storey Moderne facade and dominant fin over the central bay is identified as 'individually significant' in Appendix 8. It is currently being considered for inclusion in the VHR for the following reason:

> *The Former Clifton Motor Garage, designed by J H Wardrop is of architectural significance as a notable example of Moderne style architecture in Victoria. This is expressed through the striking 1938 street frontage to Queens Parade. Art Deco and Moderne style architecture became synonymous with car showrooms and garages, office buildings, cinemas and hotels in the 1920s and 1930s. The Former Clifton Motor Garage references the adjacent Former United Kingdom Hotel (VHR H0684), also designed by Wardrop through its scale, Moderne design and materials. The Former Clifton Motor Garage exhibits a streamlined, symmetrical design through the use of strong horizontal and vertical lines, cubic forms, vertical fins, and decorative brick and tile work. The Moderne frontage unifies the rear garage, showrooms and residence and conveys an image of the developing Australian motor car industry as modern, fast and forward thinking. [Criterion D]*

All other properties subject to HO330 within sub-precinct 5B are either vacant land or occupied by single-storey twentieth century commercial buildings or a heavily altered Edwardian-era former house and are all identified as 'not contributory' in Appendix 8.

Sub-precinct 5C

HO330 only extends over one property within sub-precinct 5C, 217 Queens Parade. This site, now being redeveloped as a 10-storey building, is identified as 'not contributory' within Appendix 8.

In summary, the existing heritage status for Precinct 5 is:

> **Victorian Heritage Register**
> VHR H0892 – Former United Kingdom Hotel (now McDonald's), 199 Queens Parade, Clifton Hill (HO92)
> A nomination for inclusion of the Former Clifton Motors Building is currently being considered.
>
> **Individual Heritage Overlays**
> None
>
> **Precinct Heritage Overlays**
> HO330 - Queens Parade Precinct, North Fitzroy/Clifton Hill

10.2.2 Recommended Changes

The following changes to the heritage controls are recommended for Precinct 5:

- The former Clifton Motors Pty Ltd Building, 205-211 Queens Parade is identified in Appendix 8 as an 'individually significant' building within HO330. Being located on the island site to the north of Queens Parade the building is isolated from the balance of buildings within HO330 and is surrounded by 'not contributory' buildings and sites. While the former Clifton Motors Building has very strong architectural similarities with the nearby Former United Kingdom Hotel, the hotel building is not within HO330 and is instead covered by its own Heritage Overlay by virtue of its state-heritage listing. Further, the former Clifton Motors Building is an atypical building within the identified significance of the Queens Parade Precinct and is considered to be of significance in its own right as an important Moderne example of a motor garage. For these reasons it is recommended that the former Clifton Motors building be removed from HO330 and included in its own site-specific Heritage Overlay (see Appendix F for a heritage citation and recommendations). It is further noted that if the place is included in the VHR it will automatically be included within its own Heritage Overlay and excluded from HO330. The heritage citation for the place should be included within the *Yarra High Streets: Statements of Significance* reference document at Clause 22.02 of the Yarra Planning Scheme (Appendix H) and Appendix 8 updated accordingly.
- With the exception of the former Clifton Motors building and the state-significant former United Kingdom Hotel, there are no other heritage buildings within Precinct 5. It is therefore recommended that the extent of HO330 be amended to exclude 201-217 Queens Parade and 10-12 Dummett Crescent from HO330. HO330 should be extended over the full extent of the historic layout of Raines Reserve (a contributory element within HO330) and should cover the historic Queens Parade boulevard to Hoddle Highway to enable heritage considerations to inform development of tram stops and other infrastructure and street furniture within the road reserve (see Appendix G for a more detailed analysis of the appropriate Heritage Overlay extent). The Statement of Significance for HO330 should be updated accordingly and included within the *Yarra High Streets: Statements of Significance* reference document at Clause 22.02 of the Yarra Planning Scheme (Appendix H). Appendix 8 should also be updated to reflect these changes.

10.3 Zoning

10.3.1 Existing Conditions

Precinct 5 is zoned MUZ.

10.3.2 Recommended Changes

The current zoning is considered appropriate for Precinct 5.

10.4 Key views

The former United Kingdom Hotel occupies a prominent location at the junction of Queens Parade and Drummett Crescent/Heidelberg Road and key views are provided of the hotel from these streets and from within Raines Reserve. These views are protected by the road reserve and public open space and do not require management through additional built form controls.

Figure 72: Former United Kingdom Hotel from Raines Reserve

10.5 Significant Streetscapes

The highly variable nature of Precinct 5 and the limited presence of heritage buildings means that there are no streetscapes that are considered to be significant.

While HO93 – Queens Parade does not extend into Precinct 5, the Statement of Significance for HO330 captures the significance of the boulevard as follows under 'Why is it significant?':

> *As one of Melbourne's early surveyed boulevards (1853) and one of the few associated with the prominent surveyor Robert Hoddle, enhanced by mature street trees (elms) and extensive beautification in the inter-war period in the form of plane trees and rockeries.*

New development should be scaled to avoid diminishing the boulevard quality of Queens Parade.

10.6 Potential Future Character Considerations

Sub-precinct 5A

Sub-precinct 5A contains the two-storey former United Kingdom Hotel and a vacant parcel of land to its rear on Dummett Crescent. New development on the vacant land should be respectful of the State-significant heritage building and should ensure its prominence is maintained when viewing the building from Queens Parade, Heidelberg Road and Raines Reserve.

Sub-precinct 5B

More intensive development can be accommodated within sub-precinct 5B as long as it does not visually dominate the potentially State-significant Moderne façade of 205-211 Queens Parade or the former United Kingdom Hotel in adjoining sub-precinct 5A. The street wall height of new development on 201-203 and 213 Queens Parade should not exceed the two-storey scale of the former United Kingdom Hotel and the Moderne façade of 205-211 Queens Parade. Higher development behind the retained front section of 205-211 Queens Parade and on adjoining properties should be set back to retain the integrity of the vertical fins (which extend back from the street boundary by approximately 4m) and allow this to be viewed as a prominent, standalone element within the streetscape.

Sub-precinct 5C

The lack of heritage buildings within sub-precinct 5C, and the island nature of the site mean that future development within this precinct is unlikely to adversely impact on heritage places.

10.7 Recommended Built Form Parameters

Sub-precinct 5A

A DDO should apply to sub-precinct 5A to inform the redevelopment of the vacant land – currently the McDonald's drive-through – at the rear of the former United Kingdom Hotel. The DDO should moderate new built form to protect the visual prominence of the former United Kingdom Hotel when viewed from Queens Parade, Heidelberg Road and Raines Reserve.

This should be achieved through built form controls that:

- Adopts a street wall height for new development adjacent to the former United Kingdom Hotel that respects the façade height of the building.
- Ensures that new development does not visually overwhelm the three-dimensional form of the State-significant former United Kingdom Hotel, particularly when it is viewed from Raines Reserve and on the north side of Queens Parade.

Sub-precinct 5B

A DDO should apply to sub-precinct 5B to inform the redevelopment of this sub-precinct. The DDO should moderate new built form to protect the visual prominence of the former United Kingdom Hotel in sub-precinct 5A and the potentially State-significant Moderne façade of the former Clifton Motors building when viewed along Queens Parade from the north-east and south-west and from the opposite side of the boulevard.

This should be achieved through built form controls that:

- Adopts a street wall height for new development adjacent to the former United Kingdom Hotel and the former Clifton Motors building that respects the façade heights of these buildings.
- Retains, conserves and incorporates the Moderne façade of the former Clifton Motors building into any redevelopment of the site. This includes ensuring that internal alterations respect the exterior form of the building and its fenestration pattern as well as ensuring the decorative vertical fin element remains prominent as a freestanding element when viewed from the public realm.
- Ensures that the aesthetically significant façade treatment of the former Clifton Motors building remains prominent and that the building retains its three-dimensional form as viewed from the public realm. This will require new upper-level development to be set back from the street wall.
- Ensures that new development does not visually overwhelm the State-significant former United Kingdom Hotel (particularly when it is viewed from Raines Reserve) or the potentially State-significant former Clifton Motors buildings (when viewed from the opposite side of Queens Parade).

Sub-precinct 5C

No built form controls are recommended on heritage grounds for sub-precinct 5C.

Section III – Built Form Testing and Recommendations

11. Built Form Testing

In order to translate the 'Recommended Built Form Parameters' in Part II into specific guidance that can be translated into a DDO control, Hansen Partnership prepared 3D computer modelling to test the appropriateness of particular built form outcomes that achieved the intent of the recommended parameters. Hansen Partnership has prepared details of the modelling approach and the methodology applied. These details are provided below.

11.1 Purpose and Scope of Built Form Testing

The Queens Parade Built Form Review Recommendations have been determined based on a comprehensive built form testing and a high-level 3D computer modelling of schematic development envelopes for the study area. It was established and operated as a 'working' massing model used to informally measure built form heights and setbacks to the properties along the length of the Queens Parade corridor (within the study area) to serve as a useful general tool in comparative visual analysis.

The development and use of such 3D massing models is common for strategic built form work of this kind to examine the general relationships between new urban form (various options or scenarios), topography and key views to existing landmarks from identified vantage points as advised by GJM Heritage.

3D massing models are commonly used in addition to more conventional 2D cross-sections (and other tools) when determining built form controls and assessing off-site impacts on surrounding land such as overshadowing and visual bulk.

The massing model utilised is 'fit for purpose' for a strategic corridor study of its kind. It does not rely on detailed site survey data, rather more general available contour, landform and cadastral information.

The modelled images provided and the base information utilised in generation of the material are general in nature and 'fit for purpose' for a strategic urban design study of a corridor of this kind. Such modelling has been reliably applied in other strategic work across Metropolitan Melbourne and presented to PPV and other Authorities.

11.2 Massing Model

The massing model is prepared in SketchUp Pro (2016 and 2017) and utilises automatic Google terrain as the basis for topography and basic aerial imagery (via 'Geo-Location' command).

The existing building envelopes surrounding the site are depicted as massing representations only and do not seek to represent existing building detail, in terms of architectural form or appearance – but rather overall scale.

The existing 'landmark' building envelopes have been modelled based on 'Google Earth' – Street View tests to determine their overall scale. These included:

- St John the Baptist Church
- Former ANZ Bank building
- Former United Kingdom Hotel
- Former Clifton Motors Building

Within the study area (areas identified for moderate, high and substantial change), the development envelope has been represented in 3-dimensions based on the site and cadastral information available from the City of Yarra and DataVIC with an assumption of 100% site coverage.

Further, a number of recent development envelopes (approved, unbuilt and/or under construction) have also been modelled to represent their overall scales and setbacks, based on endorsed architectural plans provided by the City of Yarra.

We acknowledge that the basis of the model (i.e. site boundaries and levels) is not as accurate as one generated with a detailed site survey.

For the purposes of general assessment in a broad corridor context, the use of readily available landform and development envelope information assists in examining the overall extent of compliance with or departure from (broadly represented) relevant built form controls.

Development massing for contributory and significant heritage sites has been modelled based on the following measurements:

- 11m 'street wall' (as a typical measurement for 2-storey Victorian-era parapet).
- 3.5m floor to floor height for upper levels above the 'street wall'.

Development massing for non-contributory, or non-heritage sites has been modelled based on the following measurements:

- 4.0m floor to floor height for ground level and level 1.
- 3.5m floor to floor height for level 1 and above.

11.3 Eye Level Views

Eye level view representations are approximate only. They are based on locating the 'camera' within the SketchUp Pro model and positioning it 1.7m above the Google terrain, with no tilt. The camera focal length chosen is either 50mm or 20mm (35mm equivalent), depending on the required camera position in relation to the subject.

These eye-level views are not (nor have they claimed to be) 'photomontages' prepared according to the required VCAT methodology. They do not represent a detailed representation of the proposal in terms of architectural expression, materials and finishes or landscape.

11.4 Permit Application Review

During the course of the project a number of permit applications within the Study Area were lodged with Council and were reviewed to consider the appropriateness of the built form response to the heritage fabric of the place and the precinct. These permit applications were for the development of:

101 Queens Parade
141-147 Queens Parade
193 Queens Parade
304-308 Queens Parade
388-390 Queens Parade

Having considered the applications in light of heritage analysis work undertaken, the following conclusions have been reached:

- The new street wall height should fall within the predominant height of the existing street wall (i.e. between 8m and 11m).
- New upper level development needs to be set back a minimum of 6m behind the street wall to retain the prominence of the streetscape and the legibility of the retained heritage building as having a three dimensional form.
- Preferred overall building heights combined with appropriate setbacks can achieve an appropriate built form outcome in heritage terms.
- Given the 60m width of Queens Parade, a ratio of 1:1 of new built form to street wall will generally not dominate or visually overwhelm the heritage fabric.
- Development on Queens Parade may affect the character and setting of residential properties that back onto the commercially zoned land and this interface requires management.

12. Built Form Recommendations

Having regard to the Built Form Testing utilising modelling prepared by Hansen Partnership, we recommend the following built form controls be applied to ensure an appropriate balance is struck between new development and the retention of heritage values within the study area.

12.1 Precinct 1

Built Form Element	Requirement	Rationale
Sub-precinct 1A		
No controls recommended	-	-
Sub-precinct 1B		
Facade height (heritage buildings)	Retain existing	The 'contributory' and 'individually significant' status of heritage places within the sub-precinct will ensure heritage facades are retained.
Facade height (infill development)	Match the parapet height of adjacent taller heritage building (preferred)	To ensure new built form responds to the heritage context. A discretionary control is appropriate given the variation in heights within the sub-precinct.
Street wall setback (heritage buildings)	Retain existing	The 'contributory' and 'individually significant' status of heritage places within the sub-precinct will ensure existing setbacks are retained.
Street wall setback (infill development)	Match the setback of adjacent heritage building with the lesser setback (preferred)	To ensure new built form responds to the heritage context. A discretionary control is appropriate given the variation in street wall setbacks within the sub-precinct.
Upper level setbacks (excluding the 'individually significant' building at 460 Brunswick St)	Minimum 6m (preferred)	A 6m setback will ensure new development is generally set back behind the main roof ridgeline or hip of residential heritage buildings within the sub-precinct, consistent with Council's heritage policy at Clause 22.02. It will also be sufficient to ensure that existing chimneys are retained. A discretionary approach is required to ensure an appropriate response can be achieved for each place given the variety of roof forms and the variations in height in the sub-precinct.
Upper level setback (460 Brunswick St)	Minimum 5m (mandatory)	This 'individually significant' heritage place is located prominently at the corner of Brunswick Street and Alexandra Parade/Queens Parade and marks the entrance to Fitzroy North via both Brunswick Street and Queens Parade. Given the width of Alexandra and Queens Parade, any new built form above this building will be highly visible. To ensure the volume and detail of the heritage building remains a prominent feature in the streetscape, new built form should be setback a mandatory minimum of 5m from both street frontages. A 5m setback will place new built form just behind the existing roof ridgeline and will allow for the retention of chimneys.
Building height (excluding the 'individually	9m (preferred)	Given the predominantly single-storey residential nature of this sub-precinct, it is considered that the height of new development at the rear of these buildings should generally

significant' building at 460 Brunswick St)		accord with the NRZ to adequately protect the heritage features and qualities of the area. It is therefore considered that a discretionary height control of 9m should be applied. We note that the two-storey buildings on Queens Parade may be able to achieve new rear development that exceeds 9m while meeting the desired setback requirements, therefore a discretionary control is considered appropriate.
Building height (460 Brunswick St)	18m (preferred), which equates to a 2:1 heritage street wall to new built form when viewed from the western side of Brunswick Street	Ensures that new development does not visually dominate the existing heritage fabric by requiring that new upper level built form is no greater than one third of the volume of the heritage façade when the site is viewed from the opposite side of Brunswick Street.

12.2 Precinct 2

Built Form Element	Requirement	Rationale
Sub-precinct 2A		
Street wall height (Art Deco facade)	Retain existing	The Art Deco façade, specifically identified as being of aesthetic significance in the Statement of Significance for the place, must be retained.
Street wall height (infill development)	Match the height of the top of the parapet at the former factory entrance (preferred)	The Art Deco façade is a prominent and significant decorative element within the streetscape. Adjacent built form should ensure the façade's decorative features remain prominent. It is recommended that the street wall height for infill development on the site be no higher than the top of the existing parapet at the former factory entrance (approximately 11m). However, given the variety of heights evident along the heritage parapet, an alternative infill street wall height may create an appropriate transition while ensuring the façade remains prominent, therefore a discretionary control is recommended.
Street wall setback (Art Deco facade)	Retain existing	The Art Deco façade, specifically identified as being of aesthetic significance in the Statement of Significance for the place, must be retained.
Street wall setback (infill development)	Match existing (preferred)	To ensure new infill built form responds to the heritage context. A discretionary control is appropriate as some street wall setback from the Art Deco façade may assist in ensuring the façade remains prominent in the streetscape.
Upper level setback (behind Art Deco facade)	Minimum 8m (mandatory)	The Art Deco façade is a highly decorative, aesthetically significant element within the streetscape and a mandatory upper level setback is required to ensure its prominence and elaborate architectural form is retained. An upper level setback of new built form is also necessary to ensure the three-dimensional form of the heritage building is retained and the building is not reduced to a façade. To this end, it is considered appropriate to draw on the existing fabric of the building to inform adequate setbacks. The structural bays of the factory building with the saw-tooth roof form at the western end of the site have a depth of approximately 8m. It is therefore

		considered that a mandatory 8m setback is appropriate to achieve a positive heritage outcome.
Upper level setbacks (infill development)	Minimum 5m (preferred)	A 5m upper level setback from the street wall of new infill development will ensure the Art Deco façade remains the prominent element within the streetscape and that the three-dimensional form of the former factory building retains legibility.
Building height (behind Art Deco facade)	1:1 heritage street wall to new built form (preferred)	Ensures that new development does not visually dominate the existing heritage street wall by requiring that new upper level built form is no greater than the volume of the heritage façade when the site is viewed from the opposite side of the Queens Parade boulevard.
Residential heritage precinct interface	Generally in accordance with ResCode Standard B17	To ensure an appropriate transition to the intact low-scale single-storey heritage character of Coleman Street and Newry Street.
Sub-precinct 2B		
No controls recommended	-	-
Sub-precinct 2C		
Street wall height (infill development immediately adjacent to Elizabeth Terrace on Napier Street and Alexandra Parade)	Match the parapet height of adjacent heritage building (preferred)	To ensure new built form responds to the heritage context. A discretionary control is appropriate to allow for a transition of street wall heights from the heritage fabric towards the north (Queens Parade) and east (along Alexandra Parade).
Street wall setback (infill development immediately adjacent to Elizabeth Terrace, Napier Street)	Match existing (preferred)	To ensure new infill built form responds to the heritage context. A discretionary control is appropriate as some street wall setback from the heritage terraces may assist in ensuring these buildings remain prominent in the streetscape.
Upper level setbacks (infill development immediately adjacent to Elizabeth Terrace, Napier Street and Alexandra Parade)	Minimum 6m (preferred)	A 6m upper level setback from the street wall of new infill development will ensure the heritage terrace form remains the prominent element within the streetscape. A discretionary control is appropriate to allow for a transition of upper level setbacks from the heritage fabric towards the north (Queens Parade) and east (along Alexandra Parade).
Building Height (immediate rear of Elizabeth Terrace, Napier Street)	Generally in accordance with ResCode Standard B17	To ensure Elizabeth Terrace remains prominent within the streetscape when viewed from the opposite side of Napier Street a 45 degree angle building envelope created above 11m (3 storeys) is appropriate to provide a transition to higher built form in the balance of the sub-precinct and ensure that new built form does not dominate the heritage fabric.
Sub-precinct 2D		
Facade height	Retain existing	Heritage facades must be retained. New development to the

		north of 472 Napier Street should be no higher than the existing parapet height of the heritage buildings.
Street wall setback	Retain existing	Heritage facades must be retained. New development to the north of 472 Napier Street should adopt the same setback as the adjacent heritage buildings.
Upper level setbacks	Minimum 6m (mandatory)	A 6m setback will ensure new development is set back behind the main roof ridgeline of the heritage buildings comprising Elizabeth Terrace and will ensure existing chimneys are retained. A mandatory minimum upper level setback of 6m is considered appropriate to ensure the volume and detail of the heritage terraces remains the prominent feature in the streetscape.
Building height	18m (preferred)	Application of the site line diagram in Figure 3 of Clause 22.02 (which guides the location and height of new built form behind heritage buildings in residential areas) results in a height of approximately 18m being able to be achieved behind a double-storey terraced house with a 6m setback. This is therefore considered to be a reasonable preferred height for the sub-precinct.

12.3 Precinct 3

Built Form Element	Requirement	Rationale
Sub-precinct 3A		
Street wall height (heritage buildings)	Retain existing	The 'contributory' and 'individually significant' status of heritage places within the sub-precinct will ensure heritage facades are retained.
Street wall height (infill development immediately adjacent to heritage buildings, excluding 15-33 Queens Parade)	Match the parapet height of adjacent taller heritage building (preferred)	To ensure new built form responds to the heritage context. A discretionary control is appropriate to achieve a transition to taller new built form where this can be achieved within the sub-precinct.
Street wall height (15-33 Queens Parade)	11m (mandatory)	A mandatory 11m street wall height is required for 15-33 Queens Parade to ensure key views of the St John the Baptist belfry and spire are maintained along the south side of Queens Parade from the southwest corner of Smith Street and Queens Parade.
Street wall setback (heritage buildings)	Retain existing	The 'contributory' and 'individually significant' status of heritage places within the sub-precinct will ensure existing setbacks are retained.
Street wall setback (infill development)	0m (preferred)	To ensure new built form responds to the heritage context, which overwhelmingly has a 0m street wall setback within the sub-precinct. A discretionary control is appropriate as some street wall setback from the heritage buildings within the sub-precinct may assist in ensuring these buildings remain prominent in the streetscape.

Upper level setbacks (heritage buildings, excluding 664 Smith Street and 35 & 37 Queens Parade)	Minimum 6m (preferred)	The main roof forms of the heritage buildings along Smith Street measure at approximately 11.5m deep, indicating a room depth of approximately 5-6m. Therefore, a preferred 6m upper level setback of new built form will ensure the heritage buildings remain prominent in the streetscape, will ensure that existing chimneys visible from the street are retained, and will avoid façadism by retaining a rooms depth of the heritage building. A discretionary approach is required to ensure an appropriate response can be achieved for each place in the sub-precinct given the variations in building height and roof forms.
Upper level setback (664 Smith Street)	Minimum 6m (mandatory)	To ensure the volume and detail of this 'individually significant' heritage place remains a prominent feature in the streetscape, new built form should be setback a mandatory minimum of 6m from the street frontage. A 6m mandatory minimum setback will avoid façadism by retaining a room's depth of the heritage building, ensuring the building can continue to be read as a former two-storey fire station.
Upper level setbacks (15-33, 35, 37 & 41 Queens Parade)	Minimum 6m (mandatory)	A mandatory 6m upper level setback from the street frontage is required for 15-41 Queens Parade to ensure key views of the St John the Baptist belfry and spire are maintained along the south side of Queens Parade when viewed from the south-west corner of Smith Street and Queens Parade. A mandatory 6m upper level setback will ensure that the belfry and spire remain prominent with clear sky separating these elements from any new built form.
Upper level setback (666 Smith Street)	Minimum 6m (preferred)	A 6m upper level setback from the street wall will ensure the adjoining 'individually significant' former fire station building at 664 Smith Street remains a prominent element within the streetscape. A discretionary control is appropriate to allow for a transition of upper level setbacks from the heritage fabric towards the north (Queens Parade).
Building height (heritage buildings)	1:1 heritage street wall to new built form from opposite side of Queens Parade 2:1 heritage street wall to new built form from opposite side of Smith Street (approximately 18m, 5 storeys)	Ensures that new development does not visually dominate the existing heritage street wall when the site is viewed from the opposite side of the Queens Parade boulevard or Smith Street (as relevant).
Residential heritage precinct interface	Generally in accordance with ResCode Standard B17.	To ensure an appropriate transition to the intact low-scale single-storey character of Hodgkinson Street and that new development does not dominate the scale of the heritage buildings.
Sub-precinct 3B		
No controls recommended	-	-

12.4 Precinct 4

Built Form Element	Requirement	Rationale
Precinct 4		
Street wall height (heritage buildings)	Retain existing	The 'contributory' and 'individually significant' status of heritage places within the precinct will ensure heritage facades are retained.
Street wall height (infill development)	Match the parapet height of adjacent taller heritage building (mandatory) Where there is no adjoining heritage fabric 8m minimum and 11m maximum (mandatory)	To maintain and reinforce the heritage values of the highly consistent two (Victorian) storey era street wall within the traditional Queens Parade 'High Street'. A mandatory control is appropriate given the highly consistent heritage street wall throughout the precinct.
Street wall setback (heritage buildings)	Retain existing	The 'contributory' and 'individually significant' status of heritage places within the precinct will ensure existing setbacks are retained.
Street wall setback (infill development)	0m (mandatory)	To maintain and reinforce the heritage values of the highly consistent traditional Queens Parade 'High Street' street wall. A mandatory control is appropriate given the highly consistent application of zero street wall setback throughout the precinct.
Upper level setbacks (excluding 364 and 169-197 Queens Parade)	Minimum 6m (mandatory)	To ensure the significant streetscape of the Queens Parade Activity Centre remains as a prominent and highly intact two-storey 'turn of the century' High Street, mandatory setbacks for upper-level new built form are required. The main roof forms of the heritage buildings along Queens Parade measure between approximately 8m and 12m deep, indicating room depths of approximately 4-6m. A mandatory 6m upper level setback of new built form will therefore ensure the heritage buildings within the intact streetscape remain prominent, that existing chimneys visible from the street are retained, and that façadism is avoided through the retention of at least one room's depth of the heritage building.
Upper level setback (364 Queens Parade)	Minimum 8m (mandatory)	A mandatory 8m upper level setback from the street frontage is required for 364 Queens Parade to ensure key views of the former ANZ Bank building are maintained when viewed from the south-east corner of Gold Street and Queens Parade. A mandatory 8m upper level setback will ensure that the upper storey, decorative roof form and elaborate chimneys remain prominent in the streetscape.
Upper level setbacks (169-197 Queens Parade)	Minimum 6m (preferred)	The degree of intactness of heritage fabric declines toward the south-eastern end of the precinct. A discretionary 6m upper level setback is therefore considered appropriate for 169-197 Queens Parade to ensure the strong two-storey street wall remains prominent, but allows some flexibility to respond to the more varied conditions in this portion of the precinct.
Upper level setbacks (side	Minimum 6m (preferred)	To maintain the prominence of return facades to buildings that front both Queens Parade and either Delbridge, Gold or

streets)		Michael Streets, a discretionary 6m upper level setback from the street wall is required. A discretionary control is appropriate to respond to a variety of secondary street wall conditions while ensuring heritage fabric is managed appropriately.
Building height	18m (5 storeys) (preferred), equating to an approximate 1:1 heritage street wall to new built form	Ensures that new development does not visually dominate the existing heritage street wall by requiring that new upper level built form is no greater than the volume of the heritage façade when the site is viewed from the opposite side of the Queens Parade boulevard.
Residential heritage precinct interface	Generally in accordance with ResCode Standard B17.	To ensure an appropriate transition the intact low-scale, double and single-storey heritage character of McKean Street and Hodgkinson Streets.

12.5 Precinct 5

Built Form Element	Requirement	Rationale
Sub-precinct 5A		
Street wall height	Retain existing	The State-significant building must be retained. New development to the rear of the building must not exceed the existing parapet height of the heritage building.
Street wall setback	Retain existing	The State-significant building must be retained. New development to the rear of the building should adopt the existing street wall setback of the heritage building at its south-eastern end or be further recessed to provide a visual separation between heritage and new built form.
Upper level setback (infill development)	Minimum 6m (preferred)	The depth of the parapet on the upper level of the former United Kingdom Hotel is approximately 4m. It is therefore recommended that upper level setbacks for infill development at the rear of the State-significant building be a discretionary 6m to ensure the highly significant form of the heritage building remains prominent in its three dimensional form.
Building height (infill development)	18m (5 storeys) (preferred)	To ensure that new development does not visually dominate the existing State-significant heritage place, particularly when viewed from within Raines Reserve, it is recommended that a discretionary 18m height limit be established.
Sub-precinct 5B		
Street wall height (Clifton Motors façade)	Retain existing	The Moderne façade, identified as being of potential State-significance, must be retained.
Street wall height (infill development, Queens Parade)	Match the parapet height of the former Clifton Motors building and the eaves line of the former United Kingdom Hotel building (mandatory)	The Moderne façades of the former Clifton Motors building and the former United Kingdom Hotel (both designed by the same architect) are prominent and highly significant decorative elements within the streetscape. Infill street wall along Queens Parade must preserve the strong decorative lines of the two Moderne buildings by adopting the same parapet height as the façade of the Clifton Motors building, which accords with the eaves lines of the former United Kingdom Hotel building.

Street wall setback (Clifton Motors façade)	Retain existing	The Moderne façade, identified as being of potential State-significance, must be retained.
Street wall setback (infill development, Queens Parade)	Match existing (preferred)	To ensure new infill built form responds to the heritage context. A discretionary control is appropriate as some street wall setback from the Moderne façade may assist in ensuring the façade remains prominent in the streetscape.
Upper level setback (behind Clifton Motors façade)	Minimum 6m (mandatory)	The highly decorative central 'fin' of the Moderne façade extends to a depth of approximately 4m. It is therefore appropriate that a mandatory minimum upper level setback of 6m be imposed to ensure its prominence and three dimensional form is retained within the streetscape. An upper level setback of new built form is also necessary to ensure the three-dimensional form of the garage building is retained and the building is not reduced to a facade.
Upper level setbacks (infill development)	Minimum 6m (preferred)	A 6m upper level setback from the street wall of new infill development will ensure the Moderne façades of the both the Clifton Motors building and the former United Kingdom Hotel building remain the prominent elements within the streetscape and that the three-dimensional form of the former Clifton Motors building retains legibility.
Building height (behind Clifton Motors façade and 203 Queens Parade)	1:1 heritage street wall to new built form (preferred)	Ensures that new development does not visually dominate the existing heritage street wall of the former Clifton Motors building by requiring that new upper level built form is no greater than the volume of the heritage façade when the site is viewed from the opposite side of the Queens Parade boulevard. Ensures that new development on 203 Queens Parade does not visually dominate the existing State-significant former United Kingdom Hotel, particularly when viewed from within Raines Reserve.
Sub-precinct 5C		
No controls recommended	-	-

12.6 Additional guidance

In addition to the above recommendations relating to street wall height, upper level setback and preferred ratio of new built form to street wall, we recommend that following design objectives be applied in a DDO:

- New infill development within the street wall should:
  - Interpret the historic façade rhythm, including fenestration patterns and proportions, the relationship between solid and void, and the module of structural bays.
  - Be distinguishable from the original heritage fabric and adopt a high quality and respectful contextual design response.
  - Ensure façade treatments and the articulation of new development are simple and do not compete with the more elaborate detailing of nineteenth century buildings.
  - Ensure fenestration patterns of new development generally reflects the vertical proportions of nineteenth and early twentieth century façades and avoids large expanses of glazing with a horizontal emphasis except to ground floor shopfronts.
  - Maintain the existing canopy/verandah height.

- Avoid the use of unarticulated curtain glazing or highly reflective glass.
- Avoid the replication of existing decorative features and architectural detail.
- Ensure ground floor facades facilitate activated street frontages.

- The adaptation of existing 'contributory' and 'individually significant' buildings should:
  - Discourage highly reflective glazing in historic openings.
  - Ensure the inter-floor height of the existing building is maintained and avoid new floor plates and walls cutting through historic openings.
  - Encourage the retention of solid built form behind retained facades and avoid balconies behind existing openings.

- New upper level development behind the heritage street wall should:
  - Ensure that the design and setback of the addition does not visually dominate the heritage building or surrounding heritage places.
  - Retain the primacy of the three-dimensional form of the heritage building within the streetscape.
  - Incorporate materials and finishes that are recessive in texture and colour.
  - Utilise visually lightweight materials that create a juxtaposition with the heavier masonry of the heritage facades.
  - Incorporate simple architectural detailing so it does not detract from significant elements of the existing building or streetscape.
  - Provide a recessive backdrop to the heritage street wall and individual heritage buildings.
  - Avoid highly articulated facades with recessed and projecting elements.
  - Avoid highly contrasting or vibrant primary colours.
  - Avoid unarticulated façades that give a bulky appearance, especially from oblique views.
  - Be articulated to reflect the fine grained character of narrow sites.

Appendix A: Former KG Luke Pty Ltd Factory & Showroom Heritage Citation

gard'ner jarman martin

GJM Heritage

Heritage Citation

KG LUKE PTY LTD FACTORY AND SHOWROOMS

Address: **26-52 Queens Parade, Fitzroy North (part)**

Prepared by: GJM Heritage

Date: September 2017

Place type: Factory, factory offices, showroom	**Architects:** Not known
Grading: Individually	**Builder:** Not known
Integrity: Fair	**Construction Date:** 1933 - 1965
Recommendation: Extend HO327 to include 26-52 Queens Parade (part) as shown in Figure 29. Update Appendix 8 accordingly.	**Extent of Overlay:** Refer to plan at Figure 29

Figure 1. The factory and showroom complex at 26-52 Queens Parade, Fitzroy North (GJM Heritage, May 2017).

Statement of Significance

What is significant?

The K G Luke Pty Ltd Factory and Showrooms, 26-52 Queens Parade, Fitzroy North, built from 1933 - 1965.

Elements that contribute to the significance of the place include the form, materials and detailing of the Art Deco façade and the ability to understand the place as a former factory and showroom complex.

Alterations and additions to the factory and showrooms after 1965 do not contribute to the significance of the place.

How is it significant?

The K G Luke Pty Ltd Factory and Showrooms is of local historical and architectural significance to the City of Yarra.

Why is it significant?

The K G Luke Pty Ltd Factory and Showrooms is of historical significance as a large industrial complex in Fitzroy North, which was established close to the Collingwood Gas Company gasworks and the Inner Circle Railway branch line. It is a remnant example of industrial development that occurred in this area from the 1860s through to the post-war period (Criterion A).

The K G Luke Pty Ltd Factory and Showrooms is of aesthetic significance for its particularly unusual and distinctive Art Deco façade which utilises a complex arrangement of castellated and hexagonal decorative elements (Criterion E).

The K G Luke Pty Ltd Factory and Showrooms is of historical significance for its association with Victorian manufacturer and sporting administrator, Kenneth G Luke, who moved his fledgling business to this site in 1929 and subsequently developed a successful business enterprise. Knighted in 1962, Luke was a highly successful businessman, with a range of interests and involvements in various committees and clubs (Criterion H).

Historical Themes

The place illustrates the following themes as outlined in the *City of Yarra Thematic History* (1998):

4 Developing Local Economy

- 4.2 Secondary Industry
- 4.3 Retail: Warehouses and Large Scale Purveyors

Contextual History

Lots at the west end of Queens Parade, Fitzroy North, were sold by the Crown from the mid-1860s. The south side of the Queens Parade was reserved (from east to west: Smith Street to Alexandra Parade) for the Collingwood Gas Company, Church of England (disposed of by the church in 1877) and a public reserve (Figure 2). The gasworks operated on the site bound by Queens Parade, Smith Street, Alexandra Parade and George Street from the 1860s, around which further industrial sites established, resulting in a mix of residential and manufacturing development along Queens Parade, constructed during the Victorian, Edwardian and Interwar periods.

These sites were served by a branch line of the Inner Circle Railway, which operated between 1888 and 1948. The line ran from Royal Park along Park Street through North Carlton and Clifton Hill, with a branch line running south through Edinburgh Gardens, before terminating at 'Fitzroy' station at the southern end of the gardens (since demolished). The branch line through Edinburgh Gardens was closed to passengers in 1892 after which it was used as a goods line, mainly delivering coal to the gasworks on Queens Parade. While most of the network was electrified in 1921, the branch line through Edinburgh Gardens remained

unelectrified. The line closed in 1948 and was largely removed in the mid-1980s (Fitzroy Historical Society 2014; Butler 2007:104).

The footprints of the factories and warehouse buildings establishing along the western part of Queens Parade, predominantly west of Wellington Street, are evident on the series of 1896-1902 Melbourne and Metropolitan Board of Works (MMBW) plans (Figures 3-7). Aerials dating to c1925-c1935 and 1945 show the increased industrial development at this end of Queens Parade during the Edwardian and Interwar periods, filling the vacant lots evident on the MMBW plans (Figures 8 and 9).

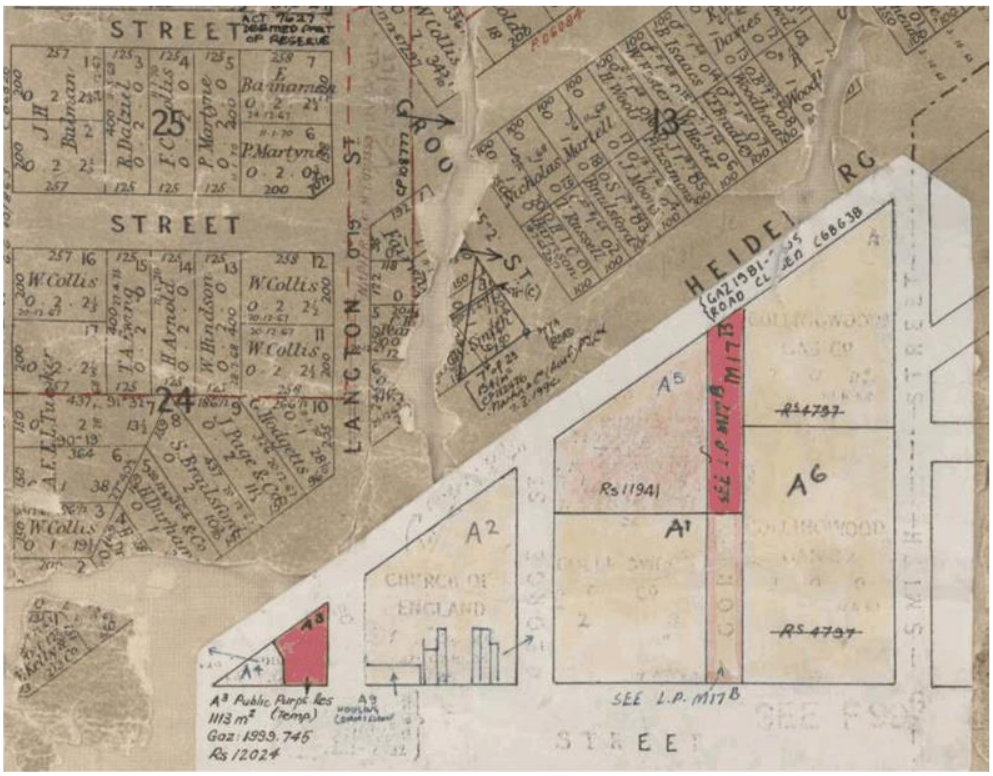

Figure 2. The North Fitzroy Township Plan (Parish of Jika Jika) showing the lots first sold be the Crown on the north side, and those on the south side reserved for (east-west) the Collingwood Gas Co, Church of England and public reserve.

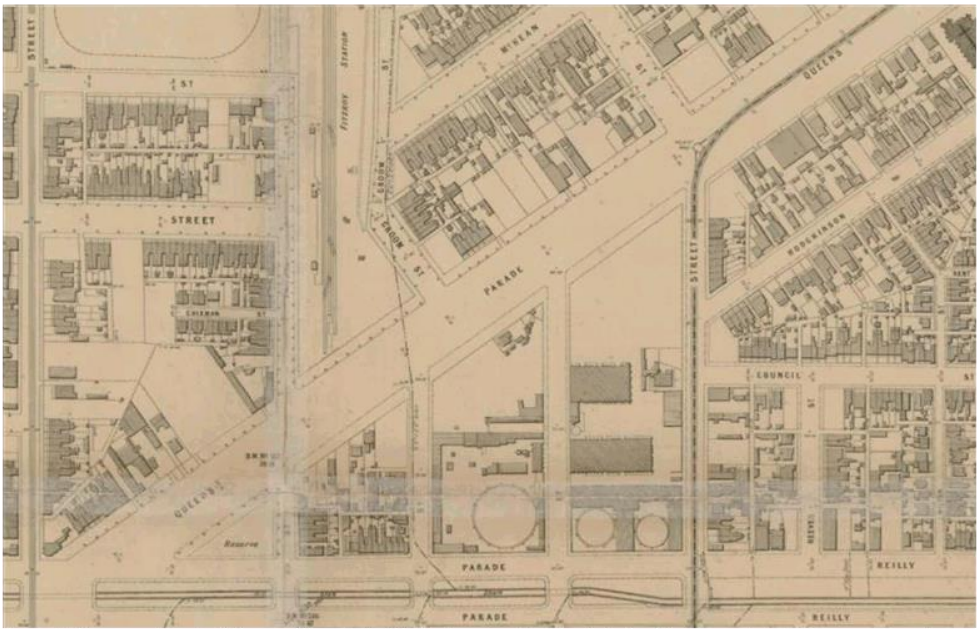

Figure 3. The west end of Queens Parade in 1896, showing the footprints of large-scale buildings amongst vacant lots and residential development (MMBW Base Map No. 29, 1896).

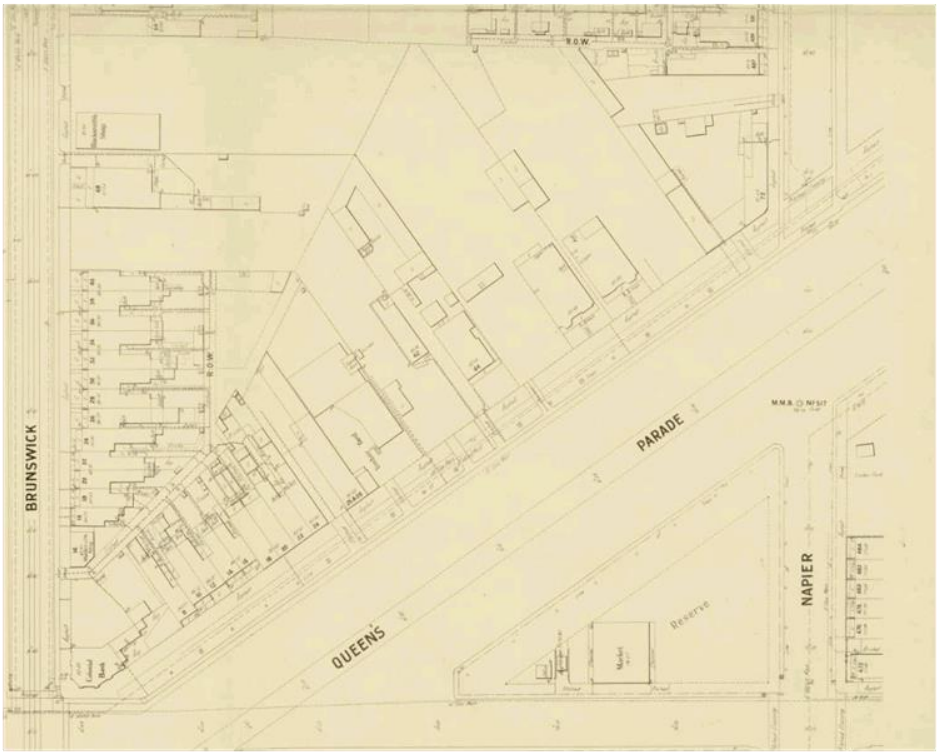

Figure 4. The west end of Queens Parade between Alexandra Parade and Napier Street in 1901 (MMBW DP No. 1257, 1901).

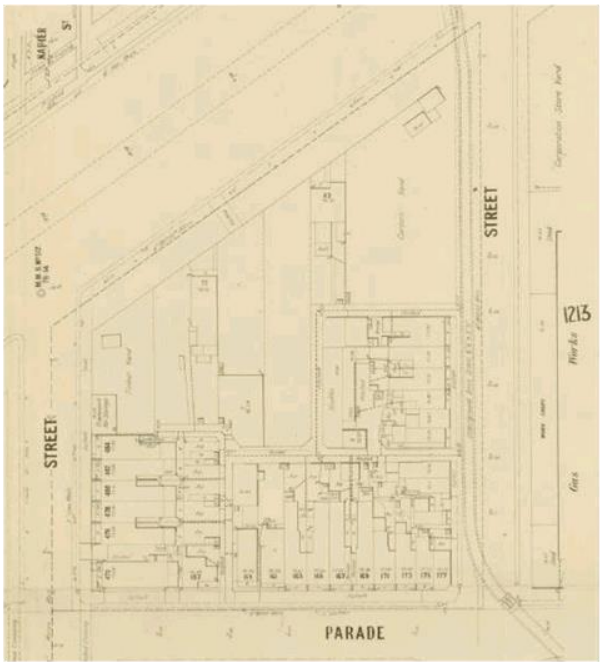

Figure 5. Queens Parade between Napier Street and George Street in 1900 (MMBW DP 1212, 1900).

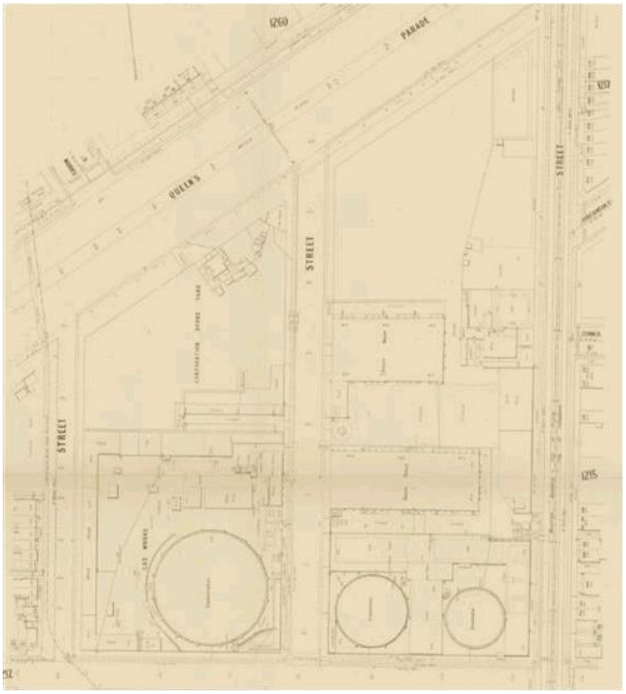

Figure 6. The gas works site in 1900, between George and Smith streets; central is the extension of Gore Street, since removed (MMBW DP 1213, 1900).

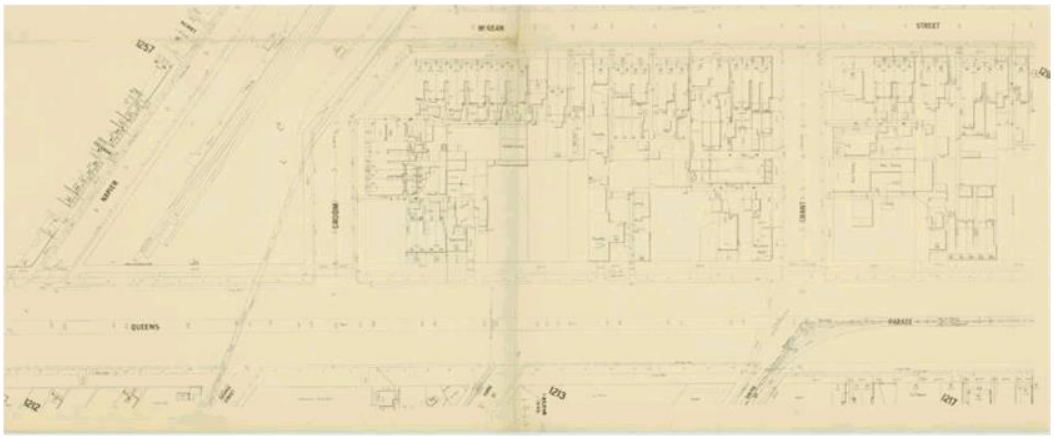

Figure 7. The north side of Queens Parade, east of Napier Street, in 1901 (MMBW DP 1260, 1901).

Figure 8. A view looking south-east at the west end of Queens Parade (which runs from the top of image to right of image), c1925-c1935. Smith Street extends off Queens Parade to the right. The gasometers of the gasworks are visible to the right (Pratt, c1925-c1935).

Figure 9. Industry in the area in 1945 (Land Victoria aerial, 1945).

Some of the industrial fabric at the western end of Queens Parade remains in 2017, while many sites previously occupied by industry have been developed with modern residential apartments. Remnants of industrial heritage in this location (between Alexandra Parade and Wellington Street) remain at (from west to east): the K G Luke complex at 26-52 Queens Parade (c1860s-c1960s), the factories and saw-tooth roof buildings on the south side of Queens Parade between George and Napier streets (some appear to date to the Interwar period), remnant gasworks buildings at 111 Queens Parade, the Selotta Shoes Pty Ltd building at no. 122 Queens Parade (1934), a factory/warehouse at no. 142-146 (c1915-c1925; the facade of which remains), and former factories at 7 Queens Parade (c1915-c1925) and 37 Queens Parade, Clifton Hill (c1900-c1915).

Place History

Kenneth George Luke became a partner in a small metal-spinning and silverware business in Carlton in 1921 and by 1925 was the proprietor, with seven employees (Dunstan, 2000). In 1926, Luke established the business K G Luke Pty Ltd, with 11 employees. Luke first 'commenced with the manufacture of Paramount brand silverware, concentrating principally on trophy cups, tea sets, cake baskets and similar products of a utility nature. An assured demand was immediately established' (*Decoration and Glass,* Vol 4, No. 5, 1 Sep 1938:66).

In 1929 Luke purchased a factory in Queens Parade, Fitzroy North and the same year K G Luke was listed for the first time as a metal spinner at 28 Queens Parade (Dunstan, 2000; S&Mc). The Sands & McDougall Directories indicate that in 1930, this section of Queens Parade was occupied by Tait & Bird, sheet metal workers, at 26 Queens Parade. Adjacent was K G Luke, metal spinner, now addressed at 28-34 Queens Parade (S&Mc). The factory was located near the Inner Circle Railway branch line that ran through Edinburgh Gardens to 'Fitzroy' station at the southern end of the gardens (since demolished), which operated between 1888 and 1948.

The K G Luke complex expanded rapidly in the 1930s. Drawings dating to 1933 show proposed additions and alterations to an existing office and factory building for K G Luke Pty Ltd at 30 Queens Parade (Figure

10). The drawings show the conversion of a single-storey workshop into a two-storey building with an Art Deco facade (Figures 11-12); the design of the parapet to Queens Parade is different to that which remains in 2017 (altered in 1937).

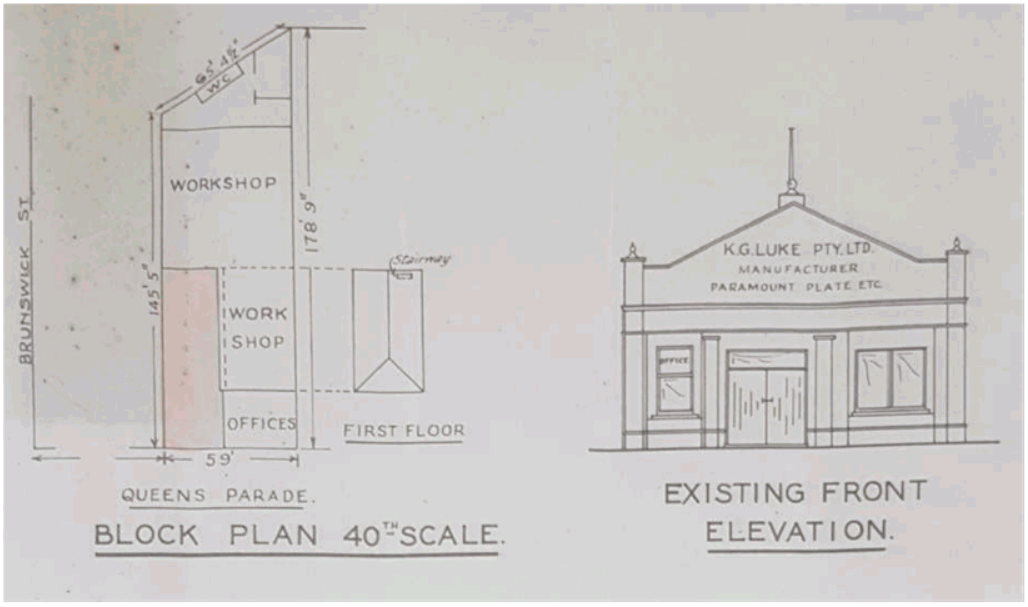

Figure 10. 1933 drawings – The 1933 footprint and facade of the factory; altered to form the section that now has a double-gabled roof (PROV, 1933).

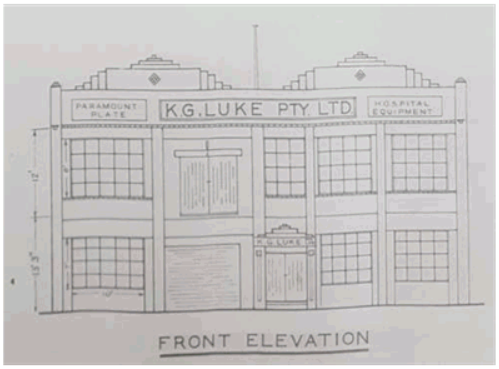

Figure 11. 1933 drawings – proposed design of the Queens Parade facade to the double-gabled building (PROV, 1933).

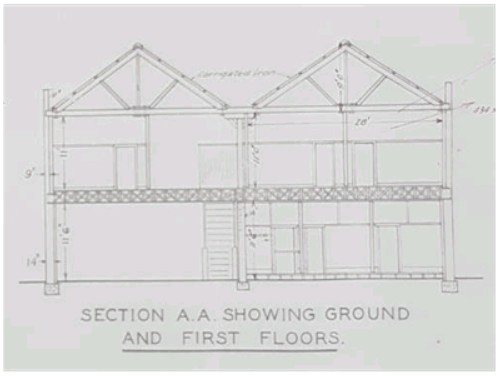

Figure 12. 1933 drawings – section of the proposed double-gabled building (PROV, 1933).

The 1936-37 rate books confirm that K G Luke Pty Ltd was the owner of a brick factory, addressed at 30 Queens Parade, with a Net Annual Value (NAV) of £170. In the same year Luke purchased a six room brick house at 26 Queens Parade (NAV of £70) from Alfred Bird and John Tait, iron merchants, and a lot of land to the east of the factory from Mary McLennan (RB).

Architectural drawings dating to 1937 (Figures 13-15) show designs for the addition of the eastern half of the Art Deco facade and its building; however the facade is shown with a simpler design compared to what was constructed, suggesting it was not the final design. The drawings show that the western half of the two-storey building (with the double-gabled roof) existed – an annotation instructs 'facade of existing

building to be altered to correspond with new elevation' which indicates that the facade of the earlier building was updated in 1937.

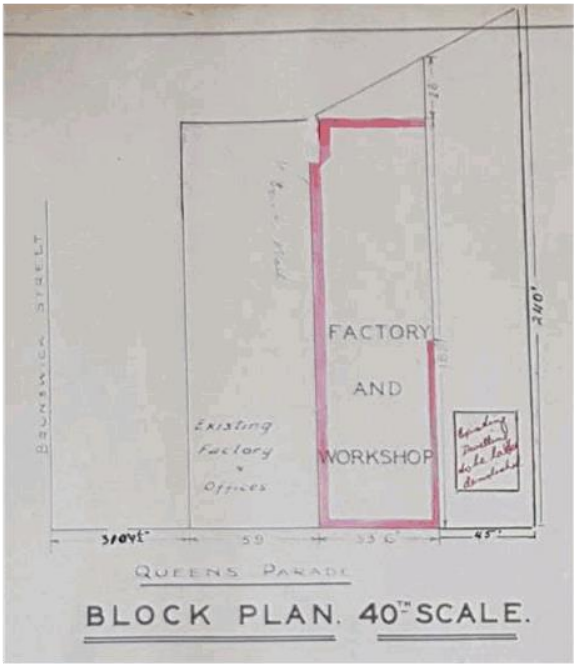

Figure 13. 1937 drawings – showing the existing factory and offices, being the double-gabled roof building with a 59' (approx 18m) frontage to Queens Parade. The 1937 addition is located to the east (red) (PROV, 1937).

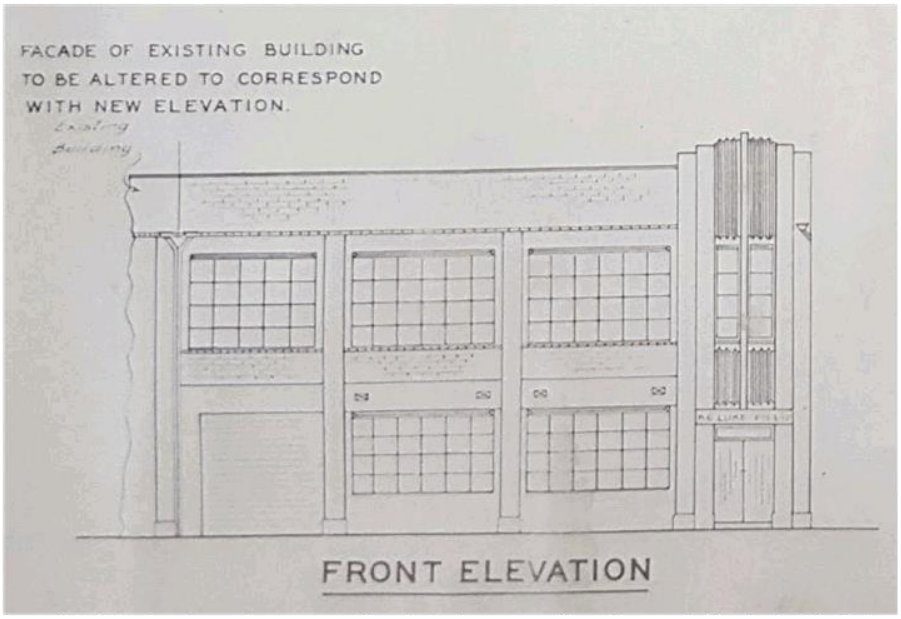

Figure 14. 1937 drawings – the proposed facade to the new section. The annotation top left indicates the facade of the existing section to the west was to be altered to match this new facade (PROV, 1937).

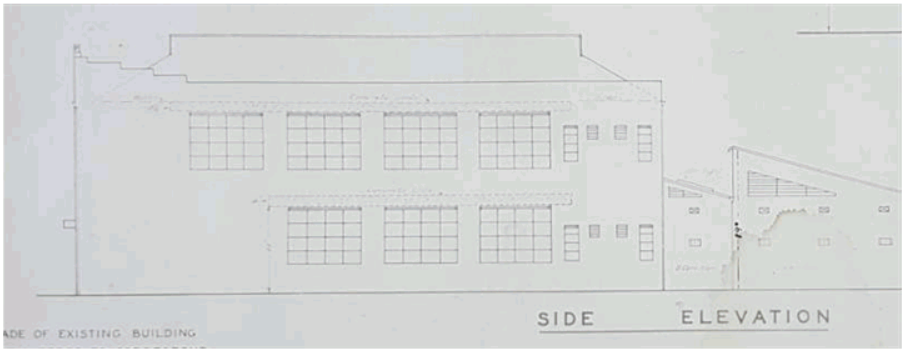

Figure 15. 1937 drawings – detail of the east elevation of the new section (PROV, 1937).

The 1937-38 rate books confirmed that K G Luke Pty Ltd owned and occupied 26-42 Queens Parade. The brick factory at 30-42 Queens Parade now had an increased NAV of £380 (RB), reflecting recent works to the property. The brick house at 26 Queens Parade had a separate NAV of £70. The 1938-39 rate books show a slight increase of the combined NAV (for both the house and factory) at 26-42 Queens Parade to £530. This value remained the same until at least 1944 (RB).

The newly completed building was featured in the journal *Decoration and Glass* (Vol 3, No. 12, 1 Apr 1938:28-29), entitled 'Southern Capitals Latest Commercial Structure'. The article featured photos and an illustration of the new facade (which remains in 2017; Figures 16-19). The illustration shows the extent of the Art Deco facade, completed by publication in April 1938.

The illustration also shows an earlier (possibly Victorian) building located at the west end of the complex, to the left of the 1937 facade (Figure 17). This building was later altered, or a new building constructed, with a facade that was sympathetic in style to the adjacent 1937 design. A 1945 aerial shows the current saw-tooth roof building, which may suggest that the existing facade was also constructed by this date (Figure 21).

The April 1938 journal article reported that

> *The new building at Queen's Parade, Fitzroy, Victoria, which houses the showrooms, administrative offices and factory of K. G. Luke Pty Ltd, marks the culmination of eleven years of progress.*
>
> *Designed in brick, with steel frame construction, a distinctly modern feeling has been imparted to the elevations. The skyline is irregular, and the cemented parapet is ornamented with several drop features in brick and cement. Both the plan and elevation were evolved with due regard to the building's location on a tree-lined street.*

The article continued, to describe in detail the interior administration area, showroom, counters and stairs, and their features and finishes, noting the modern round corners and bent glass. Further, it states:

> *Over the factory section the roof is a saw-tooth type, designed to admit daylight into the centre of the factory floor. It is covered with corrugated asbestos cement.*
>
> *Bricks used in the main front are texture surfaced and show to advantage between the cement work. The firm's name appears in large letter on the parapet, as well as an enumeration of its manufactures.*

The article noted that the architect was Eric J. Ziebell and Pollard Bros the builders, however, the May edition of *Decoration and Glass* (Vol 4, 1 May 1938:69) amended that the 'new headquarters of K. G. Luke Pty Ltd, makers of Paramount brand silverware and hospital and kitchen equipment, including work in

stainless steel, were designed and built by the company, and not by the architect and builders named in our last issue, in which the building was reviewed'.

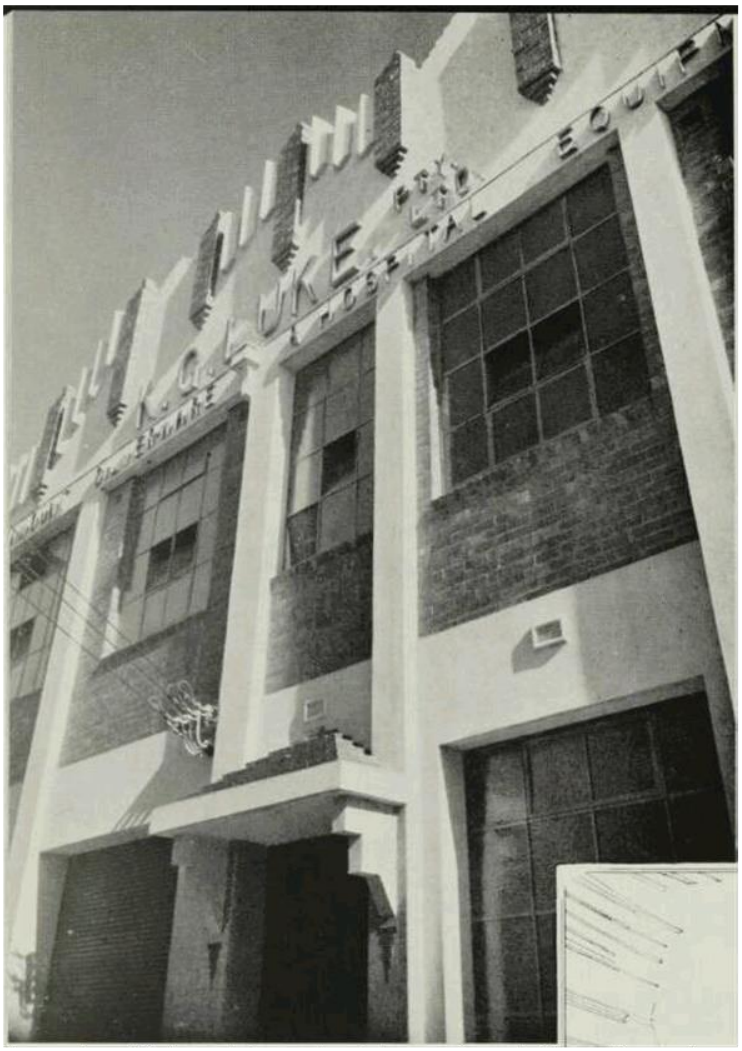

Figure 16. A 1938 photo of the new facade (*Decoration and Glass*, Vol 3, No. 12, 1 Apr 1938:28-29).

Figure 17. A 1938 'line drawing of the entire front' of the 'new building' (*Decoration and Glass*, Vol 3, No. 12, 1 Apr 1938:28-29).

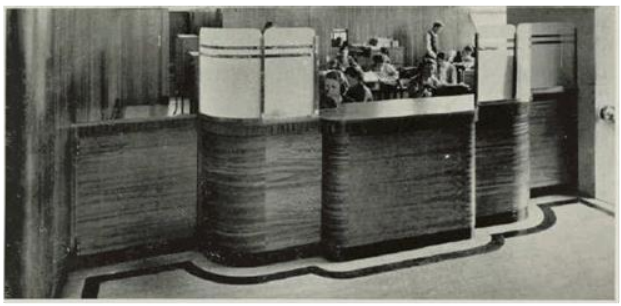

Figure 18. 1938 photo of the new interior and the curved veneer panelling of the enquiry counter with its glass screens above (*Decoration and Glass*, Vol 3, No. 12, 1 Apr 1938:28-29).

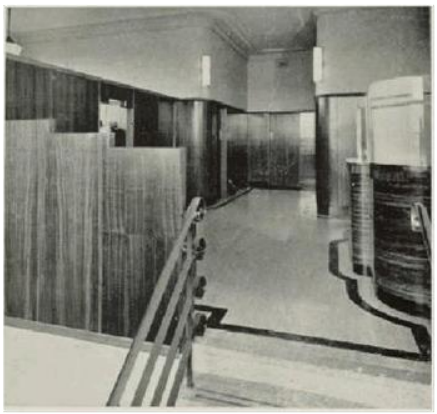

Figure 19. 1938 photo of the new interior design – the main stairs leading to the offices and showrooms (*Decoration and Glass*, Vol 3, No. 12, 1 Apr 1938:28-29).

In September 1938 *Decoration and Glass* (Vol 4, No. 5, 1 Sep 1938:64-66) again featured the Australian Firm, K G Luke Pty Ltd. The article reported that the company's activities had become nation-wide, known all over Australia as 'manufacturers of fine quality silver and electroplate ware, and hospital and kitchen equipment'. At this date the company employed over two hundred people, with the Paramount range encompassing a huge range of products for kitchens and hospitals listed in the article, which included a high number of medals and college badges. The company also served as the agents for the Sheffield Silver Plate and Cutlery Co. of England, marketed under the brands of Paramount and Cambridge (*Decoration and Glass,* Vol 4, No. 5, 1 Sep 1938:64-66).

A photo of the 'recently completed' building was also published in *Building* magazine in July 1940 (Vol 66, No. 395, 26 Jul 1940:36), which described the premises as 'modern and distinctive, particularly in regard to the serrated parapet treatment. Emphasis has also been directed towards the main entrance by a series of vertical reeds in the spandrel panels' (Figure 20). The detail to this eastern entrance bay has since been altered and the entrance removed. Other openings to the 1937 facade have also since been altered.

Figure 20. A 1940 photo of K G Luke Pty Ltd factory premises addressed at 32 Queens Parade, North Fitzroy (*Building*, Vol 66, No. 395, 26 Jul 1940:36).

Figure 21. Detail of a 1945 aerial of the site (Land Victoria, *Melbourne 848 B2D*, 1945).

During World War II, production increased at the factory (Dunstan, 2000). K G Luke Pty Ltd is known to have also functioned as a private mint that made medals, medallions, badges and buttons for all levels of government, universities and the military, including medals for the 1956 Olympic Games in Melbourne (MV).

The building at the east end of the complex was added in 1965, described as an extension to the 'factory and office block providing a large section of warehouse accommodation'. The extensions were designed by architects Meldrum & Partners (PROV). This new section is evident in the much simpler facade to Queens Parade, without a parapet (Figures 22-24). The drawings, dated 12 March 1965, show the two-storey Art Deco facade and its detail (Figures 23-24). The 1965 site plan (Figure 22) suggests that a building at the west end of the site existed, however, the elevation drawing does not illustrate it (Figures 22-23). The site plan shows Luke's increased holdings by this date.

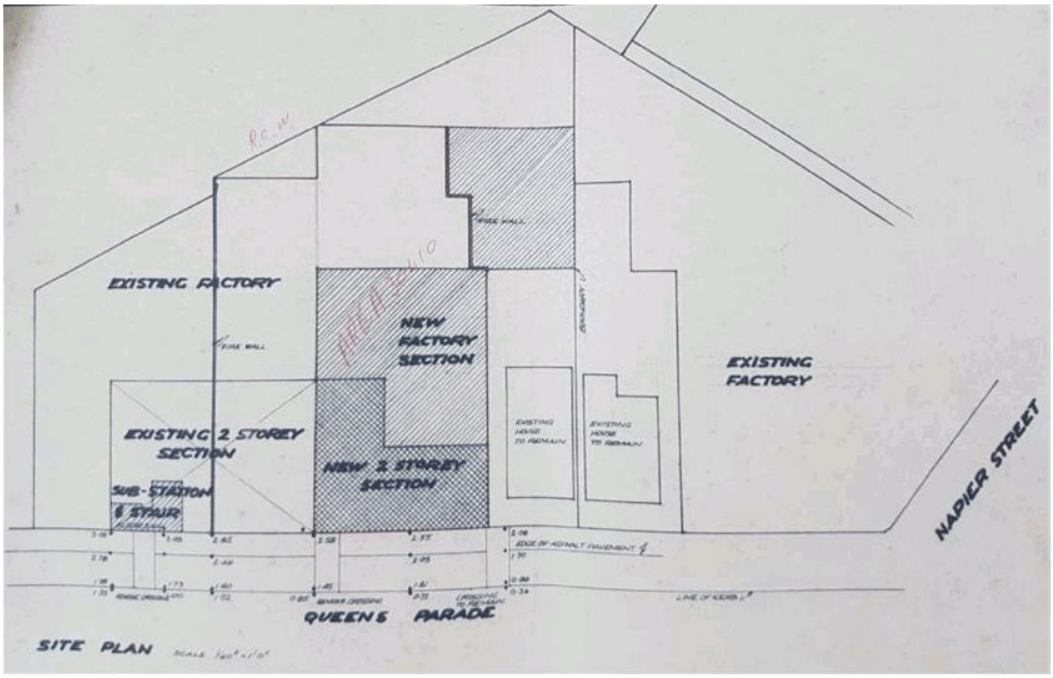

Figure 22. 1965 drawings – site plan of the 1965 extensions to the complex (PROV, 12 March 1965).

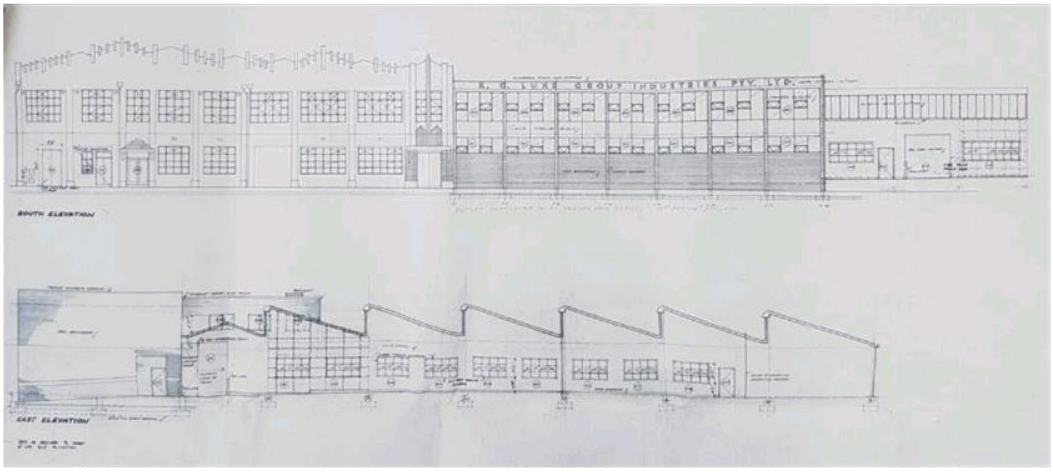

Figure 23. 1965 drawings – the south and east elevations showing the two-storey 1937 Art Deco facade and the proposed section to the east (right) (PROV, 12 March 1965).

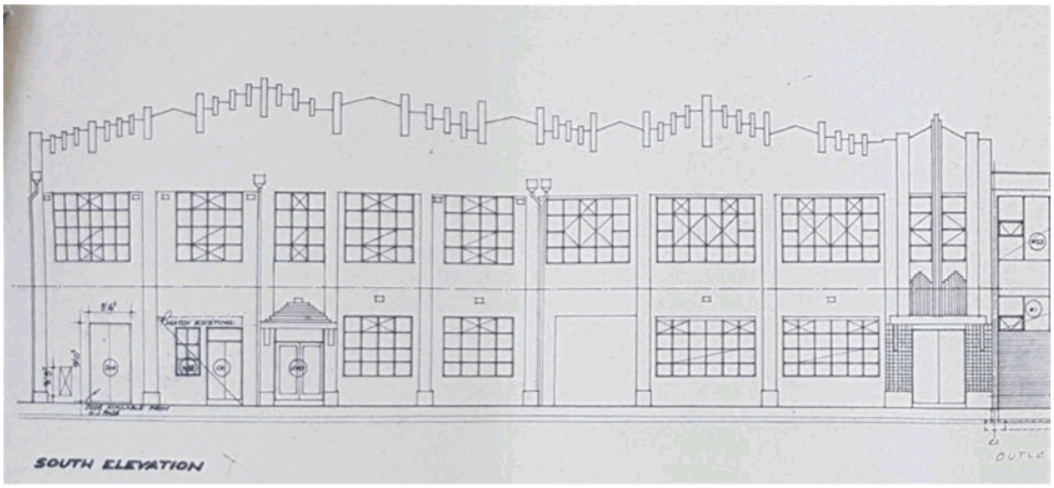

Figure 24. 1965 drawings - detail of the two-storey Art Deco facade and its detail in 1965 (PROV, 12 March 1965).

The Queens Parade complex was operated by K G Luke until at least the mid-1970s. K G Luke Group Industries Ltd changed its name to Luke Limited on 20 December 1976. Luke Limited was taken over by National Consolidated Limited on 19 October 1983 (MV).

Sir Kenneth G. Luke

The following is the *Australian Dictionary of Biography* entry for Sir Kenneth Luke (Dunstan, 2000):

> *Sir Kenneth George (Ken) Luke (1896-1971), manufacturer and sporting administrator, was born on 11 November 1896 in Port Melbourne, son of London-born parents George Edwin Luke, labourer, and his wife Minnie Annie, née Bensley. Ken attended South Brunswick State School and Ballarat High School before being apprenticed to an engraver in a silver factory at the age of 14.*
>
> *Diligent and ambitious, he saved enough from his wages to become a partner (1921) in a small metal-spinning and silverware business at Carlton. By 1925 he was its proprietor and an employer of seven. For them there was no Depression. They were still with him thirty years later—with 650 others—making an ever-increasing range of products: silverware, stainless-steel surgical equipment, plated goods and glass-washing machines. For ten years Luke never knew an evening or a weekend off; he had an alarm clock to wake him at regular intervals at night so that he could keep the machinery going. In 1929 he bought a larger factory in Queen's Parade, Fitzroy, doubled his staff and introduced new lines. At the outbreak of World War II production was again stepped up and Luke became an honorary adviser to the Department of Supply and Shipping. On 11 February 1939 he had married 29-year-old Valda Richardson with Anglican rites at the Church of the Annunciation, St Marylebone, London.*
>
> *Luke extended his interests after 1945. He had purchased the property, Deepfields, at Romsey as a weekend retreat, but the city-bred industrialist developed a passion for farming and made it a champion Dorset Horn and Poll Hereford stud. In November 1949 the Hollway government appointed him a trustee of the (Royal) Exhibition Building. During his terms as chairman (1954-57 and 1961-63) trade exhibitions came to the fore, the stadium annexe was constructed for the 1956 Olympic Games, unwanted tenants were removed, and a new western annexe was built for an international trade fair in 1963. Luke had founded (1952) the White Ensign Club for sailors. He became a notable supporter of many other charitable causes. In the 1950s and 1960s he travelled abroad to keep abreast of manufacturing trends, such as automation. By then a millionaire, he still maintained a craftsman's as*

well as a proprietor's eye over the workings of his plants. K. G. Luke (Australasia) Ltd was registered as a public company in 1953.

A cricketer, motorcyclist and yachtsman in his youth, Luke also enjoyed racing and owned the successful 'FF' horses, with names like Fighting Force and Feeling Fine. In his view, sport bred character. He was best known, however, for his involvement with Australian Rules football. In 1938-55 he was president of the Carlton Football Club. A delegate (from 1935) to the Victorian Football League, he was its vice-president (1946-55) and president (1956-71). With postwar growth generating record crowds, he consolidated central administration and professionalized the game for players. In 1962 the V.F.L. bought land at Waverley for a new ground, which Luke envisaged as an alternative to the Melbourne Cricket Ground for football finals. V.F.L. Park, his legacy, opened in 1970.

'KG', as he was known, was a self-made man, hard-working and disciplined, but also far-sighted, courteous and even-tempered. The range of his interests and the number of his involvements was remarkable—he believed that a busy man could always take on another job. He was a councillor of the Royal Agricultural Society of Victoria and a member of at least fourteen clubs, including the Savage, the Victoria Racing and the Melbourne Cricket clubs. Luke was appointed C.M.G. in 1954 and knighted in 1962. Survived by his wife and daughter, Sir Kenneth died on 13 June 1971 at his Hawthorn home and was cremated. By that time K. G. Luke Group Industries Ltd, with twelve subsidiaries, had sales of $17.2 million and an annual profit of nearly $3 million. Luke's estate was sworn for probate at $412,156. A stand at Waverley Park is named after him and a portrait by Paul Fitzgerald is held by the Australian Football League.

Physical Description

The K G Luke Pty Ltd Factory and Showrooms, 26-52 Queens Parade, Fitzroy North occupies part of a large triangular site. It comprises a series of buildings with front facades to Queens Parade constructed in the 1930s and 1960s. The western part of the site appears to have been fully developed by the late 1930s (or early 1940s) and includes a sawtooth-roofed building, with detailing to the façade parapet similar in style to the adjoining decorative parapet, and three gable roofed buildings of varying width to the east. Further east is an additional building constructed in the 1960s. All buildings are of two-storeys and are painted in a consistent colour across the extensive Queens Parade façade.

The 1930s Art Deco facade is of brick and steel-frame construction with a render finish applied to the body of the main façade and piers that divide the façade into nine unevenly spaced bays. Spandrels below the first floor windows are of face brickwork (overpainted). The highly distinctive parapet consists of a series of uneven brick (overpainted) and cement rendered vertical elements which rise and fall across the façade, creating a castellated appearance. Openings are set between the piers and some original steel-framed windows remain at first floor level, however the majority of openings have been modified. A pyramidal hood, supported by heavy corbels, remains over a secondary entrance at street level.

The contrasting highly restrained façade of the 1960s building to the east is of seven bays with simple strips of high windows to the ground floor, larger windows to the first floor and a plain parapet above. Two bays at the eastern end are recessed at ground floor level and the upper floor is supported on two plain, square columns.

The showroom and factory buildings behind the 1930s façade and 1960s building are a mix of saw-tooth and pitched roofed structures that occupy the majority of the site.

Integrity

The Art Deco façade retains a fair level of integrity to the 1937-1939 period of construction in fabric, form and detail. While the 1930s facade has undergone alterations – including overpainting, modification of

openings, removal of façade signage and remodelling of the eastern entrance bay – the place can still be understood and appreciated as a reasonably intact example of a 1930s factory complex.

The 1960s eastern addition retains a higher degree of integrity than the Art Deco façade. The façade has been overpainted and signage has been removed, otherwise the form and detailing remain largely as constructed.

The structures behind the Art Deco façade and 1960s addition are not generally visible from the public realm, but current aerial photographs suggest the roof forms are reasonably intact.

Comparative Analysis

The K G Luke Pty Ltd Factory and Showrooms at 26-52 Queens Parade, Fitzroy North is an example of a large industrial development constructed in the 1930s. The distinctive and unusual front façade of the building, illustrated in a number of contemporary journals soon after construction, does not appear to have any direct comparators in the City of Yarra. Buildings of the period were more typically designed in the popular Moderne style of the 1930s. Within the City of Yarra there are a number of buildings designed in Moderne style which are included in the Heritage Overlay of the Yarra Planning Scheme as individually significant buildings. They include:

- HO92 (& VHR H0684), Former United Kingdom Hotel, 199 Queens Parade, Clifton Hill, 1937-38
- HO252, Former Repco Offices and Laboratory, 26 Doonside Street, Richmond, c1939
- HO391, Former Relova Redressing Laundry, 129 Hoddle Street, Richmond, 1937
- HO375, Former Russell Manufacturing Company, 81-95 Burnley Street, Richmond, 1942

Figure 25. Former United Kingdom Hotel (VHD)

Figure 26. Former Russell Manufacturing Co (VHD)

Figure 27. Former Revola Redressing Laundry (VHD)

Figure 28. Former Repco Offices (VHD)

These individual comparators are representative of buildings of the interwar period and are architecturally significant as distinctive examples of the Moderne style. They exhibit characteristics typical of this style including vertical 'towers', contrasting horizontal lines of the façade and canopies, parapet signage and contrasting materials such as brickwork and render.

In comparison, the highly unusual façade of the K G Luke Pty Ltd Factory and Showrooms at 26-52 Queens Parade, Fitzroy North does not display these typical characteristics, particularly the strong horizontal emphasis of the Moderne style. Contrasting materials were part of the original design, however these have been obscured by overpainting of the façade, and the original detailed vertical entrance bay to the east has been modified.

Assessment Against Criteria

Following is an assessment of the place against the recognised heritage criteria set out in *Planning Practice Note 1: Applying the Heritage Overlay* (July 2015).

Criterion A: Importance to the course or pattern of our cultural or natural history (historical significance).

The K G Luke Pty Ltd Factory and Showrooms is of historical significance as a large industrial complex in Fitzroy North, which was established close to the Collingwood Gas Company gasworks and the Inner Circle Railway branch line. It is a remnant example of industrial development that occurred in this area from the 1860s through to the post-war period.

Criterion E: *Importance in exhibiting particular aesthetic characteristics (aesthetic significance).*

The K G Luke Pty Ltd Factory and Showrooms is of aesthetic significance for its particularly unusual and distinctive Art Deco façade which utilises a complex arrangement of castellated and hexagonal decorative elements.

Criterion H: *Special association with the life or works of a person, or group of persons, of importance in our history (associative significance).*

The K G Luke Pty Ltd Factory and Showrooms is of historical significance for its association with Victorian manufacturer and sporting administrator, Kenneth G Luke, who moved his fledgling business to this site in 1929 and subsequently developed a successful business enterprise. Knighted in 1962, Luke was a highly successful businessman, with a range of interests and involvements in various committees and clubs.

Grading and Recommendations

Only part of the Art Deco façade (at 26 Queens Parade) is currently included in the North Fitzroy Precinct Heritage Overlay (HO327).

It is recommended that HO327 be extended to include 26-52 Queens Parade (part, see Figure 29) and that the place (as defined in Figure 29) be identified as an 'individually significant' heritage place within the 'City of Yarra: Review of Heritage Overlay Areas, Appendix 8'.

Recommended extension of the Heritage Overlay (HO327)

Figure 29. Recommended extent of the place to be included in HO327 shaded in red. (adapted from nearmap, 4 May 2017)

Identified by:

Gary Vines & Matthew Churchward (1992), *Northern Suburbs Factory Study*

> The 1992 Study recommended that 'K. H. Luke metal works, 28 Queens Parade, Fitzroy North' was '*of local significance as a prominent local landmark with an unusual facade treatment. Also of interest as an example of the transport related industries which were concentrated along Heidelberg Rd from the early twentieth century.'*

Allom Lovell & Associates (1998), *City of Yarra Heritage Review*

> The 1998 Study recommended that the 'Former K G Luke Metal Works, 28 Queens Parade, North Fitzroy' was of individual significance to the North Fitzroy Precinct (Vol 4, Appendix B).
>
> However, the precinct boundary shown for North Fitzroy Precinct in this Study does not clearly include or exclude the complex.

Graeme Butler and Associates (2007, 2013), *City of Yarra Review of Heritage Overlay Areas*

> The 2007 Study included two citations for the complex (with almost duplicate information):
>
> - K.G. Luke Pty Ltd. electro plate manufacturers factory (part), 26 Queens Parade, Fitzroy North (dated 1938)

- K.G. Luke Pty Ltd. electro plate manufacturers factory, former (part), 28-58 Queens Parade, Fitzroy North (dated 1937)

Both places were found to be individually significant to North Fitzroy Precinct (HO327). The duplicate Statements of Significance found the complex to be '*historically and architecturally significant to the locality of Fitzroy North and the City of* Yarra' as

- *`An unusual large `Art Deco' (or Moderne style) factory/warehouse that has a very intact façade of architectural merit especially in its parapet profile' (part 26-58, bricks painted).*
- *This (factory) was co-located on the spur railway line through the Edinburgh Gardens and the Queens Pde `Fitzroy' rail station which was a lucrative goods railway line from 1891 to 1980, serving this and other interwar industries nearby' (Wight 2001).*
- *The building is closely associated with the nationally known (Sir) KG Luke and his highly successful business enterprise as well as his sporting links, both local and national.*

However the precinct boundary for North Fitzroy Precinct (HO327) shown in this Study included the western half of the complex and excluded the eastern half.

References:

Advocate [Melb., Vic]

Building: the magazine for the architect, builder, property owner and merchant [Australia].

City of Fitzroy Rate Books (RB), Central Ward, as cited in Graeme Butler and Associates, *City of Yarra Review of Heritage Overlay Areas*, 2007, pp 436-443.

Daily Commercial News and Shipping List [Sydney, NSW].

Decoration and Glass [periodical; NSW].

Dunstan, David, 'Luke, Sir Kenneth George (Ken) (1896–1971)' in *Australian Dictionary of Biography*, <http://adb.anu.edu.au/>, published first in hardcopy 2000, accessed online 17 August 2017.

Fitzroy History Society, *Fitzroy Railway Walk — Inner Circle railway line* (October 2014), <http://www.fitzroyhistorysociety.org.au/images/file/FitzroyRailwayWalk.pdf>, accessed August 2017.

Graeme Butler and Associates, *City of Yarra Review of Heritage Overlay Areas*, 2007.

Land Victoria, 'MELBOURNE 848B2D' aerial, 1945.

Melbourne and Metropolitan Board of Works Detail Plans and Base Maps, as cited.

Museum Victoria (MV), 'K.G. Luke, Medal & Trophy Makers, Melbourne, Victoria', <https://collections.museumvictoria.com.au/articles/2172>, accessed 18 august 2017. Objects made by K. G. Luke are shown in the online Trove pictures, photos, objects collection <http://trove.nla.gov.au/picture/>.

Pratt, Charles, Airspy collection, *Football ground in Brunswick Street North Fitzroy now known as Fitzroy Community Oval*, c1925-c1935, SLV Image H91.160/447.

Public Records Office of Victoria (PROV), KG Luke architectural drawings: VPRS 16127/P2/Unit 5, drawings dated 1933; VPRS 16127/P2/Unit 9, drawings date to 1937 (according to catalogue entry); VPRS 10150/P0/Unit 162, drawings dated 12 March 1965.

Sands & McDougall Directories (S&Mc).

The Age.

Appendix B: Row of Houses, Elizabeth Terrace Heritage Citation

GJM Heritage

Heritage Citation

ROW OF HOUSES, ELIZABETH TERRACE

Address: **472-484 Napier Street, Fitzroy North**

Prepared by: GJM Heritage

Date: November 2017

Place type: Residential	**Architects:** Not known
Grading: Locally significant	**Builder:** Not confirmed
Integrity: Very High	**Construction Date:** 1885, 1889
Recommendation: Include in the Heritage Overlay	**Extent of Overlay:** Refer to plan at Figure 10

Figure 1. Left to right: 476-484 Napier Street, 472 Napier Street.

Figure 2. Detail of 472 Napier Street.

GJM Heritage

Statement of Significance

What is significant?

Elizabeth Terrace, 476-484 Napier Street and the terrace house at 472 Napier Street, Fitzroy North, built in 1885 and 1889 respectively.

Elements that contribute to the significance of the place include (but are not limited to):

- The houses' original external form, materials and detailing
- The houses' high level of integrity to its original design
- The original front fences and gates.

Later alterations and additions to the rear of the terraces and the addition to the north of 472 Napier Street, are not significant.

How is it significant?

Elizabeth Terrace, 476-484 Napier Street and the terrace house at 472 Napier Street, Fitzroy North are of local architectural and aesthetic significance to the City of Yarra.

Why is it significant?

Elizabeth Terrace, 476-484 Napier Street and the terrace house at 472 Napier Street, Fitzroy North are fine and representative examples of terrace housing from the Victorian period. They display typical features of the Victorian Italianate architectural style popular in the 1880s boom period in Fitzroy North and across Melbourne more broadly, including a two-storey form with front verandahs and balconies, polychromatic brickwork, decorative cast iron, rendered decorative elements including balustrading and cast iron palisade fencing (Criterion D).

Elizabeth Terrace, 476-484 Napier Street and the terrace house at 472 Napier Street, Fitzroy North are well-considered and carefully detailed examples of Victorian Italianate terrace housing. The row of polychromatic terraces, with repetitive decorative elements across the facades, presents a picturesque composition of this architectural style (Criterion E).

Historical Themes

The place illustrates the following themes as outlined in the *City of Yarra Thematic History* (1998):

3 Mansions, Villas and Sustenance Housing: the Division between Rich and Poor

- 3.1 A home to call one's own

Place History

The land bound by Napier Street, Alexandra and Queens parades and George Street was originally reserved for the Church of England in 1869 (Figure 3). An application to dispose of the land by the church was gazetted in 1877, at which date the land was recorded to have held the foundations of a church and a wooden school building (ministered or occupied by Benjamin Rodda) (VGG 89, 1877:1675).

472 Napier Street

George Pickup, blacksmith, purchased the lot on the north-east corner of Napier Street and Alexandra Parade in 1880 and built the eight room brick house in 1885 (with a Net Annual Value of 40 pounds), which he occupied. In 1890, the NAV increased to 52 pounds, suggesting improvements or additions to the property (RB). Notices in *The Age* from c1891 (31 Jul 1891:8) advertised board and residence available at 472 Napier Street; Pickup remained listed in the rate books as the occupant of the house during this period (RB).

The 1896 and 1900 Melbourne and Metropolitan Board of Works (MMBW) plans show that a building occupied the rear of the property, addressed as 157 Alexandra Parade (since demolished) (Figures 4 & 5).

Upon death of Pickup in July 1905, it was noted that he had worked for Victorian Railways and that his wife Mary Ann Pickup survived him (*Argus*, 28 Jul 1905:1). In 1917, the Pickup family still resided at 472 Napier Street (*Daily Examiner*, 16 Jan 1917:6).

In 1949, the corner property was advertised for sale, described as a two-storey brick house with 8 rooms and a 3 room weatherboard cottage (*Argus* 27 Aug 1949:12; 29 Sep 1893:8). In 1982, 472 Napier Street was designated for use by the Housing Commission (North Fitzroy Parish of Jika Jika plan, Figure 3). In 2017 the house serves Yarra Community Housing Ltd, under the Department of Human Services (VGG 31, 2008).

Elizabeth Terrace (476-484)

Joseph Oliver purchased the land in 1888 and constructed the row of five houses, Elizabeth Terrace, in 1889. Each of the five terrace houses was described in the rate books as a six room brick house (each with a Net Annual Value of 46 pounds), owned by William Reynolds and Joseph J. Oliver (RB). In 1889 four of the five terrace houses were let; to a builder, caretaker, printer and waiter (RB). The 1896 and 1900 MMBW plans show the footprints of the row at this date, with verandahs to the facades and rear elevations and water closets on the rear boundaries (Figures 4 & 5).

Joseph J. Oliver was a local contractor 'of Queens Parade, North Fitzroy', as reported in 1910 (*The Advertiser*, 16 Jun 1910:7). Nothing else is known of Oliver but it is possible that he built the row.

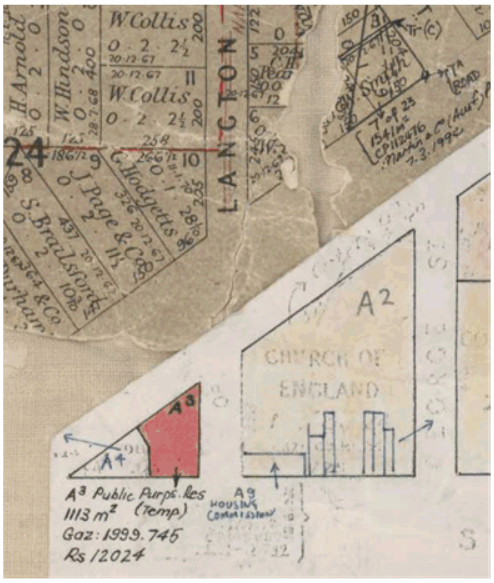

Figure 3. A detail of the North Fitzroy Parish of Jika Jika Plan, showing the block between Napier Street and George Street reserved for the Church of England, and 472 on the southern corner later reserved for the Housing Commission.

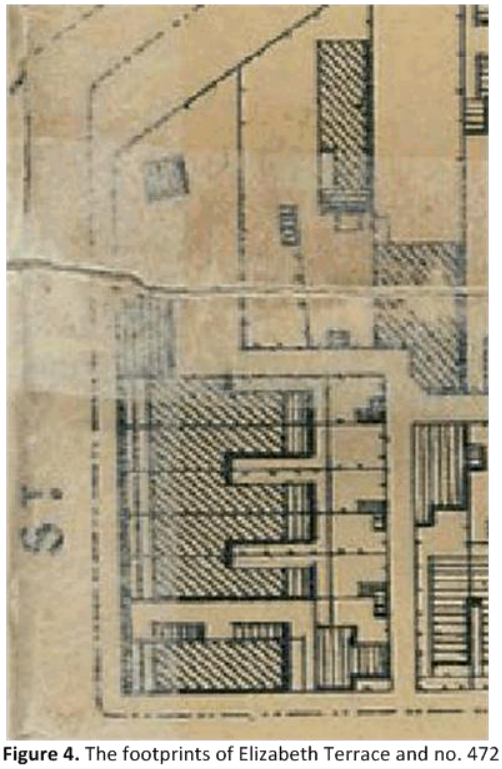

Figure 4. The footprints of Elizabeth Terrace and no. 472 in 1896 (MMBW Base Map 29).

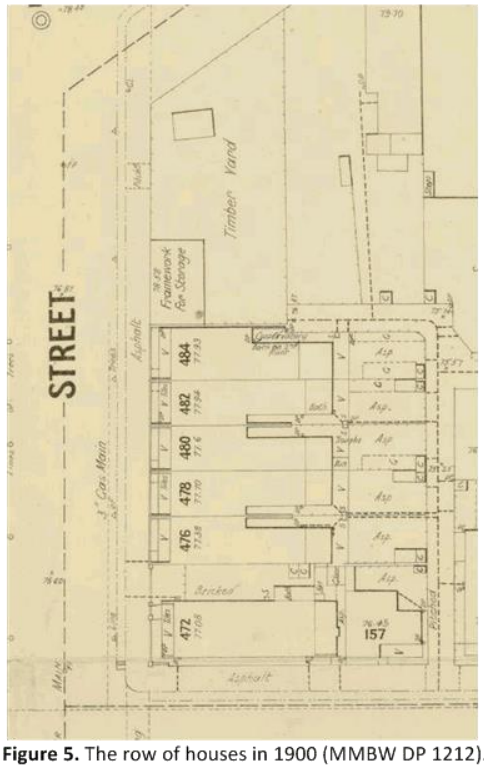

Figure 5. The row of houses in 1900 (MMBW DP 1212).

Description

The terraced houses at 472 and 476-484 Napier Street, Fitzroy North are located on the east side of Napier Street, which runs between Alexandra Parade and Queens Parade, Fitzroy North. They face a grassed triangular allotment which is located at the junction of these two main roads.

Elizabeth Terrace, 476-484 Napier Street is a row of five attached two-storey polychromatic brick terrace houses with double storey front verandahs, rendered and balustraded parapeted facades that conceal hipped roofs, and rectangular window openings. The terraces are designed as a group with a large and elaborate central arched pediment containing an acroterion ornament, a central name plate displaying the name 'Elizabeth Terraces' and rows of rosettes across all facades. Dividing wing walls, which display consoles and masks, rise to plinths and urns at parapet level. Roofed with corrugated iron, the verandahs are decorated with cast iron friezes and balustrading, and cast iron palisade fences run between wing walls at street level.

The house at 472 Napier Street is a large two-storey polychromatic brick terrace house which is located on the corner of Alexandra Parade. The front elevation to Napier Street has a double storey verandah with decorative cast iron friezes and balustrade, an exposed hipped roof, wing walls and a cast iron palisade fence. The side façade to Alexandra Parade is rendered and painted and contains simple rectangular window openings. A large rendered chimney towards the front of the house is visible from this southern view.

Integrity

The buildings retain a high degree of integrity to the Victorian period in fabric, form and detail. While the buildings have undergone some alterations – including an infill addition to the north side of 472 Napier Street – these do not diminish the ability to understand and appreciate the places as a highly intact example of Victorian terrace housing.

Comparative Analysis

The two-storey terrace houses at 472-484 Napier Street, Fitzroy North are of note as an intact and representative row of residential buildings, constructed in the Victorian period. The size of these two-storey houses, and the extent of the row of terraces, is somewhat unusual for this location in the City of Yarra, which contains predominantly single-storey Victorian terrace rows.

Substantial numbers of terrace houses were constructed within the City of Yarra, and Melbourne more broadly, particularly in the 1880s, and these typically took the form of one or two-storey rows with single or double storey front verandahs and parapeted front facades. Facades were commonly rendered or constructed in decorative polychromatic brickwork. Classical elements, such as cornices, architraves, balustrading, urns, stringcourses and pediments were commonly applied to facades, and window openings were most commonly rectangular.

Within the City of Yarra, large numbers of residential terraces built in the Victorian period are included in the Heritage Overlay, both on an individual basis and as individually significant places within precincts.

Examples of two-storey terraces include:

- 218-220 McKean Street, Fitzroy North (individually significant in North Fitzroy Precinct HO327)
- 39-41 Rushall Crescent, Fitzroy North (individually significant in North Fitzroy Precinct HO327)
- 247-249 Scotchmer Street, Fitzroy North (individually significant in North Fitzroy Precinct HO327)
- 474-480 Canning Street, Carlton North (individually significant in North Carlton Precinct HO326)

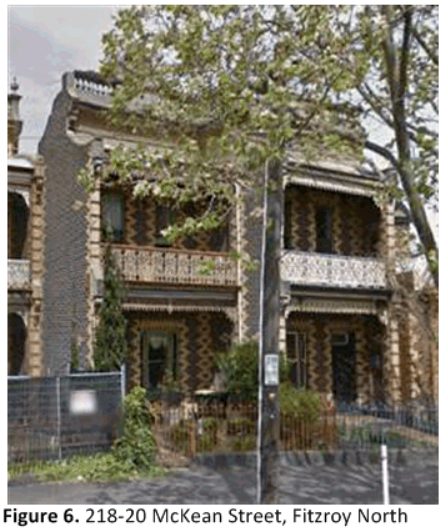

Figure 6. 218-20 McKean Street, Fitzroy North (Google Maps)

Figure 7. 39-41 Rushall Crescent, Fitzroy North (Google Maps)

Figure 8. 247-249 Scotchmer Street, Fitzroy North (Google Maps)

Figure 9. 474-480 Canning Street, Carlton North (Google Maps)

Like the places listed above, the terraces at 472-484 Napier Street, Fitzroy North display a range of characteristics that have associations with the Victorian Italianate style and the terraces remain highly intact to demonstrate these associations.

The terraces at 472-484 Napier Street, Fitzroy North demonstrate the following Victorian Italianate characteristics:

- Two-storey parapeted form (476-484) and unparapetted form (472)
- Front verandahs and balconies with decorative cast iron
- Polychromatic brickwork with cement render detailing
- Balustraded parapets concealing hipped roofs
- Rectangular window openings
- Row of attached buildings with central pediment and name plate
- Dividing wing walls with decorative detailing
- Cast iron palisade fencing and gate.

Assessment Against Criteria

Following is an assessment of the place against the recognised heritage criteria set out in *Planning Practice Note 1: Applying the Heritage Overlay* (July 2015).

Criterion D: Importance in demonstrating the principal characteristics of a class of cultural or natural places or environments (representativeness).

Elizabeth Terrace, 476-484 Napier Street and the terrace house at 472 Napier Street, Fitzroy North are fine and representative examples of terrace housing from the Victorian period. They display typical features of the Victorian Italianate architectural style popular in the 1880s boom period in Fitzroy North and across Melbourne more broadly, including a two-storey form with front verandahs and balconies, polychromatic brickwork, decorative cast iron, rendered decorative elements including balustrading and cast iron palisade fencing.

Criterion E: *Importance in exhibiting particular aesthetic characteristics (aesthetic significance)*

Elizabeth Terrace, 476-484 Napier Street and the terrace house at 472 Napier Street, Fitzroy North are well-considered and carefully detailed examples of Victorian Italianate terrace housing. The row of

polychromatic terraces, with repetitive decorative elements across the facades, presents a picturesque composition of this architectural style.

Grading and Recommendations

It is recommended that the place be included in the Heritage Overlay of the Yarra Planning Scheme as a locally significant heritage place.

Recommendations for the Schedule to the Heritage Overlay (Clause 43.01) in the Yarra Planning Scheme:

External Paint Controls?	No
Internal Alteration Controls?	No
Tree Controls?	No
Outbuildings or Fences not exempt under Clause 43.01-3?	No
Prohibited Uses Permitted?	No
Incorporated Plan?	Incorporated Plan under the provisions of clause 43.01 Heritage Overlay, Planning permit exemptions, July 2014
Aboriginal Heritage Place?	No

Each property should be indentified in 'City of Yarra: Review of Heritage Overlay Areas, Appendix 8' as 'individually significant'.

Extent of the recommended Heritage Overlay

To the property title boundaries, as indicated by the polygon on the aerial below:

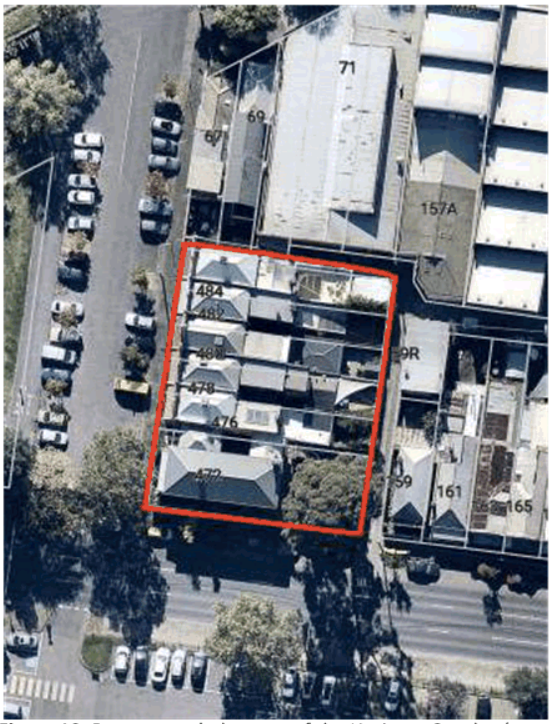

Figure 10. Recommended extent of the Heritage Overlay (nearmaps, May 2017)

Identified by:

Andrew C Ward & Associates (1989), *Collingwood Conservation Study*

> 472-484 Napier Street were identified as the significant Napier Street Streetscape (Streetscape B22) 'proposed as appropriate for special consideration by the Fitzroy City Council in development control procedures.'

GJM Heritage (2017), *Queens Parade Built Form Review*.

References:

City of Fitzroy Rate Books (RB), Central Ward:

Nos. 476-484: 1888, entries 1311-13; 1889, entries 1339-43.

No. 472: 1880, entry 615; 1885, entry 510; 1885, entry 512; 1890, entry 1349; 1892, entry 1360.

Daily Examiner [Grafton, NSW].

Melbourne and Metropolitan Board of Works (MMBW): Base Map 29 (1896), Detail Plan 1212 (1900).

North Fitzroy, Parish of Jika Jika plan.

The Advertiser [Adelaide, SA].

The Age.

The Argus.

Victoria Government Gazette (VGG):

Issue No. 31 Thursday 31 July 2008,
<http://www.gazette.vic.gov.au/gazette/Gazettes2008/GG2008G031.pdf> accessed 24 July 2017.

Issue No. 89, Friday 31 August 1877, page 1675.

Appendix C: St John the Baptist Roman Catholic Church Complex Heritage Citation

gard'ner jarman martin

GJM Heritage

Heritage Citation

ST JOHN THE BAPTIST ROMAN CATHOLIC CHURCH COMPLEX

Address: **61-87 Queens Parade, Clifton Hill**

Prepared by: GJM Heritage

Date: August 2017

Place type: Church complex	**Architects:** J. B. Denny (church), Kempson & Connolly (church spire and school), C. W. Vanheems (hall)
Grading: Locally significant	**Builder:** Swanson Bros (school)
Integrity: Very High	**Construction Date:** 1876 - 1918
Recommendation: Remove from HO330 and include in a site specific Heritage Overlay with fence controls (fence to Queens Parade in front of the Church & Presbytery) and internal controls for the Church as it relates to the Fincham Organ	**Extent of Overlay:** To property title boundaries (refer to Figure 20)

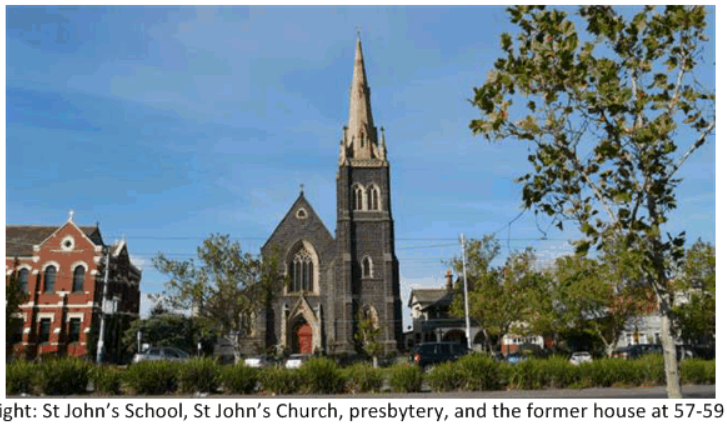

Figure 1. Left to right: St John's School, St John's Church, presbytery, and the former house at 57-59 Queens Parade.

Figure 2. Left to right: Parish Hall, St John's School and St Johns Church

Statement of Significance

What is significant?

St John the Baptist Roman Catholic Church Complex, 61-87 Queens Parade, Clifton Hill, built from 1876 to 1918, comprising the church, school, presbytery, former hall/shops, former presbytery and the Queens Parade boundary fence to the church and presbytery.

Elements that contribute to the significance of the place include (but are not limited to):

- The original external form, materials and detailing of the buildings
- The high level of integrity to their original design
- The Queens Parade fence to the church and presbytery.

Later alterations and additions, particularly the recent addition to the rear of the shops and school, are not significant.

How is it significant?

St John the Baptist Roman Catholic Church Complex, 61-87 Queens Parade, Clifton Hill, is of local historical, architectural and aesthetic significance to the City of Yarra.

Why is it significant?

St John the Baptist Roman Catholic Church Complex, 61-87 Queens Parade, Clifton Hill is a large and intact group of religious buildings, including a church, school, presbytery, hall/shops and former presbytery. This group of buildings is illustrative of a large suburban parish, with place of worship and associated educational and community facilities, established from the late nineteenth century through to the twenty-first century (Criterion A).

St John the Baptist Roman Catholic Church Complex, 61-87 Queens Parade, Clifton Hill is a fine, intact and representative example of a religious complex. It contains buildings from the Victorian and Federation periods which display typical characteristics of a range of styles, including the Victorian Decorated Gothic style of the late nineteenth century (church) and the Gothic Tudor (school), Bungalow (presbytery) and Free Style (hall) of the Federation period (Criterion D).

St John the Baptist Roman Catholic Church Complex, 61-87 Queens Parade, Clifton Hill is a well-considered and carefully detailed complex of church buildings. The church is a richly composed bluestone building with freestone dressings, designed in a Decorated Gothic style. The soaring church spire is a prominent landmark in the Clifton Hill/Fitzroy North area. The church contains several important stained glass windows including works by Ferguson and Urie and Hardman of Birmingham. The school building is an imposing two-storey red brick building, designed in a Gothic Tudor style, and the former Church Hall is a carefully composed two-storey red brick triangular building, designed in a Federation Free Style. (Criterion E).

Historical Themes

The place illustrates the following themes as outlined in the *City of Yarra Thematic History* (1998):

6 Developing Urban Institutions

- 6.1 The Establishment of a Religious and Spiritual Network

Place history

The Parish of Jika Jika plan illustrates that one acre (comprising lots 8, 9, 11 & 12) was reserved for the Roman Catholic Church in 1864, with an 88 metre frontage to Queens Parade, extending to Hodgkinson Street to the south (Figure 3).

In 1864 there were reportedly too few Catholic parishioners in Clifton Hill to warrant a church. By 1875, Thomas Edmund Verge, St John's 'princely benefactor' discovered that a memorial (petition) was being presented to the government that the Catholics were not carrying out their obligation to build in the location, petitioning the government to resume the land for other purposes. Verge's immediate response was to organise a cartload of stone and building materials delivered to the site and have it fenced off, at his own expense. A public meeting was then held at the Clifton Hill Hotel. Only six people attended, but a collection raised £52/17/-, including £25 from Verge himself. The Committee resolved that the proposed church would be 'as magnificent as any to be found anywhere in Australia' (Peterson 1999:38).

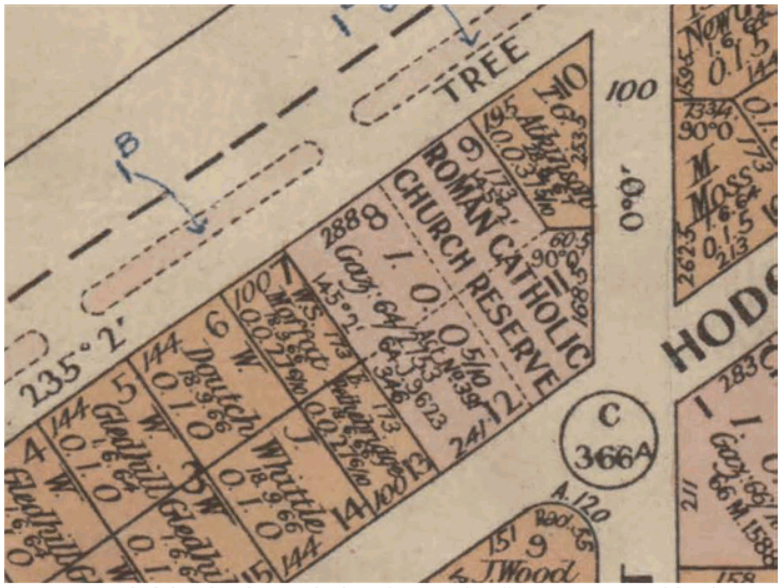

Figure 3. Detail of the Jika Jika Parish Plan, showing the allotments reserved for the Roman Catholic Church.

