



Ordinary Meeting of Council Agenda

**to be held on Tuesday 21 March 2017 at 7.00pm
Fitzroy Town Hall**

Disability - Access and Inclusion to Committee and Council Meetings:

Facilities/services provided at the Richmond and Fitzroy Town Halls:

- Entrance ramps and lifts (off Moor Street at Fitzroy, entry foyer at Richmond)
- Hearing loop (Richmond only), the receiver accessory may be accessed by request to either the Chairperson or the Governance Officer at the commencement of the meeting, proposed resolutions are displayed on large screen and Auslan interpreting (*by arrangement, tel. 9205 5110*)
- Electronic sound system amplifies Councillors' debate
- Interpreting assistance (*by arrangement, tel. 9205 5110*)
- Disability accessible toilet facilities

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Order of business

- 1. Statement of recognition of Wurundjeri Land**
- 2. Attendance, apologies and requests for leave of absence**
- 3. Declarations of conflict of interest (Councillors and staff)**
- 4. Confidential business reports**
- 5. Confirmation of minutes**
- 6. Petitions and joint letters**
- 7. Public question time**
- 8. General business**
- 9. Delegates' reports**
- 10. Questions without notice**
- 11. Council business reports**
- 12. Notices of motion**
- 13. Urgent business**

1. Statement of Recognition of Wurundjeri Land

"Welcome to the City of Yarra."

"Yarra City Council acknowledges the Wurundjeri as the Traditional Owners of this country, pays tribute to all Aboriginal and Torres Strait Islander people in Yarra and gives respect to the Elders past and present."

2. Attendance, apologies and requests for leave of absence

Anticipated attendees:

Councillors

- Cr Amanda Stone (Mayor)
- Cr Danae Bosler
- Cr Mi-Lin Chen Yi Mei
- Cr Misha Coleman
- Cr Jackie Fristacky
- Cr Stephen Jolly
- Cr Mike McEvoy
- Cr Daniel Nguyen
- Cr James Searle

Council officers

- Vijaya Vaidyanath (Chief Executive Officer)
- Ivan Gilbert (Group Manager - CEO's Office)
- Andrew Day (Director - Corporate, Business and Finance)
- Chris Leivers (Director - Community Wellbeing)
- Joanne Murdoch (Director - Advocacy and Engagement)
- Bruce Phillips (Director - Planning and Place Making)
- Jane Waldoock (Assistant Director - Planning and Place making)
- Guy Wilson-Browne (Director - City Works and Assets)
- Fred Warner (Group Manager – People, Culture and Community)
- Mel Nikou (Governance Officer)

3. Declarations of conflict of interest (Councillors and staff)

4. Confidential business reports

Item

- 4.1 Matters prejudicial to Council and/or any person
- 4.2 Proposed developments

Confidential business reports

The following items were deemed by the Chief Executive Officer to be suitable for consideration in closed session in accordance with section 89 (2) of the *Local Government Act* 1989. In accordance with that Act, Council may resolve to consider these issues in open or closed session.

RECOMMENDATION

1. That the meeting be closed to members of the public, in accordance with section 89 (2) of the *Local Government Act* 1989, to allow consideration of:
 - (a) matters prejudicial to Council and/or any person; and
 - (b) proposed developments.
2. That all information contained within the Confidential Business Reports section of this agenda and reproduced as Council Minutes be treated as being and remaining strictly confidential in accordance with the provisions of sections 77 and 89 of the *Local Government Act* 1989 until Council resolves otherwise.

5. Confirmation of minutes

RECOMMENDATION

That the minutes of the Ordinary Council Meeting held on Tuesday 7 March 2017 be confirmed.

6. Petitions and joint letters

7. Public question time

Yarra City Council welcomes questions from members of the community.

Public question time is an opportunity to ask questions, not to make statements or engage in debate.

Questions should not relate to items listed on the agenda. (Council will consider submissions on these items separately.)

Members of the public who wish to participate are to:

- (a) state their name clearly for the record;
- (b) direct their questions to the chairperson;
- (c) ask a maximum of two questions;
- (d) speak for a maximum of five minutes;
- (e) refrain from repeating questions that have been asked previously by themselves or others; and
- (f) remain silent following their question unless called upon by the chairperson to make further comment.

8. General business

9. Delegates' reports

10. Questions without notice

11. Council business reports

Item		Page	Rec. Page	Report Presenter
11.1	Australian Catholic University Amendment to Approved Development Plan - 115 Victoria Parade and 28 - 42 Young Street, Fitzroy.	8	35	Mary Osman - Manager Statutory Planning
11.2	Council Submission on State Government Fast-track Amendment Affecting 35 Johnston Street and 50 Perry Street, Collingwood (Collingwood Arts Precinct)	335	341	David Walmsley – Manager City Strategy
11.3	Potential for Council to offer Environmental Upgrade Agreements (EUA's) - 2017 update	518	520	Michael Oke – Sustainability Unit Manager
11.4	Business Advisory Group - appointment of business representatives	522	523	David Walmsley – Manager City Strategy
11.5	Yarra Libraries Local History Policy 2017	525	526	Felicity Macchion - Manager Yarra Libraries
11.6	National Disability Insurance Scheme - Public Forum Options	538	543	Adrian Murphy – Aged and Disability Services
11.7	Edinburgh Gardens WWI Memorial Future Options	544	552	Siu Chan – Business Unit Manager Arts, Culture and Venues
11.8	Potential Processes for: (a) Community / Developer Engagement re Development Proposals and (b) Sourcing of Independent Heritage Advice	601	607	Ivan Gilbert - Group Manager Chief Executive's Office

Public submissions procedure

The public submission period is an opportunity to provide information to Council, not to ask questions or engage in debate.

When the chairperson invites verbal submissions from the gallery, members of the public who wish to participate are to:

- (a) state their name clearly for the record;
- (b) direct their submission to the chairperson;
- (c) speak for a maximum of five minutes;
- (d) confine their remarks to the matter under consideration;
- (e) refrain from repeating information already provided by previous submitters; and
- (f) remain silent following their submission unless called upon by the chairperson to make further comment.

12. Notices of motion

Item		Page	Rec. Page	Report Presenter
12.1	Notice of Motion No 7 of 2017 Cr McEvoy re Bike Infrastructure Funding	613	614	Mike McEvoy - Councillor

13. Urgent business

Nil

11.1 Australian Catholic University Amendment to Approved Development Plan - 115 Victoria Parade and 28 - 42 Young Street, Fitzroy.

Trim Record Number: D17/11590

Responsible Officer: Director Planning and Place Making

Purpose

1. The purpose of this report is to assess an amendment to the approved Australian Catholic University (ACU) Development Plan.
2. The ACU have lodged declaration proceedings with the Victorian Civil and Administrative Tribunal (VCAT), therefore Council's is determining a position only on the proposed amended Development Plan.

The Site

3. Development Plan Overlay – Schedule 2 (DPO) covers the sections of land known as 115 Victoria Parade (Mary Glowery Building), 28 Young Street (car park) and 20 – 26 Brunswick Street (cathedral hall and adjacent buildings).



Figure 1: Aerial photo with DPO area overlaid.

4. For the purpose of this report, the subject site is defined as the Victoria Parade site, and Young Street site.
5. No change is proposed for the Brunswick Street, buildings.

Victoria Parade/Napier Street

6. The Victoria Parade site is an island site bound by Victoria Parade to the south, Napier Street to the east, Little Victoria Street to the North and Young Street to the west.
7. The site is occupied by a complex of three buildings forming an 'L' shaped structure with the remainder of the site generally occupied by an at grade car park.
8. The building occupying the site was formerly the Commonwealth Note and Stamp printing building and is now known as the Mary Glowery Building.



9. The building presents as a 6 storey form to both Victoria Parade and Young Street with a lower 4 - 5 storey section (depending on the slope of the land) presenting to both Napier Street and Little Victoria Street.
10. The three buildings are all connected with a visible glazed structure connecting the Young and Little Victoria Street buildings with an additional 2 – 3 storey glazed addition to Victoria Parade frontage which marks the entry to the site.
11. The remainder of the site is occupied by hardstand areas used as a 50 car space car park with access from Victoria Parade with periphery planting.
12. To Napier Street, the sites direct interface is with the open corner to Victoria Parade occupied by the at grade car park area and the lower 4 -5 storey built form.
13. Victoria Parade is a wide boulevard and a declared Main Road. Victoria Parade runs in an east-west direction with multiple traffic lanes, dedicated bus lanes and a central tree lined median accommodating tram lines. Intermittent parallel parking is provided on both sides of the Street.
14. Victoria Parade is one of the borders of the Municipality with the City of Yarra managing the north side and the City of Melbourne the south side.
15. Napier Street is a wide tree lined street with an alternating arrangement of angled and parallel parking on either side of the street. The street also operates as an important bike thoroughfare with clear markings on the road pavement; close to the Victoria Parade intersection is a Melbourne Bike share station. Currently there is no access to the ACU car park from Napier Street.

Young Street

16. The Young Street building is a 3 storey car park with parking over 4 levels (including at roof level).



17. The car park is built boundary to boundary and presents as an open deck car park structure to Young and Little Victoria Streets and solid concrete walls to Little Napier Street and the adjoining residence at 44 Young Street.



18. Vehicular access and egress for the car park is from Young Street with a secondary point from Little Victoria Street.
19. Young street is a north – south orientated Street. ACU buildings flank both sides of the street for a substantial portion, including the Daniel Mannix Building which was relatively recently constructed. For part of the streets length it presents as a 'shared zone'.

Surrounding Context

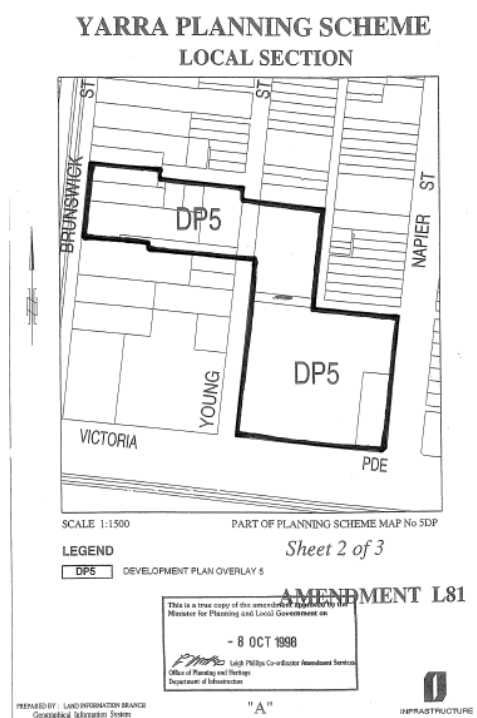
20. To the south, the site has an interface with Victoria Parade, a wide boulevard which includes a wide central median with mature tree planting and a busy tram route. The road pavement accommodates a dedicated bus lane and multiple car lanes.
21. Built form on the City of Yarra side is made up of institutional buildings including St Vincent's public and private hospitals and the subject site which can be typically described as substantially scaled built form interspersed with some lower scale Victoria era buildings. From Napier Street extending east, built form typology visibly changes with scale reducing substantially with finer grain lower scaled Victorian era built form.

22. To the north are the residential streets of Napier and Young Streets which typically comprise, single and two storey Victoria era dwellings.

Background

History

23. On 8 October 1998, the then Minister for Planning and Local Government approved amendment L81 to the Yarra Planning Scheme which introduced the Development Plan Schedule applying to the ACU site known as land described as 115 Victoria Parade, 28 Young Street and 20 – 26 Brunswick Street Fitzroy (See image below).



24. At the time of the amendment, the site was also encumbered by the then 'Urban Conservation Area No.1 & 2 (equivalent of the now Heritage Overlay) and Height Control Area 95, for land fronting Young Street which allowed a maximum height of 15.5 metres.
25. The ACU lodged a development plan (**the plan**) with Council on 9 October 1998. The plan generally showed the re-use of the existing buildings with some new works including:
- (a) a new lecture building at the rear of the Cathedral Hall building;
 - (b) an additional level to the existing Young Street Car park; and
 - (c) alterations to 115 Victoria Parade to include new entrance and glazed canopy to Victoria Parade and new glazed stair towers.
26. Other information included:
- (a) the proposed campus was anticipated to have a total of 2125 effective full time students and 180 staff;
 - (b) typical operating hours 8:00am – 9:30 pm Monday to Thursday, and 8:00am - 6:00pm Friday with the library open on the weekends; and
 - (c) a total of 255 car parking spaces.
27. Council determined at its meeting of 16 November 1998 to support the Development Plan subject to a number of conditions.
28. The Development Plan was endorsed by Council on 8 January 1999.
29. A planning permit application was lodged with Council on 30 October 1998. Council endorsed the development plan, issued a permit and endorsed planning drawings on 8 January 1999 facilitating the use and development of the site as a University.

Current Amendment to Development Plan

30. An amendment to the approved development plan was lodged with Council on 9 February 2016. The plan was advertised and 123 submissions were received.
31. A consultation meeting was held on 30 August 2016.
32. In response to the submissions and Council referral comments, the ACU lodged an amended DP with Council on 28 October 2016. The main changes can be summarised as:
 - (a) a reduction in height for the new building and additions to the Mary Glowery building by 2 storeys;
 - (b) new design for the Young Street car park site;
 - (c) redesign of access from Napier street, maintaining all car parking on the eastern side of the street and removal of 2 trees; and
 - (d) deletion of the air bridges and Young street road closure.

This version of the DP is being considered by this report.

33. Council were notified by the ACU on 21 December 2016 that a S149 appeal (declaration proceedings) had been lodged with the Victorian Civil and Administrative Tribunal (**VCAT**). The practice day hearing was held on 24 February with the compulsory conference set down for 12 May and the hearing scheduled to commence on 28 June for 3 days.
34. If an amended development plan is approved, a planning permit will be required to be lodged with Council which must be in generally in accordance with any approved DP.
35. Additionally, Council have been advised that Heritage Victoria have accepted a nomination for the Mary Glowery Building to be included on the State register. This is a separate process to the consideration of the DP and will not be discussed further as part of this report.

The Proposal

36. The proposed amendment to the DP seeks to *'formalise the strategic direction of the campus's next phase of development over the coming decade'*.
37. Key components of this strategic direction includes:

General

- (a) consolidating the campus (which does include sites outside of the DP area);
- (b) increase student numbers from 2,125 full time students to 12,900 students (inclusive of surrounding sites not included within the DP);
- (c) increase staff numbers from 180 to 1,200 staff (inclusive of surrounding sites not included within the DP);
- (d) increase the typical operating hours 8:00am – 9:30 pm Monday to Thursday, and 8:00am - 6:00pm Friday with the library open on the weekends, to 8:00am to 11:00pm with the library accessible 24 hours;

Victoria Parade

- (e) demolition of the existing glass façade and ramp arrangement attached to the Mary Glowery building as it presents to Victoria Parade and re-establish the heritage façade;
- (f) a new 12 storey plus plant (13 storey) building on the corner of Victoria Parade and Napier Street including:
 - (i) use of the building for teaching and learning space, Hub spaces and offices.
 - (ii) car parking proposed in 7 basement levels accommodating 270 cars accessed from Napier Street; and
 - (iii) Works to Napier Street including tree removal (2), removal of car parking on the east side of the Napier Street, relocation of bike share facility and alterations to the road pavement including new bike path, accessway, raised crossing.

- (g) a four storey plus plant (5 storey) addition to the northern most wing of the Mary Glowery building;



NOTE: Dark grey area outline of existing Mary Glowery Building, Orange proposed new building envelope

Young Street

- (h) demolition of the existing car building;
- (i) a new 3 level (equivalent of 4 levels) triangular shaped building surrounded by landscaping:
- (i) use of the building a hub and performance space; and
 - (ii) three basement levels used as a hub space and multi-purpose sports courts; and



Brunswick Street

- (j) no change.

Planning Scheme Provisions

38. The site is included in two zones, Mixed Use Zone and the Commercial 1 Zone.

39. Both the Young Street precinct and the Victoria Parade precinct have interfaces with Residentially Zoned land (Neighbourhood Residential Zone 1) separated by either Little Napier/Victoria Street or Napier Street.



40. The site is also included within Heritage Overlay area 334 or 347 with buildings either identified as individually significant (red) or non-contributory (green). For the purpose of this report the car park at 28 Young Street is identified as non-contributory and the Mary Glowery building to Victoria Parade being Individually significant.



41. An Environmental Audit Overlay and Design and Development Overlay – Schedule 2 – Main Roads and Boulevards also applies to the site.
42. The Development Plan Overlay - Schedule 2 Australian Catholic University Development Plan, applies to the site and is a site specific control which '*provides criteria for any future use and development*' of the site.



Extent of DPO outlined in Green

43. At Clause 3, the DPO outlines what is require to be shown in a development plan which includes requirements relevant to the use of the site, development and traffic and landscape management.
44. Clause 4 of the provision which outlines the decision guidelines Council must consider, these are:
 - (a) *any written comments received in response to the display of the development plan;*
 - (b) *the impact of the proposed development and use on adjacent and nearby properties, roads, and other physical infrastructure;*
 - (c) *the impact of traffic generated by the proposal and whether it is likely to require special traffic management or control works in the neighbourhood;*
 - (d) *points of access to and from the land and whether they are suitably located;*
 - (e) *the layout and management of car parking areas and access ways to and from them;*
 - (f) *any relevant Council policy;*
 - (g) *any applicable heritage study and applicable conservation study;*
 - (h) *the character and appearance of any proposed buildings or works and their impact on the heritage significance, character and appearance of any heritage places; and*
 - (i) *whether the location, bulk, height and appearance of any proposed buildings or works will be in keeping with the character, amenity and streetscape of the area.*

This will form the basis of Council's assessment of the development plan.

Advertising

45. As outlined in the background section of the report, the development plan was advertised and received 123 submissions. The amended development forming the basis of this report was readvertised to all the original submitters. Sixty submissions were received. The submissions raised a number of concerns including:
 - (a) the amendment to the approved development plans in unlawful;
 - (b) the university should prepare a Master Plan encompassing all their sites;
 - (c) increase in student and staff numbers are too high;
 - (d) scale of development is incongruous with surrounding residential and commercial properties;
 - (e) provision of car parking on site is too high and inconsistent with principles of sustainability as practiced by other universities;

- (f) access to car park should be maintained from Victoria Parade and not Napier Street;
- (g) safety between bikes and cars along Napier Street;
- (h) traffic impact on Napier Street from illegal manoeuvres;
- (i) impact on heritage of the South Fitzroy Precinct and the Mary Glowery Building;
- (j) proposed amendment to the development plan conflicts with the design objectives of the Design and Development Overlay.
- (k) amenity impacts on the surrounding residential area including overlooking;
- (l) privatisation of public land; and
- (m) loss of street trees.

Referrals

46. The development was referred both externally and internally to the following parties:

- (a) Open Space;
- (b) Infrastructure;
- (c) Engineering;
- (d) Strategic Transport;
- (e) Heritage; and
- (f) Urban Design (external consultant).

All referral comments can be found in full in the attachments section (16 – 21).

47. The referral comments will be discussed in the assessment section of the report.

PLANNING CONSIDERATIONS

48. The key planning considerations for Council in considering the development plan relate to:

- (a) decision guidelines outlined within the Development Plan – Schedule 2 which can broadly be described as:
 - (i) Urban Design;
 - (ii) Heritage;
 - (iii) Car Parking / Traffic / Access;
 - (iv) Amenity;
 - (v) objector concerns (other than those outlined above) which include: and
 - lawfulness of the Development Plan and Master Plan process;
 - expansion of the Use;
 - loss of trees, landscape and privatisation of public spaces; and
 - (vi) nett community benefit.

Urban Design

49. A decision guideline within the DP, requires Council to consider:

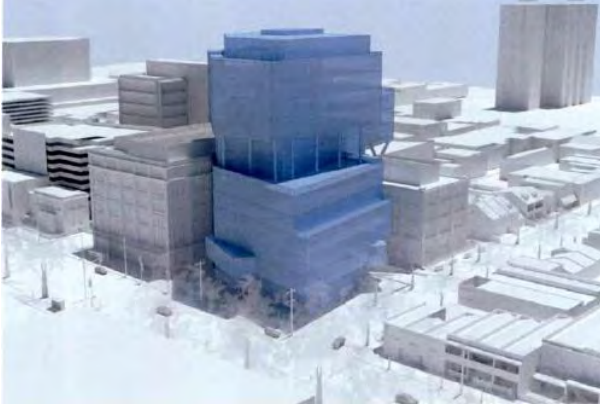
- (a) *whether the location, bulk, height and appearance of any proposed buildings or works will be in keeping with the character, amenity and streetscape of the area'.*

Victoria Parade Building

50. The sites location in effect interfaces with a number of 'different' streetscape contexts. The Victoria Parade building has an interface with the robust institutional built form along Victoria Parade which then transitions in scale to low rise single and two storey Victorian forms to both the east and to the north.

Victoria Parade design response

51. The proposed envelope for the Victoria Parade new building seeks to fill in the corner of Victoria Parade and Napier Street and seeks to mark this corner with a strong form which incorporates a taller 12 storey building massed in three parts. The lower podium levels in general terms align with the parapet height of the existing Mary Glowery building, the void area in the middle portion presents as a break in the building with upper section of the building rising to a maximum height of 59.45 metres.

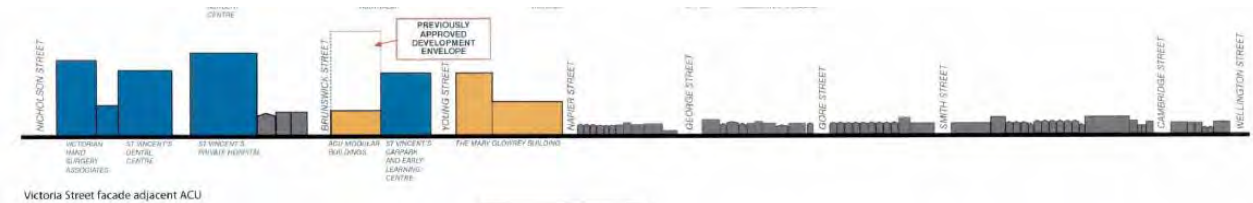


52. External urban design advice was sought by Council who were generally supportive of the scale and composition of the proposed building envelope and 'indicative architectural' response as it presents to Victoria Parade.
53. The urban design advice outlines that the proposed height no longer overwhelms the Mary Glowery building and while taller in height than existing built form, the composition of the proposal ameliorates the massing of the building.
54. It is agreed that many of the massing techniques which have been incorporated into the massing envelope and architecture do moderate some of the scale of the proposed building, however the overall height of the proposal when viewed within the context of Victoria Parade is considered excessive as it relates to both the institutional precinct and more specifically the transition to the lower scaled development to the east.
55. A 12 storey building could be supported on the site, however the higher floor to ceiling heights has exaggerated the overall height of the building to equate closer to a 15 storey commercial building or a 19 – 20 storey residential building.
56. When determining a contextual height for the site, consideration must be given to the transitional nature of the site as it interfaces with the institutional section of Victoria Parade between Nicholson and Napier streets with the lower scale built form which is prevalent east of Napier Street.

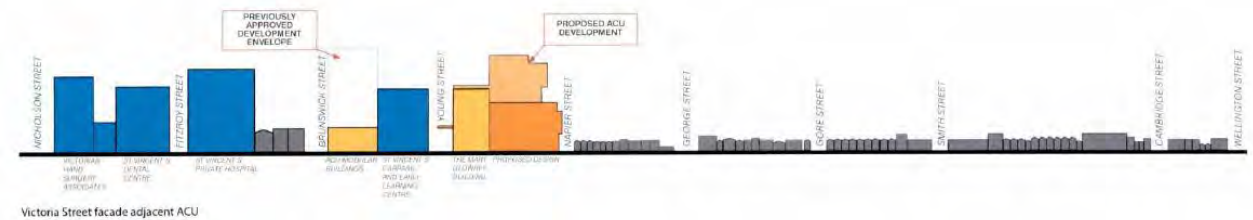


Victoria Street, north view towards Australian Catholic University

57. Within the DP documentation lodged by the applicant, the proposed streetscape view shown in section 3.0 details the existing physical context which shows this dramatic scale transition between the existing conditions on the site.

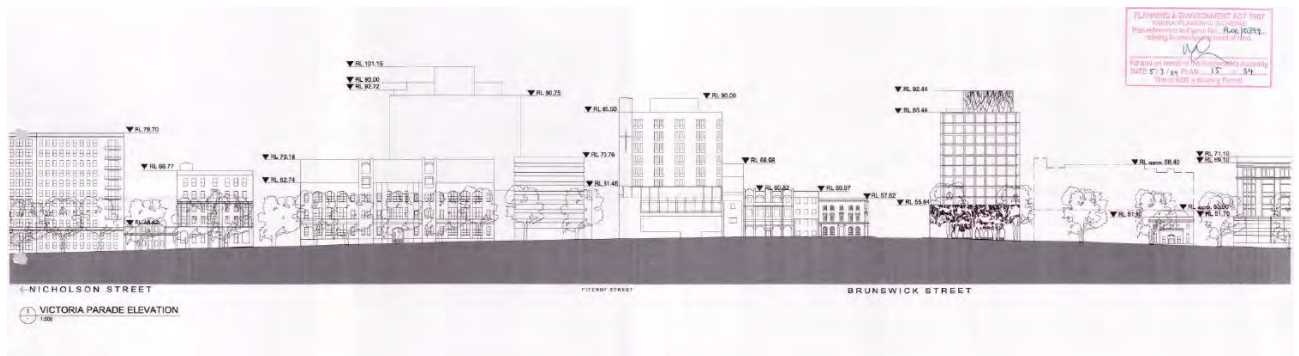


Existing streetscape inclusive of outline of previously approved building at 81 – 89 Victoria Parade.



Proposed streetscape

58. The site does form the bookend of the institutional built form 'precinct', however, it is considered that the scale proposed is too tall for its context.
59. In assessing the height of the proposed building, an examination of built form within this sub precinct needs to occur which details an emerging scale of development which historically has been developed in this precinct.



60. The figure above details the now lapsed approval which was granted by VCAT for an office building on the corner of Brunswick and Victoria Parade. The documentation which accompanied the application included a detailed survey streetscape along the institutional precinct which shows the scale of existing built form along this section of the street and starts to detail an upper level scale of development which generally sits at approximately RL 90 – 92 which equates to generally a 45 – 47 metre building.
61. As proposed, the new ACU building would roughly be at RL103 being the tallest building along this section of Victoria Parade and some 11 – 13 metres taller than surrounding built form.
62. In isolation, the issue of the proposed building envelope being the tallest building in the precinct may seem inconsequential, however, a contextual response does not support the design response at the scale proposed and the building envelope needs to be reduce to a maximum height of RL92.
63. The composition of the proposed building will require modification but the principal of the tripartite (podium, middle and top) building typology should be maintained.

Napier Street design response

64. To Napier Street, the scale of development beyond Little Victoria Street, drops dramatically from a four storey Victorian commercial building down to a two storey Victorian residential terrace typology typical of the Napier Street context.

65. Historically, this transition of scale is not uncommon in Yarra and the separation offered by Little Victoria Street demarcates the end of the commercial nature of buildings fronting Victoria Parade and the residential hinterland.



View towards Little Victoria Street from Napier Street.

66. The proposed building envelope seeks to build on top of the Young Street section of the Mary Glowery building to a height of approximately 43 metres to the top of the plant.



View of proposal from Napier Street aerial view and streetscape view

67. Council's consultant urban designer raised two concerns with the expression of the proposed building envelope stating:
- 'However, we are concerned about the volume of the 'bridging' element as it transverse between the proposed 115B form and the addition atop of the north wing. While, we appreciate and support this element and its dark, recessive treatment we consider that some recession or sculpting in the form to create a central vertical rebate is required to reference the space 'break' between the new 115B and south retained façade of the northern wing'.*
 - The revised massing strategy for 115B has also resulted in the creation of a more successful 'bridging' element which integrates the proposed new building and additions together as one cohesive form. However we note that the extent of the 'piggy back' arrangement now pushes further north and cantilevers over the existing northern façade of the Mary Glowery (north wing) and this element is also now supported by external columns which project forward to the existing façade. This results in the visual dominance of the upper level additions above the existing façade. This is further emphasised by the dark finish which will further add to the prominence of this addition, creating a 'top heavy' arrangement which is to be supported by visible external additions. We therefore, recommend that the visual prominence of the additional built form be reduced and that the external columns be removed.*

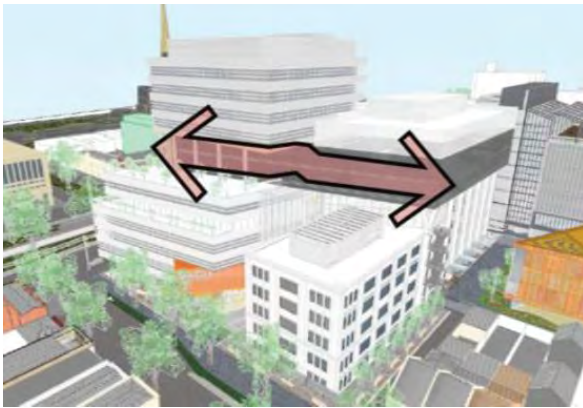


Figure relevant to point (a) above

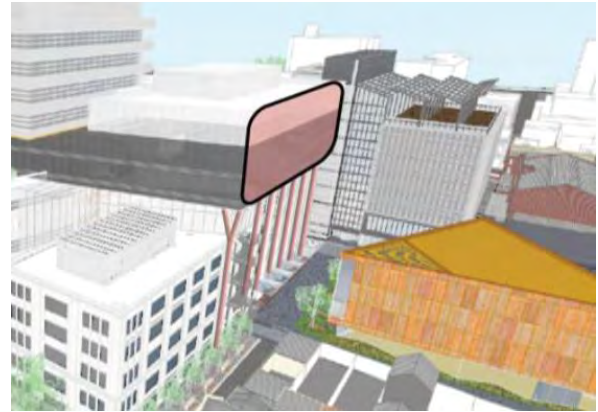


Figure relevant to point (b) above

68. In response to the above referral, schematic plans were prepared to address the concerns. In response to point (a) above. The renders show a new treatment to the bridging element, by introducing a clear glass section to the darker glazed void area and a new treatment to the stair expressing this element more clearly. This change could form a condition on any approval granted.



Lodged Design proposal



Response to Urban Design comments

69. In response to point (b), schematic plans show a reduction in the extent of the cantilever to less than one metre and the addition will sit on top of the structural columns rather than cantilever over them, the width of the column have been reduced to allow views through to the existing windows of the Mary Glowery building and the introduction of vertical elements to break up the dark glazing section. These changes could form a condition of any approval granted.