St John the Baptist Roman Catholic Church

The foundation stone of the church was laid by Archbishop Gould on 27 February 1876. Architect J. B. Denny of the firm Reed, Smart & Tappin designed the church on Puginesque principles. By July 1876 'the work of the foundation was completed' and by 1882 the first stage of works was complete, comprising the eastern aisle and nave. The first mass was held by priests from St Patrick's Cathedral on 19 November 1882. By November 1885 St John's became a separate parish under Reverend M. Carroll (Peterson 1999:38).

The foundation stone for the tower was laid by Archbishop Carr in 1887, at the commencement of the second stage of works to the church (Peterson 1999:38; NTAV; Ward 1995:469). The church was substantially completed by 1893 and on 5 March 1893 the church was dedicated (Figures 4-6) (Allom Lovell 1998:124). In 1903 the Lady Chapel and Sacristy was opened by the Archbishop and in 1906-07 the sandstone spire was completed, designed by architects Kempson & Connolly. The church was blessed in 1933 (Peterson 1999:38).

Stained glass for the church was created by Ferguson & Urie and Hardman of Birmingham (NATV). The church retains a 1924 George Fincham & Sons Pty Ltd organ, which was built to the specification of Melbourne City Organist, Dr W.G. Price. The firm was awarded the contract in December 1922 for an instrument to cost £1800. It is a two-manual organ of 24 speaking stops divided on either side of the rear gallery. The instrument includes a generous complement of romantic registers, including several string and reed stops. It is the second largest Fincham organ in Melbourne from the period to survive intact, retaining its original console with free combination pistons, tubular-pneumatic action and 1,309 pipes. The

instrument survives completely unaltered, apart from the addition of a new blowing plant, retaining its tubular-pneumatic action and voicing (NTAV; Organ Historical Trust of Australia).

George Fincham & Sons, organbuilders

George Fincham (1828-1910) was born in England in 1828 to Jonathon George Fincham, an organbuilder. Between 1842 and 1849 Fincham was apprenticed to London organbuilder Henry Bevington, and then worked as a foreman for James Bishop & Son. He arrived in Victoria in 1852 and set up as an organ-tuner and repairer at 113 Queen Street, Melbourne. Next year he visited the Ballarat diggings but returned in 1854. In 1855 he bought land in Bridge Road, Richmond, where he built his home and a bluestone factory; in 2017 the brick factory remains at 2 Stawell Street, Richmond, just off Bridge Road. For a period, Fincham worked to finance equipment and stock for his factory. By 1862 he was able to start organbuilding; churches then had funds for pipe organs and interest in organ music was stimulated by the newly-arrived organists Charles Horsley, David Lee and Rev. George Torrance. In June 1866 the Victorian government awarded Fincham £100 for organbuilding with colonial materials and metal pipes of his own manufacture. In 1878 he bought the firm of Lee & Kaye, and in 1881 established an Adelaide branch, managed by Arthur Hobday, his ex-apprentice, until it was sold in 1894 to Josiah Dodd. In 1904 he opened a branch in Sydney, while he had agents in Perth (1897) and Brisbane (1902) (Henwood 1972).

Fincham's first organ was of ten stops; at the same time he built an organ of seventeen stops. This was the first organ of any considerable size built in the colony and was opened in the factory on 21 December 1864. His organ for the Melbourne Exhibition in 1866 won an award, the first of many for his work. In 1879 his tender for an organ for the Melbourne Exhibition Building was accepted against overseas competition. The four-manual organ of seventy speaking stops was completed for the 1880-81 Melbourne International Exhibition, and remained in use in the building for nearly fifty years (Henwood 1972).

Altogether Fincham built about 200 organs for cathedrals and churches and supplied pipe work and parts to organbuilders throughout Australasia. His integrity and the quality of the organs he built overcame the prejudice towards colonial work. He was considered outstanding among Australian organbuilders for his skill, his business ability and his readiness to keep pace with modern trends. He patented many improvements; most of the organs he built had mechanical action and from 1886 some had tubular-pneumatic (Henwood 1972).

A number of Fincham organ's remain throughout Victoria, often at prominent churches and buildings. Key examples of his career's work remain at St Ignatius Church Complex, 326-348 Church Street Richmond (HO359, VHR H2146); St Bartholomew's Anglican Church, 290 Burnley Street, Burnley (HO236); Wilson Hall, 156-292 Grattan Street, Parkville (HO361, VHR H1012); Assembly Hall at 156-160 Collins Street, Melbourne (HO580, VHR H418); St Mary's Star of the Sea Church Complex, Howard and Victoria Streets, West Melbourne (HO979, VHR H2182); Church Of All Nations, 180 Palmerston Street, Carlton (HO976, VHR H2179); St Judes Anglican Church on Lygon, Palmerston and Keppel streets, Carlton (HO65, VHR H14); the former College Church, 149 Royal Parade, Parkville (HO312, VHR H394); and the former Congregational Church at 17 Black Street, Brighton (HO75, VHR H724).

Fincham was a member of the Chamber of Manufactures and was a committee member of the Workingmen's College, the Victorian Society of Organists and the Melbourne Philharmonic Society and joined the Metropolitan Liedertafel and Old Colonists' Association, amongst other interests. His business was continued by his descendants (Henwood 1972).

Figure 4. St John's Roman Catholic Church in 1893 (Museums Victoria, Item MM 8213).

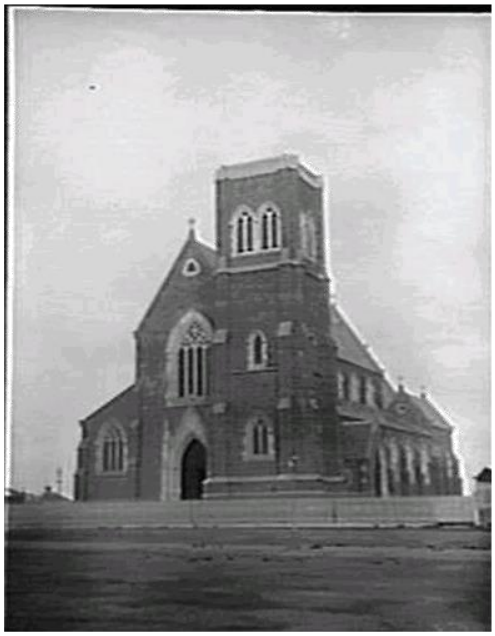

Figure 5. St John's Roman Catholic Church in 1893 (Museums Victoria, Item MM 30074).

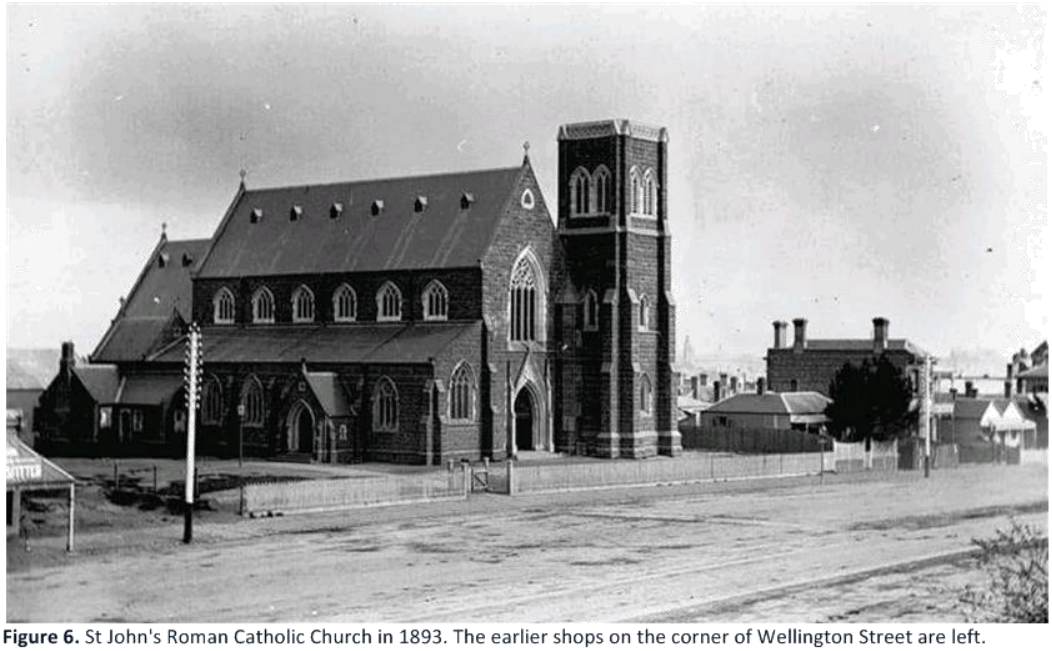

Figure 6. St John's Roman Catholic Church in 1893. The earlier shops on the corner of Wellington Street are left. Immediately right of the church is the earlier house that occupied the site of the current presbytery. Right of this is the two-storey house at 59 Queens Parade which remains today and was acquired by the Church in the 1990s. A timber fence enclosed the church grounds at this date (Museums Victoria, Item MM 8211).

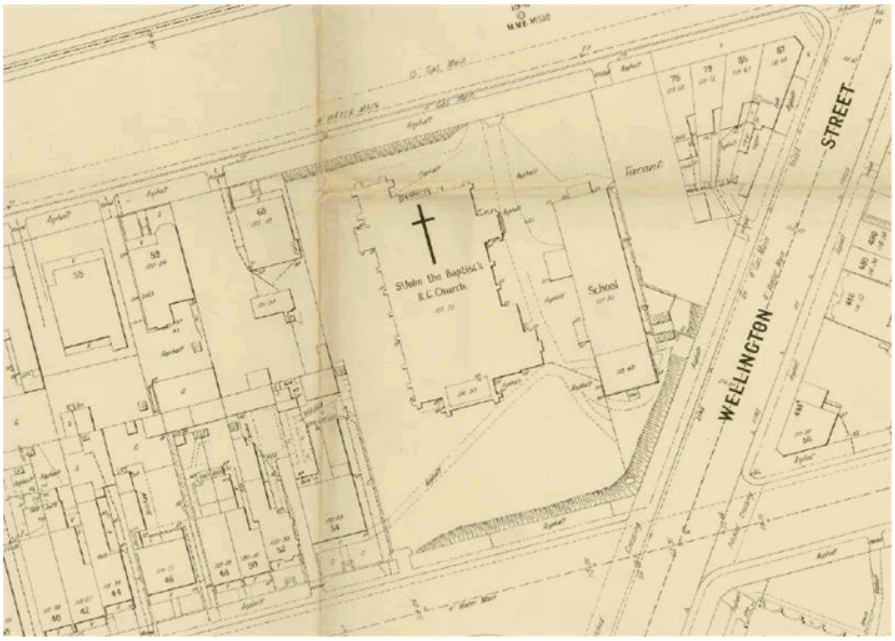

Figure 7. The complex in 1901; buildings that remain in 2017 as part of the church complex are the church, house at 59 Queens Parade and house at 54 Hodgkinsons Street (MMBW DP 1217).

Catholic Primary School, 65 Queens Parade

The first St John's school opened in 1885, held in the church for 80 children. A new school was built in 1889 by Rev. R. Collins, which became the responsibility of the Sisters of Charity in 1894 (their convent was located at the former private residence at 12 North Terrace, built 1883, converted to a convent in 1924). The 1901 Melbourne and Metropolitan Board of Works (MMBW) plan shows the earlier school building on the site, within the boundaries of the original church reserve (since demolished) (Figure 7).

The foundations for the new school were laid on 18 October 1903 by Archbishop Carr. In March 1904 Carr opened a timber building to serve as a temporary school for boys. In 1906, the existing school was opened, designed by architects Kempson & Connolly and constructed by Swanson Bros (Peterson 1999:38).

Presbytery, 61 Queens Parade (and former presbytery at 54 Hodgkinson Street)

When Clifton Hill became a separate parish in 1885 a temporary residence was rented in Delbridge Street to serve as St John's presbytery (Peterson 1999:40). Later, a cottage adjoining the rear of the church property was purchased, which appears to have been 54 Hodgkinson Street (see Figure 7). The Sands & McDougall Directories confirm that no. 54 was occupied by Rev. R. Collins by 1892, and by subsequent clergy until c1914. By 1920 the house at 54 Hodgkinson Street served as a Roman Catholic music school and by 1930 also a library (S&Mc).

The current two-storey presbytery fronting Queens Parade opened on 2 August 1914 by Archbishop Mannix (Peterson 1999:40). The architect is not known.

A cast iron palisade fence with a bluestone plinth and pedestrian and vehicular gates remains along the front boundary of the church, school and presbytery.

St John's Parish Hall (former)

The foundation stone for St John's Parish Hall on the corner of Queens Parade and Wellington Street was laid on 2 September 1917 by Archbishop Mannix and the building constructed in 1917-18 (Ward 1995:474).

The building replaced four earlier brick shops owned by the church, which were evident on the 1896 MMBW plan (Figure 7). The new hall was designed with four shopfronts to Queens Parade, flanking the main entrance to the hall to the rear (Peterson 1999:40; Ward 1995:474).

The hall was designed by architect C. W. Vanheems and opened by the Apostolic Delegate, His Excellency Archbishop Caltaneo on 18 April 1918. In the same year it was leased by Robert McLeish to show films and became an early silent movie theatre. McLeish was a notable figure in the film industry in Victoria – he operated the Northcote, Austral in Collingwood, Rivoli's in Camberwell and East Malvern, and Regal in Hartwell, operating as Junction Theatre Pty Ltd. In 1923 McLeish was president of the Victorian Exhibitors' Association and, previously, of the Green Room Club (Peterson 1999:40).

By 1919-20, the property was owned by the Trustees of the Clifton Theatre (Ward 1995:474). From 1920 that Sands & McDougall Directories note occupants of the four shops (addressed as nos. 79, 81, 85 & 87), including Percy Roberts a confectioner (who remained at no. 79 for many years), a boot shop, estate agent and hairdresser. Clifton Picture Theatre was listed at 83 Queens Parade.

In 1921, Hoyts Pictures took over the lease but attendance fell by 1929. In 1932 McLeish installed sound equipment and re-opened the Clifton Picture Theatre. The hall later became an early specialist ethnic cinema and in 1965 World Film Distributors converted the Clifton into an Italian language cinema. Fire severely damaged the interior in August 1970 and after refurbishment, the Cinema Italia re-opened on 21 December 1970. The cinema closed in 1983. In 1999 the hall and fly gallery at the rear were demolished (Peterson 1999:40). In 2017 the building bears the name Aikenhead Hall.

Other church land adjacent to the complex

The church purchased 55 Queens Parade in 1975 (now a carpark; part of 57-59 Queens Parade) and the Victorian house at 57-59 Queens Parade in 1993. In 2011, 57-59 Queens Parade was transferred into the ownership of The Salesian Society (Vic) Inc (LV:V6829/F712; V7097/F318).

Description

The complex of buildings at St John the Baptist Roman Catholic Church is located on a large site at the intersection of Queens Parade and Wellington Street, Clifton Hill. This site has a rear frontage to Hodgkinson Street. Fronting Queens Parade, from the west are the presbytery, church, school and the former hall at the corner of Wellington Street. An associated house is located to the south of the site, facing Hodgkinson Street.

Church

Facing Queens Parade, the church is a bluestone building with freestone dressings, designed in a Decorated Gothic style (see Figure 8). It comprises a six-bay nave, with clerestorey windows, side aisles and a tall tower with spire to the front north-west corner. Aisle and clerestorey windows are generally triple lancet with intersecting tracery, and the main front window contains four lancets with an oculus above. A front portal is of elaborately carved freestone with side pinnacles and a bas-relief tympanum. The spire rises from a three stage, buttressed tower with lancet windows and a pierced parapet, and is supported by flying buttresses at the angles. Four corner chapels, two side porches, a sacristy and octagonal Lady Chapel complete the composition.

The church contains several important stained glass windows including works by Ferguson and Urie and Hardman of Birmingham, and the gallery contains the largest Fincham organ (1924) to survive intact in Melbourne (Figure 9).

A cast iron palisade fence on a bluestone base lines the Queens Parade frontage of the church and adjacent presbytery and school.

Figure 8. Church and presbytery (right of church) (GJM Heritage, July 2017).

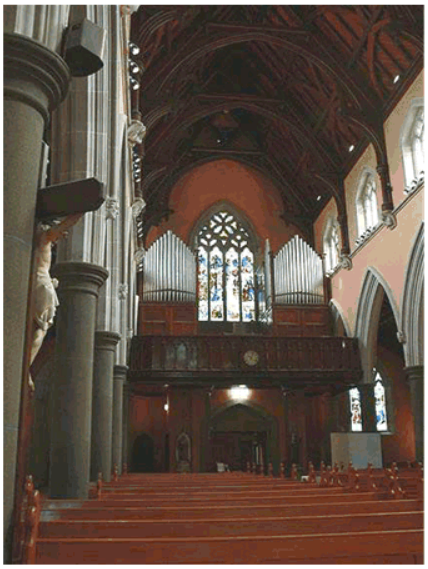

Figure 9. The 1924 Fincham organ in the church (Organ Historical Trust of Australia).

Presbytery

St John's Presbytery is a two-storey Federation Bungalow house, built of red brick with roughcast render contrast and a slate roof (see Figure 8). The main roof is hipped with small top gablets and two secondary gabled bays facing the front (Queens Parade) and the side. The hipped roof continues over a two storey, L-shaped verandah which runs between these gabled bays. This verandah has a timber (or shingled), bellcast upper balustrade and large semi-circular arcading below.

School

St John's Catholic School is a two-storey red brick building with slate roof, designed in a Gothic Tudor style (see Figure 10). Facing Queens Parade, the front façade is symmetrical and of tripartite composition with recessed central bay, containing a pedimented entrance with crocket, cusped fanlight and embossed name. Flanking side bays have gabled parapets containing occuli. Heavy buttresses divide the façade into seven bays containing rectangular windows with flat label moulds at ground level and pointed segmental-headed windows with label moulds above. Central eaves are lined with a stylised dentilled moulding and the transverse hipped roof has terracotta ridging and finials.

Parish Hall (former)

St John's Parish Hall originally consisted of four shops in Queens Parade which flanked the entrance to a rear hall/theatre. Although the rear hall/theatre has been demolished, the four shops and hall entrance remain on the Queens Parade frontage.

The remaining structure is a two-storey red brick triangular building with rendered dressings, designed in a Federation Free Style. The composition of the main Queens Parade façade is of five parts, with three canted oriel windows at first floor level and pairs of arch-headed windows inbetween. The façade is articulated with pilasters, stringcourse and cornice line, and the plain parapet is broken with two curved sections. The central bay contains the entrance to the former hall/theatre and presently bears the name 'Aikenhead Hall'. The façade continues along Wellington Street with repetition of detailing such as the stringcourse, cornice line, simple parapet and arch-headed windows at first floor level. Modifications have been made to the shopfronts however tiling, and the form of some shopfronts, have been retained.

Figure 10. Former Parish Hall (left) and School (right) (GJM Heritage, July 2017)

Former presbytery (54 Hodgkinson Street)

The Former St John's Presbytery is a single-storey Victorian brick (?) house with a substantial red brick parapeted addition to the Hodgkinson Street frontage, dating c1920s-30s. This addition has a terracotta tiled gable roof and rendered wall and three rectangular double-hung windows facing the school yard to the east. The general form of the original house has been retained however it appears that modifications were made to the fabric at the time when the large addition was constructed, including re-roofing with terracotta tiles to match the addition.

Figure 11. Former presbytery (Google, Dec 2016)

Integrity

St John the Baptist Roman Catholic Church Complex retains a very high degree of integrity to the early twentieth century in fabric, form and detail. This is particularly apparent at the church, school, presbytery and the remaining section of the hall building. While these buildings have undergone some alterations to various degrees – including demolition of the hall/theatre behind the shops in Queens Parade - these do not diminish the ability to understand and appreciate the place as a highly intact example of a church complex.

The former presbytery at 54 Hodgkinson Street has undergone additions and alterations in the 1920s/1930s and this building has a lesser degree of integrity, however the form of the earlier building can be understood and appreciated.

Comparative Analysis

St John the Baptist Roman Catholic Church Complex comprises a church, school, presbytery, hall and former presbytery building on a single site in Queens Parade, Clifton Hill. These buildings were constructed in the period from 1876 to 1918 and they remain to form a fine complex of church buildings in the City of Yarra.

A large number of churches dating from the late nineteenth and early twentieth centuries remain in the City of Yarra, however fewer complexes of church buildings remain. Church complexes in the Heritage Overlay of the Yarra Planning Scheme include:

- St Joseph's Roman Catholic Church Complex, Otter Street, Collingwood (HO117-HO120)
- St Brigid's Roman Catholic Church, 378 Nicholson Street, North Fitzroy (within North Fitzroy Precinct HO327)
- St Bartholomew's Church Complex, 290-300 Burnley Street, Richmond (HO236).

St Joseph's Roman Catholic Church Complex, Otter Street, Collingwood

St Joseph's Roman Catholic Church Complex comprises the following buildings:

- Church (1861, completed 1875 and 1891)
- School (1886), converted to a hall after construction of new school buildings
- Presbytery (1903)

- School (1904 and 1912) (Petersen).

These are all included in the City of Yarra Heritage Overlay (with separate overlay numbers HO117-HO120) as buildings of individual significance.

Figures 12-15. Former St Joseph's Catholic Church (after fire), presbytery, school and former school (Victorian Heritage Database)

St Brigid's Roman Catholic Church, 378 Nicholson Street

St Brigid's Roman Catholic Church Complex comprises the following buildings:

- Church (1870, enlarged 1881 and 1885)
- Presbytery (c1880/90)
- School (c1870/90).

Figures 16-18. St Brigid's Catholic Church, presbytery and school (Victorian Heritage Database)

St Bartholomew's Anglican Church Complex, 290-300 Burnley Street, Richmond

This church complex comprises the following buildings, all constructed in 1925-26 to designs by Gawler and Drummond:

- Church
- Hall
- Vicarage.

Figure 19. St Bartholomew's Anglican Church Complex (Victorian Heritage Database)

Summary

Like the places listed above, the St John the Baptist Roman Catholic Church Complex retains a highly intact group of church-related buildings. These buildings display a range of characteristics that have associations with a range of periods and architectural styles and remain highly intact to demonstrate these associations.

Assessment Against Criteria

Following is an assessment of the place against the recognised heritage criteria set out in *Planning Practice Note 1: Applying the Heritage Overlay* (July 2015).

Criterion A: Importance to the course or pattern of our cultural or natural history (historical significance).

St John the Baptist Roman Catholic Church Complex, 61-87 Queens Parade, Clifton Hill is a large and intact group of religious buildings, including a church, school, presbytery, hall/shops and former presbytery. This group of buildings is illustrative of a large suburban parish, with place of worship and associated educational and community facilities, established from the late nineteenth century through to the twenty-first century.

Criterion D: Importance in demonstrating the principal characteristics of a class of cultural or natural places or environments (representativeness).

St John the Baptist Roman Catholic Church Complex, 61-87 Queens Parade, Clifton Hill is a fine, intact and representative example of a religious complex. It contains buildings from the Victorian and Federation periods which display typical characteristics of a range of styles, including the Victorian Decorated Gothic style of the late nineteenth century (church) and the Gothic Tudor (school), Bungalow (presbytery) and Free Style (hall) of the Federation period.

Criterion E: Importance in exhibiting particular aesthetic characteristics (aesthetic significance)

St John the Baptist Roman Catholic Church Complex, 61-87 Queens Parade, Clifton Hill is a well-considered and carefully detailed complex of church buildings. The church is a richly composed bluestone building with freestone dressings, designed in a Decorated Gothic style. The soaring church spire is a prominent landmark in the Clifton Hill/Fitzroy North area. The church contains several important stained glass windows including works by Ferguson and Urie and Hardman of Birmingham. The school building is an imposing two-storey red brick building, designed in a Gothic Tudor style, and the former Church Hall is a carefully composed two-storey red brick triangular building, designed in a Federation Free Style.

Grading and Recommendations

It is recommended that the St John the Baptist Roman Catholic Church Complex, 61-87 Queens Parade, Clifton Hill, be removed from HO330 Queens Parade Precinct and included in a site-specific Heritage Overlay in the Yarra Planning Scheme in recognition of its significance as an individual heritage place.

Recommendations for the Schedule to the Heritage Overlay (Clause 43.01) in the Yarra Planning Scheme:

External Paint Controls?	No
Internal Alteration Controls?	Yes – Church only as it relates to the Fincham Organ
Tree Controls?	No
Outbuildings or Fences not exempt under Clause 43.01-3?	Yes – Queens Parade fence only
Prohibited Uses Permitted?	Yes
Incorporated Plan?	No
Aboriginal Heritage Place?	No

Extent of the recommended Heritage Overlay

To the property title boundary, as indicated by the polygon on the aerial below.

Figure 20. Planning Maps Online (accessed 25 August 2017) – extent outlined in red

Identified by:

Andrew C Ward & Associates (1989), *Collingwood Conservation Study*.

Andrew Ward (1995), *Collingwood Conservation Study Review.*

References:

Allom Lovell & Associates (1998), *City of Yarra Heritage Review*.

Andrew C. Ward & Associates (1995), *Collingwood Conservation Study Review*.

Henwood, John (1972), 'Fincham, George (1828–1910)' in *Australian Dictionary of Biography*, <http://adb.anu.edu.au/>, published first in hardcopy 1972, accessed online August 2017.

Jika Jika Parish Plan

Land Victoria (LV), Certificates of Title, as cited above.

Melbourne and Metropolitan Board of Works (MMBW) Detail Plan No. 1217, dated 1901.

Museums Victoria online image collection, <https://collections.museumvictoria.com.au/>, accessed July 2017. Images as cited.

National Trust of Australia, Victoria (NTAV), citation for St John The Baptist Roman Catholic Church, via Victorian Heritage Database < http://vhd.heritagecouncil.vic.gov.au>, accessed July 2017.

Organ Historical Trust of Australia, 'St John the Baptist Catholic Church, Queens Parade, Clifton Hill', <http://www.ohta.org.au/organs/organs/CliftonHillRC.html>, accessed August 2017.

Peterson, Richard (1999), *Brimstone to Bunyip: churches of Collingwood, Clifton Hill and Abbotsford, 1852-1999*, Abbotsford [Vic].

Sands & McDougall Directories (S&Mc).

Appendix D: Former Fire Station (No.11) Heritage Citation

GJM Heritage

Heritage Citation

FIRE STATION NO. 11 (FORMER)

Address: **662 Smith Street, Clifton Hill**
Prepared by: GJM Heritage
Date: August 2017

Place type: Fire station	**Architects:** Not known
Grading: Individually significant within HO317	**Builder:** Not known
Integrity: Very High	**Construction Date:** 1884

Figure 1. Former Fire Station No. 11 at 662 Smith Street, Clifton Hill

GJM Heritage

Statement of Significance

What is significant?

The Former Fire Station, 662 Smith Street, Clifton Hill, built in 1884 as the Clifton Hill Temperance Fire Brigade.

Elements that contribute to the significance of the place include (but are not limited to):

- The building's original external form, materials and detailing
- The building's high level of integrity to its original design.

Later alterations and additions are not significant.

How is it significant?

The Former Fire Station, 662 Smith Street, Clifton Hill is of local historical and architectural significance to the City of Yarra.

Why is it significant?

The Former Fire Station, Clifton Hill is of historic significance as an early and rare example of a suburban fire station which pre-dated the establishment of the Metropolitan Fire Brigade (created through the *Fire Brigades Act 1890*). The building demonstrates the period of voluntary fire brigades established prior to the formalisation of the metropolitan fire service (Criterion A & B).

The Former Fire Station, Clifton Hill is a fine and representative example of a suburban fire station. The large arch-headed driveway entrance with panelled doors, and adjacent office entry at street level, are features that clearly demonstrate the use of the building as a nineteenth century fire station (Criterion D).

Historical Themes

The place illustrates the following themes as outlined in the *City of Yarra Thematic History* (1998):

5 Local Council and Council Services

- 5.2 Civic Buildings

Thematic context

The first known fire brigade in Melbourne was the Volunteer Melbourne Fire Prevention Society, which was established in 1845. Over the years a number of volunteer brigades were formed, bearing the names of insurance companies, municipalities and other institutions such as the Carlton Brewery and the Fitzroy Temperance. By 1890 there were 56 such volunteer brigades. The *Melbourne Fire Brigades Act 1890* led to the establishment of the Melbourne Fire Brigade and the disbanding of these groups in 1891, although they were invited to join the newly created Melbourne Fire Brigade.

Fire stations have historically been situated all over what is now the City of Yarra, classed as 'A District' and 'B District'. There were four 'B District' stations: No. 10 was located on Hoddle Street, Abbotsford (opened 1891, closed 1966); No. 16 on Somerset Street, Burnley (opened in 1907 and closed in 1916); No. 17 in Lord Street, Richmond (opened 1893, replaced by a new station in Church Street in 1966); and No. 11 was located at 662 Smith Street, Clifton Hill. The reel shed for all these stations was behind the Richmond Town Hall (Allom Lovell 1998: 56).

Place history

The *Fitzroy City Press* reported on 13 December 1884 (13 Dec 1884:3) that the 'recently formed Clifton Hill Temperance Fire Brigade formally opened their new station on Thursday last', 'erected on a piece of ground with a frontage of 19 feet to Smith-street and a depth of 90 feet.' The station was 'constructed of

brick, and has cost, including the ground, about £300.' Mr S. Mauger was noted as the Superintendent of the brigade and Mr J. Trovena the Captain.

A photo dating to 1893 showed a hose cart and firemen in front of the Clifton Hill Fire Station (Figure 2). An annotation listed each fireman's name and W. A. Griff as Officer in Charge. The 1901 Melbourne and Metropolitan Board of Works (MMBW) plan shows the building with a vehicular entrance off Smith Street and outbuildings to the rear of the building (Figure 3). Stables appear to have been located at the rear of the main building. An oblique aerial dating to c1925-c1935 showed the façade during this period (Figure 4).

An article published in the *Fitzroy City Press* in March 1913 (8 Mar 1913:3) reported on the impending closure of the Clifton Hill Fire Station on Smith Street, near Queen's Parade, along with 'several others within the suburban radius'. Headquarters were to move to the St George's Road, North Fitzroy station.

The facade of the building has been recently restored and the paint removed from the brickwork.

Figure 2. Clifton Hill Fire Station in 1893 (SLV Image H8738).

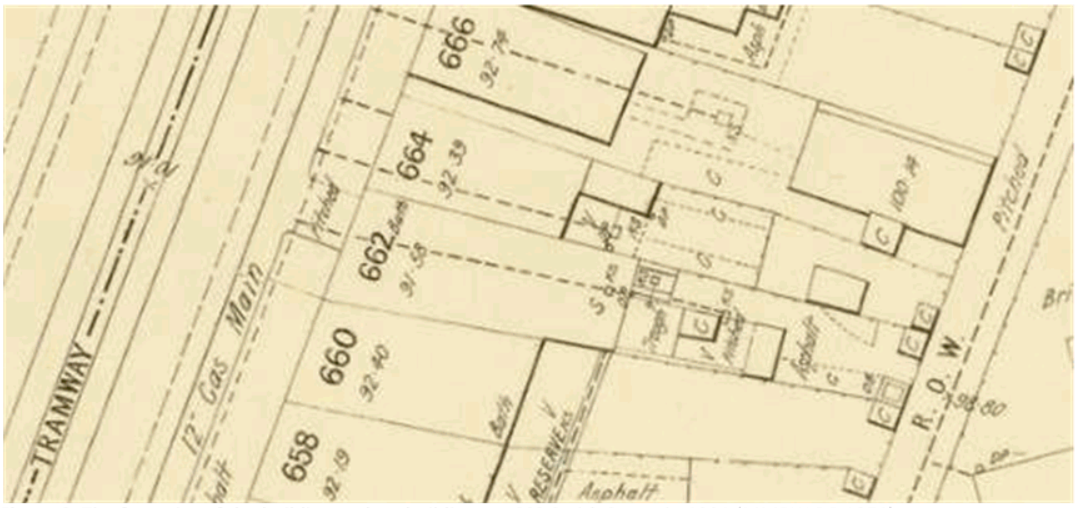

Figure 3. The footprint of the building and outbuildings at 662 Smith Street in 1901 (MMBW DP 1271).

Figure 4. Detail of a c1925-c1935 oblique aerial showing 662 Smith Street (green arrow) (SLV Image H91.160/447).

Description

The Former Fire Station, 662 Smith Street, Clifton Hill is one of a row of two-storey buildings situated on the east side of Smith Street. It is flanked by a row of five two-storey Victorian commercial premises to the south and a recent commercial building to the north. There are no front or side setbacks.

The facade of the fire station is of tuckpointed, polychromatic brickwork with the main body of brown brick, bands of red brick and cream brick window voussoirs. An elaborate rendered parapet above contains a central raised balustraded section and flanking sections comprised of circular motifs. Four urns sit atop dividing pedestals and the whole parapet is supported on pairs of rendered consoles. The upper façade contains three rectangular double hung windows with chamfered reveals and a row of decorative tiles laid within a red brick band.

The façade at street level contains two arch-headed openings of unequal size, breaking the symmetry of the upper façade. The large flat-arched opening contains four doors with glazed top panels and the

adjoining semi-circular arched opening contains a single panelled door with glazed fanlight. A tiled red brick band, matching that of the upper façade, forms an impost for cream brick voussoir arches which frame both openings.

Integrity

The building retains a high degree of integrity to the Victorian period in fabric, form and detail. While the building has undergone some alterations – including to the rear – these do not diminish the ability to understand and appreciate the place as a highly intact example of a Victorian fire station.

Comparative Analysis

Built in 1884 by the Clifton Hill Temperance Society, this fire station pre-dates the establishment of the Metropolitan Fire Brigade in Melbourne. It is an early, rare and intact example of a volunteer fire brigade building in the City of Yarra.

A small number of fire station buildings are included in the Heritage Overlay of the Yarra Planning Scheme and these all date from the period after the establishment of the Metropolitan Fire Brigade in Melbourne. They include:

- Former Carlton Metropolitan Fire Station, 131 Amess Street, Carlton North
- Former Fire Station, 131 Lord Street, Richmond
- Former Richmond Metropolitan Fire Station, 154 Somerset Street.

Former Carlton Metropolitan Fire Station, 131 Amess Street, Carlton North (included in North Carlton Precinct HO326)

This fire station was opened as the No 7 station in 'A District' in 1893, soon after the introduction of the *Melbourne Fire Brigades Act 1890* and the subsequent establishment of the MFB. It was subsequently closed in 1915 (Allom Lovell & Associates (1998), *City of Yarra Heritage Review Thematic History*: 56).

This simple two-storey parapetted, red brick and render (overpainted) building is considered to contribute to the North Carlton Precinct (HO326) as a building of individual significance.

Figure 5. Former Carlton Metropolitan Fire Station (Google Maps)

Former Fire Station, 131 Lord Street, Richmond (HO274)

This fire station was built as the No 16 station in 'B District' in 1893 to designs by prominent Melbourne architect J A B Koch, soon after the introduction of the *Melbourne Fire Brigades Act 1890* and the

subsequent establishment of the Metropolitan Fire Brigade. It was replaced by a new fire station in Church Street in 1966 (Allom Lovell & Associates (1998), *City of Yarra Heritage Review Thematic History*: 56).

Located in a predominantly residential location, this two-storey late Victorian red brick domestic-scaled building is considered to have historical and architectural significance to the City of Yarra. The exterior of the building remains largely intact.

Figure 6. Former Fire Station, Richmond (Google Maps)

Former Richmond Metropolitan Fire Station, 154 Somerset Street (HO404)

This fire station started operation as the No 16 station in 'B District' in 1907 and was possibly designed by Melbourne architect Percy Oakden, who was appointed architect to the Metropolitan Fire Brigade in 1899 (City of Yarra Citation for HO404). It is a two-storey building with gabled residential wing and attached first floor balcony that is considered to have historical and architectural significance to the City of Yarra.

Alterations have been made to the building (including the replacement of main entry doors) however it is considered to remain recognisable as a former fire station.

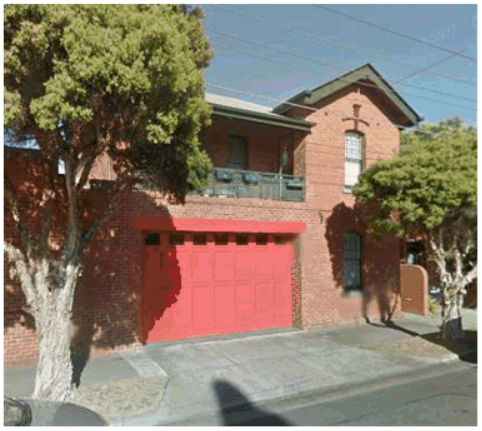

Figure 7. Former Richmond Metropolitan Fire Station (Google Maps)

Fire Station No 11, 662 Smith Street, Clifton Hill predates these examples and, in contrast, was built as a voluntary fire station in the City of Yarra before 1890. Similar to these examples, Fire Station No 11 retains its large carriageway and prominent driveway entrance.

Assessment Against Criteria

Following is an assessment of the place against the recognised heritage criteria set out in *Planning Practice Note 1: Applying the Heritage Overlay* (July 2015).

Criterion A: Importance to the course or pattern of our cultural or natural history (historical significance).

The Former Fire Station, Clifton Hill is of historic significance as an early example of a suburban fire station which pre-dated the establishment of the Metropolitan Fire Brigade (created through the *Fire Brigades Act 1890*).

Criterion B: Possession of uncommon, rare or endangered aspects of our cultural or natural history (rarity).

The Former Fire Station, Clifton Hill is of significance as a rare example of a suburban fire station which pre-dated the establishment of the Metropolitan Fire Brigade (created through the *Fire Brigades Act 1890*). The building demonstrates the period of voluntary fire brigades established prior to the formalisation of the metropolitan fire service (Criterion B).

Criterion D: Importance in demonstrating the principal characteristics of a class of cultural or natural places or environments (representativeness).

The Former Fire Station, Clifton Hill is a fine and representative example of a suburban fire station. The large arch-headed driveway entrance with panelled doors, and adjacent office entry at street level, are features that clearly demonstrate the use of the building as a nineteenth century fire station (Criterion D).

Grading and Recommendations

It is recommended that the place be included in HO317 of the Yarra Planning Scheme as an individually significant heritage place.

Identified by:

Graeme Butler and Associates (2007, 2013), *City of Yarra Review of Heritage Overlay Areas*

> Clifton Hill Fire Brigade Station at 662 Smith Street is listed in Appendix 8 (revised 2017) as contributory to the Clifton Hill Western Precinct (HO317).

GJM Heritage (2017), *Queens Parade Built Form Review*.

References:

Allom Lovell & Associates (1998), *City of Yarra Heritage Review Thematic History.*

Fitzroy City Press [Vic.]

Melbourne and Metropolitan Board of Works (MMBW) Detail Plan No. 1271 (1901).

Melbourne Fire Brigade, 'Our History', <http://www.mfb.vic.gov.au/>, accessed 16 August 2017.

State Library of Victoria (SLV), photo collection.

Appendix E: Former Doctor's Surgery & House Heritage Citation

GJM Heritage

Heritage Citation

DOCTOR'S SURGERY AND HOUSE (FORMER)

Address: **105 Queens Parade, Clifton Hill**

Prepared by: GJM Heritage

Date: September 2017

Place type: Doctor's surgery, residence	**Architect:** Not known
Grading: Individually significant within HO330	**Builder:** Not known
Integrity: Very High	**Construction Date:** c1915

Figure 1. 105 Queens Parade, Clifton Hill (GJM Heritage, September 2017).

GJM Heritage

Statement of Significance

What is significant?

The Former Doctor's Surgery and House, 105 Queens Parade, Clifton Hill, built c1915.

Elements that contribute to the significance of the place include (but are not limited to):

- The building's original external form, materials and detailing
- The building's high level of integrity to its original design.

Later alterations and additions to the rear are not significant.

How is it significant?

The Former Doctor's Surgery and House, 105 Queens Parade, Clifton Hill is of local architectural and aesthetic significance to the City of Yarra.

Why is it significant?

The Former Doctor's Surgery and House, 105 Queens Parade, Clifton Hill is a fine and representative example of a Federation house with combined use as a doctor's surgery. It displays typical features of the Federation Arts and Crafts architectural style popular in the first decades of the twentieth century in Clifton Hill and across Melbourne more broadly, including roughcast walls and chimney, both hip and gable roofs, prominent exposed rafters and projecting window bay with fishscale shingle cladding (Criterion D).

The Former Doctor's Surgery and House, 105 Queens Parade, Clifton Hill is a well-considered and carefully detailed example of a Federation Arts and Crafts house. The design, with main hipped roof, prominent front bay and central chimney, presents a picturesque composition of this architectural style, particularly within the commercial context of Queens Parade (Criterion E).

Historical Themes

The place does not illustrate a theme from the *City of Yarra Thematic History* (1998). The place does illustrate the following themes from the *Victorian Framework of Historical Themes* (2010):

8 Building community life

- 8.3 Providing health and welfare services

Place history

The 1901 Melbourne and Metropolitan Board of Works (MMBW) plan indicates that an earlier building and outbuildings, including stables, existed on the site at that time (Figure 2). The Sands & McDougall Directories listed residents at 105 Queens Parade until 1911 when Alfred L. Bishop, cartage contractor, was listed at the address until 1914 (S&Mc). No. 105 was omitted from the Sands & McDougall Directories in 1915, and was listed again in 1916, now occupied by Forbes Mackenzie, physician and surgeon. This strongly suggests that the existing building was constructed in 1915 to serve as a doctor's surgery and residence. Analysis of the architectural style of the building supports this construction date.

A notice in *The Argus* in April 1916 reported that Dr Forbes Mackenzie now practiced at both Clifton Hill and 58 Collins Street (*Argus*, 1 Apr 1916:18). The 1916 electoral rolls confirmed that John Forbes Cock Mackenzie, medical practitioner, also resided at 105 Queens Parade, Clifton Hill. Dr John Forbes Mackenzie occupied 105 Queens Parade until his death 1970 (at the age of 90 years old); Directories confirm that to c1960 Forbes Mackenzie, surgeon occupied 105 Queens Parade, and in 1970 F. Mackenzie, surgeon, was listed (S&Mc; *Argus*, 11 Jul 1921:1; 17 Mar 1943:6; PROV).

In 1940, artist Max Meldrum was awarded the Archibald prize for a painting of Dr J. Forbes Mackenzie (*Argus*, 22 Jan 1941:1). John Forbes Mackenzie was a senior consulting surgeon at St Vincent's Hospital in Melbourne (dates not known) (NLA). In 2017 the building serves as a chiropractic clinic.

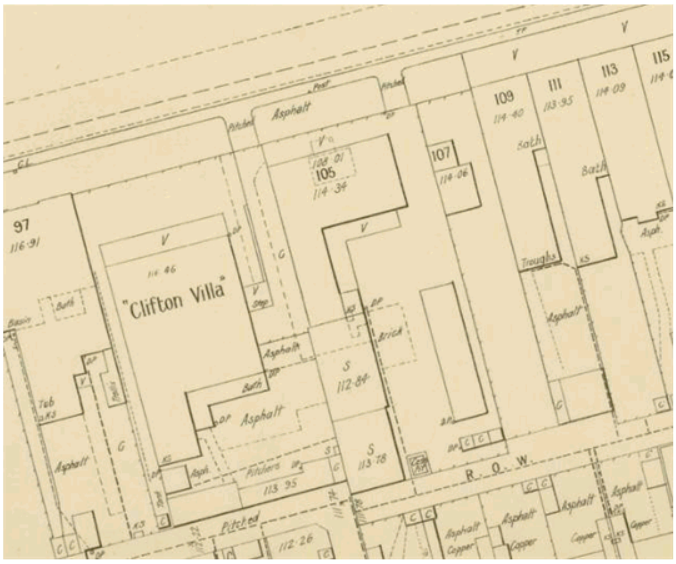

Figure 2. The 1901 MMBW shows an earlier building on the site at 105 Queens Parade (MMBW DP 1217).

Description

The Former Doctor's Surgery and House, 105 Queens Parade, Clifton Hill, is located within a commercial row of buildings which front a service road on the south side of Queens Parade. Unlike the flanking premises, the building has a side setback on the west side and a front setback with low fence of rendered masonry piers and ironwork panels and gate.

Designed in a Federation Arts and Crafts style, the two-storey red brick (overpainted) building is of domestic appearance. The upper façade, above a prominent stringcourse, is of roughcast render and the roof is clad with terracotta tiles. The roof form comprises a front transverse hip, longitudinal rear hip and gable-ends to both the front and rear. Asymmetrically composed, the front façade has a projecting side gable to the east with double-storey rectangular window bay and fishscale shingle cladding, and a smaller projecting hipped bay to the west. Rafter ends are exposed, windows are of simple rectangular form and the recessed central bay contains a corbelled chimney breast at first floor level and plain rendered chimney above. A side entrance is located on the west side of the building.

Integrity

The building retains a high degree of integrity to the Federation period in fabric, form and detail. While the building has undergone some alterations, including overpainting of the exterior brick and render walls, these do not diminish the ability to understand and appreciate the place as a highly intact example of a Federation Arts and Crafts house.

Identified by:

Graeme Butler and Associates (2007, 2013), *City of Yarra Review of Heritage Overlay Areas*

> 'House also doctor's surgery' at 105 Queens Parade is listed in Appendix 8 (revised 2017) as individually significant to the Queens Parade Precinct, North Fitzroy/Clifton Hill (HO330). No Statement of Significance provided.

GJM Heritage (2017), *Queens Parade Built Form Review*.

GJM Heritage

References:

Australian Electoral Roll, Division of Batman, Subdivision of Clifton Hill, 1916, page 60.

Melbourne and Metropolitan Board of Works (MMBW) Detail Plan No. 1217 (1901).

National Library of Australia (NLA), catalogue entry for 'John Forbes Mackenzie'.

Public Records Office of Victoria (PROV), online will & Probate listing for John Forbes Cock Mackenzie VPRS 7591/P4 unit 121, item 704/646.

Sands & McDougall Directories (S&Mc).

The Argus.

Appendix F: Former Clifton Motors Garage, Showroom & Residence Heritage Citation

gard'ner jarman martin

GJM Heritage

Heritage Citation

CLIFTON MOTORS GARAGE, SHOWROOM AND RESIDENCE (FORMER)

Address: **205-211 Queens Parade, Fitzroy North**

Prepared by: GJM Heritage

Date: September 2017

Place type: Motor garage, motor showroom, residence	**Architect:** J. H. Wardrop
Grading: Locally significant	**Builder:** Not known
Integrity: Very High	**Construction Date:** 1920s, 1938-39
Recommendation: Remove from HO330 and include in a site specific Heritage Overlay	**Extent of Overlay:** To property title boundaries

Figure 1. 205-211 Queens Parade, Clifton Hill (GJM Heritage)

Statement of Significance

What is significant?

The Former Clifton Motors Garage, Showroom and Residence, 205-211 Queens Parade, Fitzroy North.

Elements that contribute to the significance of the place include (but are not limited to):

- The building's 1938-39 external form, materials and detailing
- The building's high level of integrity to its 1938-39 design.

How is it significant?

The Former Clifton Motors Garage, Showroom and Residence, Fitzroy North is of local historical and architectural significance to the City of Yarra.

Why is it significant?

The Former Clifton Motors Garage, Showroom and Residence, Fitzroy North is of significance as an early motor garage constructed in a prominent location in Queens Parade, Fitzroy North. Operating as a garage and vehicle-related facility from at least the 1920s until the late twentieth century, the place demonstrates the increasing use of motor vehicles in Melbourne and suburbs, and their need for repair and service, for much of the twentieth century. The Moderne façade in particular illustrates the motoring age of the 1930s (Criterion A).

The Former Clifton Motors Garage, Showroom and Residence, Fitzroy North is a fine and intact example of a distinctive Moderne style motor garage, designed by noted architect James H Wardrop. The building displays typical features of the Moderne architectural style popular in the 1930s, including a dominant central element comprising a series of vertical fins and contrasting decorative horizontal treatment (Criterion E).

Historical Themes

The place does not illustrate a theme from the *City of Yarra Thematic History* (1998). The place does illustrate the following themes from the *Victorian Framework of Historical Themes* (2010):

3 Connecting Victorians by transport and communications

- 3.4 Linking Victorians by road in the 20th century

Place History

The 1904 Melbourne and Metropolitan Board of Works (MMBW) Detail Plan (Figure 2) shows that the land behind the United Kingdom Hotel remained vacant at this date, except for a saw mill. From the 1910s the east side of Queens Parade within this block was occupied by motor, coach, carriage and buggy related businesses. In 1920, Clifton Motors was flanked by a coach trimmer at no. 201-203 and coach and carriage builders, Empire Motor & Carriage Works at no. 213 (S&Mc). Two cast iron posts (possibly the remains of hitching posts) located on the footpath in front of the Clifton Motors building may date to this period.

Nos. 205-207 and 209-211 Queens Parade first appeared in the Sands & McDougall Directories in 1917. No. 205-207 was occupied by George H. Wheeler, asphalter, from 1917 to 1927 (S&Mc). In 1917 no. 209-211 was listed in the Sands & McDougall Directory as 'vacant'.

In 1918 and 1919, J. Markby was listed as the proprietor of the Clifton Motor Garage (S&Mc). A footprint of a proposed building for owner John Markby is shown on the March 1918 Property Sewerage Plan, adjacent to saw mills (Figure 3). In 1919, George Laity purchased 209-211 Queens Parade, which held a timber workshop. In 1921 the Laitys replaced the timber workshop, with a two-storey brick building (remains at the centre of the complex) and an adjoining building to the north – a footprint of the buildings owned by G. A. Laity & Son were shown on the Property Sewerage Plan dated October 1921 (Figure 4) (PSP; Catrice & Summerton 1997:99). In 1922, George Alfred Laity and Lindsay Laity were reported in *The Age* (22 Feb

1922:12) as having carried on 'business at the Clifton Hill motor garage, Queen's Parade'. The Sands & McDougall Directories in 1925 listed the business at 209-211 Queens Parade as George A. Laity & Son motor garage (S&Mc).

Figure 2. The vacant land and saw mill behind the United Kingdom Hotel at 199 Queens Parade in 1904 (MMBW Detail Plan No. 1264).

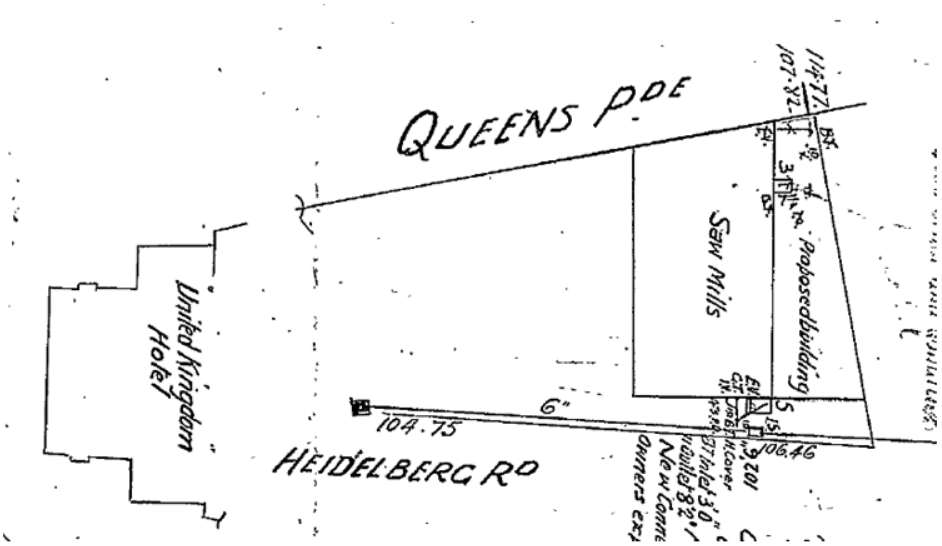

Figure 3. The site of the proposed building for owner John Markby – plans dated 1918 (PSP No. 53990).

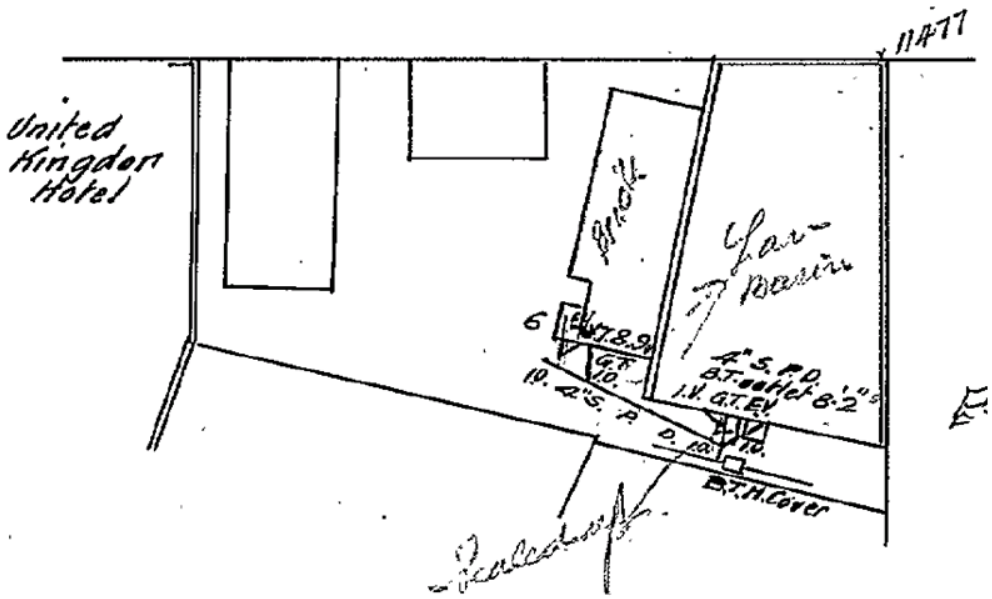

Figure 4. Footprints of buildings on the site in October 1921 for owner Laity – central is the two-storey brick building (No. 9 in the key referred to the bath on the first floor) (PSP No. 53990).

Reflecting the increased popularity of the motor car in the 1920s, Laity's holdings on Queens Parade expanded. In 1926 the Laity's purchased Wheeler's property at nos. 205-207, and by 1927 had constructed a car showroom on the site (Catrice & Summerton 1997:99). In 1928, George A. Laity & Son, motor garage, were addressed at 205 to 211 Queens Parade (S&Mc). The company Clifton Motors Pty Ltd was registered by Lindsay H. Laity in 1931 (*Argus*, 17 Jul 1931:4), before Lindsay became the sole owner of the house, garage and showroom in 1933 (Catrice & Summerton 1997:99). From c1935 Clifton Motors Pty Ltd, motor engineers, was addressed at 205-211, and G. A. Laity (then Linsday H Laity from c1940) was also listed as the occupant of 211 Queens Parade, confirming the Laity's resided in the central dwelling (S&Mc, as cited in Butler 2007:446-8).

An oblique aerial dating (c1925-c1940, but with this evidence, likely) to c1927-c1937 showed the facades of the row of buildings, prior to the construction of the 1938 façade (Figure 5). The central two-storey brick residence is evident, flanked by two industrial buildings, each with a vehicular entrance. The 1935-36 rate books confirm that George Laity and Lindsay Laity owned the showroom at 205-207 and brick house and garage at 209-211 Queens Parade, with respective Net Annual Values of 64 and 170 pounds (RB).

The new Clifton Motors building with its prominent Moderne façade was designed by architect J. H. Wardrop. A notice in *Construction* reported in May 1939 that a 'garage is being erected in Queens Parade, Clifton Hill, Victoria, by Mr. R. Hannah, of Ivanhoe. The architect is Mr. J. H. Wardrop, Bourke Street, Melbourne.'

The 1938-39 works substantially modified the site, with the 1921 house incorporated into a larger workshop and showroom, with the Moderne façade unifying the combined buildings (Catrice & Summerton 1997:99). The ground floor layout retained the earlier design (evident in Figure 5) of two vehicular entrances providing a drive through, which cut through the ground floor of the central house. The 1938-39 rate books reflected the major works to 205-211 Queens Parade, which now had an increased combined NAV of 405 pounds (RB). A Property Sewerage Plan dated December 1939 shows a footprint of the three buildings on the site at this date (Figure 6). The sewerage works shown were conducted for owner and

occupant L. H. Laity by agent S. J. Howden of Surrey Hills (PSP); Howden is known to have been a plumber in 1939 (*Argus*, 28 Nov 1939:6).

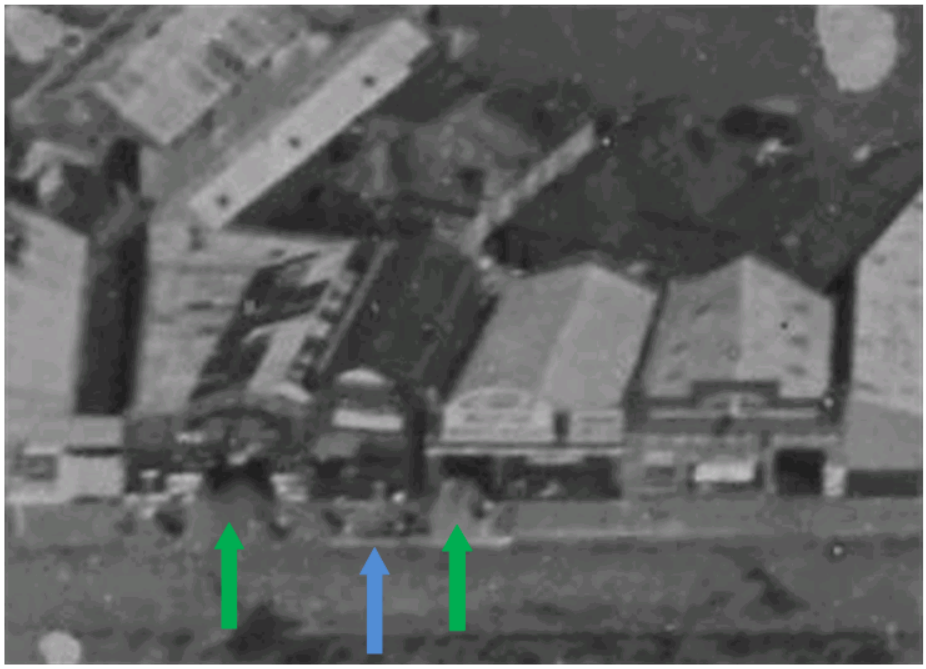

Figure 5. Detail of an aerial dating to c1927-c1937, showing the central two-storey house (blue arrow) and the flanking buildings with their driveway entrances (green arrows) (SLV Image H91.160/1611, c1925-c1940).

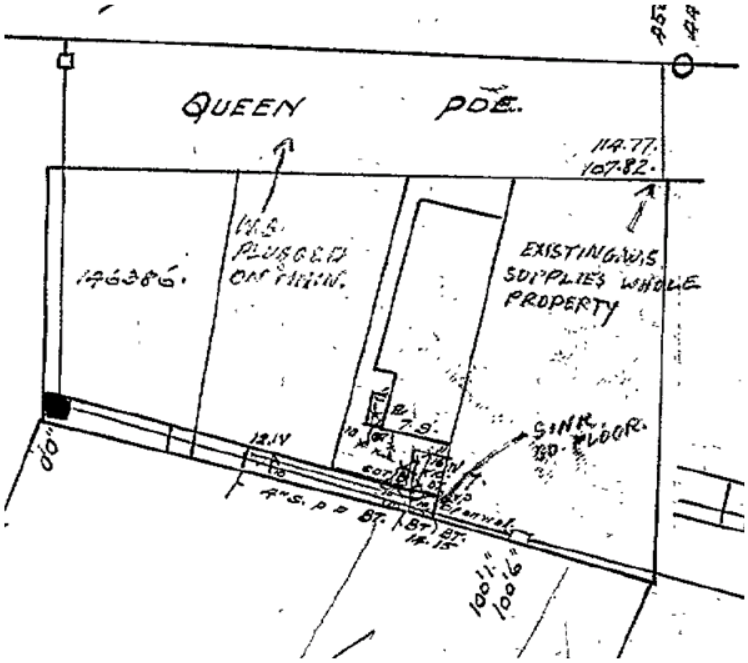

Figure 6. The property and footprint of existing buildings in 1939 – plan dated December 1939 (PSP No. 53990).

Lindsay H. Laity remained the owner of Clifton Motors Pty Ltd showroom and garage until 1944, before Edwin McKeown became the proprietor in 1945 (RB; Catrice & Summerton 1997:99). In 1949, George Alfred Laity and Lindsay Harold Laity (motor engineer) both resided at 454 Queens Parade, opposite the garage (AER).

In April 1954, Clifton Motors addressed at 205 Queens Parade, was advertised for sale by auction. The property was described as a 'garage, service station and mod flat', a 'lucrative old established business, one of the best of its kind in the Nthn district' (*Age*, 3 Apr 1954:31). Rate books indicate that R. Bayford Pty Ltd had purchased the building prior to this sale ad in 1953, with McKeown remaining the occupant (Catrice & Summerton 1997:99). In August 1954, Bayford Motors advertised at 205 Queens Parade (*Argus*, 14 Aug 1954:38). A 1955 ad for Bayfords, 'Melbourne's oldest and most experienced Ford dealers', noted locations at Clifton Hill and 75 Victoria Parade, Fitzroy (Figure 7). In 1955 the property was modified (details not confirmed, but appear to be internal as the façade remains intact in 2017). Bayford sold to Tru-Mould Tyre Service in 1964 (Catrice & Summerton 1997:99). From the mid-2000s the place served as a restaurant, and in 2017 is vacant.

Figure 7. A 1955 ad for Bayfords Motors, located at 205 Queens Parade, Clifton Hill and 75 Victoria Parade, Fitzroy (*Argus*, 28 Jul 1955:42).

Motor garages and showrooms

The following is taken from Robin Grow's *Melbourne Art Deco* (2014:99-100):

Speed and Art Deco styling: what a wonderful combination! 'Faster! Faster!' became the catchcry of the Art Deco period, as the world rushed headlong into the Jet Age...

The automobile was the future and became sleeker and faster as body shapes were rounded, running-boards disappeared, and extrusions (such as headlights and spare tyres) incorporated into the body work. The rise of the automobile transformed the layout and form of cities around the world. Cars had to be serviced, refuelled, repaired and stored and a number of new building types, including service stations,

showrooms, repair shops, parking garages and entrances to road tunnels, emerged to service the motor industry. They represented an opportunity for architects to produce some advanced and highly stylish designs...

Glamorous and elegant showrooms were constructed to meet the growing needs of an intensely competitive motor market and were adorned with Modernistic forms and detailing designed to show the shiny vehicles at their very best. Melbourne's most impressive was Melford Motors (1936), designed by prolific architect, Harry Norris, for the Ford Company ... located on a prominent corner site on wide Elizabeth Street... A prominent example was Clifton Motors in Queens Parade, Clifton Hill, designed by James Wardrop.

J. H. Wardrop, architect

James Hastie Wardrop (1891-1975) formed a partnership with Philip Hudson (1887-1952) c1919. Early commissions included the St Kilda Army & Navy Club in Acland Street, St Kilda (1923) and the Members' Luncheon Room at the Moonee Valley Racing Club (1923). In 1924, Hudson and Wardrop won first prize in the international competition for the design of the national war memorial, Melbourne's Shrine of Remembrance (completed in 1934), which became the city's most important public monument. In 1929 the firm was joined by Kingsley Ussher (1899-1973) (Statham 2012:34), however, the firm dissolved in 1932 (*Argus*, 30 Sep 1932:1).

Wardrop continued to practice and completed a range of commissions, the most notable of which were Moderne in style. Key Moderne examples are Alkira House at 18 Queen Street, Melbourne (1936-37; HO729, VHR H397), the former United Kingdom Hotel at 199 Queens Parade (1938; HO92, VHR H684), and the former Clifton Motors Garage and Showrooms at 205-211 Queens Parade, Clifton Hill (1938-39). In 1939 Wardrop is also known to have remodeled the Orrong Hotel in Armadale (*Construction*, 5 Apr 1939:20). Known residential commissions were Zaneth Manor at 33 Brighton Road, St Kilda (1930; HO7); 24 Murchison Street, St Kilda East (1933; HO391); 4A Meredith Street, Elwood (c1934; HO365); the flats at 125-135 Mooltan Street, Travancore (1939; HO133) and later, the house at 28 Carmichael Street, Hamilton (1956).

Description

The Former Clifton Motors Garage, Showroom and Residence is located on the south-east side of Queens Parade, Clifton Hill, near the northern end of Hoddle Street. The complex comprises three gable roofed buildings and a two-storey façade which was applied across the Queens Parade frontage.

Designed in a Moderne style, the front façade is symmetrically composed about a central vertical element, with flanking parapetted horizontal wings. Strong horizontal and vertical lines dominate the composition with horizontal bands of cream and slim dark glazed brick, large signage panels and curved, ribbed concrete elements, contrasting with a vertical group of three tall rendered fins which rise from the centre of the façade. Square brown glazed tiles line both the lower façade and the vertical elements that flank the central concrete fins.

The tripartite facade corresponds to three earlier buildings behind – a central residence, showroom to the right and garage to the left. At street level the façade contains three banks of windows, two large openings and a pair of entrances with cream tiled frames and curved cantilevered hoods. Openings retain glazed brick sills and tiled reveals, but windows have been replaced. At first floor level, the dominant central element is flanked by large, brick-framed signage panels set in the brick façade and a simple banded parapet of horizontal ladder-form brown brick.

The forms of the pre-1938 brick buildings, behind the front façade, are clearly evident from the rear of the property. These structures include a central, two-storey red brick building with terracotta tiled gable roof (former house) and flanking buildings with steel clad roofs.

Two cast iron posts (possibly hitching posts, but missing hitching hooks) remain on the footpath at the north and south ends of the building. The south post has the top section detached. The northern post has been broken.

Figure 8. The two cast iron posts, possibly hitching posts. Left is the more intact post in front of the south end of the building. Right is the remains of the post at the north end of the building (GJM Heritage, September 2017).

Integrity

The front façade of the building retains a very high degree of integrity to its Interwar period of construction and to its former use as a motor garage, in fabric, form and detail. While the facade has undergone some minor alterations to windows, these do not diminish the ability to understand and appreciate the place as a highly intact example of a Moderne style Interwar building and former motor garage.

The original buildings behind the façade retain a high degree of integrity to the Interwar period in fabric, form and detail. While these have undergone internal alterations, such as reconfiguration of planning, they can still be understood and appreciated as an early motor garage.

Comparative Analysis

Motor garages

A small number of motor garages are included in the Heritage Overlay of the Yarra Planning Scheme. Four motor garages in the City of Yarra were identified in D Catrice & M Summerton 'The Motor Garage & Service Station in Victoria, a Survey', 1997. These were:

- Former Laity Brothers Motor Garage, 205-211 Queens Parade, Clifton Hill (The Former Clifton Motors Garage, Showroom and Residence)
- Former Motor Garage, 214 Brunswick Street, Fitzroy, described as 1920s kerbside
- Former Motor Garage, 396 Brunswick Street, Fitzroy, described as 1920s kerbside
- Mobil Service Station, corner Nicholson and Johnston Streets, Fitzroy, described as late 1960s and rebuilt 1996. (This example is not a relevant comparator).

The motor garage at 396 Brunswick Street, Fitzroy is included in the Heritage Overlay as an individually significant building in the Brunswick Street Precinct (HO311) and the motor garage at 214 Brunswick Street, Fitzroy is included as a contributory building in this precinct. Another motor garage at 18-22 Johnston

Street, Fitzroy, not identified by Catrice and Summerton, is included in the Heritage Overlay as a contributory building in the South Fitzroy Precinct (HO334).

Motor Garage, 396-398 Brunswick Street, Fitzroy (individually significant building in precinct HO311)

Constructed c1920s at the corner of Brunswick and Leicester Streets, Fitzroy, this motor garage is a simple gable-roofed building with symmetrical front façade and central arched pediment. It retains much of its exterior form, including the front façade with large central opening.

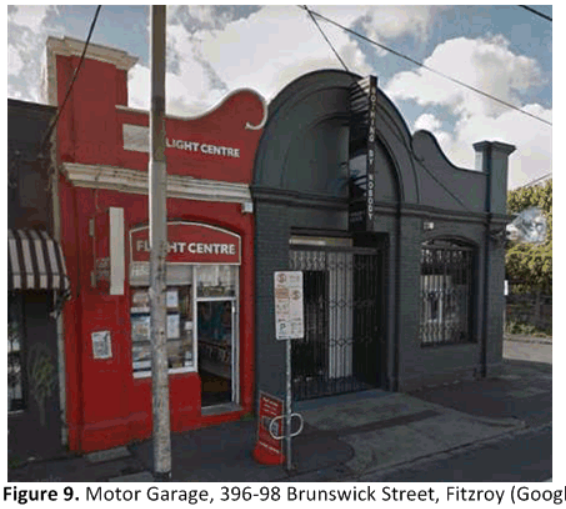

Figure 9. Motor Garage, 396-98 Brunswick Street, Fitzroy (Google Maps)

Motor Garage, 214 Brunswick St Fitzroy (contributory building in precinct HO311)

Constructed c1930, this motor garage is a simple gable roofed building with top skylight and symmetrical front façade with simplified classical elements. It retains much of its exterior form and its front façade with large central opening.

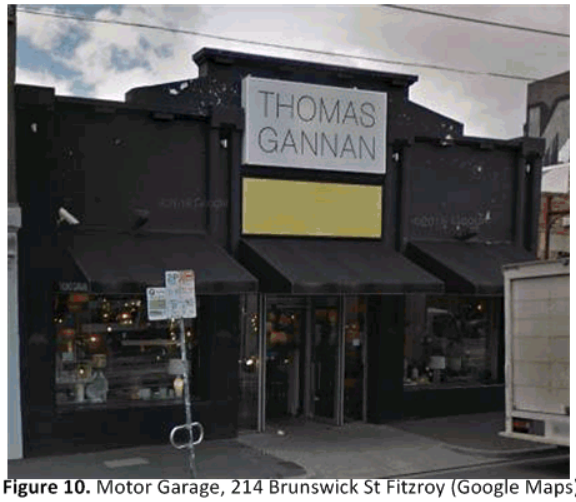

Figure 10. Motor Garage, 214 Brunswick St Fitzroy (Google Maps)

Motor Garage, 18-22 Johnston St Fitzroy (contributory building in precinct HO334)

Constructed c1920-35, this motor garage is a simple gabled roof building with brick (overpainted) and render symmetrical front façade. A central entrance porch is flanked by large openings.

Figure 11: Motor Garage, 18-22 Johnston St Fitzroy (Google Maps)

These comparators are representative of small motor garages that were established on main streets in the City of Yarra. Their existing form demonstrates their original use. The Former Clifton Motors Garage, Showroom and Residence is a much more substantial and elaborately detailed building which clearly demonstrates a similar use.

Moderne Style Buildings

The Former Clifton Motors Garage, Showroom and Residence is a highly distinctive and intact example of the Moderne style of the 1930s and early 1940s. A number of buildings designed in this architectural style are included in the Heritage Overlay of the Yarra Planning Scheme as individually significant buildings. They include:

- HO92 (& VHR H0684), Former United Kingdom Hotel, 199 Queens Parade, Clifton Hill, 1937-38
- HO252, Former Repco Offices and Laboratory, 26 Doonside Street, Richmond, c1939
- HO391, Former Relova Redressing Laundry, 129 Hoddle Street, Richmond, 1937
- HO375, Former Russell Manufacturing Company, 81-95 Burnley Street, Richmond, 1942

Figure 12. Fmr United Kingdom Hotel, Clifton Hill (VHD)

Figure 13. Former Repco Offices, Richmond (VHD)

Figure 14: Former Kelova Laundry, Richmond (VHD) Figure 15: Flint Russell Manufacturing Co, Richmond (VHD)

These individual comparators are buildings of the Interwar period that are architecturally significant as distinctive examples of the Moderne style. As with the Former Clifton Motors, they exhibit characteristics typical of this style including vertical 'towers', contrasting horizontal lines of the façade and canopies, parapet signage and contrasting materials such as brickwork and render.

The United Kingdom Hotel was constructed in close proximity to the Former Clifton Motors in Queens Parade and pre-dates the construction of the Former Clifton Motors facade by approximately one year. Both were designed by architect James H Wardrop. The United Kingdom Hotel is included in the Victorian Heritage Register for its:

> *…architectural significance as possibly Victoria's most exquisite and intact example of the Jazz Moderne style of architecture designed by one of the most notable proponents of the style, J. H. Wardrop. The building is greatly enhanced by its near-island siting and is externally and internally almost original, with the exception of post-war rear additions. It provides an interesting three-dimensional comparison with Wardrop's slightly earlier Alkira House in Queen Street, Melbourne.*

Assessment Against Criteria

Following is an assessment of the place against the recognised heritage criteria set out in *Planning Practice Note 1: Applying the Heritage Overlay* (July 2015).

Criterion A: Importance to the course or pattern of our cultural or natural history (historical significance).

The Former Clifton Motors Garage, Showroom and Residence, Fitzroy North is of significance as an early motor garage constructed in a prominent location in Queens Parade, Fitzroy North. Operating as a garage and vehicle-related facility from at least the 1920s until the late twentieth century, the place demonstrates the increasing use of motor vehicles in Melbourne and suburbs, and their need for repair and service, for much of the twentieth century. The Moderne façade in particular illustrates the motoring age of the 1930s.

Criterion E: Importance in exhibiting particular aesthetic characteristics (aesthetic significance).

The Former Clifton Motors, Fitzroy North is a fine and intact example of a distinctive Moderne style motor garage, designed by noted architect, James H Wardrop. The building displays typical features of the Moderne architectural style popular in the 1930s, including a dominant central element comprising a series of vertical fins and contrasting decorative horizontal treatment.

GJM Heritage

Grading and Recommendations

It is recommended that the place removed from HO333 (Queens Parade Precinct) and be included in a site-specific Heritage Overlay in the Yarra Planning Scheme as a locally significant heritage place.

Recommendations for the Schedule to the Heritage Overlay (Clause 43.01) in the Yarra Planning Scheme:

External Paint Controls?	No
Internal Alteration Controls?	No
Tree Controls?	No
Outbuildings or Fences not exempt under Clause 43.01-3?	No
Prohibited Uses Permitted?	No
Incorporated Plan?	Incorporated Plan under the provisions of clause 43.01 Heritage Overlay, Planning permit exemptions, July 2014
Aboriginal Heritage Place?	No

Extent of the recommended Heritage Overlay

To the property title boundary, as indicated by the polygon on the aerial below:

Figure 16. Recommended extent of the Heritage Overlay (dotted red line) (Planning Maps Online)

Identified by:

Andrew Ward (1995), *Collingwood Conservation Study Review.*

> The place was identified as 'outside of an Urban Conservation Area'. It was recommended for inclusion on the National Estate Register, Historic Buildings Register and for Planning Scheme protection. No citation was provided.

Graeme Butler and Associates (2007, 2013), *City of Yarra Review of Heritage Overlay Areas*

Recommended as Individually Significant as part of HO330 Queens Parade Precinct, North Fitzroy/Clifton Hill, with a Statement of Significance provided.

References:

Australian Electoral Rolls (AER), Victoria, Batman, Clifton Hill, 1949.

Catrice, Daniel & Michele Summerton (1997), *The motor garage and service station in Victoria : a survey*.

City of Collingwood Rate Books (RB), Lock Ward, as cited in Graeme Butler & Associates (2007) *City of Yarra Review of Heritage Overlay Areas*, Appendix 7.

Construction [Sydney, NSW].

Graeme Butler & Associates (2007) *City of Yarra Review of Heritage Overlay Areas*, Appendix 7: citation for 'Clifton Motors Pty Ltd , Motor Engineers, Show Room and Garage, later Bayford Motors, 205-211 Queens Parade, Clifton Hill'.

Grow, Robin (2014), *Melbourne Art Deco*, Melbourne.

Melbourne & Metropolitan Board of Works Detail Plan No. 1256 (dated 1904).

Museums Victoria (MV), online collection, < https://collections.museumvictoria.com.au/>.

Property Sewerage Plan (PSP) No. 53990, dated 1918, 1921, 1939.

Sands & McDougall Directories (S&Mc).

State Library of Victoria (SLV) picture collection.

Statham, John (2012), 'Philip Hudson' in Philip Goad & Julie Willis' (Eds.), *The Encyclopedia of Australian Architecture*, Port Melbourne.

The Age.

The Argus.

Appendix G: Recommendations for amendments to Heritage Gradings & Heritage Overlay Boundaries

gard'ner jarman martin

GJM Heritage

Recommendations for amendments to Heritage Gradings and Heritage Overlay Boundaries

1. 7, 9 and 11 Queens Parade, Clifton Hill

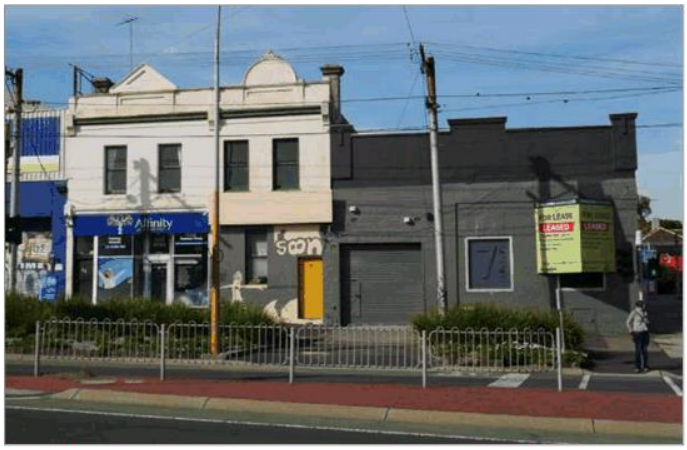

Figure 1. (Right to left) 7, 9 and 11 Queens Parade, Clifton Hill (GJM Heritage 5 May 2017).

Figure 2. Extract of Heritage Overlay map showing 7-11 Queens Parade, Clifton Hill within HO317 (© Planning Maps Online).

1.1 Current grading

The properties at 7, 9 and 11 Queens Parade are currently graded as being 'contributory' within HO317 – Clifton Hill Western Precinct[1].

Queens	Parade	7	Clifton Hill	Factory	131875	contributory	1915-1925
Queens	Parade	9	Clifton Hill	Shop & residence, former	131880	contributory	1880-1890
Queens	Parade	11	Clifton Hill	Shop & residence	131885	contributory	1880-1890

Table 1. Current entries in Appendix 8 (Graeme Butler & Associates, 2007, revised May 2017) for HO317 – Clifton Hill Western Precinct

1.2 Description

The building at 7 Queens Parade is single-storey with a tall parapet with projecting brick panels at the centre and each end. There is no decorative architectural detail below the parapet and the ground floor façade has been heavily altered with no original openings evident.

The two-storey pair of terraced shop residences at 9 and 11 Queens Parade have a relatively simple rendered form with a single pair of plain window openings with timber sash windows to the first floor. 11 Queens Parade is wider than 9 Queens Parade, but each share the same solid parapet with inlaid panel, string course and cornices terminating in vermiculated brackets. The parapet of 11 Queens Parade is surmounted by a triangular pediment while 9 Queens Parade has a semi circular pediment capped with decorative scroll work. The ground floor shopfront to 11 Queens Parade has been replaced with later aluminium framed glazing and the ground floor of 9 Queens Parade has a partly rendered masonry wall with a door and window, which appear to have been altered. The architectural detail at the top of the ground floor of 9 Queens Parade has been covered or replaced with a hoarding. Rendered brick chimneys are visible at either end of the pair; roofs are not visible from the public realm.

Figure 3. Aerial photograph of the 7, 9 and 11 Queens Parade (outlined in red) (©nearmap, 4 May 2017).

[1] Graeme Butler & Associates *City of Yarra – Review of Heritage Overlay Areas 2007: Appendix 8* (revised May 2017) (Appendix 8)

1.3 Historical Overview

The main buildings (fronting Queens Parade) at 9 and 11 Queens Parade, Clifton Hill (numbered 11 and 13 in 1901) were evident on the 1901 Melbourne & Metropolitan Board of Works (MMBW) plan and their form and remnant decorative treatment indicates a c1880s/90s construction date. The plan also shows an earlier building at 7 Queens Parade in 1901, with a driveway along the eastern boundary (Figure 4). This earlier building remains evident on a map dated c1933 (Figure 5).

An oblique aerial identified as dating between c1925 and c1935 shows the pair of two-storey buildings at 9 and 11 Queens Parade (11 with a verandah), and the current parapeted building at 7 Queens Parade, occupying the full width of the site (Figure 6). This evidence suggests that the aerial image dates from after 1933 with the current building on the front boundary of 7 Queens Parade constructed soon after 1933.

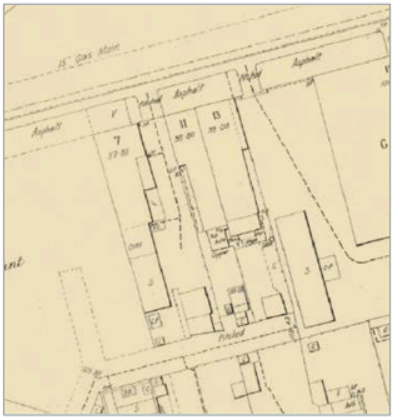

Figure 4. Footprints of the buildings on the properties in 1901 (MMBW DP 1217, 1901).

Figure 5. Footprints of the buildings on a map dating c1933 (MMBW Base Map No. 29, c1933).

Figure 6. Detail of an oblique aerial photograph showing 7, 9 and 11 Queens Parade (State Library of Victoria, Image H91.160/447, c1925-c1935).

1.4 Context

To the east of the properties, a large single-storey warehouse showroom (currently vacant) adjoins 11 Queens Parade. Adjacent to 7 Queens Parade is a post-war showroom currently occupied by a bicycle shop and two c1970/80 office/warehouses at 664 & 668 Smith Street. These neighboring properties are identified as 'not-contributory' to the precinct.

1.5 Analysis

In summary, HO317 – Clifton Hill Western Precinct is identified primarily as being of heritage significance for its Victorian and Edwardian-era residential development, its public parks and landscaping, and its 19th century industrial buildings. The three properties at 7-11 Queens Parade, Clifton Hill, are broadly captured by the following parts of the precinct Statement of Significance:

> *What is significant?*
>
> *...*
>
> *Main development period*
>
> *The main development period evident in the heritage overlay is that of the Victorian-era with a substantial contribution from the Edwardian-period. There is also a contribution from some well preserved inter-war buildings and individually significant places of all eras.*
>
> *Contributory elements*
>
> *...*
>
> 1. *Corner shops and residences with display windows and zero boundary setbacks*
>
> *And*
>
> 2. *Well preserved buildings, including one storey houses plus one and two storey industrial buildings from the pre Second World War era;*
>
> *...*
>
> *What is significant?*
>
> *...*
>
> *For the area's fine representation of Victorian-era life, as expressed by the street and public reserves, along with the dominantly Victorian-era building stock including churches, a State school, factories, early houses, a large number of late Victorian terrace houses, attached and free standing, and key house designs such as 12 North Terrace.*
>
> *...*

It is considered that the 1930s building at 7 Queens Parade does not readily accord with the identified significance of the precinct. It does not date from the key periods of significance (Victorian and Edwardian) and is not a "well preserved" inter-war building, with its façade having been substantially altered since the taking of the aerial photograph in Figure 6. It is considered that the property does not contribute to HO317.

As simple Victorian shops/residences, 9 and 11 Queens Parade broadly reflect the key development period and architectural form of the precinct's identified significance; however, they are isolated Victorian buildings within an immediate context of 'not-contributory' buildings. It is our view that while these two buildings would be identified as 'contributory' within a reasonably intact streetscape, this section of HO317 fronting Queens Parade is heavily dominated by buildings identified as 'not-contributory' and the contribution nos. 9 and 11 make to the Clifton Hill Western Precinct is very marginal. The pair do not read as part of the heritage precinct.

1.6 Recommendation

It is recommended that Appendix 8 be amended to identify 7, 9 and 11 Queens Parade as 'not contributory' within HO317.

2. 350 Queens Parade, Fitzroy North

Figure 7. (Left to right) 350, 532, 354 & 356 Queens Parade, Fitzroy North (GJM Heritage 6 September 2017).

Figure 8. Extract of Heritage Overlay map showing 350 Queens Parade within HO330 (© Planning Maps Online).

2.1 Current Grading

350 Queens Parade is currently ungraded within HO330 – Queens Parade Precinct, North Fitzroy/Clifton Hill.

Queens	Parade	344	Fitzroy North	Shop & residence	248165	contributory	1870-1890
Queens	Parade	346	Fitzroy North	Shop & residence	248170	contributory	1870-1890
Queens	Parade	348	Fitzroy North	Shop & residence	248175	contributory	1870-1890
Queens	Parade	352	Fitzroy North	Shop & residence	248185	contributory	1870-1890
Queens	Parade	354	Fitzroy North	Shop & residence	248190	contributory	1870-1890
Queens	Parade	356	Fitzroy North	Shop & residence	248195	contributory	1870-1890
Queens	Parade	360	Fitzroy North	Shop	248200	not contributory	1950-1960

Table 2. Current entries in Appendix 8 (Graeme Butler & Associates, 2007, revised May 2017) for HO330 – Queens Parade Precinct, North Fitzroy/Clifton Hill.

2.2 Description

The row of four two-storey terraced shops/residences at 350 to 356 Queens Parade, Fitzroy North, are constructed of rendered brick with parapets, elaborative decorative mouldings, cornice detail and arched window openings to the first floor. The form and architectural detail of each building is identical, each with a verandah to the ground floor.

2.3 Historical Overview

The four properties and their occupants were listed by 1892 in the Sands & McDougall Directories. The footprints of the four buildings are shown on the 1904 MMBW plan (Figure 9). The buildings' appearance strongly suggests they were constructed for the same owner at the same time.

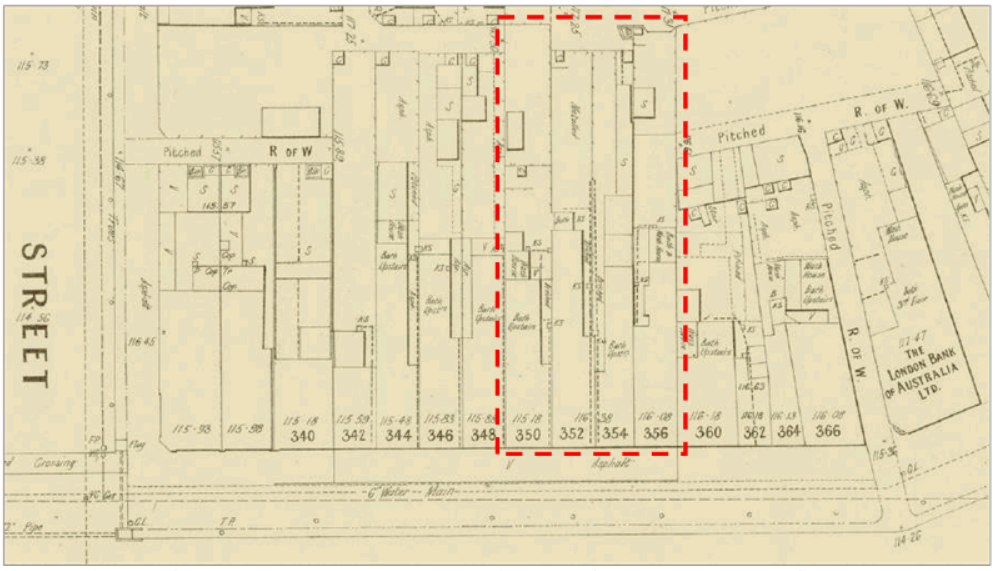

Figure 9. The row of terraces at 350 to 356 Queens Parade in 1904 (MMBW DP 1264, 1904).

2.4 Context

Neighbouring 350 Queens Parade to the west are a row of single and two-storey shops, identified as 'contributory' and 'individually significant'. To the east of no. 350, the three shop/residences identical to 350 Queens Parade are identified as 'contributory'.

2.5 Analysis

The absence of 350 Queens Parade from the Appendix 8 entry for HO330 – Queens Parade Precinct, North Fitzroy/Clifton Hill appears to be an error. As part of a consistent group of 'contributory' Victorian buildings within this highly intact heritage precinct, the property should also be identified as being 'contributory' to HO330.

2.6 Recommendation

Amend Appendix 8 to include 350 Queens Parade and grade this property as 'contributory'.

3. 201-217 Queens Parade, 10-12 Dummett Crescent, Raines Reserve & Queens Parade

Figure 10. Left to right is 205-211, 201-203 (grey) and 199 Queens Parade, Fitzroy North (GJM Heritage, 5 May 2017).

Figure 11. Left to right is 271 to 243 Queens Parade, Fitzroy North (GJM Heritage, 5 May 2017).

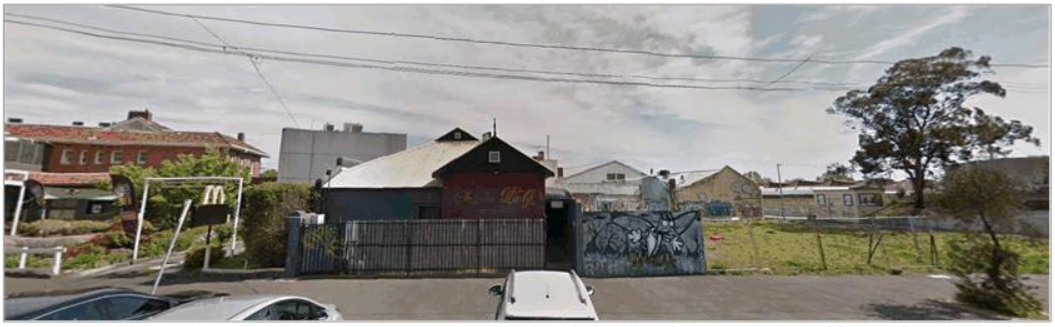

Figure 12. Dummett Crescent, Fitzroy North. The vacant land to the right includes nos. 10-12 (© Google, Oct 2016).

Figure 13. Raines Reserve looking towards the former United Kingdom Hotel (GJM Heritage, 5 May 2017).

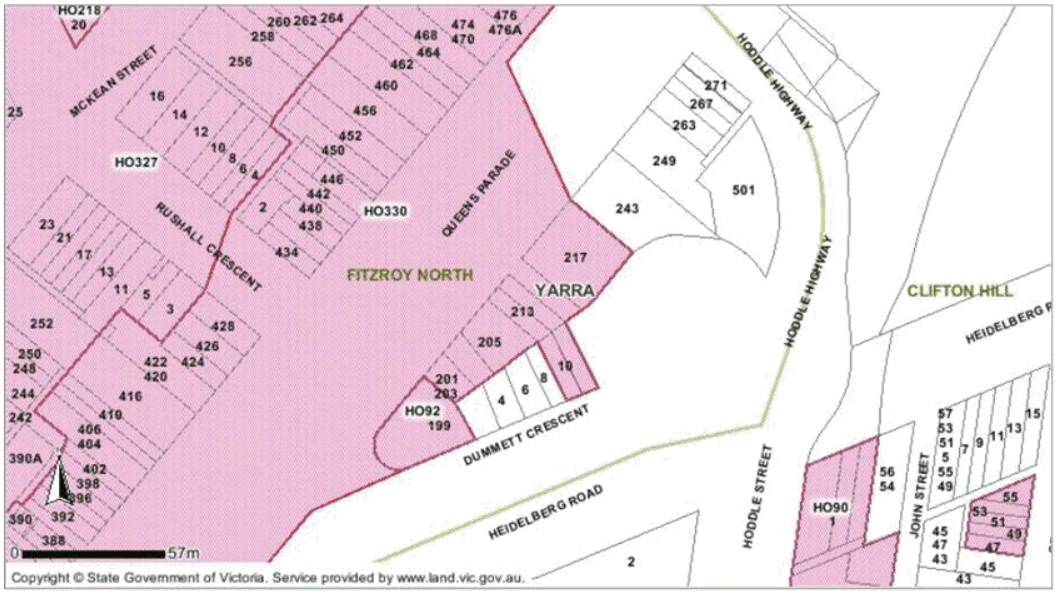

Figure 14. Extract of Heritage Overlay map showing 201-217 Queens Parade and 10-12 Dummett Crescent within HO330 (© Planning Maps Online).

3.1 Current Grading

Within the part of HO330 comprising 201-217 Queens Parade and 10-12 Dummett Crescent, Fitzroy North, only the former Clifton Motors Pty Ltd building at 205-211 is identified as being of heritage significance ('individually significant'). It is also noted that the former Clifton Motors Pty Ltd building has been recommended for inclusion in the Victorian Heritage Register (a registration decision is pending). The remaining buildings and sites are 'not contributory' to the precinct. It is noted that the former United Kingdom Hotel at 199 Queens Parade is included in the Victorian Heritage Register (H0892) and is therefore included in its own Heritage Overlay (HO92) rather than HO330.

Dummett	Crescent	10	-12	Clifton Hill	Factory/warehouse	132215	not contributory	1950-1960
Queens	Parade	201	-203	Clifton Hill		132150	not contributory	1925-35
Queens	Parade	205	-211	Clifton Hill	Clifton Motors Pty Ltd , Motor Engineers, Show Room and Garage, later Bayford Motors	132155	individually significant	1938 facade, 1921
Queens	Parade	213		Clifton Hill	Factory/warehouse	132160	not contributory	unknown
Queens	Parade	217		Clifton Hill	Factory/warehouse	132165	not contributory	1950-1960

Table 3. Current entries in Appendix 8 (Graeme Butler & Associates, 2007, revised May 2017) for HO330 – Queens Parade Precinct, North Fitzroy/Clifton Hill.

A portion of Raines Reserve is currently included within HO330 while the Heritage Overlay does not cover the remaining section (see Figure 15). The Reserve is not graded in Appendix 8.

Figure 15. Extract of Heritage Overlay map showing 199-217 Queens Parade and 10-12 Dummett Crescent within HO330, with a 2014 aerial image (© Planning Maps Online).

3.2 Description

The land subject to HO330 within the island site bound by Queens Parade, Dummett Crescent and Hoddle Street, includes the main Queens Parade carriageway generally between 199 to 217 Queens Parade and Raines Reserve (originally known as Rain's Reserve) - a small triangular public garden at the intersection of Queens Parade and Heidelberg Road/Dummett Crescent with mature plantings including a Holm oak (*Quercus ilex*). The southern-most lane of Queens Parade retains bluestone kerbs and gutters in front of the buildings and two cast iron posts (possibly the base of hitching posts) that are located in front of 205-211 Queens Parade (see Figures 16 & 17).

10-12 Dummett Crescent is identified in Appendix 8 as a 'not-contributory' factory built c.1950-1960; this land is now vacant.

201-203 Queens Parade is occupied by a heavily altered single-storey building that may date originally from 1925-1935 and is identified in Appendix 8 as 'not-contributory'.

The Moderne styled former Clifton Motors Pty Ltd building (later Bayford Motors) occupies 205-211 Queens Parade. It is identified as 'individually significant' in Appendix 8 and is currently being considered for inclusion in the Victorian Heritage Register.

213 Queens Parade is a single-storey showroom/warehouse building identified in Appendix 8 as 'not-contributory'.

North of Dummett Crescent is 217 Queens Parade which is included within HO330, identified as 'not-contributory', and is currently being developed as a ten-story building.

Figures 16 and **17.** Cast iron posts outside 205-211 Queens Parade (GJM Heritage, 5 Sep 2017).

3.3 History

Queens Parade

Queens Parade was a track shown on Robert Hoddle's Crown survey of 1837, linking Melbourne to Heidelberg. Established by 1839, it was Melbourne's first major road.[2] The track originally extended from the top of Bourke Street and followed an irregular course to the northeast, following the top of the Collingwood escarpment and running along the current alignments of Queens Parade, Heidelberg Road, Upper Heidelberg Road and Lower Plenty Road.[3] Queens Parade was surveyed by Hoddle as a three chain (60m) wide boulevard and reserved in 1853. It was one of Melbourne's earliest surveyed boulevards.[4] The 1904 MMBW plan shows the alignment of Queens Parade at its northern-most extent at that time (see Figure 18) while an aerial dated c1925-c1940 also shows the alignment and layout during the interwar period (see Figure 19).

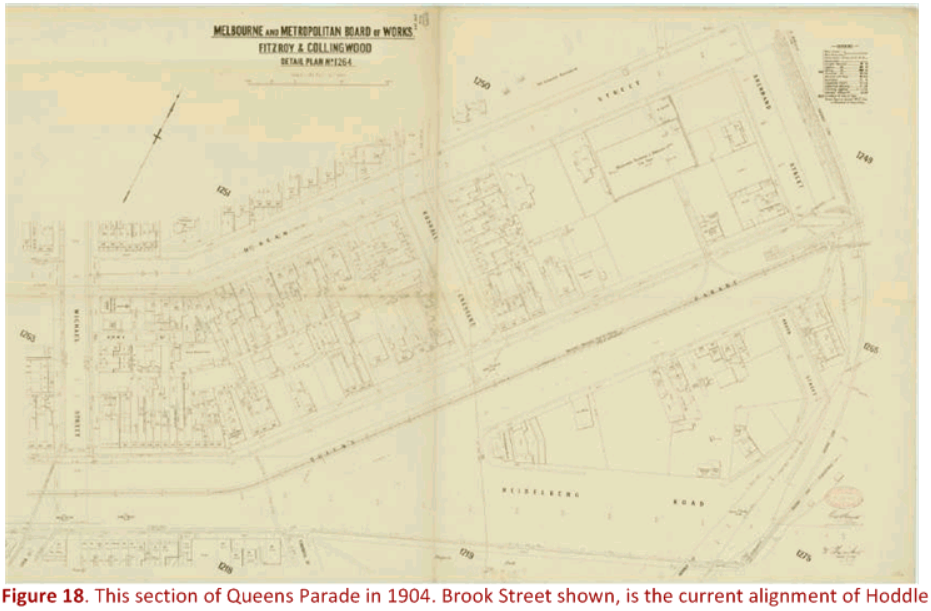

Figure 18. This section of Queens Parade in 1904. Brook Street shown, is the current alignment of Hoddle Street (MMBW DP 1264, 1904).

Figure 19. An aerial dated c1925-c1940 showing the triangular block of land, looking south. Left is Brook Street, the current alignment of Hoddle Street (SLV Image H91.160/1611, c1925-c1940).

Raines Reserve

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[2] Maxwell Lay, 'Roads', *eMelbourne:* The *City Past and Present* (www.emelbourne.net.au/biogs/EM01255b.htm). Accessed 13 June 2017. Lay also states that the road was first surveyed to Eltham in 1840 and officially opened in 1841. Hoddle's survey in 1853 may have been a re-survey of the road.

[3] Maxwell Lay, 'Roads', *eMelbourne.*

Lewis, Miles, 'The First Suburb' in *Fitzroy: Melbourne's First Suburb* (1989), 7.

4 Graeme Butler and Associates, *City of Yarra Review of Heritage Overlay Areas* (2007), 120.

A photograph of Raines Reserve, dating between 1907 and 1925, shows it planted as a garden with a similar layout to that which remains in 2017 (Figure 22). The tower and spire of St John's Church and the former ANZ Bank are clearly visible beyond. The aerial photograph at Figure 19 (c1925-1940) also clearly shows the layout of the reserve.

Figure 20. A photo dating between 1907 and 1925 of Rain's Reserve (named after Cr. William Rain) (now Raines Reserve), at the intersection of Heidelberg Road and Queen's Parade, looking west (COYL, Image No. CL PIC 13).

201-217 Queens Parade & 10-12 Dummett Crescent

The 1904 MMBW Detail Plan shows the triangular block of land and the extent of its development at this date (Figure 21); the first United Kingdom Hotel (rebuilt 1906 and 1937-38) was located at the south end, adjacent in turn to a saw mill, weighbridge, hay and corn store, coachbuilder's workshop, shoeing forge and other unlabelled buildings. When compared to a 2017 aerial (Figure 22), it does not appear that any of the buildings shown on the 1904 plan remain in 2017. From the 1910s, the east side of Queens Parade within this block was occupied by a majority of motor, coach, carriage and buggy related businesses (Sands &McDougall Directory).

Figure 21. The triangular block of land and the extent of its development in 1904. Brook Street appears to have become the alignment of Hoddle Street (MMBW Detail Plan No. 1264).

Figure 22. 2017 aerial photograph (©nearmap, 4 May 2017)

The oblique aerial dated c1925-c1940 (Figure 19) shows the triangular block of land and its complete development. Some early twentieth century buildings shown in the aerial appear to remain in 2017, but are altered from their original appearance – including the former Clifton Motors buildings, which had its façade remodeled in the 1930s after the photo was taken.

Part of this triangular block was later cleared to allow for the alignment of the on-ramp from Hoddle Street to Heidelberg Road.

3.4 Context

The two heritage buildings within this portion of Queens Parade – the former United Kingdom Hotel and the former Clifton Motors Building – exist within a context that otherwise lacks built heritage fabric.

3.5 Analysis

The Queens Parade carriageway is one of Melbourne's very early northern transport routes and is one of the earliest surveyed boulevards. It is appropriate that the full extent of this early boulevard is included within the Heritage Overlay boundary.

Raines Reserve is a turn of the century public reserve that retains its original extent and layout. It is appropriate that the full extent of the reserve is included in the Heritage Overlay boundary.

The former Clifton Motors Pty Ltd building at 205-211 Queens Parade is identified in Appendix 8 as an 'individually significant' building within HO330 and is currently being considered for state-level heritage significance. Being located on the island site to the north of Queens Parade, the building is isolated from the balance of buildings within HO330 and is surrounded by 'not contributory' buildings and sites. While the former Clifton Motors Building has very strong architectural similarities with the nearby Former United Kingdom Hotel, the hotel building is not within HO330 and is instead covered by its own Heritage Overlay by virtue of its state-heritage listing. Further, the former Clifton Motors Building is an atypical building within the identified significance of the Queens Parade Precinct (being a precinct characterised by its highly consistent two-storey Victorian and Edwardian shop/residences) and is considered to be of significance in its own right as an important Moderne example of a motor garage. For these reasons it is considered appropriate that the former Clifton Motors building be removed from HO330 and included in its own site-specific Heritage Overlay (see the Heritage Citation at Appendix F). It is noted that if the place is included in the Victorian Heritage Register it will automatically be included within its own Heritage Overlay.

With the exception of the former Clifton Motors building and the state-significant former United Kingdom Hotel, there is no other built heritage fabric within this section of Queens Parade. It is therefore considered appropriate that the extent of HO330 be amended to exclude 201-217 Queens Parade and 10-12 Dummett Crescent from HO330.

3.6 Recommendation

1. Remove the former Clifton Motors Pty Ltd building at 205-211 Queens Parade from HO330 and introduce an individual Heritage Overlay with a separate Statement of Significance to manage this property's heritage values (noting that if the place is found to be of state-level significance, this recommendation will automatically be enacted).
2. Remove 10-12 Dummett Crescent, 201-203, 213 and 217 Queens Parade from the extent of HO330.
3. Include the Queens Parade road reserve (including the main carriageway, two laneways and footpaths) as shown in Figure 23 within HO330.
4. Include the full extent of Raines Reserve within HO330 (see Figure 23).
5. Update Appendix 8 to reflect the above recommendations.

Figure 23. Heritage Overlay map – extent of proposed new individual Heritage Overlay for 205-211 Queens Parade shaded in blue. Properties to be removed from HO330 shaded in red. Proposed new Heritage Overlay boundary for HO330 dashed in red (adapted from Planning Maps Online, accessed 25 September 2017).

4. 304, 312, 316, 380 (rear of all) & 390A Queens Parade, Fitzroy North

Figure 24. The southern elevations of the outbuildings at (left to right) 304, 312 & 316 (rear of all) Queens Parade (GJM Heritage, 6 Sep 2017).

Figure 25. The northern elevations of the outbuildings at (right to left) 304, 312 & 316 (rear) Queens Parade (GJM Heritage, 6 Sep 2017).

Figure 26. The western elevation of the outbuilding at 304 (rear) Queens Parade (GJM Heritage, 6 Sep 2017).

Figure 27. The southern elevation of the outbuilding at 312 (rear) Queens Parade (GJM Heritage, 6 Sep 2017).

Figure 28. Extract of Heritage Overlay map showing 304, 312 and 316 (rear of all) Queens Parade within HO327 (© Planning Maps Online).

Figure 29. The south-west elevation of the outbuilding at 390A Queens Parade (painted red). In the foreground is the vacant lot at 380 (rear) Queens Parade (GJM Heritage, 6 Sep 2017).

Figure 30. The south-east elevations of the outbuildings at 390A Queens Parade (GJM Heritage, 6 Sep 2017).

Figure 31. The north-east elevations of the outbuildings at 390A Queens Parade (GJM Heritage, 6 Sep 2017).

Figure 32. The north-west elevations of the outbuildings at 390A Queens Parade (GJM Heritage, 6 Sep 2017).

Figure 33. Extract of Heritage Overlay map showing 380 (rear) and 390A Queens Parade within HO327 (© Planning Maps Online).

4.1 Current Grading

304, 312 and 316 Queens Parade fall within HO327 – North Fitzroy Precinct but relate to properties that have a frontage to Queens Parade and fall within HO330 – Queens Parade Precinct. None of the properties are identified in Appendix 8 within the North Fitzroy Precinct; however 312 and 316 Queens Parade are identified as being 'contributory' to the Queens Parade Precinct in Appendix 8 and 304 is identified as 'not contributory'.

380 Queens Parade, Fitzroy North, is identified in Appendix 8 as 'Shops & residences' at '380-378 Queens Parade' and graded 'contributory' within HO327 – North Fitzroy Precinct. This is an incorrect entry and should relate to the buildings fronting Queens Parade in HO330 (which are not currently graded in Appendix 8, within HO330).

390A Queens Parade is currently ungraded within HO327 or HO330.

4.2 Description

304, 312 & 316 (rear of all) Queens Parade

The site is bound on three sides by Koormiel Lane, a right of way that is accessed off Michael Street to the east and McKean Street to the north. The buildings are located behind 304-316 Queens Parade; signs on some buildings indicate they are owned/occupied by the corresponding properties fronting Queens Parade.

The buildings occupying the site are constructed of red and cream brick and form solid walls to the north, east and west sides of the site, with (vehicular and pedestrian) access provided off the south sides. The buildings were constructed at various dates. At the east end (rear of 316) is a cream brick garage, the central property (rear of 312) includes an early two-storey brick and timber stables and adjacent single-storey brick building that are no longer in use, and at the west end (rear of 304) is a large red brick building occupied by a business.

Figure 34. 2017 aerial photograph (©nearmap, 17 Sep 2017).

380 (rear) & 390A Queens Parade

The site is bound on three sides by a laneway that is accessed by Howe Street to the west and two right of ways off Queens Parade. The site is located behind 360 to 402 Queens Parade. The multiple buildings occupying the site are constructed of red brick (overpainted on the south-east elevations), built during various construction phases. They form a solid wall on the northern side which has few window openings.

At the south-west end is no. 380, a vacant lot that serves as a car park. The car park and adjacent buildings within the site are occupied by the paint store at 380 Queens Parade. At the east end is a two-storey building with one remaining entrance that bears a sign warning that the building houses a Citipower substation. Next to it, a single-storey building serves a business.

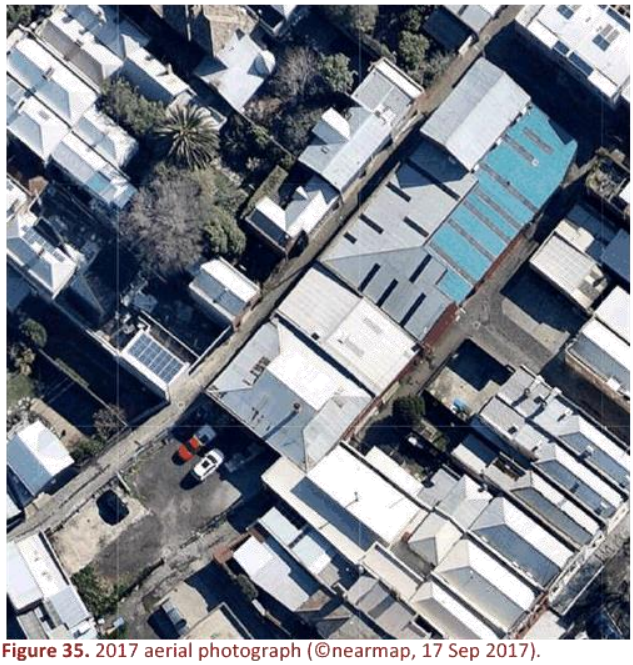

Figure 35. 2017 aerial photograph (©nearmap, 17 Sep 2017).

4.3 Historical Overview

304, 312 & 316 (rear of all) Queens Parade

The buildings appear to have been established as outbuildings for the adjacent properties, separated by a right of way that granted access to the stables (including those to the rear of 312) and other buildings that are evident on the 1904 MMBW plan (Figure 36). In 1904, the eastern end also had a washhouse with a bath. A MMBW plan dated c1933-c1950 shows the footprint of the stables as well as other buildings occupying the site by this date (Figure 37). The heritage fabric of the site indicates that the brick walls of the western-most stables (rear of 304) evident on the 1904 MMBW plan were incorporated into new structures.

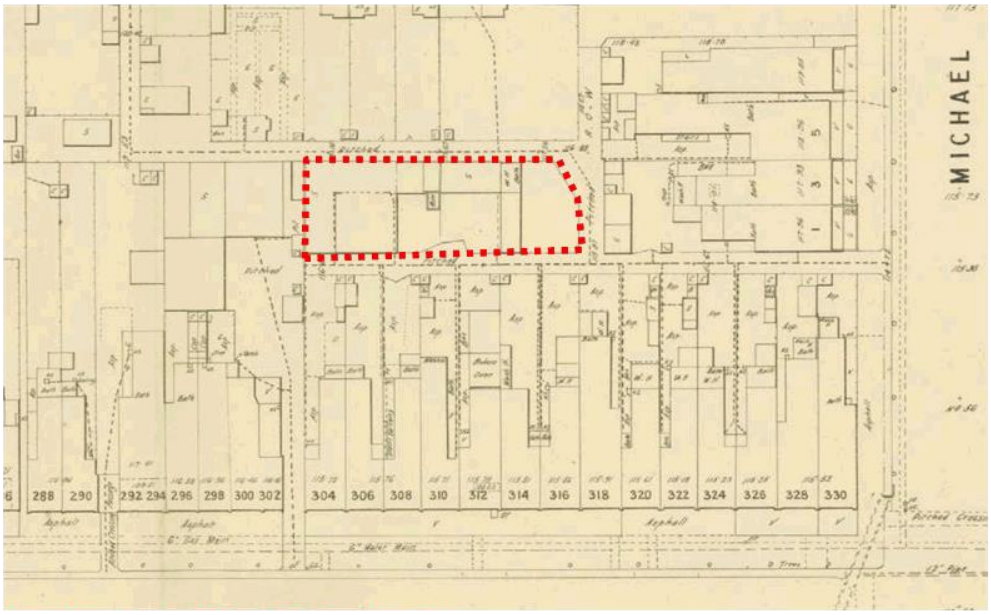

Figure 36. The area in 1904 (MMBW DP 1263, 1904).