Lodged Design proposal



Response to Urban Design comments

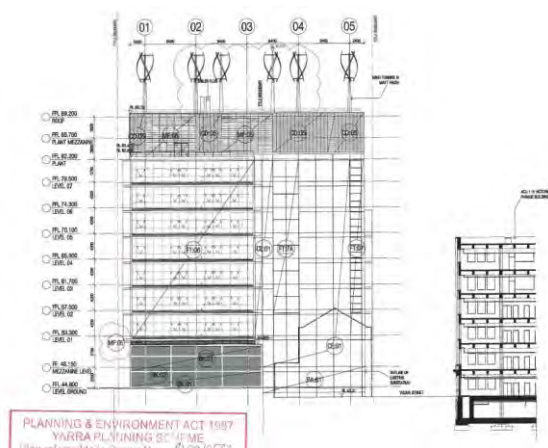


Lodged Design proposal



Response to Urban Design comments

70. The applicant has noted that the external structural elements are required to support the additions on top of the Mary Glowery Building. While Council acknowledges that advice given to the applicant, from a design perspective this is not an acceptable outcome for an addition to an individually significant heritage building and these externalised support structures should be deleted.
71. The presentation of the addition to the Mary Glowery Building to Young Street is of an appropriate scale and complements the built form of the existing Daniel Mannix building.



Endorsed plan for Daniel Mannix Building



Daniel Mannix Building (red X)

72. From the endorsed plans for the Daniel Manix building, this shows the height to the top of the building excluding the wind turbines is RL89.20. The top of the extension to the Mary Glowery building would be approximately RL86.5 which is of a comparable scale and forms an appropriate design response for this interface. The treatment of the plant area, however, needs to be reduced in scale and incorporated into the overall design of the addition.
73. The proposed building envelope of the extension projects east towards Napier Street and setback 32 metres from this street interface. The new addition interfaces with the wall of the existing car park building for a majority of its length, however, extends towards the rear private open space areas of the properties over Little Victoria Street.



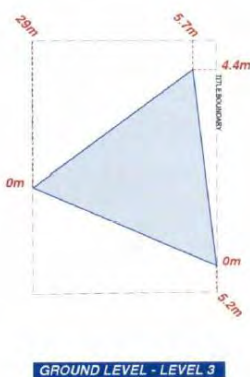
Proposed Building Envelope plan

Note: red line shows the extent of building projecting towards the sensitive private open space area of abutting dwellings.

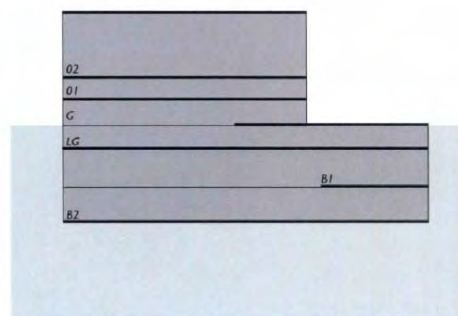
74. While separated by the 5 metre width of Little Victoria Street, the extension to the Mary Glowery building should more sensitively respond to the built form of Napier Street with the any addition commencing at the rear fence line of theses dwellings with an approximate setback of 42 metre from Napier Street.
75. With the proposed increased setback, the addition would still appear as a substantial transition of scale however view lines from the rear yards would be free of additional built form and present an 'improved' visual bulk outcome.

Young Street

76. The Young Street context similarly has a dual interface with the institutional buildings of ACU and the residential context of Young Street to the north and Napier Street to the east.
77. These interfaces need to inform the design outcomes for the site and each interface will be discussed in turn.
78. The proposed envelope seeks the construction of a triangular building set in a landscape setting. The proposed building would have a height of approximately 16.20 metres or presenting as a 4 storey building due to the double height performance space at level 2



INDICATIVE BUILDING HEIGHT
16.20m
TAKEN FROM THE MIDPOINT OF THE
WESTERN TITLE BOUNDARY OF YOUNG STREET



YOUNG STREET BUILDING

79. The general scale of the building at four storey is consistent with the scale of development along the Institutional section of Young Street.
80. To the immediate north currently, a 2.5 - 3 storey wall immediately interfaces with the adjoining residential property to Young Street. While presenting a high wall interface with the open space of this dwelling, this wall also offers some protection between the ACU and the residential precinct beyond.

81. As depicted in the proposed DP, the new interface with this property would be a fence with the closest part of the building setback a minimum of 5.7 metres from the common boundary increasing to 29 metres as depicted in the image above.
82. The building as depicted in the schematic drawings will present as a generally solid façade with glazed corners and a transparent ground level which interacts with the proposed landscaped areas.
83. Through discussions with the resident of the immediately adjoining property concern has been raised with the removal of the on boundary wall and with the perception of overlooking as a result of the glazed areas. A preference has been made for the retention of the wall to maintain a physical barrier between the residential uses and the ACU.
84. A condition could require the retention of the on boundary wall interfacing with 44 Young Street, this change is inconsequential to the design of the proposed building and provides an opportunity for use of this wall as Public Art on the ACU side.
85. The eastern interface is more problematic as the width of the building is most clearly expressed along the back yard interface with the properties facing Napier Street.
86. The setback to Little Napier Street ranges between 0 – 4.4m at it's greatest with the height equivalent to a four storey building; acknowledging the separation of Little Napier Street provides some buffer between the new building and the rear open space areas.
87. Concern with this interface was raised within the urban design referral which stated:

'The massing and scale of the revised Young Street Building (Campus Hub) is encouraged. The central and triangular shaping of the built form to create an open space that will be well surveyed by the surrounding buildings is thoughtful and responds to the existing scale of the internal spaces of the University. While this new 'Hub' building is proposed to rise to approximately 16.2m (4 – 5m higher than the existing multi-level car park) it is to be setback between 0m and 4.4 m from Little Napier Street. While we support the response, and acknowledge it will be a superior outcome for the site and a vast improvement over the existing condition, we seek more information in relation to the massing arrangement (particularly to the south end of the Little Napier Street) and comparison to the impacts of the existing blank concrete wall.

Therefore, we support this response in an overall sense, but seek clarification that the height and massing results in an appropriate response to the POS of abutting neighbourhood residential properties which front Napier Street. Ideally the visual bulk and overshadowing impacts should be the equivalent to the existing structure.

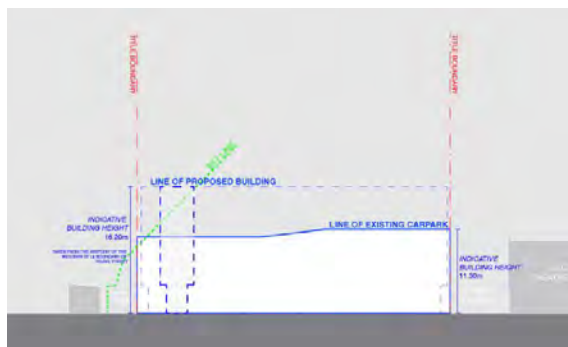
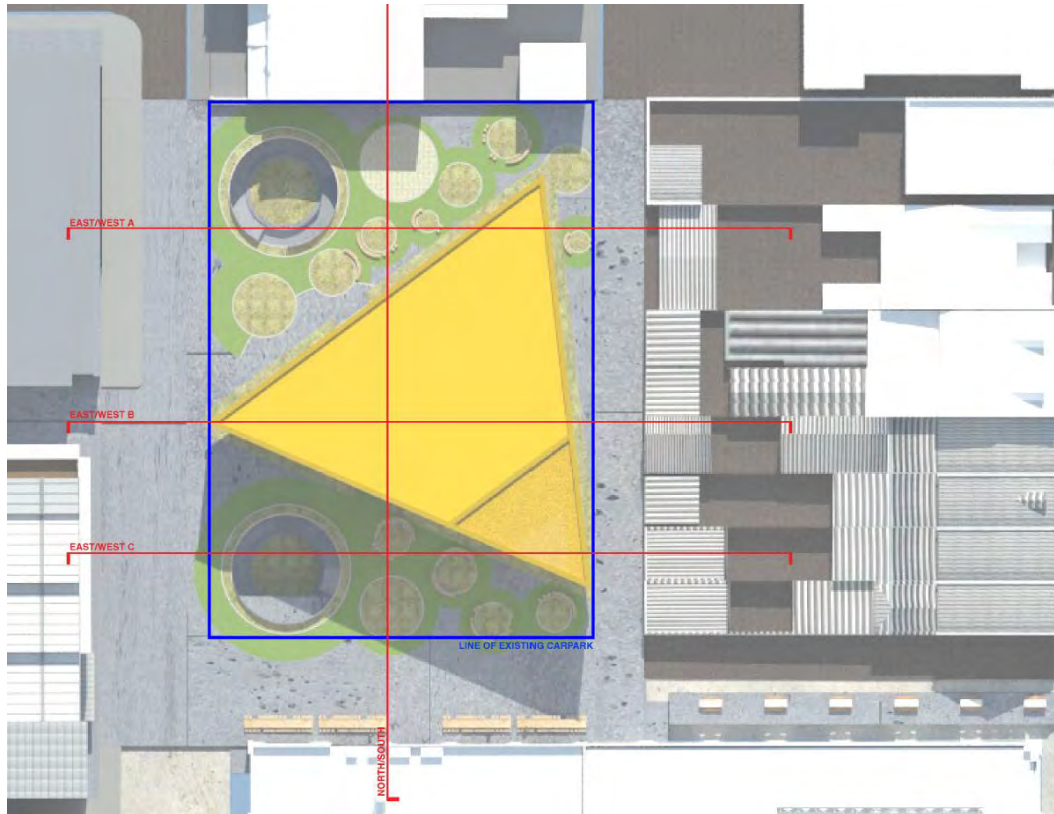


Existing conditions

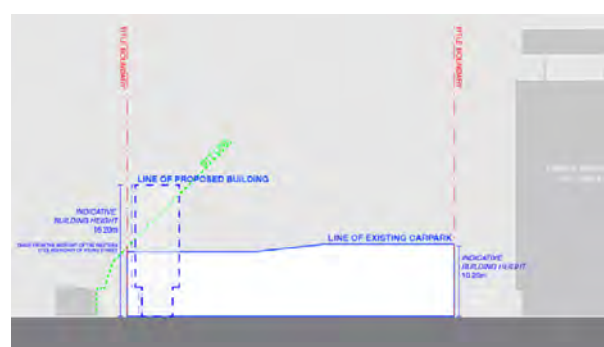


Proposed conditions

88. In response, further information was submitted by the ACU to demonstrate the differences between the two structures (existing and proposed) and included the ResCode B17 (building envelope) line which, while not technically applicable provides a guide to the impact on the adjoining dwellings.



East/West Section A



East/West Section A

89. The sectional analysis above show the degree of increased building that would be visible from the rear open space of the adjoining properties (dotted line shows height of proposed building), in response to the additional information, the concerns expressed in the urban design referral were confirmed in the revised comments.

'In relation to the height and massing of the proposed Campus Hub building, in place of the existing multi-level car park and having reviewed the additional cross-sections provided and viewed your photos from 2 adjoining private open spaces. We consider that the proposed massing will result in an unacceptable visual bulk impact on the abutting Napier Street residential properties. Given the NRZ designation these properties are unlikely to significantly change into the future and therefore, any new built form to their west needs to respond appropriately. Therefore, I would recommend either a reduction in overall height or increased setbacks from Little Napier Street to result in a comparable built form as the existing car park wall.

While, I consider the proposed Campus Hub building to represent a significant improvement in terms of architecture and public realm response to the laneway. As proposed at 16.2m in height it will be considerably higher than the existing sheer, blank concrete wall. We consider that a height and massing that generally accords with the ResCode B17 envelope, as measured from the rear (western) property boundary of the NRZ properties to be a more sensitive response.

90. From an amenity impact a reduction in the interface with the residential properties to the east is required and this could form a condition on any approval given. This will be discussed further in the amenity section of the report.

Urban Design Summary

91. Proposed envelopes for both the Victoria Parade and Young Street buildings have partly responded to their immediate contexts, however, have not given enough regard to the residential context which they abut.
92. Conditional approval of the DP should be given subject to reductions in height and scale to the Victoria Parade building and height reduction to the Young Street building.

Heritage

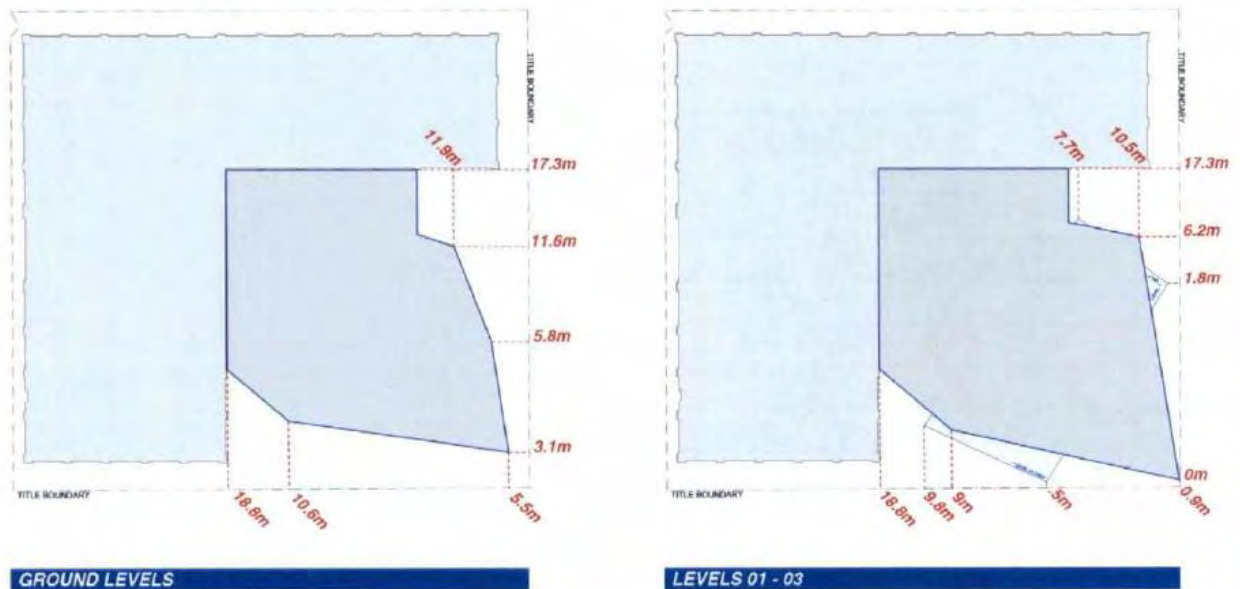
93. The relevant decision guidelines with the DP requires Council to consider:
- (a) *any applicable heritage study and applicable conservation study; and*
 - (b) *the character and appearance of any proposed buildings or works and their impact on the heritage significance, character and appearance of any heritage places.*

Demolition

94. It is clear, that neither the at grade car park area or the car park structure have any heritage value and their demolition is supported by Council policy.

Victoria Parade New Building

95. The DP was referred to Council's Heritage advisor who does not support the additions to the Mary Glowery Building or the new building proposed to the corner of Napier Street and Victoria Parade.
96. When firstly considering the scale of the new building on the corner, the advice concludes that too much emphasis has been drawn from the scale of buildings on Victoria Parade with little weight given to the context and low residential forms along Napier Street.
97. The advice outlines that a sightline taken above the Mary Glowery Building north wing should inform an appropriate envelope and it suggests that any new building on this section of the site should not exceed the height of the Daniel Mannix building.
98. This advice generally concurs with the conclusions reached as part of the Urban Design assessment which requires a reduction in the height of the new building to ensure an appropriate built form outcome for the site is achieved.
99. Concern has also been outlined with the interface of the new building with the setbacks along Napier Street. The proposed angular façade treatment appears incongruous with the prevailing parallel setbacks of the buildings along both Napier and Victoria Parade.
100. While it can readily be argued that a building doesn't not have to be identically sighted to be an appropriate response to the streetscape, the advice acknowledges that should the angular façade treatment be preferred that the setbacks at levels 1 – 3 must be increased to be no less than those afforded at ground floor level.