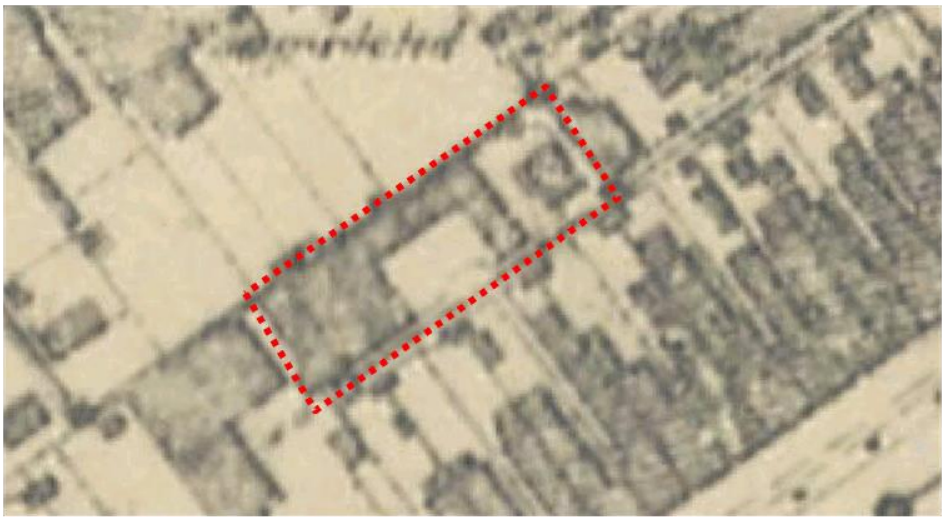

Figure 37. The area in c1933-c1950 (MMBW Base Map 29, c1933-c1950).

380 (rear) & 390A Queens Parade

This area appears to have developed to serve the adjacent properties, providing land for stables and fenced areas (presumably to hold horses) that were accessed by the right of ways, as evident in the 1904 MMBW plan (Figure 38).

An aerial dated c1925-c1940 (Figure 39) clearly shows the buildings on the site, as viewed from the north – the large two-storey building shown at the east end, and the large single-storey building at the west end, remain in 2017.

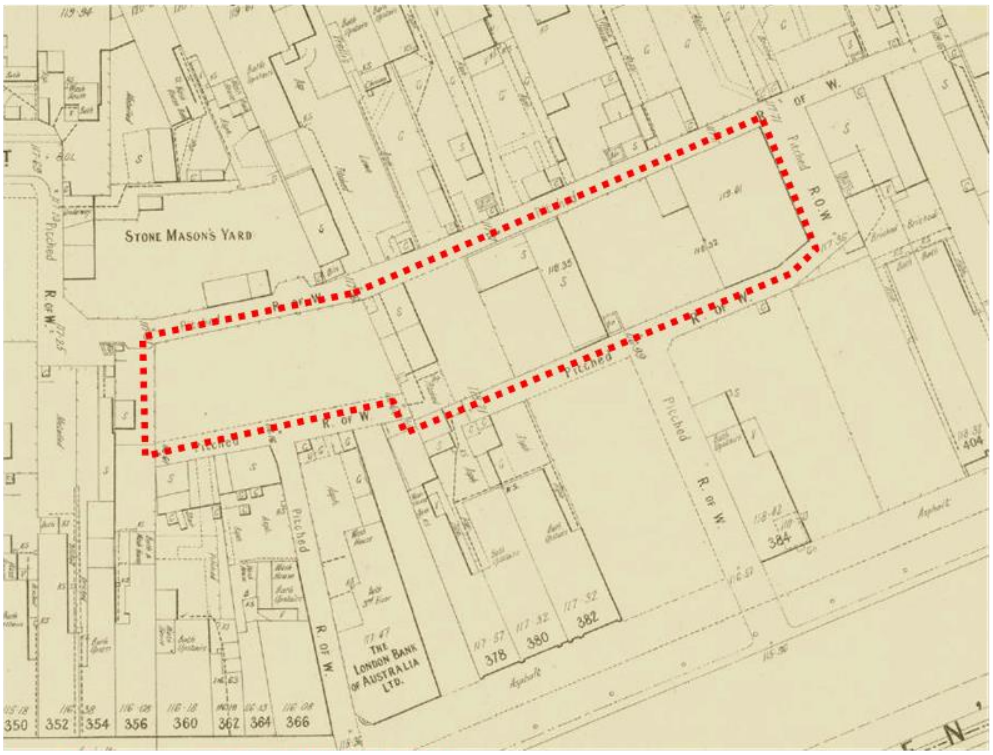

Figure 38. The area in 1904 (MMBW DP 1264, 1904).

Figure 39. Aerial dating to c1925-c1940. Looking south, showing the buildings occupying the site at this date (Pratt, SLV Image H91.160.1611).

4.5 Analysis

Historically it appears that all of the buildings at the rear of Queens Parade serviced properties fronting Queens Parade. It is therefore considered appropriate that they be removed from HO327 – North Fitzroy Precinct and included within HO330 – Queens Parade Precinct.

304, 312 & 316 (rear) Queens Parade

The existing building at the rear of 316 Queens Parade is a utilitarian cream brick garage that appears to date from the c1950s/60s. It is not considered to have heritage value.

The existing building at the rear of 312 Queens Parade retains the same footprint as shown in the 1904 MMBW plan and comprises a two-storey brick and weatherboard structure and associated yard from the late nineteenth century. While the building is in very poor condition, it retains its historic fabric and is considered to contribute to the significance of the precinct. The adjacent single-storey building at 312 is not considered to have heritage value.

The existing building at the rear of 304 Queens Parade retains some brickwork that dates from the late nineteenth century which has been incorporated into a new structure and appears to have been developed in stages throughout the twentieth century. It is a simple utilitarian structure that can no longer be understood as dating from a particular period and is not considered to have heritage value.

380 (rear) & 390A Queens Parade

The site at the rear of 380 Queens Parade is vacant and is used as an at-grade car park. It is not of heritage value.

390A Queens Parade is a large parcel of land containing four discreet structures. Based on an analysis of the built fabric on the site and the historical plans and aerial, it is considered that the only largely intact building remaining is the two-storey substation in the northeast corner of the site. The brickwork indicates the building dates from the late nineteenth century, and although some original openings have been closed, the original form and fenestration pattern remain. This building is considered to contribute to the significance of the precinct. The other buildings on the property are of more recent construction or have been substantially altered over time. They are not considered to contribute to the significance of the precinct.

4.6 Recommendation

1. Remove the properties at 390A and the rear of 304, 312, 316 and 380 from HO327 – Fitzroy North Precinct and include within the boundary of HO330 – Queens Parade Precinct.
2. Remove the entry in Appendix 8 for 'Shops & residences' at '380-378 Queens Parade' within HO327 – North Fitzroy Precinct and include 380 Queens Parade (the property fronting Queens Parade) as a 'contributory' building within HO330 – Queens Parade Precinct.
3. Identify the following sites and associated gradings in Appendix 8 under HO330 – Queens Parade Precinct:
   - 304 Queens Parade (rear, off Koormiel Lane) – 'not contributory'
   - 312 Queens Parade (rear, off Koormiel Lane; two-storey building on north boundary) – 'contributory'
   - 316 Queens Parade (rear, off Koormiel Lane) – 'not contributory'
   - 380 Queens Parade (rear, behind 360-378) – 'not contributory'
   - 390A Queens Parade (two-storey building in north-east corner) – 'contributory'
   - 390A Queens Parade (all buildings excluding two-storey building) – 'not contributory'

Appendix H: Yarra High Streets: Statements of Significance (Reference Document)

gard'ner jarman martin

Attachment 1 - Amendment C231 - GJM Heritage Analysis and Recommendations

Yarra High Streets: Statements of Significance

October 2017 (updated November 2017)

This reference document contains the Statement of Significance for all Heritage Precincts and Individually Significant Places (where a Statement of Significance has been prepared) within:

- *Swan Street Built Form Review* Study Area (City of Yarra & Tract, Oct 2017).
- *Swan Street Built Form Study: Heritage Assessments & Analysis* (GJM Heritage, Oct 2017)
- *Queens Parade Built Form Heritage Analysis and Recommendations Report* (GJM Heritage, Nov 2017)

INDEX

Ordered by Heritage Overlay number

Precincts

	Precinct HO No.	Precinct Name	Page No.
1.	HO309	Bendigo Street Precinct, Richmond	1
2.	HO315	Church Street Precinct, Richmond	2
3.	HO317	Clifton Hill Western Precinct	4
4.	HO327	North Fitzroy Precinct	8
5.	HO330	Queens Parade Precinct, North Fitzroy/Clifton Hill	14
6.	HO332	Richmond Hill Precinct	16
7.	HO335	Swan Street Precinct, Richmond	22
8.	HO364	Wellington Street Precinct	24
9.	HO474	Burnley Street Precinct, Richmond	28
10.	HO524	Swan Street West Precinct	29

Individually Significant Places

	Individual HO No. / VHR No.	Name	Address	Page No.
1.	HO92 **VHR H0684**	Former United Kingdom Hotel (now McDonald's)	199 Queens Parade, Clifton Hill	30
2.	HO93	Street Trees	Queens Parade, Between Alexandra Parade & Delbridge Street Clifton Hill/ North Fitzroy	30
3.	HO216 **VHR H0163**	Haselmere	96 Mckean Street, Fitzroy North	30
4.	HO221 **VHR H0892**	Former ANZ Bank	370-374 Queens Parade, North Fitzroy	30
5.	HO245	House	234 Coppin Street, Richmond	30
6.	HO285	Former Central Club Hotel	291 Swan Street, Richmond	30
7.	HO286	Former Burnley Theatre	365 Swan Street, Richmond	31
8.	HO288 **VHR H732**	Former State Bank	214-216 Swan Street, Richmond	31
9.	HO294	House	15 Wellington Street, Cremorne	31
10.	HO357 **VHR H48**	Former Richmond South Post Office	90-92 Swan Street, Richmond	31
11.	HO360	Dimmeys	140-160 Swan Street, Richmond	31

	Individual HO No. / VHR No.	Name	Address	Page No.
	VHR H2184		(Cremorne)	
12.	HO405	The Greyhound Hotel, later Depot Hotel, now Precinct Hotel	60-62 Swan Street, Richmond	32
13.	HO429	-	400-402 Burnley Street, Richmond	32
14.	HO440	Swan Street Drill Hall	309 Swan Street, Richmond	33
15.	HO441	-	319 Swan Street, Richmond	34
16.	HO496	St Johns Church Complex	61-87 Queens Parade, Clifton Hill	34
17.	HO498	Row of houses, Elizabeth Terrace	472-484 Napier Street, Fitzroy North	35
18.	HO504	Clifton Motors (former)	205-211 Queens Parade, Fitzroy North	36
19.	HO522	Shop and Residence	273A Swan Street, Richmond	36
20.	HO523	Pair of Shops	323-325 Swan Street, Richmond	37

Individually Significant Places within a Precinct

	Precinct HO No.	Name	Address	Page No.
1.	HO317	-	12 Hodgkinson Street, Clifton Hill	38
2.	HO317	William B Fox House, later House of Gentle Bunyip	94 Hodgkinson Street, Clifton Hill	38
3.	HO317	Baptist Church & Hall	100 Hodgkinson Street, Clifton Hill	38
4.	HO317	Residence	122 Hodgkinson Street, Clifton Hill	38
5.	HO317	Fire Station No. 11 (former)	662 Smith Street, Clifton Hill	38
6.	HO327	National Bank, former	460 Brunswick Street, Fitzroy North	39
7.	HO327	House	17 Delbridge Street, Fitzroy North	39
8.	HO327	-	218 McKean Street, Fitzroy North	40
9.	HO327	-	220 McKean Street, Fitzroy North	40
10.	HO327	Hatherlie	224 McKean Street, Fitzroy North	40
11.	HO327	K. G. Luke Pty Ltd factory (former)	26-52 Queens Parade, Fitzroy North	40

Attachment 1 - Amendment C231 - GJM Heritage Analysis and Recommendations

	Precinct HO No.	Name	Address	Page No.
12.	HO327	Selotta Shoes Pty Ltd factory (former)	122 Queens Parade, Fitzroy North	41
13.	HO327	Christian Brothers Monastery, later units, G Maher House	256-262 Queens Parade, Fitzroy North	41
14.	HO330	Clifton Hill Hotel	89 Queens Parade, Clifton Hill	42
15.	HO330	Melbourne Savings Bank, later State Savings Bank	97-99 Queens Parade, Clifton Hill	42
16.	HO330	Doctor's surgery and house (former)	105 Queens Parade, Clifton Hill	42
17.	HO330	Albert Hall, also Masonic Hall, later Samian Social Club	127-129 Queens Parade, Clifton Hill	43
18.	HO330	Daintons Family Hotel, later Normanby Hotel	139 Queens Parade, Clifton Hill	43
19.	HO330	Clifton Hill Post Office, former	141 Queens Parade, Clifton Hill	43
20.	HO330	National Bank of Australasia, former	270 Queens Parade, Fitzroy North	43
21.	HO335	Shops and Residences	454-456 Church Street, Cremorne	44
22.	HO335	Corner Hotel	57-61 Swan Street, Richmond	44
23.	HO335	H. E. McNaughton ironmongery & residence, former	69 Swan Street, Richmond	44
24.	HO335	National Bank of Australasia	105 Swan Street, Richmond	45
25.	HO335	Shops and residences	232-234 Swan Street, Richmond	45
26.	HO335	M. Ball & Co, former	236-240 Swan Street, Richmond	45
27.	HO335	Whitehorse Hotel, former	250-252 Swan Street, Richmond	45
28.	HO474	Shops and Residences	413-415 Swan Street, Richmond	45

STATEMENTS OF SIGNIFICANCE

PRECINCTS

1. Bendigo Street Precinct, Richmond (HO309)

Source: Graeme Butler & Associates (2007, 2013), *City of Yarra Review of Heritage Overlay Areas*
Updated by GJM Heritage (2017), *Swan Street Built Form Review*

What is significant?

The Bendigo Street Heritage Overlay Area was part of Crown Allotment 16 as sold to J M Chisholm in 1840.17 Plans from 1895 show that, by then, three-quarters of the area was developed with housing, mostly in Park Grove, Kimber, Brooks and Survey Streets. The Wertheim Piano Factory (HO224), later the GTV9 television studios, dominated the area from the Edwardian-era. The factory was once the largest piano factory in Australia, occupying a four acre site, complete with its own power generator and tramline. Designed c1909 by architect Nahum Barnet, the buildings are important heritage elements in the streetscape.

Nearby Richmond Park (now Burnley Park was the pleasure ground for this area as well as the rest of inner Melbourne and now forms the eastern boundary of the area.

Main development era

The main development era evident in the heritage overlay is that of the Victorian and Edwardian-eras, with a contribution from well preserved inter-war buildings and individually significant places of all eras, such as the former Wertheim Piano Factory.

Contributory elements

The contributory buildings in the Bendigo St Heritage Overlay Area include mainly (but not exclusively) small attached and detached Victorian-era and Edwardian-era one-storey houses, but with some well preserved residential examples from the immediate post First-War era, having typically:

- Consistent building scale and setbacks
- Pitched gabled or hipped roofs, with some façade parapets;
- One storey wall heights;
- Weatherboard, face brick (red, bichrome and polychrome), bluestone, or stucco walls;
- Corrugated iron roof cladding, Marseilles pattern terra-cotta tiles, with some slate roofing;
- Chimneys of either stucco finish (with moulded caps) or of matching face brickwork with corbelled capping courses;
- Post-supported verandah or porch elements facing the street;
- Less than 40% of the street wall face comprised with openings such as windows and doors; and
- Front gardens, originally bordered by typically timber picket front fences or wire fences (inter-war) of around 1m height; also
- Corner shops and residences with large display windows and zero boundary setbacks.

Public infrastructure, expressive of the Victorian and Edwardian-eras such as stone pitched road paving, kerbs and channels, and asphalt paved footpaths

How is it significant?

HO309 Bendigo Street Heritage Overlay Area, Richmond is **aesthetically and historically** significant to the City of Yarra (National Estate Register [NER] Criteria E1, A4)

Why is it significant

The Bendigo Street Heritage Overlay Area is significant for:

- Its substantially intact single-storey Victorian-era housing that varies between modest and

PRECINCTS

ornate;

- Edwardian house examples, particularly in Swan and Moore Streets, being both typical and highly decorated Edwardian dwelling types, complemented by the significant Edwardian-era former Wertheim Piano Factory;
- The retention of early materials and elements in the public domain, such as street construction, and the retention of many bluestone laneways;
- The demonstration of a typical 19th century suburban area with its attached and detached housing stock and corner shops, that has been subsequently lost in other parts of the inner suburbs; and
- The consistency of building scale and setbacks, creating cohesive and homogeneous streetscapes that are enhanced by mature plane tree avenue plantings.

2. Church Street Precinct, Richmond (HO315)

Source: Graeme Butler & Associates (2012), *City of Yarra Heritage Gaps Review (Review 1)*

What is significant?

Subdivision

The Church Street Heritage Overlay Area includes parts of the first suburban Crown Allotments sold in the City, being Crown Portions 20, 21, 26 and 27, each 25 acres in area. Allotments 21 and 22 were purchased in 1839 by Rev. Joseph Docker (squatter), Allotment 20 by Charles Williams (auctioneer), and Allotment 26 by W H Yaldwyn (squatter, banker).

The Waltham Street and Darlington Parade areas were subdivided in 1853 and further subdivided in the 1880s. Docker had subdivided his two allotments by 1853, with plans for a model village set out on the flat below his townhouse at 370 Church Street. By the turn of the century, most of the Heritage Overlay Area was developed. The topography of the Heritage Overlay Area, the highest point in Richmond, attracted both the churches and the wealthier colonists with the result that the majority of the earliest residences were of a more substantial nature compared with other sections of Richmond.

This area has remained one of the most prestigious parts of Richmond for residential development. As an example, Howard Lawson's Elmhurst Flat block of 1934 aimed to tap into this prestigious residential location, paralleling with his significant Hollywood style Beverley Hills and Stratton Heights Flats, sited across the Yarra River at South Yarra.

Large villas

By 1855, villas with large gardens and orchards had been established in Church Street between Brougham and Elm Streets. Early houses which survive include Doery House (353 Church Street) and Messenger House (333 Church Street, formerly Stonehenge), the latter being built prior to 1843 for Captain John Roach4 and remodelled in the Edwardian period.

Major church complexes

Three major church complexes were established in the Heritage Overlay Area in the mid-1800s. St Stephen's Anglican Church (1850-1876) at 360 Church Street was designed by Blackburn and Newson on land donated to the church by the Rev. Joseph Docker and is one of the earliest bluestone churches built in Victoria. The Wesleyans began the construction of a temporary timber chapel (later the schoolhouse) in 1853, bluestone chapel in 1858, and added a schoolhouse (1871) and a parsonage (1876). St Ignatius' Roman Catholic Church (326) was built in stages between 1867 and 1928, to a design by prominent architect William Wardell, with the bluestone Presbytery added in 1872.

PRECINCTS

Civic buildings
Other non-residential developments in the Heritage Overlay Area included the former Richmond United Friendly Society Dispensary (1884; 294 Church Street), and the Hibernian Hall (1872; 316 Church Street), which was built as a temperance hall. The Richmond RSL was built in 1922, as an expression of the continuing premier civic status of this part of Church Street in the 20th century. The Richmond Library is the most recent civic development in the area.

Commercial development
Commercial development extended north from the major thoroughfares of Swan St and south from Bridge Road in the late Victorian and Edwardian-eras.

Main development era
The main development period evident in the heritage overlay is that of the Victorian and Edwardian-period, with a contribution from some well preserved inter-war buildings and individually significant places of all eras. Large houses, religious and public buildings from this period are the key elements in the heritage overlay.

Contributory elements
The Church Street Heritage Overlay contributory elements include (but not exclusively) large detached Victorian-era and Edwardian-era and small attached Victorian-era, one and two storey houses having typically:

- pitched gabled or hipped roofs, with some façade parapets;
- face brick (red, bichrome and polychrome) or stucco walls;
- corrugated iron, unglazed Marseilles pattern terra-cotta tiles, and slate roofing;
- chimneys of either stucco finish (with moulded caps) or of matching face brickwork with corbelled capping courses;
- typically with post-supported verandah elements facing the street;
- less than 40% of the street wall face comprised with openings such as windows and doors; and
- front gardens, originally bordered typically by timber picket front fences of around 1m height;

Contributory elements also include shops and residences of the Victorian and Edwardian-eras, with:

- display windows and recessed entries;
- zero boundary setbacks;
- mainly one storey scale;
- attached rectilinear plan form, a parapeted roofline; and

Substantial Victorian-era ecclesiastical buildings with:

- free standing rectilinear form
- pitched roofs, some towers and spires positioned to be visible from a distance;
- fenced yards, with potential use of timber or iron pickets and a stone base for the frontage fence;
- two storey and greater wall heights;
- stone, masonry or stuccoed masonry facades, slate or tiled roofs; and
- less than 40% of the street wall face comprised with openings such as windows and doors.

Contributory elements also include:

- Public infrastructure, expressive of the Victorian and Edwardian-eras such as stone pitched road paving, kerbs and channels, and asphalt paved footpaths - mainly in side streets; and
- Many significant buildings within the Heritage Overlay Area have their own heritage overlay (HO241, HO242, and the St Ignatius complex) but nevertheless are contributory to the Church St Heritage Overlay Area.

PRECINCTS

How is it significant?
HO315 Church Street Heritage Overlay Area, Richmond is aesthetically and historically significant to the City of Yarra (National Estate Register [NER] Criteria E1, A4).

Why is it significant?
The Church Street Heritage Overlay Area is significant:

- As one of the first parts of Richmond to be subdivided and developed, as expressed by early buildings like Messenger House 333 Church Street, from the 1840s;
- As the chosen site for a high number of individually significant 19th and early 20th century buildings set in grounds and including early ecclesiastical and civic buildings, and some Melbourne landmarks, as well as substantial residential buildings that were attracted to the area by its elevated topography, high amenity and proximity to churches;
- As the site of key civic or institutional buildings in Richmond from the 19th century through to the 1920s (i.e. The Richmond RSL Hall); and
- For its significant architecture such as the William Wardell designed St. Ignatius Roman Catholic Church as a well known and prominent landmark across the metropolitan area.

The heritage character of the precinct is also supported by the commercial development extending up Church St from the Swan St and Bridge Road shopping areas with shops dating from the late 19th and early 20th centuries, as part of the cultural context of Victorian and Edwardian-era life on the hill.

3. Clifton Hill Western Precinct (HO317)

Source: Graeme Butler & Associates (2007, 2013), *City of Yarra Review of Heritage Overlay Areas* [see Study for footnote references]

What is significant?
Survey
Following the establishment of the East Collingwood Council in 1855, The East Collingwood Local Committee successfully sought permission for East Collingwood to annex what is now Clifton Hill. East Collingwood wanted to extend East Collingwood's north south streets (Smith, Wellington and Hoddle) northwards to the Heidelberg Road (today's Queens Parade) to tap the traffic and trade coming from country areas such as Heidelberg; to gain access to the quarrying area for street making materials; and for space to erect public buildings, such as a town hall and market.

Distinguished naturalist and engineer, Clement Hodgkinson, as Victorian Assistant-Commissioner of Crown Lands and Survey 1861-74, was responsible for the government subdivisions of Carlton (south of Princes St, 1860), North Carlton, North Fitzroy and Clifton Hill (1865-9), Hotham Hill (1866), South and North Parkville (1868-9). Under his supervision, suburban planning employed the cost-efficient grid system used by Hoddle. Hodgkinson's predecessor.

The P. M. Goldbrick's 1864 *Plan of Allotments in the Boroughs of East Collingwood and Fitzroy* shows the distinctive geometric forms of Darling Gardens and Mayor's Park in Clifton Hill and Edinburgh Gardens in North Fitzroy.35 The streets of Clifton Hill, North Fitzroy and North Carlton, as set out by the Colonial Government, were broader and better ordered than those created privately in Richmond, South Fitzroy and Collingwood, with the added distinction of the curved street forms in Fitzroy North.

Sale of land commenced in the 1860s with ample reserves for public purposes, the Darling Gardens and Mayors Park, a Market Reserve in Smith Street extending from Alexandra Parade to Council Street, a Public Baths Reserve in Smith Street from Council Street to Hodgkinson Street, a School Reserve in Gold Street and five Church Reserves throughout the area. By way of contrast, all the

land sold in East Clifton Hill between1871-1874 had no provision for public reserves.

Early Residential Development
In the three years from the first land sales in 1864 only six houses were built and in the next five years to 1872, 58 houses, four commercial premises (Queens Parade) and one industrial building (in Hoddle Street) were built. Early residential concentrations were at the west end of Alexandra Parade and the west end of Hodgkinson and Council Streets. These were near the horse drawn omnibuses to Northcote along Smith Street and Queens Parade (started in 1869) and employment opportunities at the Collingwood Gas Works in Smith Street, opened in 1861.

Amenity protection
Members of the public, including councils and organised pressure groups, wrote annually in relation to crown land reserves. In 1862-62 a petition from the 'Municipal District of East Collingwood' was presented to the Legislative Assembly citing the good work of the local Vigilance Committee towards improving Clifton Hill.36
The level of community involvement engaged in 'articulating public interest priorities' can be gauged by the level and nature of the correspondence of which there was a great deal.37 Demands for public parks, recreation areas, sporting grounds and botanic gardens increased.38
More recently, The House of the Gentle Bunyip, constructed in three phases by two families between 1867 and the 1920s was the subject of controversy in the late 1990s when the local community successfully saved the building from demolition, after staging a picket lasting over 400 days.

Early public gardens
Preparations for planting the two Reserves (Darling Gardens and Mayors Park) proceeded with an application made in 1864 to Dr. Mueller FRS, Botanical Gardens Melbourne, for an assortment of trees, plants and shrubs and a liberal supply of Victorian *Hakea*, for the planting of Darling Gardens.39 Many significant trees in Mayors Park and Darling Gardens have been identified as significant, including elm groups, cotton and Canary Island palms, and oaks.40 The National Trust of Australia (Vic) has identified a *Quercus ilex* and *Ulmus procera* as significant trees in the Darling Gardens.

Trees
The 1897 MMBW Record Plan 29 shows that Hoddle Street had a double avenue of trees in Clifton Hill, Wellington Street was fully planted both sides with street trees from one end to the other, and Noone Street had trees at the east end. North Terrace had ten trees planted by local identity, George Langridge, MLA and former Mayor.

Boom era
During the metropolitan boom of 1881-91, the population of Collingwood increased by half, from 23,829 to 35,070, and the number of dwellings rose from nearly 5,000 to 7,000, mostly reflecting the development of Clifton Hill.41 The character of the area, as later described in *Victoria and its Metropolis*, was of a residential suburb "... that has of late years been extensively built on with a good class of houses and numerous handsome shops. It has an elevated position, and commands an excellent view of the metropolis."42 Local politicians and business figures (such as George Langridge) promoted development and aided in home ownership via building societies.

By the end of the boom in 1891, West Clifton Hill was almost fully developed. The 1897 MMBW record plan shows few vacant blocks. Most of this development took place between 1881 and

PRECINCTS

1891, more quickly and more completely than in East Clifton Hill, North Fitzroy or North Carlton, the other government planned suburbs. This growth was due to Clifton Hill's closer proximity to employment and commerce in the south, with industries in Collingwood and Abbotsford being within walking distance of Clifton Hill, and aided by cable trams running along Queens Parade and down Smith Street.

Victorian-era industrial development
Industries such as the Clifton Hill Brewery, Ridgeway's Tannery and Stacey's Boot Factory, continued to be located in the south east of the area fronting Alexandra Parade and Hoddle Street, adjacent to the former Reilly Street drain that was essential for effluent disposal. There was also Richard Hodgson's distinctive shot manufacturer's tower.

Post Boom
The early years of the 20th century saw some reinvigoration of industry after the financial trials of the 1890s. Comparatively little residential development took place after the Boom, except for development in two small subdivisions of the original Crown allotments in streets around the Darling Gardens.

Main development period
The main development period evident in the heritage overlay is that of the Victorian-era with a substantial contribution from the Edwardian-period. There is also a contribution from some well preserved inter-war buildings and individually significant places of all eras.

Contributory elements
Contributory buildings are typically (but not exclusively) detached and attached Victorian-era and Edwardian-era mainly one-storey houses having:

- Expressed pitched gabled or hipped roofs, with some façade parapets;
- Mainly one storey wall heights but with some two storey house rows;
- Weatherboard, face brick (red, bichrome and polychrome), or stucco walls;
- Corrugated iron roof cladding, Marseilles pattern terra-cotta tiles, with some slate roofing;
- Chimneys of either stucco finish (with moulded caps) or of matching face brickwork with corbelled capping courses;
- Typically with post-supported verandah elements facing the street, set out on two levels as required with cast-iron detailing;
- Less than 40% of the street wall face comprised with openings such as windows and doors; and
- Front gardens, originally bordered by typically timber picket front fences of around 1m height; and
- Corner shops and residences with display windows and zero boundary setbacks.

And

- Well preserved buildings, including one storey houses plus one and two storey industrial buildings from the pre Second World War era;
- The Darling Gardens as a typical Victorian-era Garden Square, with mature exotic formally arranged planting;
- Significant and mature park, street and private tree plantings (including plane, oak and elm trees, palms in Mayors Park, and National Trust of Australia (Vic) significant trees);
- Provision of public and church reserves in the town plan; and
- Public infrastructure, expressive of the Victorian and Edwardian-eras such as bluestone pitched road paving, crossings, stone kerbs, and channels, and asphalt paved footpaths.

PRECINCTS

How is it significant?
HO317 Clifton Hill Western Heritage Overlay Area is aesthetically, socially and historically significant to the City of Yarra (National Estate Register [NER] Criteria E1, A4)

Why is it significant?
Clifton Hill Western Heritage Overlay Area annexed by East Collingwood Council in 1855 and substantially developed by World War One is significant:

- As an expression, in the history of its creation as part of Collingwood East, of the aspirations of tradesmen of the Collingwood Slope for improved urban development and the provision of public facilities;
- For the role of local politicians and business figures in social, physical and industrial improvement and home ownership in the area;
- As the focus of community sentiment and resident protest from its initial development in the 1860s, from the actions of the Victorian-era Vigilance Committee to the preservation movements of the 20th Century, specifically the House of the Gentle Bunyip conservation campaign;
- For the distinctive street and allotment layout created under Government surveyor, Clement Hodgkinson43, and the liberal allowance for garden squares and reserves for a market, public baths, a school, a recreation reserve and five churches, contrasting with private surveys in the City of Yarra;
- For Hoddle's provision of boulevards (Hoddle Street and Queen's Parade) skilfully merged with wide streets on a north south grid, with unusual five way intersections and triangular public spaces at each end of Queen's Parade;
- For the unique series of vistas from the crest of the hill at the end of Hoddle street that includes views of significant landmarks like the shot tower, spires of St. Johns and the former Wesleyan Church, Town Hall towers at Collingwood and Fitzroy, the Exhibition Buildings, and long vistas to the Kew hill and Studley Park, plus those to and from the Darling Gardens;
- For the area's fine representation of Victorian-era life, as expressed by the street and public reserves, along with the dominantly Victorian-era building stock including churches, a State school, factories, early houses, a large number of late Victorian terrace houses, attached and freestanding, and key house designs such as 12 North Terrace.
- For its focus on a representative and intact example of a residential garden square (Darling Gardens) based on British and European precedents, with associated significant trees;
- For the mature, largely exotic trees, including those planted by George David Langridge, MLA and former Mayor, bluestone kerbs, gutters, drains, channelling, and laneways from the area's major period of development;
- For the strong industrial theme, seen in the south of the area with several 19th Century industrial buildings including former boot factories, and a carriage factory, that links the area with the City's other creek and riverside industrial localities such as Clifton Hill East, Abbotsford and Collingwood;
- For well preserved Edwardian housing estates and individual houses as complements to the dominant Victorian-era residences; and
- For good examples of the Californian Bungalow style and other well preserved interwar houses.

PRECINCTS

4. North Fitzroy Precinct (HO327)

Source: Graeme Butler & Associates (2007, 2013), *City of Yarra Review of Heritage Overlay Areas* [see Study for footnote references]

North Fitzroy has two sub-areas:
(A) The Government planned section south of Holden St
(B) The privately planned section north of Holden St

(A) NORTH FITZROY HERITAGE OVERLAY AREA (SOUTH OF HOLDEN ST)

What is significant?
Early subdivision
The south section of North Fitzroy (south of Holden Street) was retained in Crown ownership until 1865, as part of the Melbourne township reserve. This was a ring of land extending 5 miles from Hoddle's original Melbourne town plan that was set aside in 1844 for 'orderly' development in government-planned subdivisions.75

North of Holden Street, large agricultural allotments had been sold in 1839, their boundaries surviving in the east-west streets of Miller, Barkly and Clauscen Streets. There was also an area, bounded by (approximately) Nicholson, Church, Rae and Scotchmer Sts, that was sold as private quarry allotments in 1851 to encourage production of building stone.76

The future suburb was bordered by a road to Heidelberg and the Plenty districts (later Queen's Parade) that ran diagonally to Hoddle's survey grid through the North Fitzroy's Crown reserve. It was proclaimed in 1850 as one of Melbourne's 3 chain (60 metre) government roads, now called 'Hoddle boulevards'.

To the north, the township of *Northcote on the Merri-Merri Creek* (later Westgarth) was laid out by Hoddle in 1852 with some allotments extending south of the Merri Creek to Rushall Crescent in North Fitzroy. These allotments now have substantial buildings dating from the 1850s and 1860s (see Queen's Parade Heritage Area, HO331).

St. George's Rd was another diagonal route, on the western flank of the area, used to transport construction materials to the 1854-8 Yan Yean water supply scheme near Whittlesea.77 By 1860, Fitzroy Council annexed the 480 acres now comprising North Fitzroy.

Distinguished naturalist and engineer, Clement Hodgkinson, as Victorian Assistant-Commissioner of Crown Lands and Survey 1861-74, was responsible for the government subdivisions of Carlton (south of Princes St, 1860), North Carlton, North Fitzroy and Clifton Hill (1865-9), Hotham Hill (1866), South Parkville and North Parkville (1868-9). Under his supervision, suburban planning employed the cost-efficient grid system used by Hoddle. A model town design78 in the area by Hodgkinson's predecessor. Andrew Clarke (the designer of St Vincent's Place, South Melbourne), is thought to have inspired the curved streets of Alfred and Rushall Crescents in North Fitzroy, although both streets were laid out under Hodgkinson.79

Edinburgh Gardens
Lacking a public recreation reserve, the new Fitzroy Council was temporarily granted 7 acres in 1858 bounded by Reilly Street (Alexandra Parade), Queens Parade and Smith St.80 The 1858 reserve was for a future Anglican parish church and the Collingwood (later the Metropolitan) Gas Company which commenced production in 1861. In 1862, Fitzroy Council requested an oval-shaped 50 acre public

PRECINCTS

reserve flanking the Yan Yean tramway (St. George's Road). The oval reserve was laid out under Hodgkinson and extended south in 1863, as a squared-off addition to the reserve to the line of Freeman St, to provide the Prince of Wales Cricket Club an extra playing ground.81 In 1882-3, Edinburgh Gardens was permanently granted to the Council and planting of its avenues commenced.

Land sales and development
Sale of North Fitzroy's ¼ - ½ acre allotments commenced in 1865, extending west from Rushall Crescent. Further east, lots between Brunswick and Nicholson Street were sold between 1867 and 1875.82 Settlement increased after 1869 when horse-drawn omnibuses began running from North Fitzroy along Nicholson St and Queen's Parade to the city. Development concentrated around the established quarry route (Nicholson Street) and the road to the Yan Yean Reservoir (St. George's Road).

St. Brigid's Catholic Church in Nicholson Street was commenced in 1869 and the Methodist Church, further north, in 1874. In the pre-Boom years (before 1883) this was North Fitzroy's most established area, characterised by modestly scaled brick and wood houses, shops, hotels, and commercial premises, the latter prevailing on main routes such as Rae, Reid, Brunswick Sts and St Georges Rd.

North Fitzroy Primary School was built in Alfred Crescent (HO212) overlooking Edinburgh Gardens. This was typical of 19th century government-planned suburbs where public schools were located on or near public recreation reserves. A police station was established west of the school, giving the north-west corner of the gardens a civic character, embellished by an ornamental fountain after the turn of the century.83 St Luke's Church of England in St Georges Rd was commenced in 1879.

Suburban development was rare east of St Georges Road prior to the Boom, but in 1869 philanthropist George Coppin created the Old Actor's Association village overlooking Merri Creek close to Northcote, later the site of the Old Colonists' Association (HO218). Nearby land was granted to the Licensed Victuallers' Association for asylum homes and a school (site of Fitzroy Secondary School since 1915).84

Transport
When cable tram routes along Queen's Parade, Nicholson Street and St. Georges Road commenced construction in 1883, North Fitzroy landowners began subdividing their allotments. On the Nicholson Street tram route, owners of the 1850s stone quarries and the 1839 farm allotments north of Holden St, followed suit.

When tram services began in 1887 many new houses were ready for sale or under construction. North Fitzroy emerged as a late-Victorian commuter suburb with local shopping strips along the cable tram routes, the commercial strip of St Georges Road extending east and west along Scotchmer Street. Convenience shops were built on pedestrian street corners as households multiplied. The Inner Circle Railway running via Royal Park, North Carlton, Nicholson St, and Clifton Hill was completed in 1888 with a spur line dividing the Edinburgh Gardens into two, and terminating at the 'Fitzroy' station, Queen's Parade. A new rail link direct to the city (the Clifton Hill to Princes Bridge line) opened in 1901 and saw Edwardian buildings filling out the suburban streets east of St. Georges Road and the revitalising of the main shopping strips.85 Public transport continued to support North Fitzroy as a commuter suburb during the 1920s with the opening of Rushall railway station and the electrification and extension of the St Georges Rd and Nicholson St tramways.

North Fitzroy's suburban development, especially east of St George's Road to Rushall Crescent, was far from complete when the Boom collapsed in 1893. However its good access to public transport led to a rapid recovery. A number of small factories were built in or near the 19th century commercial strip of Scotchmer St, the best architectural examples being of the late 1930s.

PRECINCTS

Main development era, south of Holden St
The main development period evident in the heritage overlay south of Holden St is that of the Victorian era with a substantial contribution from the Edwardian-period. There is also a contribution from some well-preserved inter-war buildings and individually significant places of all eras.

Contributory elements, south of Holden St
The North Fitzroy Heritage Overlay Area (south of Holden St) contributory elements include (but not exclusively) generally detached and attached Victorian-era and Edwardian-era houses having:

- Pitched gabled or hipped roofs, with some façade parapets, many elaborate;
- One storey wall heights but with many two storey house rows,
- Rectilinear floor plans, with many bayed room projections on the plans of large houses;
- Face brick (red, bichrome and polychrome) or stucco walls, some weatherboard;
- Corrugated iron and slate roof cladding, some Marseilles pattern terra-cotta tiles;
- Chimneys of either stucco finish (with moulded caps) or of face brickwork with corbelled or cemented capping courses;
- Post-supported verandah elements facing the street, set out on two levels as required with cast-iron and timber detailing, and many curved verandah wing-walls;
- Less than 40% of the street wall face comprised with openings such as windows and doors; and
- Front gardens, originally bordered by timber or iron picket front fences of around 1m height; often set between brick or cemented pillars and curved brick garden wing walls.
- Face brick (typically red) privies set on rear lanes, with some stables and lofts to the larger houses;
- Shops and residences sited on corners with display windows and zero boundary setbacks;

Contributory elements also include attached Victorian and Edwardian-era shops, and residences over, with

- Façade parapets and pitched roofs behind,
- Two storey wall heights,
- Post-supported street verandahs as shown on the MMBW Detail Plans,86
- No front or side setbacks; also
- Timber framed display windows and entry recesses.

Contributory elements also include:

- Well preserved buildings from the pre Second War era, that are visually related to the dominant scale, siting and form of the area;
- Service buildings, like pre WW2 electric substations;
- The distinctive suburban plan, enhanced by curving street forms, and defined by boulevards;
- Edinburgh Gardens as a Victorian-era residential circus, and for the other garden, boulevard and median reserves (such as Queens Parade), with mature exotic and formally arranged planting;
- Mature street tree plantings (such as plane and elm tree rows, Queensland brush box);
- The provision of public and church reserves in the town plan, including the Inner Circle Railway reserve;
- The rectilinear Victorian-era allotment plans and street layout with wide main streets, rear service lanes, all counter posed with circular parkland reserves, major angled streets and boulevards;
- The dominance of spires and towers of public buildings, churches and some large houses in the skyline; and
- Public infrastructure, expressive of the Victorian and Edwardian-eras such as bluestone pitched road paving, crossings, stone kerbs, and channels, and asphalt paved footpaths.

PRECINCTS

How is it significant?

HO327 North Fitzroy Heritage Overlay Area (south of Holden St) is aesthetically and historically significant to the City of Yarra (National Estate Register [NER] Criteria E1, A4)

Why is it significant

The Government planned section of Fitzroy North (south of Holden St) is significant

- As a demonstration of the earliest stages in the development of North Fitzroy, commencing with the 1850s Yan Yean tramway and the quarry route of Nicholson St, the establishment of North Fitzroy's first churches and its pre-Boom suburban mixture of small residential and commercial buildings coinciding with provision of horse drawn public transport.;
- For its late 19th century buildings that represent rapid growth and change in the character of the relatively remote suburb to an established residential and business area with a range of commercial and institutional buildings serving the wider population of North Fitzroy;
- For the aesthetic value of its suburban planning, with the combination of curving streets and garden reserves, and the vistas created by the intersections of standard rectilinear subdivision with the strong diagonals formed by St Georges Road and Queens Parade, the exceptionally wide streets and crescents, the triangular garden reserves, and focal views to buildings and parkland. This aesthetic is underscored by the built form seen in the construction of commercial and institutional buildings to the property alignment along a number of streets and on corners;
- The generous public domain, with street plantings of historic elms and 20th century plane trees,
- For its low rise streetscapes of intact Victorian and Edwardian buildings one and two-storey terrace and villa housing;
- For the range of Victorian and Edwardian-era building form and finish, from the modestly decorated timber cottages and stuccoed Italianate style houses, simply designed corner hotels and shops, to the rich variety of decorative buildings including an extraordinary concentration of decorative bichrome and polychrome brickwork and flamboyantly decorated Italianate residential and commercial terraces, banks and hotels, interspersed very occasionally with dominating narrow spire and tower elements of religious, commercial and institutional buildings.
- As one of Melbourne's early residential commuter suburbs served by train and cable tram services linking it to the city by 1888, with extensive Boom era terrace buildings and closely built row housing within this sub-area providing evidence of the effects of public transport on early development;
- As essentially a well preserved Boom-era suburb that, despite the abrupt economic collapse of the early 1890s, continued developing during the Great Depression years and into the first decades of the 20th century due to the amenity of its planning, parkland, local schools and shops and extensive public transport. This yielded both the generous frontages and sizes of the post-Depression villa houses and the row house forms and narrow frontages of the Boom era;
- For its traditional Victorian-era residential character, evoked by the formal presentation of the decorated façade to the street with its small ornamental front garden, low front fence, pedestrian gateway and front path, with the functional necessities of delivering coal, removal of nightsoil and occasional stabling provided by the back lanes;
- For landmark buildings and sites that formed key meeting places in the area during the main development era of the 1860s to the 1930s, including religious institutions, schools, monasteries and churches, and the buildings associated with charitable bodies such as the Salvation Army, Church of Christ Bible School, and the temperance movement. Also the former Nth Fitzroy Police Station, the former Licensed Victualler's School and Asylum site and complexes such as the Old Colonists Homes;
- For the asphalt footpaths, pitched lanes, gutters and lane crossovers and mature street and

PRECINCTS

individual plantings (such as mature elms, planes, palms, and Kurrajongs) that reinforce the unified character of the dense, relatively low-rise residential development;

- The 19th century landscape of Edinburgh Gardens and its representation of Fitzroy's cultural history in its plantings, memorials, recreation sports club grounds and pavilion buildings, plus the Inner Circle Railway reserve as a cultural landscape strip across the north of the area;
- For the outstanding Victorian and Edwardian-era streetscapes such as those surrounding the Edinburgh Gardens (Alfred Crescent, St Georges Road, Brunswick and Freeman Streets) that include a rich collection of Victorian-era Gothic and Italianate style buildings interspersed with fine buildings from the Edwardian period;
- For the important views and vistas within the area, including those of the Edinburgh Gardens, its mature trees and historic structures, as seen from many parts of the Heritage Overlay Area, and views obtained from Edinburgh Gardens to the many significant buildings at its curtilage and the city skyline, the vista from the elevated position of the Cricket Club grandstand toward the upper façades of buildings in Freeman and Brunswick Streets, and the Brunswick Street vistas (south to the spire of St Patrick's Cathedral, and north to St Luke's spire)
- For the contribution of well-preserved inter-war buildings, particularly the small intact inter-war houses where the building design has adapted to the prevailing built character of the area in siting, scale, decorative quality and stylistic variety.

(B) NORTH FITZROY HERITAGE OVERLAY AREA (NORTH OF AND INCLUDING HOLDEN ST)

What is significant?

Development north of Holden St

(Refer also to *Development south of Holden St*)

North of Holden St, the 1839 farm allotments closest to Nicholson St were subdivided for row-housing during the late Victorian-era Boom years but remained empty until after 1900. Suburban development followed in the years to 1920 with timber villas on relatively wide (approximately 10 metre frontages). Some empty sites were developed in the 1930s and 1940s with medium density, duplex and estate housing development, some of these being architecturally distinctive. Mature street tree planting includes plane trees, typical of Edwardian-era and inter-war planting. The Merri State (now Primary) School 3110 and landscaping, in Miller St, is a key early non-residential building in this part of the Heritage Overlay Area.

Main development era, north of Holden St

The main development period evident in the heritage overlay is that of the Edwardian-period with a contribution from the Victorian era. There is also a contribution from some well preserved inter-war buildings and individually significant places of all eras.

Contributory elements, north of Holden St

The North Fitzroy Heritage Overlay Area (north of and including Holden St) contributory elements include (but not exclusively) generally detached (and some attached) Victorian-era and Edwardian-era mainly one-storey houses having typically:

- Pitched gabled or hipped roofs, with few façade parapets;
- One storey wall heights but with some two storey house rows;
- Weatherboard or block fronted wall cladding, with face brick (red, bichrome and polychrome), or stucco walls;
- Corrugated iron roof cladding, with some Marseilles pattern terra-cotta tiles, with some slate roofing;
- Chimneys of either stucco finish (with moulded caps) or of matching face brickwork with corbelled capping courses;
- Post-supported verandah elements facing the street, with timber (Edwardian-era) or cast-iron

PRECINCTS

detailing;
- Less than 40% of the street wall face comprised with openings such as windows and doors; and
- Front gardens, originally bordered by timber picket front fences of around 1m height.

Contributory elements also include:
- Corner shops and residences, with display windows and zero boundary setbacks;
- One and two storey commercial strips (i.e. St Georges Road) with some key corner Victorian and Edwardian-era buildings and well preserved inter-war examples;
- Well preserved buildings, including visually related one and two storey residential buildings from the pre Second War era, are contributory to the heritage overlay;
- Long east-west streets as former rural allotment boundaries, with repeating allotment sizes and rectilinear allotment forms;
- The inner circle railway reserve, with associated railway infrastructure;
- Mature street tree plantings (typically plane trees) and reserve planting (typically palms), and
- Public infrastructure, expressive of the Victorian and Edwardian-eras such as bluestone pitched road paving, crossings, stone kerbs and channels, and asphalt paved footpaths.

How is it significant?
HO327 North Fitzroy Heritage Overlay Area (NORTH OF AND INCLUDING HOLDEN ST) is aesthetically and historically significant to the City of Yarra (National Estate Register [NER] Criteria E1, A4)

Why is it significant
North Fitzroy Heritage Overlay Area (NORTH OF AND INCLUDING HOLDEN ST) is important:
- For the clear indication of important early land development phases in the suburb as follows:
  - East-west, relatively long and narrow streets originating from private subdivision of the 1839 agricultural allotments that originally fronted Merri Creek;
  - The evident effect of the 19th century Boom era when aggressive subdivision coincided with provision of public transport, creating dense late-Victorian development near the Nicholson St cable tram terminus;
  - The more generous post-1906 subdivisions (suiting the construction of detached timber villas) representing changed values in land prices, and the relatively new suburban ideal of affordable house and garden lifestyle;
  - The slow transformation into a completed residential suburb by WW2, with the last few vacant sites accommodating interwar dwellings, some developed with medium-density housing, duplexes and flats.
- For the consistent and intact streetscapes of late Victorian terrace and row housing in the south-west corner of the area, and Italianate style and Edwardian timber villas, interspersed occasionally with inter-war houses, in the balance of the area;
- For the visual consistency and good period expression of streets in the north and east parts of the area, consistently one-storey, timber construction, decorative detail, and detached siting with consistent garden setbacks;
- For the interwar medium density development pockets, including those contained within Pilkington St and parts of Barkly St, King and Miller Streets, significant for its intactness, form and high quality design.
- For the shopping strip streetscape of St Georges Rd, significant for its landmark Victorian-era buildings, plus intact early 20th century and interwar buildings in both Classical Revival and Moderne styles; and
- For important landscape and vistas, including the view of Merri Creek Bridge framed by palms and mature elms in the Merri Primary School reserve, and mature street plantings of plane trees.

PRECINCTS

5. Queens Parade Precinct, North Fitzroy/Clifton Hill (HO330)

Source: GJM Heritage (2017), *Queens Parade Built Form Heritage Analysis & Recommendations Report*

What is significant?
Queens Parade, North Fitzroy from south-west of Delbridge Street to Brennand Street (nos. 264-492); Queens Parade, Clifton Hill from Wellington Street to Turnbull Street (nos. 61-197), Queens Parade Road Reserve from St John the Baptist Church Presbytery to the railway overpass, and Raines Reserve. NOTE: 12 and 14 Brennand Street (not contributory) are included in HO330 in Appendix 8 but mapped in HO327.

History
First established as a track linking Melbourne and Heidelberg in the late 1830s, Queens Parade was one of Melbourne's first major roads. Originally extending from the top of Bourke Street, the route followed an irregular course to the north-east and continued across Merri Creek. The road was surveyed by Hoddle as a substantial three chain (60 metre) wide boulevard, reserved in 1853, and known as Heidelberg Road at the south-west end and Plenty Road further to the north-east and crossing Merri Creek. This thoroughfare to Northcote and Heidelberg formed part of the boundary between Clifton Hill and North Fitzroy and was renamed Queens Parade in the early twentieth century.

Allotments were sold in Queens Parade from 1853, on the northern side, south of Merri Creek as part of the Northcote township reserve. The Scotch Thistle Hotel, the present Terminus Hotel, had been built as part of this township by 1854 and some early bluestone terrace houses remain nearby at the corner of Rushall Crescent. Allotments fronting both sides of Queens Parade sold from 1864, however the street remained largely undeveloped until the construction of buildings such as the Daintons Family Hotel at the corner of Gold Street in 1875 and St John's Roman Catholic Church (now St John the Baptist Church), in 1876.

This thoroughfare between Melbourne and the northern districts experienced a development boom in the 1880s, with commercial functions serving local residents as well as those travelling through, and much of the present building stock in the precinct was constructed at this time. Development on the North Fitzroy (northern) side was predominantly two-storeys, including shops with residences above, such as the building at the corner of Michael Street (336-338 Queens Parade, 1870-1890) and banks, including the National Bank of Australasia (1886) and the three-storey landmark building, the former London Chartered Bank (later the ANZ Bank, 1889). The Clifton Hill (southern) side of the street developed with some shops, large commercial enterprises and some public buildings, including the Post and Telegraph Office (1893), a row of seven shops and residences known as Campi's Buildings (1880s), the State Savings Bank (1889), and the Clifton Hill Hotel (1884) on the south-east corner of Wellington Street.

Public transport was established in the area during the Boom years of the 1880s. A cable tram service was provided along the developing Queens Parade commercial strip in 1887, terminating at the Merri Creek and this was extended to Northcote in 1889. Increasing demand for public transport resulted in the opening of the Royal Park to Clifton Hill railway in 1888.

Development in Queens Parade decreased during the depression years of the 1890s, however a 1904 Melbourne and Metropolitan Board of Works detail plan indicates it was a tree lined commercial street with verandahs over the footpaths on both sides of Queens Parade by that time. Commercial development re-commenced in the early twentieth century, adding an Edwardian presence to the existing Victorian character of the precinct. Edwardian buildings include the Clifton Hill & Northcote United Friendly Societies Dispensary (1914), the Clifton Hill Post Office (c1915-20) and the former

PRECINCTS

doctor's surgery and residence (c1915). Additionally, a substantial group of detached Edwardian houses were built on the North Fitzroy side of Queens Parade, at the north-east end.

By the close of the Edwardian period (c1918) the streetscape, that continues today to mark the entrance to Melbourne's northern suburbs, had largely been established. Notable Interwar buildings were added to the streetscape in the 1930s including a pair of landmark Moderne buildings at the north-east end - the former United Kingdom Hotel (1937-38), located on the prominent intersection of Queens Parade and Heidelberg Road, and the nearby Clifton Motors Garage (1938 façade).
Extensive beautification took place in the inter-war period in the form of landscaping and further street tree plantings. Grassed median strips were planted with plane and elm trees at the western end and a triangular garden (Raines Reserve, originally Rain's Reserve) was created at the junction with Heidelberg Road.

Description
The main development period of the Queens Parade Precinct is from the 1870s to c1920 and the streetscape largely dates to this period. A small number of buildings from both earlier and later periods make a contribution to the precinct. A number of individually significant buildings are contained within the precinct, dating predominantly from the 1870s-1920 period.
Buildings are predominantly attached Victorian and Edwardian shops and residences. They are predominantly two-storey with some single-storey shops interspersed. Towards the north-eastern end of the precinct are a row of detached single and two-storey residences, predominantly from the Edwardian period.

Elements that contribute to the significance of the place include (but are not limited to):

- The wide, open boulevard character with associated landscaping (including Raines Reserve, the planted medians, bluestone pitched road paving, crossings, stone kerbs and channels, and asphalt paved footpaths).
- Views afforded to significant buildings in and beyond the precinct (St John the Baptist Church, former ANZ Bank building, and former United Kingdom Hotel)
- The consistent Victorian and Edwardian commercial character of the precinct, which includes:
  - A consistent two-storey scale with some single-storey buildings
  - A variety of simple and some highly decorative façade parapets, with pitched roofs behind
  - No front or side setbacks
  - Face red brick or rendered walls
  - Rendered window frames, sills and hoods to upper stories
  - Rendered ornament to upper stories
  - Brickwork with corbelled capping courses
  - Strong horizontal lines formed by parapets, cornices, string courses
  - Repetitive upper floor fenestration patterns
  - Corrugated iron, slate and terracotta tiled roof cladding
  - Decorative chimneys (some with pots)
  - Original post-supported street verandahs
  - Shop fronts with display windows, timber or tiled plinths, and entry recesses.
- The predominantly Edwardian residential character at the north-eastern end, which includes:
  - Steeply pitched gabled or hipped roofs, predominantly with terracotta tiles and terracotta ridge capping and/or finials
  - Prominent, often highly decorative brick chimneys
  - Prominent gable ends
  - Predominantly red brick construction
  - Front gardens

PRECINCTS

How is it significant?

Queens Parade, North Fitzroy/Clifton Hill is of local historical, architectural and aesthetic significance to the City of Yarra.

Why is it significant?

The Queens Parade Precinct is located on a major thoroughfare that has linked Melbourne to the northern suburbs since the 1830s. It is centred on one of Melbourne's early reserved boulevards, having been surveyed by Hoddle and reserved in 1853. The wide boulevard retains distinctive median strip landscaping from the twentieth century, including Raines Reserve. The Queens Parade Precinct is an intact turn of the century 'High Street' that has functioned continuously as a key commercial centre since the 1870s. It contains a substantial collection of intact commercial buildings, predominantly from the Victorian and Edwardian periods, including shops and associated residences, hotels and other commercial and civic buildings, as well as a group of detached Edwardian residences at the north-eastern end. Together these buildings demonstrate the development of this 'High Street', particularly from the 1870s to c1920 when substantial growth along the boulevard occurred, and are illustrative of the enduring role the street has played in the economic and social life of North Fitzroy and Clifton Hill since the establishment of the suburbs [Criterion A].

The highly intact streetscape of the Queens Parade Precinct clearly demonstrates the principal characteristics of a major turn of the century 'High Street' in the City of Yarra. Typical characteristics – including predominantly two-storey Victorian and Edwardian wall heights, parapeted rendered or red brick facades with repetitive upper floor fenestration, and ground floor shopfronts – are displayed in the original forms, fabric and detailing of many of the buildings. The streetscapes on the northern side of Queens Parade from Delbridge Street to 410 Queens Parade and on the southern side of Queens Parade from Wellington Street to 169 Queens Parade are particularly intact and are highly consistent in their architectural form and expression, forming a cohesive and significant streetscape. A group of detached houses at the north-eastern end of the precinct are intact and consistent in their Edwardian character and form [Criterion D].

The Queens Parade Precinct contains a number of landmark and individually significant buildings which are well considered and carefully detailed examples of commercial and civic buildings. These include, among others, the prominent three-storey Former London Chartered Bank of Australia Ltd (later ANZ Bank building), 370-374 Queens Parade, the former Daintons Family Hotel (139 Queens Parade, built 1875), the former Melbourne Savings Bank (97-99 Queens Parade, built 1889), former National Bank of Australasia (270 Queens Parade, built 1886), Ryans Building (314 Queens Parade, built 1884), Campi's Buildings (149-167 Queens Parade, built c1880-c1890) and the corner shop at 336-338 Queens Parade, North Fitzroy (c1870-c1890). Key views to the local landmarks of the St John the Baptist Church complex and the Former United Kingdom Hotel, 199 Queens Parade, which bookend the precinct are afforded from within the precinct [Criterion E].

6. Richmond Hill Precinct (HO332)

Source: Graeme Butler & Associates (2007, 2013), *City of Yarra Review of Heritage Overlay Areas* [see Study for footnote references]

ALL SUB-PRECINCTS

What is significant?

Subdivision types

The land which now comprises the Richmond Hill Area was purchased from the Crown in 1839 and consisted of six portions each of 25 acres in the parish of Jika Jika (CPs 21-26). The purchasers were Dr.

PRECINCTS

Farquhar McCrae (CP24, surgeon), Rev. Joseph Docker (CP21-22, squatter), W H Yaldwyn (CP26, squatter, banker), Henry Gordon Brock (CP25), and John McNall (CP23, Melbourne's first butcher)96.

From 1840 onwards, Dr. McCrae sold parts of his allotment (at the north west corner of the heritage overlay) to prominent Melbourne figures, including Alexander McCrae , Alfred Woolley, Charles Williams, George Arden, and Thomas Strode (proprietors of the *Port Phillip Gazette*).97 He also subdivided off 36 half acre blocks, creating Rowena Parade, Sherwood and Rotherwood Streets. Further subdivisions took place in the 1870s and 1880s.

Yaldwyn sold his allotment (at the north-east corner of the heritage overlay) to William Meek in 1840 and, by 1853, the Waltham Street and Darlington Parade areas were subdivided, and further subdivided in the 1880s. The Vaucluse area was laid out in the 1860s and a private street was created to link Rowena Parade and Church Street, for the use of abutting landholders only. The Vaucluse, thought to be the only Victorian-era private street in the inner suburbs, has its eastern entry at St Ignatius Church. Large houses were erected on this section, including Richmond Hill for James Henty (Waltham Street, 1851) and a two-storey Italianate house for distinguished architect, James Blackburn Jr.

Docker had subdivided his two allotments by 1853 (at the south-east corner of the heritage overlay), creating a model village (Village of Clifton) set out as 365 lots on a grid plan designed by Charles Laing. Its regular layout was unlike the other haphazardly planned private subdivisions in the area. The village was bordered by Wangaratta St (inclusive) on the west, the Richmond Terrace service lane on the north, the Church St on the east and Swan Street on the south98. By 1857 Docker still owned many of the houses built on these allotments. His own townhouse was at 370 Church Street. By the 1880s, this area was almost fully developed. (See South Residential Sub-precinct)

Brock's allotment (at the north-centre of the heritage overlay) was purchased by overlander John Gardiner99 who erected a villa on it when he retired from his grazing activities to take up the position of managing director of the Port Phillip Bank in 1840.100 By 1853, Lennox Street, Rowena Parade and Goodwood Street had been laid out. Further subdivisions occurred in the 1870s and 1880s.

The allotment purchased by McNall (at the south-west corner of the heritage overlay) had undergone minor subdivision by 1853. McNall erected his house there, and his widow continued to live there following his death in the 1850s.101 In 1859, the railway was established, and Richmond Station was constructed at the south-west corner of the allotment. The land had undergone major subdivision by the 1870s and 1880s.

Major industry development

In the early 20th century the expansion of manufacturing across the nation was reflected in new industrial centres that were developed close to rail and road transport, as distinct from the Victorian-era industrial development that had occupied coastal sites or river and creek banks. Near to the transport hub of Richmond Railway Station and busy Punt Road, grew a distinctive group of factories and warehouses, mainly associated with the growth of the clothing manufacturing industry in the City and dating substantially from the early decades of the 20th century. This area had previously been residential.102

The Australian Knitting Mills Ltd. (41-43 Stewart St, knitted goods manufacturers) complex was at the centre of the Stewart-Tanner Streets industrial area development area. It was developed from 1912 when the supply for World War One uniforms was an impetus for its expansion and had another major building addition in 1922-5.103 The company created the `Golden Fleece' and `Kookaburra' brands.

PRECINCTS

Inter-war residential estates
Set away from this concentrated industrial strip, but related historically to it, was the Pelaco factory (1922-1928), located on a hill on the former site of a Victorian-era mansion.
Houses in Fordham Court and the associated part of Goodwood Street were built speculatively in 1939 for Mrs B.E. Fordham of Yarradale Rd., Toorak, on land sold by Pelaco.104 These houses provide a distinctive enclave within the surrounding Victorian and Edwardian-era development.

A. RICHMOND HILL HERITAGE OVERLAY AREA (WEST SUB-AREA)

Main development era
The main development period evident in the Richmond Hill west sub-area of the Heritage Overlay Area is that of the Victorian era with a substantial contribution from the Edwardian-period. There is also a contribution from some well preserved inter-war buildings and individually significant places of all eras.

Contributory elements
The Heritage Overlay Area contributory elements include (but not exclusively) mainly small, detached and some attached Victorian-era and Edwardian-era houses having typically:

- Pitched gabled or hipped roofs, with some façade parapets,
- One storey wall heights but with some two storey house rows,
- Face brick (red, dichrome and polychrome), weatherboard, or stucco walls;
- Corrugated iron roof and slate cladding, some unglazed Marseilles pattern terra-cotta tiles;
- Chimneys of either stucco finish (with moulded caps) or of matching face brickwork with corbelled capping courses;
- Post-supported verandah elements facing the street, set out on two levels as required with cast-iron detailing for Victorian-era houses and typically timber for Edwardian-era;
- Less than 40% of the street wall face comprised with openings such as windows and doors; and
- Small front gardens, originally bordered by typically timber picket front fences of around 1m height.

Contributory elements also include:

- Corner shops and residences with display windows and zero boundary setbacks.
- Well preserved buildings including typically one storey buildings from the pre Second War era.
- Public infrastructure, expressive of the Victorian and Edwardian-eras such as bluestone pitched road paving, crossings, stone kerbs, channels, and asphalt paved footpaths.

B. RICHMOND HILL HERITAGE OVERLAY AREA (NORTH SUB-AREA)

Main development era
The main development period of the Richmond Hill north sub-area of the Heritage Overlay Area is that of the Victorian-era with a substantial contribution from the Edwardian-period. There is also a contribution from some well preserved inter-war buildings and individually significant places of all eras.

Contributory elements
The Heritage Overlay Area contributory elements include (but not exclusively) mainly detached and some attached Victorian-era and Edwardian-era mainly one and two -storey houses having typically:

- Pitched gabled or hipped roofs, with some façade parapets,
- One and two storey wall heights but with some two storey house rows,
- Face brick (red, dichrome and polychrome), or stucco walls;
- Corrugated iron roof and slate cladding, some Marseilles pattern terra-cotta tiles;
- Chimneys of either stucco finish (with moulded caps) or of matching face brickwork with corbelled capping courses;

- Post-supported verandah elements facing the street, set out on two levels as required with cast-iron detailing for Victorian-era houses;
- Less than 40% of the street wall face comprised with openings such as windows and doors; and
- Front gardens, originally bordered by typically timber or iron picket front fences of around 1m height.

Contributory elements also include:

- Corner shops and residences with display windows and zero boundary setbacks.
- Well preserved buildings including typically one storey buildings from the pre Second War era.
- Public infrastructure, expressive of the Victorian and Edwardian-eras such as bluestone pitched road paving, crossings, stone kerbs, channels, and asphalt paved footpaths.

C. RICHMOND HILL HERITAGE OVERLAY AREA (PELACO, FORDHAM COURT/PART GOODWOOD STREET SUB-AREA)

Main development era
The main development period of the Richmond Hill Fordham Court/Goodwood Street sub-area of the Heritage Overlay Area is that of the Inter-war era.

Contributory elements

Contributory buildings in theFordham Court/Goodwood Street sub-area are typically:

- Residential;
- Single storey;
- Constructed in brick and rendered masonry;
- Of a hipped and gabled tiled roof form, some with a single hipped or gabled roof form over the two dwellings to appear as single larger houses;
- Semi detached duplexes;
- Set in relatively deep garden setbacks; and
- Fenced originally with front fences of low (500-700mm panel height) brick and rendered masonry pier and panel walls.

And

- Public infrastructure, part expressive of the Victorian and Edwardian-eras such as asphalt paved footpaths but most of the kerb and channel is concrete, as typical of the inter-war period.
- Former Pelaco factory (c1922) as an icon in the area, but unrelated in scale and form to the inter-war housing nearby.

D. RICHMOND HILL HERITAGE OVERLAY AREA (SOUTH SUB-AREA)

Main development era
The main development period evident in south sub-area of the Richmond Hill Heritage Overlay Area is that of the Victorian-era. There is also a contribution from the Edwardian-era and some well preserved inter-war buildings and individually significant places of all eras.

Contributory elements
The Heritage Overlay Area contributory elements include (but not exclusively) mainly detached and some attached Victorian-era (some early Victorian-era), one-storey houses having typically:

- Steeply pitched gabled or hipped roofs;
- One storey wall heights;
- Weatherboard (square or bead edge), face brick (dichrome), stone, or stucco walls;
- Corrugated iron roof and slate cladding;

- Chimneys of either stucco finish (with moulded caps) or of matching face brickwork with corbelled capping courses;
- Simple post-supported timber verandah elements facing the street;
- Less than 40% of the street wall face comprised with openings such as windows and doors; and
- Front gardens, sometimes minimal, originally bordered by typically timber picket front fences of around 1m height.

Contributory elements also include:

- Corner shops and residences with display windows and zero boundary setbacks;
- Well preserved buildings, including typically one storey buildings from the pre Second War era.
- A regular subdivision plan with alternating wide principal streets and narrow service streets; and
- Public infrastructure, expressive of the Victorian and Edwardian-eras such as bluestone pitched road paving, crossings, stone kerbs, channels, and asphalt paved footpaths.

E. RICHMOND HILL HERITAGE OVERLAY AREA (SOUTH INDUSTRIAL SUB-AREA)

Main development era
Main development period in the Richmond Hill south industrial sub-area of the Heritage Overlay Area is of the Edwardian-era and inter-war period.

Contributory elements
Contributory elements include (but not exclusively) industrial buildings from the early decades of the 20th century with typically:

- Wall heights ranging from 1-5 levels;
- Façade parapets, sometimes with proprietary logos, with pitched roofs behind;
- Limited or no setback from street frontages; and
- Walls of face red brick (some over-painted) with rendered bands.

Contributory elements also include:

- Public infrastructure, part expressive of the Victorian and Edwardian-eras such as some bluestone pitching and asphalt paved footpaths.

How is it significant?
HO332 Richmond Hill Heritage Overlay Area as a whole is aesthetically and historically significant to the City of Yarra (National Estate Register [NER] Criteria E1, A4)

Why is it significant
A. The Richmond Hill Heritage Overlay Area (west sub-area)

The Richmond Hill Heritage Overlay Area (west sub-area) is significant:

- As an illustration of a typical early private subdivision and development in the City, shown by the irregular street patterns, widely varying allotment sizes, and unusual house siting (onto lanes);
- As one of the earliest private development areas in the City for worker housing, dating from the 1840s, as expressed today by the many early small cottages linked historically to developers who resided in the more salubrious elevated parts of this area;

B. The Richmond Hill Heritage Overlay Area (north sub-area)

The Richmond Hill Heritage Overlay Area (north) is significant:

PRECINCTS

- For the well preserved, well-situated and sometimes large Victorian and Edwardian period houses and for the contribution of well preserved buildings from the inter-war period and individually significant buildings from all periods;
- As one of the early private housing development areas for the wealthy, evolving from the 1840s, and associated with some of Melbourne's most prominent early settlers as seen in their gracious residences, elevated siting and proximity to religious centres of most denominations;
- As particularly indicative (like the west sub-area) of Victorian-era private subdivision with irregular street patterns and lot sizes, and one private road; and
- For the views, from both inside and outside the area, to The Vaucluse Convent, the church spires and towers of St Ignatius and the utilitarian four storey red brick Pelaco factory, with its early neon sign above.

C. The Richmond Hill Heritage Overlay Area (former Pelaco factory, Fordham Court/Goodwood Street sub-area)

The Richmond Hill Heritage Overlay Area (Pelaco, Fordham Court/Goodwood Street sub-area) is significant:

- For the well preserved and visually cohesive groups of inter-war and early post WW2 residential development
- For the historical and visual links of this inter-war development with the adjacent inter-war landmark Pelaco Factory;
- For the contribution of the iconic Pelaco factory complex and illuminated sign, that is well-preserved externally, large in scale and set prominently on the hill as, historically, a symbol of Richmond's special role in the development of key manufacturing centres in the first half of the 20th century.

D. The Richmond Hill Heritage Overlay Area (south residential sub-area)

The Richmond Hill Heritage Overlay Area (south) is significant:

- As one of the earliest private development areas in the City for worker housing, dating initially from the 1840s but extending in the gold-era of the 1850s, as early small cottages set on small allotments;
- For the historical links of the street layout and subsequent housing development with the Docker family and the estate's designer, the noted architect and surveyor, Charles Laing;
- As an illustration of a rare type of model private subdivision and development in the Melbourne area in the form of a planned `village', as shown by the regular street patterns, similar allotment sizes, and the originally residential land use.

E. The Richmond Hill Heritage Overlay Area (south industrial sub-area)

The Richmond Hill Heritage Overlay Area (south industrial sub-area) is significant:

- As a distinctive and visually related group of externally well-preserved factories and warehouses, associated with the growth of the clothing manufacturing industry in the City, dating from the early decades of the 20th century and symbolic of Richmond's special role in the development of key manufacturing centres in the first half of the 20th century.
- For the precinct's strategic location, next to major transport links (railway, Punt Road) and the resultant distinctive angled siting of key buildings to face the railway.

PRECINCTS

7. Swan Street Precinct, Richmond (HO335)

Source: GJM Heritage (2017), *Swan Street Built Form Study: Heritage Assessments & Analysis*

What is significant?
Swan Street, Richmond from Botherambo Street to Brighton Street (nos. 63-221 & 90-272), Church Street, south of Swan Street to the railway line (nos. 421-425 & 454-468), Byron Street (nos. 2-8 & 140-160) and Kipling Street (nos. 1, 2, 2A, 3, 5 & 7).

History

Swan Street was designated as a road reserve in Hoddle's Crown survey of 1837. Allotments on the north side of Swan Street were sold from 1839 and the south side from 1846. By 1857 a number of commercial businesses had been established in Swan Street, particularly at the west end, and traders included butchers, drapers, fruiterers, tailors, shoemakers, hairdressers and hoteliers. Amongst the early hotels was the Swan Inn after which the street was named, and the predecessor of the present three-storey Swan Hotel (1889) at the corner of Church Street.

Swan Street ran from Punt Road to the Survey Paddock (now Burnley Park) before extension to the Yarra River sometime between 1874 and 1888. In 1859 a railway line from Princes Bridge to Punt Road provided access to Swan Street and this was extended to Church Street (East Richmond Station) and beyond to the present Burnley Park the following year. Running south of, and parallel to, Swan Street, this railway line provided transport to and from the city. In 1916 an electric tram service provided additional transport along Swan Street, encouraging further development along the length of Swan Street.

The commercial western end of Swan Street experienced a development boom in the 1880s and much of the present commercial building stock, particularly between the railway line (which was raised above Swan Street in 1887) and the Church Street intersection, was constructed at this time. This included a number of predominantly two-storey shops with residences above, including the extensive Perrins Building, 128-38 Swan Street (1884), shops opposite at 95-97 Swan Street (1885) and a highly decorative group of buildings at 224-234 Swan Street.

Swan Street was sealed in 1901 and construction of commercial buildings continued on allotments at the western end of Swan Street. Substantial and distinctive commercial and public buildings, such as the Maples Piano Showroom and the Richmond South Post Office in 1905, and Dimmeys and the former State Savings Bank of Victoria in 1907 added an Edwardian presence to the existing Victorian character of the precinct. A highly prominent clock tower with ball, added to the Dimmeys store in the 1910s, became a landmark feature in Swan Street.

Swan Street's long history as a commercial centre has also seen it play a central role in the social life of Richmond. For over 150 years, the street has provided the community with social and recreational opportunities, with the long-term commercial success of the street being predicated upon its attractiveness as a social space. It is this rich social legacy that binds the various phases of the street's development together, and which continues today to attract new residents and visitors.

Description

The main development period of the Swan Street Precinct is from the 1880s to 1920 and the streetscape dates largely from this period. A small number of buildings from both earlier and later periods make a contribution to the precinct. A number of individually significant buildings are

PRECINCTS

contained within the precinct, dating predominantly from the 1880s-1920 period.

Buildings are predominantly attached Victorian and Edwardian shops and residences. They are predominantly two-storey with some three/four and single-storey shops interspersed.

Elements that contribute to the significance of the place include (but are not limited to):

- A variety of simple and highly decorative façade parapets, with pitched roofs behind
- No front or side setbacks
- Face red brick (including polychrome) or rendered walls
- Rendered window frames, sills and hoods to upper stories
- Rendered ornament and incised decoration to upper stories
- Brickwork with corbelled capping courses
- Original post-supported street verandahs
- Strong horizontal lines formed by parapets, cornices, string courses
- Repetitive upper floor fenestration patterns
- Consistent two-storey scale with some one and three-storey buildings
- Shop fronts with display windows, timber or tiled plinths, and entry recesses
- Some red brick storage or stable buildings at the rear or side lane frontage
- Corner buildings with secondary façades to side streets or lanes
- Splayed corners to buildings at intersections
- Corrugated iron and slate roof cladding
- Bluestone pitched road paving, crossings, stone kerbs and channels and asphalt paved footpaths.

How is it significant?

Swan Street, Richmond is of local historical, architectural and aesthetic significance to the City of Yarra.

Why is it significant?

The Swan Street Precinct is a major 'High Street' in the City of Yarra and has functioned continuously as one of Richmond's two key commercial centres since the 1840s. It retains a substantial collection of intact commercial buildings, predominantly from the Victorian and Edwardian periods, including shops and associated residences, hotels and other commercial and civic buildings. Together these buildings demonstrate the development of this major 'High Street', particularly from the 1880s to c1920 when substantial growth in the street occurred, and are illustrative of the enduring role the street has played in the economic and social life of Richmond since the establishment of the suburb [Criterion A].

The highly intact streetscape of the Swan Street Precinct clearly demonstrates the principal characteristics of a major turn of the century 'High Street' in the City of Yarra. Typical characteristics, including predominantly two-storey Victorian and Edwardian wall heights, parapeted rendered or red brick facades with repetitive upper floor fenestration, and ground floor shopfronts, are displayed in the original forms, fabric and detailing of many of the buildings. The streetscapes on the south side of Swan Street between the former Richmond Post Office and Church street, on the north side of Swan Street between Carroll and Dando Streets, and the west side of Church Street between Swan Street and the railway line are particularly intact and consistent in their architectural form and expression [Criterion D].

The Swan Street Precinct contains a number of landmark and individually significant buildings which are well-considered and carefully detailed examples of commercial and civic buildings. These include, among others, the former State Savings Bank of Victoria (216 Swan Street); the former Richmond South Post Office (90-92 Swan Street); large retail stores such as Maples Pianos (122-126 Swan Street); Dimmeys (140-48 Swan Street); and M Ball & Co (236-240 Swan Street), as well as hotels such as the

PRECINCTS

Swan Hotel (425 Church Street). The clock tower and ball of the Dimmeys building is of particular prominence and is a local landmark. Variations in façade detailing throughout the precinct, including parapet ornamentation, balustrading and pediments, incised and applied decoration and polychromatic brickwork, make an aesthetic contribution to the overall character of the precinct [Criterion E].

8. Wellington Street Precinct (HO364)

Source: Graeme Butler & Associates (2012), *City of Yarra Heritage Gaps Review (Review 1)*

What is significant?

Historical background

In 1839, two years after the first land sales in the township reserve of Melbourne, Crown allotments were auctioned in Richmond, Fitzroy and Collingwood. These allotments were mainly intended for development as farmlets. However many of the purchases in Richmond were speculative for, very soon, allotments were subdivided and advertised for sale in the ``Port Phillip Patriot''. The first was William Wilton's Crown allotment 46 which was to be sold in one or more acre lots. In 1840, at a subdivision sale of Dr. Farquhar McCrae's allotment 24, the auctioneer described Richmond as "...the abode of aristocracy, wealthy and retired opulence..." and 36 half-acre blocks were sold.

This was the boom period leading up to the recession of the early 1840s. As a sign of the times, subdivisions on the Richmond flats were advertised in 1842 as "...well deserving public attention among the working class", in contrast to earlier advertising of the higher parts of Richmond as for gentlemen only.

By the mid 1840s the depression had ended and resumption of the Immigration Act resulted in a new influx of workers. The sale of Crown allotments recommenced in Richmond in 1845 and by 1851 a further fifteen Crown Portions were sold. Reserves were also created for police purposes (Crown allotments 13-15), and for churches, recreation, produce market, schools and a mechanics' institute (Crown allotment 35). Thirty-one quarry sites were set aside on Crown allotments 9 to 15 where they abutted the river. The only other clay pits shown are at the locality of Yarraberg which David Mitchell operated in Crown Portion 42, off Burnley St.

Richmond's population in 1846 was 4029. At this time, Fitzroy and Collingwood were also being rapidly subdivided, St. Kilda and Port Melbourne were fashionable picnic spots and Williamstown a busy port. The village at Brighton was the leading pleasure resort, and Heidelberg a prosperous farming community. East Melbourne was little built upon until after 1848 when Bishop Perry chose a site there for the Anglican Bishop's Palace. This gave an impetus to building and the Richmond area went ahead as a select and convenient one in which to live. In 1852 North Melbourne, St. Kilda, South Melbourne, Port Melbourne, Essendon, Remington, Carlton and Hawthorn were laid out. Melbourne's population had trebled by 1853 with people returning from the goldfields, while in Richmond major residential subdivisions had occurred in the north and west. Within the next four years, men who established their suburban villas on the Richmond hills included senior Government officials, Alexander McCrae and William Hull; newspaper proprietors Thomas Strode, George Cavanaugh and George Arden; merchants Patrick Welsh, David Stodart Campbell and Alfred Woolley; and the bankers William Highett and John Gardiner. Their "...comfortable, if not architecturally stylish villas began to dot the place".

Richmond was created a separate municipality in 1855. The survey maps of Magee and Kearney show that at this time many of the existing major streets had been laid out but that almost all buildings, with the exception of those in the Yarraberg area to the northeast, were concentrated in the western half of Richmond, near to Melbourne town and the railway route: large suburban villas and gardens on the hill, and cottages on small blocks in the north and south, often in areas of relatively intense development

PRECINCTS

isolated to individual streets. The factors influencing the location of the earliest development appear to have been a preference for high ground and a position on government roads, especially at cross roads.

Richmond's population in 1857 was 9,029 with 2,161 houses and five architects. The electors' roll for 1856-7 indicates an established retail and service trade in Swan Street and Bridge Road - butchers, drapers, shoemakers, hotels, fruiterers, tailors, hairdressers, grocers and blacksmiths.

With separation from Melbourne in 1855, Richmond, along with Collingwood, became exempt from the `Melbourne Building Act' of 1849 which controlled building and subdivision standards. Developers were free to plan streets, reduce frontages and build what they liked. Closer development of Richmond was also encouraged by the railway which was extended to Brighton from Melbourne by 1859, and by horse drawn omnibuses which connected Richmond with Melbourne along Bridge Road.

Melbourne's population in 1861 was 37,000 (including Carlton and East Melbourne); Richmond, Collingwood and Fitzroy each had about 12,000, Prahran 10,000, South Melbourne 9,000, North Melbourne 7,000 and St. Kilda 6,000. Development was apparent along Punt Road c.1860, with little development in south-east Richmond was in 1869. Unemployment was a major issue during the 1860s and in 1862 the Richmond Council sought the repeal of the `Yarra Pollution Prevention Act 'of 1855 (which forbade fellmongeries, starch and glue factories, and boiling down works discharging waste into the Yarra River upstream from Melbourne) so that the river frontages could be opened to manufacturing. By 1865 a quarry, stone crushing mill, fellmongery and abattoir had been established on the river flats in Burnley, and by the 1870's a panoramic view of Richmond carried the caption 'Industry in Arcady'.

As with Melbourne and its other suburbs, the most active period of development in Richmond was in the 1870s and 1880s. The eastern half of the town was partly subdivided by 1874 and by 1888 most subdivision patterns were complete, the major exception being Cole's paddock on Victoria Street. Richmond was proclaimed a town in 1872 and a city in 1882. Its population in 1880 was 23,395 and in 1890 it was 38,797. The residential development trend was a marked increase from the 1850s, steeply rising until c1881 and then a plateau into the 1890s Great Depression. The rate books list 52 industrial establishments in 1880.

Houses constructed between Federation and World War One make up a substantial proportion of Richmond's building stock particularly in the eastern half of the city. Cole's paddock was subdivided by this time.

Encouraged by high tariff protection, new factories and stores were also being established, most notably Bryant & May, Wertheim's piano factory, Dimmey's Model Store, Ruwolt, Rosella, Moore Paragon and Mayall's tannery. By 1919 there were nine tanneries.

This industrial expansion continued after World War One when small gaps in the urban development were filled by inter-war housing estate and Wren's race course was changed to public housing. The Second War was the end of the first wave of urban development in Richmond and hence forms a perceptible period in the historic environment that is the basis for proposed heritage precincts in Richmond, Cremorne and Burnley.

Specific history
This area is part of the 17 acre Crown Portion (CP) 1 sold to Messrs William Burnley, David Lyons and Matthew Cantler in 1849: land sales started immediately in the south-west corner with subdivided lots going to Burnley, Thomas King and Mitchell, Black & Follett. Inspired by the major population increase caused by gold finds in the Colony, William Burnley began to sell more house lots further north in 1852

PRECINCTS

from an estate plan that included the formation of today's Rout and Blanche Streets running east-west, joined by Wellington Street running north–south. Lots of 32-35 feet width faced onto Wellington Street and backed onto CP2 on the east or the 15 feet wide Huckerby Street, on the west, that Burnley had created to serve the lots facing Punt Road.

The 1853 plan shows Jessie and Cremorne Streets in place within Crown Portions 1&2 and the 1855 plan has buildings distributed across the north part of the Crown Portions, along Wellington (as far as Blanche) and Cremorne Streets, while to the south they front only Cremorne Street. The 1874 plan shows the north part of Wellington, Huckerby, Blanche and Jessie Streets in this precinct.
King, Mitchell, Black and Follett's block, created from the south part of Crown Portion 1 in 1849, was to eventually contain the residential subdivision of Melrose and Kelso Streets in 1884 (Lodged Plan 605) with lots of 33-45 feet frontages. This estate is shown on the Tuxen 1888 plan, along with added streets such as Rout, but still there was no continuity for Wellington Street which stopped at Blanche St. The MMBW Plan 911 of 1896 showed the extension of Wellington Street southwards, past Blanche Street, but named as Melrose Street.

Description
The Wellington Street, Cremorne Heritage Precinct is a largely Victorian-era residential area centred on Wellington Street and extends north from the riverside industrial precinct south of Gough Street to the commercial strip of Swan Street West on the north. The arbitrary crank in the line of Wellington Street shows the two development phases (early and late Victorian-era) and exemplifies the piecemeal nature of private development, generally, in the Richmond area.
The haphazard street alignments generate unexpected house groups and vistas. There is the long weatherboard cottage row in Gough Place that now faces out across a large development site towards Punt Road: its unbroken roofline is another testimony of how Richmond, as well as being planned on a free-market basis, was also outside of the building laws that initially applied to the other inner suburbs of Melbourne (Melbourne Building Act). The construction of small, weatherboard and brick cottages in the narrow confines of the early Huckerby and Jessie Streets is another illustration of this evasion of standard building codes. The Richmond Conservation Study (1985) notes of Cremorne Cottage, at 50 Jessie Street: `...Similar size building shown in similar location on Lands Dept 1855 Map of Richmond...' Other early houses such as 375-377 Punt Road can also be traced back in plan form to the 1855 survey map, adjoining the Rout Street entry to the precinct. A small Wellington Street house row (66-68) had rare and early brick-nogged wall construction as an indication of early construction techniques in this precinct. This method of wall construction involves brickwork placed between timber frame members and overclad with weatherboard providing for an uncommon and environmentally sound building method. The Australian Architecture Index cites two other brick nogged houses nearby in Cremorne St auctioned in 1879.

The precinct has a number of individually significant Victorian-era buildings and building rows within its boundaries.

Key buildings
Key buildings include late Victorian-era houses like the row-house pair at 397- 395 Punt Road, described in the 1990s as:

`... A double-storey, rendered, Boom terrace pair, set back with a parapet. The centrepiece is (set) *between abstracted Doric pilasters, supported by scrolls. Cornice and frieze-mould has vermiculated corbels; also to verandah, these on scrollbrackets. The skillion verandah, between wing-walls, has cast-iron lace valence and Composite posts, with first-storey balustrade in an unusual pattern of panels between balusters. Ground-window is tripartite with Tuscan fluted mullions. Doors have fan and sidelights. Chimneys have corbelled brick-bands' as an illustration of the range of ornament that was*

PRECINCTS

used in the late 19th century… '

More typical, late Victorian-era masonry row houses line Wellington Street. Balino Cottage at 44 Wellington St, is an exception:

`A characteristic double-fronted, symmetrical, rendered, Boom cottage, on the street line, with rich decoration. There is a balustraded parapet between piers, surmounted by balloons. The centrepiece has a scallop-shell in a round arch, with acroterion. Piers are supported by small scroll-brackets. A frieze and cornice-mould is supported by brackets, between festoons. The parapet and verandah wing-wall corbels are vermiculated, the latter on scroll-brackets. The brickwork beneath the verandah was exposed, decorated with diamond ceramic tiles. The tripartite window has barleysugar Tuscan mullions and bluestone cill and fanlight over door. The verandah is convex, with cast-iron posts, lace-valence and brackets. There are encaustic geometric tiles. The chimney has deep rendered Classical mould'...

Edwardian-era development is seen in houses facing Kelso Street (5, 9) as well as the former grocer's shop at 12 Kelso Street (Peter Byrne's shop in 1904 and Marcus Steel's in 1920), providing the sense of a self contained domain where provisions were available to householders within walking distance. Intermixed with these are the numerous Victorian-era houses, mainly weatherboard clad, with corrugated iron clad hipped roofs (but with some parapeted forms such as 17 Kelso Street) and little in the way of front gardens. Cremorne Court Flats in Punt Road and the Old English style house Teragram in Wellington Street, represent the well-preserved inter-war buildings that make up a minority of sites in the precinct.

Main development period
The main development period evident in the heritage overlay is that of the Victorian and Edwardian-eras, with a contribution from well preserved interwar buildings and individually significant places of all eras.

Contributory elements
Contributory elements include mainly (but not exclusively) Victorian-era and Edwardian-era houses, with some well preserved residential examples from the immediate post First-War era, having typically:

- pitched gabled or hipped roofs;
- one storey wall heights;
- weatherboard, face brick, or stucco wall cladding; corrugated iron, with some slate roofing;
- chimneys of either stucco finish (with moulded caps) or of matching face brickwork with capping courses;
- post-supported verandah elements facing the street;
- less than 40% of the street wall face comprised with openings such as
- windows and doors; and
- front gardens, bordered by low front fences, typically of timber picket for the Victorian and Edwardian-eras.

Contributory elements also include public infrastructure, expressive of the Victorian and Edwardian-eras such as stone pitched road paving, kerbs and channels, and asphalt paved footpaths.

How is it significant?
The Wellington Street, Cremorne, Heritage Precinct is aesthetically and historically significant (National Estate Register Criteria E1, A4) to the locality of Cremorne and the City of Yarra.

PRECINCTS

Why is it significant?

Wellington Street, Cremorne, Heritage Precinct is significant:

- As a well defined area of Victorian and Edwardian-era houses that matches the major growth periods in Richmond's and the City's housing history, complementing the existing adjoining Cremorne Heritage Overlay Area and individual heritage overlays within its boundaries;
- For some distinctive house groups such as in Gough Place, well preserved inter-war examples such as Cremorne Court Flats, and significant individual house examples; and
- For its role as one of the first development plans launched in Richmond, with some houses reflecting the 1850s estates.

9. Burnley Street Precinct, Richmond (HO474)

Source: Context Pty Ltd (2014), *Heritage Gap Study, Review of Central Richmond.*

What is significant?

The Burnley Street Precinct, comprising 345-389 & 370-404 Burnley Street and 395-419 Swan Street, Richmond is significant. The following buildings and features contribute to the significance of the precinct:

- The buildings constructed from c.1880 to c.1940, as shown on the precinct map.
- The overall consistency of building form (buildings with roofs concealed by parapets, with residential quarters above if two storey and behind if single storey), materials and detailing (front walls of stucco with decorative parapets, some original timber or metal-framed shopfronts with timber or tiled stallboards and recessed entries), and siting (no front and side setbacks).
- The landmark qualities of the former Bank of Australasia.
- The nineteenth century subdivision pattern comprising regular allotments served by rear bluestone laneways.

The following places are Individually Significant and have their own statement of significance:

- Former Bank of Australasia, 377 Burnley Street,
- Former shop and residence, 380 Burnley Street,
- Shops and residences, 400-402 Burnley Street, and
- Shops and residences, 413-15 Burnley Street.

Non-original alterations and additions to the Contributory buildings shown on the precinct map, and the buildings at 381 & 382 Burnley Street, and 411 Swan Street are Not Contributory.

How is it significant?

The Burnley Street Precinct is of local historic and architectural significance to the City of Yarra.

Why is it significant?

Historically, the precinct is a representative example of a shopping centre serving local needs that developed in response to the significant population growth of Richmond in the late nineteenth and early twentieth centuries. The terrace row at nos. 345-67 Burnley Street provides evidence of the first significant phase of residential development that created the need for the centre in the late nineteenth century. (Criterion A)

The location, close to a railway station, and around a major intersection and the mix of single and double storey commercial premises, some with original shopfronts, the corner hotel and former bank

PRECINCTS

are all characteristic of these local centres. The mix of residential and commercial premises is also typical with places such as nos. 373 and 375 Burnley Street that combine a Victorian house with an Edwardian shopfront demonstrating the evolution of the precinct from residential to commercial. (Criterion D)

The precinct contains late Victorian, Federation, and Inter-war shops and one hotel with consistent and characteristic parapeted form, siting and detailing, including some original shopfronts, which contrast with the Victorian residential terrace row. The former Bank of Australasia is notable as a landmark within the precinct. (Criterion E)

10. Swan Street West Precinct (HO524)

Source: GJM Heritage (2017), *Swan Street Built Form Study: Heritage Assessments & Analysis*

What is significant?
The Swan Street West Precinct comprising 30-42 Swan Street, Cremorne.
Elements that contribute to the significance of the place include (but are not limited to):

- The original external form, materials and detailing of the four buildings
- The high level of integrity of the buildings to their late nineteenth and early twentieth century design.

Later (post 1940) alterations and additions to the rear and shopfront are not significant.
How is it significant?
The Swan Street West Precinct is of local historical and architectural significance to the City of Yarra.

Why is it significant?
The Swan Street West Precinct is illustrative of historical development along a major, early commercial thoroughfare in the City of Yarra. As the only remaining group of intact commercial buildings from the Victorian and Edwardian periods west of the Swan Street railway bridge, this precinct demonstrates the commercial development at the west entrance of the major Swan Street 'High Street' up to the 1920s [Criterion A].

The small but intact Swan Street West Precinct clearly demonstrates the principal characteristics of a major Victorian and Edwardian 'High Street' in the City of Yarra. Typical characteristics, including parapeted facades with repetitive upper floor fenestration, rendered facades and ground floor shopfronts, are displayed in the variety of original forms, fabric and detailing of the four buildings [Criterion D].

INDIVIDUALLY SIGNIFICANT PLACES

1. **Former United Kingdom Hotel (now McDonald's)**
   199 Queens Parade, Clifton Hill (HO92, VHR H0684)

   Refer to Victorian Heritage Database for Statement of Significance

2. **Street Trees**
   Queens Parade, Between Alexandra Parade & Delbridge Street Clifton Hill/ North Fitzroy (HO93)

   Source: Allom Lovell & Associates and John Patrick Landscape Architects Pty Ltd (1998), *City of Yarra Heritage Review*

   Statement of Significance
   The double avenue of trees on Queens Parade is locally significant as an extensive and intact avenue planting in the City of Yarra. The trees provide a significant contribution to the heritage character of the precinct, and to the amenity of the local neighbourhood generally.

3. **Haselmere**
   96 Mckean Street, Fitzroy North (HO216, VHR H0163)

   Refer to Victorian Heritage Database for Statement of Significance

4. **Former ANZ Bank**
   370-374 Queens Parade, North Fitzroy (HO221, VHR H0892)

   Refer to Victorian Heritage Database for Statement of Significance

5. **House**
   234 Coppin Street, Richmond (HO245)

   Source: Allom Lovell & Associates (1998), *City of Yarra Heritage Review*

   234 Coppin Street, Richmond, is of local architectural significance. The house is representative of the late 19th century double-storey form, and is a good example of the Italianate style. The house is substantially intact, retaining most of the original cement render and cast iron decoration, and is notable for its unusual broken pedimented parapet.

6. **Former Central Club Hotel**
   291 Swan Street, Richmond (HO285)

   Source: Allom Lovell & Associates (1998), *City of Yarra Heritage Review*

   The former Central Club Hotel is of local architectural significance. The hotel is a good example of the late 19th century Italianate Boom style, notable for its lively asymmetrical facade composition ajid florid Victorian Mannerist cement rendered decoration. The three-storey building is a significant local landmark in Swan Street. Its association with the locally prominent Cremean family and their involvement in Catholic politics is of local historical interest.

INDIVIDUALLY SIGNIFICANT PLACES

7. **Former Burnley Theatre**
365 Swan Street, Richmond (HO286)

Source: Allom Lovell & Associates (1998), *City of Yarra Heritage Review*;
Updated by GJM Heritage (2017), *Swan Street Built Form Heritage Review*

The former Burnley Theatre is of local social and architectural significance. Until its closure in 1958, the theatre was popular with the residents of Richmond as a social gathering place. The Swan Street facade, although not of individual architectural importance, contributes to the streetscape and is a notable local landmark.

Additional contributory elements:

- Capacious building with wide symmetrical façade to Swan Street
- Strong horizontal lines define levels
- Front façade has pattern of windows and engaged columns with wider central bay
- Located on a corner with undecorated side façade to Edinburgh Street
- Large gabled hall behind front façade.

8. **Former State Bank**
214-216 Swan Street, Richmond (HO288, VHR H732)

Refer to Victorian Heritage Database for Statement of Significance

9. **House**
15 Wellington Street, Cremorne (HO294)

Source: Allom Lovell & Associates (1998), *City of Yarra Heritage Review*;
Updated by GJM Heritage (2017), *Swan Street Built Form Heritage Review*

The house at 15 Wellington Street, Richmond, is of local architectural significance. Although the overall form of the house is relatively common, this building is distinguished by its highly unusual Dutch gables, and also by the unusual proportions of the fenestration to the projecting bay. The house is an important heritage element in Wellington Street.

Works in 2016/2017 have resulted in the re-rendering of external walls, removal and replacement of cast iron friezes and balustrading and the reinstatement of a previously bricked in ground level window opening.

10. **Former Richmond South Post Office**
90-92 Swan Street, Richmond (HO357, VHR H48)

Refer to Victorian Heritage Database for Statement of Significance

11. **Dimmeys**
140-160 Swan Street, Richmond (Cremorne) (HO360, VHR H2184)

Refer to Victorian Heritage Database for Statement of Significance

INDIVIDUALLY SIGNIFICANT PLACES

12. **The Greyhound Hotel, later Depot Hotel, now Precinct Hotel**
60-62 Swan Street, Richmond (HO405)

Source: Graeme Butler & Associates (2012), *City of Yarra Heritage Gaps Review (Review 1)*; Updated by GJM Heritage (2017), *Swan Street Built Form Heritage Review*

What is significant?
The Greyhound Hotel, later Depot Hotel and Precinct Hotel has been the site of a public house and gathering place since the 1850s-60s (as 52 Swan St) when licensees included John Davies and William Perrin. The Swan Street West area had consolidated by the 1890s with continuing commercial uses evolving around the new Bowling Club Hotel. The former Bowling Club Hotel, at 36-38 Swan Street, was licensed at another Richmond location to one John Smith in 1868, with a Miss Julia Topey keeping a hotel on this site by the 1880s. The hotel was kept by a M Fitzgerald in the early 1900s but as the new century progressed, the hotel had been delicensed such that by 1920, the only hotel that had survived in this part of Swan Street was the Greyhound Hotel at the Cremorne Street corner (and rebuilt in 1926). Meanwhile shops had occupied the former Bowling Club Hotel. The hotel is a 2 storey Neo-Grec style building, with a simple and boldly executed cemented facade. A deep projecting cornice with brackets underscores the raised parapet entablature with its symmetry to the two elevations expressed through panels, with the focus at the splayed corner. Leadlight glass is evident as is the upper level and ground level joinery (part).

Additional contributory elements include:
- Two-storey wall height
- Zero front setback
- Regular pattern of rectangular fenestration at upper level
- Corner building which address both Swan Street and Cremorne Street by continuing the pattern of openings and parapet detailing
- Splayed corner
- Strong horizontal lines defining levels.

How is it significant?
The building is aesthetically, socially and historically significant (National Estate Register Criteria E1, G1, A4) to Richmond.

Why is it significant?
The building is significant: - for its good representation of a key period in the City's history, being as a site, the initial growth of commercial Swan St and as a building, the rebirth of hotel development in the 1920s after the Licence Reduction Board had eliminated other less salubrious pubs; and - as a well-preserved example of a Greek revival style public house in the local context.

13. **400-402 Burnley Street, Richmond (HO429)**

Source: Lovell Chen (2012), *City of Yarra Heritage Gaps Study (Heritage Gaps Amendment two)*

What is significant?
The property at 400-402 Burnley Street, Richmond, dates from 1912, and comprises a pair of two storied Federation red brick shops with combined residences. No. 402 is the larger of the two, with both building components matching in terms of design. The buildings have transverse gable roof forms, and rear roof pitches concealed by brick parapet walls to form asymmetrical side gables. On the east façade to Burnley Street, both buildings are walled in exposed face brick with two oriel gabled bays to the first floors. The bays and the transverse gable roofs are clad in terracotta tiles; the

INDIVIDUALLY SIGNIFICANT PLACES

expressed central and side walls are topped by stepped moulded brackets with orbs; and the gables to the bays are half-timbered with roughcast stucco, crown four-light bay windows, and have apron panels clad in roughcast stucco. The ground floor shopfronts are also original or early.

How is it significant?
The property at 400-402 Burnley Street, Richmond, is of local historical and aesthetic/architectural significance.

Why is it significant?
The property at 400-402 Burnley Street, Richmond, dates from 1912, and is of local historical significance. It is associated with the later development of eastern Richmond, in an area of Burnley Street, near the junction with Swan Street, which had developed by the early twentieth century into an established retail precinct. The property is also of local aesthetic/architectural significance. It is a well preserved and substantially externally intact example of a Federation two-storey combined residence and shop, in exposed face brick. It is distinguished by the transverse gable roof form, the prominent oriel gabled bays to the first floors, the substantially intact ground floor shopfronts, and the asymmetrical side gable evident on the south elevation which was increasingly common in early twentieth century shop design. Other elements of note include the roof decoration, brackets with orbs, half-timbered gables to the oriel bays, and the oriel apron panels clad in roughcast stucco.

14. **Swan Street Drill Hall**
309 Swan Street, Richmond (HO440)

Source: Lovell Chen (2012), *City of Yarra Heritage Gaps Study (Heritage Gaps Amendment two)*

What is significant?
The property at 309 Swan Street, Richmond, is a rectangular shaped allotment with three buildings dating from 1916 and 1937. The buildings comprise the large former drill hall abutting the Duke Street (west) boundary; the narrower storage building abutting the Lord Street (east) boundary; and a smaller brick building between these to the Swan Street boundary. Both the hall and storage shed date from 1916 and are single storey, gable-roofed and gable-ended structures, clad in corrugated galvanised iron. Both buildings retain most of their original window framing, with some exceptions. The space between the two original buildings is concreted over. The third building dates from 1937, and is a single storey, overpainted brick building, with a transverse gable roof. The 1916 drill hall is the largest building on the site, has the distinctive 'drill hall' form, and was the focus of the original training operations and subsequent Commonwealth use. The 1916 storage shed, although original, and the later 1937 building, are less distinguished elements, and more utilitarian in character and form.

How is it significant?
The property at 309 Swan Street, Richmond, is of local historical and architectural significance.

Why is it significant?
The property at 309 Swan Street, Richmond, was established as a drill hall complex in 1916 and is of local historical significance. It dates from the period when compulsory military service was introduced, and a large number of drill halls were constructed in Australia. Richmond men between the ages of eighteen and thirty, who were called upon to enlist for military service in World War One, did so at the Swan Street drill hall. They were also given some training on the site. Later Commonwealth uses included housing the 2/11th Field Regiment in the 1940s; the Department of Technical Engineering copywriting section, and the Training Depot for the Australian Army's Royal Corps of Australian Electrical and Mechanical Engineers in the 1950s; and cadet battalions in the

INDIVIDUALLY SIGNIFICANT PLACES

1970s. The former drill hall building is also of local architectural significance for demonstrating some of the principal characteristics of World War One drill halls. These include the simple gabled form, albeit carried over a large building, and the large internal space. The galvanised and overpainted corrugated iron cladding is also typical of suburban and regional drill halls around the country. In addition, it is a comparatively externally intact example of a drill hall of this era.

15. **319 Swan Street, Richmond (HO441)**

Source: Lovell Chen (2012), *City of Yarra Heritage Gaps Study (Heritage Gaps Amendment two)*

What is significant?
No. 319 Swan Street, Richmond, was constructed in 1889 as a two storey corner shop and residence. The building is rectangular in plan form with a chamfered corner entry and a hipped roof clad in galvanized corrugated steel, with two stuccoed chimneys with cornices. The first floor is largely externally intact, with original detailing including moulded stringcourses, double-hung sashes with stilted segmental arches, and moulded architraves with accentuated keystones. Other details include half-fluted pilasters (piers) which support a dentilled entablature and above that a continuous bracketed cornice and parapet with waisted balustrading. The chamfered corner is capped with a triangular pediment on two broad piers, enclosing a cartouche panel and topped by an orb finial. At ground level, the shop front has been altered. The rear portion of 319 Swan Street, facing Lord Street, is largely externally intact, albeit more simply detailed than the corner shop component. Beyond this wing is an adjoining contemporary development which is not of heritage significance.

How is it significant?
No. 319 Swan Street, Richmond, is of local historical and aesthetic/architectural significance.

Why is it significant?
No. 319 Swan Street is of local historical significance, as a combined shop and residence constructed in 1889 for James Davison, a baker. At that time commercial development was being consolidating in Swan Street; the 1880s date is also consistent with the core period of commercial building construction in the street. Unusually, the bakery operation in the building was sustained until the mid-1970s. No. 319 Swan Street is also of local aesthetic/architectural significance. While the building is broadly consistent with many nineteenth century two-storey shop and house combinations in inner suburban main road locations, it is distinguished by the complexity and vigour of the first floor elevations in particular, and the reasonably intact side elevation to Lord Street. The segmentally arched first floor windows are usual in terraced shops, enlivened here by the Corinthian pilasters with cornice breakfronts above. The parapet and corner pediment, with baluster waisting, dentil mouldings and bracketing, are also largely intact. The chamfered corner enhances the streetscape presentation.

16. **St Johns Church Complex**
61-87 Queens Parade, Clifton Hill (HO496)

Source: GJM Heritage (2017), *Queens Parade Built Form Heritage Analysis & Recommendations Report*

What is significant?
St John the Baptist Roman Catholic Church Complex, 61-87 Queens Parade, Clifton Hill, built from 1876 to 1918, comprising the church, school, presbytery, former hall/shops, former presbytery and the Queens Parade boundary fence to the church and presbytery.
Elements that contribute to the significance of the place include (but are not limited to):

INDIVIDUALLY SIGNIFICANT PLACES

- The original external form, materials and detailing of the buildings
- The high level of integrity to their original design
- The Queens Parade fence to the church and presbytery.

Later alterations and additions, particularly the recent addition to the rear of the shops and school, are not significant.

How is it significant?
St John the Baptist Roman Catholic Church Complex, 61-87 Queens Parade, Clifton Hill, is of local historical, architectural and aesthetic significance to the City of Yarra.

Why is it significant?
St John the Baptist Roman Catholic Church Complex, 61-87 Queens Parade, Clifton Hill is a large and intact group of religious buildings, including a church, school, presbytery, hall/shops and former presbytery. This group of buildings is illustrative of a large suburban parish, with place of worship and associated educational and community facilities, established from the late nineteenth century through to the twenty-first century (Criterion A).

St John the Baptist Roman Catholic Church Complex, 61-87 Queens Parade, Clifton Hill is a fine, intact and representative example of a religious complex. It contains buildings from the Victorian and Federation periods which display typical characteristics of a range of styles, including the Victorian Decorated Gothic style of the late nineteenth century (church) and the Gothic Tudor (school), Bungalow (presbytery) and Free Style (hall) of the Federation period (Criterion D).

St John the Baptist Roman Catholic Church Complex, 61-87 Queens Parade, Clifton Hill is a well-considered and carefully detailed complex of church buildings. The church is a richly composed bluestone building with freestone dressings, designed in a Decorated Gothic style. The soaring church spire is a prominent landmark in the Clifton Hill/Fitzroy North area. The church contains several important stained glass windows including works by Ferguson and Urie and Hardman of Birmingham. The school building is an imposing two-storey red brick building, designed in a Gothic Tudor style, and the former Church Hall is a carefully composed two-storey red brick triangular building, designed in a Federation Free Style. (Criterion E).

17. **Row of houses, Elizabeth Terrace**
472-484 Napier Street, Fitzroy North (HO498)

Source: GJM Heritage (2017), *Queens Parade Built Form Heritage Analysis & Recommendations Report*

What is significant?
Elizabeth Terrace, 476-484 Napier Street and the terrace house at 472 Napier Street, Fitzroy North, built in 1885 and 1889 respectively.
Elements that contribute to the significance of the place include (but are not limited to):

- The houses' original external form, materials and detailing
- The houses' high level of integrity to its original design
- The original front fences and gates.

Later alterations and additions to the rear of the terraces and the addition to the north of 472 Napier Street, are not significant.

How is it significant?
Elizabeth Terrace, 476-484 Napier Street and the terrace house at 472 Napier Street, Fitzroy North are

INDIVIDUALLY SIGNIFICANT PLACES

of local architectural and aesthetic significance to the City of Yarra.

Why is it significant?
Elizabeth Terrace, 476-484 Napier Street and the terrace house at 472 Napier Street, Fitzroy North are fine and representative examples of terrace housing from the Victorian period. They display typical features of the Victorian Italianate architectural style popular in the 1880s boom period in Fitzroy North and across Melbourne more broadly, including a two-storey form with front verandahs and balconies, polychromatic brickwork, decorative cast iron, rendered decorative elements including balustrading and cast iron palisade fencing (Criterion D).

Elizabeth Terrace, 476-484 Napier Street and the terrace house at 472 Napier Street, Fitzroy North are well-considered and carefully detailed examples of Victorian Italianate terrace housing. The row of polychromatic terraces, with repetitive decorative elements across the facades, presents a picturesque composition of this architectural style (Criterion E).

18. **Clifton Motors (former)**
205-211 Queens Parade, Fitzroy North (HO504)

Source: GJM Heritage (2017), *Queens Parade Built Form Heritage Analysis & Recommendations Report*

What is significant?
The Former Clifton Motors Garage, Showroom and Residence, 205-211 Queens Parade, Fitzroy North. Elements that contribute to the significance of the place include (but are not limited to):

- The building's 1938-39 external form, materials and detailing
- The building's high level of integrity to its 1938-39 design.

How is it significant?
The Former Clifton Motors Garage, Showroom and Residence, Fitzroy North is of local historical and architectural significance to the City of Yarra.

Why is it significant?
The Former Clifton Motors Garage, Showroom and Residence, Fitzroy North is of significance as an early motor garage constructed in a prominent location in Queens Parade, Fitzroy North. Operating as a garage and vehicle-related facility from at least the 1920s until the late twentieth century, the place demonstrates the increasing use of motor vehicles in Melbourne and suburbs, and their need for repair and service, for much of the twentieth century. The Moderne façade in particular illustrates the motoring age of the 1930s (Criterion A).
The Former Clifton Motors Garage, Showroom and Residence, Fitzroy North is a fine and intact example of a distinctive Moderne style motor garage, designed by noted architect James H Wardrop. The building displays typical features of the Moderne architectural style popular in the 1930s, including a dominant central element comprising a series of vertical fins and contrasting decorative horizontal treatment (Criterion E).