101. It is considered that the deeper setbacks afford greater flexibility in articulation of a new building on the site and if levels 1 – 3 setbacks were modified to accord with levels 4 – 5 that this would facilitate a landscaped forecourt to Napier street with some minor building projection at the upper levels, however, this cantilever would be limited to a small section central to the site. A revised setback would appropriately pull new built form back and away from the Mary Glowery Building.

Additions to the Mary Glowery Building

102. Council's heritage advisor commented on the proposed additions to the Mary Glowery Building in her original set of comments based on the first set of amended DP plans where she states:

"It is proposed to add 2 more levels and a plant level to the Mary Glowrey building (former note printing works). While the existing building is part of the existing urban fabric, an increase in height, from 6 to 8 storeys, as proposed will result in an effect on the immediate environs which will be similar to that of the Daniel Mannix building. As proposed, the additional levels will change the appearance of this building unacceptably and will also have an adverse impact spatially on the dwellings on the west side of Napier Street running northwards from Little Victoria Street and will loom over them. (p. 36) The Heritage Policy, inter alia, requires consideration of the architectural integrity and context of the heritage place or contributory element, respect for the scale and form of the existing heritage place or contributory elements to the heritage place and requires the incorporation of treatments which make upper levels less apparent. The addition does not comply with the Heritage Policy and is not acceptable"

103. Her view on the amended plans has not changed.
104. Many of the concerns expressed in the heritage referral mirror those already outlined in the urban design section of the report.
105. It is noted that the proposed additions (as amended) on top of the Mary Glowery Building have been reduced in scale in an attempt to address the concerns expressed by Council's heritage advisor however, further changes are required ensure the built form addition to the Mary Glowery building are considered acceptable, these include (many already outlined in the Urban design section of this report):
- (a) the addition to be set in from the existing external façade of the heritage building;
 - (b) deletion of the external structural support columns;
 - (c) reduction in height and extent of the plant area to not be visible from the street and designed to be integrated into the building; and

- (d) increased setback from Napier Street from the current 31.9m to a minimum of 42.0 metres.

- 106. While an ideal outcome may be no addition to the Mary Glowery Building, Council policy does not prohibit additions to individually significant buildings, and the proposed DP currently maintains the main section of the building fronting Victoria Parade, clear of any additions. Subject to changes outlined above, it is considered that the building would present an acceptable outcome.

Young Street

- 107. The heritage advice suggests, that from a heritage perspective, the heritage value of this section of Young street has been changed by the more recent Daniel Mannix building and that the design response chosen for the Young Street site is acceptable from the Young Street streetscape.
- 108. Acknowledging however that any new structure would be an improvement to the streetscape than the existing car park building, the heritage advice suggests that due to the significance of Napier Street, that the scale of the building should be moderated and that a sightline should be taken from Napier Street to inform height for this building.



Aerial view of the proposed Young Street Building.

- 109. This requirement is considered too onerous and Council's heritage policy does not require sightlines as outlined in the advice. It is acknowledged that the building would most probably be visibly above the two storey Victoria forms, this would clearly read as a backdrop to the Napier Street buildings.
- 110. A height reduction is required to the Young Street building, however, this is due to amenity impacts and not heritage.

Other Heritage Matters

- 111. Other comments of relevance which should form conditions on any approved DP relate to:
 - (a) retention of the original 'Batemans Patent Melbourne Waterworks; and
 - (b) review of signage extent and presentation to be less intrusive to the heritage context.

Heritage Summary

- 112. Conditional approval of the DP should be given subject to reductions in height and scale and setbacks to the Victoria Parade building and a reduced envelope addition to the Mary Glowery Building.

Car Parking / Traffic / Access and Safety

113. The decision guideline within the DP, requires Council to consider:

- (a) *the impact of traffic generated by the proposal and whether it is likely to require special traffic management or control works in the neighbourhood;*
- (b) *points of access to and from the land and whether they are suitably located; and*
- (c) *the layout and management of car parking areas and access ways to and from them.*

Car Parking

114. Forming part of the development plan, the proposal seeks to demolish the two existing car parking areas, including the 250 space Young Street Car park building and the 50 spaces provide at grade within the hardstand area of the Victoria Parade building.



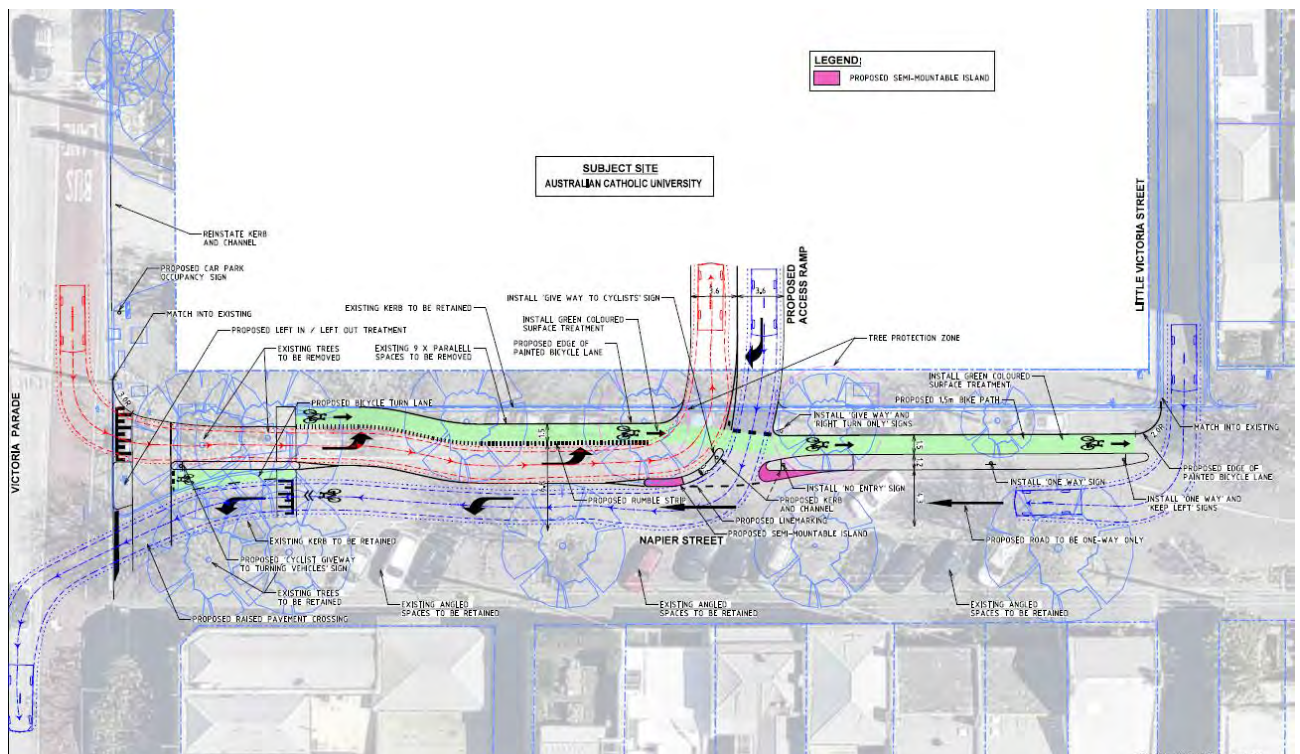
- 115. The proposed DP details a new basement car parking facility which seeks to reduce the provision of car parking for the university campus by 30 spaces being a total of 270 spaces instead of the currently 300 spaces available within the DP area.
- 116. Council's Engineers and Strategic Transport units offer no objection to the reduction in provision of 30 car parking spaces. It is considered due to the location of the site proximate to multiple excellent options for public transport that the provision of additional cars on site is not required and that there is scope for the development plan to further reduce the reliance on private vehicles.
- 117. Many of the submissions to Council raised concern with the number of car parking space being proposed and that the ACU campus should be reduced further and that the ACU's reliance on private car parking be reduced and increase reliance on more sustainable forms of transport.
- 118. The local residents also state that the potential conflicts with cyclist using Napier Street and the number of vehicle movements into and out of the car park must be addressed.
- 119. Submissions also suggested that if car parking was required that it should be provided on the Brunswick Street site owned by the University.
- 120. It is noted that Council is limited in its consideration to the boundary of the site and cannot impose requirements on land which does not form part of the DP.
- 121. The decrease in car parking numbers on the site is considered to be reasonable and supported by Council policy which seeks to reduce reliance on private motor vehicles. Officers recommend a further condition be imposed on any approval given to require more detailed analysis on the actual need to build car parking for the site and for the development of a green travel plan to support further reductions on the reliance on cars.

Car Parking other requirements

122. As part of the planning approval for the Daniel Manix Building (17 – 29 Young Street and 16 – 18 Brunswick Street, Fitzroy Permit PL09/0533), condition 22 required 12 car spaces allocated for disabled parking to be provided within the Young Street car park for staff and students, as the car park is proposed to be demolished; these 12 spaces at a minimum need to be relocated to the proposed new car park.

Access

123. The amended DP seeks to alter access to the car park from Victoria Parade and move this to Napier Street. The new access is to accommodate a 270 basement car park which would replace the proposed to be demolished Young Street Car Park and the demolished car parking areas to Victoria Parade.
124. To facilitate this, a number of works are proposed to Napier Street which include:
- (a) removal of 2 street trees on the corner of Victoria Parade and Napier Street;
 - (b) loss of all on street parallel car parking along the length of the site to Napier Street (10 spaces);
 - (c) relocation of the Melbourne Bike Share station; and
 - (d) alterations to the road pavement including new bike path, accessway, raised crossing.



125. The proposal was referred to both Council's engineering department and sustainable transport unit for comment. No objection was raised with the location of the new accessway point however a number of recommendations were made to improve the design to minimise risk of vehicles trying to turn left (heading north in Napier Street) to improve the legibility of bike priority along Napier Street and to provide safe pedestrian movement.
126. Subject to the following conditions, the proposed location of the accessway should be supported:
- (a) redesign to either raise the car park and bike lane and or convert area to a 'priority crossing' with a give way sign;
 - (b) inclusion of a sight triangle for the exit lane from the car park;

- (c) deletion of proposed use of 'concrete semi-mountable kerb and replacement with Council's bluestone semi-mountable kerb;
- (d) swept path diagrams for car existing critical spaces on the east side of Napier Street due to the island and separator;
- (e) additional flared island at the northern side of the car park entrance;
- (f) continuation of linemarking or a raised median island to the south of the car park entrance;
- (g) additional one-way arrows within the traffic lane;
- (h) installation of a 'No Through Road' sign at Victoria Parade;
- (i) dynamic parking vacancy indicator sign facing Victoria Parade;
- (j) swept paths to show that vehicles can't enter/exit the car park access via the northern section of Napier Street; and
- (k) a road safety audit and compliance with all its recommendations.

127. While a number of the submission received by Council raised concern with potential vehicle and bike conflict, based on the advice provided and subject to the above, no objection has been raised with the proposed access arrangements by Council' internal experts.

Bikes

- 128. The changes proposed to Napier Street would require the Bike Share station to be relocated. cursory information regarding the relocation of this facility has been provided for within the DP documentation and a condition would require this facility to be relocated along Victoria Parade or Napier Street frontage of the University.
- 129. On site bike parking is proposed within the DP to 80 spaces provided through the wider ACU site and not directly located within the DP area. As outlined earlier, sustainable transport needs to be a higher order initiative for the ACU with genuine sustainable transport initiatives to be incorporated within the DP. In this regard, a condition would require a minimum of 80 new bike spaces provided on site within the DP area.

Amenity

- 130. The proposed outcomes of the DP would have off site amenity impacts as proposed. The most sensitive interfaces are:
 - (a) northern interface between the existing Young Street property and the new Hub; and
 - (b) the eastern interface of the Hub with the rear open space of the Napier Street properties and the impact of the addition to the Mary Glowery building.
- 131. Both these sensitive interface have been discussed above with suggested conditions proposed to be imposed to address these.
- 132. In summary, subject to the following conditions, the amenity impacts are considered to be acceptable:
 - (a) retention of existing northern on boundary wall associated with the car park structure;
 - (b) eastern wall of the Young Street Hub to either comply with the B17 ResCode height envelope or alternatively, be no higher than the existing conditions with any higher structure to be setback to limit off site amenity impacts; and
 - (c) setback of the addition to the Mary Glowery Building to Napier Street to be increased from 31.9m to a minimum of 42 metres.

Objector Concerns - Other

133. A number of 'other' topical submissions were made to the DP, these are assessed below.

Lawfulness of the Development Plan and Master Plan process

134. Many of the submissions made to Council raised issue with the validity of the application to amend the DP as the changes proposed were too great or were 'transforming' the previously approved DP. Additionally the DP proposed new access to the site being from Napier Street and that the ACU had landholdings which extended beyond the DP boundaries with other street abutments and therefore the proposed DP should be invalid.
135. It is noted that the provisions of the Development Plan Overlay outlined at Clause 43.04-3 make it clear that *'The development plan may be amended to the satisfaction of the responsible authority'*.
136. Clause 3.0 of the Development Plan Overlay – Schedule 2 sets out the mandatory requirements for a development plan, and any amendment to the DP, and clause 4.0 sets out the decision guidelines for the assessment of such an amendment.
137. Subject to satisfying these decision guidelines an amended DP can be approved.
138. Officers acknowledge that a preference would be for a complete DP which covers the full land holdings of the ACU, however this is not what is before Council and this outcome cannot be compelled through this current process.



Expansion of the Use

139. Many submissions were made regarding the ambiguity of student and staff numbers proposed for the site. The 1999 DP clearly outlined 2,125 effective full time students and 180 staff.
140. The current DP makes reference to 10,000 students and 800 staff with an aspiration for 12,900 students and 1200 Staff. The DP needs as a condition to clarify the increase in number of students and Staff proposed to be on site (only for the DP area) and not use numbers which are campus wide which are outside the realm of consideration for the DP.