19. **Shop and Residence**
273A Swan Street, Richmond (HO522)

Source: GJM Heritage (2017), *Swan Street Built Form Study: Heritage Assessments & Analysis*

What is significant?
The two-storey corner shop and residence at 273A Swan Street, Richmond, built c1885-90.

INDIVIDUALLY SIGNIFICANT PLACES

Elements that contribute to the significance of the place include (but are not limited to):

- The building's original external form, materials and detailing
- The building's high level of integrity to its original design.

Later (post 1900) alterations and additions to the rear, shopfront and eastern side are not significant.

How is it significant?

The two-storey corner shop and residence at 273A Swan Street, Richmond is of local historical and architectural significance to the City of Yarra.

Why is it significant?

The two-storey corner shop and residence at 273A Swan Street, Richmond is illustrative of historical development that occurred along a major, early commercial thoroughfare in the City of Yarra, particularly in the 'boom' period of the 1880s [Criterion A].

The two-storey corner shop and residence at 273A Swan Street, Richmond is a fine, intact and representative example of a Victorian shop and residence. It displays typical features of the Victorian style popular in the 1880s in Richmond and across Melbourne more broadly, including a parapeted façade with repetitive ground and upper floor fenestration, articulated facades to both streets with splayed corner, rendered facades and ground floor shopfronts [Criterion D].

20. **Pair of Shops**
323-325 Swan Street, Richmond (HO523)

Source: GJM Heritage (2017), *Swan Street Built Form Study: Heritage Assessments & Analysis*

What is significant?

The two-storey pair of shops and residences at 323-325 Swan Street, Richmond, built in 1886.
Elements that contribute to the significance of the place include (but are not limited to):

- The building's original external form, materials and detailing
- The building's high level of integrity to its original design.

Later (post 1900) alterations and additions to the rear and shopfront are not significant.

How is it significant?

The pair of shops and residences at 323-325 Swan Street, Richmond is of local historical and architectural significance to the City of Yarra.

Why is it significant?

The pair of shops and residences at 323-325 Swan Street, Richmond is illustrative of historical development that occurred along a major, early commercial thoroughfare in the City of Yarra, particularly in the 'boom' period of the 1880s [Criterion A].
The commercial premises at 323-325 Swan Street, Richmond are a fine, intact and representative example of a pair of Victorian shops and residences. They display typical features of the Victorian architectural style popular in the 1880s in Richmond and across Melbourne more broadly, including a parapeted facade with repetitive upper floor fenestration, rendered facades and ground floor shopfronts [Criterion D].

INDIVIDUALLY SINGIFICANT PLACES WITHIN PRECINCTS

Some 'Individually Significant' places within heritage precincts have a Statements of Significance. Those places that have one are listed below.

1. **12 Hodgkinson Street, Clifton Hill (within HO317)**

   Source: Andrew Ward (May 1995), *Collingwood Conservation Study Review*

   Significance:
   Of local significance as a very early house in Clifton Hill, erected whilst crown land sales were proceeding. The architectural detail and general lack of enrichment are typical of its period and contrast with the majority of Clifton Hill's houses.

2. **William B Fox House, later 'House of the Gentle Bunyip'**
   94 Hodgkinson Street, Clifton Hill (within HO317)

   Source: Andrew Ward (May 1995), *Collingwood Conservation Study Review*

   Significance:
   Important at the local level as a very early house in Clifton Hill, erected whilst the crown land sales were proceeding. The architectural details and lack of enrichment is representative of the mid Victorian years.

3. **Baptist Church & Hall**
   100 Hodgkinson Street, Clifton Hill (within HO317)

   Source: Andrew Ward (May 1995), *Collingwood Conservation Study Review*

   Significance:
   Church: The Baptist Church, Clifton Hill, is of local importance as a substantially intact church designed along non-conformist lines with a distinctive bell tower. Its role as a prototype for the design of the Northcote Baptist Church is of interest.

   Former Baptist Sunday School Hall: This building forms a visual unit with the adjoining Church and is an important supportive element.

4. **Residence**
   122 Hodgkinson Street, Clifton Hill (within HO317)

   Source: Andrew Ward (May 1995), *Collingwood Conservation Study Review*

   Significance:
   No. 122 Hodgkinson Street is important as a substantial arcaded villa of which there are few in the municipality.

5. **Fire Station No. 11 (former)**
   662 Smith Street, Clifton Hill (within HO317)

   Source: GJM Heritage (2017), *Queens Parade Built Form Heritage Analysis & Recommendations Report*

   What is significant?
   The Former Fire Station, 662 Smith Street, Clifton Hill, built in 1884 as the Clifton Hill Temperance Fire

INDIVIDUALLY SINGIFICANT PLACES WITHIN PRECINCTS

Some 'Individually Significant' places within heritage precincts have a Statements of Significance. Those places that have one are listed below.

Brigade.

Elements that contribute to the significance of the place include (but are not limited to):

- The building's original external form, materials and detailing
- The building's high level of integrity to its original design.

Later alterations and additions are not significant.

How is it significant?

The Former Fire Station, 662 Smith Street, Clifton Hill is of local historical and architectural significance to the City of Yarra.

Why is it significant?

The Former Fire Station, Clifton Hill is of historic significance as an early and rare example of a suburban fire station which pre-dated the establishment of the Metropolitan Fire Brigade (created through the *Fire Brigades Act 1890*). The building demonstrates the period of voluntary fire brigades established prior to the formalisation of the metropolitan fire service (Criterion A & B).

The Former Fire Station, Clifton Hill is a fine and representative example of a suburban fire station. The large arch-headed driveway entrance with panelled doors, and adjacent office entry at street level, are features that clearly demonstrate the use of the building as a nineteenth century fire station (Criterion D).

6. **National Bank, former**
460 Brunswick Street, Fitzroy North (within HO327)

Source: Jacobs Lewis Vines Architects (July 1978), *North Fitzroy Conservation Study*

'Building citation':

This bank, built in 1884, (Architects Smith & Johnson) is an important, conservative, classical, corner bank building. The upper storey with Ionic pilasters surmounts a lower storey with Tuscan pilasters. The whole is surmounted by a dentillated cornice with a recessed splayed panel on the corner. The exterior is largely intact - the corner door has been modernised, and a door to the south face has been filled in to make a window. In this corner position, it is an important streetscape element, and is worthy of addition to the Historic Buildings Register.

7. **House**
17 Delbridge Street, Fitzroy North (within HO327)

Source: Jacobs Lewis Vines Architects (July 1978), *North Fitzroy Conservation Study*

'Building citation':

This two storey house was constructed in 1890 for Mark Allen, a photographer. It has an arcaded front with the gable running out over the balcony level and the parapet at the front line. It is possibly the best example of this type in North Fitzroy, although the present paint colours are not appropriate. The most distinctive feature of the house is the frieze band of tiles surmounting the upper arcade. The parapet, supported en a dentillated cornice, consists of an interlocking balustrade, square name plate framed by large scrolls and balls. The composition is pleasing and the arcading at both levels is finely detailed.

INDIVIDUALLY SINGIFICANT PLACES WITHIN PRECINCTS

Some 'Individually Significant' places within heritage precincts have a Statements of Significance. Those places that have one are listed below.

8. **218 McKean Street, Fitzroy North (within HO327)**

Source: Jacobs Lewis Vines Architects (July 1978), *North Fitzroy Conservation Study*

See entry below for Hatherlie, 224 McKean Street, Fitzroy North (HO327)

9. **220 McKean Street, Fitzroy North (within HO327)**

Source: Jacobs Lewis Vines Architects (July 1978), *North Fitzroy Conservation Study*

See entry below for Hatherlie, 224 McKean Street, Fitzroy North (HO327)

10. **Hatherlie**
224 McKean Street, Fitzroy North (within HO327)

Source: Jacobs Lewis Vines Architects (July 1978), *North Fitzroy Conservation Study*
[addressed as 222 McKean Street in 1978 Study]

'Building citation':
This building constructed in 1888 for Samuel Lazarus, a financier, is a two storey polychrome brick terrace house with a small polychrome attachment at the side. The detailing as a whole is not very remarkable. This terrace is best appreciated in conjunction with 218 and 220 McKean Street. These are also largely intact two storey polychrome terraces. The three terraces together combine to create an imposing streetscape element.

11. **K. G. Luke Pty Ltd factory (former)**
26-52 Queens Parade, Fitzroy North (within HO327)

Source: GJM Heritage (2017), *Queens Parade Built Form Heritage Analysis & Recommendations Report*

What is significant?
The K G Luke Pty Ltd Factory and Showrooms, 26-52 Queens Parade, Fitzroy North, built from 1933 - 1965.
Elements that contribute to the significance of the place include the form, materials and detailing of the Art Deco façade and the ability to understand the place as a former factory and showroom complex.
Alterations and additions to the factory and showrooms after 1965 do not contribute to the significance of the place.

How is it significant?
The K G Luke Pty Ltd Factory and Showrooms is of local historical and architectural significance to the City of Yarra.

Why is it significant?
The K G Luke Pty Ltd Factory and Showrooms is of historical significance as a large industrial complex in Fitzroy North, which was established close to the Collingwood Gas Company gasworks and the Inner Circle Railway branch line. It is a remnant example of industrial development that occurred in this area from the 1860s through to the post-war period (Criterion A).

INDIVIDUALLY SINGIFICANT PLACES WITHIN PRECINCTS

Some 'Individually Significant' places within heritage precincts have a Statements of Significance. Those places that have one are listed below.

The K G Luke Pty Ltd Factory and Showrooms is of aesthetic significance for its particularly unusual and distinctive Art Deco façade which utilises a complex arrangement of castellated and hexagonal decorative elements (Criterion E).

The K G Luke Pty Ltd Factory and Showrooms is of historical significance for its association with Victorian manufacturer and sporting administrator, Kenneth G Luke, who moved his fledgling business to this site in 1929 and subsequently developed a successful business enterprise. Knighted in 1962, Luke was a highly successful businessman, with a range of interests and involvements in various committees and clubs (Criterion H).

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12. **Selotta Shoes Pty Ltd factory (former)**
122 Queens Parade, Fitzroy North (within HO327)

Source: Graeme Butler & Associates (2007, 2013), *City of Yarra Review of Heritage Overlay Areas*

What is significant?
The Selotta Shoes Pty Ltd Factory at 122 Queens Parade, Fitzroy North was created in 1934 for Selotta Shoes Pty Ltd. The place has a good integrity to its creation date. It was designed by architect, H V Gillespie. Fabric from the creation date at the Selotta Shoes Pty Ltd Factory, former is locally significant within the City of Yarra, compared to other similar places from a similar era.

How is it significant?
The Selotta Shoes Pty Ltd Factory at 122 Queens Parade, Fitzroy North is historically and architecturally significant to the locality of Fitzroy North and the City of Yarra.

Why is it significant?
The Selotta Shoes Pty Ltd Factory is significant as a distinctive two-storey brick and render Moderne factory design example, with Spanish revival ornament and steel-framed windows.

`An unusual 1930s large interwar Spanish styled industrial façade that retains a high degree of architectural integrity and recalls Harry Norris's St. Kilda Rd Motor Garage and Showroom in a similar boulevard location. The status of this part of Queens Parade as a showpiece for modern industry relied on the Edinburgh Gardens spur railway to the Queens Parade Fitzroy Station which ran as a goods line, from 1891 to c1980' (Wight 2001).

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13. **Christian Brothers Monastery, later units, G Maher House**
256-262 Queens Parade, Fitzroy North (within HO327)

Source: Graeme Butler & Associates (2007, 2013), *City of Yarra Review of Heritage Overlay Areas*

What is significant?
The Christian Brothers Monastery (later G Maher House) at 256-262 Queens Parade, Fitzroy North, was created in 1929 for the St Patrick's Cathedral Melbourne Roman Catholic Trust and has historical associations with the Christian Brothers Monastery. The place has a good integrity to its creation date.

Fabric from the creation date at the Christian Brothers Monastery is locally significant within the City of Yarra, compared to other similar places from a similar era.

INDIVIDUALLY SINGIFICANT PLACES WITHIN PRECINCTS

Some 'Individually Significant' places within heritage precincts have a Statements of Significance. Those places that have one are listed below.

How is it significant?
The Christian Brothers Monastery at 256-262 Queens Parade, Fitzroy North, is historically and architecturally significant to the locality of Fitzroy North and the City of Yarra.

Why is it significant?
The Christian Brothers Monastery, later Units, G Maher House is significant as a large two-storey tile, red brick and render hipped roof monastery with a parapeted entry porch, faceted window bays, cement detailing, an early fence and tall chimneys.

Historically and socially important locally as an uncommon building type and associated with local community uses.

14. **Clifton Hill Hotel**
89 Queens Parade, Clifton Hill (within HO330)

Source: Andrew Ward (May 1995), *Collingwood Conservation Study Review*

Significance:
The Clifton Hill Hotel is a prominent and substantially intact late Victorian hotel.

15. **Melbourne Savings Bank, later State Savings Bank**
97-99 Queens Parade, Clifton Hill (within HO330)

Source: Andrew Ward (May 1995), *Collingwood Conservation Study Review*

Significance:
This former bank is an important public building in Collingwood for its prominence and free expression and rich detailing in the late Victorian Italianate mode.

16. **Doctor's surgery and house (former)**
105 Queens Parade, Clifton Hill (within HO330)

Source: GJM Heritage (2017), *Queens Parade Built Form Heritage Analysis & Recommendations Report*

What is significant?
The Former Doctor's Surgery and House, 105 Queens Parade, Clifton Hill, built c1915.
Elements that contribute to the significance of the place include (but are not limited to):

- The building's original external form, materials and detailing
- The building's high level of integrity to its original design.

Later alterations and additions to the rear are not significant.

How is it significant?
The Former Doctor's Surgery and House, 105 Queens Parade, Clifton Hill is of local architectural and aesthetic significance to the City of Yarra.

Why is it significant?
The Former Doctor's Surgery and House, 105 Queens Parade, Clifton Hill is a fine and representative

INDIVIDUALLY SINGIFICANT PLACES WITHIN PRECINCTS

Some 'Individually Significant' places within heritage precincts have a Statements of Significance. Those places that have one are listed below.

example of a Federation house with combined use as a doctor's surgery. It displays typical features of the Federation Arts and Crafts architectural style popular in the first decades of the twentieth century in Clifton Hill and across Melbourne more broadly, including roughcast walls and chimney, both hip and gable roofs, prominent exposed rafters and projecting window bay with fishscale shingle cladding (Criterion D).

The Former Doctor's Surgery and House, 105 Queens Parade, Clifton Hill is a well-considered and carefully detailed example of a Federation Arts and Crafts house. The design, with main hipped roof, prominent front bay and central chimney, presents a picturesque composition of this architectural style, particularly within the commercial context of Queens Parade (Criterion E).

17. **Albert Hall (former), also Masonic Hall, later Samian Social Club**
127-129 Queens Parade, Clifton Hill (within HO330)

Source: Andrew Ward (May 1995), *Collingwood Conservation Study Review*

Significance:
The former "Albert Hall" was an important social venue in the locality throughout the late nineteenth and early-mid twentieth century, its architectural form in the Queens Parade streetscape being expressive of this role.

18. **Daintons Family Hotel, later Normanby Hotel**
139 Queens Parade, Clifton Hill (within HO330)

Source: Andrew Ward (May 1995), *Collingwood Conservation Study Review*

Significance:
A prominent and comparatively early hotel forming part of the Queens Parade Streetscape. Ornamentation is typical of the period.

19. **Clifton Hill Post Office, former**
141 Queens Parade, Clifton Hill (within HO330)

Source: Andrew Ward (May 1995), *Collingwood Conservation Study Review*

Significance:
The former Clifton Hill post office is of local importance on account of the buildings initial use as well as for its prominence and late Victorian architecture.

20. **National Bank of Australasia, former**
270 Queens Parade, Fitzroy North (within HO330)

Source: Jacobs Lewis Vines Architects (July 1978), *North Fitzroy Conservation Study*

'Building citation':
This bank, constructed in 1886, provides an excellent example of transitional boom classicism bank architecture.
It- is a two storey corner bank with a recessed loggia to the upper floor. The lower floor has a bluestone rough dressed base. The parapet is elaborate with an intact balustrade, balls and sculptural

INDIVIDUALLY SINGIFICANT PLACES WITHIN PRECINCTS

Some 'Individually Significant' places within heritage precincts have a Statements of Significance. Those places that have one are listed below.

piece on the splayed corner. This is a significant corner building, and the ornate design and composition combine to make this building of considerable merit and streetscape importance.

21. **Shops and Residences**
454-456 Church Street, Cremorne (within HO335)

Source: O'Connor, John & Coleman, Roslyn et al. (1985), *Richmond Conservation Study : undertaken for the City of Richmond*

A distinctive group of gabled Edwardian shops by the distinguished architect Nahum Barnet.

22. **Corner Hotel**
57-61 Swan Street, Richmond (within HO335)

Source: GJM Heritage (2017), *Swan Street Built Form Study: Heritage Assessments & Analysis*

What is significant?
The two-storey hotel known as the Corner Hotel, 57-61 Swan Street, Richmond, built in the 1960s.

How is it significant?
The Corner Hotel, 57-61 Swan Street, Richmond is of local historical and social significance to the City of Yarra.

Why is it significant?
The Corner Hotel, Richmond has made a strong contribution to the commercial and social life of Richmond from its establishment in the early 1870s and an important and highly influential contribution to the music industry as an important live music venue since its rebuilding in the mid 1960s and renovation in the 1990s [Criterion A].

The Corner Hotel, Richmond is of particular social significance for its long-term and continued use as a live music venue. It is a well-established and well-known venue which is considered amongst the most pre-eminent in the City of Yarra and the broader community [Criterion G].

23. **H. E. McNaughton ironmongery & residence, former**
69 Swan Street, Richmond (within HO335)

Source: Graeme Butler & Associates (2007, 2013), *City of Yarra Review of Heritage Overlay Areas*

What is significant?
The former H.E. McNaughton ironmongery and residence at 69 Swan Street, Richmond, was created in 1924 for Henry Ernest McNaughton and has a close association with him. The place has a good integrity to its creation date. Fabric from the creation date at the H.E. McNaughton ironmongery and residence is locally significant within the City of Yarra, compared to other similar places from a similar era.

How is it significant?
The H.E. McNaughton ironmongery and residence, former at 69 Swan Street, Richmond is architecturally significant to the locality of Richmond and the City of Yarra.

INDIVIDUALLY SINGIFICANT PLACES WITHIN PRECINCTS

Some 'Individually Significant' places within heritage precincts have a Statements of Significance. Those places that have one are listed below.

Why is it significant?
The H.E. McNaughton ironmongery and residence is significant as a well preserved inter-war parapeted brick and stucco shop and residence with original or early shopfront and the owner's name cemented in bas-relief on the parapet.

24. **National Bank of Australasia**
105 Swan Street, Richmond (within HO335)

Source: O'Connor, John & Coleman, Roslyn et al. (1985), *Richmond Conservation Study : undertaken for the City of Richmond*

This suburban branch of the National Bank of Australasia was erected in 1886-7 by the notable bank architect Albert Purchas. Its imposing three storey height, prominent corner location, and fine architectural detailing are important components of the building's significance.

25. **Shops and residences**
232 & 234 Swan Street, Richmond (within HO335)

Source: O'Connor, John & Coleman, Roslyn et al. (1985), *Richmond Conservation Study : undertaken for the City of Richmond*

A rare example of a pair of small shops designed in a boom style classicism, completely intact above verandah level.

26. **M. Ball & Co, former**
236 - 240 Swan Street, Richmond (within HO335)

Source: O'Connor, John & Coleman, Roslyn et al. (1985), *Richmond Conservation Study : undertaken for the City of Richmond*

A Richmond landmark used as a drapery since 1871, retaining some original shop fittings and a now rare aerial cash conveyor.

27. **Whitehorse Hotel, former**
250-252 Swan Street, Richmond (within HO335)

Source: O'Connor, John & Coleman, Roslyn et al. (1985), *Richmond Conservation Study : undertaken for the City of Richmond*

A rare example of an early 1850's stone commercial building designed in a colonial Regency style.

28. **Shops and Residences**
413 - 415 Swan Street, Richmond (within HO474)

Source: Lovell Chen (2012), *City of Yarra Heritage Gaps Study (Heritage Gaps Amendment two)*

What is significant?
Nos 413-415 Swan Street, Richmond is a Victorian two storey brick terrace pair of shops and combined residences. The ground floor shopfronts have been modified, but the rendered first floor facades are intact, with no. 413 remaining unpainted save for the c.1920s 'Monopole Magnum Cigars'

INDIVIDUALLY SINGIFICANT PLACES WITHIN PRECINCTS

Some 'Individually Significant' places within heritage precincts have a Statements of Significance. Those places that have one are listed below.

sign. Intact details include double-hung sash windows, window architraves, brackets, supporting stringcourse and mouldings, plus pedimented parapets incorporating panels, brackets, cornices and mouldings, urn pedestals and dividers. The buildings have been extended to the rear; these later elements are not of heritage significance.

How is it significant?

Nos 413-415 Swan Street, Richmond is of local historical and aesthetic/architectural significance.

Why is it significant?

Nos 413-415 Swan Street is of local historical significance, as a pair of two storey brick shops and combined residences constructed for carpenter, J Wood in 1892. This was just after the period of commercial development consolidation in Swan Street, and unusually during the severe economic Depression of the early 1890s. The buildings were subsequently occupied by a range of retailers and commercial operations, typical for a main street of the late nineteenth and early twentieth century. These included sellers of dairy produce, a greengrocer, butcher, hairdresser and tobacconist. The subject property is also of local aesthetic/architectural significance. It is a well-preserved pair of two storey shops/residences, broadly consistent with many similar nineteenth century commercial buildings in inner suburban main road locations. However, the first floor facades retain their original form and detailing; in particular, the upper level to no. 415 has remained unpainted. This façade is further distinguished through the retention of the 'Monopole Magnum Cigars' sign. Both first floors also have prominent and well-detailed arched pedimented parapets.

Attachment 2 - Amendment C231 - DDO16

xx/xx/xxxx Propsed C231

SCHEDULE 16 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY

Shown on the planning scheme map as **DDO16.**

QUEENS PARADE

1.0 General design objectives

xx/xx/xxxx Proposed C231

- To recognise and respond to the distinct character, heritage streetscape and varying development opportunities defined by the five precincts along Queens Parade, including:
  - Precinct 1 – Brunswick Street Precinct
  - Precinct 2 – Boulevard Precinct
  - Precinct 3 – St John's Precinct
  - Precinct 4 – Activity Centre Precinct
  - Precinct 5 – North Eastern Precinct
- To support a new mid rise character (3 to 14 storeys) behind a consistent street wall in precincts 2-5.
- To ensure development respects the architectural form and qualities of heritage buildings and streetscapes and maintains the visual prominence of the St John the Baptist church belfry and spire, the former ANZ Bank building, the former United Kingdom Hotel and the former Clifton Motors garage.
- To ensure new development responds to the grand, tree-lined boulevard character of Queens Parade.
  - To ensure that the overall scale and form of new buildings provides a suitable transition to low scale residential areas and protects these properties from unreasonable loss of amenity through visual bulk, overlooking and overshadowing.

2.0 Buildings and works

xx/xx/xxxx Proposed C231

A permit is required to construct a building or construct or carry out works.

2.1 Definitions

1:1 ratio heritage street wall to new built form is where the height of the heritage street wall equals the height of the new development above street wall when viewed from the opposite side of the street from the centre of the footpath at a height of 1.6 metres above ground level

2:1 ratio heritage street wall to new built form is where the height of the heritage street wall is double the height of the new development above street wall when viewed from the opposite side of the street from the centre of the footpath at a height of 1.6 metres above ground level

Street wall is the façade of a building at the street boundary. Street wall height is measured at the vertical distance between the footpath at the centre of the frontage and the highest point of the building, parapet, balustrade or eavesat the street edge, with the exception of architectural features and building services.

Building height is the vertical distance from *natural ground level* to the roof or parapet at any point.

Building height does not include non structural elements that project above the building height and service equipment including plant rooms, lift overruns, structures associated with green roof areas, screens to service areas or other such equipment provided that all of the following criteria are met:

- The total roof area occupied by the service equipment (other than solar panels) is minimised;
- The service equipment is located in a position on the roof so as to minimise its visibility;
- The non structural elements and service equipment do not cause additional overshadowing of neighbouring properties and public spaces;
- The non structural elements and service equipment do not extend higher than 3.6 metres above the maximum building height; and
- The non structural elements and service equipment are integrated into the design of the building to the satisfaction of the responsible authority.

Parapet height does not include features such as brackets, pediments, urns, finials or other decorative elements.

Setback is the shortest horizontal distance from a building, including projections such as balconies, building services and architectural features, to the boundary.

Upper level is development above the height of the street wall.

2.2 General design requirements

The following requirements apply to an application to construct a building or carry out works and must be read in conjunction with the relevant precinct design requirements.

- A permit must not be granted to construct a building or carry out works if it does not meet the mandatory requirements specified the in the relevant Precinct Tables.
- A permit must not be granted to construct a building or carry out works which exceeds the preferred building height and setbacks shown in the relevant Precinct Tables unless the following requirments are met, to the satisfaction of the responsible authority:
  - The built form outcome as a result of the proposed variation satisfies the general design objectives in Clause 1.0; and
  - The built form outcome as a result of the proposed variation satisfies the relevant precinct requirements specified in this schedule.
- Facades at ground level must be designed with floor to floor ceiling heights suitable to accommodate commercial activity in the Commercial 1 Zone and the Mixed Use Zone.
- Development must retain, enhance and incorporate existing heritage facades and building form into future redevelopment and to create a consistent street wall height of 8-11 metres along the streetscapes.
- Future vehicle access and services must be provided from a rear laneway or side street where possible.
- Development must provide setbacks which ensure that upper level additions seen from the public realm are high quality and do not diminish the appreciation of the heritage building and streetscape.
- Development must avoid repetitive stepped built form at upper levels of development.
- Unless specified in another table in this schedule, any part of a building adjacent to land in a residential zone must comply with the following:

Attachment 2 - Amendment C231 - DDO16

Table to Clause 2.2 boundary wall height and setback requirements for development adjoining a residential zone

	boundary wall height	setback
Common boundary	5 metres	45 degrees above boundary wall height
Laneway interface	8 metres	45 degrees above boundary wall height

2.3 Heritage design requirements

Design requirements for development on land affected by a Heritage Overlay or immediately adjacent to a heritage building

The following requirements apply to an application to construct a building or carry out works and must be read in conjunction with the relevant precinct design requirements.

Element	Design Requirement
Building facades and street frontages	**Infill buildings and development adjoining a heritage building** Façade treatments and the articulation of infill buildings on land affected by a heritage overlay or immediately adjoining a heritage building must: • interpret the rhythm of the heritage streetscape and/or adjoining heritage building including fenestration patterns and proportions, the relationship between solid and void, and the pattern of structural bays • be distinguishable from the original heritage fabric and adopt a high quality and respectful contextual response • ensure façade treatments and the articulation of new development are simple and do not compete with the more elaborate detailing of nineteenth century buildings • ensure fenestration patterns of new development respects the vertical proportions of nineteenth and early twentieth century facades and avoids large expanses of glazing with a horizontal emphasis except to the ground floor shop fronts • avoid large expanses of glazing with a horizontal emphasis except to ground floor shopfronts • maintain the existing canopy/verandah height of the heritage streetscape and/or adjoining heritage building(s) • avoid the use of unarticulated curtain glazing and highly reflective glass • ensure ground floor facades facilitate activated street frontages. **Adaption of contributory or individually significant buildings** Adaptation of contributory or individually significant buildings must: • avoid highly reflective glazing in historic openings • encourage the retention of solid built form behind retained facades and avoid balconies behind existing openings • maintain the inter-floor height of the existing building and avoid new floor plates and walls cutting through historic openings • encourage the retention of heritage fabric • retain original or early shop fronts and verandahs.
Upper level behind heritage street wall	**Upper level development must**: • ensure that the design and setback of the addition does not visually dominate the heritage building or surrounding heritage places • ensure that setbacks retain key viewlines • retain the primacy of the three-dimensional form of the heritage building as viewed from

Attachment 2 - Amendment C231 - DDO16

Element	Design Requirement
	the public realm to avoid 'facadism' • retain the roof form of individually significant buildings between the parapet and new development where it contributes to the significance of the building or it is visible from the public realm • utilise visually lightweight materials and finishes that are recessive in texture and colour and provide a juxtaposition with the heavier masonry of the heritage facades • use simple architectural detailing that does not detract from significant elements of the existing building or streetscape • be articulated to reflect the fine grained subdivision character of narrow sites • avoid highly contrasting or vibrant primary colours. • avoid unarticulated façades that give a bulky appearance, especially from oblique views. • incorporate fenestration patterns and solid to void ratios that respect heritage facades • provide a recessive backdrop to the heritage street wall and individual heritage buildings.

2.4 Precinct design requirements

The following precinct specific design requirements apply in addition to the general design requirements outlined in Clause 2.2.

2.4.1 Precinct 1 – Brunswick Street

Shown on the planning scheme map as **DDO16-1**

The design requirements for Precinct 1 are as follows:

- Development must
  - ensure that upper level development is visually recessive
  - retain the visual prominence of the individually significant corner building that forms the southern gateway to Queens Parade and to Fitzroy North more broadly
  - be low rise
  - reinforce the heritage values of the precinct
  - provide for vehicular access off the laneway

Map of Precinct 1

Attachment 2 - Amendment C231 - DDO16

Table 1B – Street wall height, building height and setbacks for Precinct 1B

Built Form	Mandatory requirement	Preferred requirement
Development at and adjoining 460 Brunswick Street		
Building height	Maximum 9 metres	
Street wall height and front setback	Match the parapet height of 460 Brunswick Street	Built to boundary at ground level
Upper level setback	Minimum 5 metres	
Setbacks from side and rear boundary and a laneway		Res Code B17
Development on Brunswick Street, Queens Parade and land fronting the laneway known as Lot 1 on Title Plan TP806921 (apart from land at and adjoining 460 Brunswick Street)		
Building height	Maximum 9 metres on Lot 1 on Title Plan TP806921	Maximum 9 metres elsewhere
Street wall or façade height and setback	Retain existing	Match the parapet or eaves height of taller adjacent heritage building
Setbacks from side and rear boundary and a laneway		Res Code B17
Upper level setback		6 metres from the façade

2.4.2 Precinct 2 – Boulevard Precinct

Shown on the planning scheme map as **DDO16-2**

The design requirements for Precinct 2 are as follows:

- Development must:
  - not diminish or detract from the heritage values of the boulevard streetscape, the heritage street wall and the heritage trees along Queens Parade.
  - deliver an appropriate interface arrangement to neighbouring properties and minimise visual bulk and mass when viewed from the adjoining properties.
  - avoid a repetitive strepped form within the 45 degree angle profile.
- Development in Precinct 2A must also:
  - ensure projections above the street wall are not dominant in the skyline when viewed from the north side of Newry Street and of Coleman Street, and the WT Peterson Oval, the Fitzroy Grandstand and other locations in the southern part of Edinburgh Gardens.
  - ensure adequate solar access is provided to the Queens Parade boulevard and Napier Street Reserve at the equinox from 9am-3pm.
  - maintain the prominence and significance of the art deco facade.

- recognise the low scale, buildings and fine grain subdivision pattern of existing development to the north and west.
- encourage pedestrian permeability within and through the precinct.

- Development in Precincts 2C and 2D must also:
  - maintain the prominence of heritage buildings along Napier Street when viewed from Napier Reserve.
  - be appropriately setback at upper levels from the heritage buildings along Napier Street.
  - provide an appropriate transition in scale from the heritage buildings along Napier Street and Alexandra Parade.
  - provide vehicular access from laneways
  - provide building separation
  - to reduce visual bulk

Map of Precinct 2

Table 2 – Street wall height, building height and setbacks for Precincts 2A, 2C and 2D

Built form	Mandatory requirement	Preferred requirement
Precinct 2A		
Building height	Maximum of 31m (10 storeys)	
Front street wall height	Retain height of existing heritage façade. Maximum of 10 metres (3 storeys) where no heritage façade exists.	Street wall of development adjoining the individually significant building must not exceed the parapet height of the taller adjoining heritage building.

Attachment 2 - Amendment C231 - DDO16

Front setback		0 metres to maximum 10 metres (3 storeys)
Upper level setback from front of building		Above existing heritage façade: • Minimum 8 metre setback from 10 metres to 16 metres • Minimum 10 metre setback from 16 metres Above new street wall (where no existing heritage façade): • Minimum of 5 metre setback from 10 metres to 16 metres • Minimum of 8 metres setback from 16 metres
Setback(s) from rear boundaries north and west adjacent to NRZ and GRZ		ResCode B17 from rear boundary of adjoining properties to 10 metres Setback within 45 degree angle measured from 10-25 metres (3-8 storeys)
Setbacks from side boundary east adjacent to NRZ		0 metre setback to match party wall of existing adjoining development to 10 metres (3 storeys) Setback within 45 degree angle measured from 10-25 metres (3-8 storeys)
Setbacks from side boundary east adjacent to MUZ		0 metre setback to match party wall of existing adjoining development, or 10 metre (3 storeys) where no party wall exists. Minimum of 9 metre setback from the windows/ balconies of adjoining apartments up to 16 metres (5 storeys). Minimum of 15 metre setback above 16 metres (5 storeys).
Setbacks from side and rear boundaries west and north-west adjacent to MUZ		0 metre setback to match party wall of existing adjoining development, or 10 metre (3 storeys) where no party wall exists. Setback within 45 degree angle measured from 10-25 metres (3-8 storeys)
Precinct 2C		
Built Form	**Mandatory requirement**	**Preferred requirement**
Building height		Maximum of 28 metres (8 storeys)
Front street wall height	Maximum 18 metres (5 storeys) for development on Queens Parade, George Street and Alexandra Parade	Development on Napier Street should not exceed the parapet height of the adjoining heritage buildings.
Upper level setback		5 metres minimum

Attachment 2 - Amendment C231 - DDO16

Setback(s) from boundary of 472 to 484 Napier Street		Setback within a 45 degree angle line measured from 12 metres (5 storeys).
Side and rear setbacks		ResCode B17
Precinct 2D		
Building height		18 metres (5 storeys)
Front street wall height	Retain existing parapet height	
Upper level setback	Minimum 6 metres from facade	

2.4.3 Precinct 3 – St John's Precinct

Shown on the planning scheme map as **DDO16-3**

The design requirements for Precinct 3A are as follows:

- Development must maintain views of the belfry and spire of St John's church and maintain clear sky between the belfry and spire and new development when viewed from the centre of the footpath on the south-east corner of the intersection with Queens Parade and Smith Street. A permit cannot be granted to construct a building or carry out works if it does not meet this requirement.
- Development must
  - achieve a consistent street wall height along Queens Parade, extending along Smith Street.
  - respond to the low scale form of existing development outside Precinct 3 on Hodgkinson Street through an appropriate transition in building height.
  - recognise the fine grain character of heritage streetscapes and minimise the dominance of wide building frontages
  - deliver high quality architectural detailing that respects the heritage qualities of Queens Parade and Smith Street
  - maintain the prominence of the heritage street wall in the streetscape and the vista along Queens Parade
  - ensure that upper level development is visually recessive and does not detract from the heritage streetscape
  - use materials at upper levels that are recessive in finish and colour
  - be designed so that side walls are articulated and read as part of the overall building design
  - avoids continuous built form at upper levels

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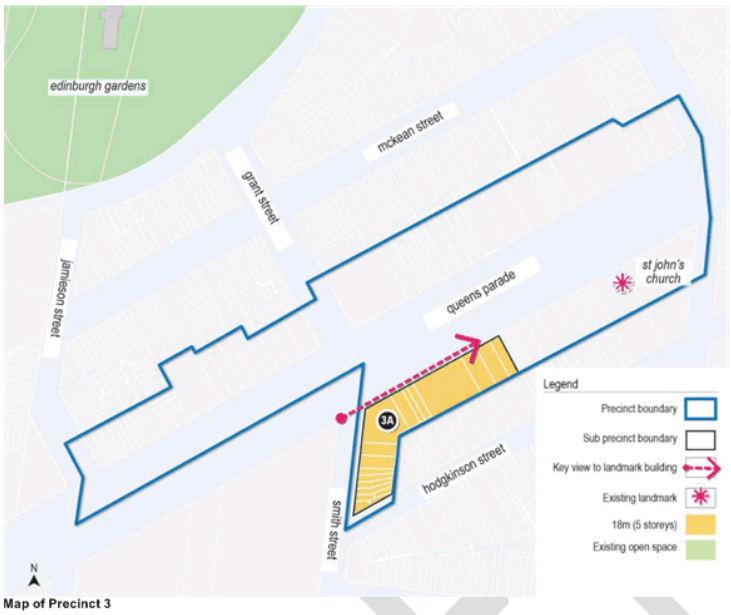
Attachment 2 - Amendment C231 - DDO16



Map of Precinct 3

Table 3 – Street wall height, building height and setbacks for Precinct 3A

Built Form	Mandatory requirement	Discretionary requirement
Building height		Maximum of 18m (5 storeys)
Front street wall height	Maximum 11 metres for 15-33 Queens Parade Retain height of existing heritage façade. Development adjoining a heritage building must match the parapet height of adjacent taller heritage building. Maximum of 14m (4 storeys) elsewhere	
Upper level setback	Minimum of 6 metres at 664 Smith Street (former Fire Station) and Minimum 6 metres at 15-41 Queens Parade	6 metres elsewhere
Street wall setback		0 metres - built to front boundary at ground level
Rear setback		45° above 8 metres from rear boundary to a laneway. 45° above 5 metres from rear boundary (no laneway).
Side setback		If adjoins NRZ, ResCode B17 0 metres elsewhere

2.4.4 Precinct 4 – Activity Centre Precinct

Shown on the planning scheme map as **DDO16-4**

The design requirements for Precinct 4 are as follows:

- Development must protect and maintain key view lines and visual prominence of the former ANZ Building from the south-west and north-east, in particular to the upper floor, roof form and chimneys. A permit cannot be granted to construct a building or carry out works if it does not meet this requirement.
- Development must:
  - retain the visual prominence of heritage buildings, their street wall and significant 'High Street' streetscapes when viewed from the opposite side of Queens Parade
  - facilitate the appropriate mid rise infill of the sites located to the rear of commercial properties fronting Queens Parade
  - retain the visual prominence of the return facades of buildings that front Queens Parade, Delbridge, Gold and Michael Streets
  - ensure that facades at ground floor incorporate verandahs which are consistent with the form and scale of adjoining verandahs
  - retain chimneys visible from the public realm
  - enhance the amenity and safety of laneways that provide pedestrian and vehicular access to buildings
  - respect the low scale, fine grain subdivision pattern of existing development on Hodgkinson Street and McKean Street

Map of Precinct 4

Table 4 – Street wall height, building height and setbacks for Precinct 4

Built Form	Mandatory requirement	Preferred requirement
Building height	21.5 metres (6 storeys)	
Front street wall height on Queens Parade	Retain height of existing heritage façade. Where no heritage façade exists, development must be:	

Attachment 2 - Amendment C231 - DDO16

	• a minimum of 8m (2 storeys) • a maximum of 11m (3 storeys) or where there is an adjacent heritage building, the parapet height of that building if taller than 11 metres.	
Front street wall height in side streets.		Retain height of existing heritage façade. Where no heritage façade exists development should be: • a minimum of 8m (2 storeys) • a maximum of 11m (3 storeys) or where there is an adjacent heritage building, the parapet height of that building if taller than 11m.
Upper level setback Queens Parade	Minimum 6 metres in significant heritage streetscape area Minimum 8 metres at 364 Queens Parade	Minimum 6 metres at 167-197 Queens Parade
Upper level setback in side streets		Minimum 6 metres
Street wall setback	0 metres - built to front boundary at ground level	
Side and rear setback (NRZ interface)		45 degree angle above 8 metres from rear boundary to laneway 45 degree angle above 5 metres where no laneway

2.4.5 Precinct 5 – North Eastern Precinct

Shown on the planning scheme map as **DDO16-5**

The design requirements for Precinct 5 are as follows:

- Development must
  - retain the visual prominence and not visually dominate the three dimensional forms of the former United Kingdom Hotel when viewed from Raines Reserve and the former Clifton Motors Garage when viewed from the opposite side of Queens Parade
  - retain, conserve and incorporate the moderne façade of the former Clifton Motor Garage (205-211 Queens Parade) in any redevelopment of the site and ensure that the three dimensional form of the façade remains prominent and the decorative vertical fin remains a prominent freestanding element when viewed from the public realm
  - be designed above street wall in Precincts 5B and 5C as a series of separate development parts with building separation.
  - establish a transition and gradual stepping down of building heights from taller forms in Precicnt 5C to existing heritage form in Precinct 5A.

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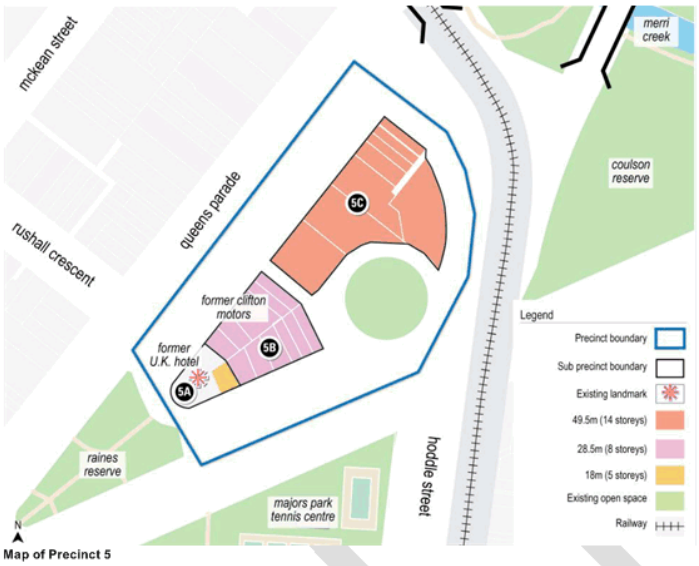

Map of Precinct 5

Table 5— Street wall height, building height and setbacks for Precincts 5A, 5B and 5C

Built form	Mandatory requirement	Preferred requirement
Precinct 5A		
Building height		18 metres (5 storey)
Street wall height	Match existing parapet or eaves height	
Upper level setback		Minimum 5 metres
Precinct 5B		
Building height		1:1 heritage street wall to new built form behind Clifton Motors and 203 Queens Parade visible from the opposite side of Queens Parade 28 metres (8 storeys) elsewhere
Front street wall height	Match parapet height of former Clifton Motor Garage and eaves line of former UK Hotel	11 metres (3 storeys) for non contributory buildings facing Queens Parade and Dummett Crescent.
Street wall setback	0 metres	
Setback from side and rear boundary	0 metres	
Upper level setback	6 metres for development at former Clifton Motor Garage	6 metres elsewhere.

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Precinct 5C		
Built Form	**Mandatory requirement**	**Preferred requirement**
Building height		49 metres (14 storeys)
Front street wall height		35 metres (10 storeys)
Upper level setback		10 metre street wall fronting Queens Parade

Application Requirements

Unless with the written consent of the Responsible Authority, an application must provide:

- Perspectives showing the form, massing, profile, material finishes and detailed design of the proposed building(s), including perspectives from the opposite side of Queens Parade.
- Perspective and modelling of the proposed building(s) in:
  - Precinct 2A when viewed from the north side of Newry Street and of Coleman Street, and the WT Peterson Oval, the Fitzroy Grandstand and other locations in the southern part of Edinburgh Gardens to Council's specfications.
  - Precinct 3A when viewed from the centre of the footpath at the south-eastern corner of the intersection of Queens Parade and Smith Street (Precinct 3A).
  - Precincts 3A and 4 when viewed from the south side of Hodgkinson Street and north side of McKean Street (Precinct 4).
  - Precinct 5 when viewed from Raines Reserve and Coulson Reserve.
  - Sight line analysis from the opposite side of the relevant street taken at eye level (1.6 metres above the footpath 1 metre from the kerb).
- The following information in the Traffic Engineering Report:
  - measures to reduce conflict and improve pedestrian and bicycle amenity
  - details regarding loading and waste collection
  - any impacts on the operational efficiency and road safety of the tram route
  - any impacts on laneway access and services in the laneway
  - any modifications to existing roads and/or laneways and/or provision of new laneway(s)
- A green travel plan prepared by a suitably qualified person outlining site-specific initiatives and actions to encourage the use of more sustainable transport options.
- An environmental wind assessment prepared by a suitably qualified professional to undertake measurements of environmental wind conditions around the proposed development and develop wind amelioration features as necessary.
- A heritage impact statement prepared by a suitably qualified professional that assesses the impact of the proposed development on the heritage values of the heritage place and nearby heritage places, as identified in the relevant heritage studies and citations.
- A waste management plan prepared by a suitably qualified waste management expert.

3.0 Subdivision

xx/xx/xxxx
Proposed C231

None specified.

Attachment 2 - Amendment C231 - DDO16

4.0 Advertising

xx/xx/xxxx
Proposed C231

None specified.

5.0 Decision guidelines

xx/xx/xxxx
Proposed C231

The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specified in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

- Whether the General Design Requirements, Heritage Design Requirements and the Precinct Design Requirements in Clauses 2.2, 2.3 and 2.4 are met.
- The architectural quality of the proposal including the design, scale height, materials, mass and visual bulk of the development in relation to the surrounding area.
- If roof decks are proposed, whether they are set back from lower levels and are recessive in appearance.
- The profile and impact of development along Queens Parade and from the north side of Newry Street and of Coleman Street, and the WT Peterson Oval, the Fitzroy Grandstand and other locations in the southern part of Edinburgh Gardens.
- The design response at the interface with existing, low scale residential properties
- The design of the streetscape interface along the primary street frontage and its contribution to an active street environment.
- Whether side and rear setbacks are sufficient to limit the impact on the amenity of existing dwellings.
- How any proposed buildings and works will impact on solar access to Queens Parade and Napier Street Reserve.
- Whether heritage buildings on street corners retain their prominence when viewed from both streets.
- Whether heritage buildings retain their three-dimensional form when viewed from the public realm.
- Whether upper level development above the heritage street wall is visually recessive and does not dominate or visually overwhelm the heritage buildings.
- Whether the proposal contributes to and improves the pedestrian environment and other areas of the public realm.
- The suitability of vehicle access arrangements and the location, layout and appearance of areas used for car parking.
- The impact of development on views to:
  - the former ANZ Bank building's tower, roof, chimney and upper level
  - the St John the Baptist Church belfry and spire
  - the former Clifton Motor Garage's Moderne façade and fin
- The wind effects created by the development.
- For residential development, an acoustic report prepared by a suitably qualified acoustic engineer, demonstrating how the requirements of the State Environment Protection Policy (Control of Noise from Commerce, Industry and Trade) No. N-1, the State Environment Protection Policy (Control of Music Noise from Public Premises) No. N- 2, sleep disturbance criteria and any other relevant Australian Standards, as applicable, will be met.

Reference Documents

Queens Parade, Clifton Hill Built Form Review prepared by Hansen Partnership – December 2017.

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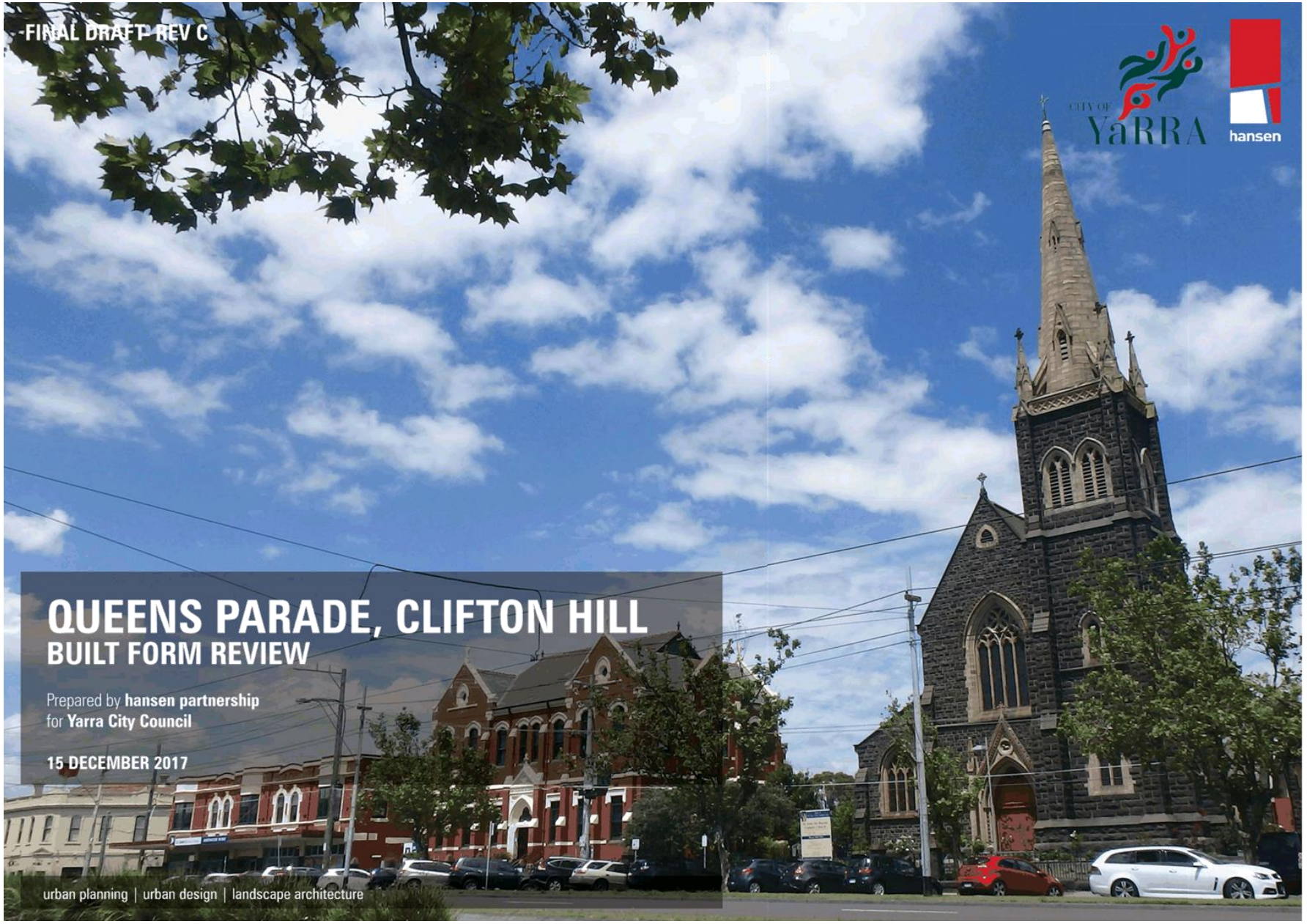

Attachment 3 - Amendment C231 - Hansen Built Form Review

DRAFT

Table of Content

1.0	**Introduction**	**3**
	The Study Area & Focus Area	5
2.0	**Contextual Analysis**	**6**
	Policy Framework	7
	Heritage Attributes	10
	Landscape & Streetscape	18
	Subdivision Pattern	23
	Building Heights & Recent Development	29
	Road Network & Circulations	33
3.0	**Existing Built Form Character Areas**	**36**
4.0	**Built Form Framework Concept**	**40**
	Preferred Future Character	42
5.0	**Precinct Guidelines**	**45**
	Precinct 1: Brunswick Street	48
	Precinct 2: Boulevard Precinct	50
	Precinct 3: St John's Precinct	54
	Precinct 4: Activity Centre Precinct	56
	Precinct 5: North East Precinct	58

Appendices

Appendix A: Queens Parade Built Form Testing

Appendix B: Queens Parade Planning Policy Framework Summary

Figures

Figure 1: Strategic Context	3
Figure 2: Study Area Boundary Map	4
Figure 3: Existing Zoning Map	8
Figure 4: Existing and Proposed Overlays Map	10
Figure 5: Existing and Proposed Heritage Precinct Overlays Map	12
Figure 6: Existing and Proposed Heritage Grading Map	16
Figure 7: Terrain and slope	18
Figure 8: Streetscape Experience Map	20
Figure 9: Lot Sizes Map	24
Figure 10: Frontage Widths Map	25
Figure 11: Lot Depths Map	26
Figure 12: Land Use Map	27
Figure 13: Building Heights & Development Trajectory Map	30
Figure 14: Access and Movement Map	34
Figure 15: Built Form Character Areas Map	38
Figure 16: Built Form Framework Map	44
Figure 17: Precinct and Sub- Precinct Boundaries Map	46
Figure 18: Precincts 1 & 2 Framework Plan	48
Figure 19: Precinct 3 Framework Plan	54
Figure 20: Precinct 4 Framework Plan	56
Figure 21: Precinct 5 Framework Plan	58

Tables

Table 1: Proposed Heritage Overlay Changes	11
Table 2: Recent Development Approvals/ Proposals	31
Table 3: Summary of Built Form Guidelines	47

Version	A	B	C
Issue Date	3 February 2017	31 October 2017	15 December 2017

Acknowledgement

Council Project Team

The following members are particularly thanked for their contribution:

- David Walmsley: Manager City Strategy
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Attachment 3 - Amendment C231 - Hansen Built Form Review

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1.0 Introduction

The following report has been prepared for the City of Yarra as a basis for a clear urban design vision for the Queens Parade corridor, between Hoddle Street and Alexandra Parade in Clifton Hill. It sets out a preferred built form framework, underpinned by urban design and heritage assessments.

This report documents the analysis and methodology used to justify the built form recommendation and sets out the prevailing attributes of Queens Parade (comprising both its existing buildings and spaces) to inform recommended future urban form directions for the corridor.

The preparation of the Queens Parade Built Form Review has been undertaken by a multidisciplinary team comprising Hansen Partnership (Urban Design & Planning) and GJM Heritage (Heritage) with the support of City of Yarra's internal working group.

A Queens Parade Built Form Heritage Analysis & Recommendations were undertaken by GJM Heritage concurrently with the Built Form Review. These Recommendations have been integrated within this Review.

Structure of the Built Form Review

1 Introduction
2 Contextual Analysis
- Policy Framework
- Heritage
- Landscape & Streetscape
- Subdivision Pattern
- Building Heights & Development Trajectory
- Road Network

3 Existing Built Form Character Areas
4 Built Form Framework Concept
- Built Form Principles
- Preferred Future Built Form Character

5 Precinct Guidelines

APPENDICES

A Built Form Testing
B Relevant Policy Framework Summary

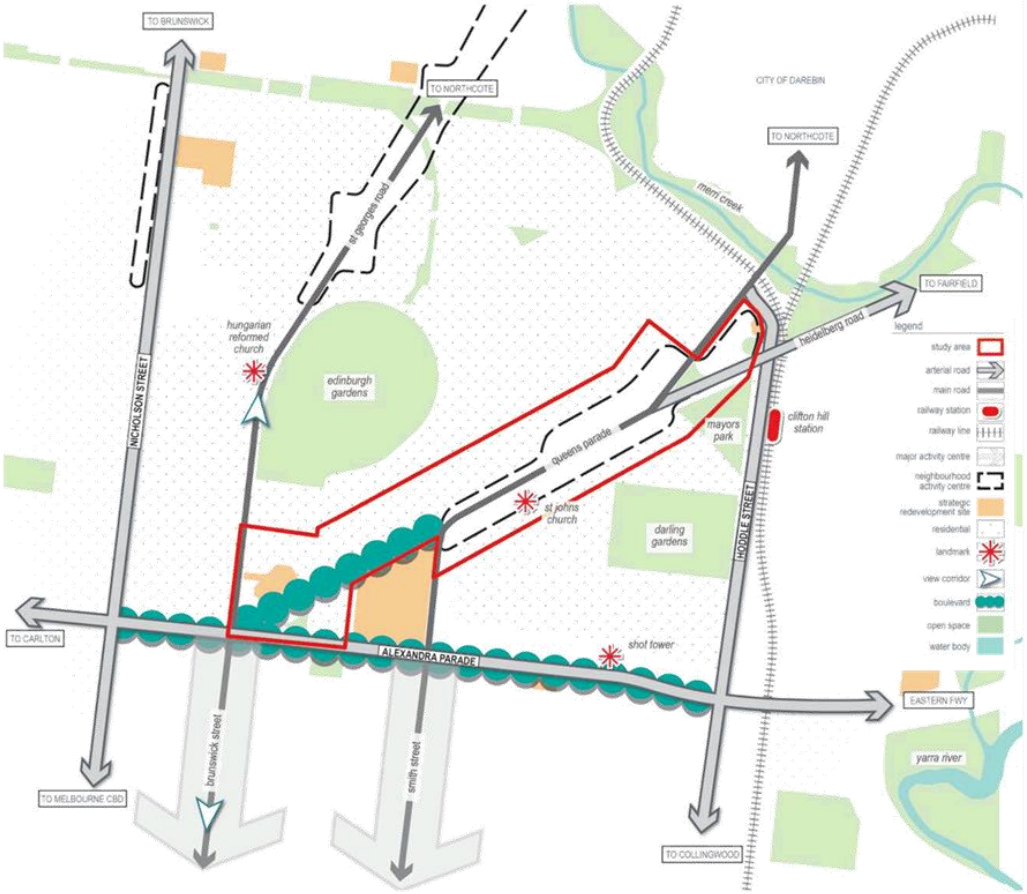

Figure 1: Strategic Context

Attachment 3 - Amendment C231 - Hansen Built Form Review

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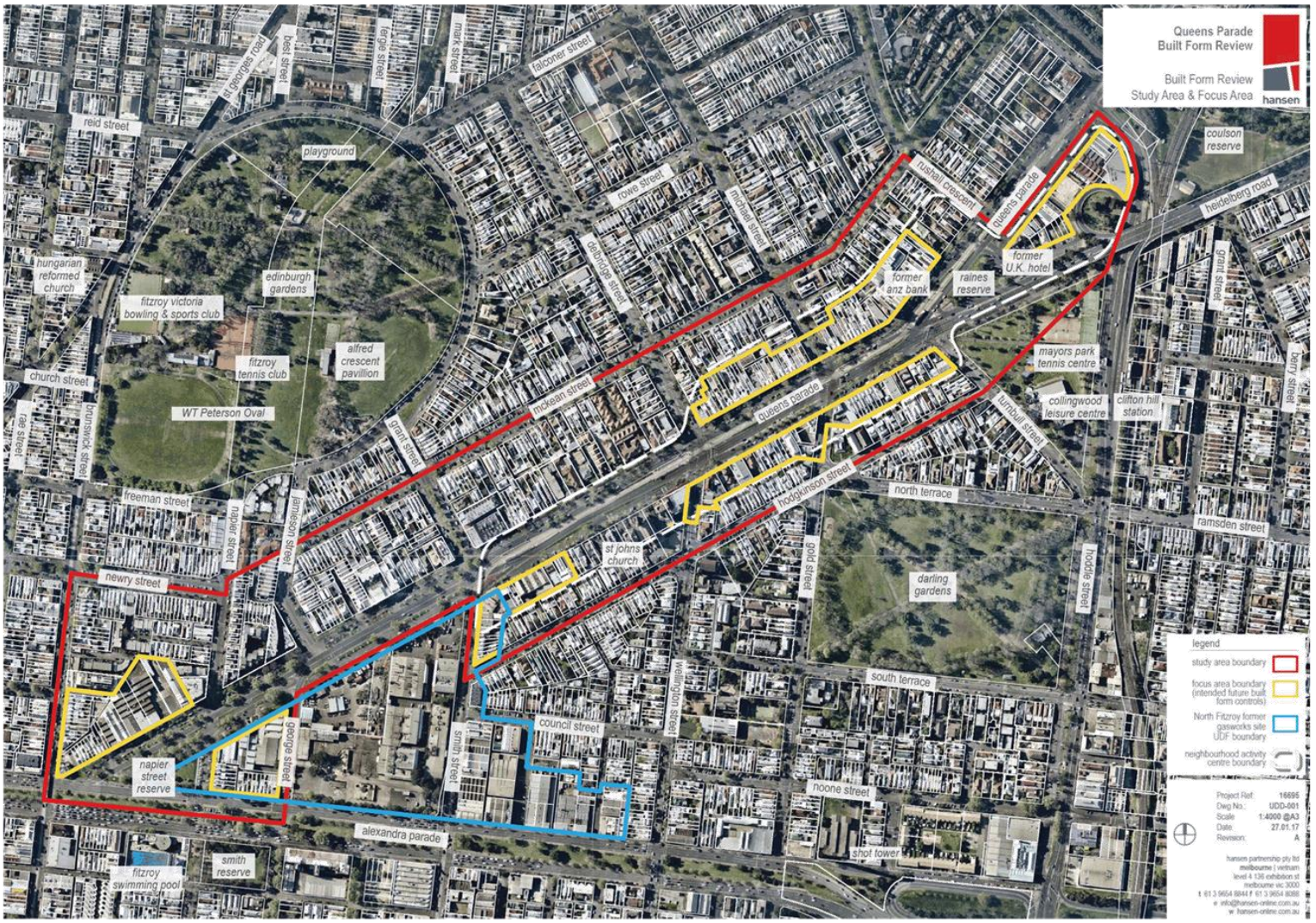

Figure 2: Study Area Boundary Map

Attachment 3 - Amendment C231 - Hansen Built Form Review

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The Study Area & Focus Area

The Study Area for the Queens Parade Built Form Review (the Review) encompasses urban blocks bound by Brunswick Street to the west, McKean Street to the north, Rushall Crescent and Hoddle Street to the east and Hodgkingson Street and Alexandra Parade to the south (refer to Figure 1 and Figure 2).

The Municipal Strategic Statement and Local Planning Policies provide policy direction with regard to future land use and expected growth within the Municipality. Whilst the Local Planning Policies include high level built form strategies that seek to support Yarra's identity as a low-rise urban form with pockets of higher development (Clause 21.05-2), there are no specific height or setback controls that apply to the NAC or the broader Study Area (on non- residential land).

Recommendations of this Review will form the basis of future development controls applicable for land that are affected by C1Z, C2Z and MUZ within the Study Area. It is noted that future redevelopment of NRZ and GRZ land in the Study Area will continue to be influenced by its existing zoning provisions.

For the purpose of this Review, a Focus Area was identified, comprising properties with frontages to Queens Parade, or properties with the potential to influence future character of Queens Parade corridor. Specifically, the **Focus Area comprises:**

- The eastern portion of Queens Parade is designated as a **Neighbourhood Activity Centre** in the Yarra Planning Scheme (Clause 21.08-Neighbourhoods) and is a traditional retail strip with land within C1Z and MUZ, albeit with a significantly wider profile compared to other 'traditional' shopping streets within the City of Yarra and Metropolitan Melbourne. It is referred to as the Clifton Hill, Queens Parade NAC throughout this report.
- Notably, the Focus Area includes Commercial zoned land (C1Z, C2Z) within proximity to Queens Parade, forming the western and eastern parts of the **North Fitzroy Gasworks Precinct**. Both the Study and Focus Area boundaries have excluded the central urban block which is currently affected by Public Use Zone (PUZ1, PUZ6). A Council adopted Urban Development Framework (UDF, 2008- prepared by MGS) will continue to influence future activities for the designated Strategic Development Site (between George Street and Smith Street) including the pattern of streets, public spaces and building heights.
- The Focus Area also includes Mixed Use zone land (MUZ) which define its western portion. These urban blocks define the important junction of Queens Parade, Alexandra Parade and Brunswick Street. Within the Planning Context, Queens Parade is considered a **'main road'** and **'boulevard'** that is strategically important in the locality. Alexandra Parade, partially located within the Study Area, is also a major arterial road that connects Melbourne's eastern suburbs to Fitzroy and Collingwood to the south and Clifton Hill to the north.
- The Focus Area is also affected by multiple **Heritage Overlays** that include buildings of both contributory and individual significance, along with the protection of the Queens Parade avenue trees. Along its south western boundary (on properties adjoining Alexandra Parade), the Study Area is partially affected by Schedule 2 to the Design and Development Overlay which requires consideration of proposed development along boulevards, however this is not applied to Queens Parade.
- Three sites within the Focus Area are identified as **Strategic Redevelopment Sites** within the Yarra Planning Scheme (Clause 21.05), where higher development may be accommodated. Whilst some of these sites are influenced by existing Heritage Overlays and varied zoning (MUZ and PUZ1, PUZ6), future development greater than 5-6 storeys on Strategic Redevelopment Sites are guided by their ability to achieve specific benefits such as:
  - Significant upper level setbacks;
  - Architectural design excellence;
  - Best practice environmental sustainability objectives in design and construction;
  - High quality restoration and adaptive re-use of heritage buildings;
  - Positive contribution to the enhancement of the public domain; and
  - Provision of affordable housing.

Attachment 3 - Amendment C231 - Hansen Built Form Review

Attachment 3 - Amendment C231 - Hansen Built Form Review

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Contextual Analysis
Policy Framework

The following is a review of the relevant State and Local Planning Policies that influence the existing and future built form outcomes within the Study Area.

Plan Melbourne 2017-2050

The updated Plan Melbourne document provides a guiding framework for Metropolitan Melbourne. Whilst the Queens Parade Neighbourhood Activity Centre and its corridor is not identified as one of 121 Major Activity Centres, it remains an area that is well serviced by public transport and infrastructure.

Some of the relevant directions are applicable to the Queens Parade Built Form Review, as follows:

- **Direction 2.1:** *Manage the supply of new housing in the right locations to meet population growth and create a sustainable city.*
- **Direction 2.2:** *Deliver more housing close to jobs and public transport.*
- **Direction 4.1:** *Create more great public places across Melbourne.*
- **Direction 4.4:** *Respect Melbourne's heritage as we build for the future.*
- **Direction 5.1:** *Create a city of 20- minute neighbourhoods.*

State Planning Policy Framework (SPPF)

- The State Planning Policy Framework (SPPF), underpinned by *Plan Melbourne 2017- 2050*, sets the planning and strategic context and metropolitan scale. It provides the basis for a network of Activity Centres (Clause 11.01-1) and the need to undertake appropriate Activity Centre planning (Clause 11.01-2). Whilst advocating for growth across Metropolitan Melbourne, particularly in areas well serviced by infrastructure (Clause 11.06-2), the SPPF also advocates for recognition of heritage value and amenable urban environment through careful management of the ongoing processes of growth and change in the urban environment (Clause 11.06-4).
- The SPPF also outlines a range of applicable polices relating to built form and heritage. Clause 15 seeks to ensure new development respond appropriately in terms of built form, landscape values and cultural context and that significant places are protected. It emphasises that good urban design should contribute positively to the local sense of place and urban character, reflects the identity and aspirations of the community, enhance the liveability and amenity of the public realm, and minimise the impact on neighbouring properties. Specific sub-clauses address Urban Design (Clause 15.01-1), Urban Design Principles (Clause 15.01-2), Cultural Identity and Neighbourhood Character (Clause 15.01-5) and Heritage Conservation (Clause 15.03-1).
- The SPPF sets out a policy framework for Housing at Clause 16 to ensure the sustainable supply of housing in readily serviceable locations within walking distance to activity centres, public transport and open space. Clause 16 seeks to locate new housing in or close to activity centres and employment corridors and at other Strategic Redevelopment Sites that offer good access to services and transport (Clause 16.01-2 – Location of residential housing), and to provide for a range of housing types to meet increasingly diverse needs (Clause 16.01-4 – Housing diversity).

Local Planning Policy Framework (LPPF)

- The Local Planning Policy Framework (LPPF) sets out the planning and strategic context at the municipal level and provides vision and policy guidance specific to the context of the City of Yarra. Clause 21.04 Activity Centres designates 5 Major Activity Centres, but as this does not include Queens Parade, by default it is designated as Neighbourhood Activity Centre.
- Clause 21.05 Built Form of the LPPF addresses matters relating to Heritage and Urban Design. Clause 21.05-1 Heritage establishing a strong policy position to acknowledge, protect and enhance heritage places within the City of Yarra. Relevant strategies include:
  - Conserve, protect and enhance identified sites and areas of heritage significance including pre-settlement ecological heritage (Strategy 14.1).
  - Support the restoration of heritage places (Strategy 14.2).
  - Protect the heritage skyline of heritage precincts (Strategy 14.3).
  - Protect the significant landscape and heritage within streets, parks, gardens, waterways or other open spaces (Strategy 14.5).
  - Protect buildings, streetscapes and precincts of heritage significance from the visual intrusion of built form both within places and from adjoining areas (Strategy 14.6).
- Clause 21.05-2 Urban Design recognises the valued low-rise urban form of the City of Yarra and that the existing urban form is punctuated by taller buildings located near activity centres as well as landmark buildings, with objectives to reinforce the existing urban framework and to retain Yarra's identity as a low-rise urban form with pockets of higher development. Relevant strategies include:
  - Maintain and strengthen the preferred character of each Built Form Character Type within Yarra (Strategy 16.2).
  - Ensure that development outside activity centres and not on Strategic Redevelopment Sites reflects the prevailing low-rise urban form (Strategy 17.1).
  - Development on Strategic Redevelopment Sites or within activity centres should generally be no more than 5-6 storeys unless it can be demonstrated that the proposal can achieve specific benefits such as: significant upper level setbacks; Architectural design excellence; best practice environmental sustainability objectives in design and construction; high quality restoration and adaptive re-use of heritage buildings; positive contribution to the enhancement of the public domain; and provision of affordable housing (Strategy 17.2) .
  - Further specifically relevant objectives and strategies of Clause 21.05-2 Urban Design include:
    - To retain, enhance and extend Yarra's fine grain street pattern (Objective 18).
    - Encourage the re-establishment of streets and laneways through new development sites where such links were part of the historic street pattern, except where this will cause detrimental traffic impacts (Strategy 18.1).
    - To create an inner city environment with landscaped beauty (Objective 19).
    - Require well resolved landscape plans for all new development (Strategy 19.1).
    - Encourage opportunities for planting suitable trees and landscape areas in new development (Strategy 19.2).
    - Encourage the retention of mature vegetation (Strategy 19.3).
    - Protect mature and healthy flora species where they have heritage value or are a valued part of the character of an area (Strategy 19.4).
    - To ensure that new development contributes positively to Yarra's urban fabric (Objective 20).
    - Ensure development is designed having particular regard to its urban context and specifically designed following a thorough analysis of the site, the neighbouring properties and its environs (Strategy 20.1).
    - Require development of Strategic Redevelopment Sites to take into account the opportunities for development on adjoining land (Strategy 20.2).
    - Reflect the fine grain of the subdivision pattern in building design where this is part of the original character of the area (Strategy 20.3).
    - To enhance the built form character of Yarra's activity centres (Objective 21).
    - Require development within Yarra's activity centres to respect and not dominate existing built form (Strategy 21.1).
    - Require new development within an activity centre to consider the context of the whole centre recognising that activity centres may consist of sub-precincts, each of which may have a different land use and built form character (Strategy 21.2).
    - Support new development that contributes to the consolidation and viability of existing activity centres (Strategy 21.3).
  - Clause 21.08-4 Clifton Hill identifies the eastern extent of the Study Area being within the Clifton Hill Queens Parade Neighbourhood Activity Centre, which is characterised as a mixed use centre focused on convenience retailing. Relevant aspects of Clause 21.08-4 seek to support development that maintains and strengthens the preferred character of the relevant Built Form Character type; and maintain the visual prominence of the Spire of St John's Church and the Shot Tower.

Attachment 3 - Amendment C231 - Hansen Built Form Review

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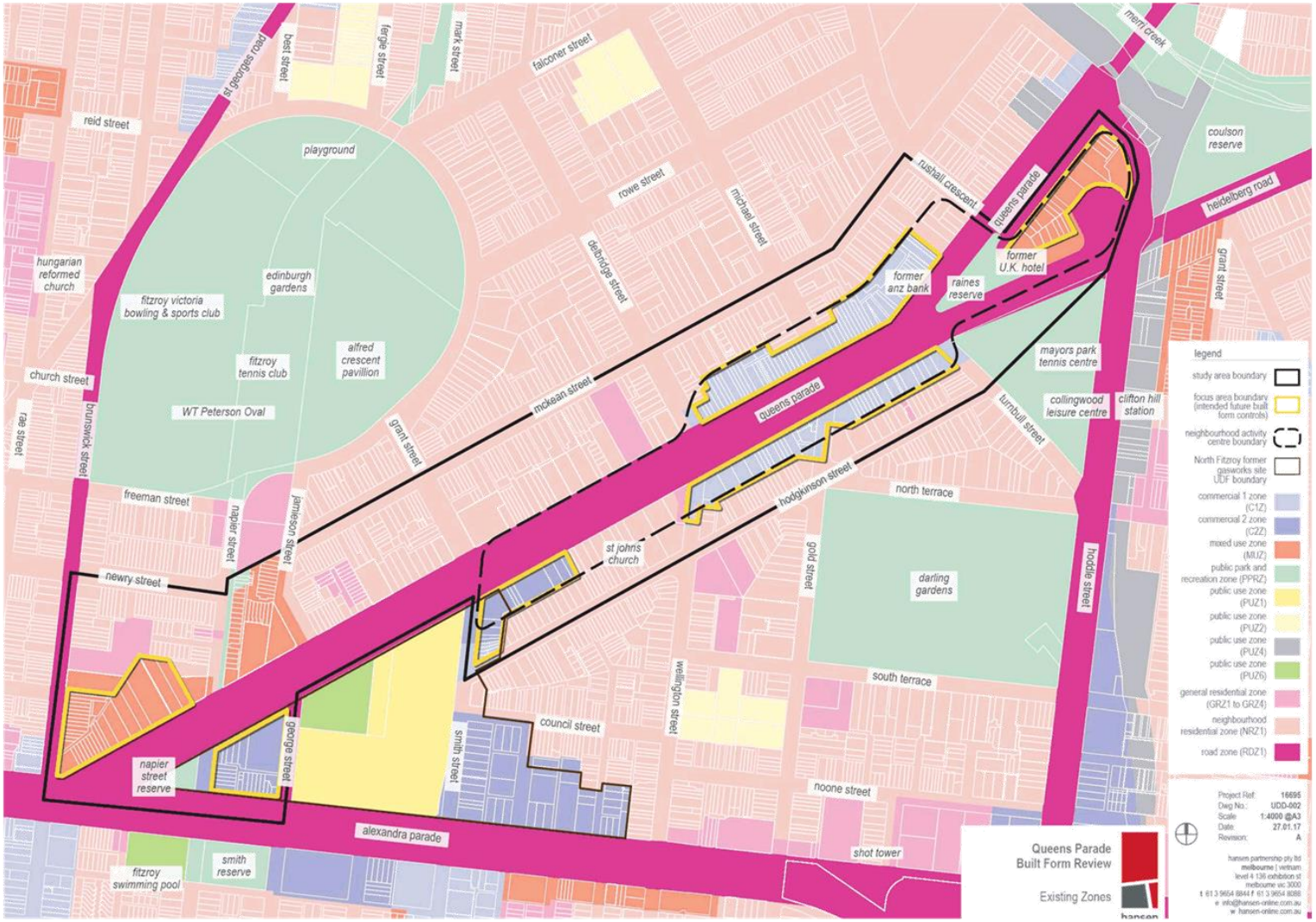

Figure 3: Existing Zoning Map

Attachment 3 - Amendment C231 - Hansen Built Form Review

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Contextual Analysis

Policy Framework

- Clause 21.08-8 North Fitzroy identifies part of the Study Area as the Queens Parade Neighbourhood Activity Centre, in addition to identifying the site at 26-56 Queens Parade as a Strategic Redevelopment Site. This seeks to encourage its redevelopment in a way that contributes positively to the urban fabric and public domain of Yarra, and (where subject to the Heritage Overlay), protect the heritage values of the site.
- With reference to the substantial extent of heritage buildings (both individually listed and precinct based), Clause 22.02 - Development Guidelines for Sites Subject to the Heritage Overlay, provides guidance for the protection and enhancement of heritage places in the City of Yarra.
- The LPPF contains further relevant Local Planning Polices relating to: landmarks and tall structures (Clause 22.03), interfaces between commercial, industrial and residential activities (Clause 22.05), development abutting laneways (Clause 22.07), built form and design for new development not affected by the Heritage Overlay (Clause 22.10) which includes design objectives to address urban form, character, setbacks and building height.
- The City of Yarra Planning Scheme has well- established heritage provisions at Clauses 21.05-1 and 22.02. Also of relevance to the protection of the heritage values of Queens Parade is Clause 22.03, which includes policy to protect the visual prominence of the St John Church spire in addition to other landmarks within the municipality.
- This section should be read in conjunction with Appendix B: Relevant Planning Framework. The following documents are also referred to and form part of the relevant Policy Context:
  - Urban Design Guidelines for Victoria (August 2017);
  - Urban Design Charter (2009); and
  - City of Yarra Urban Design Strategy (June 2011).

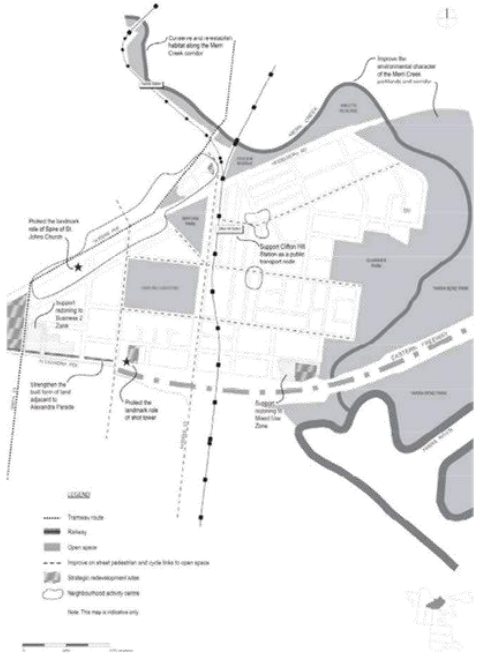
By way of providing an overarching summary of the most significant elements of the relevant policy context, the following are raised:

- The Queens Parade Study Area is comprehensively covered by Heritage Overlays which will be a key influence over its future character and opportunities for development within the area.
- The Queens Parade Neighbourhood Activity Centre is specifically covered by a heritage precinct overlay that recognises its heritage significance as a 19th century strip shopping centre. Policy provides further guidance on the heritage outcomes expected through new development.
- Strategies support enhancing the built form character of activity centres, to respect and not dominate existing built form and consider the context of the whole centre
- Policy seeks to protect the view lines to St John's Church spire.
- The former ANZ bank building and the Clifton Motors and the UK Hotel are individually significant heritage buildings and important local landmarks within the centre. There are a range of other sites within the centre and along Queens Parade that are individually significant.
- HO93 provides protection of the Queens Parade street trees.
- Development outside of Strategic Development Sites and activity centres should reflect the prevailing low-rise urban form.
- Two strategic sites are nominated within the Study Area in the Municipal Strategic Statement, 26 Queens Parade and 501-513 Hoddle Street (Dummett Crescent).
- The Dummett Crescent area has been accepted as a location for 'taller buildings' under local policy and as a strategic redevelopment site and through recent planning permit approvals.
- Development within activity centres and on Strategic Redevelopment Sites 'should' generally be no more than 5-6 storeys unless it can demonstrate specified benefits detailed in the local policy.
- The Queens Parade Activity Centre is considered a Neighbourhood Activity Centre and whilst growth expectation may be more moderated when compared to higher order Activity Centres (MAC/PAC), there is still opportunity for change in an area that is well serviced by infrastructure.
- The area surrounding, and parts of the Study Area are mostly low scale heritage residential areas with limited capacity for change due to their residential zoning (NRZ), subdivision pattern and existing use.

Activity Centres that are also subject to extensive Heritage Overlay controls (like Queens Parade) are examples of where the tension between competing planning objectives must be resolved in a balanced way.

Queens Parade has excellent public transport connections, vibrant retail, commercial and hospitality uses and a high demand for housing choice. It also contains a highly intact, turn of the century commercial precinct containing heritage fabric that is highly valued by the local community.

A balance between the demand for more intensive development with the protection of the heritage buildings and precincts is therefore required. To achieve this, the Built Form Review – and the background work that underpins it must be informed by heritage considerations which frame prospective built form parameters and related growth objectives.



Clause 21.08 (Figure 11): Neighbourhood Map - Clifton Hill

Attachment 3 - Amendment C231 - Hansen Built Form Review

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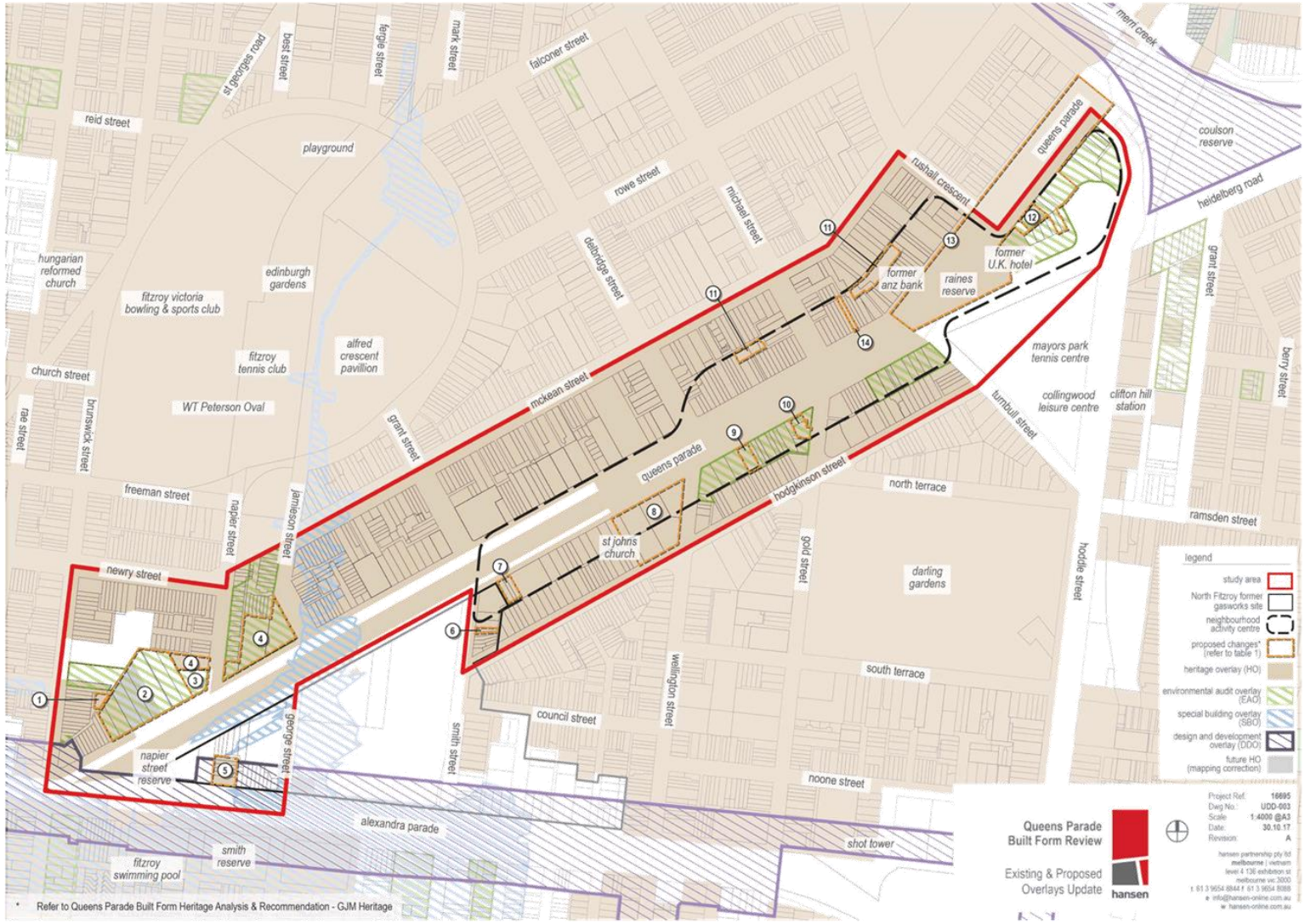

Figure 4: Existing & Proposed Overlays Map

Attachment 3 - Amendment C231 - Hansen Built Form Review

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Contextual Analysis

Heritage Attributes

Heritage Overlays

There are a number of existing Heritage Overlays that cover the Study Area (refer to Figures 4 and 5), including:

- Queens Parade Precinct, including the St John's Church complex (HO330)
- Fitzroy North Precinct (HO327),
- Clifton Hill Western Precinct (HO317),
- Queens Parade Street Trees (HO93),
- Former ANZ Bank, 370-374 Queens Parade, Clifton Hill (HO221),
- Former United Kingdom Hotel, 199 Queens Parade, Clifton Hill (HO92),
- Baptist Church and Hall, 100 Hodgkinson Street, Clifton Hill (HO317), and
- Haselmere, 96 McKean Street, North Fitzroy (HO216).

A comprehensive heritage review through desktop analysis, fieldworks and internal workshops informed heritage considerations for the Study Area (Refer to Queens Parade Built Form Heritage Analysis & Recommendation - GJM Heritage).

The heritage review sets out recommendations for future management of heritage assets, which in turn influence built form recommendation for the Study Area. It considers:

- The suitability of the extent of the Heritage Overlays for places and precincts within the Study Area.
- The heritage grading of each property within the Heritage Overlay in the Incorporated Document *City of Yarra Review of Heritage Overlay Areas 2007: Appendix 8*.
- The currency of the existing Statements of Significance for places and/or precincts to ensure they provide adequate guidance for the management of important heritage features.
- Places not currently included in the Heritage Overlay, but which warrant further consideration for inclusion in the Heritage Overlay.
- Built form parameters necessary to appropriately manage increased commercial and residential development within the context of the existing heritage place and/or precinct.

A summary of recommendations by GJM Heritage pertaining to the Built Form Review is provided in **Table 1: Proposed Heritage Updates** and areas subject for review/ update are also graphically reflected on Figures 4 to 6.

Table 1: Proposed Heritage Overlay Changes *

No.	Address	Proposed Update* (Refer to Figures 4 to 6)
1	Lot 1 (TP806921)	Update grading to 'non contributory'. The site is currently vacant.
2	54-56 Queens Parade (K.G. Luke Pty Ltd factory)	Extend the Heritage Overlay to cover the former K.G. Luke Pty Ltd factory site to the extent of the Art Deco façade and associated showroom and factory buildings.
3	58 Queens Parade	Update heritage grading from 'individually significant' to 'non contributory'. This site has been redeveloped as a five-storeys apartment building
4	80, 86 and 88 Queens Parade, 32, 33 and 34 Jamieson Street and 497 Napier Street	Update heritage grading to 'non contributory'. These sites are currently occupied by contemporary multi unit residential development.
5	472-484 Napier Street	To be included in the Heritage Overlay as an individually significant local heritage place.
6	662 Smith Street (former Fire Station)	Update heritage grading from 'contributory' to 'individually significant'.
7	7, 9 and 11 Queens Parade	Update heritage grading from 'contributory' to 'non- contributory'.
8	St John's Church complex	To be included into a site specific Heritage Overlay in recognition of its significance within the City of Yarra and trigger internal controls for the Fincham Organ.
9	105 Queens Street (Doctor's Surgery & Residence)	Retain 'individually significant' heritage grading. Prepare a site specific Statement of Significance to guide management of this atypical site.
10	137 Queens Parade	Designate 'non contributory' heritage grading to the vacant site.
11	390A Queens Parade and the rear parts of 304, 312, 316 and 380 Queens Parade	• The properties at 390A and the rear of 304, 312, 316 and 380 be removed from HO327 – Fitzroy North Precinct and include within the boundary of HO330 – Queens Parade. • 380-378 Queens Parade' within HO327 – North Fitzroy Precinct be removed and that 380 Queens Parade be identified as a 'contributory' building within HO330 – Queens Parade Precinct. • The following sites and associated gradings be identified in Appendix 8 under HO330 – Queens Parade Precinct: ▪ 304 Queens Parade (rear, off Koormiel Lane) – 'not contributory' ▪ 312 Queens Parade (rear, off Koormiel Lane) – 'contributory' ▪ 316 Queens Parade (rear, off Koormiel Lane) – 'not contributory' ▪ 380 Queens Parade (rear, behind 360-378) – 'not contributory' ▪ 390A Queens Parade (two-storeys building in north-east corner) – 'contributory' ▪ 390A Queens Parade (all buildings excluding two-storeys building) – 'not contributory'
12	205-211 Queens Parade (former Clifton Motors Pty Ltd Building)	• To be removed from HO330. • To be included in its own site-specific Heritage Overlay (note current consideration for inclusion in VHR).
13	201-217 Queens Parade and 10-12 Dummett Crescent	• To be removed from HO330. HO330 boundary should be extended over the full extent of the historic layout of Raines Reserve and should cover the historic Queens Parade boulevard to the railway bridge.
14	350 Queens Parade	Update heritage grading from 'non-specified' to 'contributory'.
	Queens Parade Precinct (HO 330)	• Update Statement of Significance for HO330 (Refer to updated Statement of Heritage Significance - Queens Parade Precinct - HO330 on page 12).

**** Refer to Queens Parade Built Form Heritage Analysis & Recommendation - GJM Heritage***

Attachment 3 - Amendment C231 - Hansen Built Form Review

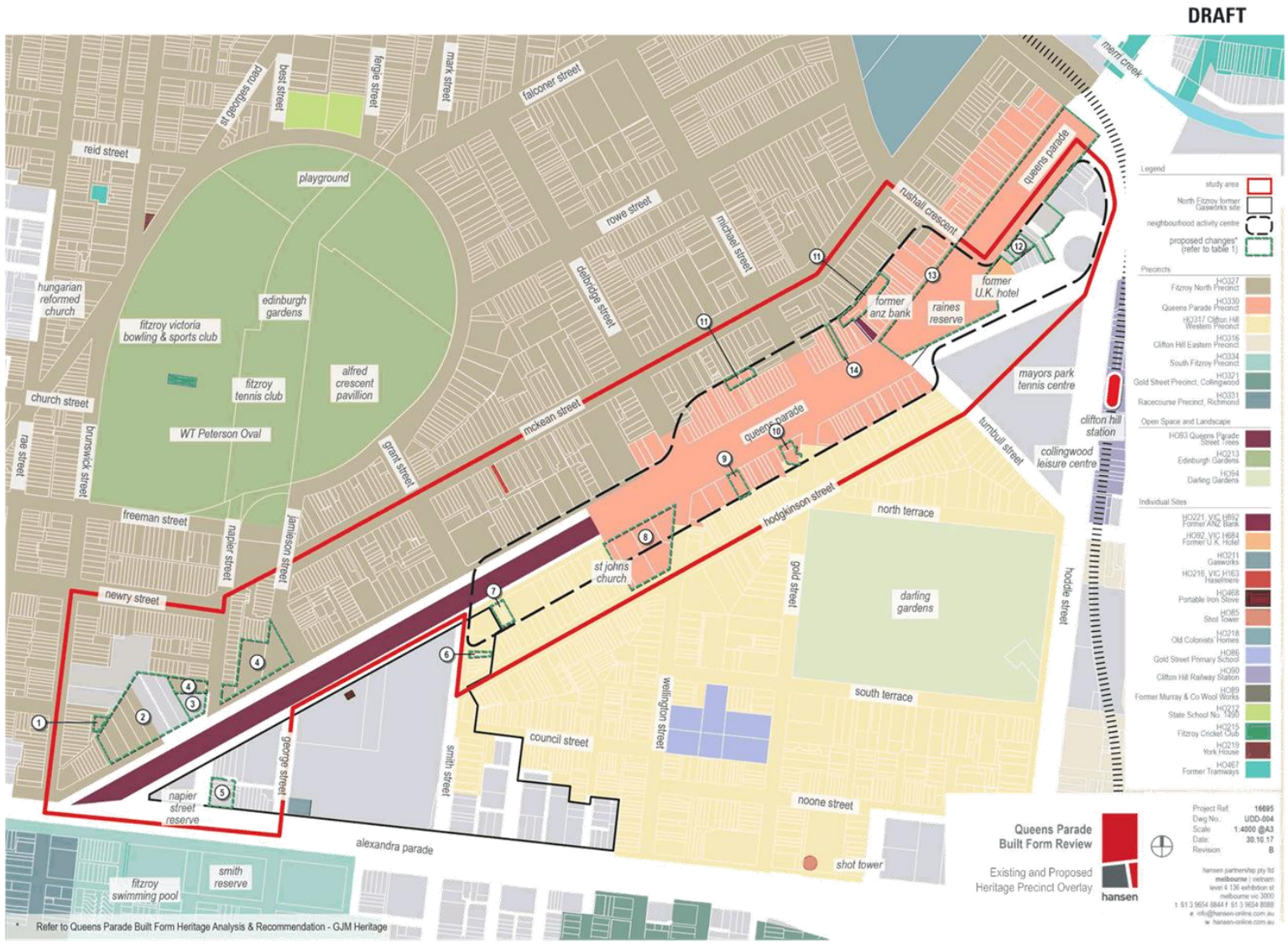

Figure 5: Existing and Proposed Heritage Precinct Overlays Map (refer to Table 1)

Attachment 3 - Amendment C231 - Hansen Built Form Review

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Contextual Analysis

Heritage Attributes

Statement of Heritage Significance

Queens Parade Precinct (HO330) - Updated 2017

- *The Queens Parade Precinct is located on a major thoroughfare that has linked Melbourne to the northern suburbs since the 1830s. It is centred on one of Melbourne's early reserved boulevards, having been surveyed by Hoddle and reserved in 1853. The wide boulevard retains distinctive median strip landscaping from the twentieth century, including Raines Reserve. The Queens Parade Precinct is an intact turn of the century 'High Street' that has functioned continuously as a key commercial centre since the 1870s. It contains a substantial collection of intact commercial buildings, predominantly from the Victorian and Edwardian periods, including shops and associated residences, hotels and other commercial and civic buildings, as well as a group of detached Edwardian residences at the north-eastern end. Together these buildings demonstrate the development of this 'High Street', particularly from the 1870s to c1920 when substantial growth along the boulevard occurred, and are illustrative of the enduring role the street has played in the economic and social life of North Fitzroy and Clifton Hill since the establishment of the suburbs [Criterion A].*
- *The highly intact streetscape of the Queens Parade Precinct clearly demonstrates the principal characteristics of a major turn of the century 'High Street' in the City of Yarra. Typical characteristics – including predominantly two-storeys Victorian and Edwardian wall heights, parapeted rendered or red brick facades with repetitive upper floor fenestration, and ground floor shopfronts – are displayed in the original forms, fabric and detailing of many of the buildings. The streetscapes on the northern side of Queens Parade from Delbridge Street to 410 Queens Parade and on the southern side of Queens Parade from Wellington Street to 169 Queens Parade are particularly intact and are highly consistent in their architectural form and expression, forming a cohesive and significant streetscape. A group of detached houses at the north-eastern end of the precinct are intact and consistent in their Edwardian character and form [Criterion D].*
- *The Queens Parade Precinct contains a number of landmark and individually significant buildings which are well considered and carefully detailed examples of commercial and civic buildings. These include, among others, the prominent three-storeys Former London Chartered Bank of Australia Ltd (later ANZ Bank building), 370-374 Queens Parade, the former Daintons Family Hotel (139 Queens Parade, built 1875), the former Melbourne Savings Bank (97-99 Queens Parade, built 1889), former National Bank of Australasia (270 Queens Parade, built 1886), Ryans Building (314 Queens Parade, built 1884), Campi's Buildings (149-167 Queens Parade, built c1880-c1890) and the corner shop at 336-338 Queens Parade, North Fitzroy (c1870-c1890). Key views to the local landmarks of the St John the Baptist Church complex and the Former United Kingdom Hotel, 199 Queens Parade, which bookend the precinct are afforded from within the precinct [Criterion E].*

North Fitzroy Precinct (HO327)

HO327 covers the northern section of the Study Area. The statement of significance outlines the following:

- *For the clear indication of important early land development phases in the suburb as follows:*
  - *East-west, relatively long and narrow streets originating from private subdivision of the 1839 agricultural allotments that originally fronted Merri Creek;*
  - *The evident effect of the 19th century Boom era when aggressive subdivision coincided with provision of public transport, creating dense late-Victorian development near the Nicholson St cable tram terminus;*
  - *The more generous post-1906 subdivisions (suiting the construction of detached timber villas) representing changed values in land prices, and the relatively new suburban ideal of affordable house and garden lifestyle; and*
  - *The slow transformation into a completed residential suburb by WW2, with the last few vacant sites accommodating inter-war dwellings, some developed with medium-density housing, duplexes and flats.*
- *For the consistent and intact streetscapes of late Victorian terrace and row housing in the south-west corner of the area, and Italianate style and Edwardian timber villas, interspersed occasionally with inter-war houses, in the balance of the area;*
- *For the visual consistency and good period expression of streets in the north and east parts of the area, consistently one-storeys, timber construction, decorative detail, and detached siting with consistent garden setbacks;*
- *For the interwar medium density development pockets, including those contained within Pilkington St and parts of Barkly St, King and Miller Streets, significant for its intactness, form and high quality design.*
- *For the shopping strip streetscape of St Georges Rd, significant for its landmark Victorian-era buildings, plus intact early 20th century and interwar buildings in both Classical Revival and Moderne styles; and*
- *For important landscape and vistas, including the view of Merri Creek Bridge framed by palms and mature elms in the Merri Primary School reserve, and mature street planting of plane trees.*

Clifton Hill Western Precinct (HO317)

The southern section of the Study Area is protected by HO317. The statement of significance outlines the following:

- *As an expression, in the history of its creation as part of Collingwood East, of the aspirations of tradesmen of the Collingwood Slope for improved urban development and the provision of public facilities;*
- *For the role of local politicians and business figures in social, physical and industrial improvement and home ownership in the area;*
- *As the focus of community sentiment and resident protest from its initial development in the 1860s, from the actions of the Victorian-era Vigilance Committee to the preservation movements of the 20th Century, specifically the House of the Gentle Bunyip conservation campaign;*
- *For the distinctive street and allotment layout created under Government surveyor, Clement Hodgkinson (43), and the liberal allowance for garden squares and reserves for a market, public baths, a school, a recreation reserve and five churches, contrasting with private surveys in the City of Yarra;*
- *For Hoddle's provision of boulevards (Hoddle Street and Queen's Parade) skilfully merged with wide streets on a north-south grid, with unusual five way intersections and triangular public spaces at each end of Queen's Parade;*
- *For the unique series of vistas from the crest of the hill at the end of Hoddle street that includes views of significant landmarks like the shot tower, spires of St. Johns and the former Wesleyan Church, Town Hall towers at Collingwood and Fitzroy, the Exhibition Buildings, and long vistas to the Kew hill and Studley Park, plus those to and from the Darling Gardens;*
- *For the area's fine representation of Victorian-era life, as expressed by the street and public reserves, along with the dominantly Victorian-era building stock including churches, a State school, factories, early houses, a large number of late Victorian terrace houses, attached and free standing, and key house designs such as 12 North Terrace.*
- *For its focus on a representative and intact example of a residential garden square (Darling Gardens) based on British and European precedents, with associated significant trees;*
- *For the mature, largely exotic trees, including those planted by George David Langridge, MLA and former Mayor, bluestone kerbs, gutters, drains, channelling, and laneways from the area's major period of development;*
- *For the strong industrial theme, seen in the south of the area with several 19th Century industrial buildings including former boot factories, and a carriage factory, that links the area with the City's other creek and riverside industrial localities such as Clifton Hill East, Abbotsford and Collingwood;*
- *For well preserved Edwardian housing estates and individual houses as complements to the dominant Victorian-era residences; and*
- *For good examples of the Californian Bungalow style and other well preserved interwar houses.*

Attachment 3 - Amendment C231 - Hansen Built Form Review

Contextual Analysis

Heritage Attributes

Landmarks & Tall Structures

The City of Yarra places great importance on its designated landmarks and tall structures. The MSS, (reinforced by Local Policies of Clauses 21.05 and 22.03) seek to **retain the prominence of its landmarks and key visual links.**

In the context of the Study Area, the following municipal landmarks and tall structures are identified:

- St John's Church spire;
- Alexandra Parade Shot Tower;
- Hungarian Reformed Church spire; and
- St Patricks Cathedral (City of Melbourne).

The following local landmarks are also identified within the Study Area:

- Former ANZ Bank, 370 Queens Parade, Clifton Hill (HO221, VHR H0892),
- Former United Kingdom Hotel, 199 Queens Parade, Clifton Hill (HO92, VHR H0684), and
- St John's Church complex (part of HO330).

Views to landmarks and tall structures

While Clause 22.03 identifies the visual protection 'spire' of the St John's Church only, the heritage analysis finds it is appropriate that views of the belfry and spire in combination should be considered (refer to diagram A).

In addition to the belfry and spire of St John's Church, GJM's heritage assessment also considered that views to the State-significant **former ANZ Bank** at 370-374 Queens Parade are important within the context of the Activity Centre. Its landmark quality is derived from its tall three-storeys scale within a lower context, the slate roofs and corner tower, the tall chimneys and the majority of the upper floor level that visually anchors the chimneys and roofs (refer to diagram B).

The Landscape & Streetscape Chapter of Contextual Analysis on page 18 have identified locations where views to these landmark buildings are available from.

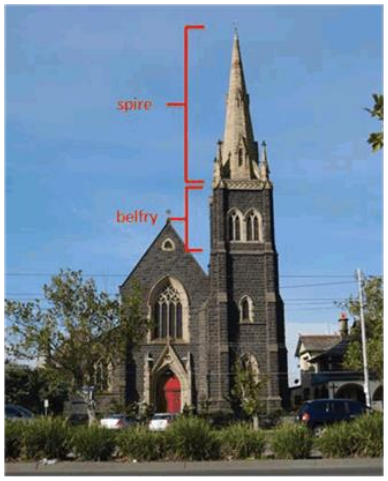

Diagram A (source: GJM Heritage)

Diagram B (source: GJM Heritage)

Significant Streetscape

GJM's heritage assessment (refer to Queens Parade Built Form Heritage Analysis & Recommendation - GJM Heritage) identifies parts of Queens Parade within the NAC to be of 'significant' streetscape value:

- The northern side of Queens Parade between Delbridge Street and Michael Street;
- The northern side of Queens Parade between Michael Street and number 410 Queens Parade;
- The southern side of Queens Parade between Wellington Street and Gold Street; and
- The southern side of Queens Parade between Gold Street and number 167 Queens Parade (see Figure 16).

These streetscapes demonstrate the following characteristics as a 'significant streetscape':

- Consist predominantly of buildings graded 'contributory' or 'individually significant' (approximately 90%) with few 'not-contributory' buildings or intrusive developments.
- Have a high degree of consistency in terms of their:
  - Overall scale;
  - Zero street setback;
  - Predominantly (Victorian) two-storeys street wall height and
  - Predominantly Victorian and early Edwardian architectural form and style.
- Date predominantly from the Victorian and early Edwardian period with few later infill developments.
- Demonstrate a high level of intactness to their turn of the century period of construction.
- Are recognised for their aesthetic significance in the existing Statement of Significance for HO330 as follows: *For the late 19th and early 20th century shops, hotels, banks, and public buildings, often with intact street verandahs, lining both sides of a wide boulevard that provides for exceptional vistas to fine commercial rows, and demonstrates the major role of Queens Parade as a commercial, banking, accommodation and public transport nucleus for the district.*
- While in some parts of Queens Parade the buildings are not considered to create a significant streetscape, the mature double avenue of trees planted throughout the wide Queens Parade boulevard are significant (HO93) and new development should be scaled to avoid diminishing the boulevard quality of Queens Parade.

Attachment 3 - Amendment C231 - Hansen Built Form Review

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Contextual Analysis

Heritage Attributes

Heritage Grading

A number of anomalies exist with regards to the designation of heritage overlay and grading within the Study Area. Heritage review (desktop and fieldworks) have identified and sought to rectify these anomalies and a summary of the proposed changes are reflected in Table 1: Proposed Heritage Overlay Changes (page 11) and Figure 6 (page16). These grading are categorised into:

Individually Significant

- Most individually significant buildings are clustered at and around the junction of Queens Parade and Delbridge Street/ Wellington Street.
- Most individually significant buildings occupy corner sites, or are visually exposed in the open streetscape setting.
- Most individually significant buildings are located at the eastern end of the Queens Parade.
- Some of the identified individually significant buildings are also listed within the Victorian Heritage Register (VHR) and may require site specific built form responses.

Contributory

- Most buildings along Queens Parade are categorised as having contributory heritage value.
- They are often infill in nature, or parts of a collection of buildings, with varied potential to accommodate future development.

Non- Contributory

- Most non-contributory buildings are concentrated within the south-western portion of the Study Area, characterised by heavy modification through recent redevelopment.
- These sites have a greater opportunity for future infill development.

Common Heritage Attributes

The following characteristics and elements are commonly found in significant, or contributory heritage forms in the Study Area and should be retained:

- Attached or terraced construction with no side yards;
- Parapeted front facades with some pitched and hipped roofs;
- Varied setbacks from the street frontage for residential forms;
- No setback from Queens Parade for commercial forms; and
- Visible chimneys to single and double storey buildings (often located within half the depth of the front room of existing buildings).

Local Landmark: Former Clifton Motors (currently being considered for inclusion in the VHR)

Local Landmark: ANZ Bank (HO221)

Municipal & Local Landmark: St John's Church Complex & Spire (part of HO330)

Local Landmark: Former United Kingdom Hotel (HO92)

Attachment 3 - Amendment C231 - Hansen Built Form Review

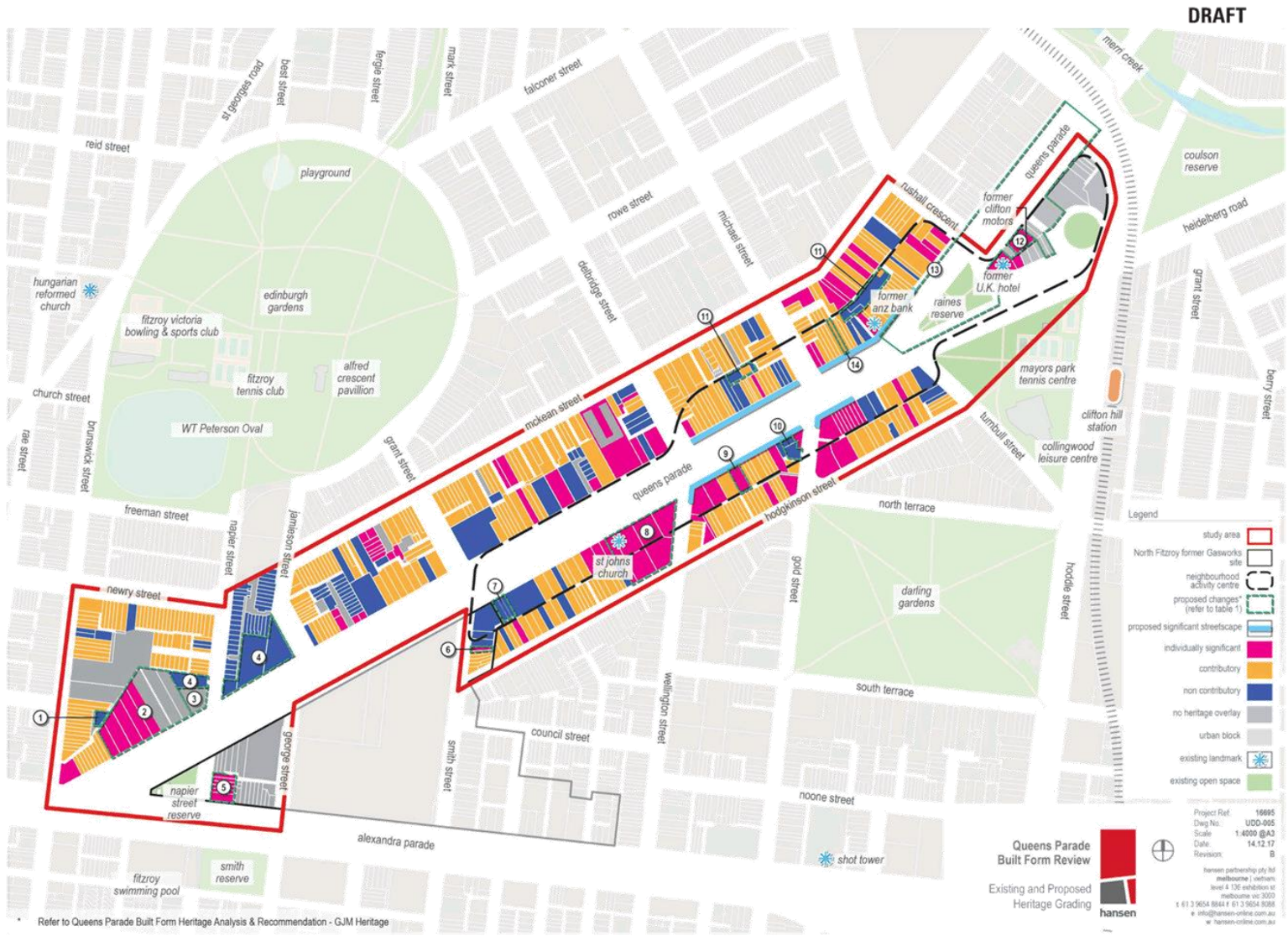

Figure 6: Existing and Proposed Heritage Grading Map (Refer to Table 1)

Attachment 3 - Amendment C231 - Hansen Built Form Review

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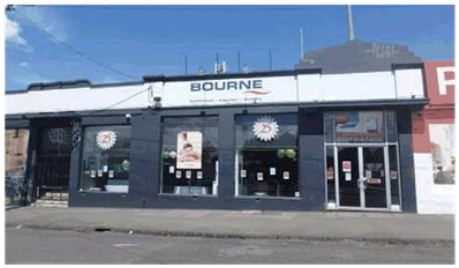
Contextual Analysis
Heritage Attributes

Examples of individually significant, corner heritage buildings

Examples of contributory heritage buildings



Examples of non- contributory/ non- heritage buildings

Built Form Implications

Heritage gradings of specific sites or precincts will have implications for future proposed built form. Critical observations are:

- Heritage significance may restrict or influence future development outcomes both on heritage listed sites and adjacent land.
- Varied development opportunities exist behind heritage forms while retaining visual dominance of the existing heritage fabrics. Whilst the ambition is not about visually 'concealing' new addition, a balance between heritage retention and creation of a reasonable development envelope through enabling varying degree of 'visibility' of new built forms above is influenced by the road widths and streetscape effects. In built form terms, the desired 'visibility' measure can be managed through appropriate building heights and setbacks.
- Potential future character in areas which is identified as a 'significant streetscape' should reflect the consistent existing streetscape with new built form constructed to the street boundary. Future street wall should be influenced by the predominantly two (Victorian - era) storeys.
- Landmarks and tall structures within both the immediate and wider context influence the experience of Queens Parade. The protection of important view lines must be taken into consideration through upper level setback and street wall definitions including:
  - The importance of views to the St John's Church belfry and spire against the sky; and
  - The importance of views to key features of local landmarks such as the former ANZ Bank and former UK Hotel
- New development should be scaled to avoid diminishing the boulevard quality of Queens Parade.
- Future infill facades should respect the materiality and relationship between solid and void established by the 'contributory' and 'individually significant' buildings.
- New development should also provide a sensitive transition to the predominantly single-storeys Victorian-era character of Hodgkinson Street to the south and the one and two-storeys Victorian-era character of McKean Street to the north.

Prospective key elements influencing built form framework:

- Retain the valued attributes of heritage buildings (i.e. facade, setbacks, verandahs, chimneys, etc).
- Retain view lines to landmarks, including the St John's Spire and Tower, former ANZ Bank and former UK Hotel.
- The proportion of visibility of upper levels can be managed by applying varied upper level to heritage street wall principle.
- Future development should respond to individually significant heritage buildings.

Attachment 3 - Amendment C231 - Hansen Built Form Review

DRAFT

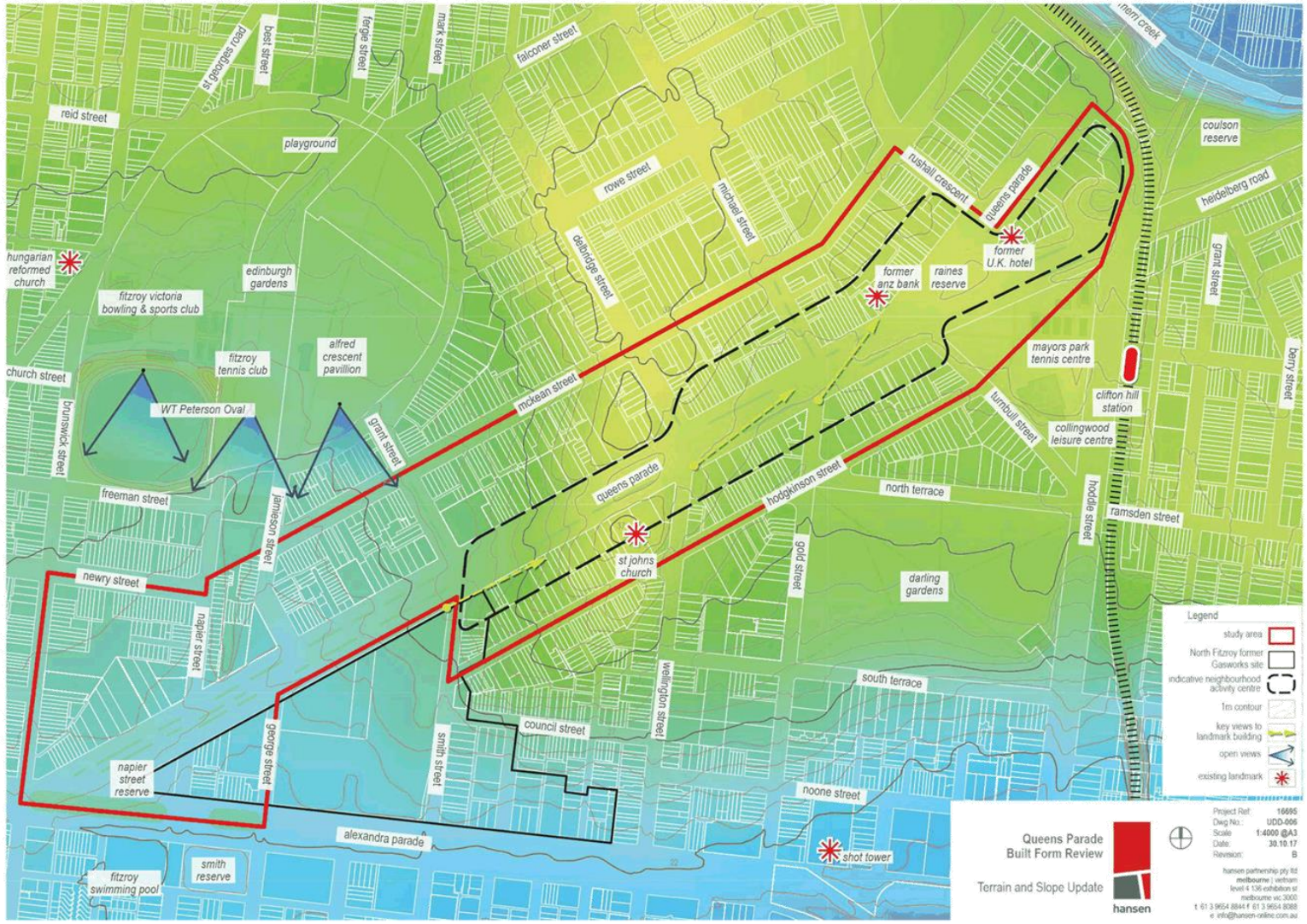


Figure 7: Terrain and slope

Attachment 3 - Amendment C231 - Hansen Built Form Review

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Contextual Analysis
Landscape & Streetscape

Terrain and Slope

- The experience of Queens Parade is significantly influenced by topography and the notable 'hill' located east of Delbridge and Wellington Streets, as well as the escarpment to the Merri Creek Reserve to the north-east.
- To the west and south, slope is less pronounced with the south side of Alexandra Parade in a relatively flat profile.
- The 'Delbridge Hill' offers long range views to the west and south, particularly along Queens Parade and north-south aligned streets.
- The topography of the precinct is critical to an understanding of Yarra's important skyline, comprising designated landmarks, traditional roofscape, notable open spaces and boulevards and a broad panorama to Central Melbourne's skyline.

Key Open Spaces

- A number of key open spaces are identified within and around the Study Area and the protection of these key open spaces is critical in shaping future urban form, including:
  - Edinburgh Gardens (HO213),
  - Darling Gardens (HO213),
  - Mayors Park,
  - Raines Reserve,
  - Napier Street Reserve (west of Napier Street), and
  - Smith Reserve (associated with the Fitzroy Swimming Pool).
- Existing canopy vegetation within the streetscapes and central median also offer high amenity and contribute substantially to the landscape character of Clifton Hill and its surrounds. As a significant streetscape feature such boulevard trees should be acknowledged and protected. These include:
  - Queens Parade Street Trees (between Brunswick Street and Delbridge Street – HO93).
  - Central median of Alexandra Parade (between Brunswick Street and Hoddle Street).

Statement of Significance

Edinburgh Gardens- HO213

- *Edinburgh Gardens is locally significant as a major recreational facility within the Fitzroy area. The site is intrinsically linked to a number of local sporting groups and the Fitzroy community generally. It contains elements that illustrate the early use of the site, particularly the railway, and contributes significantly to the heritage character and general amenity of the local neighbourhood, particularly as a setting for adjacent significant housing which culminates in a landscape of notable grandeur within the City of Yarra. The site is also significant for the notable numbers of mature trees, one of which is recorded by the National Trust of Australia (Victoria).*

Queens Parade Street Trees (central median of Queens Parade, between Brunswick Street and Grant Street) – HO93

- *The double avenue of trees on Queens Parade is locally significant as an extensive and intact avenue planting in the City of Yarra. The trees provide a significant contribution to the heritage character of the precinct, and to the amenity of the local neighbourhood generally.*

Darling Gardens – HO213

- *Darling Gardens are locally significant as an early and important recreational facility within the Collingwood and Clifton Hill area. The site has strong associations with the local community, at various times the home to sporting groups and the focus of community activities. The site effectively illustrates its ongoing development since the 1860s, and contains two trees recorded on the National Trust of Australia (Victoria) Significant Tree Register.*

Napier Street Reserve, Fitzroy North

Edinburgh Gardens, North Fitzroy

Alexandra Parade central median

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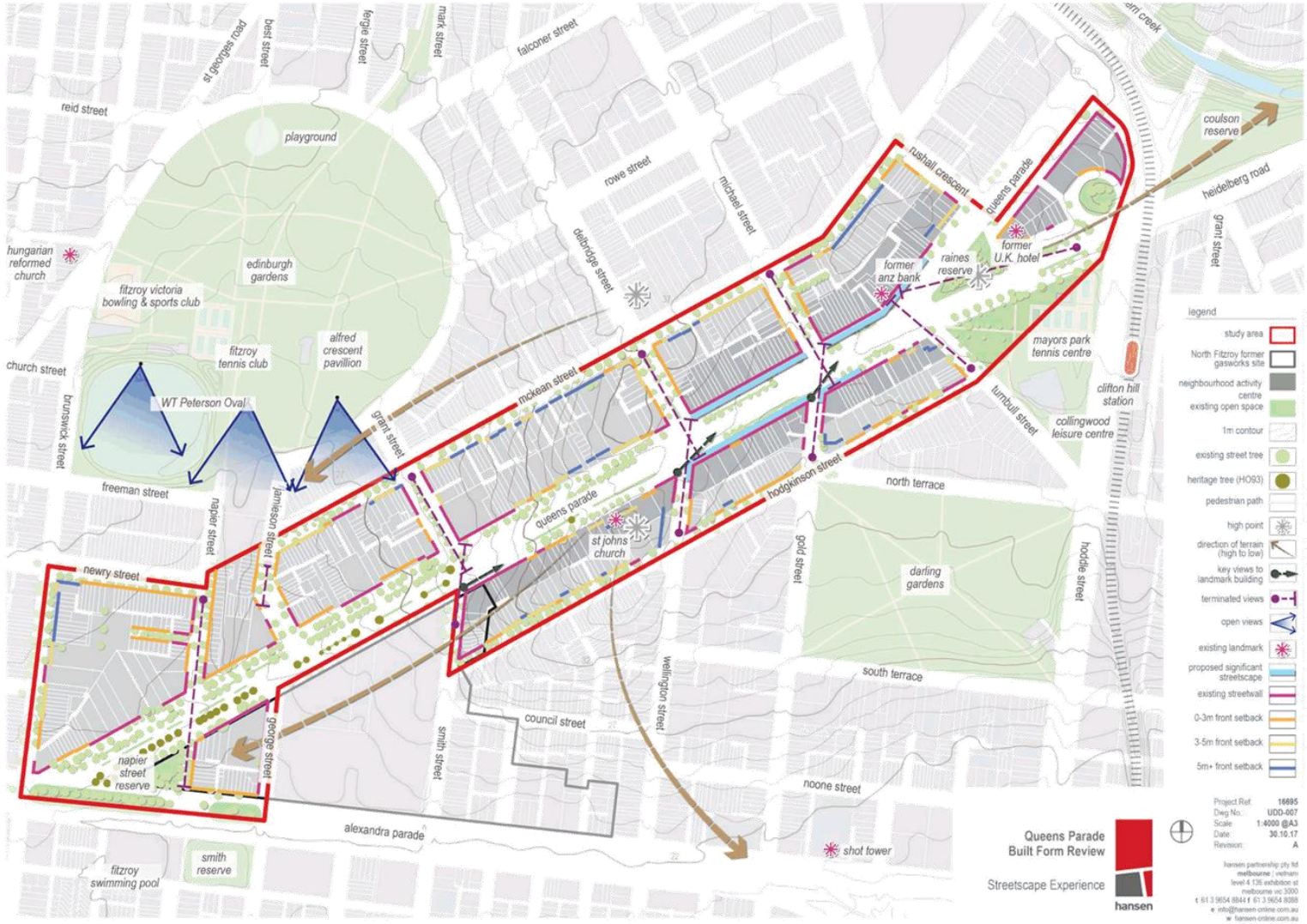

Figure 8: Streetscape Experience Map

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Contextual Analysis

Landscape & Streetscape

Streetscape Experience

- Queens Parade (west of Grant and Smith Streets) is characterised by the double avenue street trees including the heritage listed Dutch Elms, framing views along Queens Parade between Brunswick Street and Smith Street.
- The full width of Queens Parade (60m) and views to the St John's Church spire can be appreciated west of Grant and Smith Streets, where street trees are mainly accommodated within wide street verges on both sides.
- The shift in subdivision pattern and urban block arrangement from south to north means that view lines along local streets often terminate at notable corner forms.
- A higher degree of consistent and attached traditional street walls of predominantly 2 storeys define the street edges of Queens Parade in the Neighbourhood Activity Centre, between Delbridge and Turnbull Streets.
- At its north-eastern end, the Study Area is highly visible from multiple vantage points, including Hoddle Street and from the Heidelberg Road overpass.
- Residential streets that frame the Study Area boundary are typically characterised by low scale (1-2) heritage forms with consistent front gardens.
- At a macro scale level, long range views of the CBD skyline are available from a series of locations within the Edinburgh Gardens.
- Views to a number of municipal and local landmarks can be appreciated from within the Study Area. However, the contextual subdivision pattern, existing vegetation and urban block arrangement (including road width) tend to influence management of these views.
- Key oblique views of **St John's Church** belfry and spire from the west are provided from the north-western intersection of Queens Parade and Grant Street and from the south-western side of Queens Parade and Smith Street. These views are where the belfry and spire of St John's Church become a prominent landmark element within the streetscape. Whilst view from the intersection of Queens Parade and Grant Street is protected by the road reserve (similar to the view directly opposite from across Queens Parade).
- Some of the key views of the belfry and spire of St John's Church are from within the Activity Centre and from Raines Reserve looking south. Views to the local landmark of the state-significant former ANZ Bank at 370 Queens Parade, are also afforded from within the Activity Centre from Raines Reserve.
- Key views of the **former ANZ Bank** are those of the tower, roof, chimneys and upper floor where the three-storeys form, roof and chimneys of the former ANZ Bank building becomes a prominent landmark within the streetscape. These views are provided from the south-eastern intersection of Queens Parade and Gold Street; from the south-western intersection of Queens Parade and Turnbull Street; from the western end of in Raines Reserve; and from the pedestrian traffic islands at the southwest end of Raines Reserve.
- The **former United Kingdom Hotel** occupies a prominent location at the junction of Queens Parade and Dummett Crescent/Heidelberg Road and key views are provided of the hotel from these streets and from within Raines Reserve. Oblique views of the former United Kingdom Hotel (VHR H0892) are provided from the northern end of the Activity Centre, but these are protected by the road reserve and do not require management through additional built form controls.
- Whilst a distant view of the State-significant **Shot Tower** on Alexandra Parade is available from within the Queens Parade Activity Centre – and particularly at the intersection of Queens Parade and Delbridge/Gold Street, this "glimpse" of the Shot Tower is not considered to be a key view and is not one that requires management through built form controls within the Queens Parade Study Area.

View of St John's Church tower from the intersection of Smith Street

View of St John's Church tower from pedestrian traffic island on the south side of Raines Reserve

View of the former ANZ Bank from the intersection of Gold Street

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Contextual Analysis

Landscape & Streetscape

Built Form Implications

Critical observations and built form implications include:

- Respect the landform and urban structure by retaining the primacy of key landmark forms (St John's Baptist spire, the former ANZ Bank and the former UK Hotel).
- The coexistence of designated landmarks, heritage forms that project into the skyline and the silhouette of traditional commercial and residential neighbourhoods in North Fitzroy and Clifton Hill are important character elements.
- The visual dominance of the street trees is important to maintain a 'boulevard' effect and a unifying character to the south-west despite the varied building stock.
- Views to key landmarks that are visually exposed should be managed carefully. It is worth noting that these view lines are not always experienced from the centre of the street, but rather more often in a dynamic mode from footpaths to the traditional streetscape.
- The oblique views along Queens Parade (within the NAC) should continue to be defined by heritage street walls.
- Protect and preserve the intact heritage residential streetscapes along Newry and McKean Streets, including careful management of urban form within the Queens Parade corridor to the south.
- Retain the open profile of attractive, long range views to the Central City in particular from Edinburgh Gardens - looking south.
- Avoid overshadowing of footpaths, key open spaces and the central median of Queens and Alexandra Parade to ensure their long term viability and their image and amenity for existing and future residents.
- Built form controls will be required along the south side of Queens Parade to protect the key view of the belfry and spire of St John's Church from the intersection of Queens Parade and Smith Street.

Prospective key elements influencing built form framework:

- Reinforce the prevailing traditional 2 storeys street wall profile of the NAC along Queens Parade.
- Retain key view lines to landmarks including:
  - St John's belfry and spire from Queens Parade & Smith Street junction.
  - Former ANZ Building from Queens Parade & Wellington Street junction.
  - Former ANZ Building from Queens Parade & Gold Street junction.
- Retain visual dominance of the double avenue of trees along the south western part of Queens Parade and the sense of 'openness' elsewhere.
- Respect the aspect/ outlook from heritage streetscapes in residential areas beyond the Study Area.
- Shape prospective redevelopment to avoid overshadowing or amenity impact on pedestrian space/ open space.

View to St John's Church from the Delbridge Street intersection

Double avenue trees along Queens Parade

Attachment 3 - Amendment C231 - Hansen Built Form Review

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Contextual Analysis

Subdivision Pattern

The Study Area comprises varied allotment sizes which results in development of diverse building types and land uses. The subdivision pattern is based on a historical grid configuration, orientated on a diagonal alignment in the 'wedge' between Queens Parade and Brunswick Street. The grid shifts to the south of Queens Parade to a more conventional east- west alignment.

Typical Lot Sizes

Lots have generally been classified into sizes as shown on the plan (refer to Figure 9, pg 24) and described below:

- Small: less than 300sqm;
- Medium: 300 - 600sqm;
- Large: 600 – 1,200sqm; and
- Extra Large: greater than 1,200sqm.

Typical Frontage Widths

Frontage widths have generally been classified into sizes as shown on the plan (refer to Figure 10, pg. 25) and described below:

- Small: greater than 5m, less than 10m;
- Medium: greater than 10m, less than 20m;
- Large: greater than 20m.

Typical Allotment Depths

Allotment depths have generally been classified into sizes as shown on the plan (refer to Figure 11, pg 26) and described below:

- Small: less than 30m;
- Medium: 30-40m;
- Large: 41-55m; and
- Extra Large: greater than 55m.

Strategic Sites

The City of Yarra Planning Scheme identifies the following sites as Strategic Redevelopment Sites within the Study Area:

- 496- 500 Brunswick Street;
- 26 Queens Parade; and
- 501-513 Hoddle Street.

The NAC

- Within the NAC (east of Delbridge and Wellington Streets), the subdivision pattern typically has a long northwest – southwest grid pattern, perpendicular to Queens Parade.
- On the north side of Queens Parade, between Delbridge Street and Rushall Crescent, allotments are primarily of small and medium size (between 0 – 600sqm), associated with traditional shop fronts and occasional attached terraces. Lot widths are mainly within the 'Small' category (less than 10m) with varied lot depth between 20m to 50m. To the rear of properties fronting Queens Parade there is a number of wide and shallow configured commercially zoned 'mid-block' sites, accessible via rear laneways.
- On the south side of Queens Parade, between Wellington Street and Turnbull Street, the subdivision pattern is of a consistent depth (approximately 30-35m). There is also a greater number of sites with wider site frontage (greater than 10m).
- Corner allotments are typically medium or large (up to 1,200sqm) and typically accommodate retail forms as well as former and current civic/ institutional buildings.
- To the north of Dummett Crescent, between Queens Parade and Heidelberg Road, allotments are generally large (600-1,200sqm), with primary frontage to the main road and additional access from rear/ side laneways. Lot widths are large (greater than 20m) with an occasional width greater than its lot depth.
- Between Hoddle Street and Raines Reserve, this part of the NAC is characterised by 'medium' to 'large' allotments with medium to large allotment widths. Lot depths are generally shallow (less than 40m), often with a single street access off Queens Parade service road and Dunmett Crescent. However, recent lot consolidation has resulted in reconfigured allotment patterns, as demonstrated at 205-211 Queens Parade & 6-12 Dummett Crescent and 2478 249-251 Queens Parade, yielding allotment deeper, larger allotments with multiple street frontages.

Between Brunswick Street and Smith/ Grant Streets

- Along Brunswick Street, the subdivision pattern typically has a long east -west orientation with parallel laneway access. Allotments are typically 'small' (less than 300sqm) except for two extra- large sites identified in this Review as the 'Aquila Development' (496-500 Brunswick Street) and the 'Office of Housing' site (502 Brunswick Street).
- Properties fronting Queens Parade can be categorised as fine grained small to medium sized residential lots (0- 600sqm), or large and extra-large allotments associated with recent medium density residential development, or industrial remnant sites including the North Fitzroy Gasworks Precinct which anchor the south-western portion of the Study Area.

Residential Edges

- Properties within the NAC (between Delbridge Street and Turnbull Street) have direct abuttal to residential land to the rear (NRZ within Heritage Overlay). There are instances where mid-block sites (commercially zoned) behind the NAC, providing an additional 'buffer' to its residential edge.
- Rear laneways (approximately 3m) provide physical separation between land fronting Queens Parade and its rear residential interface. However, these laneways are often not continuous.
- Along Newry, McKean and Hodgkinson Streets, residential allotments are generally 'small' to 'medium' in size (below 600sqm) with occasional parallel laneway access from the rear. Lots consolidation and subdivision are generally limited to residential lots between Napier and Delbridge Streets. Private open space for properties fronting these streets are often accommodated within the rear garden of varying sizes and configuration.
- Residential lots fronting local side streets are typically orientated east -west, typically separated from the commercial spine by existing laneways. Allotments are typically small (less than 300sqm). Interface to existing laneway are often presented with side elevation with, or without habitable windows.

Existing side elevation of 338 Queens Parade (viewed from Michael Street)

Attachment 3 - Amendment C231 - Hansen Built Form Review

Figure 9: Lot Sizes Map

Attachment 3 - Amendment C231 - Hansen Built Form Review

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Figure 10: Lot Widths Map

Attachment 3 - Amendment C231 - Hansen Built Form Review

Figure 11: Lot Depths Map

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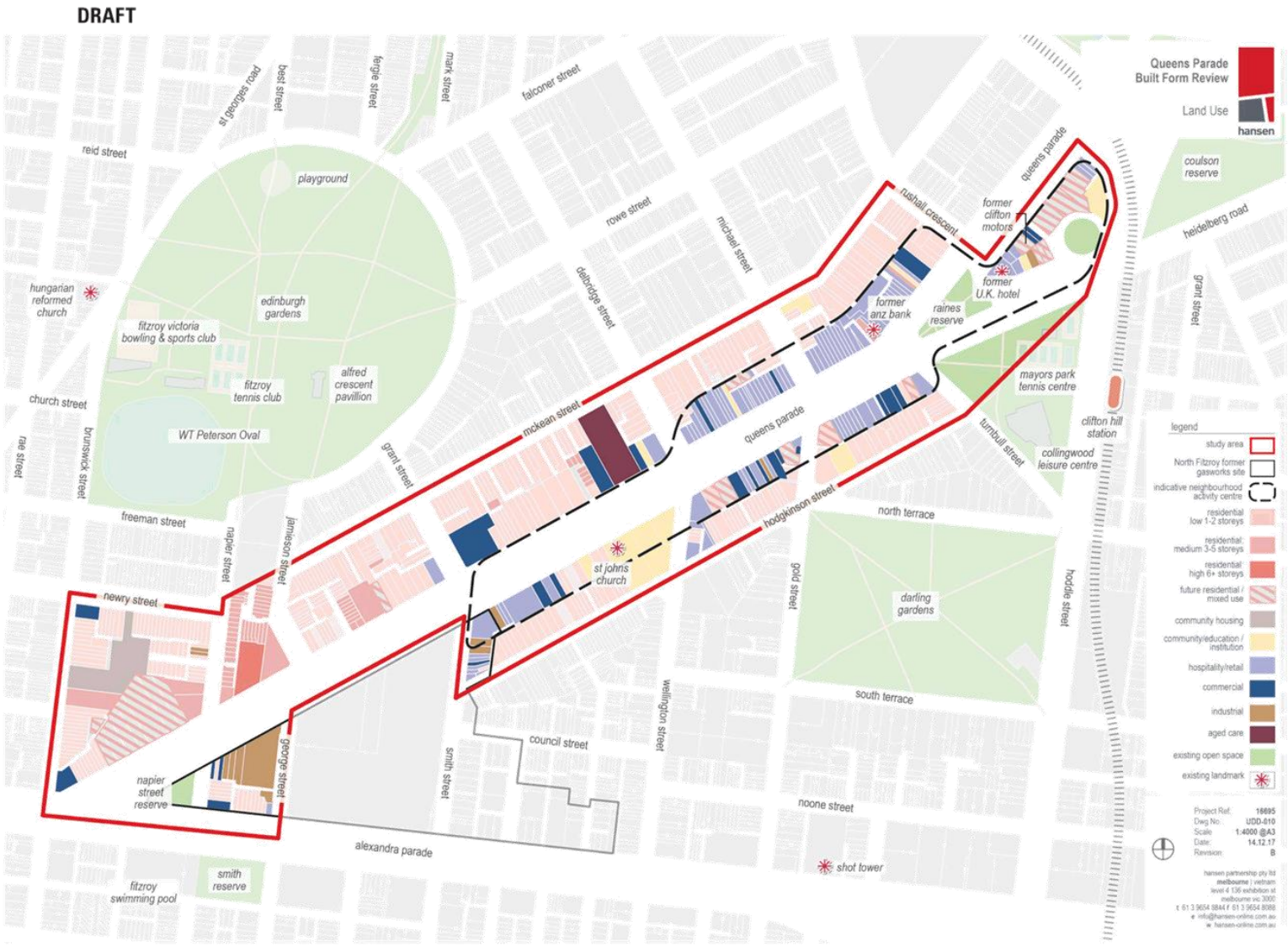

Figure 12: Land Use Map

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Contextual Analysis
Subdivision Pattern

Existing laneway interface along Koormiel Lane (middle section)

Existing laneway interface along Koormiel Lane and Michael St junction

Existing side elevation of 662 Smith St (viewed from Hodgkinson St)

Built Form Implications

Critical observations and built form implications include:

- Large lots (determined by width and depth) are often more able to accommodate greater development opportunity as the off- site impacts can be minimised and managed. There is increased flexibility for wider sites to accommodate varied development footprints and to minimise excessive visual bulk from sensitive interfaces.
- Greater development potential can be accommodated on nominated strategic redevelopment sites, or 'Large' and 'Extra Large' sites (greater than 600sqm), where there is limited (or absence) of heritage and residential sensitivities.
- Moderated development potential on 'Small' allotments (less than 300sqm) with existing low rise (1-2 storey) heritage facade and direct abuttal to residential hinterland (NRZ).
- Within fine grained allotments, increased development opportunity exists for sites with 'Large' and 'Extra Large' lot depth (greater than 40m) as feasible floor plate can be accommodated behind upper level setbacks.
- For 'Small' site frontages (less than 10m), greater development potential can be achieved through lot consolidation.
- Typically, fine grain sites have limited capacity to accommodate side setbacks on one, or both sides. Party wall construction along common side boundaries is generally acceptable in this context.
- On corner sites (with heritage attribute), where side setback is often required along the primary and secondary street frontages, side setback requirement can possibly affect feasibility of the site to be redeveloped without consolidation.
- There is greater development potential for commercial properties without a direct residential interface. Such separation is provided by existing laneways, or mid-block allotments (in commercial zone) where reduced setbacks can be contemplated.
- Allotments with access to side/ rear laneways have greater opportunity to accommodate separated vehicular access with minimal, or no impact on the street frontage.
- Maximising ground level activation and ground level presentation to establish an inviting and high amenity urban environment while retaining most of the heritage fabric is a key influence in shaping the future urban form along Queens Parade.

Prospective key elements influencing built form framework:

- Optimise development potential on larger sites fronting the Boulevard, where the constraints of heritage/ residential sensitivities are limited.
- Encourage moderate development on smaller sites which may have a direct interface with residential land, or are affected by heritage conditions.
- Support greater development potential where lot consolidation, or amalgamation can be achieved, particularly in locations affected by fine subdivision grain.
- Recognise the limitations of narrow and shallow (less than 30m) lots in achieving upper level setback form, given minimium apartment (dwelling) dimensions defined in the Better Apartment Design Standards (BADs, Clause 58).
- Manage the profile of new development to avoid adverse impact to surrounding residential areas.

Attachment 3 - Amendment C231 - Hansen Built Form Review

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Contextual Analysis

Building Heights & Recent Development

- The NAC is generally characterised by low rise traditional fabric with larger form located on corner sites. The St John's Church spire represents the tallest element (at approximately 44.5m) along the Queens Parade spine. The Queens Parade frontages are generally 1-2 storeys Victorian, Edwardian and later 19th to early 20th century buildings. Typically existing heritage buildings within the NAC are defined by parapet heights of 8-11m, usually the equivalent of 3 contemporary storeys.
- Corner forms along Queens Parade are particularly important for their heritage contribution to the streetscape/ precinct. Given the diagonal alignment of Queens Parade and perpendicular angled junctions with adjoining streets, corner sites should continue to perform a role of infill element in the broader streetscape.
- The north-eastern end of Queens Parade (NAC) has a greater concentration of early to mid-20th century single and double storeys buildings, including the Former U.K. Hotel (199 Queens Parade) and Clifton Motors Building (205-211 Queens Parade), both notable Art Deco buildings which are indicated, or currently being considered for inclusion into the VHR.
- Existing contemporary forms comprise medium density residential buildings in the form of attached townhouses and apartment (3- 6 storeys), and are primarily clustered around Napier, Jamieson and Grant Streets.
- Recently planning applications for development along Queens Parade are for taller and more intense building forms. Planning permits have been issued for buildings of up to 10 storeys that are currently under construction (VCAT approval) at 217-241 Queens Parade and 11 storeys (VCAT approval) at 243-247 Queens Parade. More recently, an application for 17 storeys at 249-265 Queens Parade has received Council's approval with a height reduction at 14 storeys. These approvals are concentrated in location where there is an absence of existing heritage and residential sensitivity, noting large allotment sizes.
- To date, there are limited number of permit applications within the NAC, possibly due to small and narrow allotments, access constraints in absence of continuous rear laneways and concentration of heritage buildings. Furthermore, there are no development applications currently for residential allotments fronting McKean, Newry and Hodgkinson Streets.
- The broader area includes an eclectic mix of late 19th century through to mid-20th century residential, commercial and civic forms with some recent medium density residential infill. Pronounced skyline elements represented by the Shot Tower along Alexandra Parade, public housing high rise (20 storeys) further south east, Collingwood silo structures and developments (including the Porsche Sign Silos, Islington Silos and Northumberland Street silos). These latter industrial complexes range between 9-11 storeys in height and (by virtue of their proportion) have a strong sense of verticality set against their low rise context.

249-265 Queens Parade
(Council's approval at 14 storeys)

243-247 Queens Parade - under construction
11 storeys (approved-VCAT)

217-241 Queens Parade- under construction
10 storeys (approved-VCAT)

205-211 Queens Parade & 6-12 Dummett Crescent
9-11 storeys (planning assessment)

101-103 Queens Parade
5 storeys (Council's approval)

193 Queens Parade
5 storeys (Planning assessment)

26-56 Queens Parade
10 storeys (awaiting VCAT decision)

Rear 26-52 Queens Parade
4 storeys (awaiting VCAT decision)

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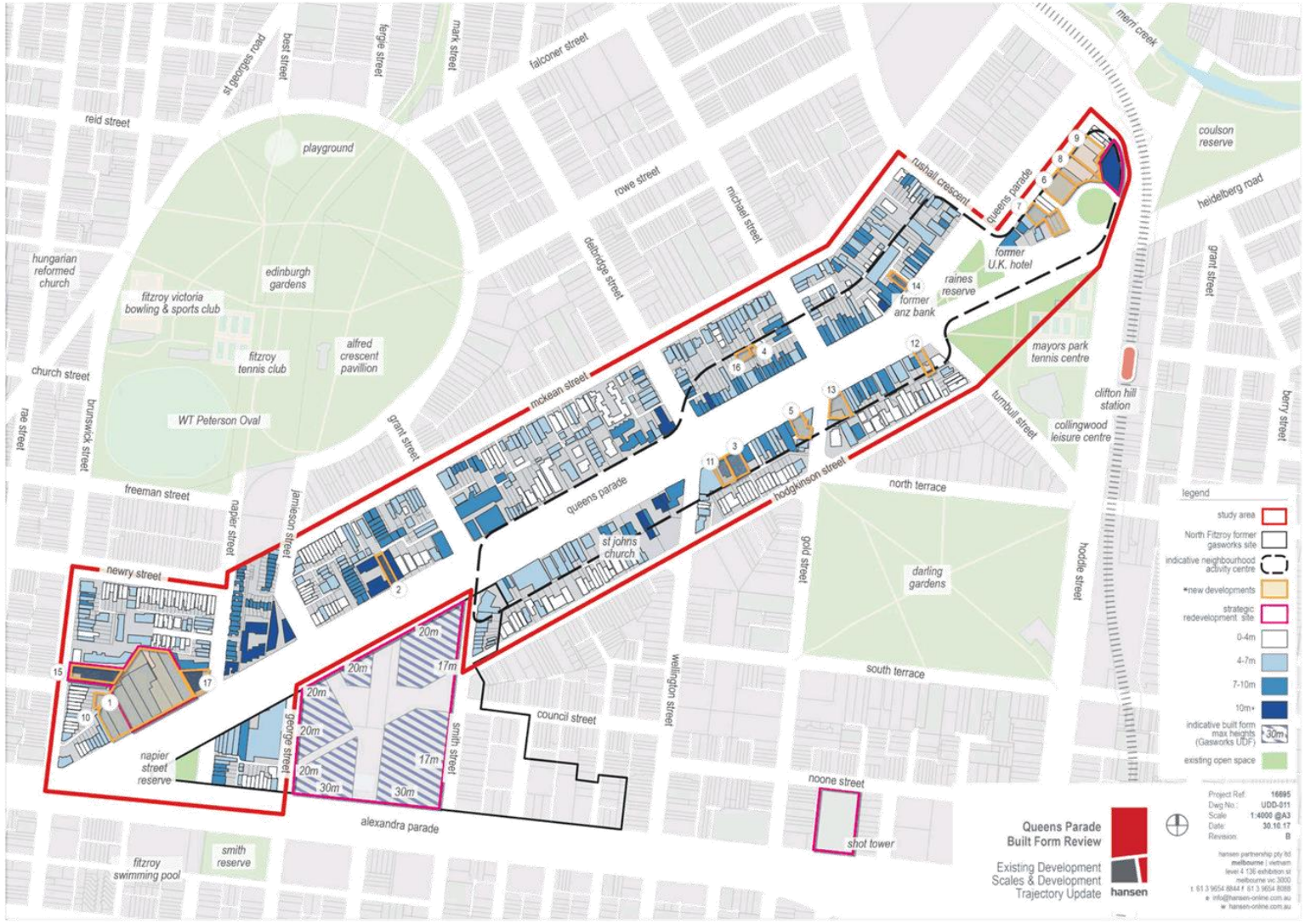

*Figure 13: Building Heights & Development Trajectory Map *Refer to Table 2*

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Contextual Analysis

Building Heights & Recent Development

Table 2: Recent Development Approvals/ Proposals (refer to Figure 13)

Map No.	Application No.	Address	Proposed Development	Current Status
1	PLN16/0434	26-52 Queens Parade	10 storeys (31m)	Interim Order issued by VCAT, awaiting VCAT decision
2	PLN11/0906	142-146 Queens Parade	4 storeys	Approved
3	P1913/2017	101-103 Queens Parade	5 storeys (21.82m)	Planning Application
4	PLN16/0078	312 Queens Parade	3 storeys	Approved
5	PLN14/0209	137 -139 Queens Parade	4 storeys	Approved
6	PLN15/1101 PLN14/0989-06	217-241 Queens Parade	10 storeys (35.5m)	Under Construction
7	PLN16/0923	205-211 Queens Parade & 6-12 Dummett Crescent	9 and 11 storeys	Planning Application
8	PLN14/0311	243-247 Queens Parade	8 and 11 storeys (40.5m)	Under Construction
9	PLN16/0494	249-265 Queens Parade	17 storeys	Approved at 14 storeys
10	PLN16/0732	Rear 26-56 Queens Parade	4 storeys	Planning Application
11	PLN16/1097	97-99 Queens Parade	3 storeys (10.77m)	Planning Application
12	PLN16/1209	193 Queens Parade	5 storeys	Planning Application
13	PLN17/0718	141-147 Queens Parade	4 storeys	Planning Application
14	PLN17/0705	388-390 Queens Parade	5 storeys	Planning Application
15	PLN10/1057	496-500 Brunswick Street	5 storeys	Completed
16	PLN16/1201	Rear 304-308 Queens Parade	4 storeys	Planning Application
17	PLN160732	58-72 Queens Parade	5-6 storeys	Completed

Recent VCAT Proceedings

QPF Developments Pty Ltd v Yarra CC & Ors [2011] VCAT 1572 :
58-72 Queens Parade, North Fitzroy

- This case involves the proposal of a 5 storeys apartment building at 58-72 Queens Parade, North Fitzroy.
- The application lodged a failure to grant a permit within the prescribed time. If Yarra City Council were in a position to determine the application, it would have issued a notice of decision subject to conditions.
- A number of objectors held the view that proposed height was not consistent with the single, double and three storeys building heights in the area, particularly in Napier Street.
- The Tribunal found that the height, scale, building mass and design is an appropriate response to the policy and physical contexts of the site. Noting that the site is one of a few sites that abut a Strategic Redevelopment Site and not affected by a Heritage Overlay. The proposal is an appropriate fit to the emerging character of the area and does not detract from the tree lined ambience and character of the Queens Parade boulevard.

TLC Aged Care Pty Ltd v Yarra CC [2015] VCAT 1601 (6 October 2015):
217-241 Queens Parade, Fitzroy North

- VCAT refusal of an 11 storeys building with roof top garden and 3 levels of basement for an aged care facility. The proposal sought demolition of a building in a Heritage Overlay.
- Grounds of refusal based on failing to respond to the prevailing low rise built form and heritage context.
- The Tribunal determined that the proposal did not provide an appropriate transition with neighbouring buildings in a highly visible location when viewed from major roads and the surrounding neighbourhood. It did not provide architectural or sustainability design excellence to justify a building of this height, nor did it offer a social housing component.
- Permit issued 14 July 2016 for 10 storeys.

Intellect Projects Pty Lyd v Yarra CC [2012] VCAT 537:
137- 139 Queens Parade, Clifton Hill

- This is an application to partially demolish the existing building and construct a 4 storeys building at 137- 139 Queens Parade, Clifton Hill.
- The application was refused by the Responsible Authority on the grounds relating to heritage considerations.
- The site contains a 2 storeys hotel building which is identified as being individually significant.
- Yarra City Council and a number of objectors held the view that the key concern was the impact of the proposed upper most level (fourth floor) on the appearance and presentation of the existing hotel building.
- The Tribunal agreed with the Council and objectors that the proposed fourth floor will dominant the hotel building and detract from the building's prominence within and contribution to the heritage precinct.

Gurner 25-56 Queens Parade Pty Ltd v Yarra CC [2017] VCAT 1745:
25-56 Queens Parade, North Fitzroy

- This case involves the construction of a 10 storeys mixed use development comprising 295 dwellings (281 apartments and 14 townhouses). The arrangement of development comprises 4 tower elements above a 3 storeys podium. The application comprises 2 parcels; a main site (larger parcel) and rear site (smaller parcel) which are divided by a lane to the north west.
- The applicant made an application to the Tribunal given that Council did not decide within the prescribed period. Council decided that if it had been able to determine the application, it would have refused a planning permit.
- While there are a wide range of issues raised by Yarra City Council and a number of objectors, the Tribunal focused on two main issues:
  - Whether the development of the main site responds appropriately to the significance of the heritage place in which the main site is located.
  - Whether the development of the main site, particularly in its presentation to Queens Parade, responds appropriately to the relevant built form objectives in the scheme.
- At the time of this Review, the status of the VCAT proceeding is yet to be determined.

Attachment 3 - Amendment C231 - Hansen Built Form Review

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Contextual Analysis

Building Heights & Recent Development

58-72 Queens Parade
5-6 storeys (completed, approved at VCAT)

137-139 Queens Parade
4 storeys (unbuilt, approved at VCAT)

Built Form Implications

- Development interest around Dummett Crescent and Hoddle Street have the capacity for greater change given limited heritage and residential sensitivity.
- There is strategic support for increased development density and opportunity on Strategic Redevelopment Site at one of the Municipal entries. Future redevelopment of the site should transition to the Former UK Hotel landmark building.
- The eastern reach of the Study Area have recently experienced land amalgamation to accommodate taller mid rise forms (10 to 14 storey).
- Generally, recent development approvals within the NAC have retained existing heritage facade. New street walls have typically matched the parapet height of adjoining heritage facade.
- Along sensitive residential interface (rear), recent developments within the NAC have typically adopted ResCode B17 envelope.
- Recent apartment approvals have generally observed a typical 4.5m setback from common boundaries (side and rear) above the street walls to achieve equitable development outcomes.
- Future development should minimise amenity impact onto existing development. A greater side, or rear setback may be required to achieve greater development opportunity.
- Existing contemporary apartment of a mid rise typology and townhouses (strata titled) has limited capacity to change in the long terms.
- The St John's Church complex and its tower profile continue to serve as important features in the streetscape, without any development potentially 'crowding in' or diluting its visual integrity, or prominence.

Prospective key elements influencing built form framework:

- Reinforce a consistent street wall presentation along Queens Parade, particularly for lots in commercial zone and in the NAC.
- Optimise development potential on larger sites along the Boulevard, with limited heritage/ residential sensitivities and on Strategic Redevelopment Sites.
- Ensure equitable development through appropriate side setbacks that allow for either 'common' party boundary walls, or otherwise, generous spacing for outlook.
- Manage the profile of new development to avoid adverse impact on adjoining residential land by adopting a rear transition embodied by the ResCode B17 envelope, or other agreed 'stepped' tests.

Attachment 3 - Amendment C231 - Hansen Built Form Review

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Contextual Analysis
Road Network & Circulations

The movement and connectivity network within the Study Area is dominated by the urban grid network of Clifton Hill and North Fitzroy. The Study Area itself is well- defined by main arterial roads including the Queens Parade spine, as well as Hoddle Street and Heidelberg Road (east), Brunswick Street (west) and Alexandra Parade (south).

Queens Parade

- Queens Parade is a 60m wide Category 1 roadway that runs diagonally between Brunswick Street to the west and Hoddle Street to the east.
- The 60m road reserve of Queens parade between Brunswick and Smith Streets accommodates 4 traffic lanes and parallel service roads on both sides and associated car parking separated by landscaping within central median.
- The Tram Route 86 runs along Smith Street and Queens Parade east, connecting the Activity Centre to Melbourne CBD to the south and Northcote to the north.
- There is a limited north – south movement across Queens Parade between Smith Street and Wellington Street due to change in levels between the main carriageway (including tram tracks) and the service lane.
- East of Gold Street and Michael Street, Queens Parade intersects with Heidelberg Road flyover, connecting the Study Area to Hoddle Street to the east.
- Access roads provide access for properties away from main thoroughfare.

Local Streets

- Local streets are secondary north- south and east – west access roads within the Study Area.
- Within North Fitzroy (north of Queens Parade), local streets have a consistent width of 30m (except for Napier Street), comprising one traffic lane in each direction, dedicated bike lane, on street car park, central median and nature strip and pedestrian footpath on both sides. Examples of local streets within North Fitzroy includes Jamieson Street, Grant Street, Delbridge Street, Michael Street, Rushall Crescent and Newry and McKean Streets.
- The Collingwood urban grid and street network is extended further north beyond Alexandra Parade and within Clifton Hill (south of Queens Parade), local streets are generally more narrow with consistent width of 20m. Examples of local streets within Clifton Hill includes Smith Street, Wellington Street, Gold Street and Hodgkinson Street.
- Brunswick and Smith Streets accommodate public transport facilities including tram routes 112 and 86.

Laneways

- Some allotments within the Study Area have access to rear or side laneways.
- These laneways are varied in widths (3-4m) and have to date provided secondary egress points to facilitate car park access, or primary access for mid- block lots.
- It is noted that urban block north of Queens Parade generally has better access to laneways, predominantly from local streets.

Pedestrian Links

- The Clifton Hill railway station is located approximately 400m from the NAC, on the eastern side of Hoddle Street and pedestrian connections are available from Turnbull Street, or the pedestrian underpass across Heidelberg Road.
- Signalised pedestrian crossings are available at regular intervals east of Queens Parade between Smith Street and Rushall Crescent to facilitate pedestrian connection to existing median tram stops.
- Due to level changes and width of Queens Parade, there is limited north – south pedestrian movement.

Existing condition of parallel service road along Queens Parade

Existing service road along Queens Parade with limited north - south access

Existing central median tram stop along Queens Parade

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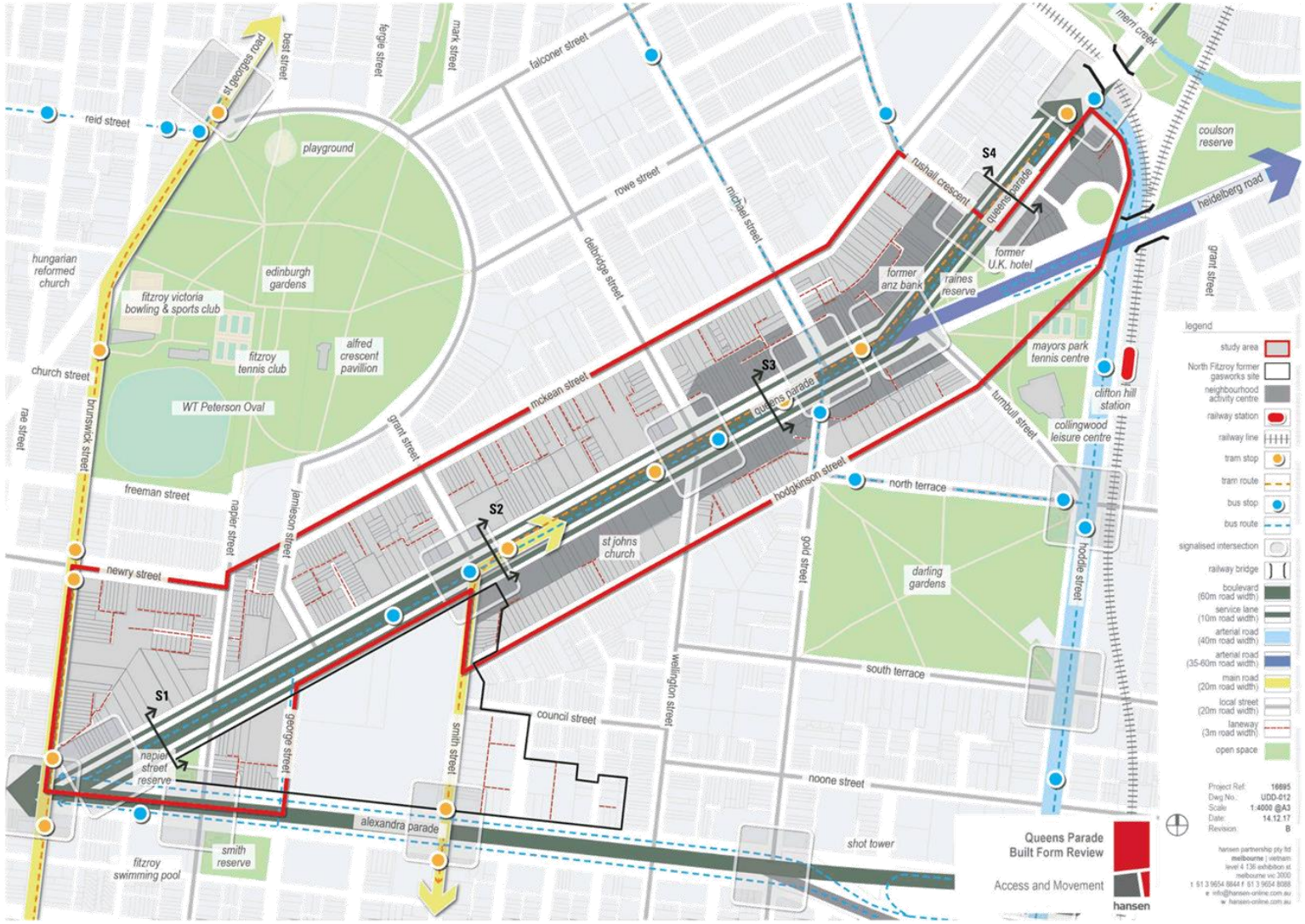

Figure 14: Access and Movement Map

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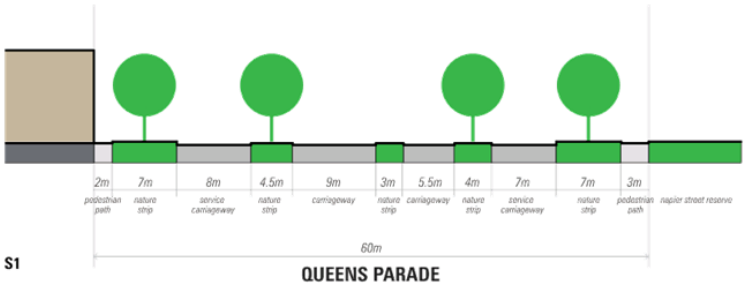
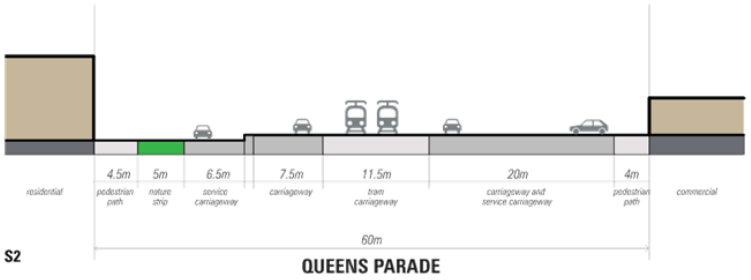
Contextual Analysis

Road Network & Circulations

Queens Parade- Typical Sections



S1 QUEENS PARADE



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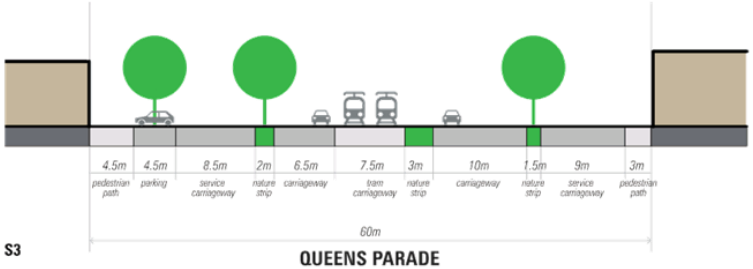

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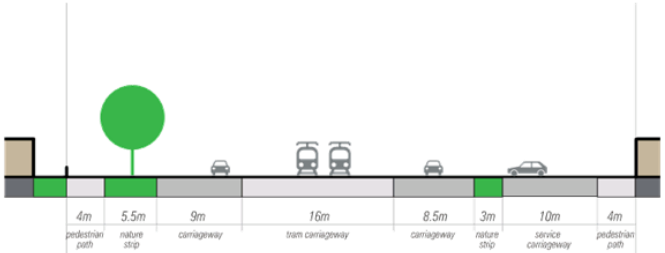

S4 QUEENS PARADE

Built Form Implications

Critical observations and built form implications include:

- Streets within the Study Area could generally be categorised into 4 types:
  - Arterial road character: typified by Hoddle Street and Heidelberg Road.
  - Main road character: typified by Brunswick Street, Smith Street and the western part of Queens Parade.
  - Boulevard character: typified by Queens Parade and Alexandra Parade.
  - Local street character: typified by other streets with wide verges and nature strips.
- The oblique and long views to significant streetscapes (within the NAC) and landmarks are valued character attributes which are afforded by the generous road width along Queens Parade.
- Development potential can be enhanced with the provision of rear access via laneways or side streets, which maximises activation of the ground floor along Queens Parade.

Prospective key elements influencing built form framework:

- Retain the 'open' profile of Queens Parade, afforded by its width through setting back of upper level form where appropriate.
- Encourage vehicular access from rear laneway/ side streets to avoid intervention and interruption of street walls.

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Attachment 3 - Amendment C231 - Hansen Built Form Review

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3.0 Existing Character Areas

Based on the preceding contextual analysis and the subsequent built form implications, the following precincts within the Study Area have been identified based on distinctive built form characteristics. Figure 15 on page 38 identifies the extent of each of the precincts, and potential constraints and opportunities:

Precinct 1: Brunswick Street Precinct

Precinct 1 defines the south- western corner of Queens Parade, anchored by the high quality heritage corner building which demarcates the entrance to Queens Parade and North Fitzroy more broadly. It currently comprises fine grain, low rise (predominantly 1-2 storeys) attached heritage fronting Brunswick Street and Queens Parade. Some buildings are setback from the streets to accommodate small front gardens or terraces, with primary open space and some car parking are accommodated within the rear backyards, abutting rear laneway.

Recent development at 496-500 Brunswick Street (a Strategic Redevelopment Site known as 'Aquila'), is up to 6 storeys in scale. It is a contemporary mixed use form with a 2 storeys street wall and recessive upper levels behind, with car parking contained within the basement level.

Key Considerations

- Modest change accommodated on small allotments behind heritage form.
- Strengthen and retain the traditional facade height and setbacks.
- Notable heritage building demarcating the important corner site (at the Queens Parade and Brunswick Street junction) should retain the legibility of its heritage form along perpendicular side streets and along Queens Parade.
- Existing apartment development ('Aquila') has limited growth potential, or to change in the long terms.
- Refurbishment and protection of heritage buildings and its frontages (i.e. front setbacks).

Precinct 1B: Brunswick Street Precinct, Queens Parade

Precinct 2: Boulevard Precinct

Precinct 2 primarily comprises domestic and remnant industrial/ warehouse allotments (zoned C2Z) including existing attached terraces along Napier Street, currently used for residential purpose. A number of existing contemporary townhouse and apartments of 3- 5 storeys contribute to a 'renewal' character on the north side of Queens Parade, while responding to prevailing low rise residential surrounds (within Heritage Overlay). The existing Art Deco facade associated with the former K.G. Luke Pty Ltd. Electro plate manufacturers factory (part) at 26 Queens Parade defines the character of this precinct. Precinct 2 is hemmed in by low scale heritage forms to the north and west, which sits within the Fitzroy North heritage precinct (HO327).

Properties fronting Queens Parade are generally low to mid rise, supporting the boulevard image of the corridor and associated established avenue planting. Avenue trees and the adjacent service road along Queens Parade create a strong streetscape character that is different to the more open and exposed Alexandra Parade condition.

Key Considerations

- Limited future opportunity on existing contemporary 5-6 storeys infill apartments in the long term.
- Greater development opportunity on large allotments with no heritage constraint.
- Continue the contemporary built form character established by recent mid rise infill that is respectful of its heritage context.
- Ensure a compatible scale transition to adjacent fine grained residential areas.
- Ensure the intensity and image of the Heritage Overlay intact heritage residential streetscape characters along Newry and McKean Streets.
- Limit the profile of new development in the skyline and retain attractive long range views to the CBD to the south.
- Future development should take into consideration its broader visual impact including not substantially intervene into key view lines, such as the CBD views from Edinburgh Gardens.
- Ensure new development does not overwhelm, or diminish the civic quality of Queens Parade boulevard.
- Recognise the absence of rear laneways to facilitate vehicular access away from Queens Parade.
- Strengthen the street wall height datum established by existing heritage street walls.
- Ensure visual dominance of the street trees in Queens Parade to maintain a 'boulevard' effect and a unifying character along its southern extent despite the widely varied existing building stock.
- Realise improved activation to Queens Parade south and define a 'continuous' address and uniform 'infill' format to the north side of the streetscape.
- Opportunity for taller development on larger allotment, shaped so as to limit the extent of overshadowing of the Queens Parade avenue and other local open space including the Napier Street reserve and established residential precinct.
- Realise appropriate transition from taller development (where opportunities exist on larger sites) to existing heritage asset within, or outside the Study Area boundary.

Precinct 2A: Boulevard Precinct, Queens Parade

Precinct 2D: Heritage terraces along Napier Street

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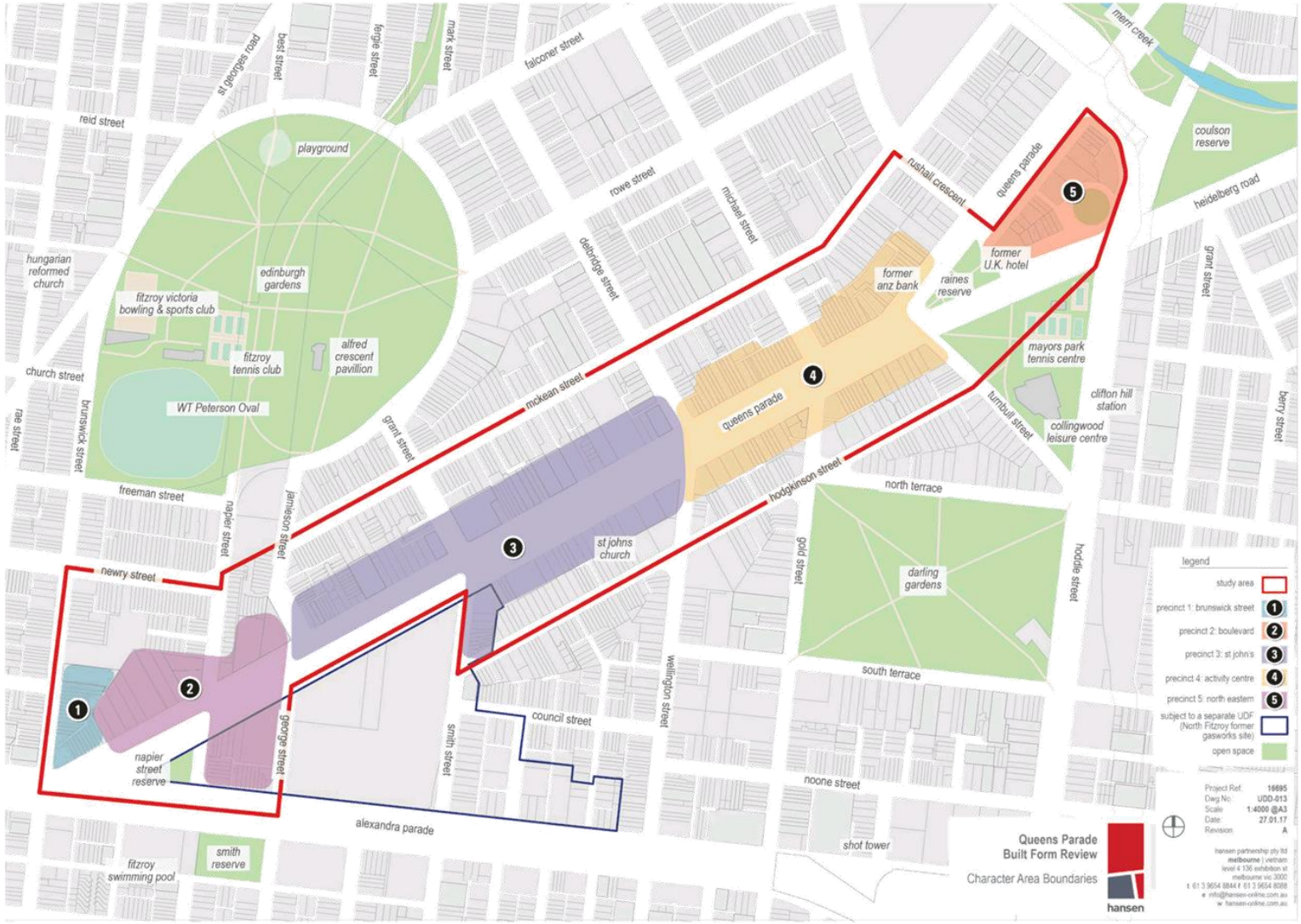

Figure 15: Built Form Character Areas Map

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3.0 Built Form Character Areas

Precinct 3: St John's Precinct

Precinct 3 is primarily dominated by residential use fronting Queens Parade and the St John's Church complex but also includes a defined commercial cluster of the terminus of Smith Street. The prominence of the St John's Church belfry and spire is a key visual landmark within this precinct given its position on high ground (around Delbridge and Wellington Streets).

Existing built form is generally low-rise (up to 3 storeys) and varied in typology and architectural era including Victorian, Edwardian, Art Deco, large format commercial forms and some recent contemporary medium density residential construction (primarily at the north western section of Precinct 3).

The streetscape presentation of this part of Queens Parade is 'open' in an absence of the double avenue planting. However, the provision of central median tram track limits north - south movement and reinforces a sense of separation between the north and south sides of the street.

Key Considerations

- Inconsistent street wall facade due to varied setbacks.
- Limited future change on residential zoned land (acknowledged by Neighbourhood Residential Zone).
- Protection of unobstructed key view lines to St John's Church belfry and spire.
- Low scale, domestic attached and detached heritage buildings define character of this part of the corridor.
- Retention of existing heritage buildings.
- Opportunity to accommodate varied, or taller street walls along the northern end of Smith Street and along Queens Parade (non contributory sites) provided view to the St John's belfry and spire are retained.
- Provide transition between potential future development at the North Fitzroy Gaswork Site (south) and the St John's Church complex.

Precinct 4: Activity Centre Precinct, Queens Parade

Precinct 4: Activity Centre Precinct

Precinct 4 comprises a more consistent fine grain streetscape of Victorian and Edwardian shopfronts of 1-3 storeys scale and bookended by notable heritage corner forms. The heritage streetscape is highly intact, with limited recent alterations.

The former ANZ Bank (370 Queens Parade) represents a prominent heritage form in the streetscape, framing the change in Queens Parade alignment and terminating view line along Turnbull Street.

Rear laneways and mid- block allotments exist behind some of the heritage forms to the north and south, providing vehicular access to private car parking and garages to commercial and residential lots.

The streetscape experience along this part of Queens Parade is generally open given the absence of the double avenue planting and low scale heritage forms. The role, however, of the tram line continues to separate the north and south side of the corridor.

Key Considerations

- A more moderate opportunity for change within fine grain sites given heritage values and narrow frontage widths, with only occasional laneway access.
- Heritage significance will strongly influence (and may restrict) future development both on heritage listed sites and adjacent land.
- Potential loss of fine grain subdivision grain through lot consolidation.
- Width of Queens Parade means that future additions are more visible above street wall than along narrower streets.
- The need to ensure a sympathetic scale transition to adjacent fine grained residential areas behind.
- Compliment and connect the street wall height established by existing heritage street walls.
- A modest degree of change could be accommodated on small allotments, but to be subservient to main heritage facade.
- Realise partial activation of the rear laneway and back of house areas.
- Restore and repair traditional street walls and other features of heritage buildings.
- Highlight viewlines to corner heritage buildings.

Precinct 5: North Eastern Precinct

Precinct 5 is recognised as an 'island' precinct given its location surrounded by three major roads (Queens Parade, Hoddle Street and Heidelberg Road) and set behind layers of service roads.

The existing fabric of low scale showrooms and commercial buildings fronting Queens Parade, Hoddle Street and Dummett Crescent (service road) has the greater capacity to change given its zone and localised attributes. There is a degree of consistent street wall presentation along Queens Parade (south of Dummett Crescent) but a more ad- hoc presentation elsewhere. Dummett Crescent and Hoddle Street are currently perceived as the 'back of house' to the urban block.

Art Deco buildings define the heritage character of this precinct, in particular to the west with the Former United Kingdom Hotel (currently McDonald's) demarcating the important junction between Queens Parade and Heidelberg Road, as well as the Former Clifton Motors Building which is located mid-block.

Recent development approvals (under construction and unconstructed) demonstrate the precinct's capacity to accommodate change, befitting of its strategic location proximity to public transport hub (Clifton Hill Station), recreational facilities (Mayors Park) and an absence of immediate sensitive residential abuttals.

Key Considerations

- The potential impact on the amenity of key open spaces.
- Poor activation and address at ground level fronting both Queens Parade, Heidelberg Road and Hoddle Street.
- Unusual subdivision pattern and lot shapes and sizes.
- The abuttals to major road and bridge infrastructure.
- Maintaining the presence of notable corner heritage building demarcating the important corner site.
- Ensuring an acceptable and gradual transition to existing heritage forms.
- Establish a visual and contemporary urban presentation to the Hoddle Street junction at the Municipal threshold while still recognising the contextual attributes of the precinct, including significant heritage buildings further south.
- Exposure to Hoddle Street (and ramps), Heidelberg Road and further north beyond the creek corridor.
- Opportunity for slender projection to be located away from heritage sensitivity, closer to Hoddle Street and Heidelberg Road frontages with minimal visual impact along Queens Parade, or Raines Reserve.
- Opportunity to establish varied street interface condition to Dummett Crescent and Hoddle Street with taller street walls.
- Establish a built edge to Hoddle Street that recognises the precinct's role to define Yarra's municipal boundary
- Restore heritage facades and establish infill that can activate an isolated parcel.

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4.0 Built Form Framework Concept

Framework Principles

Statement of Intent

Queens Parade is a key diagonal boulevard in Yarra's north east which defines a critical undulating passage through Clifton Hill and North Fitzroy on approach to Yarra's traditional main road grid (comprising Alexandra Parade, Smith Street, Brunswick Street).

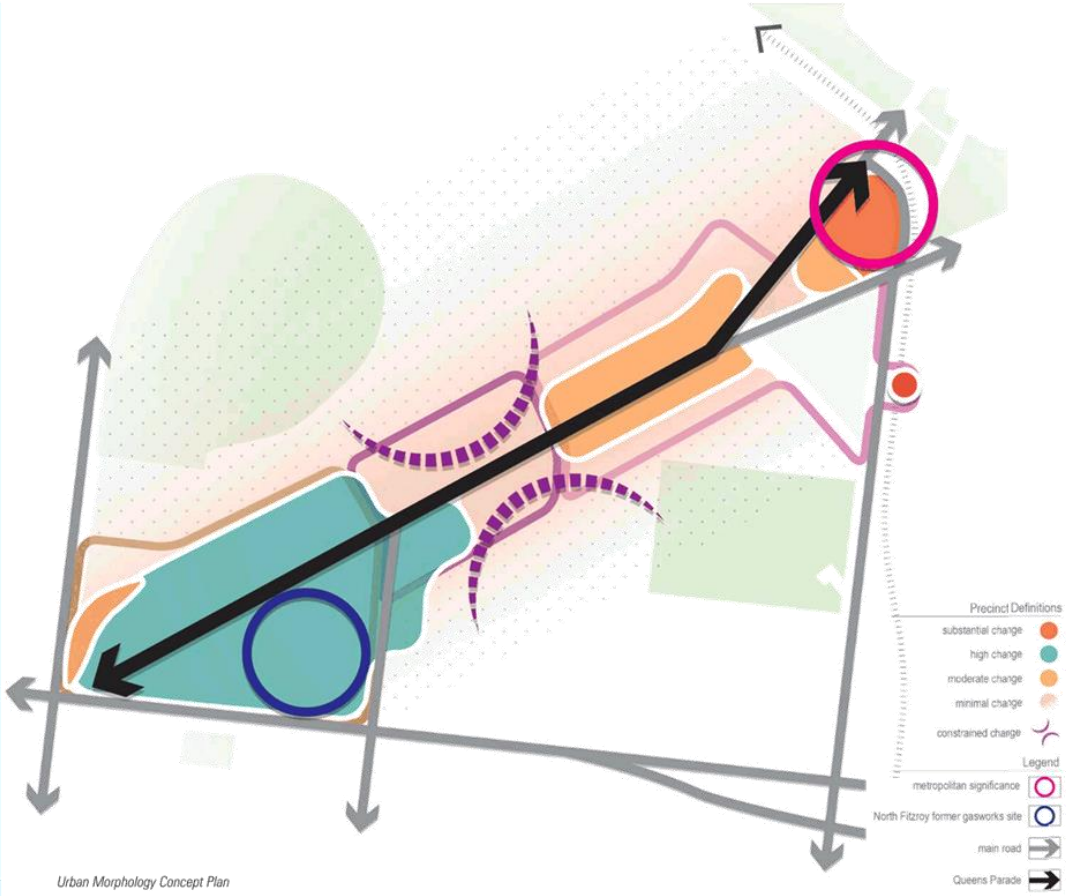
In response to typically varied fabric of the corridor (comprising Neighbourhood Activity Centre, Strategic Redevelopment Sites and established heritage residential neighbourhoods) **the future profile of the Parade will build on its very diversity**. This includes:

- Notable high density development in a concentrated cluster to the north east at the major metropolitan junction with Hoddle Street.
- Moderate evolution of the Clifton Hill NAC commercial precinct that supports protection of its valued heritage frontage.
- Strengthening of the Parade's boulevard qualities at the south western precincts, reinforced by mid rise development that respects the skyline profile of Yarra's heritage context.
- Reinforcing the integrity of traditional (heritage) residential subdivision and building stock typically on rising land around St John's Church in the 'neck' of the corridor.

Emanating from this Statement of Intent are **5 Built Form Principles** which underpin the urban morphological concept illustration.

Built Form Framework Principles

1. Acknowledge different precincts along Queens Parade defined by their heritage contexts.
2. Recognise and strengthen key view lines to key landmarks and structures.
3. Encourage built form outcomes that foster the character of each precinct.
4. Strengthen the sense of arrival into Queens Parade at its municipal gateway.
5. Respond to sensitive residential streetscape and interfaces.



Urban Morphology Concept Plan

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4.0 Built Form Framework Concept

Preferred Future Character

Key Elements Influencing the Built Form Framework

The following preferred future character statements set out the urban form ambitions for each of the defined precincts, influenced by local existing conditions and the following key urban design determinants (underpinned by the preceeding contextual analysis in Chapter 2), including:

Strategic Context

- Optimise development potential on larger sites along the Boulevard, with limited heritage/ residential sensitivities and on Strategic Redevelopment Sites.
- Encourage moderate development on smaller sites which may have a direct interface with residential land, or are affected by heritage conditions.
- Support greater development potential where lot consolidation, or amalgamation can be achieved, particularly in locations affected by fine subdivision grain.
- Recognise the limitations of narrow and shallow (less than 30m) lots in achieving upper level setback form, given minimium apartment (dwelling) dimensions defined in the Better Apartment Design Standards (BADs, Clause 58).

Heritage Significance

- Reinforce the prevailing traditional 2 storeys street wall profile of the NAC along Queens Parade.
- Retain the valued attributes of heritage buildings (i.e. Facade, setbacks, verandahs, chimneys, etc).
- The proportion of visibility of upper levels can be managed by applying varied upper level to heritage street wall principle.
- Future development should respond to individually significant heritage buildings.

Key View Lines

- Retain key view lines to landmarks including:
  - St John's belfry and spire from Queens Parade & Smith Street junction.
  - Former ANZ Building from Queens Parade & Wellington Street junction.
  - Former ANZ Building from Queens Parade & Gold Street junction.

Streetscape Character

- Reinforce a consistent street wall presentation along Queens Parade, particularly for lots in commercial zone and in the NAC.
- Retain visual dominance of the double avenue of trees along the south western part of Queens Parade and the sense of 'openness' elsewhere.
- Retain the 'open' profile of Queens Parade, afforded by its width through setting back of upper level form where appropriate.
- Respect the aspect/ outlook from heritage streetscapes in residential areas beyond the Study Area.
- Encourage vehicular access from rear laneway/ side streets to avoid intervention and interruption of street walls.

Amenity Impacts

- Manage the profile of new development to avoid adverse impact on adjoining residential land by adopting a rear transition embodied by the ResCode B17 envelope, or other agreed 'stepped' tests.
- Shape prospective redevelopment to avoid overshadowing or amenity impact on pedestrian space/ open space.
- Ensure equitable development through appropriate side setbacks that allow for either 'common' party boundary walls, or otherwise, generous spacing for outlook.

Understanding the Level of Anticipated Change

The following criteria were used to determine varying degree for change for each precinct to influence its preferred future character. It is noted that this work is not supported by any capacity or yield/ demand analysis.

Level of Anticipated Change	Criteria	Preferred Typology
Minimal Change	• Recently developed, or strata titled • Within residential land (NRZ)	• Retain existing
Moderate Change	• Small to medium allotments • Presence of low rise (1-2 storeys) contributory, or individually significant heritage forms on site • Within a significant heritage streetscape context • Proximity to sensitive residential abuttals/ interface (NRZ within Heritage Overlay) • Presence and/ or retention of viewlines to key landmark buildings	• Retain significant and contributory heritage forms • Low to mid rise infill
High Change	• Strategic Redevelopment Site designation • Limited impact on viewline to key landmark buildings • Large allotments, with greater opportunity to achieve consolidated land holding • Limited heritage sensitivity • Limited residential sensitivity • Addresses (primary and/or secondary) to arterial roads including Alexandra Parade, Heidelberg Road and Hoddle Street • Existing mid rise forms (3 to 5 storeys)	• Retain significant and contributory heritage forms • Mid rise infill
Substantial Change	• Strategic Redevelopment Site designation • No Heritage Overlay • No residential sensitivity • Large allotments with multiple point of access and addresses • Limited amenity impact to public realm • Existing (under construction/ approved) taller mid rise development	• Tower infill

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4.0 Built Form Framework Concept

Preferred Future Character

Preferred Future Character

This Review identifies 5 distinct precincts along Queens Parade, each with different spatial qualities and characteristics. There are opportunities for more considerable development to be accommodated in certain precincts as distinct from a more tempered response where heritage and residential sensitivities exist. The preferred future character statements for each precinct is set out below (refer to Figure 16).

Precinct 1: Brunswick Street Precinct

Moderate infill setback behind fine grained heritage frontages.

Properties fronting Brunswick Street and turning the corner to Queens Parade will continue to be defined by fine grained low scaled heritage buildings with the opportunity for moderate future infill behind the main heritage frontage, supported by rear lane access. The heritage building on the corner of Brunswick Street and Queens Parade should retain primacy.

Summary of preferred outcomes

- **Anticipated change:** moderate.
- **Preferred typology:** low rise infill.
- **Preferred maximum building height:** 9m on properties fronting Brunswick Street and Queens Parade (typical).

Precinct 2: Boulevard Precinct

Consolidated mid rise infill addressing the boulevard and its sensitive heritage context.

The designated strategic redevelopment site and other large (former industrial and warehouse) parcels fronting either side of Queens Parade and Alexandra Parade will realise consolidated infill that reinforces the strong heritage and boulevard character of the streetscape with a transition down to the established surrounding heritage neighbourhood. The profile of redevelopment will complement Yarra's acknowledged heritage skyline and be sensitive to views from Newry Street and the Edinburgh Gardens.

Summary of preferred outcomes

- **Anticipated change:** high to minimal.
- **Preferred typology:** mid rise infill.
- **Preferred maximum building height:**
  - 28.5m to 31m on strategic redevelopment sites, and
  - 18m on heritage sites along Napier Street.

Precinct 3: St John's Precinct

Mixed residential heritage fabric with renewal and infill potential around the Smith Street junction.

The varied pattern of subdivision to either side of Queens Parade as it rises to St John's Church complex will retain a consistent heritage character, with a discrete opportunity for moderate consolidation around the Smith Street junction. Redevelopment of this commercial precinct will be shaped by its relationship to the adjoining Former Gasworks Site and an effective transition to heritage residential land (south and east), while retaining the primacy of the St John's Church landmark and view to its tower, as well as paying regards for heritage buildings along Smith Street.

Summary of preferred outcomes

- **Anticipated change:** moderate and minimal.
- **Preferred typology:** mid rise infill.
- **Preferred maximum building height:** 18m.

Precinct 4: Activity Centre Precinct

Recessive upper level redevelopment behind a heritage commercial streetscape.

The diverse arrangement of heritage buildings (varied eras, scale, subdivision grain and profile) in the Queens Parade, Clifton Hill NAC will be retained as the predominant feature, with the potential for redevelopment to include additional upper level. Sympathetic upper level infill will reinforce the street wall definition, subdivision grain of the significant streetscape and transition down to residential abuttals to the rear. The profile of upper level redevelopment should acknowledge the importance of significant heritage corner forms and retain key view line to the local landmark (former ANZ Building).

Summary of preferred outcomes

- **Anticipated change:** moderate.
- **Preferred typology:** mid rise infill.
- **Preferred maximum building height:** 18m.

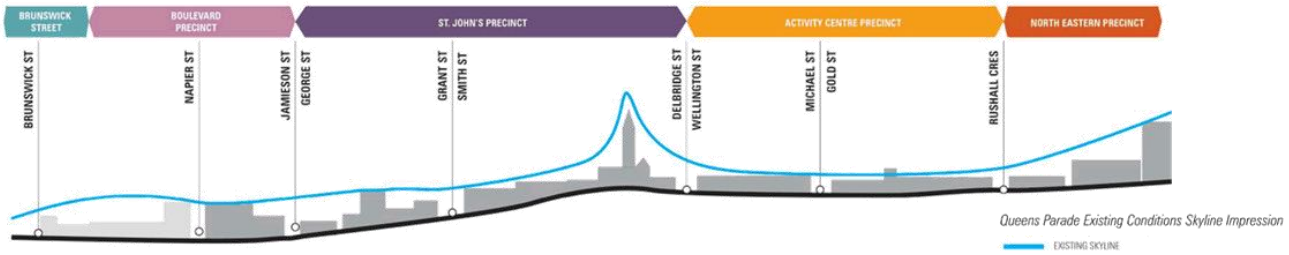
Precinct 5: North Eastern Precinct

Concentrated taller mid rise form on an island precinct defining the Municipal threshold.

The discrete pocket of mixed use land bound by Queens Parade, Heidelberg Road and Hoddle Street serves as a junction of Metropolitan presence and will accommodate taller, contemporary form on larger unencumbered sites (heritage and residential abuttals). Commensurate with its subdivision grain, development opportunity and form will gradually transition down to the south west with regard for State- significant heritage buildings (and potentially Former Clifton Motors and UK Hotel). Land to the north east of Dummett Crescent will define a new urban character of taller forms contributing to the Yarra skyline, while retaining the prominence of the Former UK Hotel building from the southern reaches.

Summary of preferred outcomes

- **Anticipated change:** moderate to substantial.
- **Preferred typology:** mid rise infill.
- **Preferred maximum building heights:**
  - 18m on heritage site.
  - up to 27m south of Dummett Crescent, and
  - up to 45m on strategic redevelopment site north of Dummett Crescent.



Queens Parade Existing Conditions Skyline Impression

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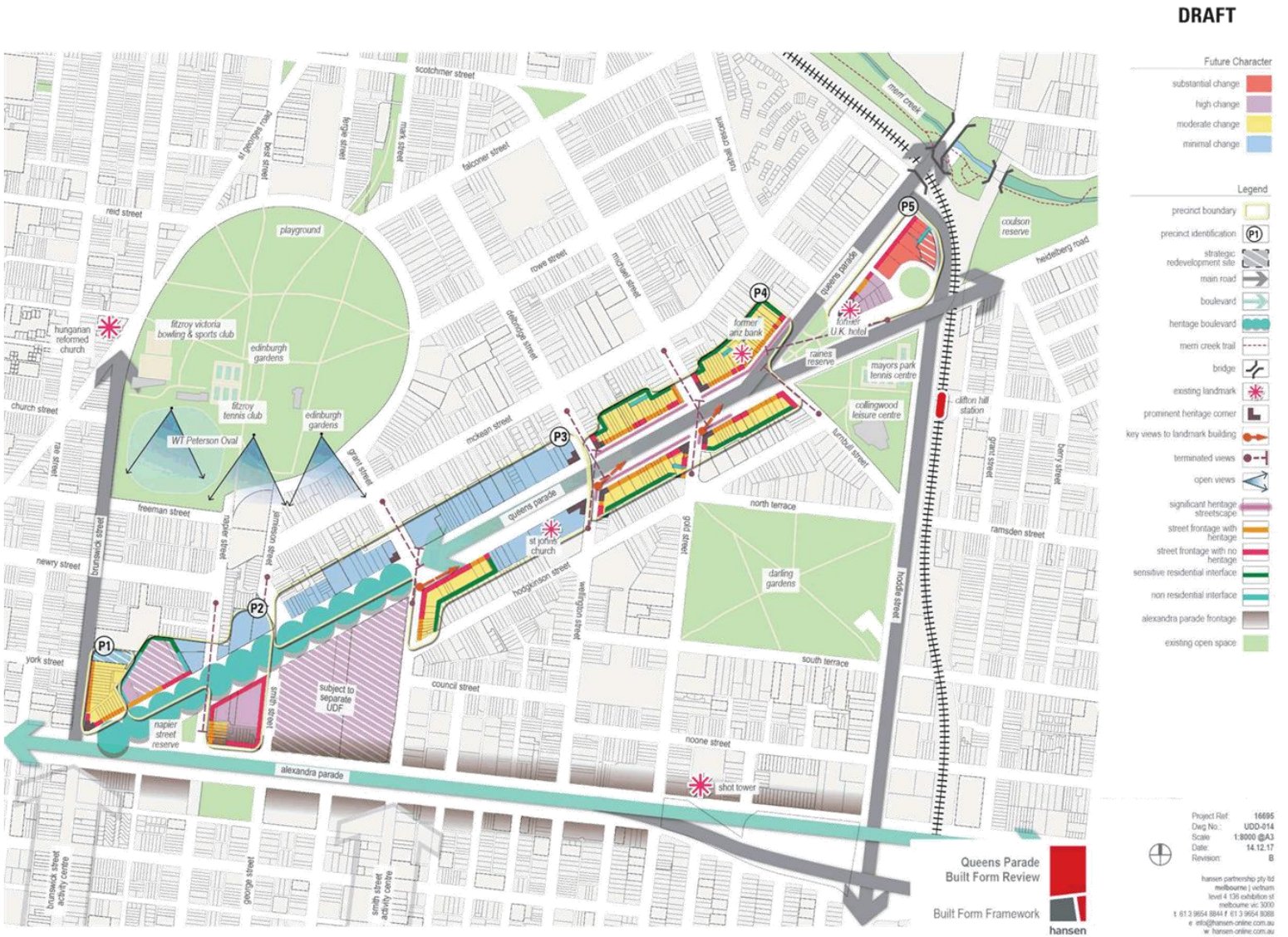

Figure 16: Built Form Framework Map

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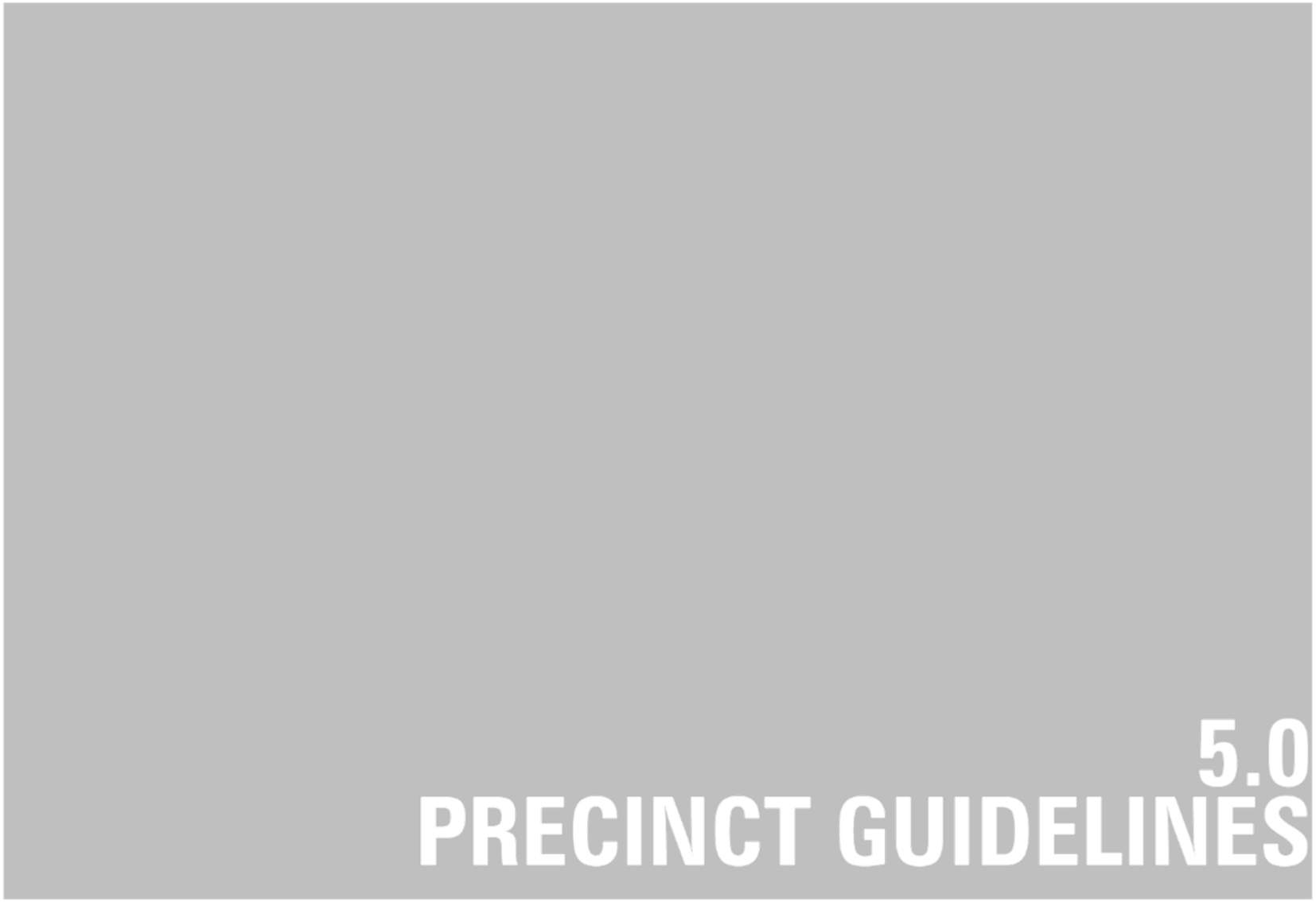

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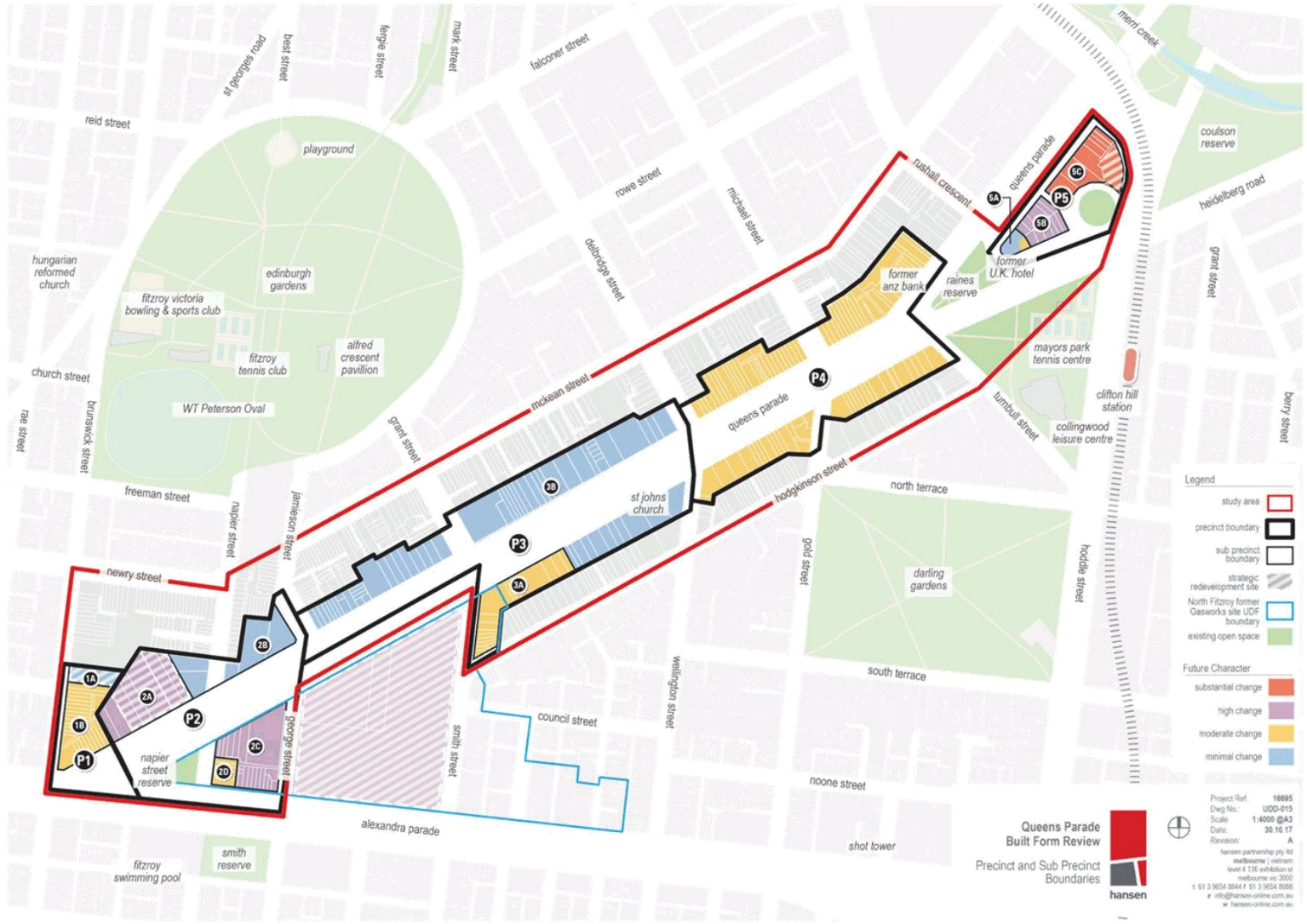

Figure 17: Precinct And Sub Precinct Boundaries Map

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5.0 Precinct Guidelines

Built Form Recommendations

- A range of specific built form recommendations and design guidelines have been developed for all precincts.
- The built form guidelines focus on the following 5 precincts (refer to Table 3- Summary of Built Form Guidelines), where future development is anticipated and requires design guidance.
- The shaping of building heights within Queens Parade have been influenced by many factors, including:
  - Practice Note No.60 - Height and Setback Controls for Activity Centre;
  - Practice Note No.59- The Role of Mandatory Provisions in Planning Schemes;
  - Anticipated form outside the Study Area (i.e. Residential Zone);
  - Council adopted North Fitzroy Former Gasworks Urban Development Framework (2008);
  - Views to existing landmarks from the public realm;
  - Transitional relationships – especially to and around traditional or heritage elements and sensitive residential;
  - Recognition of Yarra's heritage skyline;
  - Strategic Redevelopment Sites identified in Clause 21.03 of the Yarra Planning Scheme;
  - Recent development approvals (including those currently under construction) - refer to page 28 of this report;
  - 3-Dimensional computer modelling of built form testing for the Study Area (refer to Appendix A);
  - Heritage and existing character considerations, consistent with heritage advice and advice provided by heritage advisors (GJM Heritage); and
  - Extensive site inspections and workshops with Council's officers.

Formula for Deriving Numerical Building Heights

The maximum building height has acknowledged the higher floor to floor heights typically found in heritage buildings (between 8-11m), with further allowance for other design elements usual to buildings (parapets, railings, etc) as follows:

- Ground floor: 4m/ floor
- Level 1 and above: 3.5m/ floor

It is noted that higher numerical floor levels for mixed use development where reduced floor to floor height requirement exist for residential use (typically measured at 3m-3.1m/ level). The Review has not precluded the opportunity for non- residential use to be accommodated within the upper levels (above ground and level 1).

Mandatory and Discretionary Height & Setback Controls

PPN59 notes that the VPPs are predominantly performance-based and that mandatory provisions are the exception. The Built Form and Heritage Analysis and Recommendations have identified some areas where mandatory controls can be contemplated on the basis of urban design and heritage attributes as follows:

- **Maximum building height** on sites which have the capacity to influence Yarra's acknowledged heritage skyline, heritage streetscape along Newry Street and heritage boulevard along Queens Parade.
- Protection of **key views to heritage landmarks** including:
  - St. John's Church belfry and spire from the Smith Street intersection.
  - The former ANZ Bank building (upper level and roof) from the Gold Street and Wellington Street intersections.
- Protection of parts of Queens Parade identified as a '**significant heritage streetscape**'.

Basis of Height Parameters

In most locations, preferred maximum building heights have been derived with regard to the necessary street wall height, front upper level setback and rear boundary transition conditions - noting a minimum dwelling depth of 10-12m. It has also been determined on heritage sites by proportional relationships between intact front elevations (street walls) and the upper form.

Upper Level Setbacks Behind Street Walls

In commercial areas, almost all urban design guidelines seek to introduce a 'street wall' and 'upper level front setback' measure. The purpose of such setbacks are to ensure predominance of any traditional parapet while also seeking to reinforce view lines to local landmarks from key vantage points.

Generally, and subject to the street wall conditions and heritage advice, a setback distance of 6m (to built form and balconies) is preferred. This allows for preservation of the heritage 'volume', and ensures a viable 'articulation zone' in the elevation with spatial separation between the street wall frontage and upper levels behind.

A 6m default setback also allows for usable terrace or outdoor space above the street wall parapet. In particular locations, the proposed setback behind the podium maybe reduced in the absence of heritage and residential sensitivities, or increased on sites with individually significant heritage forms, or key landmark views (Refer to Queens Parade Built Form Heritage Analysis & Recommendations - GJM Heritage).

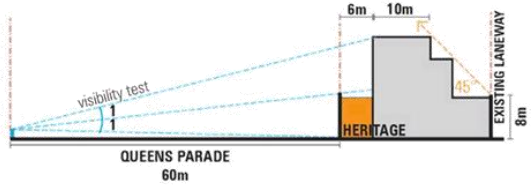

Table 3: Summary of Built Form Guidelines

Precincts	Sub-Precincts	Anticipated Change	Preferred Typology	Preferred Maximum Building Heights
Precinct 1: Brunswick Street Precinct ***Moderate infill setback behind fine grained heritage frontages.***	1A	minimal	retain existing	retain existing
	1B	moderate	low rise infill	9m (up to 3 storeys residential)
Precinct 2: Boulevard Precinct ***Consolidated mid rise infill addressing the boulevard and its sensitive heritage context.***	2A	high	mid rise infill	31m (up to 10 storeys residential)
	2B	minimal	retain existing	retain existing
	2C	high	mid rise infill	28.5m (8 storeys)
	2D	moderate	mid rise infill	18m (5 storeys)
Precinct 3: St John's Precinct ***Mixed residential heritage fabric with renewal and infill potential at the Smith Street junction.***	3A	moderate	mid rise infill	18m (5 storeys)
	3B	minimal	retain existing	retain existing
Precinct 4: Activity Centre Precinct ***Recessive upper level redevelopment behind a heritage commercial streetscape.***	4	moderate	mid rise infill	18m (5 storeys)
Precinct 5: North Eastern Precinct ***Concentrated taller mid rise form on an island precinct defining the Municipal threshold.***	5A	minimal to moderate	retain existing and mid rise infill	18m (5 storeys)
	5B	high	mid rise infill	28.5m (8 storeys)
	5C	substantial	tower infill	49.5m (14 storeys)

Attachment 3 - Amendment C231 - Hansen Built Form Review

Figure 18: Precinct 1 & 2 Framework Plan

Attachment 3 - Amendment C231 - Hansen Built Form Review

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5.0 Precinct Guidelines

Precinct 1: Brunswick Street

Moderate infill setback behind fine grained heritage frontages.

Preferred Character Statement for Precinct 1

Properties fronting Brunswick Street and turning the corner to Queens Parade will continue to be defined by fine grained low scaled heritage buildings with the opportunity for moderate future infill behind the main heritage frontage, supported by rear lane access. The heritage building on the corner of Brunswick Street and Queens Parade should retain primacy.

Summary of preferred outcome

- **Anticipated change:** moderate
- **Preferred typology:** low rise infill
- **Preferred maximum building height:** 9m (up to 3 residential storeys) on properties fronting Brunswick Street and Queens Parade (typical).

Precinct 1B

Built Form Objectives

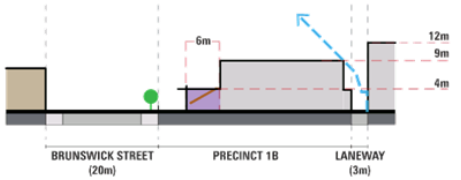
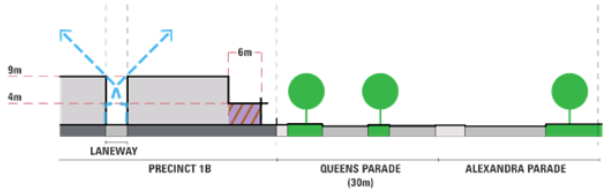
- To encourage moderate infill development that is setback behind the traditional street frontage of Brunswick Street and Queens Parade that respects the prevailing streetscape character and subdivision grain (Clause 21.05).
- To reinforce the heritage value of the precinct and support the retention of the traditional street frontages, including street setbacks, facades and subdivision pattern (Clause 21.05, 22.02, 22.10).
- To retain the primacy of the significant heritage form on the Brunswick Street and Queens Parade corner (Clause 22.02).
- To encourage future vehicle access and services be provided off rear laneway (Clause 22.07).
- To ensure visually recessive, high quality and sympathetic upper level elevations that are exposed to the public domain (Clause 22.10).

(Refer to Figure 18: Precincts 1 & 2 Framework Plan)

Built Form Guidelines- Precinct 1B (Refer to Precinct Framework Plan on page 48) — 1B

Built Form Elements	Requirements	Objectives
Building height	• 9m (up to 3 storeys residential) for properties fronting Brunswick Street and Queens Parade.	• To realise a moderate mixed use redevelopment that respects the heritage and streetscape value of the precinct.
Street wall height	• Retain existing (significant, contributory). • Development adjoining the individually significant corner building should match the parapet height of that building.	• To reinforce the heritage values of the precinct. • To support the realisation of amenable mixed use redevelopment behind the traditional frontage.
Front setback *Refer to Sections 1-1 and 1-2*	• Retain existing (significant, contributory). • Development adjoining the individually significant corner building should match the setback of that building.	
Upper level front setback*	• 6m behind the heritage façade/ street wall along Queens Parade and Brunswick Street.	
Setback(s) from side boundary	• ResCode Standard B17.	
Setback(s) from rear boundary	• 45° angle above 8m from rear boundary to a laneway. • 45° angle above 5m from rear boundary (no laneway).	

Built Form Guidelines - Precinct 1B (Refer to Precinct Framework Plan on page 48) — 1B

Section No.	Interface	Typical Section
S1-1	Brunswick Street	
S1-2	Queens Parade	

*Note: * For site specific upper level setbacks (as identified on Figure 18), refer to Queens Parade Built Form Heritage Analysis & Recommendations - GJM Heritage.*

Attachment 3 - Amendment C231 - Hansen Built Form Review

5.0 Precinct Guidelines

Precinct 2: Boulevard Precinct

Consolidated mid rise infill addressing the boulevard and its sensitive heritage context.

Preferred Character Statement for Precinct 2

The designated strategic redevelopment site and other large (former industrial and warehouse) parcels fronting either side of Queens Parade and Alexandra Parade will realise consolidated infill that reinforces the strong heritage and boulevard character of the streetscape with a transition down to the established surrounding heritage neighbourhood. The profile of redevelopment will complement Yarra's acknowledged heritage skyline and be sensitive to views from Newry Street and the Edinburgh Gardens.

Summary of preferred outcome

- Anticipated change: minimal, moderate and high
- Preferred typology: mid rise infill
- Preferred maximum building height: 28.5m to 31m on strategic redevelopment site & 18m on heritage sites along Napier Street.

Precinct 2

Built Form Objectives- Precincts 2A, 2C and 2D

- To encourage redevelopment of the land and adaptive reuse of the former 'K.G. Luke Pty Ltd. Electro plate manufacturers factory (part)' in any re-development of the site (Clause 21.04, 21.05, 21.08, 22.02, 22.10).
- To support greater development intensity (high change) that contributes positively to the urban character of Fitzroy North (Clause 21.04, 21.05, 21.08).
- To encourage moderate infill development that is setback behind the traditional heritage street frontage of Napier Street that respects the prevailing streetscape character and subdivision grain in Precinct 2D (Clause 21.05).
- To retain, enhance and incorporate the existing heritage street wall into future redevelopment to realise a consistent parapet effect along the streetscape (Clause 22.02)
- To ensure that new development does not diminish, or detract from the heritage values of the boulevard streetscape, the heritage street wall and the heritage trees along Queens Parade (Clause 22.02, 22.10).
- To moderate the prominence of development above the street wall with respect to long range panoramic and streetscape views and the Yarra skyline from the Edinburgh Gardens (HO213) and Newry Street (HO327- Fitzroy North Precinct) (Clause 21.05, 22.02, 22.10).
- To ensure appropriate transition in building scale having regard to existing and preferred future format of land to the north and west (Clause 21.05, 22.02, 22.08).
- To recognise the discrete size and format of local development in the makeup of redevelopment as a series of parts, separated by links and/or connection through Precinct 2A (Clause 22.10).
- To establish new 'urban grain' to Queens Parade which reflects surrounding streetscape patterns, including a series of separate development parts (Clause 21.04, 21.05 21.08, 22.10).
- To ensure adequate solar access is provided to the Queens Parade boulevard and the adjoining Napier Street Reserve (Clause 22.10).
- To encourage future vehicle access and services be provided off existing or continuing rear laneways for Precincts 2C and 2D (Clause 22.07).

Built Form Guidelines - Precinct 2A (Refer to Precinct Framework Plan on pg 48 & Typical Sections on pg 52-53) 2A

Built Form Elements	Requirements	Objectives
Building height	• 31m (up to 10 storeys residential).	• To realise consolidated infill that reinforces the strong heritage and boulevard character of the streetscape. • To ensure that projections above the street wall are not dominant in the skyline when viewed from Newry Street and the Edinburgh Gardens. • To ensure adequate solar access to the Queens Parade boulevard and the adjoining Napier Street Reserve at the equinox (09.00-15.00). • To ensure a gradual transition in development scale to the north and west.
Street wall height	• Retain existing heritage facade (significant). • Where there is no existing heritage facade: 11m.	• To incorporate the existing heritage street wall into redevelopment of the site and establish a consistent parapet along the streetscape. • To ensure the primacy of the landscape boulevard within the Queens Parade viewshed. • To ensure new development does not detract from the heritage values of the boulevard streetscape, the heritage street wall and the heritage trees along Queens Parade.
Front setback	• Retain existing heritage facade (significant). • Where there is no existing heritage facade: 0m.	
Upper level front setback *Refer to Section 2-1*	Above existing heritage facade: • 8m above 11m vertical height; and • 11m above 16m vertical height. Above new street wall: • 5m above 11m vertical height; and • 8 metres above 16m vertical height.	
Setback(s) from side/ rear boundaries – North and North West (NRZ and GRZ) *Refer to Section 2-4*	• ResCode Standard B17 measured from rear boundary of adjoining properties; • 45° angle above 11m up to 25m vertical height; and • 30m from the boundary above 25m vertical height.	• To realise an appropriate interface arrangement and minimise visual bulk and mass when viewed from neighbouring properties. • To avoid repetitive stepped form within the 45° profile. • To ensure the heritage and streetscape character of Newry Street is not compromised.
Setback(s) from side/ rear boundaries – East (NRZ)	• 0m to match party wall of existing adjoining development up to 10 metres; • 45° angle above 10m up to 25m vertical height; and • 30m from the boundary above 25m vertical height.	• To realise an appropriate interface arrangement and minimise visual bulk and mass when viewed from neighbouring properties. • To avoid repetitive stepped form within the 45° profile. • To ensure adequate amenity to existing apartments located to the east.
Setback(s) from side boundary – East (MUZ) *Refer to Section 2-3*	• 0m to match party wall of existing adjoining development. • 9m from the windows/ balconies of adjoining apartments up to 5 storeys. • 15m above 16m vertical height.	
Setback(s) from side/ rear boundaries – West and North (MUZ) *Refer to Section 2-5*	• 0m to match party wall of existing adjoining development to the west, or 10m where there is no party wall; • 45° angle above 10m up to 25m vertical height; and • 45° angle above 25m.	• To realise an appropriate interface arrangement and minimise visual bulk and mass when viewed from neighbouring properties. • To avoid repetitive stepped form within the 45° profile. • To ensure adequate amenity to existing apartments located to the north west. • To ensure a gradual transition in development scale to the north and west.

(Refer to Figure 18: Precincts 1 & 2 Framework Plan)

Attachment 3 - Amendment C231 - Hansen Built Form Review

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5.0 Precinct Guidelines

Precincts 2C & 2D

Built Form Guidelines - Precinct 2C (Refer to Precinct Framework Plan on pg 48) — 2C

Built Form Elements	Requirements	Objectives
Building height	• 28.5m (8 storeys).	• To realise consolidated infill that reinforces the strong boulevard character of the streetscape along Queens Parade and Alexandra Parade. • To ensure adequate solar access to the adjoining Napier Street Reserve at the equinox. • To ensure adequate solar access to the south side of Alexandra Parade at winter solstice (UDF, 2008). • To ensure a gradual transition in development scale from east of George Street (Former Gasworks Site) to south west (heritage properties along Napier Street).
Street wall height*	• 18m	• To ensure the primacy of the landscape boulevard within the Queens Parade viewshed (UDF, 2008) • To minimise the visual impact of upper levels from adjoining street and when viewed in the oblique along the street above parapets (UDF, 2008) • To ensure new development does not detract from the heritage values of the boulevard streetscape and the heritage trees along Queens Parade.
Front setback	• 0m	
Upper level front setback*	• 6m above street wall	
Setback(s) from rear/ side boundary	• 0m	
Setback(s) from rear/ side boundary - (Precinct 2D interface)*	• 45° angle above 18m vertical height	• To ensure adequate amenity to existing residential located in Precinct 2D. • To realise appropriate interface arrangement and minimise visual bulk and mass when viewed from neighbouring heritage properties. • To avoid repetitive stepped form within the 45° profile.

Built Form Guidelines - Precinct 2D (Refer to Precinct Framework Plan on pg 48 & Typical Sections on pg 53) — 2D

Built Form Elements	Requirements	Objectives
Building height	• 18m (5 storeys).	• To realise a moderate infill redevelopment that respects the heritage and streetscape value of the precinct.
Street wall height	• Retain existing.	• To reinforce the heritage values of the precinct. • To support the realisation of amenable mixed use redevelopment behind the traditional frontage.
Front setback	• Retain existing.	
Upper level front setback	• 6m above heritage facade to Napier Street.	
Setback(s) from side boundary	• 0m	
Setback(s) from rear boundary	• 0m	

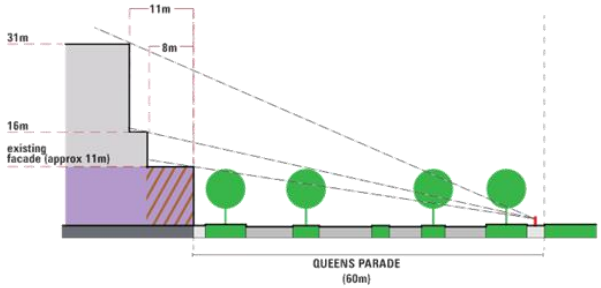
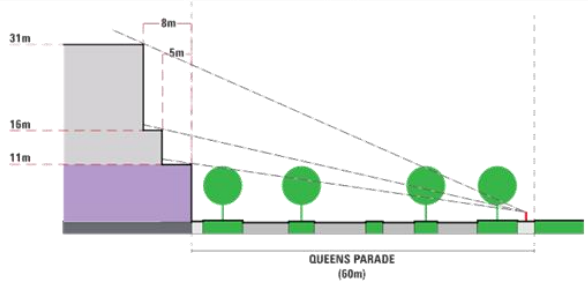
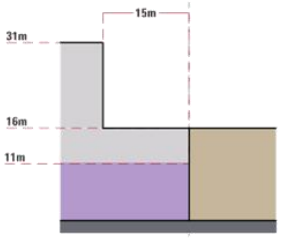
*Note: * For site specific upper level setbacks (as identified on Figure 18), refer to Queens Parade Built Form Heritage Analysis & Recommendations - GJM Heritage.*

Attachment 3 - Amendment C231 - Hansen Built Form Review

5.0 Precinct Guidelines

Precinct 2A: Typical Interfaces

Built Form Guidelines - Precinct 2A (Refer to Precinct Framework Plan on page 48) — 2A

Section No.	Interface	Typical Section
S2-1	Queens Parade (With Heritage Facade)	
S2-2	Queens Parade (New Street Wall)	
S2-3	Mixed Use Zone Interface (East)	

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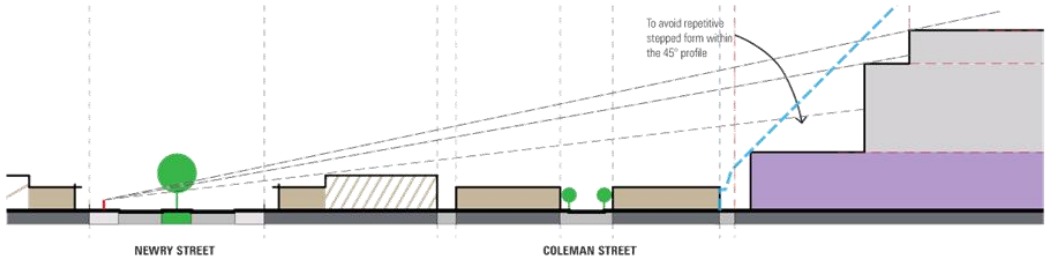
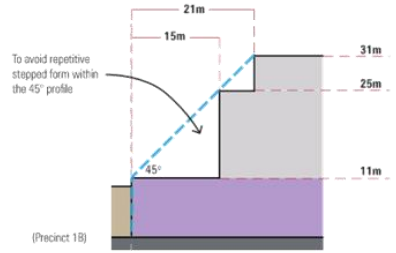
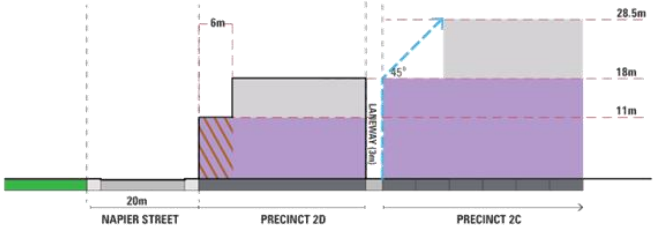
5.0 Precinct Guidelines

Precinct 2A & Precinct 2D: Typical Interfaces

Built Form Guidelines - Precinct 2A (Refer to Precinct Framework Plan on page 48) — 2A

Section No.	Interface	Typical Section
S2-4	Residential Zone Interface (North)	
S2-5	Mixed Use Zone Interface (West)	

Built Form Guidelines - Precinct 2D (Refer to Precinct Framework Plan on page 48) — 2D

Section No.	Interface	Typical Section
S2- 6	Napier Street	

Attachment 3 - Amendment C231 - Hansen Built Form Review

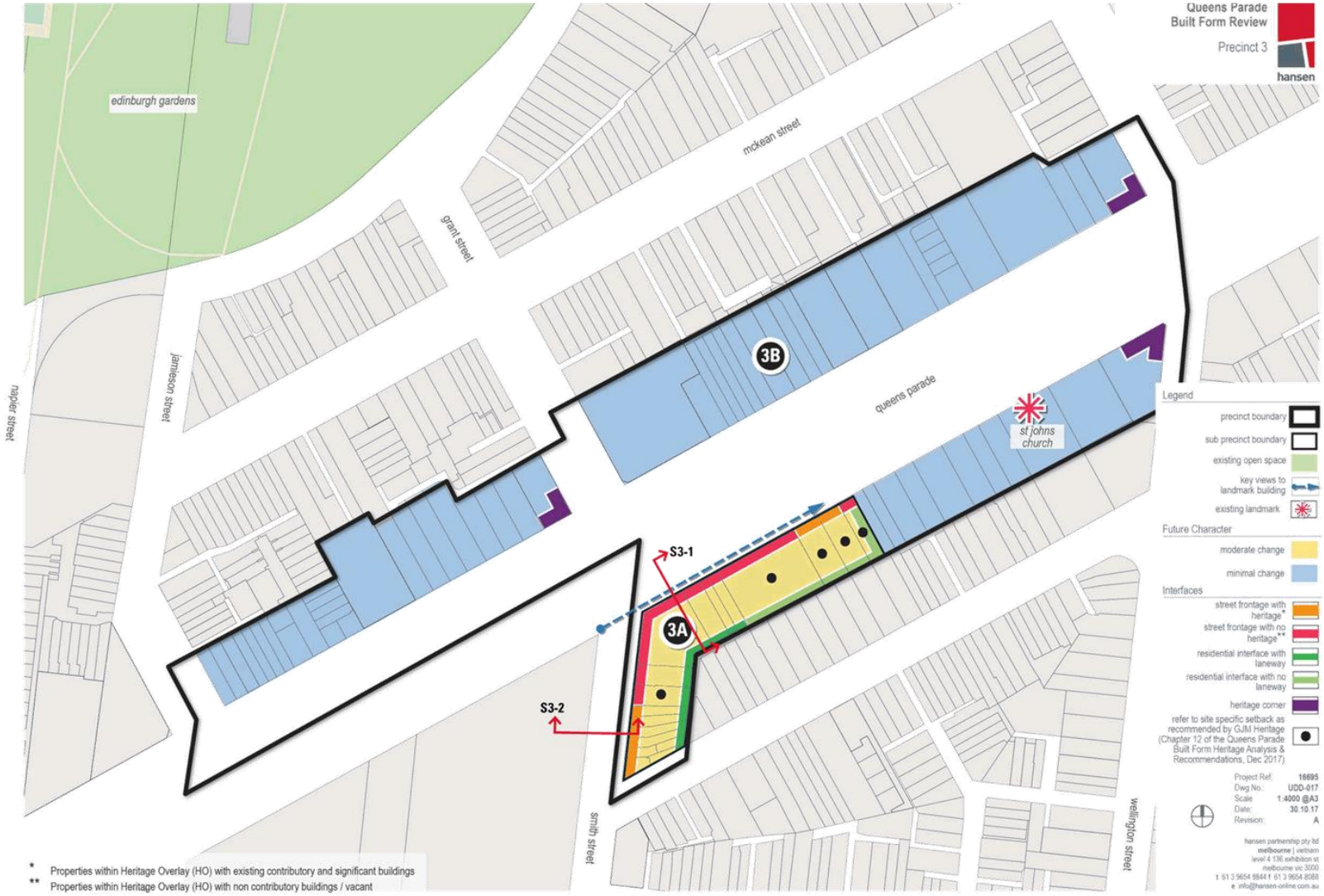


Figure 19: Precinct 3 Framework Plan

Attachment 3 - Amendment C231 - Hansen Built Form Review

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5.0 Precinct Guidelines

Precinct 3: St John's Precinct

Mixed residential heritage fabric with renewal and infill potential at the Smith Street junction.