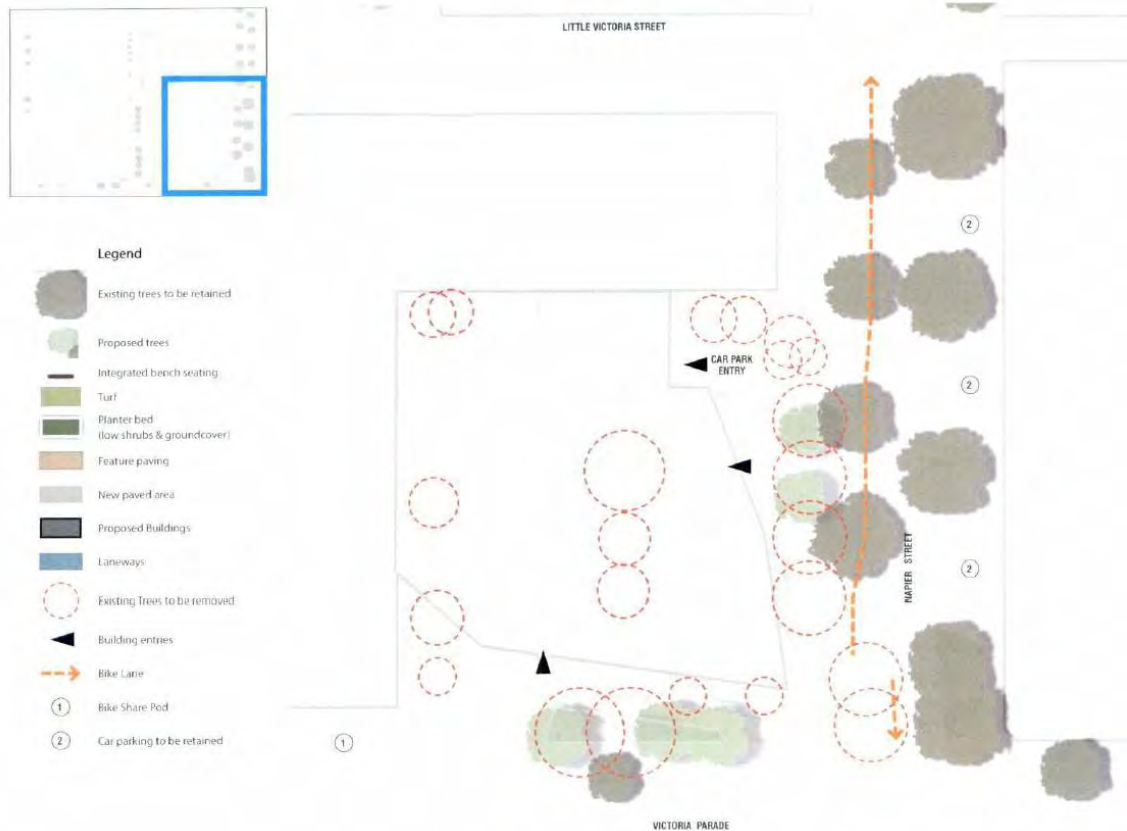
Loss of Trees, landscape and privatisation of public spaces

141. The amended DP as proposed, removes all vegetation from the Victoria Parade site and the removal of two street trees at the intersection of Napier Street and Victoria Parade.

Victoria Parade

142. The site currently has some plantings within the hardstand car park area which is proposed to be removed along with the 2 Council street trees. The landscape plan within the DP details that within the entry forecourts, some new additional low shrubs and trees being planted surrounded by feature paving.

410'S INDICATIVE VICTORIA PARADE ENTRY PLAN

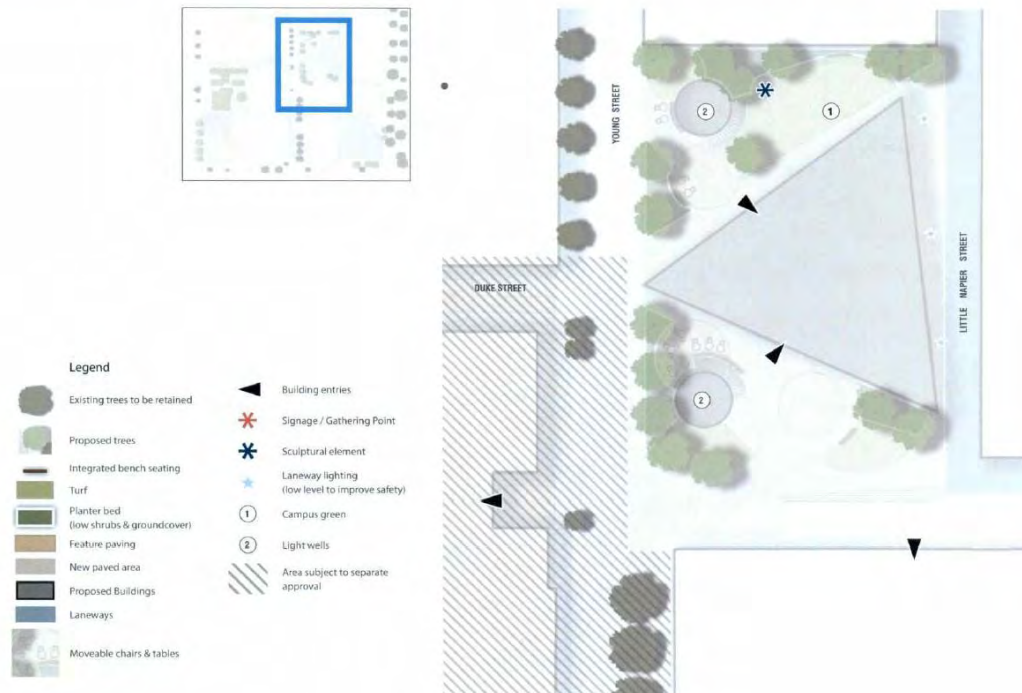


143. Council's Open space unit has suggested retention of some of the existing trees on site namely adjacent to Napier Street. It is understood that this is not possible as they are all located within the construction zone.
144. More detail of the planter beds and consideration for more planting has been suggested to the ACU and this would form part of any planning permit application lodged in the future.

Young Street

145. The Young Street building proposes to increase open space in the area.

4.10.7 ACU FORECOURT / STUDENT HUB DETAIL PLAN



146. The triangular shaped Hub building is proposed to be sited within a landscaped area. Council's Open Space unit supports the proposal subject to details which would accompany a planning permit application.
147. Additional information is required to ensure appropriate lighting is incorporated into the scheme for safety and is appropriately baffled to protect the adjoining residential properties.

Privatisation of public space

148. A number of submissions were made regarding this topic; that is, submitters refer to the blur between the university land and Council land specifically regarding the Young Street building. As outlined in the landscape plan, no works are proposed within Little Napier Street. All lighting would be located with the ACU site.

Net Community Benefit – Balancing objectives

149. As discussed above, the sites location has two distinct contexts which have different purposes being potential growth for the ACU within the Commercial 1 and Mixed Use Zone and limited change areas of the Neighbourhood Residential Area.
150. The tension between these areas requires a balancing of potentially conflicting policy objectives to determine what is an 'acceptable outcome' for the site.
151. In the decision of the Tribunal *The University of Melbourne v Minister for Planning (2011)* the growth of the Melbourne University was being considered relative to the loss of a heritage building. The VCAT decision provides some direction regarding competing tensions within the planning scheme and outlines what is an acceptable outcome and the issue of Net Community benefit.
152. The decision states:

77. The Supreme Court has recently considered the meaning of "acceptable outcomes" within the terms of clause 65 in Rozen v Macedon Ranges SC. After referring to the provisions of what is now Clause 10.04 and clause 65, Osborn J said:

[171] The test of acceptable outcomes stated in the clause is informed by the notions of net community benefit and sustainable development. An outcome may be acceptable despite some negative characteristics. An outcome may be acceptable because on balance it results in net community benefit despite achieving some only of potentially relevant planning objectives and impeding or running contrary to the achievement of others.

[172] The weight to be given to the various considerations which may be relevant on the one hand, and to particular facts bearing on those considerations on the other hand, is not fixed by the planning scheme but is essentially a matter for the decision maker.

[173] Furthermore, the potential complexity of issues raised by a particular application renders the question of what would be the optimal form of development or use in a particular case fundamentally difficult of resolution and one on which different minds might reasonably differ.

[174] In Knox City Council v Tulcan Pty LTD, I observed:

The planning scheme does not require an ideal outcome as a prerequisite to a permit. If it did, very few, if any, permits for development would ever be granted and there would be difficult differences of opinions as to whether the outcomes were in fact ideal. The Tribunal is entitled to grant a permit where it is satisfied that the permit will result in a reasonably acceptable outcome having regard to the matters relevant to its decision under the planning controls.

[175] For these reasons I accept the appellants' submission that the test which the Planning Scheme requires to be applied is one of acceptable and not ideal outcomes.

[176] The question in the present case is whether the Tribunal's reasons read in context demonstrate that it has failed to apply the correct test. The underlying task of the Tribunal is after all to reach the 'correct or preferable' decision on the material before it.

[177] In this sense the preferable outcome is not to be equated with the 'ideal' outcome.'

153. When considering *Net Community Benefit* associated with an education facility such as the ACU, consideration must be given to the broader benefits associated with education and employment and how these would impact on the municipality; however this doesn't mean at the cost of all else.

154. It is considered that the 'on balance' approach established in this case is a reasonable approach and that based on the assessments above, subject to conditions, the proposed Amendment to the DP is an acceptable outcome for the site and should be supported.

Financial Implications

155. The application is proceeding for a declarations hearing at VCAT; substantial costs will be incurred defending Council's position.

Economic Implications

156. The proposal would facilitate an expansion of the ACU and increase the number of students and employees within this area.

Sustainability Implications

157. Sustainability has been considered as part of the assessment of the DP.

Social Implications

158. No particular social implications are known.

Human Rights Implications

159. No Human Rights Implications are known.

Communications with CALD Communities Implications

160. No CALD community implications are known.

Council Plan, Strategy and Policy Implications

161. All relevant policies have been used to assess the application.

Legal Implications

162. Appropriate representation will be engaged to defend Council's position on the proposed DP.

Conclusion

163. The proposed amendment to the DP, subject to changes outlined within the report, is an acceptable outcome which facilitates the future growth of the University while also acknowledging its location and potential off site impacts and seeks to manage those aspects.
164. Subject to the changes outlined within the recommendation section below, Council should advise the Victorian Civil and Administrative Tribunal that Council supports the issue of an amendment Development Plan for the site.

RECOMMENDATION

1. That Council:
- (a) note the report of officers regarding the proposed amendment to the Development Plan for the Australian Catholic University;
 - (b) advise the Victorian Civil and Administrative Tribunal that if Council were in a position to determine the application it would approve the amendment to the Development Plan for the Australian Catholic University at 115 Victoria Parade and 28 – 42 Young Street, Fitzroy subject to the following conditions:

Element	Conditions
Victoria Parade Building	<p><u>New Building</u></p> <ol style="list-style-type: none"> Reduction in height of the proposed new building to have a maximum height of RL 92. Proposed setbacks of level 1 – 3 to be increased to mirror those of levels 4 – 5. Maintain the tripartite (podium, middle, top) composition of the building form as depicted in the DP. Revised façade detail to the central bridging element introducing clear glazing and amended stair treatment. <p><u>Addition to Mary Glowery Building</u></p> <ol style="list-style-type: none"> Setback of the addition to the Mary Glowery Building to Napier Street to be increased from 31.9m to a minimum of 42 metres. The addition to the Mary Glowery building to be set in from the existing external façade of the heritage building. Deletion of the external structural support columns. <p><u>General</u></p> <ol style="list-style-type: none"> Reduction in height and extent of the two plant areas to not be visible from the street and designed to be integrated into the building. Review of signage extent and presentation to be less intrusive to the heritage context.
Young Street Building	<ol style="list-style-type: none"> Retention of existing northern on boundary wall associated with the car park structure adjacent to 44 Young Street. Eastern wall of the Young Street Hub to either comply with the B17 ResCode height envelope or alternatively, be no higher than the existing conditions with any higher structure to be setback to limit off site amenity impacts.

	12. Reduction in height and extent of the plant area to not be visible from the street and designed to be integrated into the building.
Cars and Vehicular Access	<p>13. Redesign to either raise the car park and bike lane and or convert area to a 'priority crossing' with a give way sign.</p> <p>14. Inclusion of a sight triangle for the exit lane from the car park.</p> <p>15. Deletion of proposed use of 'concrete semi-mountable kerb and replacement with Council's bluestone semi-mountable kerb.</p> <p>16. Swept path diagrams for car existing critical spaces on the east side of Napier Street due to the island and separator.</p> <p>17. Additional flared island at the northern side of the car park entrance.</p> <p>18. Continuation of linemarking or a raised median island to the south of the car park entrance.</p> <p>19. Additional one-way arrows within the traffic lane.</p> <p>20. Installation of a 'No Through Road' sign at Victoria Parade.</p> <p>21. Dynamic parking vacancy indicator sign facing Victoria Parade.</p> <p>22. Swept paths to show that vehicles can't enter/exit the car park access via the northern section of Napier Street.</p> <p>23. A road safety audit and compliance with all its recommendations.</p> <p>24. A minimum of 12 disabled car parking spaces accessible by staff and students.</p> <p>25. A car parking study to inform the number of car parking spaces required on site with the aim of further reducing numbers.</p> <p>26. Retention of the original 'Batemans Patent Melbourne Waterworks grate' in Napier Street.</p>
Sustainable Transport	<p>27. A minimum of 80 new bike spaces to be provided with the DP area.</p> <p>28. Details and location of the new Melbourne Bike Share facility along Victoria Parade or Napier Street within the University frontage.</p> <p>29. A Green Travel Plan for the University examining implementation of more sustainable transport measures.</p>
Landscaping	30. Additional information regarding lighting of the Hub landscape areas to ensure pedestrian safety while controlling light spill to adjoining residential areas.
Use	31. Accurate information applicable only to the DP area regarding increased student and staff numbers reflecting the existing figures being 2,125 full time students and 180 staff.
Other	32. Any other changes as a consequence of the above.

CONTACT OFFICER: Mary Osman
TITLE: Manager Statutory Planning
TEL: 9205 5300

Attachments

- 1** ACU Development Plan - Cover letter Urbis
- 2** ACU Development Plan - Part 1(2)
- 3** ACU Development Plan - Part 2
- 4** ACU Development Plan - Part 3
- 5** ACU Development Plan - Part 4
- 6** ACU Development Plan - Part 5
- 7** ACU Development Plan - Heritage
- 8** ACU Development Plan - Traffic Part 1
- 9** ACU Development Plan - Traffic Part 2
- 10** ACU Development Plan - Traffic Part 3
- 11** ACU Development Plan - Wind
- 12** ACU Development Plan - Landscape
- 13** ACU Development Plan - Massing Comparison
- 14** ACU Development Plan - Applicant response to Referrals
- 15** ACU Development Plan - Applicant response to Objections
- 16** ACU Development Plan - Referral Memo - Open Space, Streetscapes and Arboriculture
- 17** ACU Development Plan - Proposed Traffic Management in Napier Street Fitzroy
- 18** ACU Development Plan - Engineering Comments
- 19** ACU Development Plan - Strategic Transport Planning Memo
- 20** ACU Development Plan - Heritage Advice
- 21** ACU Development Plan - Urban Design Assessment Memo

Attachment 1 - ACU - Development Plan - Cover letter Urbis



**LEVEL 12
120 COLLINS STREET
MELBOURNE VIC 3000**

URBIS.COM.AU
Urbis Pty Ltd
ABN 50 105 256 228

28 October 2016

Ms Mary Osman
Manager - Statutory Planning
Yarra City Council
333 Bridge Road, Richmond VIC 3121

Dear Mary

ACU DEVELOPMENT PLAN - AMENDMENTS FOLLOWING COMMUNITY CONSULTATION

Further to the community consultation held on 30 August, review of referral comments and our recent discussions, we are pleased to submit the amended plan response for the amended Australian Catholic University (ACU) Development Plan.

Following detailed consideration by ACU with their project team, ACU has made amendments, including a reduction in above ground floor area of approximately 2300 sq m (across both 115B Victoria and Young Street), to address community and Council feedback.

SUMMARY OF KEY CHANGES

The following provides a summary the key changes in response to feedback. A full list of response in response to both referrals and objections is provided in the accompanying documents.