The varied pattern of subdivision to either side of Queens Parade as it rises to St John's Church complex will retain a consistent heritage character, with a discrete opportunity for moderate consolidation around the Smith Street junction. Redevelopment of this commercial precinct will be shaped by its relationship to the adjoining Former Gasworks Site and an effective transition to heritage residential land (south and east), while retaining the primacy of the St John's Church landmark and view to its belfry and spire, as well as paying regards for heritage buildings along Smith Street.

Summary of preferred outcome

- **Anticipated change:** moderate and minimal
- **Preferred typology:** mid rise infill
- **Preferred maximum building height:** 18m.

Precinct 3A

Built Form Objectives

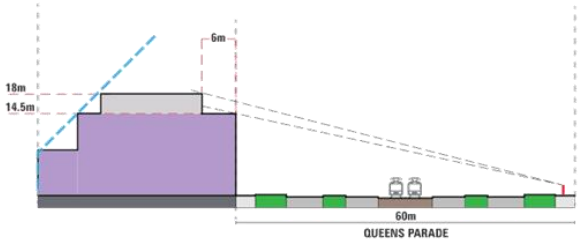
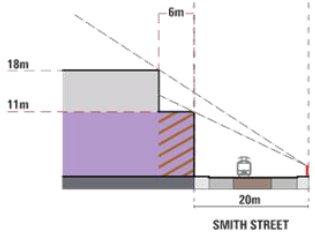
- To support greater development intensity (moderate change) that contributes positively to the urban character of Clifton Hill (Clause 21.04, 21.05 21.08).
- To support infill redevelopment on larger parcels that defines the Smith Street junction with a transition away from the Former Gasworks Site (west) (Clause 21.04).
- To setback building frontages to maintain the visual prominence and primacy of the St Johns complex (including tower and spire) against sky backdrop from the public realm (Clause 22.02, 22.03, 22.10).
- To reinforce the heritage value of the precinct and support the retention of the traditional street frontages, including street setbacks, facades and subdivision pattern (Clause 21.05, 22.02, 22.10).
- To ensure appropriate transition in building scale across the site having regard to existing format of land to the south and east (Clause 21.05, 22.02, 22.08).
- To encourage future vehicle access and services be provided off existing or continuing rear laneways (Clause 22.07).
- To ensure high quality and sympathetic upper level elevations that are exposed to the public domain (Clause 22.10).

(Refer to Figure 19: Precinct 3 Framework Plan)

Built Form Guidelines- Precinct 3A (Refer to Precinct Framework Plan on pg 54) — 3A

Built Form Elements	Requirements	Objectives
Building height	• 18m (5 storeys).	• To realise a moderate infill redevelopment that respects the heritage and streetscape value of the precinct.
Street wall height*	• Retain existing (contributory/ significant). • 14.5m (4 storeys) for non- contributory sites.	• To reinforce the heritage values of the precinct, including the traditional street wall along Smith Street. • To retain views to the St John's Church spire and belfry. • To ensure a suitable transition to sensitive residential land to south and east.
Front setback	• Retain existing (contributory/ significant). • 0m (non- contributory sites).	
Upper level front setback*	• 6m above street wall/ heritage facade.	
Setback(s) from side boundary	• 0m.	
Setback(s) from side boundary (NRZ)	• ResCode Standard B17.	
Setback(s) from rear boundary (NRZ)	• 45° angle above 8m from rear boundary to a laneway.	

Built Form Guidelines - Precinct 3A (Refer to Precinct Framework Plan on page 54) — 3A

Section No.	Interface	Typical Section
S3-1	**Queens Parade (New Street Wall)**	
S3-2	**Smith Street (With Heritage Facade)**	

*Note: * For site specific upper level setbacks (as identified on Figure 19), refer to Queens Parade Built Form Heritage Analysis & Recommendations - GJM Heritage.*

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Figure 20: Precinct 4 Framework Plan

Attachment 3 - Amendment C231 - Hansen Built Form Review

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5.0 Precinct Guidelines

Precinct 4: Activity Centre Precinct

Recessive upper level redevelopment behind a heritage commercial streetscape.

Preferred Character Statement for Precinct 4

The diverse arrangement of heritage buildings (varied eras, scale, subdivision grain and profile) in the Queens Parade, Clifton Hill NAC will be retained as the predominant feature, with the potential for redevelopment behind the street wall at upper levels. Sympathetic upper level infill will reinforce the prevailing street wall and subdivision grain of significant streetscapes and transition down to residential abuttals to the rear. The profile of upper level redevelopment should also acknowledge the importance of significant heritage corner forms and retain key view lines to local landmarks (former ANZ Building).

Summary of preferred outcome

- Anticipated change: moderate
- Preferred typology: mid rise infill
- Preferred maximum building height: 5 storeys (18m).

Precinct 4

Built Form Objectives

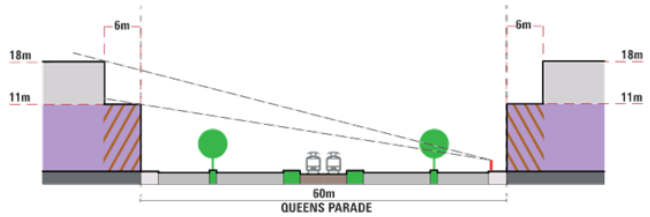
- To reinforce the heritage value of the precinct and support the retention of the traditional street frontages, including street setbacks, facades and subdivision pattern (Clause 21.05, 22.02, 22.10).
- To retain the primacy of the significant heritage forms on Queens Parade (Clause 22.02).
- To support infill development behind the traditional street wall that contributes positively to the urban character of the Clifton Hill NAC (Clause 21.04, 21.05 21.08).
- To retain heritage frontages to maintain the visual prominence and primacy of the St Johns complex and other significant heritage buildings within the NAC (Clause 22.02, 22.03, 22.10).
- To ensure appropriate transition in building scale across the site having regard to existing format of land to the north and south (Clause 21.05, 22.02, 22.08).
- To encourage future vehicle access and services be provided off existing or continuing rear laneways (Clause 22.07).
- To ensure high quality and sympathetic upper level elevations that are exposed to the public domain (Clause 22.10).

(Refer to Figure 20: Precinct 4 Framework Plan)

Built Form Guidelines- Precinct 4 (Refer to Precinct Framework Plan on page 56) — 4

Built Form Elements	Requirements	Objectives
Building height	• 18m (5 storeys).	• To realise a moderate infill redevelopment that respects the heritage and streetscape value of the precinct.
Street wall height*	• Retain existing (contributory/ significant). • 11m (3 storeys) for non contributory sites.	• To reinforce the heritage values of streetscape for this part of Queens Parade, including the traditional street wall. • To retain views to the St John's Church's (spire and belfry) and other significant heritage buildings including the Former ANZ Building at 376 Queens Parade. • To ensure a suitable transition to residential land. • To ensure equitable development opportunity and amenity to mid- block sites (C1Z). • To avoid repetitive stepped form within the 45° profile. • To realise an appropriate interface arrangement to minimise visual bulk and mass when viewed from neighbouring properties. • To minimise overshadowing impact onto residential properties to the south.
Front setback	• Retain existing (contributory/ significant). • 0m (non- contributory).	
Upper level front setback*	• 6m above street wall/ heritage facade.	
Setback(s) from side/ rear boundary (NRZ interface)	• 45° angle above 8m from rear boundary to a laneway. • 45° angle above 5m from rear boundary (no laneway).	
Setback(s) from rear boundary (C1Z interface)	• 0m up to 11m. • 3m setback above 11m.	

Built Form Guidelines - Precinct 4 (Refer to Precinct Framework Plan on page 56) — 4

Section No.	Interface	Typical Section
S4-1	Queens Parade (With Heritage Facade)	

*Note: * For site specific upper level setbacks (as identified on Figure 20), refer to Queens Parade Built Form Heritage Analysis & Recommendations - GJM Heritage.*

Attachment 3 - Amendment C231 - Hansen Built Form Review

Figure 21: Precinct 5 Framework Plan

Attachment 3 - Amendment C231 - Hansen Built Form Review

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5.0 Precinct Guidelines

Precinct 5: North East Precinct

Concentrated mid rise and tower infill within an island precinct defining the Municipal threshold.

Preferred Character Statement for Precinct 5

The discrete pocket of mixed use land bound by Queens Parade, Heidelberg Road and Hoddle Street serves as a junction of Metropolitan presence and will accommodate taller, contemporary forms on larger unencumbered sites. Commensurate with its broad subdivision grain, development opportunity and scale will gradually transition down from the north-east terminal junction to the south west with an interface with the significant heritage buildings (the Former Clifton Motors and UK Hotel buildings). Land to the north east of Dummett Crescent will define a new urban character of tower forms contributing to the Yarra skyline.

Summary of preferred outcome

- **Anticipated change:** moderate to substantial
- **Preferred typology:** mid rise infill (south of Dummett Crescent) & taller mid rise infill (north of Dummett Crescent)
- **Preferred maximum building heights:** between 18m to 28.5m (5-8 storeys) south of Dummett Crescent & up to 49.5m (14 storeys) north of Dummett Crescent.

Built Form Objectives- Precincts 5A, 5B and 5C (refer to Figure 21)

- To encourage land consolidation and substantial redevelopment of the land in Precinct 5C that contributes positively to the urban character of the Queens Parade precinct and the Yarra skyline (Clause 21.04, 21.05 21.08).
- To support the presentation of contemporary taller forms to the north east of the precinct (Precinct 5C), comprising a new street wall to Queens Parade, with increased height to the Hoddle Street frontage (Clause 21.05).
- To ensure suitable separation between rising taller forms to enable development equity and amenity, while also contributing to the management of building/ visual bulk in Precinct 5C (Clause 22.10).
- To support higher density mixed use development of attached mid rise form and sensitive adaptive reuse of individual significant heritage parcels in Precincts 5A (south west) that complement the significant heritage elements (Clause 21.04, 21.05, 21.08, 22.02, 22.10).
- To retain, conserve and incorporate the existing heritage frontage into redevelopment of Precinct 5B to realise a compatible streetscape effect (Clause 22.02).
- To ensure the significant heritage fabric at the south western corner (the Former UK Hotel in Precinct 5A is conserved and remains a prominent feature in any redevelopment (Clause 22.02).
- To moderate the prominence of development above the heritage frontage with respect to long range panoramic and streetscape views and the Yarra skyline (Clause 21.05, 22.02, 22.10).
- To ensure appropriate transition in building scale across the precincts (5C to 5A) having regard to existing and preferred future format of land (Clause 21.05, 22.02, 22.10).
- To avoid a singular vertical projection which detracts from the prominence of key landmark buildings from dominating the precinct skyline (Clause 21.05, 22.02, 22.10).

Built Form Guidelines- Precinct 5A (Refer to Precinct Framework Plan on page 58) — 5A

Built Form Elements	Requirements	Objectives
Building height*	• Retain the existing Former UK Hotel building (state significant heritage).	• To realise a mid rise infill redevelopment that complements the heritage value of the site.
Street wall height*	• Retain existing.	• To reinforce the heritage values of the Former UK Hotel building and protect its integrity from the principal south west approach.
Front setback*	• Retain existing heritage setback.	
Upper level front setback*	• 6m above street wall along Dummett Crescent.	
Setback(s) from side boundary	• 0m	
Setback(s) from rear boundary	• 0m	

Built Form Guidelines- Precinct 5B (Refer to Precinct Framework Plan on page 58) — 5B

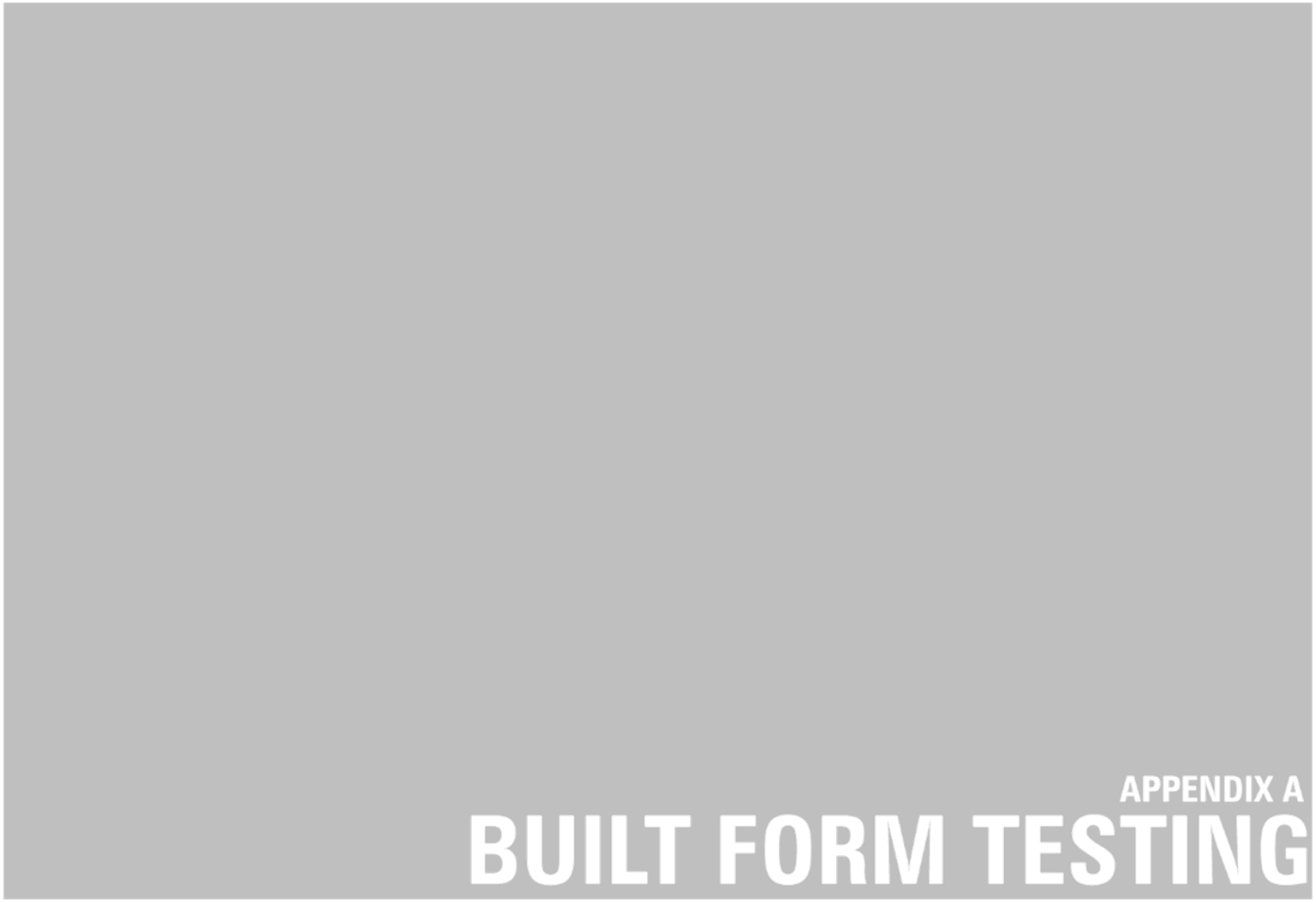
Built Form Elements	Requirements	Objectives
Building height	• 28.5m (8 storeys).	• To realise a mid rise infill
Street wall height*	• Retain existing heritage facade of the Former Clifton Motors Building (state significant heritage). • 11m (3 storeys) for non contributory sites along Queens Parade.	• To reinforce the heritage values of the precinct and protect the heritage integrity of the principal south west corner form. • To create a consistent street wall condition to all street frontages. • To realise attached mid rise form, with marginal variance in upper level setbacks.
Front setback	• 0m	
Upper level front setback*	• 6m above street wall/ heritage facade to Queens Parade. • Mandatory 6m above heritage facade at the Former Clifton Motors Building.	
Setback(s) from side boundary	• 0m	
Setback(s) from rear boundary	• 0m	

Built Form Guidelines- Precinct 5C (Refer to Precinct Framework Plan on page 58) — 5C

Built Form Elements	Requirements	Objectives
Building height	• 49.5m (14 storeys)	• To realise tower redevelopment of the Metropolitan junction with a profile to Hoddle Street. • To establish skyline projections that complement the profile of Yarra's existing skyline and designated landmarks.
Street wall height	• 35.5m (10 storeys) along Queens Parade.	• To denibstrate a progressive shift in form that addresses the junction of Queens Parade and Hoddle Street.
Front setback	• 0m	
Upper level front setback	• 10m above street wall	
Setback(s) from side boundary	• 4.5m from centreline of laneway, or from adjoining habitable windows/ balconies.	• To ensure adequate amenity and development equity to future development.
Setback(s) from rear boundary	• 4.5m from centreline of laneway, or from adjoining habitable windows/ balconies.	

*Note: * For site specific upper level setbacks (as identified on Figure 21), refer to Queens Parade Built Form Heritage Analysis & Recommendations - GJM Heritage.*

Attachment 3 - Amendment C231 - Hansen Built Form Review



Attachment 3 - Amendment C231 - Hansen Built Form Review

DRAFT

A. Built Form Testing

A. Built Form Testing

To test the validity of the 'Recommended Built Form Parameters' and support their useful translation into a DDO control, Hansen has prepared 3D computer massing modelling of the precinct. The tests demonstrate the visual implications of built form outcomes to enable assessment. Details of the modelling approach and methodology are provided below.

Purpose and Scope of Built Form Testing

- The Queens Parade Built Form Review Recommendations were tested a high-level 3D computer modelling of development envelopes. It was established and operated as a 'working' massing model used to informally measure built form heights and setbacks to properties along the length of the Queens Parade corridor (within the Study Area) as a useful general tool in comparative analysis.
- The development and use of such 3D massing models is common for strategic built form work of this kind to examine the general relationships between new urban form (various options or scenarios), topography and key views to existing landmarks from identified vantage points as advised by GJM Heritage.
- 3D massing models are commonly used in addition to more conventional 2D cross-sections (and other tools) when determining built form controls and assessing off-site impacts on surrounding land such as overshadowing and visual bulk.

Massing Model

- The massing model utilised is 'fit for purpose' for a strategic corridor study of its kind. It does not rely on detailed site survey data, rather more general available contour, landform and cadastral information. Such modelling has been reliably applied in other strategic work across Metropolitan Melbourne.
- The massing model is prepared in SketchUp Pro (2016 and 2017) and utilises automatic Google terrain as the basis for topography and basic aerial imagery (via 'Geo-Location' command).
- The existing building envelopes surrounding the site are depicted as massing representations only and do not seek to represent existing building detail, in terms of architectural form or appearance – but rather overall scale.
- The existing 'landmark' building envelopes have been modelled based on 'Google Earth – Street View' tests to determine their overall scale. These include:
  - St John the Baptist Church;
  - Former ANZ Bank building;
  - Former United Kingdom Hotel; and
  - Former Clifton Motors Building .
- Within the Study Area (areas identified for moderate, high and substantial change), the development envelope has been represented in 3-dimension based on the site and cadastral information available from the City of Yarra and DataVIC with an assumption of 100% site coverage.
- Further, a number of recent development envelopes (approved, but unbuilt and under construction) have been modelled to represent their overall scales and setbacks, based on endorsed architectural plans provided by the City of Yarra.
- We acknowledge that the basis of the model (i.e. site boundaries and levels) is not as accurate as one generated with a detailed site survey.
- Development massing for contributory and significant heritage sites has been modelled based on the following measurements:
  - 11m 'street wall' (as a typical measurement for 2-storeys Victorian-era parapet); and
  - 3.5m floor to floor height for upper levels above the 'street wall'.
- Development massing for non-contributory, or non-heritage sites has been modelled based on the following measurements:
  - 4.0m floor to floor height for ground level; and
  - 3.5m floor to floor height for level 1 and above.

Attachment 3 - Amendment C231 - Hansen Built Form Review

DRAFT

B. Photo Match Model Views

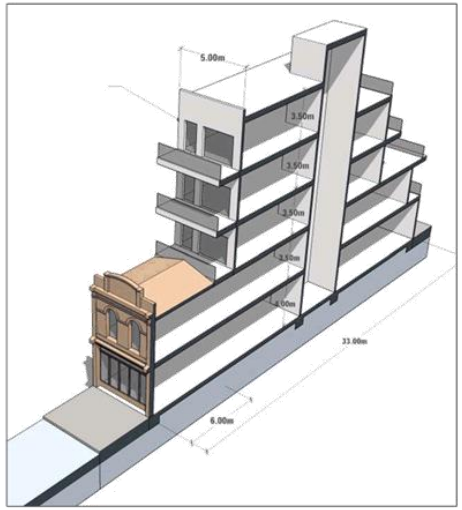
Purpose and Scope of Built Form Testing

- 10 locations are identified by GJM Heritage to determine critical key views to heritage landmarks from the public realm and the required built form response as identified on Figure 1 (overleaf). This testing has utilised the working massing model (prepared in Sketch Up, as described in Part A), but was registered accurately with surveyor's information and photographic settings to represent a genuine eye level views.
- The photos used in the photomontages were taken using a Digital SLR camera (Canon EOS 60D) with a 18mm lens setting. The digital lens has a 1.6x multiplier, hence a digital lens setting of 18mm is equivalent to a 'full frame' 35mm film camera using a 28.8mm lens (30 x 1.6 = 28.8), which provides an angle of view in the order of 64 degrees per frame.
- The positioning of the camera was set upon a spirit levelled tripod oriented towards the heritage landmarks and taken at a height of 1.6m above ground level. The photographs were taken on the 31 July 2017.
- The preparation of photo match model views undertaken utilising the following software programmes:
  - **3ds Max 2016** (3D modeller); for importing the massing model (prepared in Sketch Up as described in Part A). Positioning and alignment of the three-dimensional massing model is based on photo locations and control points obtained on site by Geocomp Consulting Pty Ltd, referenced to both Australian Height Datum and Australian Map Grid data.
  - **VRay** (rendering software); for applying colours to the massing model to express varying attributes.
  - **Adobe Photoshop CC2016**; for rendering views from the 3 dimensional massing model camera. Locations are superimposed into the photograph without any distortion or manipulation, except for necessary changes to provide a true representation of the proposal within its context.

C. Case Studies

Purpose and Scope of Built Form Testing

- 5 sites in Precincts 3 and 4 were selected by the Council project team to test the feasibility of the built form recommendation on typical fine grained sites along Queens Parade and Smith Street, with varying allotment depths as identified on Figure 1 (overleaf).
- This case study test has utilised both 2D and 3D testing tools to demonstrate how the built form recommendation are realised on single, or consolidated sites, noting varied site attributes and without detrimental impact on the heritage and residential sensitivities.



Attachment 3 - Amendment C231 - Hansen Built Form Review

APPENDIX A

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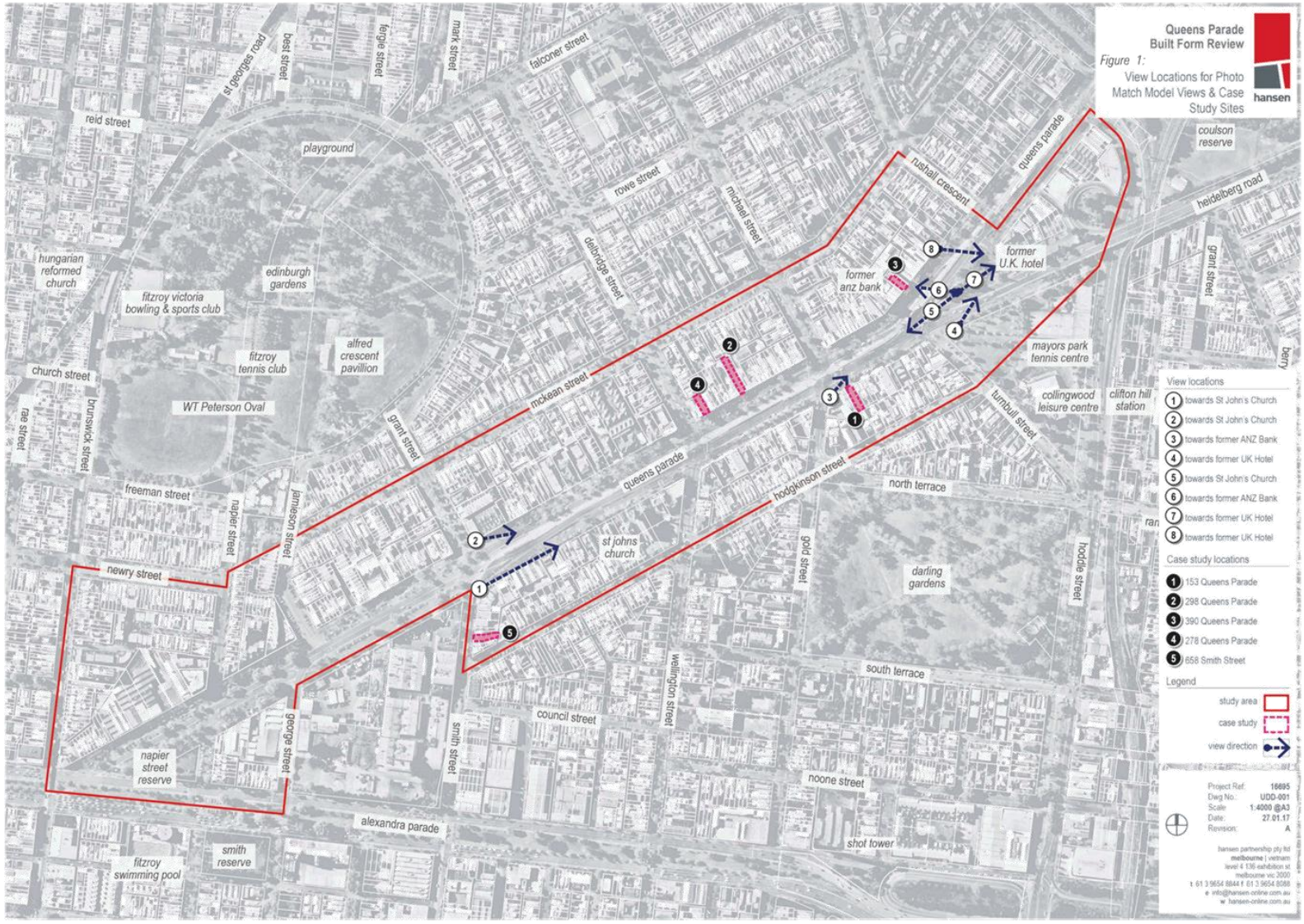

Attachment 3 - Amendment C231 - Hansen Built Form Review

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View location 01: View from intersection of Smith Street to the St John's Church

Existing condition

Key

- Possible future envelope on non heritage sites (precinct 3)
- Possible future envelope on heritage sites (precinct 4)
- Possible future envelope on non heritage sites (precinct 5)

3D massing demonstrating built form and heritage recommendations for precinct 3A

Attachment 3 - Amendment C231 - Hansen Built Form Review

DRAFT

View location 02: View from intersection of Grant Street to the St John's Church

Existing condition

Key

- Possible future envelope on non heritage sites (precinct 3)
- Possible future envelope on heritage sites (precinct 3)
- Possible future envelope on heritage sites (precinct 4)

3D massing demonstrating built form and heritage recommendations for precinct 3A

Attachment 3 - Amendment C231 - Hansen Built Form Review

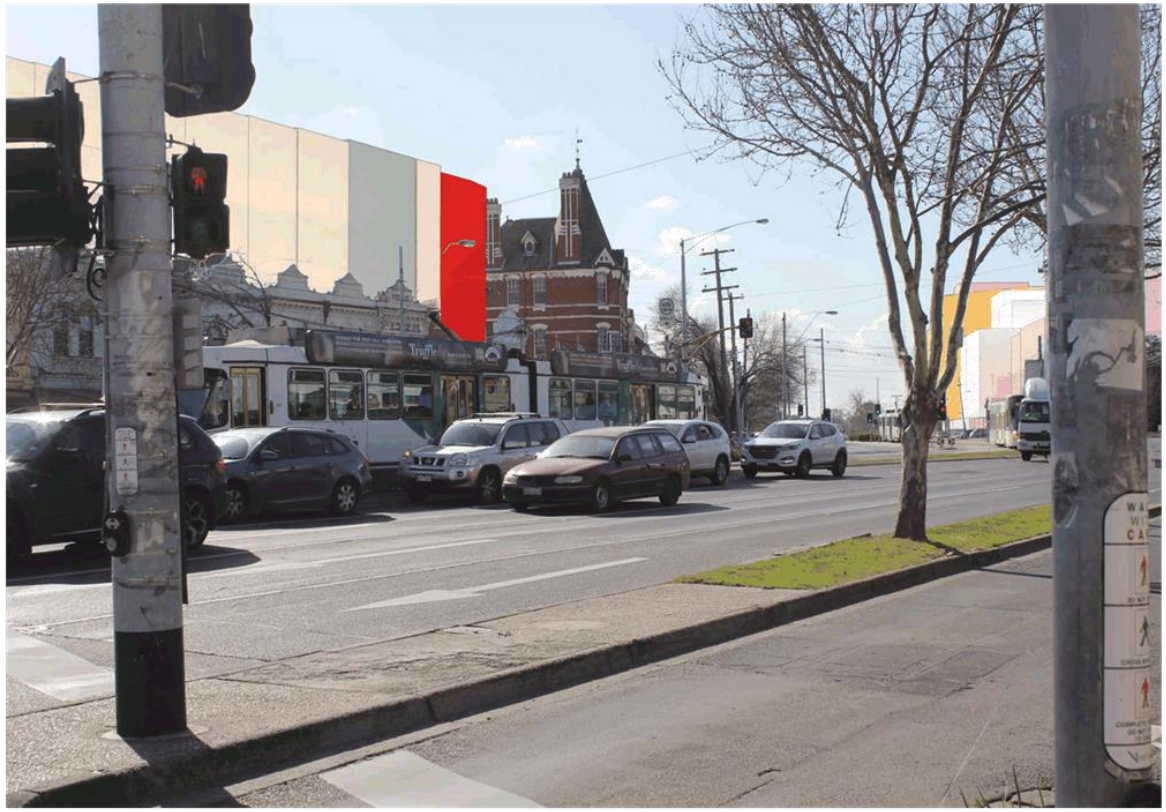
DRAFT

View location 03: View from intersection of Gold Street to the former ANZ Bank

Existing condition

Key

- Possible future envelope on non heritage sites (precinct 4)
- Possible future envelope on heritage sites (precinct 4)
- Possible greater upper level setback is required to retain key view line to the former ANZ building (precinct 4)
- Possible future envelope (precinct 5)
- Development approval indicative envelope (precinct 5)



3D massing demonstrating built form and heritage recommendations for precinct 4

Attachment 3 - Amendment C231 - Hansen Built Form Review

DRAFT

View location 04: View form Mayors Park (east of Heidelberg Road) to the former UK Hotel

Existing condition

Key

Possible future envelope (precinct 5)

3D massing demonstrating built form and heritage recommendations for precinct 5

Attachment 3 - Amendment C231 - Hansen Built Form Review

DRAFT

View location 05: View from Raines Reserve to St John's Church

Existing condition

Key

- Possible future envelope on non heritage sites (precinct 4)
- Possible future envelope on heritage sites (precinct 4)
- Possible greater upper level setback is required to retain key view line to the former ANZ building (precinct 4)

3D massing demonstrating built form and heritage recommendations for precinct 4

Attachment 3 - Amendment C231 - Hansen Built Form Review

View location 06: View from Mayors Park (east of Heidelberg Road) to the former ANZ Bank

Existing condition

Key

- Possible future envelope on non heritage sites (precinct 4)
- Possible future envelope on heritage sites (precinct 4)
- Possible greater upper level setback is required to retain key view line to the former ANZ building (precinct 4)
- Development proposal indicative envelope (precinct 4)

3D massing demonstrating built form and heritage recommendations for precinct 4

Attachment 3 - Amendment C231 - Hansen Built Form Review

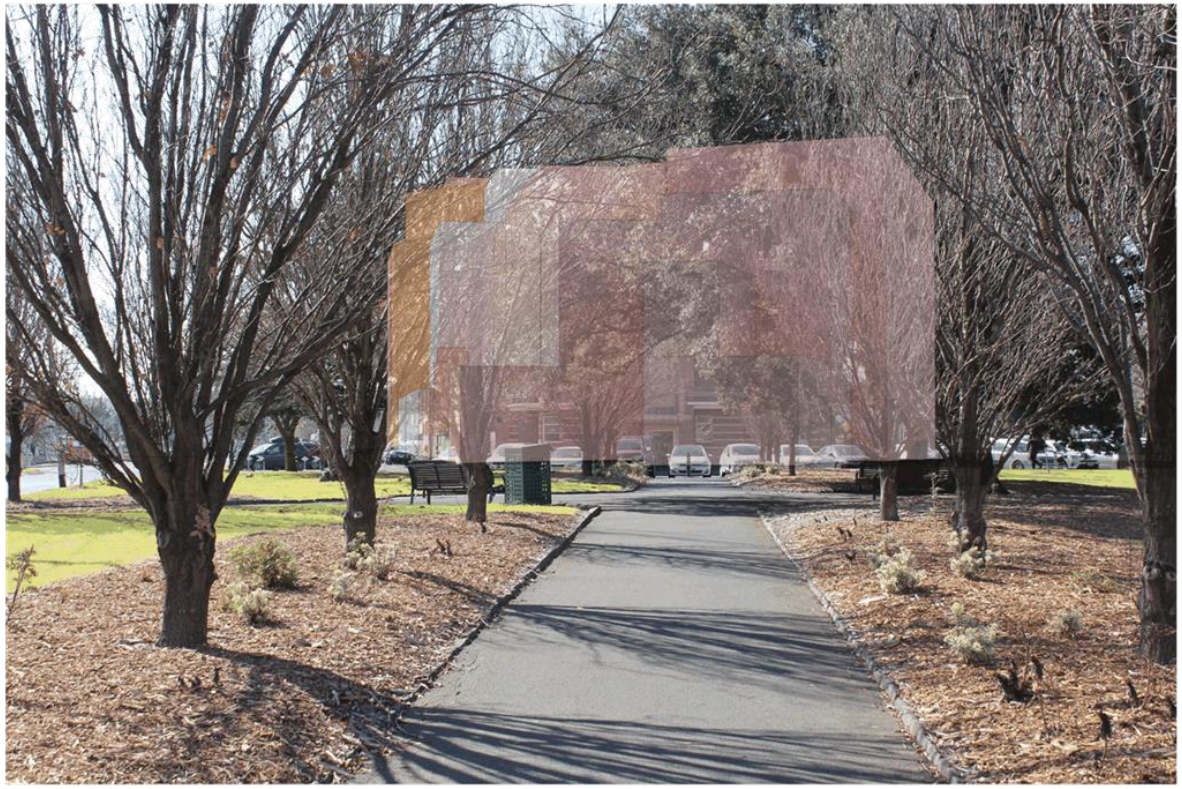
DRAFT

View location 07: View from Raines Reserve to the former UK Hotel

Existing condition

Key

- Existing development indicative envelope - under construction (precinct 5)
- Possible future envelope (precinct 5)
- Development approval indicative envelope (precinct 5)



3D massing demonstrating built form and heritage recommendations for precinct 5

Attachment 3 - Amendment C231 - Hansen Built Form Review

DRAFT

View location 8: View from Queens Parade (north side) to the former UK Hotel

Existing condition

3D massing demonstrating built form and heritage recommendations for precinct 5

Key

- Existing development indicative envelope under construction (precinct 5)
- Possible future envelope (precinct 5)

Attachment 3 - Amendment C231 - Hansen Built Form Review

DRAFT

LOT WIDTH S | LOT DEPTH M | HERITAGE S

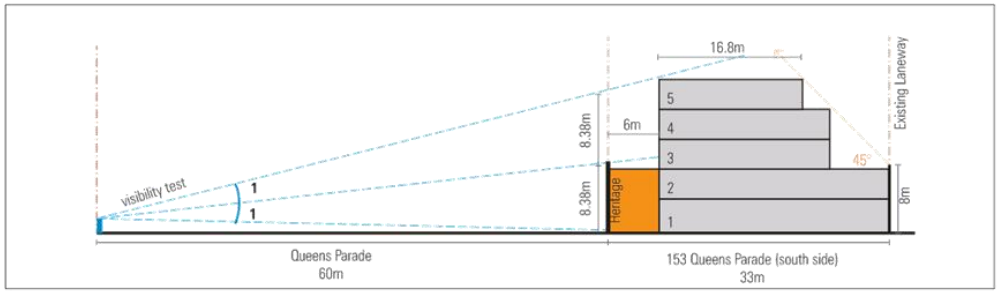
CASE STUDY 01: Precinct 4

Context	
Address	153 Queens Parade (south side)
Lot width	5m
Lot depth	33m
Heritage grading	Individually significant
Rear laneway	Yes
Rear interface	NRZ + HO

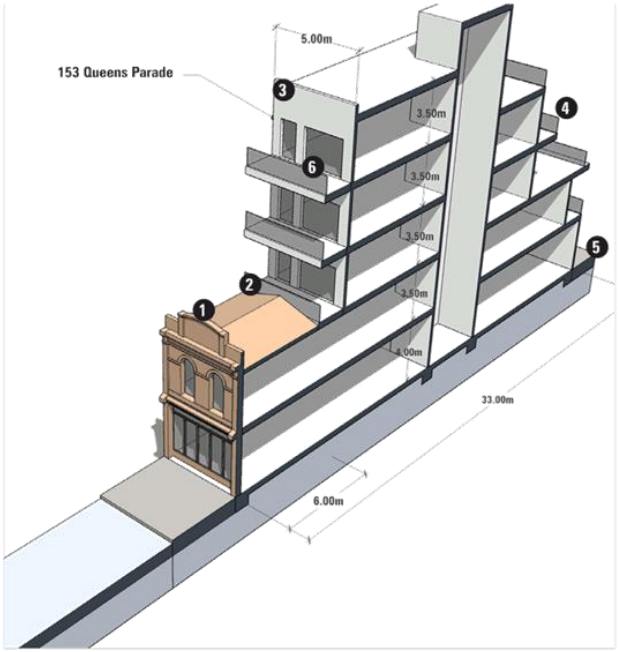
Precinct Guidelines	
1	Reinforce the heritage value of the precinct and support the retention of the traditional facade.
2	Retain the primacy of significant heritage form on Queens Parade.
3	Support infill development behind the traditional street wall that contributes positively to the urban character of Clifton Hill.
4	Ensure appropriate transition in scale to sensitive interfaces.
5	Encourage future vehicular access and services be provided of existing laneway.
6	Ensure high quality and sympathetic upper level elevations that are exposed to the public domain.

Discussion	
Heritage 'visibility' test*	Based on heritage 'visibility' test requirement of maximum 1:1 ratio (1 part heritage to 1 part new upper levels), viewed from the opposite side of Queens Parade (60m width), the recommended 6m setback from Queens Parade frontage will allow development height of up to 18m to be accommodated on the site.
Lot consolidation test	Should the site be consolidated to gain a wider street frontage, the limited lot depth cannot achieve a feasible floor plate depth above 5 storeys whilst retaining the necessary setbacks.
Rear interface test	The rear transition recommendations aim to minimise amenity impact onto residential properties along Hodgkinson Street (NRZ + HO). The 2D + 3D testings demonstrate that without site consolidation, a feasible development footprint of up to 5 storeys can be accommodated on the site.
Overshadowing test	The recommended 5 storey (18m) form and setback provisions can successfully minimise overshadowing impact to residential hinterland (NRZ), measured at the equinox.
Visual bulk test	The rear interface (to NRZ and laneway) will need to be carefully managed so as not to result in an overtly stepped building, or 'wedding cake' profile. The 3D modelling demonstrates this could be effectively managed by adopting 2 setback measurements to the rear.

*Note: * Refer to Queens Parade Built Form Heritage Analysis & Recommendations - GJM Heritage.*



Satellite image of existing condition
(source: google.com c.2017)



Attachment 3 - Amendment C231 - Hansen Built Form Review

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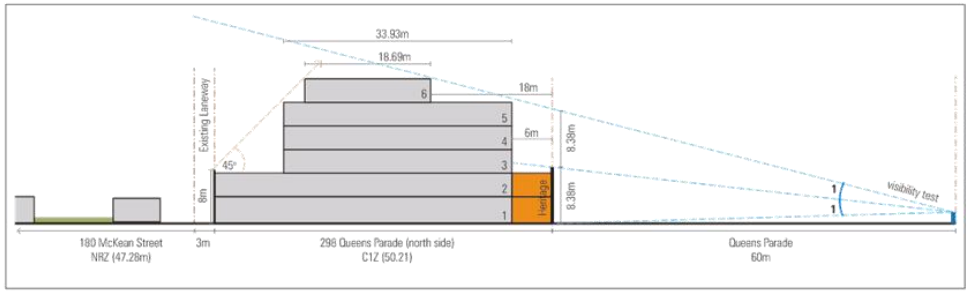
LOT WIDTH S | LOT DEPTH L | HERITAGE C

CASE STUDY 02: Precinct 4

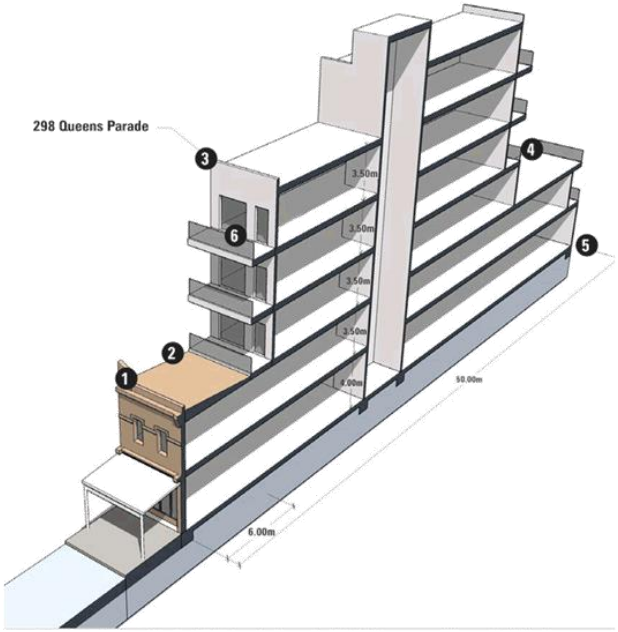
Context	
Address	298 Queens Parade (north side)
Lot width	4.65m
Lot depth	50.31m
Heritage grading	Contributory
Rear laneway	Yes
Rear interface	NRZ

Precinct Guidelines	
1	Reinforce the heritage value of the precinct and support the retention of the traditional facade.
2	Retain the primacy of significant heritage form on Queens Parade.
3	Support infill development behind the traditional street wall that contributes positively to the urban character of Clifton Hill.
4	Ensure appropriate transition in scale to sensitive interfaces.
5	Encourage future vehicular access and services be provided of existing laneway.
6	Ensure high quality and sympathetic upper level elevations that are exposed to the public domain.

Discussion	
Heritage 'visibility' test*	Based on heritage 'visibility' test requirement of maximum 1:1 ratio (1 part heritage to 1 part new upper levels), viewed from the opposite side of Queens Parade (60m width), the recommended minimum 6m setback from Queens Parade frontage can comfortably accommodate development height of up to 18m on this site. With greater setback from the Queens Parade frontage (greater than 16m), an additional floor can be accommodated (up to 21.5m) whilst meeting the 'visibility' test.
Lot consolidation test	Given the site depth (up to 50m), the site can accommodate development scale of up to 21.5m (6 storeys) without site consolidation.
Rear interface test	The rear transition recommendations aim to minimise amenity impact onto residential properties along Hodgkinson Street (NRZ + HO). The 2D + 3D testings demonstrate that without site consolidation, a feasible development footprint of 5-6 storeys can be accommodated on the site.
Overshadowing test	The site is located south of NRZ properties and does not result in unreasonable amenity impact (overshadowing) onto NRZ properties to the north.
Visual bulk test	The rear interface (to NRZ and laneway) will need to be carefully managed so as not to result in an overtly stepped building, or 'wedding cake' profile. The 3D modelling demonstrates this could be effectively managed by adopting 2 setback measurements to the rear.



Satellite image of existing condition (source: google.com c.2017)



*Note: * Refer to Queens Parade Built Form Heritage Analysis & Recommendations - GJM Heritage.*

Attachment 3 - Amendment C231 - Hansen Built Form Review

DRAFT

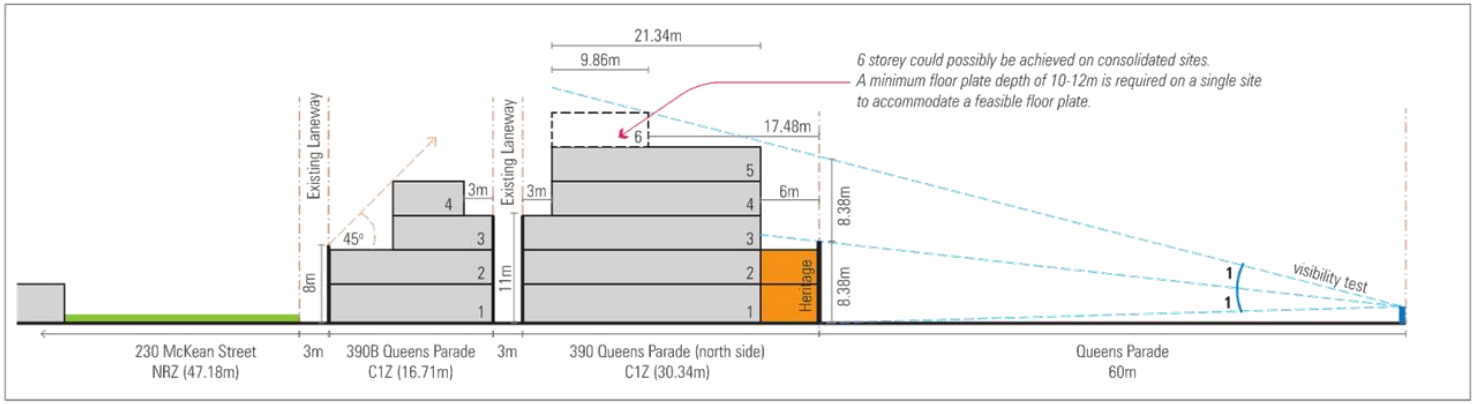
LOT WIDTH S | LOT DEPTH M | HERITAGE C

CASE STUDY 03: Precinct 4

Context	
Address	390 Queens Parade (north side)
Lot width	4.5m
Lot depth	30m
Heritage grading	Contributory
Rear laneway	Yes
Rear interface	C1Z

Context	
Address	390A Queens Parade (north side/ mid- block)
Lot width	approx. 50m (irregular)
Lot depth	approx. 16-17m (irregular)
Heritage grading	former warehouse (now substation) - contributory; balance of site - not contributory
Rear laneway	Yes
Rear interface	NRZ + HO

Satellite image of existing condition (source: google.com c.2017)



Attachment 3 - Amendment C231 - Hansen Built Form Review

DRAFT

LOT WIDTH S | LOT DEPTH M | HERITAGE C

CASE STUDY 03: Precinct 4 (continued)

Precinct Guidelines	
1	Reinforce the heritage value of the precinct and support the retention of the traditional facade.
2	Retain the primacy of significant heritage form on Queens Parade.
3	Support infill development behind the traditional street wall that contributes positively to the urban character of Clifton Hill.
4	Ensure appropriate transition in scale to sensitive interfaces.
5	Encourage future vehicular access and services be provided of existing laneway.
6	Ensure high quality and sympathetic upper level elevations that are exposed to the public domain.

Discussion - 390 Queens Parade	
Heritage 'visibility' test*	Based on heritage 'visibility' test requirement of maximum 1:1 ratio (1 part heritage to 1 part new upper levels), viewed from the opposite side of Queens Parade (60m width), the recommended 6m setback from Queens Parade frontage will allow development height of up to 18m to be accommodated on the site.
Lot consolidation test	The site's depth (30m) is insufficient to accommodate feasible floor plate (above 18m) whilst meeting the heritage 'visibility' test. Diagram B demonstrates that wider site frontage (9m) can be achieved through lot consolidation. There is opportunity to accommodate an additional floor with a floor plate depth of 9-10m to yield a feasible development footprint, increasing the overall building height to 21.5m.
Rear interface test	The rear transition recommendations aim to provide equitable development response to mid-block commercial allotment to the rear (C1Z + non contributory HO). The 2D + 3D testings demonstrate that without site consolidation, a feasible development footprint of up to 18m (5 storeys) can be accommodated on the site. Noting its rear interface to a laneway and C1Z, a less onerous setback (compared to standard B17) to the rear can be contemplated. In this instance, a minimum 4.5m setback from the laneway centreline (above 11m) will achieve the necessary 9m separation at the upper levels when replicated across the laneway.
Overshadowing test	The site is located south of NRZ properties and does not result in unreasonable amenity impact (overshadowing) onto NRZ properties to the north.
Visual bulk test	The rear interface can effectively be managed by adopting a single setback, which can be replicated on 390B Queens Parade.

*Note: * Refer to Queens Parade Built Form Heritage Analysis & Recommendations - GJM Heritage.*

Diagram A

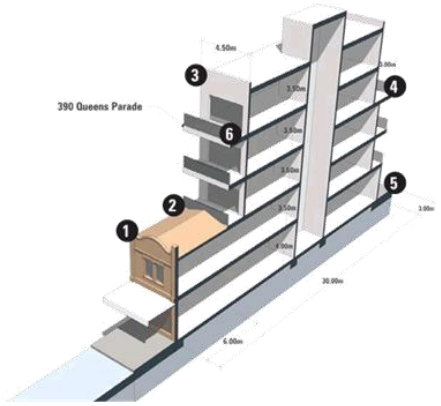
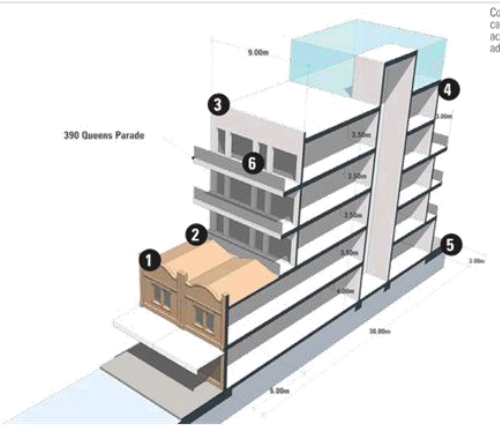


Diagram B



Attachment 3 - Amendment C231 - Hansen Built Form Review

DRAFT

LOT WIDTH S | LOT DEPTH M | HERITAGE C

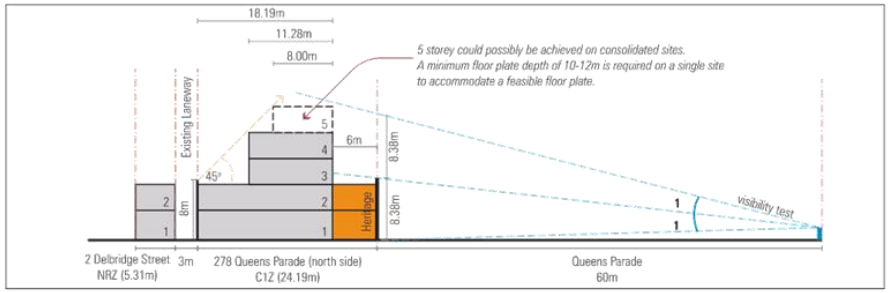
CASE STUDY 04: Precinct 4

Context	
Address	278 Queens Parade (north side)
Lot width	5.35m
Lot depth	24.19m
Heritage grading	Contributory
Rear laneway	Yes
Rear interface	NRZ

Precinct Guidelines	
1	Reinforce the heritage value of the precinct and support the retention of the traditional facade.
2	Retain the primacy of significant heritage form on Queens Parade.
3	Support infill development behind the traditional street wall that contributes positively to the urban character of Clifton Hill.
4	Ensure appropriate transition in scale to sensitive interfaces.
5	Encourage future vehicular access and services be provided of existing laneway.
6	Ensure high quality and sympathetic upper level elevations that are exposed to the public domain.

Discussion	
Heritage 'visibility' test*	Based on heritage 'visibility' test requirement of maximum 1:1 ratio (1 part heritage to 1 part new upper levels), viewed from the opposite side of Queens Parade (60m width), the preferred 18m (5 storeys) form is unlikely to be achieved on this site (if not consolidated).
Lot consolidation test	Whilst the site depth is limited, the preferred 18m (5 storeys) outcome can be achieved through lot consolidation to yield a wider site frontage. It demonstrates that on a consolidated site, a shallow but wide floor plate can be accommodated within the top floor, whilst meeting the 'visibility' and rear setback requirement.
Rear interface test	The rear transition recommendations aim to minimise amenity impact onto residential properties along Hodgkinson Street (NRZ + HO). It is noted that in this instance, the site's rear interface abuts a laneway and a side boundary (not rear boundary), with 1-2 storeys party wall with no window. Whilst this condition may change in the future, the existing interface condition is less sensitive with opportunity to vary the rear setback accordingly. The 2D + 3D testings demonstrate that without site consolidation, a feasible development footprint of up to 5 storeys can be accommodated on a consolidated site.
Overshadowing test	The site is located south of NRZ properties and does not result in unreasonable amenity impact (overshadowing) onto NRZ properties to the north.
Visual bulk test	The rear interface (to NRZ and laneway) will need to be carefully managed so as not to result in an overtly stepped building, or 'wedding cake' profile. The 3D modelling demonstrates this could be effectively managed by adopting a single setback measurement to the rear.

*Note: * Refer to Queens Parade Built Form Heritage Analysis & Recommendations - GJM Heritage.*



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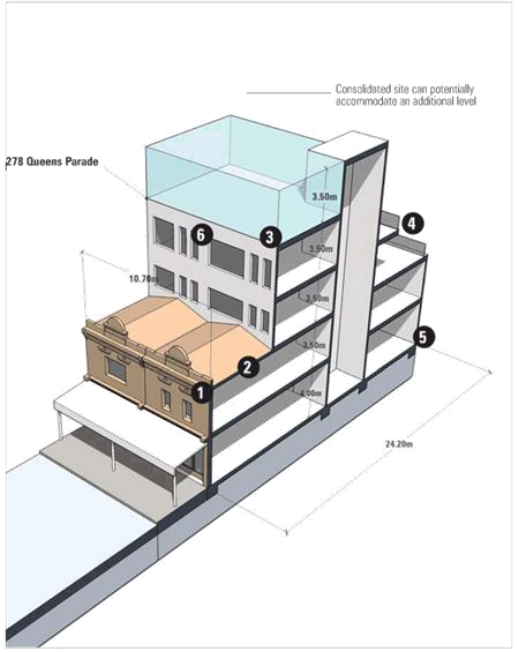


Attachment 3 - Amendment C231 - Hansen Built Form Review

APPENDIX A

DRAFT

LOT WIDTH **S** LOT DEPTH **M** HERITAGE **C**

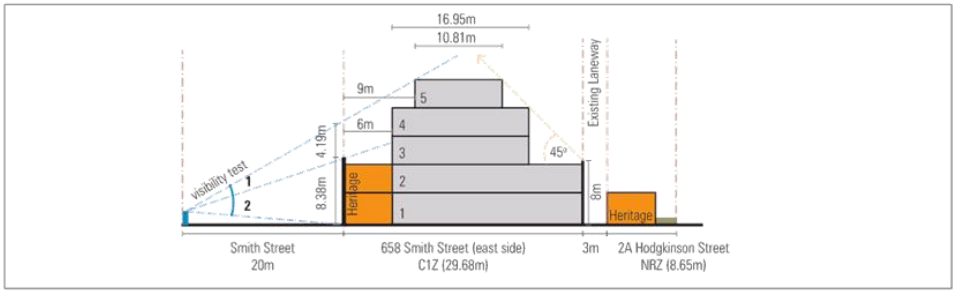
CASE STUDY 05: Precinct 3

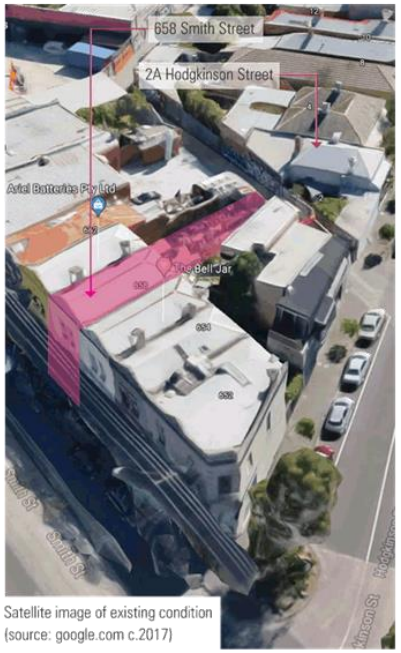
Context	
Address	658 Smith Street (east side)
Lot width	5.11m
Lot depth	29.68m
Heritage grading	Contributory
Rear laneway	Yes
Rear interface	NRZ + HO

Precinct Guidelines	
1	Support greater development intensity.
2	Reinforce the heritage value of the precinct and support the retention of traditional street frontages.
3	Ensure appropriate transition in scale in response to sensitive residential interface.
4	Encourage future vehicular access and services be provided of existing laneway.
5	Ensure high quality and sympathetic upper level elevations that are exposed to the public domain.

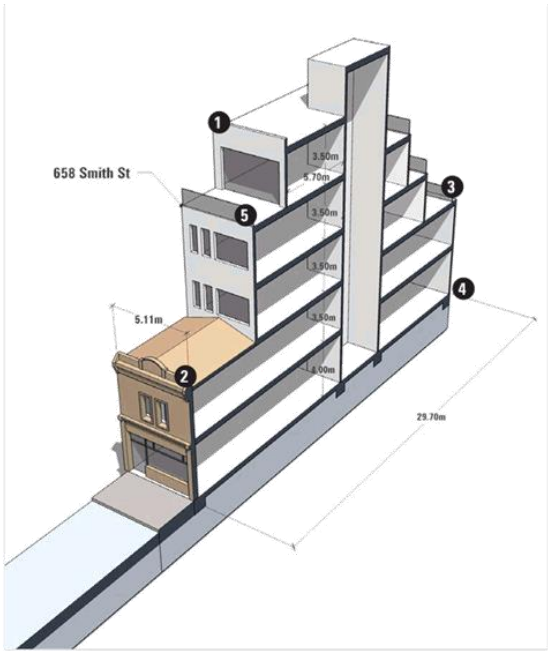
Discussion	
Heritage 'visibility' test*	Based on heritage 'visibility' test requirement of maximum 2:1 ratio (2 parts heritage to 1 part new upper levels), viewed from the opposite side of Smith Street (20m width), the preferred 18m (5 storeys) can be achieved with a setback of 6-9m above the heritage street wall.
Lot consolidation test	Given limited lot depth and decreasing lot width to the rear, there is limited opportunity for development greater than 5 storeys on a single, or consolidated sites.
Rear interface test	The rear transition recommendations aim to minimise amenity impact onto residential properties along Hodgkinson Street (NRZ + HO). The 2D + 3D testings demonstrate that without site consolidation, a feasible development footprint of up to 5 storeys can be accommodated on the site.
Overshadowing test	Development scale up to 18m (5 storeys), paired with the recommended setback provisions can successfully minimise overshadowing impact to residential hinterland (NRZ), measured at the equinox.
Visual bulk test	The rear interface (to NRZ and laneway) will need to be carefully managed so as not to result in an overtly stepped building, or 'wedding cake' profile. The 3D modelling demonstrates this could be effectively managed by adopting 2 setback measurements to the rear.

*Note: * Refer to Queens Parade Built Form Heritage Analysis & Recommendations - GJM Heritage.*





Satellite image of existing condition (source: google.com c.2017)



Attachment 3 - Amendment C231 - Hansen Built Form Review

Attachment 3 - Amendment C231 - Hansen Built Form Review

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QUEENS PARADE BUILT FORM POLICY REVIEW

State Planning Policy Framework

The State Planning Policy Framework (SPPF) sets out state level policies to guide planning in Victoria. The following provisions of the SPPF are considered relevant to the study area:

Clause 9: Plan Melbourne

The relevant objectives of Plan Melbourne include:

- To plan for the expanded central city to become Australia's Largest Commercial and Residential Centre by 2040
- To create a City of 20-minute neighbourhoods.
- To protect Melbourne and its suburbs from inappropriate development.
- To create neighbourhoods that support safe communities and healthy lifestyle.
- To respect our heritage as we build for the future.
- To achieve and promote design excellence.

Clause 11 – Settlement

The objective of Clause 11 is to ensure that planning responds appropriately to the needs of existing and future communities by providing for appropriately zoned and serviced land. It specifically identifies objectives and strategies relevant to the planning and development of activity centres. Relevant objectives include:

Clause 11.01-1 – Activity Centre Network

To build up activity centres as a focus for high-quality development, activity and living for the whole community by developing a network of activity centres.

Clause 11.01-2 – Activity Centre Planning

To encourage the concentration of major retail, residential, commercial, administrative, entertainment and cultural developments into activity centres which provide a variety of land uses and are highly accessible to the community.

Clause 15 – Built Environment and Heritage

Clause 15 ensures that new uses and developments respond appropriately in terms of built form, landscape values and cultural context and that significant places are protected. Good urban design should contribute positively to the local sense of place and urban character, reflects the identity and aspirations of the community, enhance the liveability and amenity of the public realm, and minimise the impact on neighbouring properties. Relevant objectives include:

Clause 15.01-1 – Urban Design

To create urban environments that are safe, functional and provide good quality environments with a sense of place and cultural identity.

Clause 15.01-2 – Urban Design Principles

To achieve architectural and urban design outcomes that contribute positively to local urban character and enhance the public realm while minimising detrimental impact on neighbouring properties.

Clause 15.01-5 – Cultural Identity and Neighbourhood Character

To recognise and protect cultural identity, neighbourhood character and sense of place.

Clause 15.03-1 – Heritage Conservation

To ensure the conservation of places of heritage significance.

Clause 16 – Housing

Clause 16 ensures the sustainable supply of housing in readily serviceable locations within walking distance to activity centres, public transport and open space. Relevant objectives include:

Clause 16.01-2 – Location of residential housing

To locate new housing in or close to activity centres and employment corridors and at other strategic redevelopment sites that offer good access to services and transport.

Clause 16.01-4 – Housing diversity

To provide for a range of housing types to meet increasingly diverse needs.

Local Planning Policy Framework

The Local Planning Policy Framework (LPPF) of the Yarra Planning Scheme contains the Municipal Strategic Statement (MSS) and Local Planning Policies which elaborate on and add local content to the objectives of the SPPF.

Municipal Strategic Statement

The MSS outlines the vision for the future development of the City of Yarra. It outlines how the municipality will accommodate the expected population growth through housing, employment and services and defines the objectives and strategies to achieve this vision. The objectives and strategies relate to the four categories of land use, built form, transport, and environmental sustainability. Relevant clauses are outlined below.

Clause 21.03 – Vision

The relevant objectives of the City of Yarra Vision include:
Built Form

- Yarra's historic fabric which demonstrates the development of metropolitan Melbourne will be internationally recognised;
- Yarra will have a distinctive identity as a low-rise urban form, with areas of higher development and highly valued landmarks; and
- All new development will demonstrate design excellence.

Clause 21.04 – Land Use

Clause 21.04-1 – Accommodation and housing

It is identified at clause 21.04-1 that, due to the established nature of residential areas in Yarra, the majority of new residential development is expected to be located on strategic redevelopment sites near activity centres as well as mixed use and commercial zones. Relevant objectives and strategies include:

Objective 1

- *To accommodate forecast increases in population.*

Strategy 1.1

- *Ensure that new residential development has proper regard for the strategies applicable to the neighbourhood in question identified in clause 21.08.*

Strategy 1.2

- *Direct higher density residential development to Strategic Redevelopment Sites identified at clause 21.08 and other sites identified through any structure plans or urban design frameworks.*

Strategy 1.3

- *Support residual population increases in established neighbourhoods.*

Objective 3

- *To reduce potential amenity conflicts between residential and other uses.*

Strategy 3.1

Attachment 3 - Amendment C231 - Hansen Built Form Review

- *Ensure new residential development in the Mixed Use, Business 1, Business 2, and Business 5 Zones and near Industrial and Business Zones is designed to minimise the potential negative amenity impacts of existing non-residential uses in the vicinity.*

Clause 21.04-2 Activity Centres

The focus of activity centres in Yarra is on creating vibrant communities through the presence of easily accessible service, retail and entertainment uses. Clause 21.04-2 places an emphasis on the need to ensure neighbourhood activity centres are capable of providing for the local day-to-day needs of the community. Relevant objectives and strategies include:

Objective 5

- *To maintain the long term viability of activity centres.*

Strategy 5.1

- *Limit expansion of the Business 1 Zone and new parcels of Business 1 zoning.*

Strategy 5.2

- *Support land use change and development that contributes to the adaptation, redevelopment and economic growth of existing activity centres.*

Strategy 5.3

- *Discourage uses at street level in activity centres which create dead frontages during the day.*

Strategy 5.4

- *Permit residential development that does not compromise the business function of activity centres.*

Clause 21.05 – Built Form

Clause 21.05-1 – Heritage

The large amount of heritage sites and places within the City of Yarra underpins the valued character of the municipality. Clause 21.05-1 outlines objectives and strategies to protect and enhance the heritage values of Yarra.

Objective 14

- *To protect and enhance Yarra's heritage places.*

Strategy 14.1

- *Conserve, protect and enhance identified sites and areas of heritage significance including pre-settlement ecological heritage.*

Strategy 14.2

- *Support the restoration of heritage places.*

Strategy 14.3

- *Protect the heritage skyline of heritage precincts.*

Strategy 14.5

- *Protect the significant landscape and heritage within streets, parks, gardens, waterways or other open spaces.*

Strategy 14.6

- *Protect buildings, streetscapes and precincts of heritage significance from the visual intrusion of built form both within places and from adjoining areas.*

Clause 21.05-2 – Urban Design

Clause 21.05-2 recognises the valued low-rise urban form of the City of Yarra. This urban form is punctuated by taller buildings located near activity centres as well as landmark buildings. Relevant objectives and strategies include:

Objective 16

- *To reinforce the existing urban framework of Yarra*

Strategy 16.2

- *Maintain and strengthen the preferred character of each Built Form Character Type within Yarra.*

Objective 17

- *To retain Yarra's identity as a low-rise urban form with pockets of higher development.*

Strategy 17.1

- *Ensure that development outside activity centres and not on Strategic Redevelopment Sites reflects the prevailing low-rise urban form.*

Strategy 17.2

Development on strategic redevelopment sites or within activity centres should generally be no more than 5-6 storeys unless it can be demonstrated that the proposal can achieve specific benefits such as:

- *Significant upper level setbacks*
- *Architectural design excellence*
- *Best practice environmental sustainability objectives in design and construction*
- *High quality restoration and adaptive re-use of heritage buildings*
- *Positive contribution to the enhancement of the public domain*
- *Provision of affordable housing.*

Objective 18

- *To retain, enhance and extend Yarra's fine grain street pattern.*

Strategy 18.1

- *Encourage the re-establishment of streets and laneways through new development sites where such links were part of the historic street pattern, except where this will cause detrimental traffic impacts.*

Objective 19

- *To create an inner city environment with landscaped beauty.*

Strategy 19.1

- *Require well resolved landscape plans for all new development.*

Strategy 19.2

- *Encourage opportunities for planting suitable trees and landscape areas in new development.*

Strategy 19.3

- *Encourage the retention of mature vegetation.*

Strategy 19.4

- *Protect mature and healthy flora species where they have heritage value or are a valued part of the character of an area*

Attachment 3 - Amendment C231 - Hansen Built Form Review

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Objective 20

- *To ensure that new development contributes positively to Yarra's urban fabric.*

Strategy 20.1

- *Ensure development is designed having particular regard to its urban context and specifically designed following a thorough analysis of the site, the neighbouring properties and its environs.*

Strategy 20.2

- *Require development of Strategic Redevelopment Sites to take into account the opportunities for development on adjoining land.*

Strategy 20.3

- *Reflect the fine grain of the subdivision pattern in building design where this is part of the original character of the area.*

Objective 21

- *To enhance the built form character of Yarra's activity centres.*

Strategy 21.1

- *Require development within Yarra's activity centres to respect and not dominate existing built form.*

Strategy 21.2

- *Require new development within an activity centre to consider the context of the whole centre recognising that activity centres may consist of subprecincts, each of which may have a different land use and built form character.*

Strategy 21.3

- *Support new development that contributes to the consolidation and viability of existing activity centres*

Clause 21.08 – Neighbourhoods

Clause 21.08-4 – Clifton Hill

Clause 21.08-4 identifies part the study area as the Queens Parade Neighbourhood Activity Centre within Clifton Hill. It is characterised as a mixed use centre focused on convenience retailing. There is limited scope for intense development of this centre.

Clause 21.08-4 seeks to facilitate the implementation of built form strategies in clause 21.05 includes:

- *Supporting development that maintains and strengthens the preferred character of the relevant Built Form Character type.*
- *Encouraging the redevelopment of the following sites in a way that contributes positively to the urban fabric and public domain of Yarra, and where subject to a Heritage Overlay, protects the heritage of the site and of the area: Site 1 94 Alexandra Parade, Clifton Hill (shot tower site), Site 2 135 – 169 Noone Street (Australian Dyeing Company Precinct), Site 3 Dummet Crescent.*
- *Strengthening the built form of land adjacent to Alexandra Parade*
- *Maintaining the visual prominence of the Spire of St Johns and the Shot Tower*

Local Planning Policies

Clause 22.02 – Development Guidelines for Sites Subject to the Heritage Overlay

Provides guidance for the protection and enhancement of heritage places in the City of Yarra. Objectives include:

- *To conserve Yarra's natural and cultural heritage.*
- *To conserve the historic fabric and maintain the integrity of places of cultural heritage significance.*
- *To retain significant view lines to, and vistas of, heritage places.*
- *To preserve the scale and pattern of streetscapes in heritage places.*
- *To encourage the preservation, maintenance, restoration and where appropriate, reconstruction of heritage places.*
- *To ensure the adaptation of heritage places is consistent with the principles of good conservation practice.*
- *To ensure that additions and new works to a heritage place respect the significance of the place.*
- *To encourage the retention of 'individually significant' and 'contributory' heritage places.*
- *To protect archaeological sites of cultural heritage significance.*

Clause 22.03 – Landmarks and tall structures

The purpose of Clause 22.03 is to ensure that important landmarks and tall structures within the City of Yarra are retained and view lines towards these sites are maintained and the silhouettes/profiles are protected.

The objective includes:

- *To maintain the prominence of Yarra's valued landmarks and landmark signs.*

The policy identifies the spire of St Johns Church on Queens Parade, the Shot tower on Alexandra Parade and St Patrick's Cathedral at the intersection of Brunswick Street and St Georges Road as significant landmarks within and nearby to the study area.

Clause 22.05 – Interface Uses Policy

Clause 22.05 provides guidelines for the development of diverse land mixes to reduce conflict between commercial, industrial and residential activities. Objectives include:

- *To enable the development of new residential uses within and close to activity centres, near industrial areas and in mixed use areas while not impeding the growth and operation of these areas as service, economic and employment nodes.*
- *To ensure that residential uses located within or near commercial centres or near industrial uses enjoy a reasonable level of amenity.*

Clause 22.07 – Development Abutting Laneways

Recognising the importance of retaining access to laneways, Clause 22.07 provides guidance for development abutting laneways. Objectives include:

- *To provide an environment which has a feeling of safety for users of the laneway.*
- *To ensure that development along a laneway acknowledges the unique character of the laneway.*
- *To ensure that where development is accessed off a laneway, all services can be provided to the development.*
- *To ensure that development along a laneway is provided with safe pedestrian and vehicular access.*

Clause 22.10 – Built Form and Design Policy

Clause 22.10 applies to new development not affected by the Heritage Overlay. Objectives include:

- *Ensure that new development positively responds to the context of the development and respects the scale and form of surrounding development where this is a valued feature of the neighbourhood character.*
- *Ensure that new development makes a positive contribution to the streetscape through high standards in architecture and urban design.*
- *Limit the impact of new development on the amenity of surrounding land, particularly residential land.*
- *Design buildings to increase the safety, convenience, attractiveness, inclusiveness, accessibility and 'walkability' of the City's streets and public spaces.*
- *Create a positive interface between the private domain and public spaces.*
- *Encourage environmentally sustainable development.*

22.10-3.2 Urban form and character

Design objectives include:

- *To retain and extend the City's fine grain of street pattern and urban form.*
- *To ensure that developments contribute positively to the urban fabric and public realm.*

Attachment 3 - Amendment C231 - Hansen Built Form Review

- *To improve the transparency and legibility of the city's urban form and structure.*
- *To achieve continuity in the built form having regard to rhythm and spacing of buildings and any distinctive street pattern (as identified in the Site Analysis Plan and Design Response).*

22.10-3.3 Setbacks & Building Height

Design objectives include:

- *To ensure that the setbacks of new development complement the desired neighbourhood character of the area (as identified in the Site Analysis Plan and Design Response, the Municipal Strategic Statement and any relevant local planning policies).*
- *To ensure that the height of new development is appropriate to the context of the area (as identified in the Site Analysis Plan and Design Response) and respects the prevailing pattern of heights of the area where this is a positive contribution to neighbourhood character.*
- *To ensure new roof forms respect any prevailing roof forms in the area (as identified in the Site Analysis Plan and Design Response) and contribute to the skyline silhouette.*

Zoning

The study area is characterised by a mix of zones. A summary of each follows.

Commercial 1 Zone (C1Z)

The north east of Queens Parade (surrounding the Heidelberg Road junction) is subject to the Commercial 1 Zone (C1Z). The C1Z seeks to:

- *Create vibrant mixed use commercial centres for retail, office, business, entertainment and community uses.*
- *Provide for residential uses at densities complementary to the role and scale of the commercial centre*

Commercial 2 Zone (C2Z)

Land subject to the Commercial 2 Zone is located to the south west of the study area along Queens Parade and Alexander Parade. The C2Z prohibits residential use and seeks to

- *encourage commercial areas for offices, appropriate manufacturing and industries, bulky goods retailing, other retail uses, and associated business and commercial services.*
- *ensure that uses do not affect the safety and amenity of adjacent, more sensitive uses*

Mixed Use Zone (MUZ)

The MUZ is applied to the northeast and southwest of the study area along Queens Parade. The MUZ seeks to:

- *Provide for a range of residential, commercial, industrial and other uses which complement the mixed-use function of the locality.*
- *Provide for housing at higher densities.*
- *Encourage development that responds to the existing or preferred neighbourhood character of the area.*
- *Facilitate the use, development and redevelopment of land in accordance with the objectives specified in a schedule to this zone.*

General Residential Zone (GRZ)

Small pockets of land within the study area are located within the General Residential Zone (GRZ). The GRZ seeks to

- *encourage development that respects the neighbourhood character of the area.*
- *implement neighbourhood character policy and adopted neighbourhood character guidelines.*
- *provide a diversity of housing types and moderate housing growth in locations offering good access to services and transport.*
- *allow educational, recreational, religious, community and a limited range of other nonresidential uses to serve local community needs in appropriate locations.*

Neighbourhood Residential Zone (NRZ)

The majority of residential land within the study area is located within the Neighbourhood Residential Zone (NRZ). The NRZ seeks to:

- *Recognise areas of predominantly single and double storey residential development.*
- *Limit opportunities for increased residential development.*
- *Manage and ensure that development respects the identified neighbourhood character, heritage, environmental or landscape characteristics.*
- *Implement neighbourhood character policy and adopted neighbourhood character guidelines.*
- *Allow educational, recreational, religious, community and a limited range of other nonresidential uses to serve local community needs in appropriate locations.*

.Public Park and Recreation Zone (PPRZ)

The Public Park and Recreation Zone (PPRZ) applies to areas designated for public recreation and open space.

Public Use Zone (PUZ)

The PUZ applies to land to the southwest of the study area and seeks to:

- *Recognise public land use for public utility and community services and facilities.*
- *Provide for associated uses that are consistent with the intent of the public land reservation or purpose.*

Overlays

Heritage Overlay (HO)

The study area contains a number of heritage overlays covering both heritage precincts and individual buildings and sites.

Queens Parade Precinct (HO330)

HO330 covers the main commercial strip of Queens Parade and includes contributory, non-contributory and individual significant buildings. The statement of significance identifies the following features as significant to the precinct:

- *As one of Melbourne's early surveyed boulevards (1853) and one of the few associated with the prominent surveyor Robert Hoddle, enhanced by mature street trees (elms) and extensive beautification in the inter-war period in the form of plane trees and rockeries;*
- *For the contribution of the old government surveyed town of Northcote (part), as a valuable historical depiction of Melbourne's early relationship to then nearby country districts, with early extant buildings such as the Terminus Hotel at the Merri Creek bridge (in use since 1854), substantial 1860s row houses and other indications of residential development prior to the government suburban subdivisions and their development in the 1880s Boom era;*
- *For the late 19th and early 20th century shops, hotels, banks, and public buildings, often with intact street verandahs, lining both sides of a wide boulevard that provides for exceptional vistas to fine commercial rows, and demonstrates the major role of Queens Parade as a commercial, banking, accommodation and public transport nucleus for the district;*
- *As a good representation of building styles from the mid-19th century to the late 1930s, enhanced by prominent landmarks such as the former banks, St. John's Catholic Church complex, and the United Kingdom Hotel, the latter two being primary foci in the precinct;*
- *For the remarkable collection of original street verandahs particularly on the north side of the street;*
- *For the picturesque shop-row skyline, visible from across Queens Parade, with its gabled or hipped roof forms and many original chimneys; and*
- *For fine groups of early 20th century villas, set in gardens, in the residential areas to the west and north-east ends of Queens Parade.*

North Fitzroy Precinct (HO327)

HO327 covers the northern residential section of the study area.

- *For the clear indication of important early land development phases in the suburb as follows:*

Attachment 3 - Amendment C231 - Hansen Built Form Review

DRAFT

- *East-west, relatively long and narrow streets originating from private subdivision of the 1839 agricultural allotments that originally fronted Merri Creek;*
- *The evident effect of the 19th century Boom era when aggressive subdivision coincided with provision of public transport, creating dense late-Victorian development near the Nicholson St cable tram terminus;*
- *The more generous post-1906 subdivisions (suiting the construction of detached timber villas) representing changed values in land prices, and the relatively new suburban ideal of affordable house and garden lifestyle;*
- *The slow transformation into a completed residential suburb by WW2, with the last few vacant sites accommodating inter-war dwellings, some developed with medium-density housing, duplexes and flats.*

- *For the consistent and intact streetscapes of late Victorian terrace and row housing in the south-west corner of the area, and Italianate style and Edwardian timber villas, interspersed occasionally with inter-war houses, in the balance of the area;*
- *For the visual consistency and good period expression of streets in the north and east parts of the area, consistently one-storey, timber construction, decorative detail, and detached siting with consistent garden setbacks;*
- *For the interwar medium density development pockets, including those contained within Pilkington St and parts of Barkly St, King and Miller Streets, significant for its intactness, form and high quality design.*
- *For the shopping strip streetscape of St Georges Rd, significant for its landmark Victorian-era buildings, plus intact early 20th century and interwar buildings in both Classical Revival and Moderne styles; and*
- *For important landscape and vistas, including the view of Merri Creek Bridge framed by palms and mature elms in the Merri Primary School reserve, and mature street plantings of plane trees.*

Clifton Hill Western Precinct (HO317)

The southern section of the study area is protected by HO317. The statement of significance outlines the following:

- *As an expression, in the history of its creation as part of Collingwood East, of the aspirations of tradesmen of the Collingwood Slope for improved urban development and the provision of public facilities;*
- *For the role of local politicians and business figures in social, physical and industrial improvement and home ownership in the area;*
- *As the focus of community sentiment and resident protest from its initial development in the 1860s, from the actions of the Victorian-era Vigilance Committee to the preservation movements of the 20thCentury, specifically the House of the Gentle Bunyip conservation campaign;*
- *For the distinctive street and allotment layout created under Government surveyor, Clement Hodgkinson (43), and the liberal allowance for garden squares and reserves for a market, public baths, a school, a recreation reserve and five churches, contrasting with private surveys in the City of Yarra;*
- *For Hoddle's provision of boulevards (Hoddle Street and Queen's Parade) skilfully merged with wide streets on a north south grid, with unusual five way intersections and triangular public spaces at each end of Queen's Parade;*
- *For the unique series of vistas from the crest of the hill at the end of Hoddle street that includes views of significant landmarks like the shot tower, spires of St. Johns and the former Wesleyan Church, Town Hall towers at Collingwood and Fitzroy, the Exhibition Buildings, and long vistas to the Kew hill and Studley Park, plus those to and from the Darling Gardens;*
- *For the area's fine representation of Victorian-era life, as expressed by the street and public reserves, along with the dominantly Victorian-era building stock including churches, a State school, factories, early houses, a large number of late Victorian terrace houses, attached and free standing, and key house designs such as 12 North Terrace.*
- *For its focus on a representative and intact example of a residential garden square (Darling Gardens) based on British and European precedents, with associated significant trees;*
- *For the mature, largely exotic trees, including those planted by George David Langridge, MLA and former Mayor, bluestone kerbs, gutters, drains, channelling, and laneways from the area's major period of development;*
- *For the strong industrial theme, seen in the south of the area with several 19th Century industrial buildings including former boot factories, and a carriage factory, that links the area with the City's other creek and riverside industrial localities such as Clifton Hill East, Abbotsford and Collingwood;*
- *For well preserved Edwardian housing estates and individual houses as complements to the dominant Victorian-era residences; and*
- *For good examples of the Californian Bungalow style and other well preserved interwar houses.*

Other sites of heritage significance within the study area include:

- Queens Parade Street Trees (HO93)
- Elm & Plane double avenue, street trees (HO93)
- Haselmere, 96 McKean Street, North Fitzroy (HO216)
- Former ANZ Bank, 370 Queens Parade, Clifton Hill (HO221)
- Former United Kingdom Hotel, 199 Queens Parade, Clifton Hill (HO92)
- Baptist Church and Hall, 100 Hodgkinson Street, Clifton Hill (HO317)

Environmental Audit Overlay (EAO)

The EAO affects a number of the sites within the study area. The EAO applies to land that may potentially be contaminated and requires assessment before development.

Special Building Overlay (SBO)

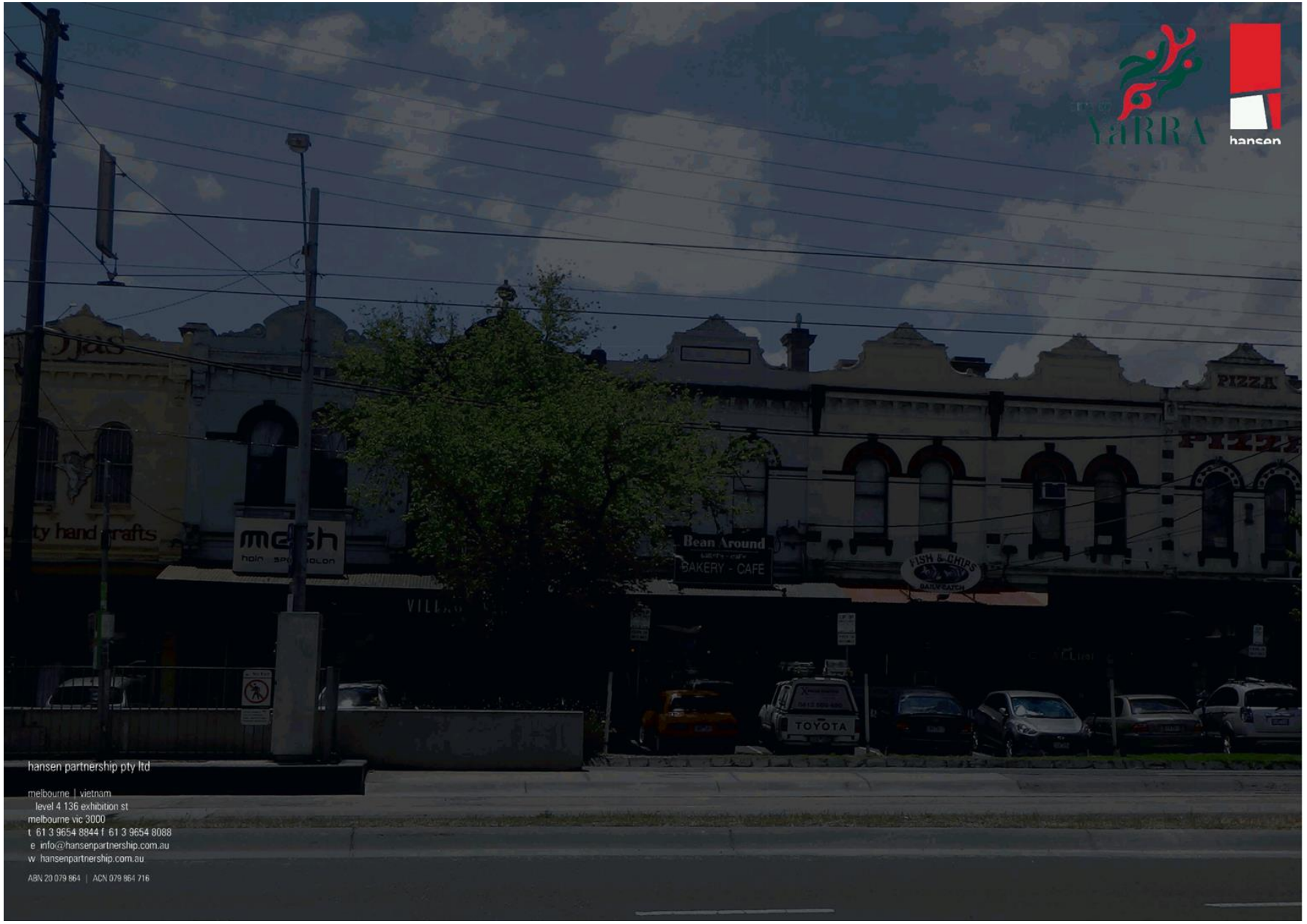
The SBO applies to land identified as liable to inundation from floodwaters and requires new development to allow for the free passage of floodwaters and to minimise flood damage.

Design and Development Overlay – Schedule 2(DDO2)

A portion of the study area along Alexandra Parade is affected by the DDO2 which applies to main roads and boulevards within the municipality, providing design objectives and guidelines. The key objectives include:

- To recognise the importance of main roads to the image of the City.
- To retain existing streetscapes and places of cultural heritage significance and encourage retention of historic buildings and features which contribute to their identity.
- To reinforce and enhance the distinctive heritage qualities of main roads and boulevards.
- To recognise and reinforce the pattern of development and the character of the street, including traditional lot width, in building design.
- To encourage high quality contemporary architecture.
- To encourage urban design that provides for a high level of community safety and comfort.
- To limit visual clutter.
- To maintain and where needed, create, a high level of amenity to adjacent residential uses through the design, height and form of proposed development.

Attachment 3 - Amendment C231 - Hansen Built Form Review



11.10 Review - Heritage Advisory Committee and the Terms of Reference

Trim Record Number: D17/199663
Responsible Officer: Group Manager Chief Executive's Office

Purpose

1. To:
   (a) further consider the Heritage Advisory Committee Review Report (2017) prepared by Beverley Kliger and Associates on the effectiveness of the committee and make recommendations to Council about the future of the committee; and
   (b) to review the Terms of Reference for the Heritage Advisory Committee.

Background

2. The Heritage Advisory Committee (HAC) was established in 2013 for a 4 year term to provide advice to Council on strategic heritage matters, the promotion of heritage and to provide advice on protection of the natural, built and cultural heritage in the City of Yarra.
3. HAC comprises 12 community representatives and a nominee from the National Trust. Membership is for two years with the possibility of serving four years. The HAC meets bi-monthly and is chaired by a Councillor. Currently, more than half of the initial members remain on the current HAC committee.
4. The terms of reference were initially adopted by Council in April 2013 and revised in July 2014.
5. Council in September 2017 resolved:

   "That Council notes the officer report on the Review of the Heritage Advisory Committee (HAC) prepared by Beverley Kliger and Associates and its findings and recommendations.

   That Council, having considered the HAC Review and the officer recommendations, resolves to:
   (a) defer consideration of the matter for a month in order to enable further input by the Heritage Advisory Committee and other members of the community; and
   (b) extend the term of the existing HAC until a new Heritage Strategy Advisory Committee is appointed."

HAC Review 2017

6. In early 2017 Beverley Kliger & Associates was commissioned by the City of Yarra to review the effectiveness of the Yarra Heritage Advisory Committee (HAC) and make recommendations to Council on the future of the committee.
7. The review involved consideration of:
   (a) The HAC Terms of Reference;
   (b) The membership of the HAC and committee processes;
   (c) The HAC Agenda and Minutes to understand attendance and range of topics discussed;
   (d) Reporting of HAC to full Council;
   (e) The Yarra Heritage Strategy and Implementation Plan (2014-2018);
   (f) HAC's role in relation to the Yarra Heritage Strategy and implementation actions; and
   (g) Consultation with Councillors, Council staff and HAC members.

8. Managing heritage at a local level is complex. Local governments need to identify, conserve and manage significant physical and cultural heritage whilst ensuring the municipality is vibrant and experiences social and economic growth and development to meet the needs of its growing community.

9. The review found there are differing and strong opinions about Council's role in heritage. The purpose of advisory committees is to provide Council with access to expert advice and engage local stakeholders in strategy and policy development.

Proposal for Future Directions for HAC

10. The HAC Review Report (July 2017) **(Attachment 1)** presents 3 options for the future direction of the HAC (excerpt from report).

Option One – Maintain the Current Heritage Advisory Committee

11. There are problems with the current operation of the Heritage Advisory Committee in that it spends too much time on local issues (without a strategic focus). In addition the current membership, with its emphasis on resident groups means current consultation is more akin to community consultation, rather than a heritage advisory group providing peer review.

Option Two – Adopt a New Reconstituted Heritage Advisory Committee

12. A reconstituted HAC could operate more effectively by introducing changes including membership and redirecting the Committee to operate in accordance with a revised term of reference. A renewed emphasis on policy could ensure that the City of Yarra's heritage strategy and polices meet best practice.

13. It would also ensure the Committee is project and policy focussed with a specified lifespan. A reconstituted Committee would also offer the opportunity to include other professional expertise interests such as the property sector, a conservation planner, archaeologist, or historian aboriginal representative, and landscape architect/specialist.

14. It is considered that the Council would benefit from the advice of an independent, multi-disciplinary, professional and expert advisory committee, to weigh up competing evidence, analysis and information when it arises.

Option Three – Dissolve the Current Heritage Advisory Committee

15. Dissolving the current HAC would not limit Council's ability to consult broadly on heritage matters with a range of experts and the wider community as it is not the only means of consulting with these groups.

16. The options were presented to the HAC in 2017; the committee members were keen to retain the HAC with some changes.

17. The report focusses on Option 2 and provides specific recommendations for a renewed Heritage Advisory Committee to provide effective strategic advice to Council by revising the Terms of Reference, renewing the committee membership, providing a structured approach to the committee's operations and changing the name of the committee to the Heritage Strategy Advisory Committee.

18. The report concludes that the committee's assistance in developing the Heritage Strategy has provided that value in the past. The operation of the committee in the future needs to continue to add that value. In order to do that, a restructuring of the HAC is highly desirable.

HAC Review Recommendations

19. The Review Report (July 2017) provides 17 recommendations for the future of the committee outlining specific changes to the Terms of Reference, membership and committee processes and to clarify the role of the committee in the implementation of the Heritage Strategy actions.

Future of the committee

20. The report emphasises that there is a role for the committee in providing strategic heritage advice to Council, in a more clearly defined manner. The report supports the strategic role and value of the committee which can be enhanced by changing the name of the committee to more clearly nominate its strategic role.

21. Responses to the review included a range of views about the change to the committee name with some HAC members supporting retention of the current name, some supporting including reference to 'strategy' or 'strategic' in the new committee's name. The inclusion of a reference to the strategic purpose of the committee would help to convey the scope of the committee's work and is considered

Recommendation

a. Appoint a Heritage Strategy Advisory Committee for a four-year term with quarterly meetings

Terms of Reference (Purpose and Objectives)

22. The current Terms of Reference whilst intended to focus on strategic matters, are extremely broad and appear to focus on built heritage. The breadth of the terms of reference may have contributed to differing views on the role of the HAC by its members. Hence, a renewed Heritage Advisory Committee should have more concise terms of reference that clarify the committee's strategic focus and provide an outline of the expectations regarding roles and responsibilities of committee members, Council officers and Councillors.

23. I have revised the Terms of Reference and note from a governance perspective it should be made clear as to the purpose, expectations and role of the HAC. – refer **Attachment 3**.

Recommendation

a. Revise and simplify the purpose and objectives of the committee's terms of reference.

Membership

24. To ensure that the HAC provides effective advice to Council requires a renewal of the membership as follows:

    (a) attracting members with professional expertise across a range of disciplines such as history, landscape and culture; and

    (b) members should be from representatives of local heritage groups, in line with the terms of reference which are written with the intention that community members are representatives of local heritage/community groups.

25. The proposed changes seek both continuity within the committee and generation of new ideas and direction by encouraging a mix of new and existing members. Renewal of membership encourages diversity of expertise and possibility of new strategic considerations being provided to the Council.

Recommendations for the membership of HAC

a. The committee membership be a mix of those drawn from local heritage and community groups and at minimum a third of members have specific expertise from a range of sectors (conservation architecture, landscape architecture, aboriginal heritage, history and cultural planning).

b. Encourage a mix of new and existing members for the committee membership and require all existing committee members seeking another term to re-nominate

c. Ensure committee members with long standing involvement in, or who hold a position in a local heritage /community groups are representative of the local heritage /community group at the HAC.

Operations of the HAC

Meetings

26. It is good practice to set both annual and meeting objectives for an advisory committee. The committee currently meets for two hours bi-monthly, reducing the meetings to quarterly would still maintain the integrity of the committee but remove the resourcing requirements for one meeting per year. To ensure maximum value is gained from the meeting clear goals should be set for each year for the committee. If, as in 2017, Council is aware of a major heritage policy review (Heritage Policy in the Yarra Planning Scheme) the committee's advice on this policy can be clearly set out as a goal for the year and then become a priority action for meetings.

27. Involving the HAC meeting's chairperson in the development of the meeting agenda, objectives, and intended outcome(s) should assist the chairperson in their role of managing the meeting and committee members' expectations. This should be managed jointly with the officer coordinating the HAC and with the input from committee members.

Delegate Reporting to Council

28. The recommendations of the committee are intended to assist Council in making informed decisions on complex strategic and policy matters. Of critical importance from a governance perspective, as an advisory committee, it only has the capacity to 'advise' or' refer' matters to Council for a decision.

29. More recently the HAC chair has provided a delegate's report to a Council meeting as a means of informing the Council of the Committee's deliberations and any recommendations or advice. This appears to be effective in raising the profile of the HAC and its work and as such, reporting to the Council meeting via a Councillors Delegate Report is considered appropriate.

Coordination by Council officers

30. Responsibility for the HAC and Heritage Strategy Implementation Plan has to date been with the Strategic Planning team. There has been significant achievement over 10 years of strategic planning work to implement improved heritage protections into the Yarra Planning Scheme. Now that Council has appointed a Senior Heritage Advisor the future the actions in the Implementation Plan will be more aligned with a number of other Council departments. Monitoring and resourcing the Implementation Plan will be very important.

31. The unofficial allocation of officer time to coordinate the HAC and lead the progress of actions in Heritage Strategy Implementation Plan could be formalised to provide clarity.

32. The review has identified opportunities for improving the operations of the committee through setting annual goals, meeting objectives and including the chair in the preparation of the committee meeting agenda.

Recommendations for operational improvements for HAC:

a. The committee sets realistic and measurable annual goals at its first meeting to guide the focus and priorities for the year in order to maximise its effectiveness.

b. Meeting objectives with clear outcomes be set for each meeting and be set out in the agenda.

c. Monitor the progress of the achievement of the annual goals at each meeting.

d. The role of the Chairperson be expanded to include participation in planning the agenda and setting the objectives and proposed outcome for each committee meeting

e. Formalise the staff resourcing of the HAC and coordination of the Heritage Strategy and Implementation Plan.

f. All new agenda items be accompanied by background information and statement of the key issue(s) requiring consideration by the committee and be circulated, at minimum, a week prior to the meeting. This includes providing committee members with the opportunity to nominate agenda items for meetings.

g. Clarify the role and responsibility of the committee members progressing the actions in the Heritage Strategy Implementation Plan.

h. A Councillor Delegate Report be presented to Council reporting on the activities of the committee after each meeting.

HAC Role in the Heritage Strategy

33. The Heritage Strategy needs a strategic review and as it expires in 2018, the Strategy to be redrafted with fewer objectives and provide greater clarity on priorities and associated resources to support implementation and set more realistic expectations of the role of volunteer committee members. It should avoid addressing Council's operational matters. Similarly, the promotion objectives and priorities should be reviewed in line with priorities and associated resources to support implementation. This review would enable consideration of inclusion of promotion of aboriginal cultural heritage. The redrafted Heritage Strategy should include a flow chart setting out the relationship between the Heritage Strategy, the Council Plan and other key Council policies.

34. The revised Heritage Strategy should set clear priorities and outline the responsibilities and resourcing for the committee and relevant Council departments. The current Heritage Strategy expires in 2018 and a review/renewal of the Heritage Strategy is proposed for the latter part of 2018/19.

35. <u>Recommendations for the Heritage Strategy</u>

a. The Heritage Strategy be reviewed and revised to ensure achievement of the objectives. Clear priorities to be set for achievement of objectives.

b. The roles and responsibilities of the committee and relevant Council departments, priorities and resourcing for the delivery of the Heritage Strategy be made clear as part of setting realistic implementation actions.

c. The heritage committee contribute to the implementation of the Heritage Strategy and receive reports on the status of implementation activity.

d. The committee contribute to the Heritage Strategy review and update in 2018/19

External Consultation

36. Telephone interviews were held with current members who participated in the HAC prior to 2017 and two professional expert members who have resigned from the committee. An interview guide was used to ensure all interviews encompassed the same issues. Telephone interviews were also conducted with Councillor members of the HAC, two current members and a retired Councillor who had been on the HAC from its inception.

37. A brief workshop was held with the HAC members in March 2017 regarding the possible future direction for the HAC. This workshop followed a short presentation of the findings of interviews with HAC community members and City of Yarra staff who have supported the HAC.

38. The final HAC Review report was circulated to members for comment and was discussed at the HAC meeting in July 2017 and again in September 2017. Four members had provided written comments. There was general support for the recommendations. A summary of their comments is included in the attachment to this report **(Attachment 2).**

Internal Consultation (One Yarra)

39. A group discussion was held with strategic planning Council officers who have worked with and supported the HAC. Also, a telephone interview with the former Coordinator of Strategic Planning who held this position at the time the HAC was established.

Financial Implications

40. The financial implications are the resource costs to Council to coordinate the committee.

41. Currently, Council invests significant resources in terms of officer time and funding to support the operation of the committee and implementation of the Heritage Strategy and Implementation Plan.

42. For 2016/17 the HAC had a budget of $70,000 for Heritage Strategy implementation actions and Council contributed $50,000 to the Victorian Heritage Restoration Fund.

43. This is a substantial investment in Heritage activities for Council.

44. The current level of resourcing would be appropriate to implement the recommendations from this review regarding: the remit of the committee; membership; and operational improvements; as well as review of the Heritage Strategy in the context of what can realistically be achieved in each year.

45. It is also important to note that Council resolved, at its meeting on 1 August 2017, to appoint a dedicated in-house Heritage Adviser from existing staffing resources.

46. This arrangement has been implemented and the Officer has taken up her role.

Economic Implications

47. There are no known economic implications from considering the future of the HAC.

Sustainability Implications

48. There are no known sustainability implications from considering the future of the HAC.

Social Implications

49. The social implications from considering the future of the HAC include the potential for a range of community members and professionals in Yarra with an interest and/or expertise in Heritage and links to heritage groups to be involved in providing strategic advice to Council through the committee.

Human Rights Implications

50. There are no known human rights implications.

Communications with CALD Communities Implications

51. The review involved engagement with the HAC members and associated Council staff and Councillors. There are no CALD community members on the committee.

Council Plan, Strategy and Policy Implications

52. The local planning heritage policy in the Yarra Planning Scheme is being reviewed as part of the wider Planning Scheme rewrite, the current HAC members have been providing advice on the development of the policy. This could continue with a renewed HAC or the current members could be invited to future engagement events and provide advice on the draft policy.

Legal Implications

53. There are no known legal implications.

Other Issues

54. Council established the HAC for a four year term, which has now expired. A new Committee needs to be appointed early in the new year. In order to avoid a hiatus between the current HAC and the appointment of a new HAC, Council in September, formally extended the term of the current HAC until such time as Council appoints a new HAC. By providing this ‘interim’ arrangement the HAC is still able to meet and advise Council and maintain its involvement with the development of the new heritage planning policy.

55. If Council supports establishing a new committee, an expression of interest process will be conducted and a report and recommendations provided to council on the outcome in early 2018.

Options

56. The HAC Review Report presents 3 options for the future direction of the HAC which are discussed earlier in this report.

Conclusion

57. The HAC Review Report indicates the HAC has provided some value to Council in the past four years, the most notable contribution being the development of the Heritage Strategy and Implementation Plan. Council invests significant resources in terms of officer time and funding to support the operation of the committee and implementation of the Heritage Strategy and Implementation Plan.

58. As noted the Review Report provides three options for the future of the committee.

59. Option 2 is the preferred option being: the appointment of a renewed Heritage Strategy Advisory Committee for a further 4 year term to provide effective strategic advice to Council.

60. The referenced revised Terms of Reference are framed to accord with good governance principles.

61. The office would continue to service the committee and coordinate the Heritage Strategy Implementation.

RECOMMENDATION

1. That Council notes the further officer report on the Review of the Heritage Advisory Committee (HAC) prepared by Beverley Kliger and Associates and its findings and recommendations.

2. That Council, having considered the HAC Review and the officer recommendations, resolves to:

    (a) Appoint a new Heritage Advisory Committee for a four year term with quarterly meetings and call for nominations for membership of the Committee;

    (b) In the matter of the Committee membership:

        (i) seek expressions of interest for new committee members to the Heritage Advisory Committee to comprise 12 community members and an officer from the National Trust with at least a third of the twelve community members having specific expertise from conservation architecture, landscape architecture, aboriginal heritage, history and cultural planning;

        (ii) encourage a mix of new and existing members for the committee membership and require all existing committee members seeking another term to re-nominate; and

        (iii) require that community members with long standing involvement in local heritage / community groups represent the views of that group at the Heritage Strategy Advisory Committee; and

    (c) adopt the revised amended Terms of Reference for the HAC as per **Attachment 3;**

    (d) In respect of the operation of meetings:

        (i) endorse the recommendations of the HAC Review in relation to the operation of the current meetings and ensure the new committee meetings and agendas are amended accordingly.

    (e) Having regard to the Heritage Strategy:

        (i) continue to support the implementation of the current Heritage Strategy and ensure that the new` committee contributes to the preparation of a revised Heritage Strategy to be prepared in 2018/2019; and

(ii) endorse the recommendations of the HAC Review in relation to the current Heritage Strategy to ensure that the new strategy more clearly defines how its objectives are to be achieved and projects implemented, the roles and responsibilities of committee members and Council in implementing the strategy and the resources required to deliver it.

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Attachments
1 Review of City of Yarra Heritage Advisory Committee
2 Summary of HAC Members Submissions
3 Terms of Reference

Review of City of Yarra Heritage Advisory Committee (HAC)

Beverley Kliger & associates

July 2017

Attachment 1 - Review of City of Yarra Heritage Advisory Committee

Executive Summary

The Heritage Advisory Committee (HAC) was established in 2013 for a 4 year term, in response to community members indicating they had value to add in assisting Council with strategic heritage matters.

In early 2017, Beverly Kliger & Associates was commissioned by the City of Yarra to review the effectiveness of the Yarra Heritage Advisory Committee (HAC) and make recommendations to Council on the future of the committee, suggestions for improvements and outline resource implications.

After a review of the HAC Terms of Reference (ToR), meeting agendas and minutes, the Yarra Heritage Strategy and Implementation Plan Progress Reports and consultation with the committee members, Council staff and Councillors the following conclusions have been drawn:

- A major achievement of the committee was assisting Council in the development of the Heritage Strategy and Implementation Plan.
- Other achievements include contributing to Council policy development, identifying strategic issues such as the Theatres Study, the nominations of state significance of the World War I Honour roles.
- A significant achievement from the Heritage Strategy Implementation Plan was securing new heritage protection through the Heritage Overlay in the Yarra Planning Scheme for a large number of additional places identified in Heritage Gap studies.
- After completion of the Heritage Strategy the committee lost some qualified members.
- The committee is divided on its role with a number of current members seeking to engage in operational matters especially statutory planning matters.
- The Terms of Reference of the HAC are complex and appear to focus on the built environment to the exclusion of natural and cultural heritage.
- There has been consistent desire of HAC members to engage with Council's operational process especially those related to individual planning decisions.
- There is room for improvement in the chairing and management of HAC meetings
- There has been a lack of engagement by members of the HAC in promotional activities set out in the Heritage Strategy.
- Although the Heritage Strategy Implementation Plan is ambitious with a large number of actions without clear priorities, significant progress has been made in completing or commencing the 35 actions.
- The HAC should participate in the review of the Heritage Strategy when this occurs.

Council advisory committees should enable Council to access expert advice and engage local stakeholders in strategy and policy development. To ensure that the heritage advisory committee provides effective strategic advice to Council, requires a revision of the terms of

reference, a renewal of the committee membership and a structured approach to the committee's operations. A consolidated list of recommendations is provided below.

CONSOLIDATED LIST OF RECOMMENDATIONS:

Recommendation for the future of the HAC:

1. Appoint a Heritage Strategy Advisory Committee for a four-year term with quarterly meetings

Recommendation for the Terms of Reference:

2. Revise and simplify the purpose and objectives of the committee's terms of reference.

Recommendations for the membership of HAC:

3. The committee membership be a mix of those drawn from local heritage and community groups and at minimum a third of members have specific expertise from a range of sectors (conservation architecture, landscape architecture, aboriginal heritage, history and cultural planning).
4. Encourage a mix of new and existing members for the committee membership and require all existing committee members seeking another term to re-nominate
5. Ensure committee members with long standing involvement in, or who hold a position in a local heritage /community groups are representative of the local heritage /community group at the HAC.

Recommendation for operational improvements for HAC:

6. The committee sets realistic and measurable annual goals at its first meeting to guide the focus and priorities for the year.
7. Meeting objectives with clear outcomes be set for each meeting and be set out in the agenda.
8. Monitor the progress of the achievement of the annual goals at each meeting.
9. The role of the Chairperson be expanded to include participation in planning the agenda and setting the objectives and proposed outcome for each committee meeting
10. Formalise the staff resourcing of the HAC and coordination of the Heritage Strategy Implementation Plan (equivalent to 60% of an officers full time role at a Band 6 level)
11. All new agenda items be accompanied by background information and statement of the key issue(s) requiring consideration by the committee and be circulated, at minimum, a week prior to the meeting.
12. Clarify the role and responsibility of committee members progressing the actions in the Heritage Strategy Implementation Plan.