115B Victoria Building

- Reduced building height to 12 storey plus plant (previously 14 storey plus plant)
- Increased setbacks to Napier Street. The setback have increased as follows:
 - Minimum setback of 17.1m tapering to 31.9m at Level 6 and 7 (previously 3.7m – 9.8m at Level 6 and 8.2m – 14.3m at Level 7)
 - Consistent 15.3m setback for Levels 8 - 11 (previously 8.2m – 14.3m at Level 8 and 9 and 11.82m to 17.85m at Level 10 – 13 noting that Level 12 and 13 have been deleted).
- Amended design in response to referral comments including reduced podium height in relation to the Mary Glowrey building, details of materials showing a clear distinction between original and new building forms, and design detail to demonstrate the high architectural quality of the building.

Napier Street Design

- Retention of all on street car parking on the east side of Napier Street (previous 6 permit zone spaces proposed to be removed)

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Attachment 1 - ACU - Development Plan - Cover letter Urbis



- Tree removal reduced to only 2 trees located in the garden bed near the corner of Napier and Victoria Parade (previously 7 trees to be removed). This has been achieved by the relocation of the vehicle crossover to the 115B building and by an amended Napier Street design. It is proposed to replace these trees with 5 new trees on the 115B site.
- Amended Napier Street design – which provides an improved design with bicycle priority and prevents vehicles from travelling north past ACU into the residential area of Napier Street.

Young Street Building

- Amended design (triangular building) with larger public open space area (increase from 635 sq m to 1205 sq m)
- Removal of the proposed closure of Young Street
- Removal of the proposed air bridges over Young Street

SUPPORTING DOCUMENTS

To support the submission please find enclosed the following documents:

- Three copies of the amended ACU St Patricks Campus Fitzroy Development Plan October 2016
- Two A4 copies of the amended Integrated Transport and Access Plan by Cardno dated October 2016
- Amended Heritage Assessment by Bryce Raworth dated 25 October 2016
- Arborist Report by Simon Howe of John Patrick Landscape Architects
- Wind Assessment Report by Aecom
- Massing comparison set by Lyons highlighting the changes made to the building envelopes for both 115B and Young Street buildings.
- Referral response table by Urbis outlining the document reference and response to each item.
- Objection response table by Urbis outlining a response to the key objections
- USB containing the above documents.

We trust the above and enclosed information is satisfactory and we look forward to the material being circulated to the objectors prior to proceeding to an IDAC meeting toward the end of the year.

If you have any questions please don't hesitate to contact me on 8663 4823.

Yours sincerely,

A handwritten signature in dark ink, appearing to be "A. Barclay", written over a faint, stylized graphic element.


Anna Barclay
Associate Director

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Attachment 2 - ACU Development Plan - Part 1(2)



Attachment 2 - ACU Development Plan - Part 1(2)

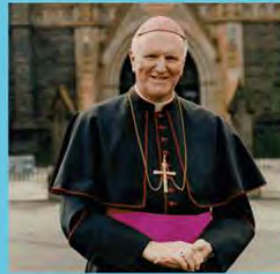
		
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Attachment 2 - ACU Development Plan - Part 1(2)



TESTIMONIALS



Message from
the Archbishop
Denis Hart

It is with great pleasure that I introduce the ACU Development Plan. It is my privilege as Archbishop of Melbourne and President of ACU Limited to have the opportunity to reflect on what has been achieved by the Vice-Chancellor Professor Greg Craven, and the University staff under the wise guidance of the Chancellors General Sir Peter Cosgrove AK, MC, and the Hon. John Fahey AC with the support and encouragement of the Archdiocese of Melbourne at the Australian Catholic University's St Patrick Campus.

It is a particular source of pride for me with St Patrick's Campus located nearby to St Patrick's Cathedral and the administration of the Archdiocese to observe its development and witness the active engagement of the University with the Church and community in Melbourne. I welcome the opportunity for your comments and input to the further development of the Campus to further enhance this growing educational environment in Fitzroy.

Integral, as indeed primary for this whole activity, is the work done for the welfare of the students. We hope to provide our students with areas which will open out and provide the space for recreation, for the important dialogues which are part of University life, for friendship and for mutual understanding. The further expansion of the Melbourne Campus of the University I hope, will be a long-standing inspiration to the young people who come here, to look at the possibilities that we can all do for other people and to carry the work of the University further into the community, so that the contribution of all made here will be something which is remarkably authentic, human, guided by the respect for the dignity of the human person with the unique gifts and abilities of each young person placed at the service of those around them.

- Archbishop Denis Hart



Message
from the
Vice-Chancellor
Professor Greg Craven

It is with great pleasure that I introduce the ACU Development Plan. Australian Catholic University (ACU) has developed and grown substantially over the last five years and its strategic positioning has now entered a new phase. ACU has transitioned from a University striving towards ambitious targets to one that is comprehensively embedding excellence.

The University is ambitious in achieving its goals and vision whilst at the same time is clear about its Catholic Identity and Mission. The University continues a legacy that spans over 2,000 years of the Catholic intellectual tradition's engagement in faith and reason. The ACU Development Plan will ensure we continue to work together towards the shared goals of academic excellence and as a provider of innovative and quality higher education courses; delivered by leading academics, to meet the needs of a diverse student body in conjunction with our partners and the community.

I look forward to working with the ACU community and our stakeholders to achieve these goals through the next phase of the University's development.

- Professor Greg Craven - Vice-Chancellor

Attachment 2 - ACU Development Plan - Part 1(2)



1.0 INTRODUCTION

1.1 ACU Fitzroy

This Development Plan has been prepared to guide future planning permit applications for development at St Patrick's Campus of the Australian Catholic University (ACU) in Fitzroy.

The main campus is located in the block bounded by Brunswick Street, Napier Street, Victoria Parade and Gertrude Street. The properties owned by ACU within this block are shown on the aerial image.

This Development Plan applies to the ACU land within the Development Plan Overlay (DPO2) in the Yarra Planning Scheme.

Other areas of the campus will be subject to a separate planning approvals process.



Attachment 2 - ACU Development Plan - Part 1(2)



1.2 Aims of the Development Plan

ACU's Development Plan is to deliver a landmark mixed use development at the St Patrick's Campus in Fitzroy, Melbourne to accommodate the planned growth in students, teachers and research activities, improve the quality of teaching and learning environments and improve the pedestrian and public environment for students and the wider community.

The existing Australian Catholic University Development Plan was approved over 16 years ago (in 1999) and the works proposed under this Development Plan have since been completed.

This Development Plan replaces the 1999 Development with a contemporary document that formalises the strategic direction of the campus's next phase of development over the coming decade.

1.3 Background of ACU and St Patrick's Campus

The Melbourne Campus at Fitzroy, incorporating Cathedral Hall, was established in 2000 after the ACU's existing two Melbourne campuses (Mercy and Christ) were formally amalgamated. Mercy and Christ had both been colleges of the Victorian Institute of Catholic Education between 1975 and 1990.

At the heart of the Fitzroy neighbourhood is the historic Cathedral Hall, at 20 Brunswick Street, near the Victoria Parade corner. Connected to the Hall is the former Exhibition Boot Factory which, having provided the Hall with a foyer, cloakroom, ticket office and club rooms, was refurbished in the early 2000s.

Built in 1873, the Boot Factory turned out footwear for the Exhibition Boot Company for nearly 30 years before it was bought by the then Archbishop of Melbourne, Dr Thomas Joseph Carr in 1903.

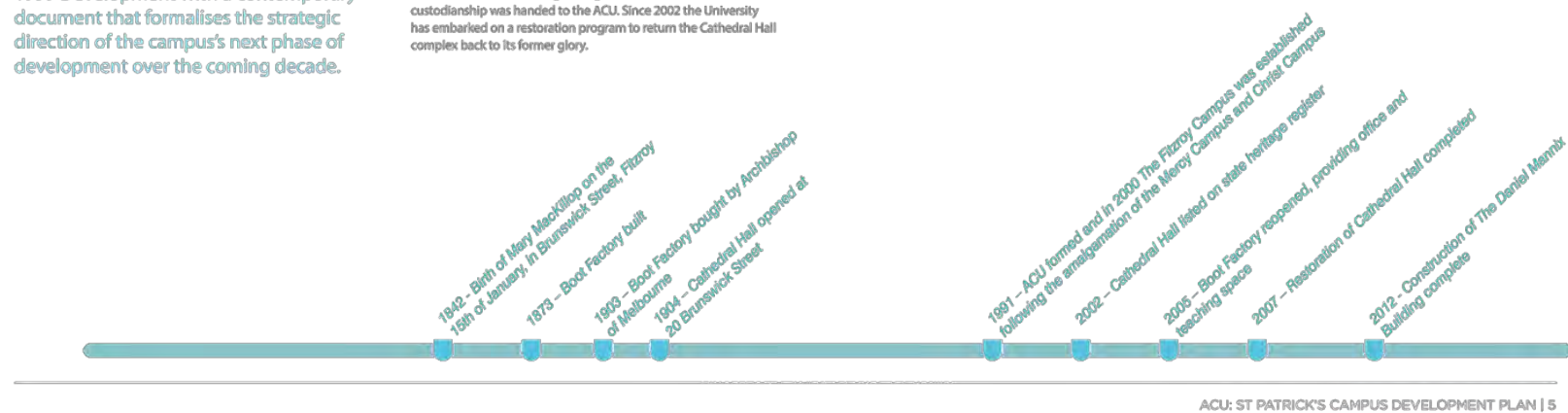
Archbishop Carr set about refurbishing the Boot Factory to provide the Catholic community with its own class, club and meeting rooms. The facility was opened in 1903 when Archbishop Carr also bought two adjacent properties and it is here where Cathedral Hall was built and opened in 1904, followed by a supper room, opened in 1908. Since then, this complex of buildings has served the local Catholic and wider community as the venue for congresses, concerts, lectures, meetings and socials, and provided nearby Catholic schools with an assembly hall.

Cathedral Hall was showing real signs of wear and tear when custodianship was handed to the ACU. Since 2002 the University has embarked on a restoration program to return the Cathedral Hall complex back to its former glory.

The Supper Room was the first part of the Cathedral Hall complex to be restored, with a contribution to the project from the National Institute of Youth Performing Arts. A fully restored and renovated Boot Factory, providing office and teaching space, was reopened in November 2005. In October 2007, following its restoration, Cathedral Hall was reopened. This was made possible through generous contributions from the Ian Potter Foundation and the Archdiocese of Melbourne. The Hall now provides space for lectures, with music and drama students using the Supper Room. The complex provides a focal point for the intellectual and cultural life of the University.



St Mary of the Cross, Australian Catholic University,
St Patrick's Campus, Fitzroy



Attachment 2 - ACU Development Plan - Part 1(2)



1.3 Background of ACU and St Patrick's Campus

The University chooses to focus on areas of higher education and research that are closely connected with its particular character as a University that is Catholic, public and national. The focus areas are Education and Arts, Health Sciences, Law and Business and Theology and Philosophy.

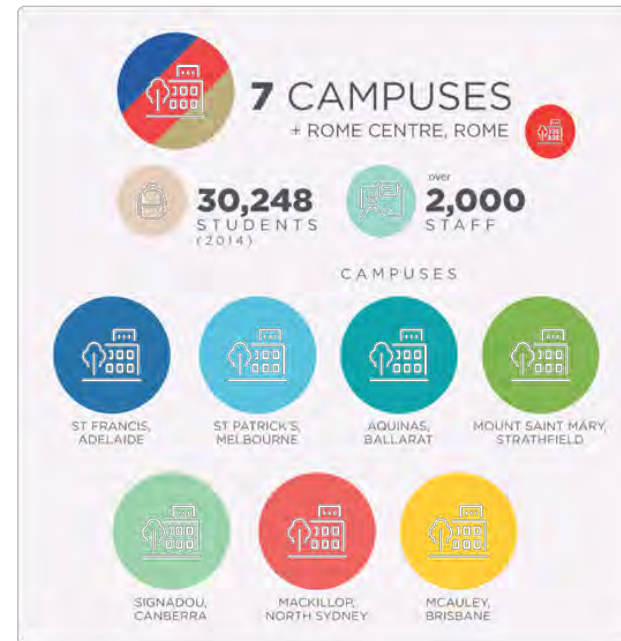
The University has the following Research Institutes, several of these have substantial teams based in Melbourne:

- Institute for Religious and Critical Inquiry (Melbourne)
- Learning Sciences Institute Australia (Melbourne)
- Mary MacKillop Institute for Health Research (Melbourne)
- Institute for Positive Psychology and Education
- Institute for Social Justice
- Institute for Health and Ageing (Melbourne)
- Institute for Religion, Politics, and Society (Melbourne)

As a Catholic University, the ACU is grounded in the Catholic Intellectual tradition, affirming the value of rational inquiry and the human search for meaning, the combination of faith and reason. Knowledge is cultivated in an environment that fosters intellectual freedom, personal development and equity for all. The ACU's Catholic identity is animated through a commitment to serving others, developing an active learning community and fostering a just society.

ACU has a long tradition of nurturing the intellectual and spiritual life of students and staff. This is grounded in a commitment to caring for the whole person – mind, body and spirit, to support staff and students integrate learning, work, faith and community engagement.

ACU's Institute for Advancing Community Engagement (IACE), facilitates University-community partnerships that respects the wellbeing and dignity of people and through collaboration, seeks to enhance wellbeing of individuals and their communities locally, nationally and internationally. Australian Catholic University's (ACU) community engagement capacity has grown through its strategic partnerships with more than 170 organisations, including Catholic and other faith-based community groups and schools, as well as professions and corporations. Respect for each person, appreciation of the sacred in life and commitment to the common good are core values within the ACU's community engagement, which draws upon the capacities and scholarship of staff and students in its collaboration with people and communities here and overseas.



FACULTIES, INSTITUTES AND CENTRES

Australian Catholic University (ACU) has a wide range of institutes and centres as well as four faculties:

<p>Education and Arts - areas include education, arts and humanities, global studies and international development studies, media communications, social science, youth work, and creative arts, visual arts and design.</p>	<p>Health Sciences - areas include counselling, environmental science, exercise physiology, exercise science, mental health, midwifery, nursing, occupational therapy, paramedicine, physiotherapy, public health, psychology, social work and speech pathology.</p>	<p>Law and Business - areas include commerce, business administration, accounting and finance, information technology, human resource management, law, marketing, supply chain management, occupational health and safety, and sustainability.</p>	<p>Theology and Philosophy - both these disciplines encourage students to search for wisdom, knowledge and truth.</p>
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Attachment 2 - ACU Development Plan - Part 1(2)



1.4 The Archdiocese, ACU and the Memorandum of Understanding

The Roman Catholic Archdiocese of Melbourne (Archdiocese), together with others, formed the ACU to establish and conduct the Australian Catholic University. In its conduct of the University, the ACU shares common objectives with the Archdiocese. The Archdiocese conducts many Church and other activities from St Patrick's Cathedral and other properties in the vicinity of the St Patrick's Campus.

The Archdiocese and ACU have entered into a Memorandum of Understanding (MoU) to identify and meet the common property arrangements in and around the St Patrick's Campus. As part of this MoU, the parties agreed to develop and adopt a master plan and design solution of Australian excellence for the land on and adjacent to Victoria Parade that provides an appropriate balance of built form and open space on the St Patrick's Campus.

The purpose of developing the Master Plan is to clearly communicate the plan for future sustainable development and provide long term certainty for all in relation to proposed property usage, infrastructure development and operational matters.

Recognising the value of the relationship between the ACU and the City of Yarra, the two parties have agreed to:

- strengthen the relationship between the Council and the ACU to ensure there is a high-level of strategic co-ordination between the two organisations
- facilitate the development and management of joint projects which realise broad community benefits
- collaborate to promote key initiatives undertaken by the ACU and the City of Yarra.