13. A Delegate Report be presented to Council reporting on the activities of the committee after each meeting.

Recommendations for Heritage Strategy and Implementation Plan:

14. The Heritage Strategy and Implementation Plan be reviewed and revised to ensure achievement of the objectives. Clear priorities to be set for achievement of objectives.
15. The roles and responsibilities of the committee and relevant Council departments, priorities and resourcing for delivery of the Heritage Strategy be made clear as part of setting realistic implementation actions.
16. The heritage committee contribute to monitoring the implementation of the Heritage Strategy and receive reports on the status of implementation activity.
17. The committee contribute to the Heritage Strategy review and update in 2018/19

Attachment 1 - Review of City of Yarra Heritage Advisory Committee

Review of City of Yarra Heritage Advisory Committee (HAC) 1

Executive Summary i

1. Introduction and Background 1

2. What is an Advisory Committee? 2

3. Overview of Yarra Heritage Advisory Committee 3

3.1 Resourcing the HAC 3

4. Review of the Heritage Advisory Committee 5

4.1 Terms of Reference 5

Terms of Reference Purpose and Objectives 5

4.1.1 Key findings - Terms of Reference (Purpose and Objectives) 7

4.2 HAC Membership 8

4.2.1 Key findings –Membership 9

4.3 Operations of the Heritage Advisory Committee 9

4.3.1 Key Findings – Operations of the HAC 10

5. Recommendations for the Future of the Heritage Advisory Committee 13

5.1 Heritage Strategy Advisory Committee 13

5.2 Revise Advisory Committee Purpose and Objectives 15

5.3 Renewed Committee Membership 15

5.4 Operation of the Renewed Heritage Advisory Committee 18

6. Heritage Strategy and Implementation 20

6.1 Key Finding – Heritage Strategy and Implementation 20

7. Stakeholder Engagement 22

7.1 HAC Members and Council Officers 22

7.2 Interviewee Suggestions for Future Direction of HAC 24

7.3 Councillor Interviews 26

7.3.1 Findings - Councillor Interviews 27

8.Proposal for Future Directions for HAC 28

8.1 Key Finding Future Options 29

Attachment 1 – Interview Guide 32

Attachment 2 List of Consultation Interviewees 33

Attachment 3 HAC Meeting Minutes Summary 35

Attachment 1 - Review of City of Yarra Heritage Advisory Committee

Review of City of Yarra Heritage Advisory Committee (HAC)

1. Introduction and Background

In February 2017 Beverly Kliger & Associates was contracted by the City of Yarra to review the effectiveness of the Yarra Heritage Advisory Committee (HAC) and make recommendations to Council on the future of the committee.

The review involved consideration of:

1. The HAC Terms of Reference (ToR)
2. The membership of the HAC and committee processes
3. The HAC Agenda and Minutes to understand attendance and range of topics discussed
4. Reporting of HAC to full Council
5. The Yarra Heritage Strategy and Implementation Plan (2014-2018)
6. HAC's role in relation to the Yarra Heritage Strategy and implementation actions
7. Consultation with Councillors, Council staff and HAC members.

The City of Yarra Heritage Advisory Committee (HAC) commenced in May 2013. It comprises 12 community representatives and a nominee from the National Trust. The HAC meets bi-monthly and is chaired by Councillor. Currently, more than half of the initial members remain on the current HAC committee.

The terms of reference were initially adopted in April 2013 and subsequently revised in July 2014. The terms of reference set out that *the HAC ceases to exist four years from the date of its appointment unless extended by the full Council.*[1] In mid-2017 the HAC will cease to exist unless extended by Council.

This report sets out recommendations relating to the future of the HAC, proposed changes to the ToR, membership and committee processes and to clarify the role of the committee in the implementation of the Heritage Strategy.

A key aspect of the review was an analysis of in-depth interviews with Councillors, Council staff and HAC members.

Telephone interviews were held with current members who had participated in the HAC prior to 2017 and two professional expert members who have resigned from the committee. An interview guide was used to ensure all interviews encompassed the same issues (Attachment 1). Telephone interviews were also conducted with Councillor members of the HAC, two

[1] Heritage Advisory Committee Terms of reference Clause 8 (a) Terms of the committee

current members and a retired Councillor who had been on the HAC from its inception. (Listing of all interviewees Attachment 2).

A group discussion was also held with strategic planning Council officers who have worked with and supported the HAC. Also, a telephone interview was held with the former Coordinator of Strategic Planning who held this position at the time the HAC was established.

A brief workshop was held with the HAC members on 27th March 2017 regarding the possible future direction for the HAC. This workshop followed a short presentation of the findings of interviews with HAC community members and City of Yarra staff who have supported the HAC.

2. What is an Advisory Committee?

The Review uses the International Association for Public Participation definition of an advisory committee that sets out that an Advisory Committee is a structured group of community or stakeholder representatives that meets regularly and operates under a Terms of Reference. It can vary from members providing their own feedback or ideas, to members acting as a conduit between the broader community and the organisation[2].

Advisory committees are usually a small group and work well in situations that have:

- a high level of complexity
- political considerations
- where there is a need for ongoing engagement

Advisory Committees provide Council with the possibility of engaging expert advice to inform decision making and involve stakeholders in Council's strategy and policy development. In this way, Advisory Committees provide Council with the opportunity to maximise the skills and expertise that exist in the community to complement the role of Council in policy-making.

It appears that the Advisory Committees in the City of Yarra have been established to provide expert advice to Council and program and policy development. A perusal of the ToR of several Yarra's Advisory Committees revealed that these ToR are concise and have clear roles and responsibilities. The focus of these Advisory Committees is strategy development it is clearly set out in the ToR. Moreover, it is very clear that none of the Advisory Committees have decision - making powers or a role in consideration of the operational functions of Council.

[2] IAP2 – International Association for Public Participation

3. Overview of Yarra Heritage Advisory Committee

The Yarra HAC was established in response to community members indicating they had value to add in assisting Council with strategic heritage matters. The concept of establishing the HAC was supported by Council at the 21 August 2012 meeting, the HAC proposed to strengthen Council's response to strategic heritage matters and provide a link with the community. The first draft of the Terms of Reference (ToR) for the committee was adopted at the meeting.

Revised ToR and recommendations for nominations for 12 community representatives were adopted by Council in April 2013. HAC meetings are undertaken on a bi-monthly basis. Membership is for two years with the possibility of serving four years. Two Councillors are members and one is the appointed Councillor who chairs the meetings. Additionally, there is a representative from the National Trust.

The first meeting of the HAC was held on 30 May 2013, more than half of the initial members remain on the current committee.

Community representatives are appointed for a two-year period and are selected via a public notification process. Applicants with associations with heritage/community groups were encouraged to apply.

The initial HAC had a membership with a breadth of knowledge, expertise and experiences. At commencement, the HAC members with long standing links to local heritage and resident groups were complimented by members with professional training and expertise in archaeology, aboriginal and cultural heritage, heritage architecture, and the arts. This ensured that the initial HAC that was instrumental in ensuring the Heritage Strategy undertook a comprehensive consideration of heritage.

3.1 Resourcing the HAC

Since its establishment the HAC has been coordinated by the Strategic Planning Team in the City Strategy Branch at Yarra City Council.

The following administration is required for each meeting:

- Booking meeting rooms and catering
- Preparing a draft Agenda and circulating to Councillors for approval
- Circulating the Agenda to the Committee a week in advance of meetings
- Preparing for the meeting, which often requires preparation of materials for distribution, co-ordination of guest speakers, preparation of presentations to the committee, designing and facilitating workshop activities

Attachment 1 - Review of City of Yarra Heritage Advisory Committee

Review of City of Yarra Heritage Advisory Committee (HAC)

- Attendance at the HAC meetings bi-monthly for 2.5 -3 hours
- Preparing Minutes from the meetings and circulating to the committee
- Preparing a Delegate Report for Council on each meeting
- Making Agendas, Minutes and Delegate Reports available on Council's website
- Responding to HAC e-mail enquiries and requests for information in between meetings

In addition to the HAC meetings the Heritage Strategy Implementation and reporting to the committee is also led by the Strategic Planning team. The Heritage Strategy and Implementation was developed in Year 1 of the HAC term, it contains 35 actions. The focus for the subsequent years has been on completing the actions, 32 actions are completed or underway and only 2 are yet to commence. The original intent was that the committee would have a role in progressing some of the actions, however this has largely not been the case. An annual report is prepared for Council to track progress against the 35 actions and more frequent updates are provided to the HAC.

A significant amount of strategic work has been undertaken in the past four years to achieve the Heritage Strategy actions which required planning scheme amendments to implement changes in the Yarra Planning Scheme. This included introducing statutory overlays to protect Heritage places identified in heritage gap studies, introduce permit exemptions for minor works and review and update Council's data on the Victorian Heritage Database. This was a significant achievement for Council and a huge investment of resources from the strategic planning team.

It is estimated that to coordinate the HAC, Heritage Strategy Implementation and Heritage Planning Scheme Amendments required approximately 60% of a full time Strategic Planner's time each week, the Strategic Planning Coordinator and Manager to review outputs prepared by the Strategic Planner and preparation of materials and attendance at HAC Meetings.

For 2016/17 the HAC had a budget of $70,000 for Heritage Strategy implementation actions and Council contributed $50,000 to the Heritage Restoration Fund.

This is a substantial investment in Heritage activities for a local Council.

Attachment 1 - Review of City of Yarra Heritage Advisory Committee

4. Review of the Heritage Advisory Committee

This section sets out a summary of the reviews key findings relating to:

- The Terms of Reference of the HAC
- Committee membership
- Operations of the HAC

4.1 Terms of Reference

The HAC was established to provide advice to Council on heritage strategic matters, the promotion of heritage, provide advice on protection of the natural, built and cultural heritage in the City of Yarra.

The current terms of reference for Council's Heritage Advisory Committee were adopted by Council on 22 July 2014.

Terms of Reference Purpose and Objectives

The Purpose and Objectives of the current Terms of reference

1. Purpose

(a) To provide advice to Council on strategic heritage matters.
(b) To support Council in the promotion of heritage matters.
(c) To raise awareness of existing and new heritage services available and to assist in the development of future heritage services.
(d) To facilitate the views of residents and heritage groups to Council on heritage matters.

2. Objectives, roles and responsibilities

(a) To provide input into strategic heritage related projects as requested (for example capital works projects, tourism projects, heritage studies, planning scheme amendments), having regard always, to statutory obligations concerning disability access and inclusion and safety principles.
(b) To make recommendations to Council about further work required to document and protect Yarra's natural, built and cultural heritage.

(c) To provide advice to Council on issues affecting heritage across the municipality (for example trends in development, sustainability, neglect of heritage places).

(d) To provide advice to Council on recommendations for nominations of state, national or international significance.
(e) To provide an advocacy role for heritage in Yarra including marketing, promotion and raising awareness of heritage matters and services.
(f) To assist Council in sourcing external funding opportunities to further heritage conservation, promotion, management and education.
(g) To provide feedback on the impacts of planning decisions on heritage in Yarra. A HAC's role is not to provide advice to Council on specific planning applications or to have any role in the determination of applications.

A review of the minutes of the meetings of the HAC from May 2013 to January 2017 indicates the important contribution that HAC has made to the development of the City of Yarra Heritage Strategy and feedback on policies at the State and Council level (summary of Minutes in Attachment 3). The HAC has also raised local planning decisions that indicate a strategic heritage concern, such as Smith Street heritage overlay, and alerted the Council to nominations of state significance, such as the World War I Honour roles.

However, the HAC appears to have had limited input into cultural or natural heritage matters and advice on promotion of heritage.

There appears to have been continual interest by HAC members to engage with Council's operational process related to individual statutory planning decisions.

It is likely that the tension between discussion of the operation of Council's heritage planning decisions and discussion of strategic issues and promotion of heritage has been fuelled by the breadth of the HAC terms of reference. For example:

The Terms of reference purpose includes an overarching statement

- *To facilitate the views of residents and heritage groups to Council on heritage matters.*

This could be interpreted to allow the HAC members to raise all matters of concern including operational matters such as heritage training of the statutory planners.

Additionally, it is unclear that the following statement in the Purpose of the terms of reference refers to:

To raise awareness of existing and new heritage services available and to assist in the development of future heritage services.

The objectives are very broad:

- *To provide advice to Council on issues affecting heritage across the municipality (for example trends in development, sustainability, neglect of heritage places).*
- *To provide feedback on the impacts of planning decisions on heritage in Yarra. A HAC's role is not to provide advice to Council on specific planning applications or to have any role in the determination of applications.*

Additionally, the objective to provide feedback on the impacts of planning decisions, whilst curtailing discussion of specific applications, may have contributed to a view that only built heritage is a focus of the HAC, rather than the intent that the HAC focus on the broader strategic implications of these decisions.

4.1.1 Key findings - Terms of Reference (Purpose and Objectives)

The Terms of Reference whilst intended to focus on strategic matters, are extremely broad and appear to focus on built heritage.

A comparison with other City of Yarra advisory committee terms of reference (Yarra Environment and Bicycle Advisory Committees) indicates that the HAC terms of reference are more extensive and complex than those of other committees. Additionally, the Environment Advisory Committee which has a focus on strategy and implementation of policy states that the membership should be a *mix of skills and attributes to complement other members of the Committee, and have some members with specific knowledge and capacity.* Similarly, the HAC has a focus on policy and strategy and should have a mix of skill, knowledge and capacity.

The terms of reference do not make clear that heritage includes the natural, cultural and built heritage as set out in the City of Yarra Heritage Strategy. The focus of the HAC's work appears also to have been primarily on built heritage.

The terms of reference Purpose includes a clause that is not clear

(a) *To raise awareness of existing and new heritage services available and to assist in the development of future heritage services.*

The breadth of the terms of reference may have contributed to differing views as to the role of the HAC by its members. Hence, a renewed Heritage Advisory Committee should have more concise terms of reference that clarify the committee's strategic focus.

4.2 HAC Membership

Heritage is a multidisciplinary field and expertise includes conservation architecture, public policy, town planning, historic archaeology, aboriginal heritage, history, landscape architecture and heritage interpretation. The HAC had such a mixed membership with professional experts when it was initially established and provided advice on the drafting of the Heritage Strategy.

However, after the completion of the Heritage Strategy several of the professional expert members found that the HAC discussions had moved away from strategic considerations. The focus on local planning issues, and operational matters coupled with time commitment resulted in expert members resigning.[3]

The loss of the expert members on the HAC may have resulted in a focus of members on planning decisions of Council, for example a desire to scrutinise the role and decisions of the Heritage Advisory Officers, a request to review the heritage training of statutory planning officers and further a request for statutory planning officers to attend HAC meetings. These are not appropriate considerations for an Advisory Committee with a focus on strategy.

There is currently a lack of wider representation from the expert heritage professionals and from a wide range of stakeholders who have a link to the City of Yarra. While many of these skills are represented, there is an opportunity to encourage the participation of additional members to augment the community interests already represented.

The comments from the two past members interviewed raised a further concern that the committee was not chaired well and as such some of the members with links to local resident action groups appeared to be able to persistently raising issues outside the remit of the committee regarding individual planning decisions.

The current HAC has a majority of members having a link with local heritage or community organisations, but few members with professional expertise. The current membership frequently asks questions of Council on various statutory planning issues and proposes new work programmes for Council which may not be strategically justified. When the HAC was established, the Council made it clear that statutory planning matters would not be referred to the HAC as there are statutory consultation processes already in place and this was not the objective of the committee.

Currently, whilst several committee members are members of local heritage or community groups they are not representatives of these groups. Currently the members are appointed as individuals with links to, and interest in, heritage via their membership in local heritage groups. This makes the status of their input to the HAC unclear – are the comments the opinion of a local heritage/community group or of the individual? Moreover, it makes the terms of

[3] Information drawn from consultations with past members of the HAC.

reference provision 3(h) that representative members of heritage/community *groups nominate a replacement when they are unable to attend a meeting* redundant.

4.2.1 Key findings –Membership

To ensure that the HAC provides effective advice to Council requires a renewal of the membership of the HAC by attracting members with professional expertise across a range of disciplines such as history, landscape and culture.

Given that the HAC is intended to provide advice on strategic and policy matters the membership of the HAC should be from representatives of local heritage groups. This is in line with the terms of reference which are written with the intention that community members are representatives of local heritage/community groups. Moreover, it appears from a perusal of a number of local government heritage advisory committees (e.g. City of Melbourne, City of Greater Bendigo) members are appointed as representatives of local heritage groups. In some instances, local heritage group membership rotates annually.

4.3 Operations of the Heritage Advisory Committee

Whilst the terms of reference state that a Councillor will chair the meetings it does not appear that there has been consistency in the chairing or involvement by the appointed Chair in the development or oversight of the HAC agenda.

A two-hour meeting is unlikely to deal with more than 3 key items.

All matters that are added to the agenda should be accompanied by information circulated to members before the meetings setting out background information, and what the issue is and why it is being raised at the HAC i.e. what are the points for consideration by the HAC.

There were suggestions that the committee could be more effective by taking on tasks in subcommittees. Subcommittees appear to be most effective when there are clear aims, allocation of tasks and responsibility for meeting timeframes and supported by staff from Council who can gather background information and provide a secretariat service. Thus, without specific staff resources available to support subcommittees it is not advisable that such groups be established. Sub committees had been established by HAC at its commencement. However, the lack of focus and clear goals combined with stretched staff resources was a contributing factor to the lack of effectiveness of these subcommittees. As discussed at a recent meeting, there is an expectation that committee members work between meetings and there is support for the establishment of informal working groups by interested members without

Council staff in-between committee meetings. The outcomes of these informal working groups could be presented to the HAC when the outcome of the work of was in line with the annual goals of the committee.

There was concern from some members that the committee did not sufficiently engage with all division across Council that are involved in heritage matters. The lack of participation from statutory planners was a primary concern for several HAC members. At the same time, it was acknowledged it is the role of the committee to consider the policy implications arising from planning decisions not individual decisions. Whilst other sections of Council also undertake heritage activities, such as the libraries, arts and culture and communications the strategic planning section has the primary coverage of heritage policy and strategy at this time. The Heritage Strategy Implementation Report indicates there has been a good level of cross-council engagement in the delivery of actions from the implementation plan with projects commenced and delivered with involvement from Economic development, Libraries, Communications and Engagement and Community Partnerships).

At the same time, it was raised that the committee has not been particularly engaged in heritage promotion. It was proposed in the consultations that a heritage promotion activity could be undertaken with the committee's oversight. To assist with likely successful achievement of a promotional /education project overseen by the renewed heritage committee, it would also be advisable for the committee to develop a project plan. The project plan could include the intended objective and outcome of the project, the resources required and resources available from Council and other funding sources as well as committee member organisations to achieve the promotional/education activity. There would need to be a direct link with an action in the Heritage Strategy Implementation Plan that relates to heritage promotion.

In 2017, a Delegate Report was introduced for reporting the activities and recommendations of the HAC to Council. The Delegate Report is tabled at a full Council meeting. HAC members requested a copy of the Delegate Report and details of any Council discussion of the Delegate Report.

4.3.1 Key Findings – Operations of the HAC

The HAC is an advisory committee to Council as such should have Councillor engagement in the content of meetings and the key objectives for each meeting. There does not appear to be a clear statement for each meeting of the desired outcome of that meeting. Setting out the key objectives for each meeting that are in line with annual goals of the committee may assist with ensuring that the meeting does not become diverted into other issues and the agenda has the priority issues as the first items.

As the focus of the heritage committee is strategic it is not appropriate for the statutory planners to attend the meetings. Whilst the strategic planning division has been allocated responsibility for the implementation of the Heritage Strategy, it may be useful for officers from other sections of the Council to attend committee meetings to discuss progress on implementation of the Heritage Strategy. Given the conclusion of the planning scheme amendments implementing the Heritage Gap Studies the responsibility for implementing the strategy may be better placed in another department in the future.

The HAC is an advisory committee to Council made up of experts, professionals, community representatives and Councillors and council staff. The recommendations of the committee are intended to assist Council in making informed decisions on complex strategic and policy matters. At the same time as an advisory committee it only has the capacity to 'advise' or' refer' matters to Council for a decision. As such reporting, direct to the Council meeting is appropriate.

Responsibility for the HAC and Heritage Strategy Implementation is currently with the Strategic Planning team, given the significant achievement of over 10 years of strategic planning work to implement improved Heritage protections into the Yarra Planning Scheme, in the future there may be more alignment with other Council departments.

4.4 Heritage Strategy and Implementation

The Heritage Strategy seeks to encompass all aspects of tangible and intangible heritage, cultural, physical and natural, whilst acknowledging that approximately 60% of properties the City of Yarra are covered by a Heritage overlay. A key aspect of City of Yarra's heritage is cultural heritage which is set upon a foundation of Aboriginal history linked to the Wurundjeri people of the Kulin Nation the traditional Owners. This cultural history extends from colonisation, industrialisation, suburbanisation and includes the role of the buildings in the development of Australia's political development. Additionally, Yarra has experienced a change of social demographic with settlement of migrants and change in the type of housing and employment. Moreover, there are key sites of natural heritage significance.

The Strategy is a 4-year plan that sets out Yarra's responsibilities and actions required to assess, document, protect conserve and celebrate Yarra's significant heritage whilst recognising the role of other tiers of government. The Heritage Strategy sets out Council's vision, principles and four strategic directions with goals, strategies and implementation priorities. The Strategy is very broad – it contains 35 actions and although an ambitious implementation plan a significant amount of progress has been made with 33 of the 35 actionS being completed or in progress. A number of the actions were large and complex multi-year projects. The development of the Heritage Strategy and Implementation Plan was the primary focus of the HAC in the first year of

the committee with the second year more focussed on the actions in the implementation plan. The final year has been focussed on the Yarra Planning Scheme Rewrite and update of the Heritage local policy which has also reduced the officer time available to progress the actions in the implementation plan.

The Council Plan states that heritage is a major aspect of Yarra's uniqueness and identity. The Council Plan recognises the importance of Yarra's Aboriginal cultural heritage as well as promoting Yarra's arts, culture, history, diversity and vitality[4]. However, the Heritage Strategy lists a range of Council policies and strategies but does not indicate the relationship between the Heritage Strategy, the Council Plan and Yarra Planning Scheme as well other key strategies/policies. Showing the relationship to key Council policy drivers would more clearly demonstrate the importance of heritage to the Council.

The Strategy states that a HAC has been established to advise Council on strategic heritage matters.

Heritage Strategy Implementation Plans for years one and two have been developed. The Implementation Plan for year one set out a time line and lists for each action which Council department and external agency and/or the Heritage Advisory committee would be involved in the action.

After reviewing the Year 1 plan, improvements were made to the Year Two Implementation Plan and progress reports to set clear leadership for actions and it appears to set more achievable goals. Additionally, the role of the HAC in the year two implementation actions has focused on the promotion of heritage activity which is more in line with the current skills and interest of the committee.

The HAC received reports on the achievements of the implementation plan which indicated where follow up action was required.

4.4.1 Key Findings – Heritage Strategy and Implementation

The Heritage Strategy appears to be well structured and clear however is very ambitious given the resources available to implement the 35 actions. Some of the actions are complex and require extensive investigation and resourcing. For example, the planning scheme amendments and Heritage Policy work are multi-year projects or the action designated to HAC responsibility *to identify and establish relationships with the custodians of major heritage places and record their needs and ideas for heritage in a database* (Implementation Plan Year 2 action 3.2.1).

[4] City of Yarra Council Plan 2013 -2017 pg. 14

Moreover, no relationship is set out in the Heritage Strategy with the Council Plan and the major policy drivers for Council. This is despite the importance of heritage being clearly set out in these overarching Council policies.

The Implementation Plan for year 2 clarified that strategic planning is responsible for the implementation of the Heritage Strategy. However, the difficulty in implementation is in part due to the ambitious nature of the Heritage Plan and complex nature of some of the larger projects such as the planning scheme amendments (which were a significant achievement for Council and the culmination of over 10 years of strategic planning work). For example, in the Implementation Plan Year 2 the HAC has designated primary responsibility for three promotion implementation activities and is included in two further implementation activities. Moreover, no priority or time is allocated to these activities. It is unrealistic that an advisory committee with volunteer members have the time to engage in so many activities. It is also unrealistic to expect that the responsibility for implementing the Heritage Strategy can be the sole domain of the strategic planning team when there are a number of projects such as oral histories and staff training in heritage among others that have no relation to strategic planning. Some of the actions are more operational matters for Council than they are actions for a heritage strategy eg staff training in heritage, support arts and culture with their heritage related events and review Council's heritage resources.

Notwithstanding the issues outlined above considerable progress has been made to achieve improved Heritage outcomes in the municipality, the ambitious nature of the Strategy has ensured officers have strived to progress almost all of the actions in the plan.

5. Recommendations for the Future of the Heritage Advisory Committee

Managing heritage at a local level is complex. Local governments need to identify, conserve and manage significant physical and cultural heritage whilst ensuring the municipality is vibrant and experiences social and economic growth and development to meet the needs of its growing community. There are differing and strong opinions about Council's role in heritage. Hence it is not surprising that the consultation revealed that there are tensions within the committee regarding the role of the HAC.

This section sets out the key findings and recommendations drawn from all aspects of the analysis. The recommendations set out the specific changes proposed to the current Heritage Advisory Committee.

5.1 Heritage Strategy Advisory Committee

Attachment 1 - Review of City of Yarra Heritage Advisory Committee

Review of City of Yarra Heritage Advisory Committee (HAC)

The Heritage Advisory Committee contributed effectively to the development of the Heritage Strategy and provided useful advice to Council on heritage policies and strategies. However, there has been consistent desire of HAC members to engage with Council's operational process especially those related to individual planning decisions.

If there had been no HAC, Council could have obtained community and expert input into the development of the Heritage Strategy with a series of targeted engagement activities. However, a one-off engagement process may not have provided the breadth of input and expertise from comprehensive consideration and discussion that was provided by the HAC members.

There is value in providing Council with expert advice on heritage policy and strategy from both community members with extensive engagement in heritage matters and professional experts who live or work within the City of Yarra. However, the cost to Council to resource the committee needs to be considered. Coordinating the committee and implementing the Heritage Strategy is a resource intensive operation, there needs to be identifiable benefits. Does the input of the HAC on strategic heritage matters lead to better outcomes than would otherwise be the case if the committee did not exist?

This report argues that there is a role for the committee in providing strategic heritage advice to Council, albeit in a more tightly defined role than currently. If the Council is to justify the future commitment of resources to support the committee, a revision of the terms of reference, frequency of meetings and a renewal of the membership of the HAC is required. Reducing the frequency of meetings to quarterly instead of bi-monthly will still maintain the integrity of the committee but will slightly reduce the resources required by removing the administration of one meeting. The terms of reference need to clearly articulate the role of the HAC providing advice on heritage policy and strategy and make it clear that its function is not to comment on individual statutory planning decisions and other operational matters. It is likely that the tension between discussion of the operation of Council's heritage planning decisions and discussion of strategic issues and promotion of heritage has been fuelled by the breadth of the current HAC terms of reference.

As such the HAC should be continued for a further four years in a renewed form commencing with renaming the committee as the Heritage Strategy Advisory Committee (HSAC) to make clear that the focus of the committee is at a strategic level not individual permit applications or the operational activities of the Council's statutory planning division and to align with the Heritage Strategy.

Recommendation:

1. Appoint a Heritage Strategy Advisory Committee for a four-year term with quarterly meetings

5.2 Revise Advisory Committee Purpose and Objectives

The current HAC terms of reference do not make clear that the Heritage includes the natural, cultural and built heritage as set out in the City of Yarra Heritage Strategy. The current terms of reference are complex and could be revised and simplified in line with other City of Yarra advisory committee's terms of reference such as the Yarra Environment Committee's terms of reference.

Recommendation:

2. Revise and simplify the purpose and objectives of the committee's terms of reference.

The proposed revised purpose and objectives are set out below:

The purpose of the Heritage Strategy Advisory Committee is to provide Council with strategic heritage advice and to assist Council continue to identify, conserve and manage Yarra's significant heritage. Advise Council on a range of strategic heritage-related matters which are of interest to the community by providing expertise, local knowledge and guidance on heritage matters.

The Advisory Committee works to:

- *Advocate heritage principles and practice*
- *Engender continuous improvement in Councils heritage policies and strategies*
- *Promote public awareness, appreciation and support for heritage - cultural, natural and built*
- *Assist council in the development of policies and strategies in relation to Council's adopted Heritage Strategy, the Yarra Planning Scheme and Heritage Policy and the management of natural and cultural heritage generally in the City of Yarra.*

Objectives

- *To support Council in advancing the implementation actions of the Heritage Strategy and in the development and implementation of strategies, policies and programs related to heritage.*
- *Promote the natural, cultural and historic heritage of Yarra and to ensure it continues to develop a strong sense of place and identity*
- *Assist Council to source and allocate funding opportunities to further heritage conservation, promotion, management and education*
- *To recommend projects for which funding could be sought in line with the Heritage Strategy*
- *Provide access to the general community to distribute information and for public input into heritage management, e.g. provision of historical information from historical societies and heritage groups to improve citations and statements of significance for heritage places.*

5.3 Renewed Committee Membership

Attachment 1 - Review of City of Yarra Heritage Advisory Committee

Review of City of Yarra Heritage Advisory Committee (HAC)

Heritage is a multidisciplinary field and expertise includes conservation architecture, public policy, town planning, historic archaeology, aboriginal heritage, history, landscape architecture and heritage interpretation. The renewal of the committee provides an opportunity to encourage the participation of members with heritage expertise to augment the community interests currently represented. The membership would benefit from being a mix of those drawn from local heritage and community groups and at minimum a third of members have specific expertise from a range of sectors, such as conservation architecture, landscape architecture, aboriginal heritage, history and cultural planning.

It would be appropriate to aim to have at least a third of the renewed committee members with professional heritage expertise. The committee has 12 community members which appears to be standard number for advisory committee members at the City of Yarra.

The membership of the committee should include people who have a link to the City of Yarra (reside or work in the municipality).

Council should seek both continuity within the committee and generation of new ideas and direction by encouraging a mix of new and existing members. As part of the renewal process all existing members seeking another term should be required to re-nominate. Generally advisory committee members are appointed for a limited time period of two years, with a capacity to extend their term to a maximum of four years. Renewal of membership ensures diversity of expertise and possibility of new strategic considerations being provided to the Council.

Recommendations

3. The committee membership be a mix of those drawn from local heritage and community groups and at minimum a third of members have specific expertise from a range of sectors (conservation architecture, landscape architecture, aboriginal heritage, history and cultural planning).
4. Encourage a mix of new and existing members for the committee membership and require all existing committee members seeking another term to re-nominate.

It would enhance transparency and accountability for HAC committee members with long standing involvement in or who hold positions with local heritage /community groups to be formally appointed as representatives of these groups. This may assist with a clear flow of information exchange regarding the heritage activities of these groups to Council and the possibility of undertaking combined projects between Council and local heritage /community groups.

Recommendation

Attachment 1 - Review of City of Yarra Heritage Advisory Committee

5. Ensure committee members with long standing involvement in, or who hold a position in a local heritage /community groups are representative of the local heritage /community group at the HAC.

5.4 Operation of the Renewed Heritage Advisory Committee

It is good practice to set both annual and meeting objectives for an advisory committee. The committee currently meets for two hours bi- monthly. To ensure maximum value is gained from the meeting clear goals should be set for each year for the committee. If, as in 2017, Council is aware of a major heritage policy review (Heritage Policy in the Yarra Planning Scheme) the committee's advice on this policy can be clearly set out as a goal for the year and then become a priority action for meetings.

At the same time, setting objectives and proposed outcomes for each meeting, and stating them in the agenda circulated prior to the meeting, could assist in ensuring the meeting effectively considers the priority issues and time is allocated accordingly. These meeting objectives should also reflect the annual objectives.

The new process of setting annual goals and meeting objectives and outcomes should be reviewed each twelve-month period to assess the effectiveness of the renewed heritage committee process.

Involving the HAC meeting's Chairperson in the development of the meeting agenda, objectives, and intended outcome(s) should assist the Chairperson in their role of managing the meeting and committee member's expectations.

Currently, Council invests significant resources in terms of officer time and funding to support the operation of the committee. At the commencement of the committee with the development of the Heritage Strategy the support from a strategic planner to the committee was increased to facilitate the drafting of the Strategy. It appears that the heritage committee members that have had continued membership of the committee maintain an expectation of increased Council officer support time for the committee.

In my view the resourcing is more than appropriate for an advisory committee with a strategic focus. There have been significant achievements on strategic heritage matters over the term of the committee and substantial progress on the actions of the Heritage Strategy Implementation Plan. With the implementation of the recommendations from this review regarding the remit of the committee, membership and operational improvements and a review of the Heritage Strategy in the context of what can realistically be achieved in each year the current level of resourcing would be appropriate. The unofficial allocation of 60% of an officers time to coordinate the HAC and coordinate the progress of actions in Heritage Strategy Implementation Plan could be formalised to provide clarity. Some other Victorian Council's have a dedicated Strategic Heritage Officer, this may be worth considering in the future.

Attachment 1 - Review of City of Yarra Heritage Advisory Committee

To ensure the effective operation of the renewed heritage committee and promote realistic expectations of committee members the level of strategic planning staff support that is available for the committee needs to be made clear.

Recommendation

6. The committee sets realistic and measurable annual goals at its first meeting to guide the focus and priorities for the year.
7. Meeting objectives with clear outcomes be set for each meeting and be set out in the agenda.
8. Monitor the progress of the achievement of the annual goals at each meeting.
9. The role of the Chairperson be expanded to include participation in planning the agenda and setting the objectives and proposed outcome for each committee meeting
10. Formalise the resourcing of the HAC and coordination of the Heritage Strategy Implementation Plan (equivalent to up to 60% of an officers full time role at a Band 6 level)

The setting of annual goals and meeting objectives should not preclude the committee from dealing with strategic issues as they arise during the year. However, new issues that are to be dealt with by the committee should only be undertaken after circulation, in advance of the meeting, of background information, and a statement of the issue(s) requiring attention of the committee. The item should only be included on the agenda where it can be clearly articulated what the strategic heritage issue is.

These new issues should only be considered by the committee with agreement of the committee chair and determination of the time commitment entailed. Providing the background information and issues in advance will enable the committee members to participate in a considered discussion. To assist with ensuring the committee stays focused on strategy and annual goals the Chairperson could review new items for relevance and priority.

Recommendation

11. All new agenda items be accompanied by background information and statement of the key issue(s) requiring consideration by the committee and be circulated, at minimum, a week prior to the meeting.

The promotion activities undertaken by the committee should be those set out in the Heritage Strategy and associated Implementation Plan. The Heritage Strategy - Year 2 Implementation Plan sets a number of promotion activities that are responsibility of the Heritage Committee, including HAC preparing a listing of events that celebrate Yarra's heritage such as restoration talks, open house schemes, heritage festivals and exhibition, and HAC participating in reviewing and updating the new resident kit to include heritage information. However, it does not appear that these tasks have been carried out.

As such the committee's promotion project plan could set priorities and time lines for achievement of the Heritage Strategy Implementation Plan promotion activities where the HAC has designated responsibility. It would be useful to also set out in the project plan intended objective and outcome, resources required and funding available for the projects.

The role and responsibility of committee members needs to be made clear.

Recommendations

12. Clarify the role and responsibility of committee members progressing the actions in the Heritage Strategy Implementation Plan.

The Delegate Report process should be continued with the renewed heritage committee. To ensure transparency a copy of the Delegate Report and Council minutes be circulated to the heritage committee members.

Recommendation.

13. A Delegate Report be presented to Council reporting on the activities of the committee after each meeting.

5.5 Heritage Strategy and Implementation Plan

The Heritage Strategy would benefit from a full review and be redrafted with fewer objectives and provide greater clarity on priorities and associated resources to support implementation and set more realistic expectations of the role of volunteer committee members. It should avoid addressing Council's operational matters. Similarly, the promotion objectives and priorities should be reviewed in line with priorities and associated resources to support implementation. This review would enable consideration of inclusion of promotion of aboriginal cultural heritage. The redrafted Heritage Strategy should include a flow chart setting out the relationship between the Heritage Strategy, the Council Plan and other key Council policies.

The revised Heritage Strategy Implementation Plan should set clear priorities and outline the responsibilities for the heritage committee and relevant Council departments.

Recommendations.

14. The Heritage Strategy be reviewed and revised to ensure achievement of the objectives. Clear priorities to be set for achievement of objectives.
15. The roles and responsibilities of the committee and relevant Council departments, priorities and resourcing for delivery of the Heritage Strategy be made clear as part of setting realistic implementation actions.

Attachment 1 - Review of City of Yarra Heritage Advisory Committee

16. The heritage committee contribute to monitoring the implementation of the Heritage Strategy and receive reports on the status of implementation activity.
17. The committee contribute to the Heritage Strategy review and update in 2018/19

7. Stakeholder Engagement

This section summaries the comments made during the consultations were held with

- members of HAC present and former members
- City of Yarra officers and former officers who supported the HAC
- Councillors who were members of the HAC current and former representatives.

7.1 HAC Members and Council Officers

Below is a table summarising the interview discussions with the HAC members and Council officers set out as strengths and weaknesses. As all the discussions were confidential no interviewee quotes are identified. The list of those interviewed is set out in Attachment 2.

Table 1: Overview of HAC member's comments set out as Strengths and Weakness

	Positive – Strengths	Negative - Weakness	Consultant's Commentary
Effectiveness of HAC including achievements and contributions	• Majority agreement that HAC was instrumental in the development of the Heritage Strategy, a substantive Council-wide policy • Raised the profile of heritage in Council • Has enabled local issues to be raised as indicators of strategic issue – e.g. development of the Lyric theatre led to review of all historic theatres, review Smith Street overlay and inclusion of area from Gertrude Street to Victoria Parade, the World War I Honour Board gaining state significance • *Keep the Council honest"* as such HAC has acted as a heritage watchdog	• Not affect positive change to culture of City of Yarra to make heritage a priority i.e. embedded in Council Plan • Focus on statutory planning with a concentration on the built environment and limited consideration of cultural and natural heritage • Only engage with Strategic planners, but heritage encompasses all aspects of Council • Not a cohesive group • After achievement of the Heritage Strategy HAC has had no clear direction • Some members are unclear about the contribution beyond the Heritage Strategy '*hard to see achievements'* • Some members concerned that the HAC is an avenue for critiquing Council when it should not be • Not appear to achieve the educational /promotional activities set out in implementation plan despite skill of the HAC members	• Lack of cohesion in the HAC limits achievements and contribution with some concern about the use of the HAC as a platform for "*Council bashing*" • Some members are unclear about the contribution beyond the Heritage Strategy *hard to see* achievements • The view that HAC operates heritage watch dog, *keeping the Council honest* makes clear the mismatch between the aims of some HAC members and the role and purpose of the HAC as set out in its ToR and the need to make explicit the committee's focus is strategy.

Attachment 1 - Review of City of Yarra Heritage Advisory Committee

Review of City of Yarra Heritage Advisory Committee (HAC)

	Positive – Strengths	Negative - Weakness	Consultant's Commentary
	• Commentary on a range of Council policies and reports		
Terms of reference (ToR)	• General agreement with current ToR • Agreement that 2(g) needs to limit discussion to strategic implications	• No report of the annual progress of the HAC per clause 7 (c) • No clarity of the role of the HAC very broad "*motherhood statement*" of purpose and role • No detail of what is required as member's time commitment • One comment that ToR needs a tighter focus	Very limited in-depth consideration of the ToR A progress report was presented to HAC and Council in Dec 2016
Membership	• At the commencement, a good mix of skills with professional experts and members with knowledge of architecture and conservation archaeology and cultural heritage • Some local community representatives content with current membership stating that they hold sufficient expertise	• Concern expressed that some local representatives are advocates for local planning issues; hard to get a focus on broad strategic matters • Concern that currently insufficient number of members with professional skills who can assist the HAC with technical jargon and what is possible within the complexity of heritage in architecture and planning • Lack of representation to consider Aboriginal cultural heritage • Not clear process for re-election members' and no maximum term for community representative 3(f)	• Divergent views regarding the best mix of membership between expertise and local representation
Operations	• New delegate reporting to Council seen as a major improvement and *good* process that provides accountability for HAC • Wiggo provided a *good* communication and information exchange process for members	• Agenda too large and results in little time devoted to issues • HAC effectiveness determined by the staff resources and capacity of the support • Some concern that the direction of the HAC determined by the staff e.g. setting the agenda • No clear reporting to HAC of heritage matters across Council or the implementation of the Heritage Strategy; Need a clear process on how to engage with other areas of Council beyond strategic planning • Currently no clear direction for the HAC, appears to be event driven with a focus on poor planning decisions	• Divergent view regarding sub-committees with some members believing they are essential. Whilst others members feel they have not been effective as subcommittees need Council officer support and increased time commitment. Also, there was a concern that subcommittees can be dominated by local interests • Some local members suggested that if a new position of Heritage Coordinator was created in Council the HAC should

Attachment 1 - Review of City of Yarra Heritage Advisory Committee

Review of City of Yarra Heritage Advisory Committee (HAC)

	Positive – Strengths	Negative - Weakness	Consultant's Commentary
		• Suggestion that there needs to be more direction '*shaping*' of the HAC agenda • Strongly held view that chairing of the committee has not been professional. There was strong statement that there is a need to manage discussion of local, technical or operational issues – this is a view held by most interviewees *"bombarded with local requests."* • Often issues raised not followed up or reported on, e.g. heritage awards, oral histories.	work to that position not strategic planning. • Appears to be tension within the HAC membership about the extent of focus on local planning issues • The Agenda and Chairing of the meeting could be improved to ensure it is more focussed on implementation of the Heritage Strategy whilst allowing time for provision of advice on key strategic matters such as the revision of the Heritage Policy in the Planning Scheme and less time spent discussing issues that are outside the remit of the committee
Resourcing	Council officer support	Lack of clarity as to how much officer time is allocated to support work of the HAC	Council resourcing is not a matter for consideration of the committee. Attempts have been made in recent meetings to clarify the resources available.
Was Members' participation worthwhile	Provided networking and learning for members	After Heritage Strategy, not clear what the HAC could or should do	HAC needs a collective identity

7.2 Interviewee Suggestions for Future Direction of HAC

The discussions with the HAC members indicated a desire for the HAC to improve the way the HAC operates and to have planned activity/projects related to heritage. It was felt that for members to have a commitment, and to foster the HAC working as a cohesive group requires working on a combined project. The HAC members proposed a range of changes for the HAC. These changes can be summarised under two headings:

1. Operational improvements
2. Promotion, Education, Community Engagement Project

Where the proposals are not in line with the strategic intent of the heritage committee or encompass operational matters a comment to this effect has been set out in *italics*.

Review of City of Yarra Heritage Advisory Committee (HAC)

The proposed operational improvements:
These are proposals made by HAC members:

- Copy of new Delegate's Report should to go to the HAC members and include a comment on the Council's response to the report
- The HAC meetings be a balance of on-going issues and new issues raised. *This is not an effective way to operate a committee with a focus on strategy with clear annual goals and meeting objectives.*
- Need a clear process on how to engage with other areas of Council beyond strategic planning, such as reports from other departments about what they are doing about heritage. *It is not appropriate for an advisory committee to introduce additional reporting requirements for other Council department. Updates should continue in an informal manner.*
- Set an agenda ahead for example quarterly with 60% of the agenda focusing on core issues so that the HAC has a direction.
- Provide training and support for the Councillor chairperson/s.
- Proposals to include a flow chart /road map of the heritage policies and strategies across Council and how these link to the Council Plan, i.e. set out the City of Yarra heritage strategic and policy framework.
- Increased resourcing from Council and clarity as to the level of resourcing. *Council has finite resources to allocate to support the committee and these need to be balanced with other work program commitments. Updates have been provided to the committee at recent meetings.*

Promotion, Education, Community Engagement

A range of ideas were proposed that the HAC could work on. It was proposed that promotion of heritage could lead to wider recognition and outcomes, promote history, culture and architecture as well as broaden the understanding of heritage to include culture, Aboriginal heritage, natural environment - parks, gardens and waterways.

It was agreed that promotion and education in relation to heritage was a major aspect of Heritage Strategy and could improve Council information on what is considered in *heritage*.

At the same time, there was some concern that the HAC could not undertake the projects but rather work with the National Trust Council and the local heritage groups to ensure the completion of the projects. For example, tap into Heritage week working with the National Trust to have a range of promotional and educational activities across the municipality.

It was also suggested that community engagement could bring groups together and use volunteer and local resources working with Council building on the work already undertaken by these local groups. It was proposed that the HAC develop a promotion – education action plan that includes social media. A number of projects were proposed including:

- The projects could include: calendar of heritage events, open houses, oral histories, awards program, celebrating house birthdays with information from the local library, walks, talks, newsletters – awareness of resources on Heritage, information setting our components of good design, information about inappropriate development. *This is an action in the Heritage Strategy Implementation Plan.*
- Oversee the establishment database of heritage buildings. *The Council has list of all heritage building in the Planning Scheme and in a database*.
- Provide information on what is entailed in cultural heritage – tangible and intangible
- A guide to how heritage considerations are made in planning applications. *However, is this is not an appropriate activity for a strategic advisory committee, the Victoria Planning Provisions set out how to balance heritage considerations in planning decisions.*

There was also concern expressed that it is may be difficult to get the group to work together as some will fight for local interests.

It was proposed that in developing a promotional or education project, it would need to commence with deciding the target audience and then determine what is needed. Once again this would need to be linked to the current Heritage Strategy Implementation Plan or included in the revised strategy.

7.3 Councillor Interviews

Phone interviews were conducted with Councillors currently on the HAC and Councillors who were previously on the HAC. There was general agreement that:

- HAC's focus should be strategic and is currently needed to provide advice on the revision of the planning scheme heritage policy as this is the policy framework that statutory planners use to make decisions.
- HAC should not be delving into operational matters such as staff training and employment of heritage officers
- There is need to be a strong chair to ensure all voices are heard given the strong passionate voices that are hard to contain

- Could be useful to include residents who have professional expertise in heritage matters as well as local heritage experts and advocates
- The implementation of the Heritage Strategy had not been effective due to lack of clarity as to leadership and role of the HAC.

Statutory planning decisions impacts on local communities and are the core of community concern. However, there are differing views of how the HAC engages with statutory planning. Whilst it was seen that it is valid to raise a local planning decision if strategic impacts can be drawn from the example as with the Lyric Theatre. There was a suggestion that the HAC oversee an annual forum regarding statutory planning and heritage decision making. *However, this is more an operational activity of Council and is not compatible with the strategic focus of the HAC.*

There was also a recognition that the HAC focused on built heritage however, it was also felt built heritage is a major issue confronting Council and is an appropriate priority for the HAC.

7.3.1 Findings - Councillor Interviews

There is agreement that the focus of the HAC is strategic. However, there are different understandings of what that entails. There was agreement that there is need to have priorities for meetings to ensure that the key strategy and policy items are considered.

Promotional /education projects that could be overseen by the HAC should be aligned to the activities set out in the Heritage Strategy Implementation Plan that have been assigned to the HAC's responsibility.

8. Proposal for Future Directions for HAC

At the meeting of the HAC committee on 27 March 2017, 3 options for the future direction were presented to the HAC. As there was limited time available for a discussion of the options the HAC members were given the opportunity of making written comments. Below is a summary of the workshop and written comments regarding the three options. Written comments were received from 6 current members of the HAC.

- Margaret O'Brien
- Ian Wight
- Greg Spark
- Janet Taylor
- Anne Holmes
- Angela Zivkovic

The three options presented for comment are set out below.

Option One – Maintain the Current Heritage Advisory Committee

There are problems with the current operation of the Heritage Advisory Committee in that it spends too much time on local issues and the current membership, with its emphasis on resident groups means current consultation is more akin to community consultation, rather than a heritage advisory group providing peer review.

Option Two – Adopt a New Reconstituted Heritage Advisory Committee

A reconstituted HAC could operate more effectively by introducing changes including membership and redirecting the Committee to operate in accordance with a revised term of reference. A renewed emphasis on policy could ensure that the City of Yarra's heritage strategy and polices meet best practice.

It would also ensure the Committee is project and policy focussed with a specified lifespan. A reconstituted Committee also offers them the opportunity to include other professional expertise interests such as the property sector, a conservation planner, archaeologist, or historian aboriginal representative, and landscape architect/specialist.

The Council would benefit from the advice of an independent, multi-disciplinary, professional and expert advisory committee, to weigh up competing evidence, analysis and information when it arises.

Option Three – Dissolve the Current Heritage Advisory Committee

Dissolving the current HAC would not limit Council's ability to consult broadly on heritage matters with a range of experts and the wider community as it is not the only means of consulting with these groups. The officer time currently devoted to servicing the committee could be diverted to implementing other heritage projects.

The advantage offered by the HAC is that it is already operational and it has an established a body of knowledge and experience which may not always be available in an ad hoc consultation process.

Summary of comments on the Three Options

The current members of the HAC were keen to retain the current HAC with some changes. There was a strong view expressed that there was extensive expertise within the current HAC membership. It was also felt that:

- the terms of reference are appropriate
- the members with lay expertise due to their interest in heritage who have relationship local historical and residential groups are essential to HAC
- retain the current membership to maintain continuity
- could reduce the numbers on the HAC
- More structured agendas and setting priorities for the HAC with time timelines for project circulation of papers prior to meetings and
- representative from departments other than strategic planning attend meetings. such as statutory planning
- whilst agreement that the HAC should focus on strategy several HAC members cited the need for a Heritage Officer
- Not keen for professional expert representation

There was agreement that the process of operation of the HAC could be improved by:

- Planning the agenda ahead and keeping to priority issues. It was suggested that the agenda consider themes and issues
- If a HAC member wants to nominate a specific item for the agenda state how much time need for discussion, specify questions wanted answered, circulate prior to the meeting.

8.1 Key Finding Future Options

The heritage committee should only be continued if it is clear that the role of the committee is strategic and provides advice on policy and strategy and assists with implementation of the Heritage Strategy and other relevant strategic heritage matters. The Council needs to be clear

that the resources devoted to the operation of the committee add value to strategic heritage matters. The committee's assistance in developing the Heritage Strategy has provided that value in the past. The operation of the committee in the future needs to continue to add that value. In order to do that, a restructuring of the HAC is required.

Each meeting should have clear annual goals and objectives related to Council's strategic activity in relation to heritage across areas of the built, natural environment and culture – indigenous and diversity within Yarra. To achieve this re-visioning of the heritage committee will require:

- Rewriting of the Terms of Reference to make explicit the role and purpose is strategic advice to Council
- Renewal of the committee membership to include people with professional expertise and some turnover of members who have been on the committee since its commencement
- Refocusing on promotion activity that is related to the Heritage Strategy Implementation Plan.
- Setting of annual and meeting goals that will encourage improved management of meeting processes to ensure effective use of Council resources in supporting the committee and strategic heritage outcomes for the community of Yarra.

Attachment 1 - Review of City of Yarra Heritage Advisory Committee

Review of City of Yarra Heritage Advisory Committee (HAC)

Attachment 1 – Interview Guide

HAC Achievements

Strengths

- What have been the key achievements of the HAC?
- How has HAC contributed to Council's heritage activities?
- The Heritage Strategy is a key achievement of the HAC. Do you think the Heritage Strategy is realistic and achievable?

HAC purpose, role and membership

- Are there any issues with the role and function of the Committee? What are your two major concerns?
- What could be done to improve the role and function of the Committee?
- What are your thoughts on the purpose of the Committee set out in the terms of reference?
- Does the membership provide the range of heritage expertise required to advise Council on strategic heritage matters?

Reporting

- Is the new Delegate's Report an effective way of reporting to Council?

General Comment

- Do you think your participation in the HAC is worthwhile?

Attachment 1 - Review of City of Yarra Heritage Advisory Committee

Attachment 2 List of Consultation Interviewees

List of HAC members Interviewed

Name	Membership	Network
Paul Beekman	Current	Fitzroy: Melbourne Heritage Action Group; Residents' group - Gore Street, Fitzroy
Margaret O'Brien	Current	Fitzroy: Yarra Residents' Coalition; Fitzroy Residents' Association; Carlton Residents' Association
Jill Anwyl	Current	North Carlton: Princes Hill North Carlton Heritage Network: Yarra Residents Coalition
Fiona Bell	Current	Carlton North: Friends of Hardy Gallagher Reserve; Princes Hill North Carlton Heritage Network; Protectors of Public Lands
Janet Taylor	Current	Clifton Hill: Collingwood Historical Society' Fitzroy History Society; Yarra Residents Coalition
Anne Holmes	Current	Fitzroy North: Librarian; Collingwood Historical Society; Fitzroy Historical Society; Hawthorn Historical Society; - State Library of Victoria - - digitising local history committee
Alix Hill	Current	National Trust representative
Lucinda Owen	Current	Richmond: Architect Australia ICOMOS; Australian Institute of Architects (VIC) ArchTeam Co-Operative Ltd
Ian Wight	Current	Richmond: Ian Wight Planning and Heritage Strategies
David Langdon	Current	Richmond: Richmond and Burnley Historical Society
Mark Stephenson	Resigned	Architecture firm (specialises in heritage and is based in Richmond; Merlynston Sustainability Group: ICOMOS
Rachel Fensham	Resigned	Abbotsford: Royal Society of the Arts; Melbourne University - head of School of Culture and Communication

List of Council Officers interviewed

David Walmsley	City of Yarra	Manager Strategic Planning
Claire Baker	City of Yarra	Coordinator of Strategic Planning
Amanda Haycox	City of Yarra	Strategic Planner - HAC support officer
Erika Russell	City of Yarra	Strategic Planner – previous HAC support officer
Sherry Hopkins	Formerly of City of Yarra	Previous Coordinator of Strategic Planning

Attachment 1 - Review of City of Yarra Heritage Advisory Committee

Review of City of Yarra Heritage Advisory Committee (HAC)

List of Councillors Interviewed

Jackie Fristacky	Current Council member of HAC
James Searle	Current Council member of HAC
Sam Gaylord	Previous Council member of HAC, retired Councillor

Attachment 1 - Review of City of Yarra Heritage Advisory Committee

Attachment 3 HAC Meeting Minutes Summary

Terms of reference (ToR) adopted Council April 2013 New Terms of reference adopted 22 July 2014

First Meeting 30 May 2013

Date	Issues covered	Actions
30 May 2013	Working groups	1. Local Planning Policy Framework (MSS and local policy) 2. Activity centres / public realm 3. Heritage Strategy 4. Environment and heritage (including the balancing of different policies) 5. Advocacy 6. Promotion and Education
Aug 2013	ToR to address casual vacancies and acting arrangements/alternative members Bi monthly meeting Subgroups meet month between Push for heritage strategy	**Priority sub group** 1. Local & State Policy 2. Environment and heritage (including the balancing of different policies Zoning etc.
Oct 2013	Information papers review planning scheme. Heritage funding article Consultation re Uniting Church Fitzroy from member	Consultation with HAC review Planning review summary with minutes Melbourne Metropolitan Plan draft overview 2 key sub groups • Local & State Policy • Heritage Strategy
Dec 2013	Heritage Strategy priority Federal & State funding Updates on key issues – zones, East - West link	Site identification for funding
Feb.2014	Information re heritage funding and planning issues	Refined Heritage consultant's brief
June 2014	Revised ToR	Revised TOR Request casual vacancy, more than 2 year terms, clarity as to renomination Updates of planning, Heritage Strategy and funding opportunities Advice of speaker for Heritage Strategy Forum Interaction heritage overlay & Residential Growth Zones Comments draft Heritage Strategy HAC to review submissions to Heritage Strategy
Aug 14		4 years; casual heritage/community
Sept 14	Focus on Heritage Strategy comments and draft	Tickets for 2 HAC members to international conference

Attachment 1 - Review of City of Yarra Heritage Advisory Committee

Review of City of Yarra Heritage Advisory Committee (HAC)

Date	Issues covered	Actions
Oct 14	Draft Heritage Strategy – 1st year One, implementations achieved; notification of amendments Guidelines draw from HAC members Napier Street consultation process	Notify of heritage overlay and amendments to planning scheme WW 1 Honour Board building state significance One HAC resignation
Feb 2015	New residential zones Planning Scheme review – heritage policy rewrite part of review involvement e HAC	Heritage Strategy adopted
April 15	Update heritage amendments Heritage Amendment Gap work	The role of the Heritage Strategy is to inform the revised policy and any changes proposed. It will be referenced in the Yarra Planning Scheme to support a strategic approach to heritage HAC to send comments Heritage Strategy - two further reports were requested 1. the use of volunteer community involvement in the implementation of the Strategy, 2. budget considerations and priorities to achieve year 1 actions. Johnston Street local area plan heritage issue – mural NO ATSI member of HAC
Aug 15	ToR raised, substitutes & Statutory planners Oral histories subcommittee update	Heritage polices to be circulate to HAC Report on Planning Schemer review and HAC interaction Report Implementation Plan HAC involved in Council response to Heritage Act review Brunswick Street shopfront review - request information
Oct 15	Updates of reviews local and state	Request involve with heritage advisors' role review Propose Year 2 implementation plan Community engagement Specific engaging – signage, theatre review
Feb 2016	David C to provide information periodically on heritage amendments when proceeding to Council.	Specifics re heritage implementation plan progress Darling Gardens master plan noted
April 16	Statutory Planning representation on the HAC – but the ToR HAC are clear that it is not the HAC's role to provide advice or have any role in the determination of planning applications.	Propose implementation an update of projects (a running sheet) be circulated in advance to HAC Heritage Budget allocation Members would like to invite a representative from Statutory Planning to a future meeting to hear the discussions taking place
June 16	Not appoint members as June 17 deadline for the HAC No implementation plans circulated	Raise specific heritage issues e.g. • Burnley park • Outhouses on laneways

Attachment 1 - Review of City of Yarra Heritage Advisory Committee

Review of City of Yarra Heritage Advisory Committee (HAC)

Date	Issues covered	Actions
Aug 16	Planner discuss specific sites	HAC want to review heritage report on site before goes to Council Year 2 implementation plan - HAC wants to year 1 implementation – 35 items of which 7 were completed, 18 underway/ongoing and 10 yet to commence. add oral histories
Dec 16	Planning scheme review – HAC comments on policy Delegate report Comments on specific buildings Vic heritage funding	Issue of St Vincent's Further consider the heritage awards as part of the review of the HAC Outline of HAC review
Jan 2017	New process Delegate Report from HAC to Council	Concern around developer consultation – facilitated review of Planning Scheme Heritage Policy engagement with develop Deep discussion of Heritage Policy rewrite – good summary of key points of discussion Reject subcommittee not work – better whole HAC Suggested work *The primary focus will be on administering the HAC, progressing the Heritage policy for the Yarra Planning Scheme Rewrite and the Heritage Strategy.*

Attachment 2 - Summary of HAC Members Submissions

HAC Review
Summary of submissions received from HAC members

Summary of main Issues from interviews	Officer Response
Membership / skills required on HAC	We are looking for a range of skills and will call for expressions of interest for new members. When we call for expressions we will give examples of the sorts of skills we are looking for but the list will not be exhaustive or seek to exclude any interested party.
Name of committee	The consultant suggested Heritage Strategy Advisory Committee. 'Heritage Strategic Advisory Committee' has been suggested to avoid confusion with just being focussed on the Heritage Strategy. Alternatively, the existing name could be retained.
HAC members undertaking education/promotion	One of the reasons the HAC was established initially was because community representatives said to Council that they had expertise to share with the Council and their network of contacts could also be useful. We are looking for members to have a more active role in heritage strategy implementation in general.
Strategic planning matters /individual statutory planning applications	The existing TOR specifically states that the purpose of the committee is to provide strategic heritage advice to Council on heritage matters and that it is not to look at specific statutory planning applications or to have any role in determining them. Current heritage policy review is an example of examining decisions to improve policy.
Why are we having a HAC review?	Current TOR has a sunset clause. There is also a clear sentiment from members that the current operation of the committee could be improved.
HAC reporting to Council on the review	Submissions from HAC members will be included as appendix to Council report. A summary of submissions will also be included in officer report.
Heritage training for Council staff	We currently have a Heritage Strategy that includes a large list of items and our current work should focus on the existing strategy. Specific training is an operational matter for Council.

Attachment 3 - Terms of Reference

ATTACHMENT 3

CITY OF YARRA HERITAGE ADVISORY COMMITTEE - TERMS OF REFERENCE

Version 19/12/2017

1. Purpose

To provide advice to Council on heritage matters including Yarra's natural, built and cultural heritage.

2. Role and Responsibilities

(a) To provide advice and recommendations to Council on:

(i) the monitoring and implementation of the *Yarra Heritage Strategy;*

(ii) the ongoing review of Yarra's heritage policies, plans and strategies;

(iii) any further work required to document and protect Yarra's natural, built and cultural heritage;

(iv) issues affecting heritage across the municipality (e. g. trends in development, sustainability or neglect of heritage places);

(v) policy and strategic objectives concerning City of Yarra's responsibilities for the World Heritage Environs Area within Yarra's boundary;

(vi) the effective promotion of Yarra's heritage including raising public awareness of heritage matters and services; and

(vii) nominations of state, national and world heritage significance.

3. Membership: and attendance

3.1 Appointments to the committee shall be made by Council and will include:

(a) three Councillors (one from each ward), appointed on an annual basis, to take it in turns to Chair the HAC meetings and also present the Delegate Report to Council.

(b) twelve community representatives from across the City of Yarra whom shall be drawn from:

(i) local heritage and community groups:

(ii) at least one third of the twelve members to have specific expertise from a range of sectors (conservation architecture, landscape architecture, aboriginal heritage, history and cultural planning); and

(iii) a representative from the National Trust.

3.2 *Representation:*

(a) A requirement that committee members with long standing involvement in, or who hold a position in a local heritage / community groups, are a formal representative of that local heritage /community group at the HAC;

Attachment 3 - Terms of Reference

(b) That Community representatives shall be formally appointed by the Council for a period of four years and will be selected via a public notification process which calls for nominations at least three months before the committee term expires; and

(c) Should a vacancy occur, Council may appoint a replacement for the balance of the term.

3.3 Attendance at meetings

(a) Staff in attendance at meetings will include the Senior Heritage Advisor, the Group Manager Chief Executive's Office (or representative), and the officer servicing the Committee,

(b) Other officers will be available to attend meetings of the HAC as required by the agenda or for particular purposes as requested by the Chair or Senior Heritage Advisor.

4. Selection process and criteria for community members

(a) The Council will seek applicants for membership of the HAC through public notices at least three months prior to the end of the current term.

(b) Subject always to the requirements clause 3.1(b), as far as practical, the membership will reflect diversity in gender, cultural background and locality representation.

(c) Applicants will be requested to submit a short statement of capabilities addressing the following criteria:

(i) A demonstrated interest or expertise in heritage.

(ii) That they have the approval of their respective local heritage /community group to be their representative;

(iii) An understanding of community needs, concerns and issues relating to heritage in the City of Yarra.

(iv) An understanding of the role that Local Government has in heritage matters.

(v) Ideas on how the promotion of heritage services could be undertaken.

(vi) An ability and commitment to consider and value a wide cross section of community views.

(vii) A commitment to participate in meetings on a quarterly basis, or at other times as determined necessary by the HAC.

(d) Applicants will be shortlisted by a panel of relevant staff involved in heritage, and Councillors on HAC. Shortlisted applicants may be interviewed by the panel prior to a formal report and recommendations being made to Council for approval.

(e) Existing members may nominate to renew their membership on the HAC.

5. Member responsibilities

(a) Members must declare any personal interest, connection or association with any matter brought before the HAC.

(b) Members must not make improper use of information acquired as a consequence of membership of the HAC.

(c) If matters of a confidential nature are discussed by the HAC, Members must respect that confidentiality.

Attachment 3 - Terms of Reference

(d) A member of the HAC may resign at any time. Notice of resignation is to be provided in writing to Council and the Group Manager Chief Executive's Office.

(e) Membership of the HAC may be terminated for any of the following reasons:

(i) failure to attend two consecutive meetings without prior notice; and

(ii) conduct unbecoming to a member, for example, a breach of confidentiality.

(f) If Council intends to make a decision to terminate a membership of the HAC it will give the member written notice setting out the intended decision and the grounds on which it is based. The member will have the opportunity to address the Council prior to it making a decision.

6. Meeting procedure

(a) Meetings of the committee are to be undertaken on a quarterly basis or at other times as determined necessary by the HAC.

(b) HAC meetings are to be chaired by the appointed Councillor on a rostered basis or if unavailable, one of the other two Councillors. If all Councillors are absent, a member agreed to by the Committee will act as Chair.

(c) The Chair of the meeting has a casting vote (this applies to Councillors only).

(d) The quorum for any meeting of the HAC shall be seven members.

7. Reporting and circulation

(a) The HAC Chair will present a Delegate Report to Council after every HAC meeting on issues and advice that arise from HAC meeting with a copy provided subsequently to all HAC members.

(b) A staff member from Council's Governance Branch (or other available officer) will be responsible for attending meetings, distributing agendas and minutes, being the central contact point for the committee and for providing any background information as required.

(c) The Agenda and all new background information to be circulated at least a week prior to the meeting.

(d) An annual progress report will be prepared to update Council on ~~the~~ progress of the HAC, highlighting achievements, activities and major heritage issues affecting the municipality. This will be prepared including input and advice by the HAC and reported to Council within 2 months of the anniversary of the Committee.

8. Terms of the committee

(a) The HAC ceases to exist after four years from the date of its appointment by Council, unless extended by Council resolution.

(b) The terms of reference for the HAC must be reviewed during each HAC term and before consideration of appointment of any new HAC. Any changes proposed, will require approval by Council.

(c) The HAC has no delegated authority to make determination, to act or to incur expenditure on behalf of Council.

11.11 Passage of the Marriage Amendment (Definition and Religious Freedoms) Bill

Trim Record Number: D17/199477
Responsible Officer: Group Manager Chief Executive's Office

Purpose

1. This report has been prepared in response to the passage of the Marriage Amendment (Definition and Religious Freedoms) Bill 2017 in the Australian Federal Parliament on 7 December 2017 and its signing into law the following day. The report proposes a number of actions resulting from the success of the Yarra City Council's long standing campaign for marriage equality in Australia, and its support for the Lesbian Gay Bisexual Transgender Intersex and Queer (LGBTIQ) community in general.

Background

2. On 11 August 2004, the Australian Federal Parliament passed the Marriage Amendment Bill 2004, legislating a definition of Australian marriage for the first time. The Act defined marriage as "*the union of a man and a woman to the exclusion of all others, voluntarily entered into for life*" and also removed the recognition in Australia of same sex marriages conferred overseas.

3. According to the Australian Bureau of Statistics, the highest number of same sex couples in Victoria reside in the City of Yarra. Council has a long standing commitment to supporting its large and diverse community of LGBTIQ people and has long held a position in opposition to the narrow definition of marriage in the Marriage Act enshrined in 2004.

4. On 15 November 2017, Senator Dean Smith introduced a Private Member's Bill into the Senate seeking a change to the Marriage Act to give effect to the outcome of the national postal survey and to change the law to allow people of the same sex to marry. Following the passage of that bill in the Senate, Mr Warren Entsch MP, introduced it to the House of Representatives on 4 December 2017. The Bill passed both houses of Parliament on 7 December 2017.

5. On 8 December 2017, the Marriage Amendment (Definition and Religious Freedoms) Act 2017 was signed into law. This significant milestone was welcomed by people across Yarra and Australia.

Marriage Equality Campaign

6. On 20 October 2015, Yarra City Council passed a resolution to publicly support marriage equality, irrespective of sex or gender identity. Council wrote to the Prime Minister to request that the Australian Government take action to amend the Commonwealth Marriage Act 1961 to support marriage equality.

7. On 2 May 2017, Yarra City Council re-affirmed its public support marriage equality, irrespective of sex or gender identity, this time writing to all Members of Federal Parliament to request that the Australian Government take action to amend the Commonwealth Marriage Act 1961 to support marriage equality. These letters were sent on 2 June 2017.

8. On 15 August 2017, Yarra City Council resolved unanimously to note the Council resolution of 2 May 2017, re-affirming its public support for marriage equality regardless of sexual orientation or gender identity.

9. In addition to other actions, Council resolved to write to the Federal Government expressing its disapproval of plans to implement a costly and unprecedented postal plebiscite and asking that teenagers who turn 18 during the period of the postal poll be enfranchised to take part in the survey if it goes ahead.

Civic Flag Policy

10. On 4 April 2017, Council last reviewed the Civic Flag Policy to govern the flying of flags at Council's town halls, civic flagpoles and other facilities. The policy includes a schedule of

flags to be flown throughout the year on Council's three civic flagpoles located near to the town halls at Collingwood, Fitzroy and Richmond.

11. On 2 May 2017, Council updated the civic flag schedule and determined to fly the Rainbow Flag "*until such time as marriage equality becomes law in Australia*". This is in addition to the annual flying of the flag on 17 May (International Day Against Homophobia Biphobia and Transphobia). When marriage equality became law on 8 December 2017, the Chief Executive Officer determined to use the powers granted under the policy to keep the flag flying to enable a further resolution from Council.

12. Given the need to use the civic flagpoles for a range of purposes, and the success of the marriage equality campaign, it is recommended to continue flying the Rainbow Flag on Council's three civic flagpoles at Collingwood, Fitzroy and Richmond until Tuesday 9 January 2018, being the first date on which most same sex marriages can take place in Australia. Thereafter, it is recommended that the Rainbow Flag continue to be flown on 17 May and on 8 December (the anniversary of the Royal Assent of the Marriage Amendment (Definition and Religious Freedoms) Bill 2017).

Relationship Declaration Register

13. On 19 December 2006, Council directed Council officers to investigate the establishment of a Relationship Declaration Register to enable all couples to formally declare their union. On 20 February 2007, Council resolved to "*establish a Relationships Declaration Program (and) liaise with officers of the Melbourne City Council in the development of the Program policies and operational guidelines to ensure maximum consistency of operation in adjoining municipalities*."

14. The Relationship Declaration Register was subsequently established in conjunction with the City of Melbourne and allows couples who are not legally married (including same-sex couples) to declare their union. Whilst the Register does not confer legal rights in the way marriage does, it can be used to demonstrate the existence of de facto relationships in relation to the Property Law Act 1958, the Administration and Probate Act 1958 and other legislation involving domestic partnerships. The move was described at the time by the City of Melbourne as "*a first step towards social equity for all relationships – it is now up to the State and Federal Governments to introduce legislation that will provide for official recognition of civil partnerships.*"

15. From 2008, the Victorian Relationships Act established in law the similar concept of a 'registrable domestic relationship', which is a relationship between two adults (irrespective of gender and whether they are living together) who are not married to each other, but are a couple where one or each of the persons in the relationship provides personal or financial commitment and support of a domestic nature for the material benefit of the other. Persons who are in a 'registrable domestic relationship' are able to make an application to the Victorian Registrar of Births, Deaths and Marriages to register their relationship. Despite this change, both the Melbourne City Council and Yarra City Council's registers continued to operate.

16. Officers from the City of Melbourne have advised that in light of the legalisation of same sex marriage in Australia, the Melbourne City Council Relationship Declaration Register is currently closed to new applications and a report will be presented shortly to enable Council to make a determination on its future.

17. It is recommended that as same sex marriage is now legal in Australia, the Yarra City Council close its Relationship Declaration Register to new applications after 8 January 2018 (as marriages can legally take place after that date). This change will have no impact on existing registrants (who will continue to be listed on the register) or on any existing applicants (there are currently no outstanding applications for inclusion on the register).

External Consultation

18. Officers from the City of Melbourne have been consulted in relation to the status of the Melbourne City Council Relationship Declaration Register.

19. No further external consultation has been conducted in the development of this report.

20. Of note, in the Australian Marriage Law Postal Survey 2017, the Federal electorate of Melbourne (which comprises the majority of Yarra) returned the highest 'Yes' vote in Australia, at 83.69%.

Internal Consultation (One Yarra)

21. This report has been developed in partnership between Council's Community Partnerships and Governance Support branches.

Financial Implications

22. Direct mail to supporters of the marriage equality campaign will require minor expenditure associated with officer time to prepare a letter and postage costs. These costs are able to be accommodated within Council's adopted budget.

23. The alteration of the Civic Flag policy comes at a minor cost to Council for staffing and wear and tear on the Rainbow flag. These costs are able to be accommodated within Council's adopted budget.

24. The cost of administering the Relationship Declaration Register is primarily offset by the $50.00 fee which applies to applications. Its closure will have no impact on Council's adopted budget.

Economic Implications

25. There are no economic implications arising from this report.

Sustainability Implications

26. There are no sustainability implications arising from this report.

Social Implications

27. There are no social implications arising from this report.

Human Rights Implications

28. There are no human rights implications arising from this report.

Communications with CALD Communities Implications

29. As there has been no external consultation in the development of this report, there has been no need to consider the communication needs of Yarra's culturally and linguistically diverse communities.

Council Plan, Strategy and Policy Implications

30. The proposal ties in with two key objectives of the Council Plan 2017-2021:

    (a) Community health, safety and wellbeing are a focus in everything we do; and

    (b) Inclusion, diversity and uniqueness are welcomed, respected and celebrated.

Legal Implications

31. There are no human rights implications arising from this report.

Other Issues

32. There are no other issues arising from this report.

Options

33. Council may determine to depart from the officer's recommendation in resolving in relation to this matter.

Conclusion

34. This report concludes that the appropriate course of action for Council following the passage of the Marriage Amendment (Definition and Religious Freedoms) Bill 2017 is to:

    (a) write letters of thanks to relevant stakeholders;

    (b) alter the Civic Flag Policy; and

(c) close the Relationship Declaration Register to new applications.

RECOMMENDATION

1. That the Mayor write letters to the following:
   (a) Senator Dean Smith, Senator for Western Australia, thanking him for drafting the Marriage Amendment (Definition and Religious Freedoms) Bill 2017 and introducing it to the Senate on 15 November 2017;
   (b) Mr Warren Entsch MP, Member for Leichhardt, thanking him for introducing the Marriage Amendment (Definition and Religious Freedoms) Bill 2017 to the House of Representatives on 4 December 2017; and
   (c) all Members of Federal Parliament who voted in favour of the Marriage Amendment (Definition and Religious Freedoms) Bill 2017 (either in the Senate or House of Representatives), thanking them for their support on the floor of Parliament.
2. That the Civic Flag Policy be updated to reflect that the Rainbow Flag be flown on the Civic Flagpoles at Collingwood, Fitzroy and Richmond:
   (a) until and including Tuesday 9 January 2018, being the first date on which most same sex marriages can take place in Australia; and
   (b) thereafter on 17 May and 8 December each year.
3. That no new applications for inclusion on the Yarra City Council Relationship Declaration Register be accepted after Monday 8 January 2018.

CONTACT OFFICER: **Rhys Thomas**
TITLE: **Senior Governance Advisor**
TEL: **9205 5302**

Attachments
There are no attachments for this report.

11.12 Annual Plan Quarterly Progress Report - September 2017

Executive Summary

Purpose

To present the 2017/18 Annual Plan Quarterly Progress Report – September 2017 to Councillors for noting.

Key Issues

The 2017/18 Annual Plan has 70 actions with 63 due to commence in the first quarter.

Annual targets set a requirement for 75% of Annual Plan actions to be Complete or On Track (>90%) by 30 June each year.

At the end of September 76% or 48 of 63 actions that were due to commence are On Track or Complete.

Financial Implications

There are no financial implications.

PROPOSAL

That Council note the 2017/18 Annual Plan Quarterly Progress Report – September 2017.

11.12 Annual Plan Quarterly Progress Report - September 2017

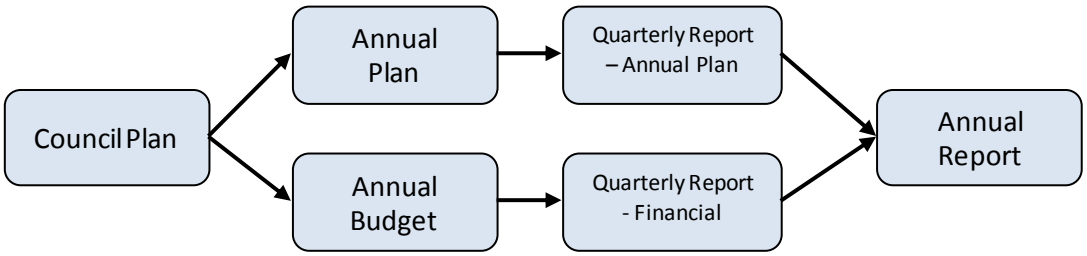
Trim Record Number: D17/200186
Responsible Officer: Manager Corporate Performance

Purpose

1. To present the 2017/18 Annual Plan Quarterly Progress Report – September 2017 to Councillors for noting.

Background

2. This year, 2017/18, represents the first year of the 4-year *Council Plan 2017-21*, adopted by Council on 1 August 2017.
3. The Annual Plan and Annual Plan Quarterly Progress Reports are two of Council's key accountability documents to the community.



4. The 2017/18 Annual Plan was endorsed by Council on 19 September 2017 and details the organisation's annual response to Initiatives contained in the 4-year Council Plan.
5. Council Plan Initiatives are significant projects and activities that are proposed to be worked on over the term of the Council Plan.
6. The Annual Plan Quarterly Progress Report notes the year-to-date progress of the Annual Plan Actions and Milestones providing a performance rating and supporting commentary.
7. The 2017/18 Annual Plan contains 70 actions spread across the Council Plan's Strategic Objectives. It is achievable, delivers on Council's priorities and reflects the organisation's focus on delivering the Council Plan's Initiatives in 2017/18.

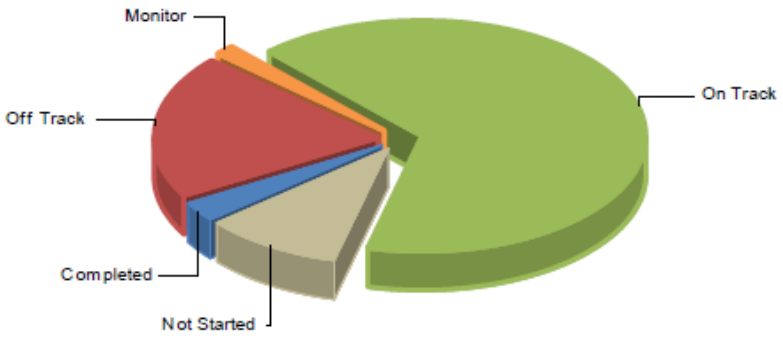
Strategic Objective	Number of Actions 2017/18 Annual Plan
A healthy Yarra: a place where Community health, safety and wellbeing are a focus in everything we do	10
An inclusive Yarra: a place where Inclusion, diversity and uniqueness are welcomed, respected and celebrated	9
A sustainable Yarra: a place where Council leads on sustainability and protects and enhances its natural environment	6
A liveable Yarra: a place where Development and growth are managed to maintain and enhance the character and heritage of the city	13
A prosperous Yarra: a place where Local businesses prosper and creative and knowledge industries thrive	11
A connected Yarra: a place where Connectivity and travel options are environmentally sustainable, integrated and well-designed	12
A leading Yarra: a place where Transparency, performance and community participation drive the way we operate	9
Total	**70**

8. There are 49 Strategies in the *Council Plan 2017-21,* with 99 associated Initiatives. Not all of these have a corresponding action in the 2017/18 Annual Plan. The Council Plan is a four-year document and Initiatives will commence across a range of years. A number of Initiatives are predominantly operational in nature and will be reported on through the 'Other Council Plan Initiatives' section of the Annual Plan Quarterly Progress Report six monthly in the December and June reports.
9. To ensure the integrity and transparency of the Annual Plan, which is endorsed by Council, actions including their descriptions and milestones can only be changed by resolution of Council.
10. Officers or Councillors may propose changes to the Annual Plan.

Quarterly performance

11. The progress of an action is measured by the status of its individual milestones which are weighted to represent the relative time and effort they contribute to achievement of the overall action.
12. The following thresholds are used to determine the status of an action:
    (a) On track ≥ 90%
    (b) Monitor 75-89%
    (c) Off track < 75%

13. Annual Plan Action progress summary as at 30 September 2017:



Strategic Objective	No. of Actions Reported	Complete	On track (>=90%)	Monitor (75-90%)	Off track (<75%)	Not Started
A healthy Yarra	10	0	8	0	1	1
An inclusive Yarra	9	0	4	0	4	1
A sustainable Yarra	6	0	5	0	1	0
A liveable Yarra	13	0	11	1	0	1
A prosperous Yarra	11	0	6	0	4	1
A connected Yarra	12	1	7	0	1	3
A leading Yarra	9	1	5	0	3	0
	70 (100%)	2 (2.86%)	46 (65.71%)	1 (1.43%)	14 (20.00%)	7 (10.00%)

14. The 2017/18 Annual Plan has 70 actions with 63 due to commence in the first quarter.

15. Annual targets set a requirement for 75% of Annual Plan actions to be Complete or On Track (>90%) by 30 June each year.

16. At the end of September 76% or 48 of 63 actions that were due to commence are On Track or Complete.

17. The following actions are recorded as Not Started, Off Track or Monitor at the end of September. These actions have experienced delays to one or more of their milestones. More detail is contained in the attached report.

    <u>Not Started</u>

    1.07 Develop Leisure Centres Master Plan

    2.09 Develop Public Spaces policy

    4.05 Review Heritage Strategy

    5.10 Review Economic Development Strategy

    6.04 Advocate for DDA compliant trams stops

    6.07 Advocate for electric bus trial

    6.08 Develop Car Share Policy

    <u>Monitor</u>

    4.09 Seek planning controls for Queens Parade, North Fitzroy

Off track

1.08 Develop Yarra Homelessness Strategy

2.01 Develop a Volunteer Strategy

2.02 Develop Human Rights Charter

2.06 Develop LGBTIQ Strategy

2.08 Develop new Access and Inclusion Strategy and Action Plan

3.01 Develop Biodiversity Strategy

5.01 Review Library facilities

5.06 Deliver Village Activation program

5.07 Address shop vacancies

5.09 Deliver Business Events Program

6.06 Advocate for improved Inner Regional public transport

7.02 Engage young people

7.03 Review Strategic Advocacy Framework

7.08 Focus on customer responsiveness

2016/17 Annual Plan Actions in Progress Update

2016/17 Annual Plan action	**Status as of 30 September 2017**
Action 2.03 – Leisure Centre improvements	Collingwood HVAC and Fitzroy change room works nearing completion
Action 3.01 – Strategic Land Acquisition	Development of new Open Space Strategy (2017/17 Annual Plan Action 1.06) will guide future land acquisition for open space
Action 3.07 – Wellington street bicycle lane	2017/18 Annual Plan Action 6.10 - Progress Wellington Street Bike Lane (Stage 2)
Action 3.08 – New parking technology	2017/18 Annual Plan Action 6.09 - Bridge Road parking trial in progress
Action 3.12 – Yarra Activity Centre built form review	2017/18 Annual Plan Action 4.10 - Undertake activity centre built form analysis studies
Action 4.01 – Urban Wildlife Management Plan	Plan being developed in parallel with the Biodiversity Strategy (2017/18 Annual Plan Action 3.01)
Action 3.11 – Urban Growth management	2017/18 Annual Plan Action 4.06 - Develop Yarra Housing Strategy
Action 4.01 – Adaptive Assets Program	Solar panel installations are in progress

External Consultation

18. Significant community engagement and consultation was undertaken during the development of the *Council Plan 2017-21*. The 2017/18 Annual Plan reflects the community priorities identified during this process, included in the *Council Plan 2017-21* initiatives.

19. Members of the community will have the opportunity to comment on the progress report when it is presented to Council on 19 December 2017.

20. Projects contained in the 2017/18 Annual Plan are subject to external consultation and engagement on a case-by-case basis.

Internal Consultation (One Yarra)

21. Managers and Directors were consulted during development of the 2017/18 Annual Plan Actions, and are responsible for providing quarterly updates on their progress against delivery of these actions.

Financial Implications

22. Actions in the 2017/18 Annual Plan are resourced within the 2017/18 Budget.

Economic Implications

23. The *Council Plan 2017-21* includes the Strategic Objective *A prosperous Yarra: a place where Local businesses prosper and creative and knowledge industries thrive*. The 2017/18 Annual Plan includes 11 actions that respond to initiatives under this Strategic Objective.

Sustainability Implications

24. The *Council Plan 2017-21* includes the Strategic Objective *A sustainable Yarra: a place where Council leads on sustainability and protects and enhances it natural environment*. The 2017/18 Annual Plan includes 6 actions that respond to initiatives under this Strategic Objective.

Social Implications

25. The *Council Plan 2017-21* includes the Strategic Objective *A healthy Yarra: a place Community health, safety and wellbeing are a focus in everything we do*. The 2017/18 Annual Plan includes 10 actions that respond to initiatives under this Strategic Objective.

Human Rights Implications

26. The *Council Plan 2017-21* includes the Strategic Objective *An inclusive Yarra: a place where inclusion, diversity and uniqueness are welcomed, respected and celebrated*. The 2017/18 Annual Plan includes 9 actions that respond to initiatives under this Strategic Objective.

Communications with CALD Communities Implications

27. CALD groups were specifically targeted as part of the engagement plan for the *Council Plan 2017-21* and were represented in a group workshop which included Council's advisory groups.

28. Translation service assistance is available via Council's website for members of the CALD community and this service extended to the Council Plan engagement process.

Council Plan, Strategy and Policy Implications

29. The 2017/18 Annual Plan represents Year 1 of the *Council Plan 2017-21* adopted on 1 August 2017.

Legal Implications

30. There are no legal implications.

Other Issues

31. There are no other issues.

Options

32. The report does not include any options.

Conclusion

33. The Annual Plan Quarterly Progress Report – September 2017 is presented to Council for noting.

RECOMMENDATION

1. That:

   (a) Council note progress against the 2017/18 Annual Plan as reported in the Annual Pan Quarterly Progress Report – September 2017.

CONTACT OFFICER: **Shane Looney**
TITLE: **Corporate Planner**
TEL: **9205 5397**

Attachments
1 Annual Plan Quarterly Progress Report September 2017

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

Introduction Council Plan 2017-2021

The Yarra City Council adopted its Council Plan 2017 – 21 on 1 August 2017. The Council Plan 2017 – 21 sets out the medium-term direction of Council and the outcomes sought by Councillors for their term. For the first time, the Council Plan incorporates the Health and Wellbeing Plan. This financial year, 2017/18 is Year One of the Council Plan 2017 – 21.

Under the Local Government Act 1989 (the Act), each council is required to produce a four-year Council Plan by 30 June in the year following a general election. The Plan must include Strategic Objectives, Strategies, Strategic Indicators and a Strategic Resource Plan.

Council has identified a number of initiatives under each Strategic Objective which are significant projects and activities that are proposed to be worked on over the term of the Council Plan, subject to approval through the annual budget process.
Council produces an Annual Plan alongside each year's Budget, setting out specific projects and activities that Council will undertake towards achieving the Strategic Objectives. This will include priority projects, capital works projects, actions in response to initiatives in the Council Plan and other Council strategies and plans as well as service reviews and improvements.

The Council Plan 2017 – 21 has seven Strategic Objectives which relate to a different aspect of service delivery:

A healthy Yarra: Community health, safety and wellbeing are a focus in everything we do. Council plays a key role in contributing to our community's health and wellbeing. From delivering maternal and child health services to ensuring access to open space, enforcing noise regulations to supporting community and cultural initiatives, community health, safety and wellbeing is central to everything we do.

An inclusive Yarra: Inclusion, diversity and uniqueness are welcomed, respected and celebrated. Identity was a passionate and recurring theme throughout community consultation for the Council Plan. Our community values its diversity. Creating a safe place where people of all ages, genders, sexualities, abilities, cultures and backgrounds feel welcome and connected is a priority for Council.

A sustainable Yarra: Council leads on sustainability and protects and enhances its natural environment. As Victoria's first carbon-neutral council, we are proud of our commitment to sustainability. Protecting our natural environment and supporting our community to reduce its environmental footprint will continue to be a priority for Council.

A liveable Yarra: Development and growth are managed to maintain and enhance the character and heritage of the city. With demand for inner city housing increasing, Council is mindful of the importance of balancing the needs of new and existing residents, and ensuring that development does not encroach on the amenity or heritage of our city.

A prosperous Yarra: Local businesses prosper and creative and knowledge industries thrive. Yarra is a great place to do business and to work. Supporting local businesses and creative industries not only contributes to Yarra's economy, but also increases local employment opportunities, enhances street life and fosters community connectedness.

A connected Yarra: Connectivity and travel options are environmentally sustainable, integrated and well-designed. Council is committed to creating a city that is accessible to all irrespective of levels of personal mobility, to support a fulfilling life without the need for a car.

A leading Yarra: Transparency, performance and community participation drive the way we operate. Council is committed to change through an energised, cohesive team of professionals, recognised for our leadership, innovation and service.

In response to its Strategic Objectives, Council has committed to 70 projects and activities from a broad cross-section of services in the 2017/18 Annual Plan.

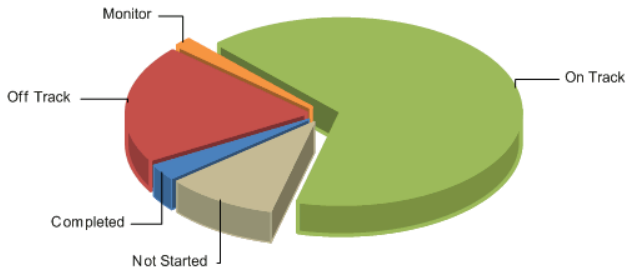
Progress of these projects and actions will be reported in the 2017/18 Annual Plan Quarterly Progress Reports.
Further information can be found in the published version of the Council Plan 2017 – 21 on the City of Yarra's website (https://www.yarracity.vic.gov.au/about-us/council-information/council-plan).

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

Quarter Summary

Council has committed to 70 actions across a range of services. Any variations to the Annual Plan are made openly and transparently in the context of priorities that arise over the course of the year.

The status of actions is classified based on the percentage of targets achieved as assessed by the responsible officer (forecast milestones compared to actual work completed).



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	70 (100%)	2 (2.86%)	46 (65.71%)	1 (1.43%)	14 (20.00%)	7 (10.00%)

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

1. A healthy Yarra

a place where…Community health, safety and wellbeing are a focus in everything we do

Council plays a key role in contributing to our community's health and wellbeing. From delivering maternal and child health services to ensuring access to open space, enforcing noise regulations to supporting community and cultural initiatives, community health, safety and wellbeing is central to everything we do.

Strategies

Council's work to achieve this Strategic Objective includes the following strategies:

1.1 Maintain and enhance Yarra's open space network to meet the diverse range of community uses
1.2 Promote a community that is inclusive, resilient, connected and enjoys strong mental and physical health and wellbeing
1.3 Provide health promoting environments that encourage healthy eating and active living
1.4 Assist to reduce the harms from alcohol and drugs on individuals and the community in partnership with State Agencies and key service providers
1.5 Promote environments that support safe and respectful sexual practices, reproductive choices and gender equity
1.6 Promote a gender equitable, safe and respectful community
1.7 Promote an effective and compassionate approach to rough sleeping and advocate for affordable, appropriate housing
1.8 Provide opportunities for people to be involved in and connect with their community

The following actions are being undertaken in 2017/18 to work toward achieving Council's strategic objective of A healthy Yarra.

Action Progress Summary

- At least 90% of action target achieved
- Between 75 and 90% of action target achieved
- Less than 75% of action target achieved
- Not Started
- Completed

Target
% Complete

Action	Start Date / End Date	
1.01 Prevent Family Violence	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
1.02 Advocate for a medically supervised injecting facility	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
1.03 Deliver Public Realm safety improvements to the Lennox and Victoria Street intersections	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
1.04 Work with the community to address alcohol-related harm for young people	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
1.05 Create new open space	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
1.06 Develop new Open Space Strategy	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
1.07 Develop Leisure Centres Master Plan	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
1.08 Develop Yarra Homelessness Strategy	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

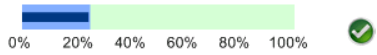
Action		Start Date / End Date	
1.09	Implement the National Aged Care and Disability Reforms and Future Directions Planning	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
1.10	Promote community connection and wellbeing	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

1.01 Prevent Family Violence

Council Plan initiative:
Work with local and regional partners for greater awareness and intervention to prevent family violence and elder abuse

Council is committed to the prevention of family violence in the Yarra community. This year Council will conduct an audit of awareness and intervention strategies in Yarra and whether these are meeting the current and emerging needs of Yarra residents. It will also review how well-equipped Council services are in identifying and responding to family violence, particularly tertiary interventions.
The results of this project will be summarised in a public document that outlines Yarra Council's statement of commitment to responding to family violence.



Branch *Social Policy and Research*

Quarterly Milestones

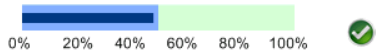
September	Q1 Analyse service provisioning for family violence in the City of Yarra
December	Q2 Report on the status of family violence prevention and intervention developed and discussed at a stakeholder workshop
March	Q3 Deliver draft Statement of Commitment for Yarra to Council based on feedback at the stakeholder workshop
June	Q4 Launch Statement of Commitment

Quarterly Progress Comments An analyses of service provisioning for family violence in the City of Yarra has been completed via consultations and a desktop review.

1.02 Advocate for a medically supervised injecting facility

Council Plan initiative:
Advocate to the state government for a medically supervised injecting facility and other harm minimisation measures in the Victoria Street precinct

Council supports a public health approach to illicit drug use and has voted unanimously to advocate to the State Government to implement a medically supervised injecting facility (MSIF) to help save lives by connecting vulnerable people with vital health services, help take drug use off the streets and make our public places safer and more welcoming.



Branch *Social Policy and Research*

Quarterly Milestones

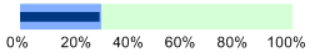
September	Q1. Report on the finding from the Inquiry into the drugs, poisons and controlled substances amendment (pilot medically supervised injecting centre) bill 2017
March	Q3. Report to Council on the findings from Law Reform, Road And Community Safety Committee Inquiry Into the effectiveness of laws, procedures and regulations relating to illicit and synthetic drugs

Quarterly Progress Comments The Legislative Council Inquiry into the Drugs, Poisons and Controlled Substances Amendment (Pilot Medically Supervised Injecting Centre) reported their findings in September 2017. While the report did not directly recommend an MSIC trial, it did note that the issue had 'reached crisis level' and described residents dealing with overdoses in their laneways and discarded syringes in front gardens as intolerable; that MSICs improve the health of IDUs, address visible impacts and reduce demand for ambulances; and MSICs do not have a 'honey pot' effect on crime. Council was briefed on the Yarra City Council submission and were informed of the Inquiry Findings.

1.03 Deliver Public Realm safety improvements to the Lennox and Victoria Street intersections

Council Plan initiative:
Work with local partners and agencies to increase health and education, community partnerships and harm minimisation from the abuse of drugs and alcohol

Council is engaging the community including traders and stakeholders on plans to improve the intersection of Victoria and Lennox Street as one of the ways to address the complex and longstanding health , safety and amenity issues in this precinct. Plans include installing lighting, street furniture, paving and landscaping and improvements to pedestrian and cycle movements. Council has received $200,000 State Government funding for this project and is committing a further $240,000 in the 2017/18 Budget.



Branch *City Strategy*

Quarterly Milestones

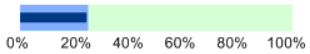
September	Q1. Consider public submissions
	Q1. Undertake initial project scoping
	Q1. Determine design solution
December	Q2. Finalise project proposal
	Q2. Complete detailed design works and tender project
March	Q3. Run social marketing campaign over the summer holiday season
	Q3. Commence construction
June	Q4. Evaluate and report to Council
	Q4. Complete construction
	Q4. Commence preparations for evaluation to the funding body Department of Justice and Regulation

Quarterly Progress Comments The project has been scoped, public feedback considered and the design solutions identified.

1.04 Work with the community to address alcohol-related harm for young people

Underage drinking is associated with poorer academic outcomes, increased likelihood of tobacco and illicit drug use, exposure to episodic harms such as physical and sexual assault and can cause alterations in the structure and function of the developing brain, which continues to mature into the mid-to-late twenties.

In partnership with RMIT and Communities that Care (CTC), educational materials will be produced to raise awareness and educate parents of the harms and risks associated with underage drinking and the positive role they can play in promoting youth health and wellbeing. Council will use its own communication channels and its local partners to distribute this messaging in the community using traditional media, social media and other appropriate forums.



Branch *Social Policy and Research*

Quarterly Milestones

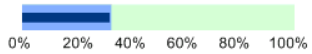
September	Q1. Undertake initial project scoping
December	Q2. Finalise project proposal
March	Q3. Run social marketing campaign over the summer holiday season
June	Q4. Evaluate and report to Council

Quarterly Progress Comments Initial scoping has commenced and partnership opportunities are also being explored. Once partners are established they will need to sign off on the brief.

1.05 Create new open space

Council Plan initiative:
Increase the amount and quality of green open space through the strategic acquisition of land, the conversion of urban land to open space and/or the creation of pocket parks in high density areas.

Yarra's parks are highly valued by the community and contribute to a quality open space system that meets the recreational and environmental needs of the local community, as identified in the Open Space Strategy. Council will create and commence construction of a new park at Charles Street and Mollison Street Abbotsford .



Branch *Recreation and Open Space*

Quarterly Milestones

September	Q1. Report to Council on the Road Discontinuance process
March	Q3. Complete project documentation and advertise tender
June	Q4. Appoint contractor and commence construction

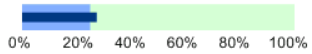
Quarterly Progress Comments Road discontinuance approved by Council

1.06 Develop new Open Space Strategy

Council Plan initiative:
Develop an open space strategy to ensure Yarra's public open space is managed as a functional network that encourages shared use and active living.

The Yarra Open Space Strategy guides the future provision, planning, design and management of public open space in Yarra. A new Open Space Strategy will result in a renewed direction for the provision and enhancement of the open space network, including changes in community needs since the last strategy was developed. The strategy aims to achieve a cohesive, linked and well managed network of open space to meet the full range of residents' needs.

Development of the strategy will occur over a two year period with the first year focussing on land use and landscape analysis, with extensive consultation, while Year Two will involve adoption of the Strategy, followed by an amendment to the Yarra Planning Scheme.



Branch *Recreation and Open Space*

Quarterly Milestones

September	Q1. Seek fee proposals and prepare to appoint consultant
December	Q2. Complete background research
March	Q3. Complete spatial analysis
June	Q4. Complete Stage 1 of community consultation

Quarterly Progress Comments A project brief was sent to three consultants with an invitation to tender. Two submissions were received back. The submissions were evaluated by the tender panel against the stated selection criteria and a preferred consultant has been appointed.

1.07 Develop Leisure Centres Master Plan

Council Plan initiative:
Investigate a Leisure Centres Master Plan .

Council manages leisure facilities in Richmond, Collingwood and Fitzroy. These facilities are highly valued by the community and attract in the order of 1 million visits per annum. A high level facilities plan will be developed to guide future investment in these facilities.

Branch *Leisure Services*

Quarterly Milestones

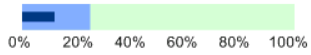
June	Q4. Present report to Executive to inform future investment in Leisure facilities

Quarterly Progress Comments The report is due to be completed and presented to Executive in quarter 4.

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

1.08 Develop Yarra Homelessness Strategy

The development of the Homelessness Strategy will provide the policy framework for Council to ensure an effective and compassionate approach to rough sleeping. The development of the strategy will involve wide consultation and engagement with stakeholders.



Branch *People, Culture and Community*

Quarterly Milestones

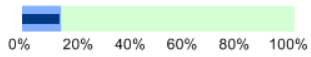
September	Q1. Discuss and investigate other local government Homelessness Strategies
December	Q2. Commence consultation with stakeholders
March	Q3. Complete consultation with stakeholders
June	Q4. Complete Draft Homelessness Strategy

Quarterly Progress Comments Initial literature search completed. Investigation included local, national and international homelessness strategies with a focus on responses to rough sleeping. Investigations will be completed by December 2017

1.09 Implement the National Aged Care and Disability Reforms and Future Directions Planning

Council Plan initiative:
Continue to implement the National Aged Care and Disability reforms and develop new strategic directions for support of older people and people with disability.

The national aged care and disability reforms will continue to roll-out over 2017 – 2018. The reforms have significant implications for how Yarra residents will be supported, and allows Council to consider what role it can best play in ensuring the wellbeing of people 50+ and people with disability in our municipality.



Branch *Aged and Disability Services*

Quarterly Milestones

September	Q1. Review the Commonwealth Government's Future Reform – an integrated care at home program to support older Australians discussion paper and submit a local and regional submission
December	Q2. Complete the Regional Northern Metropolitan Council Commonwealth Home Support Program Review and report findings, and identified options for Council consideration Q2. Finalise support of all eligible clients as they complete phasing-in to the National Disability Insurance Scheme program
March	Q3. Advocate for continuation of the MetroAccess program for the year 2018-2019 Q3. Complete community engagement and consultation to inform Council's future directions decision-making process
June	Q4. Continue transition of the Linkages Case Management program to the Home Care Packages program as determined by the Commonwealth Government Q4. Finalise the analysis of options and present recommendations to Council on its future role in aged care service delivery, and strategic directions for supporting our 50 yrs + residents and people with disability

Quarterly Progress Comments Submission on the Commonwealth Government's Future Reform – an integrated care at home program to support older Australians discussion paper was prepared and submitted. Copies were provided to the Municipal Association of Victoria and Northern Regional Councils group to inform regional and state submissions.

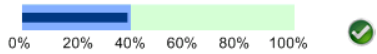
1.10 Promote community connection and wellbeing

Council Plan initiative; Continue to provide a range of services and work collaboratively with external groups to improve the health and wellbeing of the Yarra community.

Council plays a key role in contributing to community health and wellbeing through the provision of services, programs, facilities and initiatives in leisure, libraries, family, youth and children's, aged and disability, arts and culture and social policy and research to support participation, community connectedness and resilience.

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

2017/18 Annual Plan Quarterly Progress Report September



Branch *Library Services*

Quarterly Milestones

September	Q1. Provide opportunities for community members to meet, socialise and develop by: providing access to affordable and appropriate community facilities Q1. Run targeted events and programs promoting health, life-skills and life-issues via Yarra Libraries
December	Q2. Promote programs and services aimed at increasing participation at Councils Leisure facilities by the CALD and at risk sections of the community
March	Q3. Collaborate on a number of community events that will promote the health, wellbeing, development and social inclusion of children, youth and their families via the Family Youth and Children's services branch
June	Q4. Support attendance at Live Music events for people of all abilities, through the facilitation of the Band Mates Project

Quarterly Progress Comments The Library ran the following programs on Health and Wellbeing this quarter with 675 attendances:

55+ Pilates Taster, Qigong, Introduction to drawing, Physical movement workshops for 50+, Know Your Rights sessions, Resume Rescue one on one session. Resume rescue – Group session of Key selection criteria, Food Know How sessions, Swing at the Library, Rescope event, Sustainable House Expo, Conversation lounge - Richmond, Conversation Lounge - Carlton, Tea Talk and Tech sessions – Collingwood, Tea Talk and Tech sessions - Fitzroy, Chinese Writers Festival, Chinese Reading Month events, Food family & culture: The Community Grocer, Homelessness in Yarra panel discussion, Collingwood Library clothing swap, Weaving a new life presentation, Living under drone attack and surveillance, Imagining art through technology, Thursday Write-In program, Collage Club.

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

Highlights and Achievements

Branch ***Family, Youth and Children's Services***

Unit ***Service Planning and Development***

Through research, analysis and project management supported review of occasional child care and outside of school hours care services to inform the FYC Branch Service Review. Activities included consulting via on line survey and interviews at leisure centres with 96 parents about occasional child care.
Lead project planning to develop new children and youth strategy for 0 to 25 age group to replace completed or soon to be completed strategies.

Branch ***Family, Youth and Children's Services***

Unit ***Youth and Middle Years***

Yarra Youth Services hosted the launch of the Jobs for Youth Campaign 2017 at Bargoonga Nganjin on 17 July. The event was hosted by Saba, and 3 young people presented from YEN program (as well as presentations from the Mayor and Oxygen/Coles). Event was attended by nearly 120 people

Yarra Youth Services / FreeZA program programmed a number of gigs as part of the Leaps & Bounds Music Festival (13 – 23 July). This included working with The Push (Pop-Up Shops and Push Summit); delivering The Turn Up @ The Laundry; and Dream On Dreamer gig at The Evelyn. Really good engagement from young people, and lots of opportunities for young people from FreeZA & Rising High music programs to perform live in their community.

Yarra Youth Services was heavily involved in this year's Gertrude Street Projection Festival, and received special thanks at their VIP night on 26 July. All our the Artists in Residence had opportunities to showcase their work throughout the festival; we supported a number of other young artists to exhibit; and participants from the Fashion & Textile Design program designed and modelled outfits made specifically for the festival. The feedback from young people, GSPF organisers and the community was sensational.

Joining the Dots Forum run by INLLEN, opened by Mayor Cr Stone. Was the final forum as part of INLLEN's long-term grant for Joining the Dots project (which ends 31/12/2017). Very strong engagement from schools, community organisations.

YYAC meeting #3, 6 young people attended, plus 2 Councillors. Young people provided insight into 0-25 plan consultation with young people, plus narrowed down focus of work they want to do through YYAC (Youth Employment, Drugs & Alcohol, and Mental Health).

Yarra Youth Services put on First Aid training for young people at Youth Centre (based on similar Training Calendar model used by Oxygen Youth Services). 15 participants took part, with overwhelmingly positive feedback. Will look at delivering another session later this year (based on feedback/ideas from young people).

In a first, two participants in our L2P Learner Driver Mentor Program, Betul and Sofia, both passed their 'Ps test' on the same day (23/08). Story featured in Monday Musings 28/08. Betul was also the 100th young person to get her P-plates through Yarra's L2P Program (and a celebration event is planned for November).

Yarra Youth Services & the drum youth services both launched their Peer Leader program at the Yarra Youth Centre on 29/08, with an event celebrating the six new Youth Peer Leaders commencing with both organisations (2 @ YYS, 4 @ the drum). The event included an acknowledgement of country and performance from Culture Evolves, presentation from the drum, and yummy food and lots of conversation with young people, staff, and reps from local services (including NJC).

Yarra Youth Services hosted the Yarra Youth Providers Network meeting on 1 September at the Fitzroy Reading Room. The Rainbow Network presented on 'Breaking Down the Binary- An introduction to supporting gender and sexual diversity and challenging homo/bi/trans-phobia in youth work settings'. Approximately 25 workers from the Youth sector in Yarra were in attendance.

The September School Holiday Program took place during the school holidays, with excursions to Arthur's Seat High Ropes Course, Bounce! Trampolining centre, and The Escape Room, amongst others. The Youth Centre was also opened up on each Thursday to trial a more casual drop-in style service, allowing

Highlights and Achievements

young people to come and hang out and engage with staff, have lunch and participate in various in-house activities.

Yarra's Youth-led & Annual Grants were assessed by two separate panels on 19 & 20 September respectively. The Youth-led Grants received 5 applications (up from 2 last year), and was highly competitive. Annual Grants were assessed by joint panel across FY&C Branch, who looked at Early Years, Middle Years & Youth grants together for the first time.

The graduation for the Young Entrepreneurs Network (YEN) was held on Tuesday 12 September at the Fitzroy Town Hall. The YEN is a collaboration between City of Yarra and City of Moreland, which provides young people who have business ideas the opportunity to receive training and be mentored with the ultimate hope of seeing their idea come to fruition. The graduation was attended by Mayor Amanda Stone, plus a councillor from Moreland, and included presentations from the young people who graduated from the 2017 program.

2. An inclusive Yarra

a place where…Inclusion, diversity and uniqueness are welcomed, respected and celebrated

Identity was a passionate and recurring theme throughout community consultation for the Council Plan. Our community values its diversity. Creating a safe place where people of all ages, genders, sexualities, abilities, cultures and backgrounds feel welcome and connected is a priority for Council.

Strategies

Council's work to achieve this Strategic Objective will include the following strategies:

2.1 Build resilience by providing opportunities and places for people to meet, be involved in and connect with their community Continue to create a resident-friendly city that reduces isolation, improves access to the built environment and builds social connections.
2.2 Remain a highly inclusive Municipality, proactive in advancing and advocating for the rights and interests of specific groups in the community and community issues Encourage greater social cohesion and participation through volunteer initiatives and community development activities.
2.3 Continue to be a local government leader and innovator in acknowledging and celebrating Aboriginal history and culture in partnership with Traditional Owners Deliver Council services that meet community priorities and needs.
2.4 Acknowledge and celebrate our diversity and people from all cultural backgrounds.
2.5 Support community initiatives that promote diversity and inclusion.

The following actions are being undertaken in 2017/18 to work toward achieving Council's strategic objective of An inclusive Yarra.

Action Progress Summary

- At least 90% of action target achieved
- Between 75 and 90% of action target achieved
- Less than 75% of action target achieved
- Not Started
- Completed

Target
% Complete

Action	Start Date / End Date	
2.01 Develop a Volunteer Strategy	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
2.02 Develop Human Rights Charter	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
2.03 Stolen Generations Marker project	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
2.04 Implement Aboriginal Partnership Plan	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
2.05 Implement Multicultural Partnerships Plan 2015-18	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
2.06 Develop LGBTIQ Strategy	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
2.07 Develop new Positive Aging Strategy and Action Plan	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
2.08 Develop new Access and Inclusion Strategy and Action Plan	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%

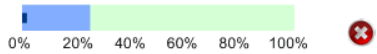
Attachment 1 - Annual Plan Quarterly Progress Report September 2017

Action	Start Date / End Date	
2.09 Develop Public Spaces policy	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%

2.01 Develop a Volunteer Strategy

Council Plan initiative:
Develop and adopt a Volunteer Strategy.

A volunteer strategy will be developed by consultation and engagement with stakeholders . A draft strategy will be presented to Council. A Volunteer Strategy will provide clear direction for Council in how it supports volunteering in Yarra.



Branch *People, Culture and Community*

Quarterly Milestones

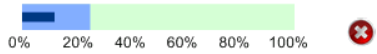
September	Q1. Investigate other local government Volunteer Strategies
December	Q2. Commence consultation with stakeholders
March	Q3. Complete consultation with stakeholders
June	Q4. Complete Draft Volunteer Strategy

Quarterly Progress Comments Investigation of other volunteer strategies has been extended to December 2017

2.02 Develop Human Rights Charter

Council Plan initiative:
Investigate developing a Council social justice/human rights charter to better articulate council's commitment .

The development of a Human Rights Charter for Council will provide an over-arching commitment to human rights which will then inform everything Council does. The charter will be developed in consultation and engagement with stakeholders.



Branch *People, Culture and Community*

Quarterly Milestones

September	Q1. Discuss and investigate other local government Human Rights Charters
December	Q2. Commence consultation with stakeholders
March	Q3. Complete consultation with stakeholders
June	Q4. Complete Draft Human Rights Charter

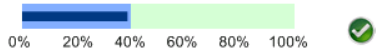
Quarterly Progress Comments Officers are in the process of doing a desktop review of other local government human rights and social justice charters, and where they sit within their respective Councils.

A project plan is also being drafted. Investigation of other Human Rights Charters will continue until December 2017.