A key focus area of the agreement includes broad objectives for Place Making and Urban Design for the St Patrick's Campus. These objectives are to:

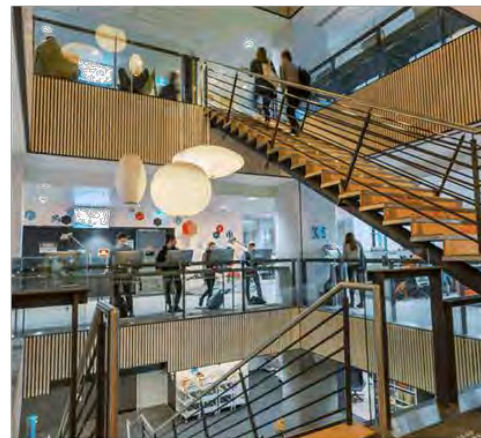
- collaborate on placemaking initiatives in the Campus by ensuring that preliminary consultations between Council and the ACU take place at the conceptual stage concerning the ACU's capital works programs
- ensure that there are best outcomes for the broader community through the design of urban spaces and the interface between ACU and Council land
- ensuring the integrity of the site's heritage features are maintained.



Australian Catholic University, St Patrick's Campus, 113 & 117 Victoria Parade, Fitzroy



Australian Catholic University, St Patrick's Campus - St. Mary of the Cross, monument



Students at Australian Catholic University, St Patrick's Campus



Students at Australian Catholic University, St Patrick's Campus

Attachment 2 - ACU Development Plan - Part 1(2)



1.5 ACU Vision, Expansion and Consolidation Plans

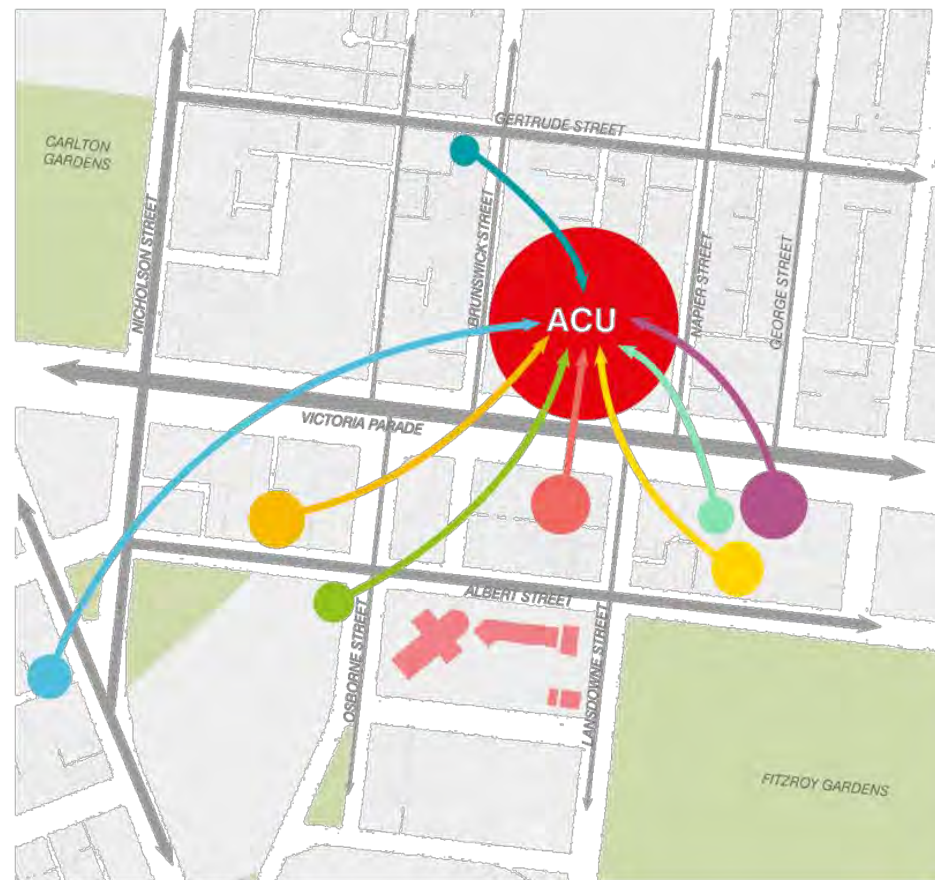
The ACU provides a diverse range of subjects including Accounting and Finance, Commerce, Business, Marketing, IT, Law, Arts, Education and Teaching, Theology, Exercise Science and Allied Health, Philosophy, Paramedicine, Psychology, Nursing, Midwifery and Science. 10,000 students and approximately 800 staff attend the campus throughout the academic year. Since the ACU was formed on 1 January 1991, it has seen continued growth in the demand for its tertiary education student placements.

St Patrick's Campus will need to grow to accommodate approximately 12,900 students and 1,200 staff by 2020. The growth of the research functions will also continue in response to emerging opportunities.

The Development Plan has been developed to accommodate the ACU's required growth in students, teaching staff and research from 2015 to 2025. It has been developed to enable the ACU to further consolidate its teaching and research activities onto the Campus.

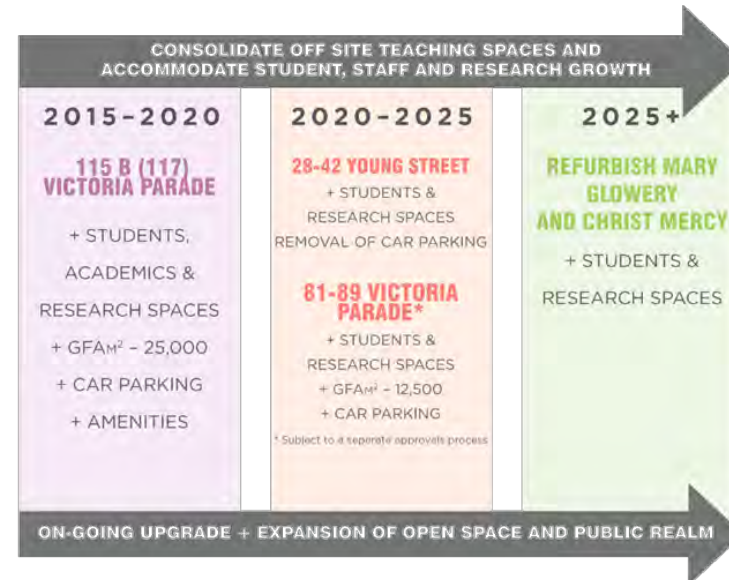
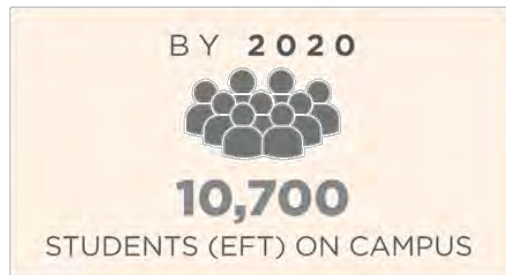
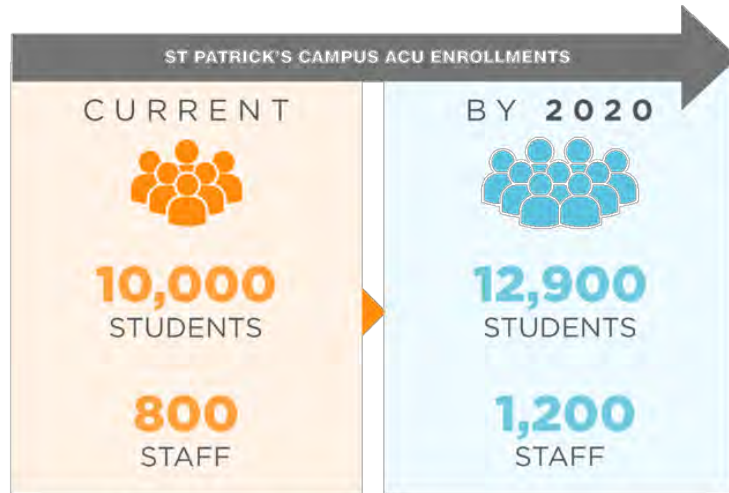
To enable the expected growth to be accommodated and campus consolidation objectives to be achieved, additional floor area will be required on the main campus.

The majority of new floor area will be located at the Victoria Parade frontage of the site which is the more robust interface, away from residential properties further north. New buildings will be constructed at 115B Victoria Parade (corner of Napier Street) and 81-89 Victoria Parade (corner of Brunswick Street) – not part of the Development Plan Overlay. The Mary Glowrey building will also be refurbished and extended.



Intended campus consolidation

Attachment 2 - ACU Development Plan - Part 1(2)



The campus generally operates from 8am to 11pm for staff and students, with the library facility accessible 24 hours. There are also some conference areas that run for extended hours to allow for international phone conferences and the like.

The tertiary sector in Victoria is expected to grow by 2.9% and based on these projections ACU anticipates 12,900 enrolments. This would equate to 10,700 Equivalent Full Time (EFT) students across the Melbourne Campus. Approximately 93% of these students are full time and 7% are part time.

The anticipated growth in the Development Plan Overlay area is included in this campus total and the new floor area is largely to accommodate teaching and staff spaces required as a result of campus consolidation.

Attachment 2 - ACU Development Plan - Part 1(2)



2.0 URBAN CONTEXT AND SITE

2.1 Regional and Strategic Context

Within the wider strategic document, Plan Melbourne, it is recognised that Melbourne is home to a range of world ranked universities and that the knowledge based sector is fundamental to the city's ability to compete within the global economy and provide a quality skills base for future growth and prosperity.

2.2 Capital City Context and the Future Growth of Melbourne

Population growth and the changing economy are increasing the importance of cities and changing the way they function. Cities will continue to grow and shoulder a higher proportion of population growth. As cradles of innovation and creativity they are the major wealth generators and centres of opportunity, attracting people, business and investment from around the world.

Melbourne is growing rapidly, with a projected population of up to 7.7 million by 2051. Over the same period an additional 1.7 million jobs are forecast. A large percentage of this growth is to be accommodated in the expanded central city.

The 'Knowledge Economy' has been identified as a crucial economic development goal for Australian cities and specifically, Melbourne. Knowledge based industries typically cluster in defined locations, indicating there will be continued demand for significant growth in the central city, including the internationally recognised hospital, health and medical research precincts on Victoria Parade. In June 2014, the

Education and Training and Health Care Industries contributed \$19 billion (5.5%) and \$24 billion (6.8%) per annum, respectively, to Victoria's Gross State Product.

Tertiary education is vital to the Knowledge Economy and the future development of our young people. Higher education is a key driver of research and development, the provider of a skilled and educated workforce and a major contributor to the city's liveability and socio-cultural fabric. The Australian Catholic University is an important player and ideally placed for its ongoing contribution to the Victorian education sector and the knowledge economy.

2.3 The Expanded Central City

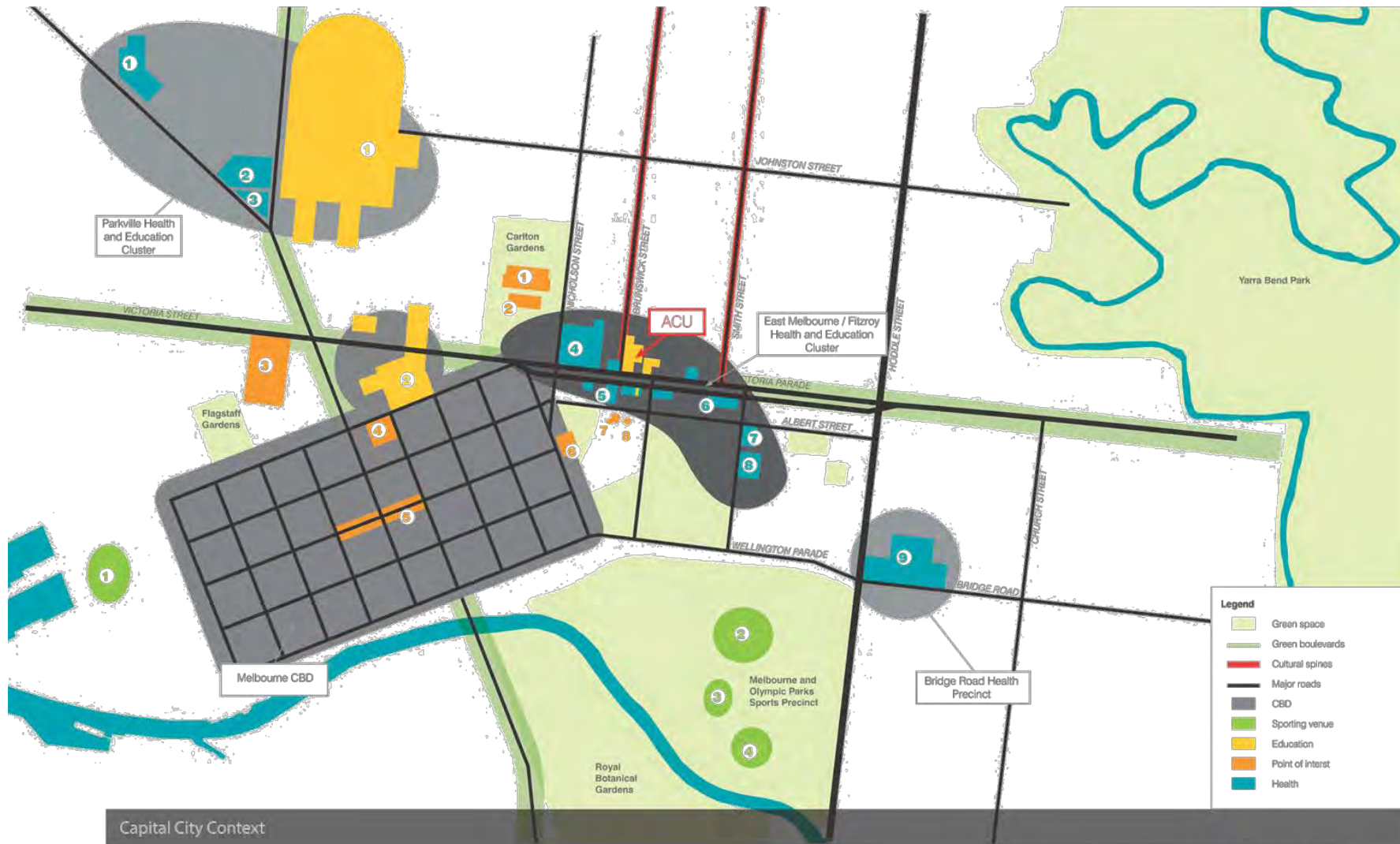
The City of Yarra is part of the expanded central city. The expanded central city is forecast to become Australia's largest business centre with an additional 260,000 - 320,000 jobs to create a total of 1 million jobs by 2031.

The Fitzroy/East Melbourne Health and Education precinct is a significant State cluster of health and education services. It includes the ACU, St Vincent's Public and Private Hospitals, The Eye and Ear Hospital, Epworth Freemason's Hospital and Medical Centre on Victoria Parade, Epworth Freemasons Hospital on Albert Street and the St Vincent's Private Hospital on Grey Street.

Nearby, Collingwood and the North Richmond to Victoria Park Station corridor adjacent to Hoddle Street are identified as Urban Renewal opportunities to accommodate new residential and commercial developments and large scale public works projects to make better use of underutilised land that is well located close to jobs, services and transport.