2.03 Stolen Generations Marker project

Council Plan initiative:
Complete the Stolen Generations Marker project in partnership with the local aboriginal community .

Yarra is deeply proud of its connection to the Aboriginal community and its commitment to reconciliation . The Stolen Generations Marker is a project initiated by the Aboriginal Advisory Group which leads Council to commissioning a public artwork to remember the Stolen Generations. It is imperative that the Aboriginal community and in particular members of the Stolen Generation have ownership of the process from beginning to end.



Branch *People, Culture and Community*

Quarterly Milestones

September	Q1. Secure funding for the project
	Q1. Select artist
December	Q2. Finalise artwork design
March	Q3. Fabricate artwork
June	Q4. Install and launch the Stolen Generations Marker

Quarterly Progress Comments

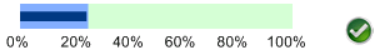
An expression of interest process to select an artist was unsuccessful, instead an artist has been commissioned to produce a concept design.

Funding for the project has been secured through the 2017/18 budget process, supplemented by other fund raising activities.

2.04 Implement Aboriginal Partnership Plan

Council Plan initiative:
Implement the Aboriginal Partnership Plan.

The Aboriginal Partnerships Plan sets out Council's commitment and work with the Aboriginal and Torres Strait Islander community. Council is seen as a leader, not only in its programs, but also in its commitment to social justice and the way it approaches issues, opens discussion, and listens to the community. Council will continue to implement its Aboriginal Partnerships Plan through annual action plans endorsed by Council.



Branch *People, Culture and Community*

Quarterly Milestones

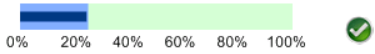
September	Q1. Continue implementation of the Aboriginal Partnerships Action Plan 2017
December	Q2. Continue implementation of the Aboriginal Partnerships Action Plan 2017
March	Q3. Continue implementation of the Aboriginal Partnerships Action Plan 2017
June	Q4. Continue implementation of the Aboriginal Partnerships Action Plan 2017

Quarterly Progress Comments

The following Aboriginal Partnerships Action Plan 2017 activities have been undertaksn;
- January 26th project: 13 recommendations were adopted at the 15th August Council meeting.
- Charcoal Lane Aboriginal mural: Aboriginal artist Robert Young was commissioned & completed this mural on the Charcoal Lane building.
- Stolen Generation marker project: the steering group has engaged an Aboriginal artist, who cannot be named yet, to produce a concept design to present to the steering group.
- Smith St Dreaming festival (July) was a great success, with as many as 500 people attending over the day.

2.05 Implement Multicultural Partnerships Plan 2015-18

The implementation of the Multicultural Partnerships Plan is Council's primary way to acknowledge and promote the contribution to Yarra that people from culturally, linguistically and religiously diverse backgrounds have made. Council will continue to implement its Multicultural Partnerships Plan through annual action plans endorsed by Council.



Branch *People, Culture and Community*

Quarterly Milestones

September	Q1. Continue implementation of the Multicultural Partnerships Action Plan 2017
December	Q2. Continue implementation of the Multicultural Partnerships Action Plan 2017
March	Q3. Multicultural Partnerships Action Plan 2018 has been endorsed by Council
June	Q4. Continue implementation of the Multicultural Partnerships Action Plan 2018

Quarterly Progress Comments

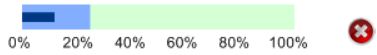
The following Multicultural Partnerships Action Plan 2017 activities have been undertaken;
- Ongoing relationship building of the Yarra Multicultural Advisory Group, with a meeting being held in August 2017.
- Continued promotion of Council Information Pack in languages other than English to Yarra CALD groups.
- Involvement in induction training re cultural diversity and inclusion conducted August and September 2017.

- Yarra Settlement Forum meetings held, July, August, September 2017.
- Ongoing support provided to Yarra Interfaith Network, with regards to their funding application to the Victorian Multicultural Commission, and connecting to libraries staff re historical project.
- Ongoing work continuing on Anti-vilification project "Stand Together" which will run throughout 2017. Resources are due to be printed and planning under way for training Community Facilitators.

2.06 Develop LGBTIQ Strategy

Council Plan initiative:
Advocate to support social justice and community issues including preventing family violence, LGBTIQ rights and welcoming refugees

Council's LGBTIQ working group will develop a draft LGBTIQ Strategy. The development of this strategy will involve consultation and engagement with stakeholders.



Branch *People, Culture and Community*

Quarterly Milestones

September	Q1. Discuss and investigate other local government LGBTIQ Strategies
December	Q2. Commence consultation with stakeholders
March	Q3. Complete consultation with stakeholders
June	Q4. Complete Draft LGBTIQ Strategy

Quarterly Progress Comments Officers are in the process of conducting a desktop review of other municipalities LGBTIQ policies and strategies.

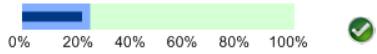
A project plan is also being drafted. Investigation of other LGGBTIQ strategies has been extended to December 2017.

2.07 Develop new Positive Aging Strategy and Action Plan

Council Plan initiative:
Work with the community and other levels of government and advocate to challenge discrimination , and address disadvantage, whether based on income, age, gender, sexuality, cultural background, religion or abilities .

The current Positive Ageing Strategy and Action Plan is due for renewal and with the support of Council's Active Ageing Advisory Group (AAAG), a new strategy and plan will be prepared this year. In the midst of the national aged care reforms, it provides an opportunity to consider strategic directions to ensure our 50+ residents remain engaged, active and independent.

The current strategy and plan is due for renewal and with the support of Council's Disability Advisory Committee (DAC), a new strategy and plan will be prepared this year. In the midst of the national disability care reforms, it provides an opportunity to consider Council's strategic directions to ensure people with disability remain engaged , active and empowered.



Branch *Aged and Disability Services*

Quarterly Milestones

September	Q1. Initiate community planning process and seek advice of the Active Aging Advisory Group to guide community consultation and planning.
December	Q2. Finalise Background Paper, including demographic analysis, current approaches to supporting people to age well, trends and indicative issues.
March	Q3. Complete community consultation and develop draft Strategic Objectives; Key Themes and proposed actions.
June	Q4. Present the Positive Ageing Strategy / Action Plan 2018 – 2022 for adoption by Council.

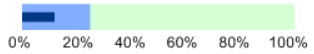
Quarterly Progress Comments The draft Project Plan for the development of the new Positive Aging Strategy has been provided to the Active Aging Advisory Group for comment/feedback,

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

2.08 Develop new Access and Inclusion Strategy and Action Plan

Council Plan Initiative:
Work with the community and other levels of government and advocate to challenge discrimination , and address disadvantage, whether based on income, age, gender, sexuality, cultural background, religion or abilities.

The current strategy and plan is due for renewal and with the support of Council's Disability Advisory Committee (DAC), a new strategy and plan will be prepared this year. In the midst of the national disability care reforms, it provides an opportunity to consider Council's strategic directions to ensure people with disability remain engaged , active and empowered.



Branch *Aged and Disability Services*

Quarterly Milestones

September	Q1. Initiate community planning process and seek advice of the DAC to guide community consultation and planning
December	Q2. Finalise Background Paper, including demographic analysis, current approaches to supporting people with disability to live an ordinary life, trends and indicative issues
March	Q3. Complete community consultation and develop draft Strategic Objectives; Key Themes and proposed actions
June	Q4. Present the Access and Inclusion Strategy / Action Plan 2018 – 2022 for adoption by Council

Quarterly Progress Comments A draft Project Plan is being developed and will be presented to the Disability Action Committee at their meeting in November for feedback.

2.09 Develop Public Spaces policy

Council Plan initiative:
Implement policies and procedures that enable community groups to conduct events and activities.

Council will provide a one stop shop for event organisers to make event management easier and more transparent to event organisers, to provide Council with an overview of the activities and events across the municipality and to ensure public safety and risk management is sufficiently managed. The one stop shop is a single point of contact for internal and external customers, and provides advice and referral.

Branch *Arts, Culture and Venues*

Quarterly Milestones

December	Q2. Invite and assess Expressions of Interest for Major Events
March	Q3. Invite and assess Expressions of Interest for Major Events
June	Q4. Review Events in Public Spaces Policy as part of continuous improvement

Quarterly Progress Comments Process commences in quarter 2.

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

3. A sustainable Yarra

a place where…Council leads on sustainability and protects and enhances its natural environment

As Victoria's first carbon-neutral council, we are proud of our commitment to sustainability. Protecting our natural environment and supporting our community to reduce its environmental footprint will continue to be a priority for Council.

Strategies

Council's work to achieve this Strategic Objective will include the following strategies:

3.1 Investigate strategies and initiatives to better manage the long term effects of climate change. Continue to identify opportunities to convert road spaces and laneways for parks or improved pedestrian spaces.
3.2 Support and empower a more sustainable Council and Community. Advocate to protect green and open spaces on Yarra's public housing estates.
3.3 Lead in sustainable energy policy and deliver programs to promote carbon neutral initiatives for the municipality and maintain Council as a carbon neutral organization. Seek to achieve more communal private open space within large developments.
3.4 Reduce the amount of waste-to-landfill with a focus on improved recycling and organic waste disposal.
3.5 Promote responsible water usage and practices
3.6 Promote and facilitate urban agriculture with a focus on increasing scale and uptake in the community.
3.7 Investigate strategies and initiatives to improve biodiversity.

The following actions are being undertaken in 2017/18 to work toward achieving Council's strategic objective of A sustainable Yarra.

Action Progress Summary

- At least 90% of action target achieved
- Between 75 and 90% of action target achieved
- Less than 75% of action target achieved
- Not Started
- Completed

Target
% Complete

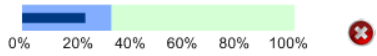
Action	Start Date / End Date	
3.01 Develop Biodiversity Strategy	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
3.02 Develop Urban Forest Strategy	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
3.03 Embed climate adaptation	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
3.04 Reduce energy use and emissions	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
3.05 Reduce organic waste	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
3.06 Integrate environmental and sustainability policies and strategies	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

3.01 Develop Biodiversity Strategy

Council Plan initiative:
Develop and adopt a Biodiversity Strategy .

Over the past year, a biodiversity study was undertaken in the City of Yarra. Council will draw on the research completed as part of the Biodiversity Health Survey to inform a city-wide strategy that will help in preserving and enhancing biodiversity values on public land in the municipality.



Branch *Recreation and Open Space*

Quarterly Milestones

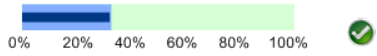
September	Q1. Complete project brief
December	Q2. Appoint consultants
June	Q4. Complete draft Biodiversity Strategy for presentation to Council

Quarterly Progress Comments The Streetscapes and Natural Values team has completed 75% of a Biodiversity Strategy scoping document that forms the basis of a project brief. The scoping document has been developed in conjunction with external consultants.

3.02 Develop Urban Forest Strategy

Council Plan initiative
Develop an Urban Forest Strategy and implement recommendations to achieve social and environmental outcomes.

Council commenced development of an Urban Forest Strategy in 2016/17. This year Council will finalise and adopt the Urban Forest Strategy. The Strategy will provide broad directions and key principles for the delivery of urban greening initiatives with a particular focus on reducing the impact of the Urban Heat Island effect .



Branch *Sustainability and Strategic Transport*

Quarterly Milestones

September	Q1. Present Urban Forest Strategy to Councillor Briefing
December	Q2. Present Draft Urban Forest Strategy to Council for adoption
June	Q4. Develop implementation plan

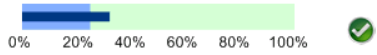
Quarterly Progress Comments The Urban Forest Strategy was adopted by Council in September 2017.

The Strategy guides Council's long term work to manage Yarra's urban forest , taking into consideration the current context, and the environmental, social and economic benefits trees provide.

3.03 Embed climate adaptation

Council Plan initiative:
Embed adaptation sustainability across Council decision making processes

In 2016/17 Council introduced a quadruple bottom line tool to inform the assessment of proposed projects to be included in the 2017/18 Budget. This process will be reviewed and, if necessary, improved as part of the preparation and development of the 2018/19 Budget to further embed climate adaptation into Council processes.



Branch *Sustainability and Strategic Transport*

Quarterly Milestones

September	Q1. Review and update processes to embed adaptation based on lessons from 2016/17
December	Q2. Integrate updated embedding adaptation process as part of the project funding request process
June	Q4. Evaluate outcomes and process

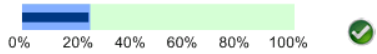
Quarterly Progress Comments The Award Winning Quadruple Bottom Line Tool has been updated, based on feedback received, for use again in the New Initiative Bid process.

A new Adaptation Guidance Tool and Training has been developed for new projects to better understand the potential climate impacts of their project, and develop an appropriate climate adaptation plan for it.

3.04 Reduce energy use and emissions

Council Plan initiative:
Continue to invest in initiatives to reduce energy use and emissions from Council operations.

Over many years Council has endeavoured to dramatically reduce its energy use and emissions. Previous programs have included the Energy Performance Contract and various capital works programs to install solar panels on many Council buildings. In 2016/17 Council participated in a novel tender process investigating the potential for a new Victorian renewable energy supply to meet Council's energy needs. In 2017/18 Council will consider whether to commit to the project which would commence delivery of energy in 2019.



Branch *Sustainability and Strategic Transport*

Quarterly Milestones

September Q1. Council to consider report on Melbourne Renewable Energy Project
December Q2. Complete installation of Solar Panels project
Q2. Complete and submit Corporate emissions inventory to claim carbon neutral status for 2016/17
June Q3. Update Council on status of the MREP project (If Council determines to join project),

Quarterly Progress Comments A report on the Melbourne Renewable Energy Project was presented to a confidential meeting of Council in September for consideration and action. Follow up activities are now underway.

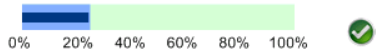
The project seeks to source Victorian renewable electricity to meet 100% of Councils needs

3.05 Reduce organic waste

Council Plan initiatives:
Investigate, implement and promote initiatives to divert organic waste from landfill, and

Reduce volume of kerbside waste collection per capita by behavior change and increase of recycling.

Yarra's Waste and Resource Recovery Strategy has identified the opportunity to potentially significantly reduce landfill costs by removing organic waste from the waste stream. A grant has been sought to assist in purchase of the necessary infrastructure to pilot a potential solution. It is also necessary to investigate how to most successfully engage with the community in adapting to this new waste service.



Branch *Sustainability and Strategic Transport*

Quarterly Milestones

September Q1. Brief Council on proposed food organics waste program
December Q2. Commence collection of food organic waste in pilot program
Q2. Launch phase 1 of food organic waste pilot program
June Q4. Evaluate food organic waste pilot program

Quarterly Progress Comments Council was briefed on the proposed food organics waste program in August 2017.

The Food Scraps Collection trial will introduce a new kerbside collection service to Yarra residents to recover food waste. This is a 3 year project being delivered in two distinct phases in conjunction with a grant from the Sustainability Victoria Resource Recovery Infrastructure Fund.

Phase One runs from November 2017 to December 2018 and will involve up to 500 households with the collected material being taken for composting at Veolia NRS Dandenong.

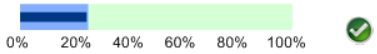
Attachment 1 - Annual Plan Quarterly Progress Report September 2017

Phase Two runs from January 2019 to March 2020 and will involve an additional 4500 households and include the procurement of an in-vessel composting unit to be housed and operational within Yarra.

3.06 Integrate environmental and sustainability policies and strategies

Council Plan initiative:
Improve integration across environmental and sustainability policies and strategies.

Each year Council can consider the adoption of new strategies. Council has established advisory committees that are able to provide feedback on new strategies. The Yarra Environment Advisory Committee will be invited to provide feedback on any new strategies that are developed



Branch *Sustainability and Strategic Transport*

Quarterly Milestones

September	Q1. Complete Home for All Seasons joint project with the HACC
December	Q2. Seek feedback from Yarra Environment Advisory Committee on any new Council strategies
March	Q3. Confirm integration project to be trialled by Embedding Green Infrastructure Project
June	Q4. Seek feedback from Yarra Environment Advisory Committee on any new Council strategies

Quarterly Progress Comments Home for All Season joint project with the HACC has now been completed. The aim of the project was to make the homes of HACC clients more comfortable and energy-efficient and improve their protection against extreme weather (heat-waves and cold snaps).

The project was run jointly between the Sustainability Unit and HACC as an integrated service. 16 Houses were retrofitted with 49 draught proofing products installed. The project has a lasting legacy with the home maintenance worker now now incorporating draught-proofing and blinds into his home maintenance work.

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

Highlights and Achievements

Branch ***Sustainability and Strategic Transport***

Unit ***Waste Minimisation***

Council successfully delivered the priority actions in the Waste and Resource Recovery Strategy . The highlights include:

The Food Know How program has evolved into a 5 week Challenge which guides households and the community on the steps of good food planning, shopping, storage, preparation, and composting.

RecycleUs; Councils bin inspection program, Recycle Us, educates and encourage residents to recycling right by given them direct relevant feedback about what is in their recycling and rubbish bin. On average the program has reduced contamination in recycle bin by 20% and reduced resource loss by 10% in the rubbish bin.

MUD Recycling Program, Our program for multi-unit developments produced a new way to engage with buildings and residents with the development of waste and recycling posters that are free for residents to access and download on our website. The program directly engaged with 20 locations per year, ranging in size and scale, from units at Richmond Department of Housing to 500 unit high rise towers.

Yarra's Community Resource Recovery Hubs, Three large outdoor and four indoor hubs (in libraries and neighbourhood houses) have been successful implemented throughout the municipality. These Hubs collect clothes, textiles, homewares, and small electrical items. We have consistently diverted just over 100 tonnes of material per year.

Commercial Shop Recycling Project, Engagement and education around recycling and managing waste better is now integrated into education program. The engagement package included one on one talks, information sheets, internal recycling bins, and stickers for external recycling bins.

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

4. A liveable Yarra

a place where... Development and growth are managed to maintain and enhance the character and heritage of the city

With demand for inner city housing increasing, Council is mindful of the importance of balancing the needs of new and existing residents, and ensuring that development does not encroach on the amenity or heritage of our city.

Strategies

Council's work to achieve this Strategic Objective includes the following strategies:

4.1 Protect Yarra's heritage and I neighbourhood character.
4.2 Actively plan for Yarra's projected growth and development and advocate for an increase in social and affordable housing.
4.3 Plan, promote and provide built form, open space and public places that are accessible to all ages and abilities.
4.4 Protect Council assets through effective proactive construction management.
4.5 Encourage and promote environmentally sustainable building, urban design, place-making and public realm outcomes.
4.6 Provide direction and improve decision making on infrastructure projects through the application of the Strategic Community Infrastructure Framework.
4.7 Encourage engagement with the community when developments are proposed.

The following actions are being undertaken in 2017/18 to work toward achieving Council's strategic objective of A liveable Yarra.

Action Progress Summary

- At least 90% of action target achieved
- Between 75 and 90% of action target achieved
- Less than 75% of action target achieved
- Not Started
- Completed

Target
% Complete

Action	Start Date / End Date	
4.01 Embed Community Infrastructure Planning Framework	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
4.02 Develop and implement Development Contribution Plan	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
4.03 Negotiate Joint Use Agreements with Richmond High School	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
4.04 Implement Heritage Strategy 2015-18	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
4.05 Review Heritage Strategy	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
4.06 Develop Yarra Housing Strategy	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
4.07 Develop Social and Affordable Housing Strategy	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
4.08 Re-write Yarra Planning Scheme	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

Action		Start Date / End Date	
4.09	Seek planning controls for Queens Parade, North Fitzroy	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
4.10	Prepare a built form analysis as part of the preparation of structure plans for major activity centres	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
4.11	Develop Activity Centre Structure Plans	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
4.12	Alphington Paper Mill site development	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
4.13	Understanding Planning in Yarra	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%

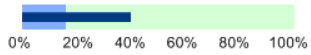
Attachment 1 - Annual Plan Quarterly Progress Report September 2017

4.01 Embed Community Infrastructure Planning Framework

Council Plan initiative:
Embed and integrate the Strategic Community Infrastructure Planning Framework in Council planning and decision making.

Following the endorsement of the Community Infrastructure Planning Policy and Strategic Community Infrastructure Framework (SCIF), Council will work on embedding the SCIF in planning and decision making processes.

Work commenced on the Community Infrastructure Plan (CIP), incorporating ten neighbourhood infrastructure plans in 2016/17. Five plans have been completed and the CIP will be presented to Council for endorsement . The remaining five neighbourhood plans will be completed this year.



Branch *Corporate Planning and Performance*

Quarterly Milestones

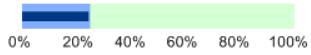
September	Q1. Complete Community Infrastructure Plan (including the first five neighbourhoods)
December	Q2. Complete remaining five neighbourhood infrastructure plans
	Q2. Present Community Infrastructure Plan (including first five neighbourhoods) to Council for endorsement
March	Q3. Present Community Infrastructure Plan (including remaining five neighbourhoods) to Council for endorsement
	Q3. Report to Executive on processes the SCIF informs
June	Q4. Commence implementation of process changes as agreed by Executive

Quarterly Progress Comments The Community Infrastructure Plan Stage 1 has been completed and will be presented to Council for adoption in the next quarter.

4.02 Develop and implement Development Contribution Plan

Council Plan initiative:
Progress a Planning Scheme amendment to implement a Development Contribution Plan .

Council will prepare a Planning Scheme Amendment to implement a Development Contributions Plan (DCP). A DCP will be used to collect payments towards the provision of infrastructure triggered by new development. Contributions will assist with the cost of providing roads, drainage, open space and community infrastructure to respond to the needs of a growing population.



Branch *Office of the Director Planning and Place Making*

Quarterly Milestones

September	Q1. Continue to prepare draft Development Contribution Plan
December	Q2. Report to Council on draft Development Contributions Plan to seek 'authorisation' by DELWP for public exhibition
March	Q3. Prepare for Panel hearing
June	Q4. Report to Council on Planning Panel report

Quarterly Progress Comments Work is progressing on the preparation of the draft Development Contribution Plan . Officers will seek a resolution from Council to seek authorisation from the Minister for Planning late this year to exhibit the proposed planning scheme amendment.

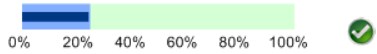
4.03 Negotiate Joint Use Agreements with Richmond High School

Council Plan initiative:
Campaign for appropriate joint use agreements for shared use community facilities as part of the Richmond High School project.

The new Richmond High School, in the Richmond Town Hall Precinct, will commence operation in 2018 for Year 7

students. There is a need to facilitate community use of the school grounds and for the school to be able to use some Council facilities subject to certain provisions.

Council will work with the Department of Education and Training to develop a Joint Use Agreement for community facilities as part of the Richmond High School project.



Branch *Office of the Director Planning and Place Making*

Quarterly Milestones

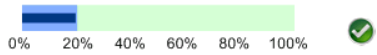
September	Q1. Commence discussions with school principal
December	Q2. Commence drafting Joint Use Agreement
March	Q3. Advocate draft Joint Use Agreement to DET
June	Q4. Seek DET approval for Joint Use Agreement

Quarterly Progress Comments Council has discussed the opportunity for joint use agreements with the Richmond High School Principal, who has indicated he is supportive in principle of developing these, and that the specifics will be determined as the needs and operations of the school are better understood.

4.04 Implement Heritage Strategy 2015-18

Council Plan initiative:
Strengthen the protection of Yarra's heritage through the planning scheme, education, and resource provision.

Council is committed to protecting and enhancing the City's unique heritage. The Heritage Strategy 2015-18 sets out the framework within which Council seeks to address the challenges and opportunities presented by Yarra's tangible and intangible heritage.
Council is progressively implementing its Heritage Strategy 2015-18 and adopted implementation plan.



Branch *City Strategy*

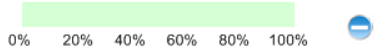
Quarterly Milestones

September	Q1. Commence oral history background report identifying priorities and methodology
December	Q2. Review content of 17 existing heritage walks
	Q2. Finalise heritage oral history report
	Q2. Commence preparations for improved fact sheets promotions regarding Yarra heritage
June	Q4. Complete format and platform for at least five heritage walks

Quarterly Progress Comments The oral history background report identifying priorities and methodology has commenced.

4.05 Review Heritage Strategy

The existing Heritage Strategy 2015-18 sunsets during the council term. Council will review and prepare a new Heritage Strategy and Action Plan which will commence in 2018.



Branch *City Strategy*

Quarterly Milestones

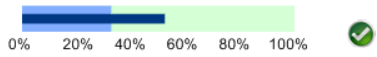
March	Q3. Scope the review of the Heritage Strategy
June	Q4. Commence the review of the Heritage Strategy

Quarterly Progress Comments Project scheduled to commence in quarter 3.

4.06 Develop Yarra Housing Strategy

Council Plan initiative:
Prepare a Housing Strategy to manage residential growth .

A housing Strategy will assist Council to engage with the State Government on growth in Melbourne . It is also a fundamental aspect to underpin the re-write of the Yarra Planning Scheme. The Strategy will assist Council in seeking to manage residential growth.



Branch *City Strategy*

Quarterly Milestones

September	Q1. Brief Councillors on draft policy directions and consultation
December	Q2. Complete draft strategy
March	Q3. Present draft strategy to Council.

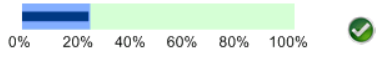
Quarterly Progress Comments	A Councillor workshop was held in April 2017, briefing paper presented to Council in September 2017 Early draft strategy completed, capacity testing to be completed

4.07 Develop Social and Affordable Housing Strategy

Council Plan initiative:
Advocate to federal and state governments through Yarra IMAP . MAV, ISMMF, VLGA on affordable and community housing .

Through Council Plans and other strategic documents, Council has expressed its commitment to maintain and support a socially, economically and culturally diverse community. This commitment is further evidenced by the financial support it has provided to the community housing sector, its resolute pursuit of affordable housing outcomes at major development sites and strategic advocacy to state and commonwealth governments.

In 2017/18, a Policy Guidance Note will be published so that property development applicants, and other interested parties, can understand Council's expectations relating to affordable housing outcomes at significant redevelopment sites. A broader Social and Affordable Housing strategy, encompassing strategic directions for Council through its roles as a planning authority and advocate, will be developed.



Branch *Social Policy and Research*

Quarterly Milestones

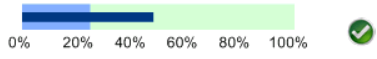
September	Q1. Council endorse Policy Guidance Note: Affordable Housing in Private Developments
December	Q2. Launch Policy Guidance Note and promote to stakeholders
March	Q3. Seek Council endorsement for public consultation on Draft Social and Affordable Housing Strategy
June	Q4. Council endorse the Social and Affordable Housing Strategy

Quarterly Progress Comments	The Policy Guidance Note: Affordable Housing in Significant Developments will go to Council in October for approval for public exhibition. SPR facilitated a stakeholder consultation session on two affordable housing topics – i.e. the policy note and positions on DHHS renewal.

4.08 Re-write Yarra Planning Scheme

Council Plan Initiative:
Complete the re-writing of policies in the Yarra Planning Scheme

Council is required to review its Planning Scheme each four years. The review has been completed and Council now needs to re-write an updated Yarra Planning Scheme with particular regard to the State Government's Metropolitan strategy known as Plan Melbourne. Solid strategic justification is required on a number of key aspects to underpin the re-write. The re-write will be completed this year and the statutory amendment process will commence.



Branch *City Strategy*

Quarterly Milestones

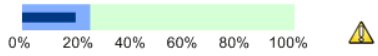
September	Q1. Continue preparation of draft policy material
December	Q2. Continue preparation of draft policy material
March	Q3. Brief Council on draft scheme structure and policies
June	Q4. Prepare report to Council to seek authorisation of amendment for exhibition

Quarterly Progress Comments Preparation of the draft policy material is progressing, a briefing of Council will take place during the third quarter.

4.09 Seek planning controls for Queens Parade, North Fitzroy

Council Plan initiative:
Prepare a Planning Scheme amendment seek permanent Development and Design Overlay controls for Queens Parade, North Fitzroy.

Council has worked to seek planning controls for the Queen's Parade precinct. Further work is now required which will include preparation of a Planning Scheme amendment to introduce new planning controls for Queens Parade.



Branch *City Strategy*

Quarterly Milestones

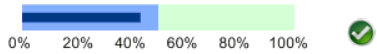
September	Q1. Finalise built form analysis and draft amendment
December	Q2. Report to Council to seek authorisation of the planning scheme amendment for exhibition
March	Q3. Exhibit amendment subject to receiving authorisation by Minister for Planning
June	Q4. Seek Planning Panel to consider submissions subject to decision by Minister for Planning

Quarterly Progress Comments Draft urban design and heritage reports are being finalized, draft amendment documents are being prepared for presentation at a November Council Briefing.

4.10 Prepare a built form analysis as part of the preparation of structure plans for major activity centres

Council Plan initiative:
Prepare a built form analysis as part of the preparation of structure plans for major activity centres.

In order to seek to manage development pressure in the City and to be able to prepare key aspects of the Yarra Planning Scheme re-write Council will undertake built form analysis studies of the major shopping centre areas (and environs). The built form analysis studies will inform the preparation of Structure Plans.



Branch *Office of the Director Planning and Place Making*

Quarterly Milestones

September	Q1. Complete studies for Swan Street, Richmond, Queens Parade North Fitzroy and Johnston Street, Abbotsford (revisions)
December	Q2. Complete studies for Brunswick Street and Smith Street; Victoria Street and Bridge Road

Quarterly Progress Comments The Swan Street Built form analysis is being completed, a request to the Minister for Planning for a planning scheme amendment authorisation will take place in the next quarter.

Queens Parade North Fitzroy Built form work is being finalised and will be presented to Council in the next quarter.

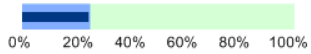
Johnston Street, Abbotsford Amendment C220 currently on exhibition

4.11 Develop Activity Centre Structure Plans

Council Plan initiative:
Continue to develop structure plans for Yarra's major activity centres which build on the unique character of each precinct.

In order to seek to best manage development pressures it is important to develop structure plans for the main activity centres. This requires solid analysis on a number of key land use, transport and development matters. Structure Plans will begin to be developed once these important preliminary studies are completed.

Swan Street major activity centre already has an adopted Structure Plan. Council's priority this year is to develop Structure Plans for the Brunswick Street, Smith Street, Victoria Street and Bridge Road major activity centres.



Branch *City Strategy*

Quarterly Milestones

September	Q1 Commence Structure Plan scoping
December	Q2. Continue Structure Plan scoping
March	Q3. Prepare background material for Structure Plans
June	Q4. Prepare draft Structure Plans

Quarterly Progress Comments Scoping the development of Activity Structure Master Plans has commenced.

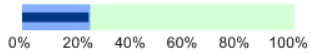
4.12 Alphington Paper Mill site development

Council Plan initiative:
Implement the 2016 approved development plan for the former Alphington Paper Mill (AMCOR) site.

The Alphington Paper Mill site is a 16.5 hectare parcel of land located on the corner of Heidelberg Road and the Chandler Highway and extending down to the Yarra River. The site is set to be developed into a major residential precinct with shops, offices, open spaces and community facilities.

On 2 December 2015, Yarra City Council unanimously approved the revised Development Plan for the Alphington Paper Mill subject to conditions including extra protections for the Yarra River frontage and the establishment of a community reference group.

This year Council will implement the Development Plan through statutory approval processes and infrastructure approvals.



Branch *Office of the Director Planning and Place Making*

Quarterly Milestones

September	Q1. Assess planning permit applications and civil work proposals against the provisions of the Development Plan
December	Q2. Assess planning permit applications and civil work proposals against the provisions of the Development Plan
March	Q3. Assess planning permit applications and civil work proposals against the provisions of the Development Plan
June	Q4. Assess planning permit applications and civil work proposals against the provisions of the Development Plan

Quarterly Progress Comments Two Planning Applications were issued during the quarter. One subdivision permit has been issued and is awaiting completion, six subdivision applications have been lodged and are being considered for permits.

4.13 Understanding Planning in Yarra

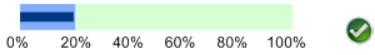
Council Plan initiative:
Facilitate community dialogue to assist the community's understanding of Council's role, responsibility and

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

limitations in delivering planning and development outcomes.

All Councils administer a Planning Scheme in their municipal district that are framed within the Victorian Planning Provisions. The Victorian Planning Provisions limit the content of the Planning Scheme and Council's ability to change it.

Council will work to inform the community of the limitations that apply to the municipality regarding planning provisions and controls to facilitate better understanding of the Victorian Planning Provisions .



Branch *City Strategy*

Quarterly Milestones

September	Q1. Include a planning feature in Yarra News
December	Q2. Prepare communications plan to promote an improved understanding of the Victorian Planning System to the community Q2. Finalise communications plan for the heritage strategy implementation
March	Q3. Publish second planning feature in Yarra News
June	Q4. Continue to roll out material and information to assist the community understanding of planning matters in Yarra and limitations on Council
Quarterly Progress Comments	A feature on Planning Ahead for Housing and Jobs appeared in the September/October Yarra news. While this focus was more about strategic work currently underway it also focused on the planning space in Yarra and provided links to finding out more about Yarra.

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

5. A prosperous Yarra

a place where…Local businesses prosper and creative and knowledge industries thrive

Yarra is a great place to do business and to work. Supporting local businesses and creative industries not only contributes to Yarra's economy, but also increases local employment opportunities, enhances street life and fosters community connectedness.

Strategies

Council's work to achieve this Strategic Objective include the following strategies:

5.1 Maintain and strengthen the vibrancy and local identity of retail and commercial precincts.
5.2 Strengthen and monitor land use change and economic growth including new and emerging economic clusters
5.3 Create local employment opportunities by providing targeted and relevant assistance to facilitate business growth, especially for small and medium size enterprises and entrepreneurs through the attraction and retention of businesses.
5.4 Develop Innovative Smart City solutions in collaboration with government, industry and community that use technology to embrace a connected, informed and sustainable future.
5.5 Facilitate and promote creative endeavour and opportunities for the community to participate in a broad range of arts and cultural activities
5.6 Attract and retain creative and knowledge industries in Yarra.
5.7 Ensure libraries and neighbourhood houses, support lifelong learning, wellbeing and social inclusion

The following actions are being undertaken in 2017/18 to work toward achieving Council's strategic objective of A prosperous Yarra.

Action Progress Summary

- At least 90% of action target achieved
- Between 75 and 90% of action target achieved
- Less than 75% of action target achieved
- Not Started
- Completed

Target
% Complete

Action	Start Date / End Date	
5.01 Review Library facilities	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
5.02 Support live music	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
5.03 Protect and create affordable and appropriate creative infrastructure	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
5.04 Expand the arts sector	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
5.05 Develop shopping strip masterplans	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
5.06 Deliver Village Activation program	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
5.07 Address shop vacancies	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
5.08 Develop Yarra Spatial Economic Employment Strategy	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

2017/18 Annual Plan Quarterly Progress Report September

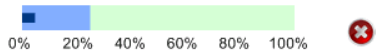
Action	Start Date / End Date	
5.09 Deliver Business Events Program	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
5.10 Review Economic Development Strategy	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
5.11 Develop Open Data Policy	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

5.01 Review Library facilities

Council Plan initiative:
Provide spaces, expertise and practical supports to encourage creative uses in Libraries.

Council will identify flexible spaces to facilitate creative uses across Yarra Libraries.



Branch *Library Services*

Quarterly Milestones

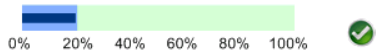
September	Q1. Conduct an audit of existing library facilities
December	Q2. Identify opportunities for creative spaces within each facility
March	Q3. Develop report on findings
June	Q4. Develop implementation plan

Quarterly Progress Comments Council has started with Carlton Library and redesigning the layout

5.02 Support live music

Council Plan initiative:
Continue to recognise and support live music in Yarra through advocacy, grants, funding and partnerships..

Council will deliver a range of activities and initiatives to support live music to encourage economic development and business viability, cultural and tourism development and celebrate the live music scene. Yarra is an established city of music, we have many live music venues, some of them with international profile. The creative sector is a major industry of Yarra.



Branch *Arts, Culture and Venues*

Quarterly Milestones

September	Q1. Collaborate on the commissioning of a sculpture of Molly Meldrum for Richmond
December	Q2. Promote funding and schemes that are specifically targeted to live music
March	Q4. Develop an industry development strategy to be delivered as part of Leaps and Bounds Festival
June	Q4. Design and deliver a live music round table to discuss sector issues
	Q4. Launch the Molly Meldrum Sculpture

Quarterly Progress Comments Fundraising for the Molly Meldrum sculpture is almost complete at $110,000. An application has also been submitted to Creative Victoria for further funding.

A Commissioning Agreement has been entered into with artist Louis Laumen.

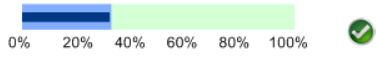
Consultation with commenced with Open Space with respect to the upgrades at Wangaratta Reserve.

Expected completion May 2018.

5.03 Protect and create affordable and appropriate creative infrastructure

Council Plan initiative:
Implement initiatives that will protect affordable and appropriate creative infrastructure including Council owned spaces for arts purposes.

Council will work to protect and create creative spaces. Access to affordable and appropriate creative spaces is vital to protecting Yarra's reputation as a creative city and creatives as a significant employment sector.



Branch *Arts, Culture and Venues*

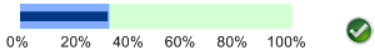
Quarterly Milestones

September	Q1. Implement a short term trial of creative residencies in Council facilities, and private facilities where available
December	Q2. Review creative residencies trial and investigate new residency opportunities
March	Q3. Support Dancehouse to complete and acquit the improvements funded by Room to Create at 150 Princes St Carlton
Quarterly Progress Comments	Three Council facilities have been approved for a short term trial of creative residences: William's Reserve, Radio Room, Florence Peel. These are short three month residencies. Information is being collated and further opportunities are being explored.

5.04 Expand the arts sector

Council Plan initiative:
Identify opportunities to support retain and expand the arts sector as a viable and thriving industry in Yarra .

The creative sector is important to Yarra socially, culturally and economically. Supporting this sector to flourish has many benefits to the City of Yarra, this includes providing financial support, skills development opportunities and facilitating other activities that promote sustainability.



Branch *Arts, Culture and Venues*

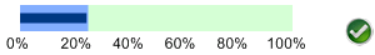
Quarterly Milestones

September	Q1. Support live music venues and businesses through the Leaps and Bounds Festival.
December	Q2. Promote the arts industry to business networks such as the Business Advisory Group.
March	Q3. Provide and promote skills and training for the arts sector to encourage resilience and sustainability.
Quarterly Progress Comments	Council produced the 5th Leaps and Bounds Music Festival in July 2017. This included gigs, special events and industry development events across the city. Many musicians, producers, venue bookers and owners and music business owners were involved and gave positive feedback about the impact the festival has on promoting live music and supporting the sector in winter.

5.05 Develop shopping strip masterplans

Council Plan initiative:
Undertake at least three streetscape Master plans for retail shopping strips based on Place Making principles.

Masterplans for the main shopping centres are used to guide capital works proposals of Council and State agencies and to advocate for specific improvements. Over the 4-year period of the Council Plan Council will prepare at least three streetscape masterplans for public realm improvements across Yarra's retail strip centres .



Branch *City Strategy*

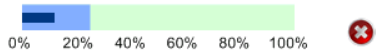
Quarterly Milestones

September	Q1. Finalise the draft Bridge Road Master plan following consultation
December	Q2. Seek Council's adoption of the Bridge Road Masterplan
March	Q3. Complete consultation on draft Brunswick Street master plan
June	Q4. Finalise draft Brunswick Street master plan
Quarterly Progress Comments	Consultation on the Bridge Road Master Plan has been completed and officers are currently in the final stages of preparing the Master Plan which will be presented to Council for adoption in October.

5.06 Deliver Village Activation program

Council Plan initiative:
Promote the benefits of doing business locally including the benefit of access by walking and cycling.

Shopping local reduces travel and benefits local trade. Council has previously encouraged local trade and this year a Village Activation program will be undertaken as a collaborative marketing campaign with businesses that promotes shopping locally. Council will work with local businesses to develop the program.



Branch *City Strategy*

Quarterly Milestones

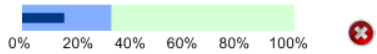
September	Q1. Develop promotional program in consultation with local businesses
December	Q2. Implement program
March	Q3. Complete program evaluation
June	Q4. Commence planning for next activation incorporating key learnings from evaluation

Quarterly Progress Comments Village Activation promotional program was developed in consultation with local businesses . A pilot Shop Local campaign commenced with Queens Parade traders. A subsequent campaign was developed with North Fitzroy Village traders, incorporating key learnings from the pilot.

5.07 Address shop vacancies

Council Plan initiative:
Engage with local traders, leasing agents and property owners to strengthen the viability of Yarra's activity centres such as Bridge Road including the activation of empty spaces.

Retail strips have considerable competition through other centres and online shopping. Vacancies in some retail centres such as Bridge Road have been high for some time. Council will continue to consult with property managers/owners on opportunities for addressing vacancies.



Branch *City Strategy*

Quarterly Milestones

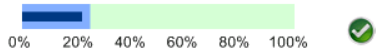
September	Q1. Discuss program with Bridge Road traders association and select property managers and owners of vacant properties
March	Q3. Implement program
June	Q4. Review and evaluate program

Quarterly Progress Comments A decal has been designed and costed in consultation with Bridge Road Mainstreet Association for installation on the window of a vacant properties, this will target a couple of vacant premises on Bridge Road. Funding and a lack of uptake by property owners is the reason for the limited trial.

5.08 Develop Yarra Spatial Economic Employment Strategy

Council Plan initiatives:
Prepare the Yarra Spatial Economic and employment Strategy to guide Yarra's long term capacity for employment growth.

The Yarra Spatial Economic Employment Strategy (SEES) provides a long term direction for managing land capacity to support Yarra's employment and economic growth that will inform the development of new planning policies and the re-write of the Yarra Planning Scheme.



Branch *City Strategy*

Quarterly Milestones

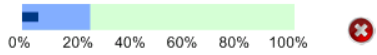
September	Q1. Finalise the strategy
December	Q2. Report draft SEES to Council for consideration
March	Q3. Commence preparing draft policies (based on the findings in the Strategy)
June	Q4. Complete draft policies (based on the findings in the Strategy)

Quarterly Progress Comments The Spatial Economic Employment Strategy is near completion and Council will be briefed on next quarter.

5.09 Deliver Business Events Program

Council Plan initiative:
Promote local employment and facilitate initiatives to assist small business to enter the local market.

For many years, Council has provided targeted business events to promote small and medium size business and networking opportunities. Council will continue to deliver a broad range of affordable and inspiring business seminars , workshops, events and support services to assist small and medium size enterprises and entrepreneurs.



Branch *City Strategy*

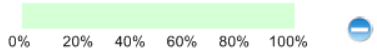
Quarterly Milestones

September Q1. Consult and review Business Event Program
December Q2. Prepare events calendar that includes programs that target small and medium sized businesses
March Q3. Promote and commence implementing the events program
June Q4. Continue to implement the events program

Quarterly Progress Comments The review of the Business Events Program has commenced and will continue next quarter..

5.10 Review Economic Development Strategy

The Economic Development Strategy outlines how Council can best support economic development by fostering greater investment and jobs growth in the municipality. Yarra's current strategy ends in 2018 and will require reviewing and updating to provide Council with a new strategy for the next few years.



Branch *City Strategy*

Quarterly Milestones

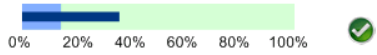
December Q2. Prepare project scope and seek funding
June Q4. Commence review

Quarterly Progress Comments Project scheduled to commence next quarter.

5.11 Develop Open Data Policy

Council Plan initiative:
Develop an Open Data Policy which provides open access for appropriate data sets to businesses and community organisations.

As part of the Council Plan 2017-2021, Council identified a goal to develop an Open data Policy. The Open Data Policy aims to foster greater transparency, responsiveness and accountability, to drive innovation and economic opportunities within the City of Yarra. The Open Data Policy will support how Council provides the community with access to meaningful data aids that empower and help shape Yarra in the future.



Branch *Information Services*

Quarterly Milestones

September Q1. Report to Executive seeking endorsement of the following:
- Purpose and Principles
- Endorsement to participate and publish data via MAV platform

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

December	Q2. Pilot internal Data Audit process (test case City Works)
	Q2. Develop Draft City of Yarra Open Data Policy
March	Q3. Develop Data Governance Framework
	Q3. Establish a data auditing schedule
	Q3. Identify top 10 data priorities for publishing
June	Q4. Continue to publish data via MAV Platform
Quarterly Progress Comments	Report presented to Executive on the use of data.gov.au
	Draft policy is being developed and will going to Executive in December 2017.
	Process to identify and audit data currently collected by the City Works branch is currently underway . This will be used to inform the development of the Data Governance Framework.
	Existing Council data identified for immediate upload to data.gov.au website. Other data to be identified and prioritised by the Information Services Steering Committee in 2018.

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

Highlights and Achievements

Branch ***Library Services***

Unit ***Community Learning and Partnerships***

We have run a lot of programs on Health and Wellbeing this quarter. Below are the names of the events and statistics.

	Bookings	Attendance
55+ Pilates Taster	15	7
Qigong	15	10
Introduction to drawing	13	9
Physical movement workshops for 50+	19	13
Know Your Rights sessions	16	4
Resume Rescue one on one session		8
Resume rescue – Group session of Key selection criteria	20	8
Food Know How sessions		18
Swing at the Library	53	26
Rescope event	84	25
Sustainable House Expo		50
Conversation lounge - Richmond		27
Conversation Lounge - Carlton		32
Tea Talk and Tech sessions – Collingwood		62
Tea Talk and Tech sessions - Fitzroy		46
Chinese Writers Festival		80
Chinese Reading Month events		68
Food family & culture: The Community Grocer	28	13
Homelessness in Yarra panel discussion	38	25
Collingwood Library clothing swap		30
Weaving a new life presentation	23	16
Living under drone attack and surveillance	35	23
Imagining art through technology		16
Thursday Write-In program		36
Collage Club		23

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

6. A connected Yarra

a place where…Connectivity and travel options are environmentally sustainable, integrated and well-designed

Council is committed to creating a city that is accessible to all irrespective of levels of personal mobility, to support a fulfilling life without the need for a car.

Strategies

Council's work to achieve this Strategic Objective includes the following strategies:

6.1 Manage traffic movement and promote road safety within local roads
6.2 Work in partnership with Vicroads and influence traffic management and road safety on main roads
6.3 Investigate and implement effective parking management options
6.4 Improve accessibility to public transport for people with mobility needs and older people
6.5 Develop and promote pedestrian and bicycle infrastructure that encourages alternate modes of transport, improves safety and connectedness
6.6 Advocate for increased infrastructure and performance of public transport across Melbourne

The following actions are being undertaken in 2017/18 to work toward achieving Council's strategic objective of A connected Yarra.

Action Progress Summary

- At least 90% of action target achieved
- Between 75 and 90% of action target achieved
- Less than 75% of action target achieved
- Not Started
- Completed

Target
% Complete

Action	Start Date / End Date	
6.01 Implement traffic management initiatives	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
6.02 Trial Station Street closure	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
6.03 Submit traffic safety initiatives and innovation grant applications	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
6.04 Advocate for DDA compliant trams stops	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
6.05 Advocate for improvement to tram stops near key schools sites	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
6.06 Advocate for improved Inner Regional public transport	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
6.07 Advocate for electric bus trial	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
6.08 Develop Car Share Policy	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
6.09 Undertake Bridge Road parking trial	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
6.10 Progress Wellington Street Bike Lane (stage 2)	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

Action	Start Date / End Date	
6.11 Manage Dockless Bikes	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
6.12 Develop advocacy strategy for Walmer Street Bridge upgrade	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

6.01 Implement traffic management initiatives

Council Plan initiatives:
Transition Local Area Traffic Management program to Local Area Place Making programs , and

Continue to provide low speed environments and community education for pedestrians, cyclists, motor-cyclists and vehicle drivers and passengers.

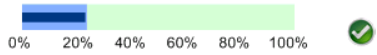
Council will implement initiatives to transition the Local Area Traffic Management program to Local Area Place Making programs.

Traffic calming measures and safety improvements will be delivered in the following LAPMs:

Consultation for LAPM 3 (Scotchmer), LAPM 13 (Abbotsford) and LAPM 19 (Bendigo)

Completion of works in LAPM 10 (Gold)

In addition to its LAPM program, Council will trial other initiatives to improve road safety including a 30 km/h speed trial in LAPM 9 (Rose) and LAPM 10 (Gold) and implementation of its Safe Travel Strategy.



Branch *Traffic Services and Special Projects*

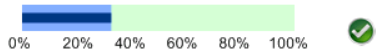
Quarterly Milestones

September	Q1. Submit application for external funding from the VicRoads Safe Travel Speeds on Local Streets program
	Q1. Submit report on proposed 30kph trial to Council
December	Q2. Obtain external funding for 30kph trials if approved
	Q2. Commence consultation and study process for LAPM 3 (Scotchmer)
March	Q3. Commence 30kph trial in LAPM 9 (Rose) and LAPM 10 (Gold) if approved
	Q3. Commence consultation and study process for LAPM 13 (Abbotsford) and LAPM 19 (Bendigo)
June	Q4. Deliver 2017/18 financial year actions from Council's Safe Travel Strategy
	Q4. Complete delivery of outstanding works in LAPM 10 (Gold)

Quarterly Progress Comments A report on the proposed 30kph trial was presented to Council in September and Council agreed to undertake the trial. An application for external funding has been submitted to VicRoads, the outcome should be known in early November 2017. The trial is scheduled to occur later in the 2017/18 financial year.

6.02 Trial Station Street closure

In 2016/17 Council resolved to progress a proposal to temporarily close Station Street at the intersection of Princes Street, North Carlton, to vehicles as part of a traffic diversion experiment. This experiment will be trialled initially for a 12 month period and will include a place making assessment to explore how the southern end of Station Street could be activated to the benefit of the community with a temporary or longer term road closure in place. The decision to proceed with the traffic diversion experiment will be made by Council following further community consultation.



Branch *Traffic Services and Special Projects*

Quarterly Milestones

September	Q1. Submit report and associated documents to VicRoads on road closure proposal
December	Q2. Undertake community consultation and report to Council on temporary road closure in Station Street, North Carlton
March	Q3. Submit internal funding proposal to undertake temporary road closure proposal (subject to Council adoption of proposal)

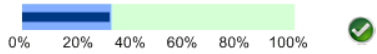
Quarterly Progress Comments A report and associated documents have been submitted to VicRoads. VicRoads has submitted its report on this matter which has been presented to Council. Council has resolved to undertake consultation with the North Carlton community on this matter.

Officers have commenced consultation process with community. A letter and public notice has been prepared. Consultation due to start on 23 October and will finish on 22 November 2017

6.03 Submit traffic safety initiatives and innovation grant applications

Council Plan initiative:
Investigate and apply for appropriate funding opportunities relating to road safety, innovation and streetscape improvements.

Council will apply for grants to deliver key projects such as 30km/h speed limit and Safe Travel Strategy initiatives.



Branch *Traffic Services and Special Projects*

Quarterly Milestones

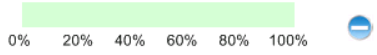
September	Q1. Apply for Federal Black spot and TAC Local Government grants
March	Q3. Complete blackspot projects at Shelley Street/Elizabeth Street, Richmond and Hunter Street/Nicholson Street, Abbotsford
June	Q4. Complete 2017/18 investigation projects applied for in August 2017 (if funding provided).

Quarterly Progress Comments The following traffic safety funding applications have been submitted:
- Blackspot funding application submitted to address safety issues at Wellington St / Langridge Street intersection.
- TAC funding application for Rose Street Feet First project and design project for Canning Street / Richardson Street intersection submitted.

6.04 Advocate for DDA compliant trams stops

Council Plan initiative:
Continue to facilitate the upgrade of local tram stops to comply with the Disability Discrimination Act to support both access and viability of activity centres.

Public transport in the City of Yarra needs to be made more accessible for people with disability and to comply with the provisions of the Commonwealth Government's Disability Discrimination Act (1992 and the Disability Standards for Accessible Public Transport (2002). Accessibility is at the forefront of ensuring independence and engagement for people 50+ and people with disability in our municipality. Council will focus on DDA compliant tram stops at Swan Street Punt Road as part of the Streamlining Hoddle Street project and Brunswick Street and Nicholson Street (Route 96).



Branch *Sustainability and Strategic Transport*

Quarterly Milestones

December	Q2. Update Council on Streamlining Hoddle Street project (which includes proposed DDA compliant design for Swan Street tram stop)
March	Q3. Discuss the Brunswick Street tram stops with Transport for Victoria
June	Q4. Brief Council on status of Route 96 DDA compliance project

Quarterly Progress Comments Activity scheduled to commence next quarter.

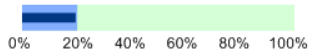
6.05 Advocate for improvement to tram stops near key schools sites

Council Plan initiatives:
Monitor and manage traffic, road safety and parking and advocate to the State Government for improvement to tram stops and pedestrian safety measures around schools such as the Richmond High School development, and

Advocate to the state government for improved accessibility to public to public transport services.

With the imminent opening of a new high school in Richmond, it is important to ensure that students are able to

access safe and sustainable transport options as part of their independent trips. Existing schools also attract students from wider Melbourne and provision of improved tram stops assists in improving safety around schools.



Branch *Sustainability and Strategic Transport*

Quarterly Milestones

September Q1. Develop concept for interim improved tram stop on Bridge Road at Richmond Town Hall

December Q2. Update Council on outcomes of advocacy for improved tram stop on Bridge Road at Richmond Town Hall
Q2. Advocate to State for inclusion of tram stop improvements outside Richmond Town Hall in 2018/19 State budget

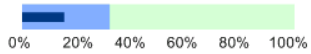
June Q4. Update Council on outcomes of advocacy for improved tram stop on Bridge Road at Richmond Town Hall
Q4. Advocate to State Government for the inclusion of tram stop improvements outside Richmond Town Hall in 2018/19 State budget

Quarterly Progress Comments A concept for an interim upgrade to the tram stop at Richmond Town Hall has been prepared and sent for consideration to VicRoads and YarraTrams. A meeting has been scheduled with the CEOs of PTV and VicRoads for November. It is considered that improvements being proposed by VicRoads under a TAC project may potentially include the interim works. The tram stop at Melbourne Girls College will also be discussed at that meeting.

6.06 Advocate for improved Inner Regional public transport

Council Plan initiative:
Advocate to the state government for improved public transport services to meet population growth including Chandler Highway north-south bus route, Doncaster Rail, Hoddle Street Study, Airport Rail and Alexandra Parade.

Much of Melbourne's transport network is under the authority of VicRoads rather than local government. Major state funded road infrastructure projects can also improve public transport service and reliability for the tram and bus network. However it is common that these transport modes are not the primary consideration of the project scope. Advocacy before, as well as during development of these major projects, can greatly improve the transport outcomes of the projects.



Branch *Sustainability and Strategic Transport*

Quarterly Milestones

September Q1. Provide input to VicRoads Alphington to Burnley corridor study
Q1. Provide feedback on VicRoads designs for Streamlining Hoddle Street project
Q1. Work with YarraTrams on opportunities to improve tram services in Yarra
Q1. Provide feedback on VicRoads designs for new Chandler Highway bridge project

December Q2. Provide feedback on VicRoads designs for new Chandler Highway bridge project
Q2. Inform Council on VicRoads construction schedule for Streamlining Hoddle Street project
Q2. TransDev to brief Council on Bus Rapid Transport project for Eastern Freeway
Q2. Continue to advocate for improved bus connection along Alexandra Parade

March Q3. Update Council on VicRoads Streamlining Hoddle Street project
Q3. Continue liaison with VicRoads regarding Chandler Highway construction

June Q4. Update Council on VicRoads Streamlining Hoddle Street project
Q4. Update Council on new Chandler Highway bridge project

Quarterly Progress Comments Quarterly project planning meetings are taking place between Council and YarraTrams. Current projects under discussion include Lygon Street tram track renewal works. Feedback provided to YarraTrams about the benefits of good consultation on the project. A meeting with Ward Councillors is scheduled in the next quarter.

Feedback provided on multiple stages of the design packages for the Chandler Highway bridge project.

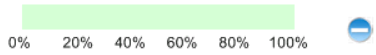
Officers continue to advocate for excellent pedestrian and bicycle facilities , and excellent urban design and landscaping outcomes for the Chandler Highway project.

Officers are progressing discussion about ultimate maintenance arrangements for the new and existing infrastructure for the Chandler Highway and Streamlining Hoddle Street projects.

6.07 Advocate for electric bus trial

Council Plan initiative:
Advocate for the trial of an electric bus scheme .

Electric buses have been the first major demonstration of electric powered transport in a number of cities. The public transport bus fleet is under the authority of Transport for Victoria . To achieve an electric bus trial it is necessary to engage with bus service contractors and advocate to the State Government to implement a trial of the vehicles.



Branch *Sustainability and Strategic Transport*

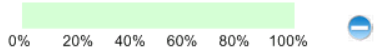
Quarterly Milestones

December Q2. TransDev to brief Council on Bus Rapid Transport project for Eastern Freeway
Q2. Develop advocacy strategy to promote an electric bus scheme
March Q3. Implement advocacy strategy

Quarterly Progress Comments Activity scheduled to commence in the next quarter.

6.08 Develop Car Share Policy

Car Share schemes have operated in Yarra for over a decade. Previously provision of parking spaces for the car share vehicles has been able to be provided without impacting private on-street parking space availability. A new policy will assist allocation of existing and potentially new spaces between competitor companies, and the distribution of spaces across the municipality to reduce the demand for private on street parking.



Branch *Sustainability and Strategic Transport*

Quarterly Milestones

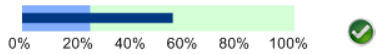
December Q2. Present draft Car Share Policy to Council for consideration for public exhibition
June Q4. Present Car Share Policy to Council for adoption

Quarterly Progress Comments Activity scheduled to commence in the next quarter.

6.09 Undertake Bridge Road parking trial

Council Plan initiative:
Continue to utilise data, technology and community consultation to inform the management of parking .

Council will complete a parking trial and management strategy for Bridge Road to be used as a model for other precincts. The trial will include the installation of in ground sensors to allow for detailed data collection and analysis that could lead to a review parking restrictions and/or the fee charged for parking.



Branch *Compliance and Parking Services*

Quarterly Milestones

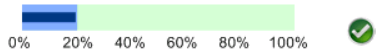
September Q1. Finalise the brief for the parking management strategy in Bridge Road
December Q2. Complete the first data reporting from the in-ground technology

March	Q3. Finalise the model and project plan for a trial
June	Q4. Implement the trial
Quarterly Progress Comments	An external consultant has been appointed to assist in the development of the Bridge Road parking Management Strategy and have met with the trader group to help develop the plan. In ground sensors have been installed in 340 bays in Bridge Road to allow for the collection of the occupancy data. The report will be prepared by 31/10/2017 and then be presented to the trader group for discussion and agreement on the trial methodology. Trial is likely to commence in February 2018 .

6.10 Progress Wellington Street Bike Lane (stage 2)

Council Plan initiative:
Complete construction of the Wellington Street Bike (Copenhagen style) Lane to Johnston street.

Wellington Street Bicycle Lane (Stage 1) involved the construction of a fully separated bicycle lane on both sides of the road from Victoria Parade to Gipps Street and was completed in 2015. The original concept for project extended the bicycle lane to Johnston Street; resulting in 1km of fully separated bicycle infrastructure. This project will complete the second stage of the original concept.



Branch *Sustainability and Strategic Transport*

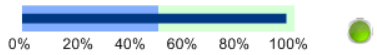
Quarterly Milestones

September	Q1. Finalise tender documentation
December	Q2. Inform community of upcoming tender and tender works
March	Q3. Report to Council on tender outcomes and determine if town planning permit required or not
June	Q4. Inform community of project status Q4. Lodge town planning permit application if required. If no town planning permit required, commence construction.
Quarterly Progress Comments	Tender documents have been finalised and tenders will be sought in the next quarter.

6.11 Manage Dockless Bikes

Dockless Bike Share companies operate a disruptive business model in many cities in Europe, Asia and the USA. The operator companies have identified Melbourne and Sydney as their next market, and one company has recently commenced operations in inner Melbourne. Other companies are expected to follow before the end of the year.

Experience overseas has shown that dockless bikes can become an unacceptable imposition on the community through poor management, storage, and maintenance, and through abuse and abandonment of the bicycles. A MOU with the operator companies may assist in improved management of the bicycles.



Branch *Sustainability and Strategic Transport*

Quarterly Milestones

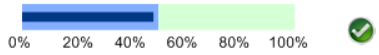
September	Q1. Commence discussions for a MOU to manage dockless bike share rollout with City of Melbourne, City of Port Philip and scheme operators.
December	Q2. Finalise preferred MOU to manage dockless bike share rollout with City of Melbourne, City of Port Philip and scheme operators
Quarterly Progress Comments	The Memorandum of Undestanding with O Bikes was endorsed by City of Yarra, City of Melbourne and City of Port Philip.

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

6.12 Develop advocacy strategy for Walmer Street Bridge upgrade

The Walmer Street bridge connecting Kew to East Richmond was built over 100 years ago and is used daily by many hundreds of cyclists and pedestrians as part of their daily commute as well as for recreational purposes. It is narrow, and creates a potential conflict point for users.

The City of Boroondara has plans to replace the approach infrastructure on its side of the bridge, and as part of a development application the approach infrastructure on the Yarra side will be upgraded in the next 18 months. This leaves the bridge itself to be upgraded which would require a multi -million dollar commitment to fund the replacement. An advocacy strategy to attract funding for the project will be developed and implemented.



Branch *Sustainability and Strategic Transport*

Quarterly Milestones

September	Q1. Commence preparation of an advocacy strategy for replacement of Walmer Street bridge (with City of Boroondara)
June	Q4. Inform Council of progress of advocacy strategy for replacement of Walmer Street bridge (with City of Boroondara)
Quarterly Progress Comments	Council officers have now held 5 meetings with stakeholders. An advocacy campaign has been drafted to seek commitment from state politicians to replace the Walmer St bridge.

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

Highlights and Achievements

Branch ***Traffic Services and Special Projects***

Unit ***Admin and Management - Traffic Services and Special Projects***

Applications for external funding also submitted for additional Blackspot and TAC road safety projects . Outcome of grant applications expected later in 2017.

Trial of road paintings on approach to school crossings with reported safety issues delivered .

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

7. A leading Yarra

a place where…Transparency, performance and community participation drive the way we operate

Council is committed to change through an energised, cohesive team of professionals, recognised for our leadership, innovation and service.

Strategies

Council's work to achieve this Strategic Objective includes the following strategies:

7.1 Ensure Council's assets and financial resources are managed responsibly to deliver financial sustainability
7.2 Continue to develop a culture of continuous improvement and innovation
7.3 Maintain a culture of transparency, governance, ethical practice and management of risks that instils a high level of community respect and confidence in Council decision-making
7.4 Ensure Council services are efficient, well-planned, accessible and meet community needs
7.5 Provide the community with meaningful and genuine opportunities to contribute to and participate in Council planning and decision making processes with a focus on young people, hard to reach and traditionally underrepresented communities
7.6 Enable greater transparency and access to the conduct of Council Meetings
7.7 Continue a 'customer centric' approach to all service planning and delivery
7.8 Advocate for the best interests of our community

The following actions are being undertaken in 2017/18 to work toward achieving Council's strategic objective of A leading Yarra.

Action Progress Summary

- At least 90% of action target achieved
- Between 75 and 90% of action target achieved
- Less than 75% of action target achieved
- Not Started
- Completed

Target
% Complete

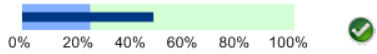
Action	Start Date / End Date	
7.01 Implement Community Engagement Policy	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
7.02 Engage young people	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
7.03 Review Strategic Advocacy Framework	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
7.04 Implement Continuous Quality Improvement	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
7.05 Implement Service Review Program	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
7.06 Develop Services Policy	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
7.07 Develop Information Services Strategy	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
7.08 Focus on customer responsiveness	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%
7.09 Transmission of Council meetings	01/07/17 30/06/18	0% 20% 40% 60% 80% 100%

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

7.01 Implement Community Engagement Policy

Council Plan initiative:
Design and deliver planned engagement processes to encourage community involvement in Council decision making in line with the Community Engagement Policy.

Implement the Community Engagement Policy with a focus on access and inclusion and capacity building initiatives. Actions will build the capacity of the organisation to design and deliver communications and engagement plans that consider the diverse needs of the Yarra community.



Branch *CEO Office*

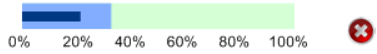
Quarterly Milestones

September	Q1. Continue to foster an Internal Community Engagement practitioner network that offers capacity building opportunities for staff
December	Q2. Continue to develop and promote communications and engagement resources to staff, including templates for stakeholder analysis and 'how to' engage with under-represented communities
March	Q3. Develop a communications and engagement strategy that incorporates the Community Engagement Policy, Social Media Policy and Internal Communications policy
June	Q4. Develop an action plan for the Communications and Engagement Strategy that will meet organisation and community needs
Quarterly Progress Comments	Key actions have been completed, including a staff toolkit of engagement resources, capacity building training and internal practitioner network established and meeting quarterly, sharing learnings and problem solving.

7.02 Engage young people

Council Plan initiative:
Promote programs to educate and encourage young people in decision making and participation in their local community.

Council will work to encourage young people to become engaged in Council's decision making. This year Council will engage children and young people on relevant initiatives and in the development of key strategies and plans, through both formal and informal engagement.



Branch *Family, Youth and Children's Services*

Quarterly Milestones

September	Q1. Commence consultation with children and young people for the development of the Yarra 0-25 Plan Q1. Encourage young people to participate in consultations regarding the re-development of Council's Municipal Strategic Statement, with an emphasis on Housing and the Planning Scheme amendment Q1. Ensure that young people are represented on relevant project reference groups such as Fitzroy gasworks and Victoria Street redevelopment
December	Q2. Provide opportunities for young people to be consulted on key social initiatives such as advocacy for a proposed safe injecting facility Q2. Complete consultation with children and young people for the development of the Yarra 0-25 Plan
March	Q3. Ensure engagement of young people in key consultations around open space Q3. Work with the Yarra Youth Advisory Committee to further identify areas of interest for young people and provide opportunities for engagement
June	Q4. Work with the Yarra Youth Advisory Committee to further identify areas of interest for young people and provide opportunities for engagement Q4. Advise Council of other further consultations the young people have engaged with
Quarterly Progress Comments	The Yarra Youth Advisory Committee has a representative on the Fitzroy Gasworks PRG, however, there were no meetings during the quarter. Young people have not been involved to date in a formal project reference group for the Victoria Street

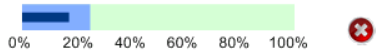
re-development. However, consultations were held with young people (via the Richmond Soccer Program), giving them the opportunity to provide input. Young people from the Yarra Youth Advisory Committee also meet quarterly with two Councillors, giving them an avenue to provide input on Council-wide strategies and initiatives.

The Housing and the Planning Scheme was discussed at Yarra Youth Advisory Committee meeting in July, though there has not been any direct input from members on the Housing and the Planning Scheme amendment. This will be explored further in early 2018.

7.03 Review Strategic Advocacy Framework

Council Plan initiative:
Continue Council's strategic advocacy program, advocating to other levels of government and stakeholders in the best interests of the Yarra community.

Council's first Strategic Advocacy Framework was adopted in 2013/14. This year Council will review and adopt a revised set of Strategic Advocacy Framework actions in line with the new Council Plan priorities.



Branch *CEO Office*

Quarterly Milestones

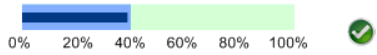
September	Q1. Review the Strategic Advocacy Framework actions
December	Q2. Adopt new Strategic Advocacy Framework actions for Council Term
March	Q3. Commence implementation of actions
June	Q4. Continue implementation of actions

Quarterly Progress Comments Review of the Strategic Advocacy Framework has commenced and a report will be taken to a Council meeting in December.

7.04 Implement Continuous Quality Improvement

Council Plan initiative:
Continue to train staff in the application of appropriate continuous improvement methodologies.

Executive has endorsed a Continuous Quality Improvement framework. This is being progressively rolled out, initially through implementation of improvement plans arising from Service Reviews. As part of this framework, Council has been working on building staff capability through training and experiential learning.
This year Council will continue to implement this framework with a focus on business process improvement, skills development and establishing a 'community of practice'.
Council's CQI program will sit within its broader Business Improvement Framework, currently being developed.



Branch *Corporate Planning and Performance*

Quarterly Milestones

September	Q1. Conduct business improvement workshop with staff
	Q1. Commence development of a Business Improvement Framework
December	Q2. Present draft Business Improvement Framework to Executive for endorsement
March	Q3. Identify training needs
June	Q4. Facilitate staff training in business improvement

Quarterly Progress Comments A draft Business Improvement framework has been developed and presented to senior officers in Corporate, Business and Finance Division. The framework has been developed with significant input and contributions from staff at the Branch and cross-organisational levels.

The draft framework identifies the operating context, goals, key activities, outputs and outcomes to be delivered. It builds on the work in recent years for Branch Service Plans and Service Reviews and incorporates a stronger and more consistent approach to continuous quality improvement through the adoption of new methods and revised project management. It is intended that the framework will ensure that

Yarra's business improvement priorities are driven by a stronger customer-focussed approach in a financially sustainable way.

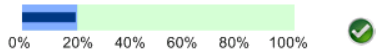
The draft framework is scheduled to be finalised in the second quarter with implementation to commence by the third quarter.

A Business Improvement workshop was held on 22 September with 21 staff attending. The workshop comprised a presentation from a leading Local Government practitioner on continuous improvement. The presentation provided an overview of the continuous improvement program at the City of Melbourne, with insights on key learnings, opportunities and challenges along the way.

7.05 Implement Service Review Program

Council Plan initiative:
Continue to implement the Service Review program.

An evaluation of Council's Service Review program was undertaken in 2016/17. Service Reviews are one aspect of a broader Business Improvement Framework which is currently being developed and will be informed by the evaluation. Executive will determine the priorities for Service Reviews in accordance with the Business Improvement Framework.



Branch *Corporate Planning and Performance*

Quarterly Milestones

September	Q1. Commence development of a Business Improvement Framework
December	Q2. Establish Service Review priorities
	Q2. Present Service Review Framework to Executive for endorsement
March	Q3. Implement Service Reviews in accordance with agreed priorities
June	Q4. Implement Service Reviews in accordance with agreed priorities

Quarterly Progress Comments

Family Youth and Children's Services
The Family, Youth and Children's Services Review is nearing completion and is scheduled to be presented to Council Briefing in mid October. Key outcomes from the review, particularly in Children's Services, are an improved customer focus, revised policies, procedures and reviewed processes and a significant improvement in financial sustainability.

Recreation and Open Space
The scoping phase for the Recreation and Open Space Service Review commenced in the last quarter of 2016/17. A draft Project Plan is currently being developed and will be scheduled to be presented to Executive, pending the status of other current priorities for this Branch.

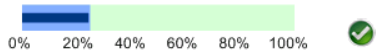
The next phase of Service Reviews will be informed by the work on the Business Improvement Framework. It is intended that the framework will ensure that Yarra's business improvement priorities are driven by a stronger customer-focussed approach in a financially sustainable way. The draft framework is scheduled to be finalised in the second quarter with implementation to commence by the third quarter.

Executive will determine the priorities for Service Reviews in accordance with the Business Improvement Framework. This will require a realignment of milestones under this Action.

7.06 Develop Services Policy

Council Plan initiative:
Develop a Services Policy and establish performance standards and service levels.

Council will develop a Services Policy to guide the types of services and service levels it provides. In response to Council's resolution when adopting the 2017/18 Budget, a report will be presented outlining a proposal to undertake a participatory/deliberative engagement process to inform development of a Services Policy.



Branch *Corporate Planning and Performance*

Quarterly Milestones

September	Q1. Commence development of report to Council on a participatory/deliberative engagement process to develop a Services Policy
December	Q2. Report to Council on a participatory/deliberative engagement process to develop a Services Policy
March	Q3. Commence implementation of endorsed proposal
June	Q4. Complete implementation of endorsed proposal

Quarterly Progress Comments

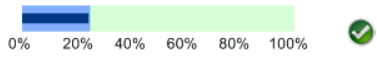
Following the Council resolution of August 1 2017 which requested a report to Council on a participatory/deliberative engagement process to develop a Services Policy, a Project Control Group (PCG) was established comprising officers from Corporate Planning and Performance, Social Policy and Research, and Advocacy and Engagement under the Sponsorship of the Acting Director City Works and Assets.

Initial focus for the PCG was on the project scope, determining roles and responsibilities and examining appropriate methods for deliberative engagement. These have been determined and the focus of the PCG in the second quarter is developing a robust and cost effective proposal to present to Executive and Council on a deliberative engagement process to develop a Services Policy.

7.07 Develop Information Services Strategy

Council Plan initiative:
Continue to implement strategies that enhance customer and community experiences with Council across all services.

Council is developing an Information Services Strategy to identify the strengths and weaknesses of current practices and the future ICT needs required to deliver effective and efficient services to the community and internal stakeholders.



Branch *Information Services*

Quarterly Milestones

September	Q1. Complete development of the new three year Information Services Strategy
December	Q2. Commence implementation and reporting on first year actions within the Information Services Strategy
March	Q3. Continue implementation and reporting on first year actions within the Information Services Strategy
June	Q4. Continue implementation and reporting on first year actions within the Information Services Strategy

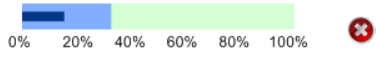
Quarterly Progress Comments

Draft Information Services policy 2018-2021 is complete and a report will go to Executive in December 2017.

7.08 Focus on customer responsiveness

Council Plan Initiative:
Continue to implement strategies that enhance customer and community experience with Council across services.

Council's customer responsiveness is focussed on providing exceptional customer service, delivering seamless experiences and resolving enquiries at the first point of contact.



Branch *Corporate Planning and Performance*

Quarterly Milestones

September	Q1. Establish the Customer Experience core group to drive business and process improvements
March	Q3. Develop a reporting framework for customer feedback and complaints
June	Q4. Streamline complaint resolution process

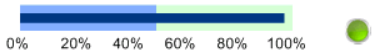
Attachment 1 - Annual Plan Quarterly Progress Report September 2017

Quarterly Progress Comments The Customer Experience core group terms of reference have been drafted and are currently under review.

7.09 Transmission of Council meetings

Council Plan initiative:
Implement a cost effective system to broadcast Council meetings

Council will implement a twelve month trial of recording and publication of Council meetings on Council's website .



Branch *Governance and Support*

Quarterly Milestones

September Q1. Develop a proposal for digital transmission of meeting proceedings and submit to Council for endorsement

December Q2. Commence recording and transmission of Council meetings

Quarterly Progress Comments A proposal for digital transmission of meeting proceedings has been submitted and was endorsed by Council in August 2017. Since September 2017, Council meetings have been recorded and published as an audio stream from Council's website.

Attachment 1 - Annual Plan Quarterly Progress Report September 2017

Highlights and Achievements

Branch ***CEO Office***

Unit ***Admin and Management - Advocacy and Engagement***

Support to over 85 communication and engagement projects across the organisation, including open space, stolen generations marker, 30 km/h speed trial, LAPMS (3), Affordable Housing, Bridge Rd masterplan, Victoria Street consultation (just to name a few).

The launch of Yarra's first corporate EDM- Yarra Life (working for you campaign), with over 7,000 subscribers receiving a fortnightly newsletter from Council.

A number of advocacy campaigns were rolled out including MSIF, marriage equality, Stand With Us, Australia Day

The Richmond Football Club-AFL finals were also supported by Council through communications and marketing.

Branch ***Corporate Planning and Performance***

Unit ***Corporate Planning***

Successful implementation of a new customer request system with ongoing digital improvements in customer personalisation.

In this period, Council's Customer Service team also received 43,939 customer calls achieving a first point of contact resolution of 83%.

11.13 Hydrotherapy Pool report

Executive Summary

Purpose

To report on the evaluation of the 10 week hydrotherapy pool trial conducted at the Epworth Hospital.

Key Issues

In July 2017, Council resolved to conduct a 10 week hydrotherapy pool trial at the Epworth Hospital. Council further resolved that the trial needed to reach a number of benchmarks. The program initially achieved a high degree of participation, however, by week ten this rate had reduced to 50%, and officers did not extend the trial, in line with the previous Council resolution.

As part of the evaluation, participants were surveyed in the last two weeks of the trial to better understand their experience and to what extent the program met their needs. The vast majority (91%) of respondents indicated that overall they were satisfied with their experience of the program, with a similar amount (88.5%) of respondents indicating that the program met their needs. The majority of participants also indicated high levels of satisfaction with the facility, cleanliness of water and supervision provided by staff.

The majority of participants were satisfied with the unstructured nature of the session activities, but suggested that additional structured sessions such as Aqua Exercise and or Physio led hydrotherapy sessions would have also been desirable. Participants in the trial were overwhelmingly Yarra residents and almost half were not currently frequent users of the leisure centres. Anecdotally, the group activity also fostered a sense of community and social connectedness.

The average attendance at the program over the ten weeks was 8 people per session (54%). Individuals each visited the program on average 0.55 visits per week or once per fortnight. Despite the high levels of overall satisfaction, it is unclear whether the low level of attendance meant that the program achieved objective health benefits for participants. The program ran at a loss of $5,044 for the ten weeks. This loss did not include marketing costs and administrative labour associated with the set up and ongoing administration of the program.

The trial has provided a locally based hydrotherapy experience for some Yarra residents. Whilst participants overall indicated a high degree of satisfaction with the program, it did not meet an expressed preference for more pool sessions and hours or the desire for group based exercise or physiotherapy. The cost of the program was relatively significant for the low number of participants and the identified risks.

PROPOSAL

1. That Council:

   (a) notes the results of the hydrotherapy pool trial evaluation.

11.13 Hydrotherapy Pool report

Trim Record Number: D17/198813
Responsible Officer: Acting Director Community Wellbeing

Purpose

1. To report on the evaluation of the 10 week hydrotherapy pool trial conducted at the Epworth Hospital.

Background

2. In 2016, Council was approached by a member of the community, proposing that Council consider building a Hydrotherapy Pool at the Richmond Recreation Centre. Council also received several emails in support of the proposal and a petition was tabled at Council meeting on 17 May 2016.
3. In response, council officers undertook a feasibility study looking at the potential to build a hydrotherapy pool at each of Councils leisure facilities, namely Richmond Recreation Centre, Collingwood Leisure Centre and Fitzroy Pool.
4. The feasibility study determined that the most suitable site would be the Collingwood Leisure Centre and would likely cost approximately $8.1 million.
5. Council subsequently determined that it was unable to commit capital funds to the project and asked officers to investigate the potential for a shared use/hire arrangement with other providers of Hydrotherapy pools in and adjacent to Yarra.
6. Officers considered a number of options. Ultimately four local hospitals and an Aged Care facility were approached; Austin Hospital, Mercy Hospital, Epworth Hospital, Alfred Hospital and the Rathdowne Place Aged Care facility.
7. In July 2017, Council received a further report recommending the use of the Epworth hospital hydrotherapy pool on a trial basis. Subsequently, Council resolved to;
   (a) endorse a 10 week trial at the Epworth Hospital on the following basis:
      (i) sessions Tuesday at 4.30pm to 6.30pm and Saturdays 2.30pm to 4.30pm;
      (ii) entry fees to be equivalent to the cost of a swim at the leisure centre;
      (iii) entry fees purchased as a 5 or 10 session pass;
      (iv) customers must book for the session;
      (v) customers to be charged for the session if they book and don't attend;
      (vi) that during the trial period officers explore the opportunity for a group session;
   (b) authorise officers to:
      (i) cancel the 10 week trial if attendance does not reach 50% by week 3 of the trial; and
      (ii) not extend the trial if participation does not reach 80% by week 10 of the trial; and
   (c) request Officers to evaluate the trial and report back to Council with the outcomes once the trial has concluded; and
   (d) widely promote the trial through Council's website, social media, notices/advice at Council Pools, Library Branches, Aged Care Centres and facilities, Community Centres, Neighbourhood Houses, local housing estates, through Council contact lists, mailing lists including Advisory Committee members and directly to members of the community who have sought access to hydrotherapy facilities.

Marketing

8. The promotion of the program was extensive, and intended to ensure that as many members of the Yarra community were aware of the trial and to maximise active participation. The 10 week trial was initially programmed to commence on the 1st September, but the decision was made to defer the start until the 19th September so that a feature article could be included in the Yarra News that was being published in the first week of September.

9. A range of marketing and promotional activities took place leading up the start of the program including;

   (a) Bulk email to 6,639 Leisure members with a 46.9% open rate;

   (b) Bulk email to 7,860 pass holders, swim participants with article in the Yarra Leisure News.

   (c) Feature article in the Yarra News to 45,000 households;

   (d) 100 posters and 2,000 flyers distributed to 20 organisations including;

      (i) five libraries;

      (ii) nine neighbourhood houses;

      (iii) two town halls;

      (iv) local community health centres;

   (e) Advertisements were posted on both the Council and leisure websites;

   (f) Digital slides on leisure, town hall and libraries televisions;

   (g) Direct email promotion sent to council officers in Aged Services to they could distribute to their client lists;

   (h) Three separate posts on Facebook reaching 4,482 people; and

   (i) Twitter posts with 1.23 impressions;

Booking

10. Due to the Epworth's condition of entry, including 'no casual walk-ins' along with a record of who was attending, participants were initially required to enrol in the program at any one of Yarra's leisure centres by purchasing a 5 session pass at $33.50 or a 10 session pass at $67.

11. Thereafter, participants could book into any session, either in person at any of the Leisure centres or by phone, up to one week in advance.

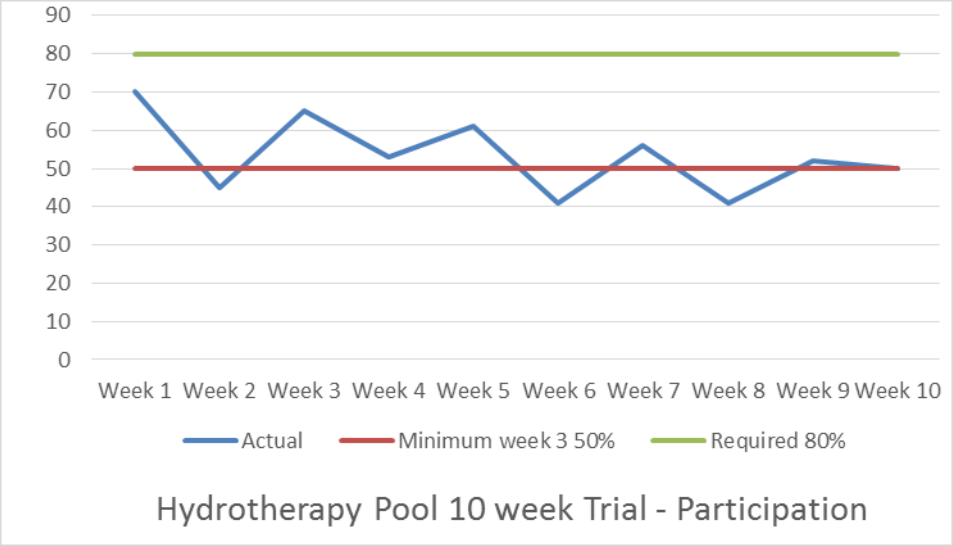
Participation

12. Advocacy from members of the community indicated that there was a level of demand for such a program. 15 places per session were made available, with four one hour sessions offered per week.

13. Council had determined that the trial needed to reach 50% participation of the available places by week three, and 80% by week ten. Initial uptake of the program was strong with 42 visits (70% of available places) utilised.

14. At week three, the program had achieved the first agreed milestone. However, demand for the program did not continue to remain at this level and declined over subsequent weeks, with just 30 visits (50%) in the final week, averaging just 54% for the ten weeks. This equates to approximately 8 individuals attending per session (4 sessions per week). Figure 1 below, illustrates actual participation against the agreed targets.

Figure 1: Participation rates compared to target.



15. Officers tracked participation rates of individuals that attended. There was a total of 71 passes purchased by 58 individuals who attended 322 times at an average of 32 visits. On average 8 people attended per session. The average attendance per person was 0.55 visits per week or once per fortnight.

External Consultation

16. As part of the overall evaluation, participants were surveyed in the final two weeks of the program to better understand their needs, preferences and experience. 29 responses (50% of total participants) were received with results summarised below:

    (a) 65.2% of survey respondents agreed that they were satisfied with the session times made available, with a further 26.9% indicating a neutral response. Survey respondents indicated that more sessions would have been desirable, but no clear preference on times was apparent;

    (b) 88.5% of survey respondents indicated that the program met their needs. The majority of participants also indicated high levels of satisfaction with the facility, cleanliness of water and supervision provided by staff;

    (c) The majority of respondents were satisfied with the unstructured nature of the session activities, but suggested that structured sessions such as Aqua Exercise and or Physio led hydrotherapy sessions would have also been desirable;

    (d) The majority of participants indicated that overall they were satisfied with the marketing and promotion of the program. However 23% of respondents (6 people) were not satisfied with the materials. Whilst specific information on what could have been improved was not recorded, the program was the subject of comprehensive promotion across a range of media;

    (e) The greater majority of respondents indicated that their preference would have been for added structured sessions to be provided in addition to the offered session times;

    (e) The demographic profile of survey respondents indicated that 85% were female, with 69% being over the age of 55 years. More than 65% of respondents held a concession card or senior's card and 39% considered themselves to be living with a permanent disability;

    (f) Almost all respondents (92%) identified as residents of Yarra and 54% of respondents were current users of the leisure centres, suggesting that almost half of the respondents were not engaged in the leisure centres before the trial; and

    (e) 91% of respondents indicated that overall, they were satisfied with their experience of the program.

Internal Consultation (One Yarra)

17. Advice was sought from the Manager of Risk; Audit & Procurement on insurance requirements for running a program at non-council owned and operated facilities.

18. Leisure Services staff liaised with the Aged and Disability Branch as a potential stakeholder to understand the likely interest and demand for the proposed trial program.

19. Leisure Services team was consulted prior to, and throughout the process on the pilot programming and ways to ensure the best customer experience. This pilot required the team to draw on the skills and experience across many portfolios.

Financial Implications

20. The program ran at a loss of $5,044 for the ten weeks. This loss only included direct labour and hire costs. This loss did not include marketing costs and administrative labour associated with the set up and ongoing administration of the program.

21. The level of subsidy ranged from $10.44 per visit in week 1, to $22.12 per visit in week 8. With an average subsidy of $15.56 per visit for the ten weeks.

Table 2.

Week	Visits	% Occupancy	Income	Expense	Net Cost	Subsidy Per Visit
1	42	70%	$280	$720	-$438	-$10.44
2	27	45%	$181	$720	-$539	-$19.96
3	39	65%	$255	$720	-$464	-$11.19
4	32	53%	$214	$720	-$506	-$15.81
5	37	61%	$247	$720	-$473	-$12.78
6	25	41%	$167	$720	-$552	-$22.10
7	34	56%	$228	$720	-$492	-$14.47
8	25	41%	$167	$720	-$553	-$22.12
9	31	52%	$208	$720	-$512	-$16.51
10	30	50%	$210	$720	-$610	-$17.00
Total	**322**	**54%**	**$2,156**	**$7,200**	**-$5,044**	**-$15.56**

22. The annualised cost of the program its current format (assuming the same level of attendance), would be $26,288.

Economic Implications

23. The program evaluation has no economic implications.

Sustainability Implications

24. There are no sustainability issues associated with this report

Social Implications

25. Feedback from the users indicates that providing access to a Hydrotherapy Pool has been beneficial for those involved, especially for the aged, those with disabilities and those recovering from injury or surgery.

26. However, with participation from individuals averaging once per fortnight, it is unclear whether the program was sufficient to deliver significant meaningful physical health benefits to all participants.

27. Participants in the trial were overwhelmingly Yarra residents and almost half were not currently frequent users of the leisure centres.

28. Anecdotally, the group activity also fostered a sense of community and social connectedness.

Human Rights Implications

29. There are no Human Rights issues associated with this report.

Communications with CALD Communities Implications

30. The program was available to those from a CALD background which is strongly represented by those that already participate in the broad range of programs offered at the Leisure Centres

31. Language advisory text was used on the posters, flyers and advertorial in the Yarra News.

Council Plan, Strategy and Policy Implications

32. Facilitating access to a Hydrotherapy trial for our community supports Strategic Objective 1 of the Council Plan 2017-2021; *City of Yarra, a place where… Community health, safety and wellbeing are a focus in everything we do.*

Legal Implications

33. Whilst not required for the trial, the Epworth has indicated a preference that any use of the facility would normally require a twelve month lease agreement.

Other Issues

34. Delivery of a small program from a satellite facility posed several challenges:

    (a) oversight of quality and safety standards was difficult as senior officers were not located on site and could only reasonably dedicate time for occasional contact visits at the site;

    (b) there may have been an increase Councils exposure to risk, considering the target group includes the elderly and injured and the lower levels of supervising staff on site. Whilst staff demonstrate high levels of supervision, the limited number of staff on site requires a higher degree of vigilance; and

    (c) attracting appropriately qualified staff to the program was difficult as the shift times are shorter than shifts offered in the leisure centres. Short shifts are not financially rewarding for staff therefore are often difficult to fill reliably.

35. The Victorian Auditor General's Office (VAGO) requires regular reporting from Council in regard to the efficiency of its leisure centres. Results are compared across industry. Whilst only a 10 week trial, the addition of a highly subsidised program will dilute the efficiency rating of Yarra's leisure centres compared with industry results.

36. Competitive Neutrality Policy may arguably be applied to this program if hydrotherapy sessions were deemed to be a significant business activity.

37. Within the context of the current resource constrained environment, the Service Delivery Plan of the leisure centres calls for a focus on core service and prioritising efforts towards programs that reach many participants as opposed to specialist programs that are sought by and only benefit a few within the community.

38. The participation survey, when combined with the extensive marketing already undertaken, does not seem to indicate that there is a significant level of unmet demand for the program, beyond those individuals who participated in the trial.

Conclusion

39. The Trial program appears to have successfully attracted the intended target demographic and has also successfully drawn a participant base from with Yarra's municipal area. Almost half of the participants are likely to be persons who were not previously engaged within the leisure centres.

40. Participants have indicated that they have benefitted from their participation in the trial program and have expressed a high degree of overall satisfaction with their experience in the program.

41. However, the low level of participants (only 58 individuals attended the program in 10 weeks) and the declining frequency of attendance, (from 70% occupancy at commencement of the trail to 50% in its concluding week) suggests that the potential participant base may not be large. Alternatively it may indicate that conditions of the trial program were too constrictive to meet the needs of many within the target demographic.

42. The barriers to entry for the program include that Hydrotherapy sessions can only be offered across four hours per week and that the Epworth requires that there be no “walk up” casual visits. Alternative options may be more attractive for some customers, such as Hawthorn recreation centre. Whilst not in Yarra, it is located 3.4km from Richmond recreation centre, and offers a dedicated warm water pool for customers for 102.5 hours per week.

43. The Epworth is an off-site location that poses logistical challenges and further restrictions imposed by the Hospital Management. The booking process is restrictive; the low number of hours on offer are not the most desirable for participants. The Leisure Management presence can only be minimal and reliable staffing is uncertain. Ultimately the risk profile of the off-site model is greater than it would be if the program were provided from within one of Council’s leisure centres.

44. The trial has provided a locally based hydrotherapy experience for some Yarra residents. Whilst participants overall indicated a high degree of satisfaction with the program, it did not meet an expressed preference for more pool sessions and hours or the desire for group based exercise or physiotherapy. The cost of the program was significant for the low number of participants and the identified risks.

45. Based on the Council resolution of July 2017, officers have not extended the trial beyond the initial 10 week period.

RECOMMENDATION

1. That Council notes the results of the hydrotherapy pool trial evaluation.

CONTACT OFFICER: **Peter Watson**
TITLE: **Manager Leisure Services**
TEL: **9205 5393**

Attachments
There are no attachments for this report.

11.14 Report on Assemblies of Councillors Held - August to December 2017

Trim Record Number: D17/197559
Responsible Officer: Group Manager Chief Executive's Office

Purpose

1. To report on the number of Assemblies of Councillors held since the previous Report.

Background

2. The *Local Government Act* 1989 (The Act) now requires that …"The Chief Executive Officer must ensure that the written record of an assembly of Councillors is, as soon as practicable:
   (a) reported at an ordinary meeting of the Council; and
   (b) incorporated in the minutes of that Council meeting…..".

Consultation

3. Not applicable.

Financial Implications

4. Not applicable.

Economic Implications

5. Not applicable.

Sustainability Implications

6. Not applicable.

Social Implications

7. Not applicable.

Human Rights Implications

8. Not applicable.

Communications with CALD Communities Implications

9. Not applicable.

Council Plan, Strategy and Policy Implications

10. Not applicable.

Legal Implications

11. Section 80A (2) of The Act, was amended and becoming operative from 24 September 2010 to now require the above information be reported to the next available Council Meeting and also be recorded into the Minutes of the Council.

Other Issues

12. Not applicable.

Options

13. Nil.

Conclusion

14. That Council formally note and record the Assemblies of Councillors report as detailed in Attachment 1 hereto.

RECOMMENDATION

1. That Council formally note and record the Assemblies of Councillors report as detailed in Attachment 1 hereto.

CONTACT OFFICER: **Mel Nikou**
TITLE: **Administration Officer - Governance Support**
TEL: **9205 5158**

Attachments
1 Assemblies of Councillors Held Report - September to December 2017

Attachment 1 - Assemblies of Councillors Held Report - September to December 2017

Assemblies of Councillors Held - Period: 4 September to 4 December 2017
Reported to Council: Meeting: 19 December 2017

Assembly No. for Period	Date: Time: Venue:	Attendance: - Councillors - Officers	Conflict of Interest Disclosures	Briefing Items
1	04.09.2017 6.30pm Richmond Town Hall	**Councillors:** - Stone - Coleman - Fristacky - McEvoy - Searle **Officers:** - Chief Executive Officer - Group Manger Chief Executive's Office - Director Planning & Place Making - Director Corporate, Business & Finance - Director City Works and Assets - Group Manager People, Culture and Community - Manager Corporate Planning and Performance - Sustainability Unit Manager - Community Partnerships Unit Manager - Manager City Strategy - Manager Traffic and Special Projects - Manager Open Space and Recreation **External Guests:** Audit Committee Members	NO	**Councillors Briefing** **No. 1** Audit Committee Update **No. 2** Draft 2017/18 Annual Plan **No. 3** Activities to promote identity of the Aboriginal Community **No. 4** Urban Forest Strategy **No.5** Melbourne Renewable Energy **No.6** Heritage Advisory Committee **No.7** Lennox and Nicholson Street Intersection **No.8** Events in Public Places Permit – Reclink Community Cup and Peel Street Festival **No.9** Rose Street Feet First Pedestrian Improvement Project **No.10** Collingwood College Basketball Proposal **No.11** Update on proposed Rushall Reserve **No. 12** Confidential Matter **No. 13** Confidential Matter **No. 14** Review of Forward Report Schedule **No. 15** Agenda Review

Attachment 1 - Assemblies of Councillors Held Report - September to December 2017

Assembly No. for Period	Date: Time: Venue:	Attendance: - Councillors - Officers	Conflict of Interest Disclosures	Briefing Items
				No. 16 Royal Exhibition Buildings – Protection and Promotion **No. 17** Results of Retail Audit **No. 18** VCAT Activity Report **No. 19** Hoddle Street Clearway
2	12.09.2017 4.30pm – 6.30pm Willow Centre, Abbotsford	**Councillors:** - McEvoy **Officers:** - Senior Coordinator Strategic Planning - Disability Planning Officer - Coordinator Home Care - Coordinator Community Planning - Senior Advisor City Heritage - Senior Coordinator Design and Place Making - Urban Designer - Aged Care Project Officer	NO	**Disability Advisory Committee** **Matters Discussed:** Draft Public Toilet Strategy, new Disability Action Plan and feedback on Draft Bridge Road Streetscape Masterplan
3	18.09.2017 6.30pm Richmond Town Hall	**Councillors:** - Stone - Chen Yi Mei - Coleman - Bosler - Fristacky - McEvoy - Searle **Officers:** - Chief Executive Officer - Group Manger Chief Executive's Office - Director Planning & Place Making - Director Corporate, Business & Finance - Director City Works and Assets - Manager Traffic and Special Projects - Executive Planner - Community Partnerships Unit Manager - Manager City Strategy - Manager Open Space and Recreation	NO	**Councillors Briefing** **No. 1** Confidential Matter **No. 2** Update on the Redevelopment of 433 Smith Street **No. 3** Draft January 26 Project Implementation Plan **No. 4** Draft Housing Strategy **No.5** Strategic Planning for Activity Centres **No.6** Proposal to consult on an expanded Wangaratta Street Reserve **No. 7**

Attachment 1 - Assemblies of Councillors Held Report - September to December 2017

Assembly No. for Period	Date: Time: Venue:	Attendance: - Councillors - Officers	Conflict of Interest Disclosures	Briefing Items
		- Chief Financial Officer - Coordinator Valuations **External Guests:** Landers and Rogers Development Victoria		State Government Property Valuation Reforms **No. 8** Review of Forward Report Schedule **No. 9** Agenda Review **No. 10** Rushall Reserve
4	25.09.2017 6.00pm Collingwood Town Hall Labour Room	**Councillors:** - Jolly **Officers:** - Group Manger Chief Executive's Office - Senior Coordinator Strategic Planning - Coordinator Strategic Planning	YES	**Heritage Advisory Committee** **Matters Discussed:** - HAC review - Heritage Policy Rewrite - Heritage Training for staff - Feethought Hall at St Vincents
5	28.09.2017 8.00am Exchange Workspaces on 285 Lennox Street Richmond	**Councillors:** - Searle **Officers:** - Coordinator Economic Development - Business Unit Manager Arts, Culture and Venues - Project Officer Retail - Economic Development Officer – Communications and Marketing - Business Development Officer	NO	**Business Advisory Group** **Matters Discussed:** - Leaps and Bounds 2017 - Business Events Program - Promoting a shop Local message in Yarra's Village precincts - Feedback on BAG meetings.
6	02.10.2017 6.00pm Richmond Town Hall	**Councillors:** - Stone - Bosler - Fristacky - Searle **Officers:** - Chief Executive Officer - Group Manager Chief Executive's Office - Director Planning and Place Making - Assistant Director Planning and Place Making - Director Corporate Business and Finance - Acting Director Community Wellbeing - Manager City Works	NO	**Councillors Briefing** **No. 1** Passenger Fleet Update **No. 2** Property Strategy **No. 3** Standing Committee on Legal and Social Issues **No. 4** Evaluation of Special Charge Scheme for Bridge Road **No.5** Yarra Spatial Economic and Employment Strategy

Attachment 1 - Assemblies of Councillors Held Report - September to December 2017

Assembly No. for Period	Date: Time: Venue:	Attendance: - Councillors - Officers	Conflict of Interest Disclosures	Briefing Items
		- Manager Property Services - Unit Manager Social Policy and Research - Coordinator Economic Development - Manager City Strategy - Manager Compliance - Senior Policy Advisor		**No.6** Swan Street Activity Centre Built Form Framework **No.7** Northumberland **No. 8** Confidential Matter **No.9** Review Forward Report Schedule **No.10** Agenda Review
6	05.10.2017 11.00am Richmond Town Hall	**Councillors:** - Chen Yi Mei - Stone - Nguyen **Officers:** - Community Partnerships Unit Manager - Special Projects Officer - Community Planner – Aboriginal Partnerships	NO	**Aboriginal Advisory Group** **Matters Discussed:** Aboriginal Affairs, Stolen Generations, January 26th
7	10.10.2017 6.30pm Collingwood Town Hall Wurundjeri Room	**Councillors:** - McEvoy **Officers:** - Business Unit Manager Arts, Culture and Venues - Coordinator Arts and Culture - Arts Development Officer	NO	**Room to Create Panel** **Matters Discussed:** - Room to create program - Next distribution of interest from Room to Create Fund - Room to Create responsive grant program commencement and progress - Fundraising Initiatives for Room to Create Fund - The Circle program overview and progress
8	16.10.2017 6.30pm Richmond Town Hall	**Councillors:** - Stone - Fristacky - McEvoy - Nguyen - Searle **Officers:** - Chief Executive Officer - Group Manager Chief Executive's Office	NO	**Councillors Briefing** **No. 1** Family Youth and Children's Services – completion of service review **No. 2** Proposed Extension to clearway, Hoddle Street **No. 3** Draft Public Toilet Strategy

Attachment 1 - Assemblies of Councillors Held Report - September to December 2017

Assembly No. for Period	Date: Time: Venue:	Attendance: - Councillors - Officers	Conflict of Interest Disclosures	Briefing Items
		- Director Planning and Place Making - Assistant Director Planning and Place Making - Acting Director Corporate Business and Finance - Acting Director Community Wellbeing - Acting Director City Works and Assets - Manager City Strategy - Executive Planner - Manager City Works		**No. 4** Amendment C220 – Johnston Street **No.5** Confidential Matter **No.6** Update on reimagining Victoria Street **No.7** Graffiti **No. 8** Confidential Matter **No.9** Review Forward Report Schedule **No.10** Agenda Review
9	30.10.2017 6.30pm Richmond Town Hall	**Councillors:** - Stone - Bosler - Fristacky - McEvoy - Searle **Officers:** - Chief Executive Officer - Group Manager Chief Executive's Office - Assistant Director Planning and Place Making - Acting Director Corporate Business and Finance - Acting Director Community Wellbeing - Acting Director City Works and Assets - Group Manager People, Culture and Community - Manager City Strategy - Executive Planner - Coordinator Youth and Middle Years - Services and Projects – Family, Youth and Children's Services	NO	**Councillors Briefing** **No. 1** Preparation of a Council Plan for Children and Young People **No. 2** Services Review Out of School Hours Care Services **No. 3** Bridge Road Streetscape Masterplan **No. 4** Yarra Planning Scheme Amendment C188 **No.5** Amendment C173 Heritage Overlay Follow-up **No.6** Confidential Matter **No.7** 13-15 Brunswick Street Fitzroy Laneway **No.8** Confidential Matter **No.9** Review of Forward Report Schedule **No. 10**

Attachment 1 - Assemblies of Councillors Held Report - September to December 2017

Assembly No. for Period	Date: Time: Venue:	Attendance: - Councillors - Officers	Conflict of Interest Disclosures	Briefing Items
				Agenda Review
10	31.10.2017 2.00pm Richmond Town Hall	**Councillors:** - Nguyen **Officers:** - Community Partnerships Team Leader - Acting Group Manager Chief Executive's Office - Community Planner – Multicultural Affairs and Neighbourhood Houses - Services and Project Officer - Youth Development Officer – Engagement Program - Special Projects Officer - Community Planner – Aboriginal Partnerships	NO	**Yarra Multicultural Advisory Group** **Matters Discussed:** 0-25 Strategy and January 26
11	20.11.2017 6.30pm Fitzroy Town Hall	**Councillors:** - Stone - Coleman - Bosler - Fristacky - McEvoy - Nguyen - Searle **Officers:** - Chief Executive Officer - Group Manager Chief Executive's Office - Assistant Director Planning and Place Making - Director Corporate Business and Finance - Acting Director Community Wellbeing - Acting Director City Works and Assets - Manager Aged and Disability Services - Coordinator Organisational Performance - Unit Manager Social Policy and Research - Coordinator Strategic Planning - Manager City Strategy - Executive Planner - Manager Corporate Planning and Performance - Manager Advocacy and Engagement	NO	**Councillors Briefing** **No. 1** National Aged Care Reforms – Future Directions **No. 2** Outline of Deliberative Approach for Development of a Yarra Services Policy **No. 3** 2018/19 Budget **No. 4** Amendment C231 Queens Parade Local Activity Centre **No.5** Planter Box Fees **No.6** Confidential Matter **No.7** Review of Forward Report Schedule **No. 8** Agenda Review

Attachment 1 - Assemblies of Councillors Held Report - September to December 2017

Assembly No. for Period	Date: Time: Venue:	Attendance: - Councillors - Officers	Conflict of Interest Disclosures	Briefing Items
12	04.12.2017 6.30pm Fitzroy Town Hall	**Councillors:** - Stone - Fristacky - McEvoy - Nguyen - Searle **Officers:** - Chief Executive Officer - Group Manager Chief Executive's Office - Director Planning and Place Making - Assistant Director Planning and Place Making - Director Corporate Business and Finance - Acting Director Community Wellbeing - Acting Director City Works and Assets - Group Manager People, Culture and Community - Manager City Strategy - Manager Leisure Services - Business Unit Manager Arts, Culture and Venues - Executive Planner - Manager Advocacy and Engagement - Coordinator Communications and Engagement **External Guests:** Development Victoria VicRoads	NO	**Councillors Briefing** **No. 1** 433 Smith Street – Presentation **No. 2** Preston CBD Cycle Corridor **No. 3** Yarra Sculpture Gallery Tenancy **No. 4** Gender Neutral Signage for Leisure Services **No.5** Richmond Town Hall Banner System **No.6** Yarra Planning Scheme Amendment C225 **No.7** Amendment C223 Proposed Rezoning of 81-95 Burnley Street Richmond **No. 8** Confidential Matter **No. 9** Review of Forward Report Schedule **No. 10** Agenda Review

12.1 Notice of Motion No. 23 of 2017 - Planning Applications in the World Heritage Environs Area

Trim Record Number: D17/199093
Responsible Officer: Chief Executive Officer

I, Councillor Stephen Jolly, hereby give notice that it is my intention to move the following motion at the Ordinary Meeting of Council to be held on 19 December 2017:

"That planning applications for development:

(a) within the shaded area shown in clause 22.14 (Development Guidelines for Heritage Places in the World Heritage Environs Area) of the Yarra Planning Scheme (City of Yarra properties only); OR

(b) within the area designated as the "Buffer Zone" in Map 8 of the World Heritage Environs Area (City of Yarra properties only)

be determined by Council's Internal Development Approvals Committee where they are taller than the highest point of the existing building."

Background:

1. Extract Yarra Planning Scheme – being Clause 22.14

22.14 DEVELOPMENT GUIDELINES FOR HERITAGE PLACES IN THE WORLD HERITAGE ENVIRONS AREA

19/11/2009
C118

This policy applies to all land covered by the Heritage Overlay within the World Heritage Environs Area (WHEA) Area of Greater Sensitivity as shown in Figure 1.

Specifically, this policy applies to land within HO361 (World Heritage Environs Area Precinct).

The policy outlined in Clause 22.02 of the Yarra Planning Scheme also applies.

Figure 1

22.14-1 Policy Basis

19/11/2009
C118

The MSS highlights the importance of the WHEA in providing a buffer zone for the World Heritage Listed Royal Exhibition Building and Carlton Gardens. The WHEA provides a setting and context of significant historic character for the World Heritage property.

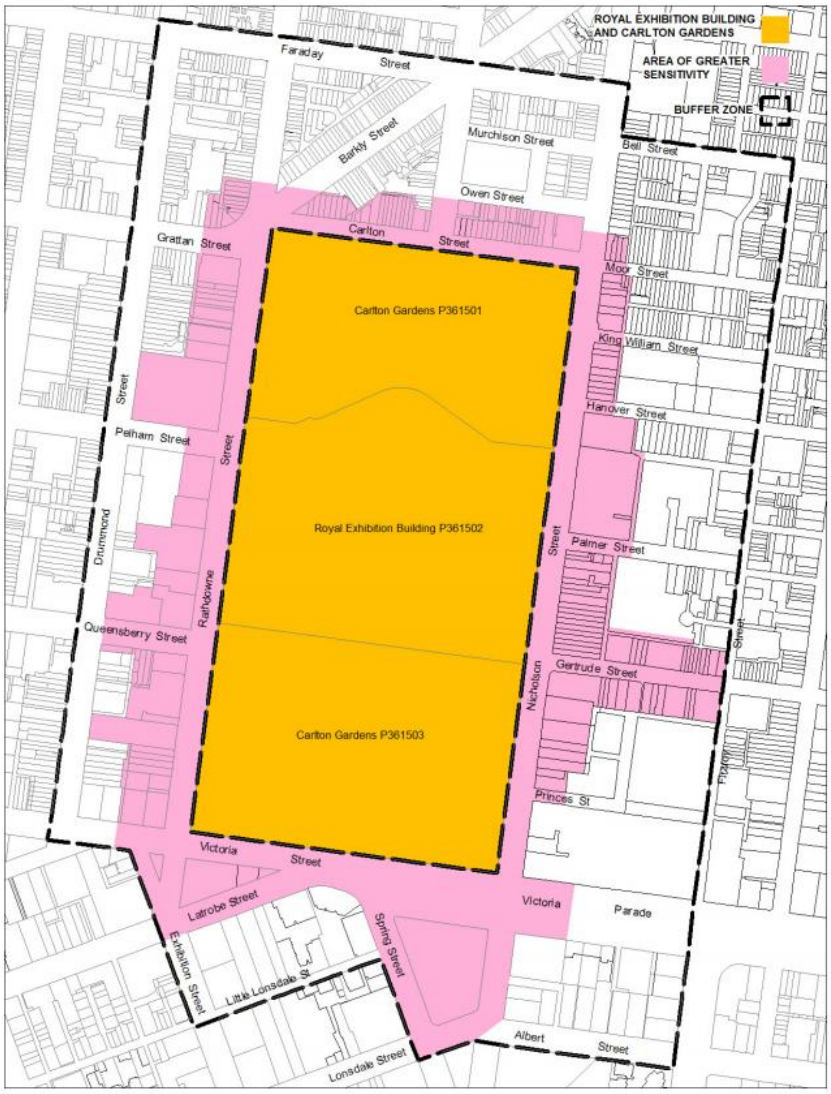
22.14-2 Objectives

19/11/2009
C118

- To protect significant views and vistas to the Royal Exhibition Building and Carlton Gardens.
- To maintain and conserve the significant historic character (built form and landscapes) of the area.
- To ensure new development in the area has regard to the prominence and visibility of the Royal Exhibition Building and Carlton Gardens.

2. Map 8 of World Heritage Environs Area

APPENDIX 8 MAP OF THE SITE SHOWING THE BUFFER ZONE



Royal Exhibition Building and Carlton Gardens and Buffer Zone, showing Area of Greater Sensitivity in pink.

Taken from Royal Exhibition Building and Carlton Gardens World Heritage Management Plan (October 2013)

RECOMMENDATION

1. That planning applications for development:
   (a) within the shaded area shown in clause 22.14 (Development Guidelines for Heritage Places in the World Heritage Environs Area) of the Yarra Planning Scheme (City of Yarra properties only); OR
   (b) within the area designated as the "Buffer Zone" in Map 8 of the World Heritage Environs Area (City of Yarra properties only);

   be determined by Council's Internal Development Approvals Committee where they are taller than the highest point of the existing building.

There are no attachments for this report.