Legend	
1	Royal Children's Hospital
2	Royal Melbourne Hospital
3	Victorian Comprehensive Cancer Centre
4	St Vincent's Private Hospital
5	Royal Victorian Eye and Ear Hospital
6	Epworth Freemason's Maternity Centre
7	Epworth Freemason's Hospital
8	St Vincent's Hospital
9	Epworth Health Care, Richmond
1	University of Melbourne
2	RMIT University
1	Melbourne Museum
2	Royal Exhibition Building
3	Victoria Market
4	Melbourne Central
5	Bourke Street Mall
6	Parliament House
7	St Patrick's Cathedral
8	Roman Catholic Archdiocese of Melbourne
1	Etihad Stadium
2	MCG
3	Rod Laver Arena
4	AAMI stadium

Attachment 2 - ACU Development Plan - Part 1(2)



ACU: ST PATRICK'S CAMPUS DEVELOPMENT PLAN | 11

Attachment 2 - ACU Development Plan - Part 1(2)

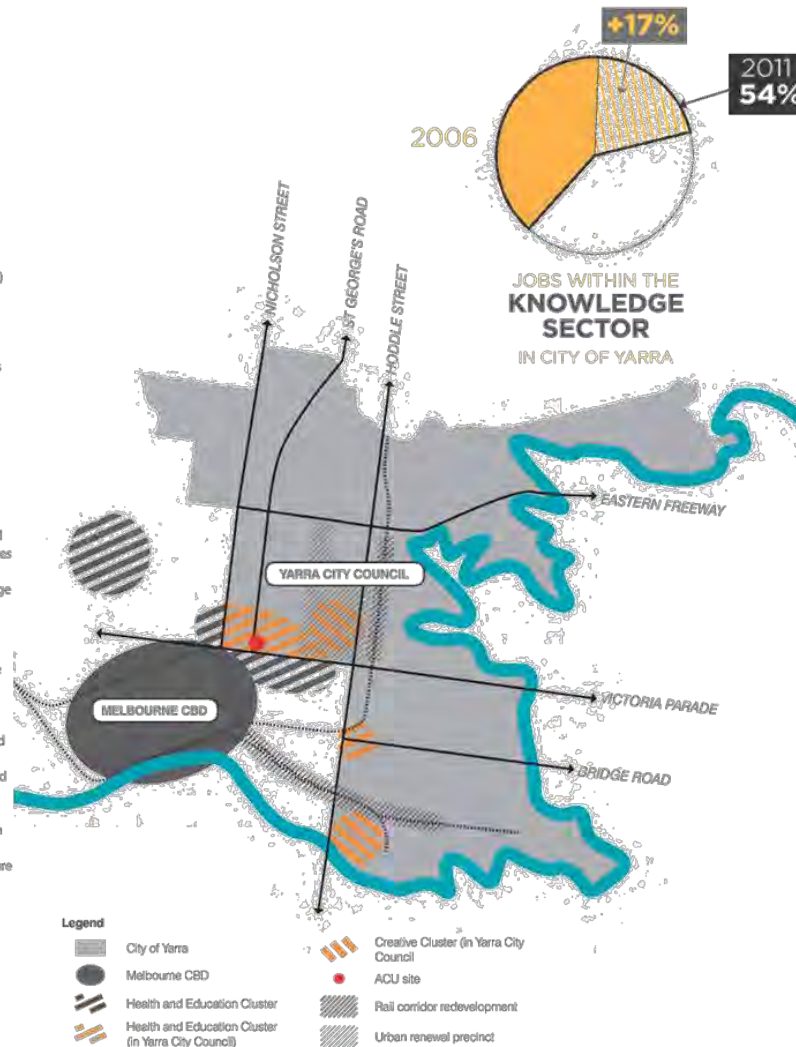


2.4 ACU'S Contribution To The City Of Yarra

The City of Yarra's Economic Strategy 2015-2020 (the Strategy) reinforces the importance of the knowledge economy to the City. In 2011, 30,950 or 46% of workers in the City of Yarra were employed in the Knowledge Sector. Between 2006 and 2011, knowledge sector jobs grew by 17%, or 5,279 jobs, representing 54% of employment growth across all industries over this period.

The Strategy identifies the importance of the 'smart city' concept to the local economy. The 'smart city' concept recognises and supports the growth of urban areas where the key drivers of innovation, productivity and growth in the knowledge economy occur. Typically they are areas where research institutions, medical complexes, universities and clusters of high-tech and creative firms are located and attract the co-location of other businesses, entrepreneurs and professionals. Mapping businesses in the City of Yarra illustrates a number of distinct geographical clusters where businesses and institutions have clustered to provide economic advantage from collocation and collaboration. These include the Fitzroy Health and Education Cluster and the nearby Collingwood Creative Cluster.

In 2011, the Fitzroy Health and Education Cluster featured the greatest concentration of employment in the municipality with over 5000 employees. The area functions as a significant research and medical education cluster. Melbourne's growing and ageing population is expected to drive increased demand for these services in the future. Consultation with St Vincent's by the City of Yarra has indicated that major growth is required over the next 5 years. It is anticipated there will be significant flow on effects to surrounding businesses and opportunities for co-location of other activities, particularly in the education and research sectors. The City of Yarra recognises that these activities have the potential to dramatically influence the future urban and economic growth of the municipality.



In recognition of these important drivers of change, the ACU and City of Yarra have entered into a Memorandum of Understanding (MOU) which identifies areas for collaborative work. This includes:

Place Making and Urban Design

- collaborate on placemaking initiatives in the Education Precinct by ensuring that preliminary consultations between Council and the ACU take place at the conceptual stage concerning the ACU capital works program
- ensure that there are best outcomes for the broader community through the design of urban spaces and the interface between the ACU and Council land
- ensure that the integrity of the site's heritage features are maintained.

Education and Professional Development

- provide opportunities for the training and professional development of the employees of the Council by the ACU
- provide opportunities for relevant employees of the Council to assist as guest lecturers/tutors at the ACU
- collaborate in relation to research and learning opportunities that might arise from time to time, including research into relevant portfolio area.

Health and Wellbeing

- improve the health and well-being of the Yarra community through creating a stronger relationship between the ACU's public health expertise and the work of Council.

Economic Development

- work together to expand the benefit of the ACU's activities to a wider local community.

Research

- develop cooperative research projects across a range of portfolio areas which harness the strong research expertise of the ACU in a way that assists Council to improve its evidence base and advocate for better social outcomes.

Yarra City Council Clusters

Attachment 2 - ACU Development Plan - Part 1(2)



2.5 Local Context

The ACU is strategically located on Victoria Parade, one of the Melbourne's most important entrance boulevards to the CBD, alongside St Kilda Road and Royal Parade. Melbourne's entrance Boulevards are very distinctive features of the city's urban fabric. Characterised by a number of large institutional and commercial buildings, Victoria Parade is a very wide, generous, tree lined space where relatively tall buildings are both suitable and can be accommodated given the width of the road, the size of many of the lots and the dominance of the Boulevard's tree canopy.

The Campus sits at the north east entrance of the Melbourne CBD and the northern entrance to the Brunswick Street Activity Centre. The local area includes the Gertrude Street shopping precinct and the Smith Street Activity Centre.

The ACU is in walking distance or a 5 minute tram ride to the CBD and Parliament Station. It sits within the expanded Central City zone which will continue to grow to accommodate health, education and knowledge intensive activities. A number of prominent landmarks can be seen from or are in walking access from the campus including, St Patrick's Cathedral, the Carlton Gardens, Royal Exhibition Buildings, the Melbourne Museum, Parliament House and the Fitzroy Gardens.

Victoria Parade is the central spine of the Fitzroy/East Melbourne Health and Education Precinct. It accommodates a cluster of capital city health and knowledge economy activities including the Epworth Freemason's Hospital, the Eye and Ear Hospital and St Vincent's Public and Private Hospitals and the Catholic Theological College. There are also a number of large tenancies along the Boulevard accommodating a mixed use of medical, government and business services.

The geographic location of the campus provides a landmark opportunity to signify both the location of the St Patrick's Campus and a key component of the health and education cluster located along the length of Victoria Parade with higher buildings clustered at the western end of Victoria Parade including the Eye and Ear Hospital and St Vincent's Public and Private Hospitals.

Victoria Parade contains a number of sites that can accommodate significant growth, including the ACU St Patrick's Campus. The ACU Campus is ideally located to accommodate growth given:

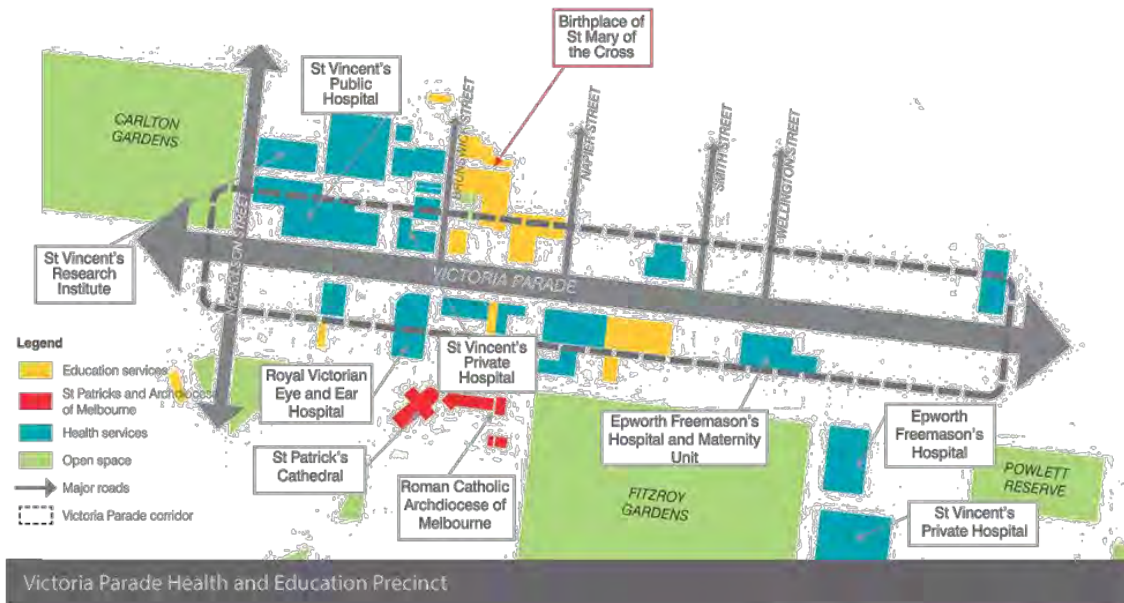
- the relatively large lot size
- the sites location on a major civic boulevard which provides the opportunity to accommodate greater scale and height similar to St Kilda Road
- the scale and orientation of the ACU Campus which is to the south of the fine grain residential precincts in Fitzroy, meaning any overshadowing from high scale development will be minimal

- the size of the Campus provides the opportunity for higher scale development to be located on the Victoria Parade frontage with height transitioning down to the north to assist in minimising the impact of bulk on the sensitive residential uses to the north of the Campus

As identified in the Yarra City Council Economic Strategy 2015-2020, the Australian Catholic University and St Vincent's Hospital's need to accommodate major growth over the next 5 years given increased demand for Education, Medical and Research activities and to capitalise on the productivity and job growth which will drive development investment within the

education and health precincts in both the City of Melbourne and the City of Yarra. Growth of jobs in these sectors is of State importance.

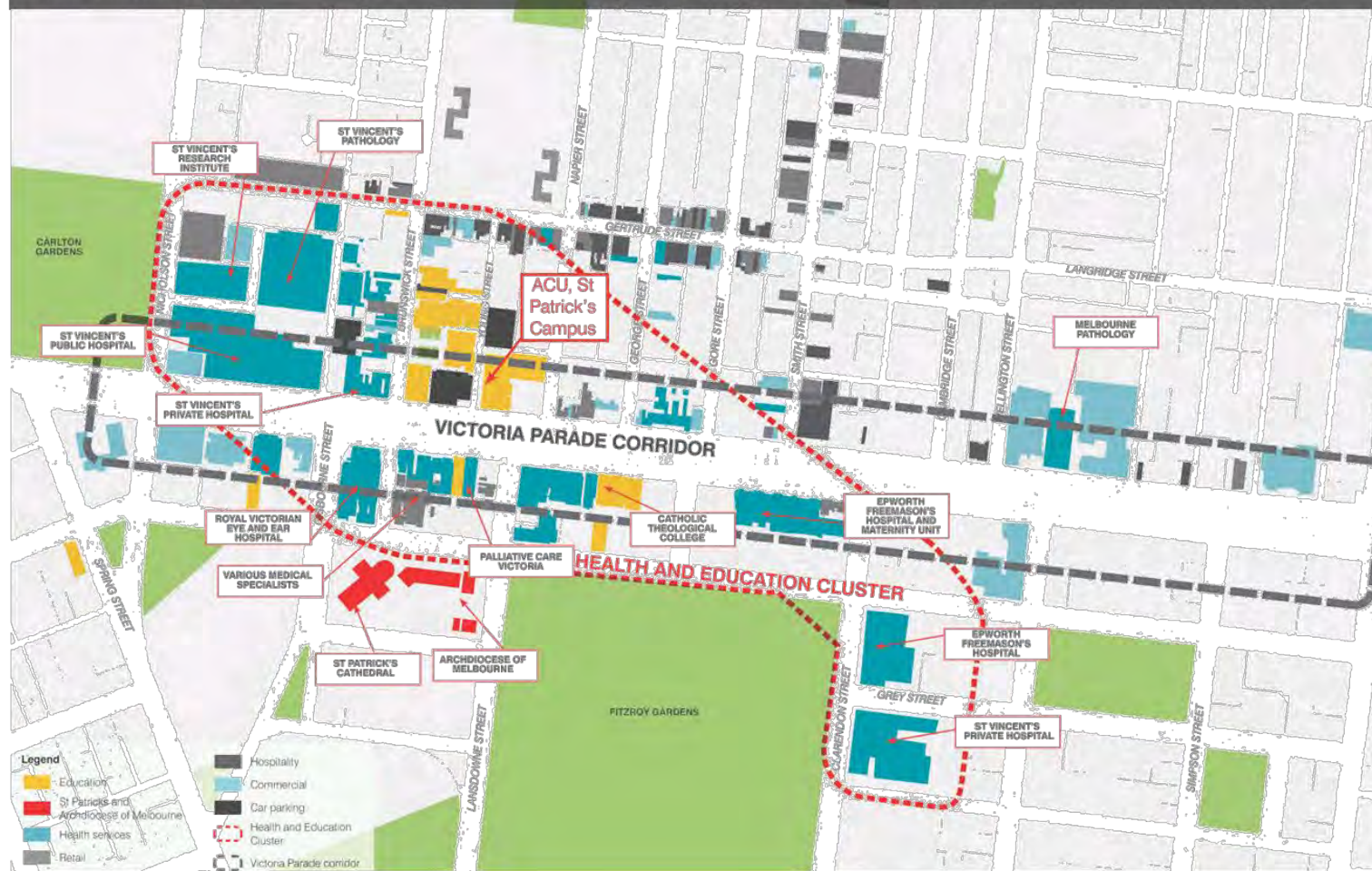
To the north, east and west of the site, the Campus abuts a mix of development types including larger lots accommodating St Vincent's Hospital and a mix of one, two and three storey finer grain terraces and warehouse conversions. To the north of Little Victoria Street, Napier Street comprises primarily one and two storey heritage terraces with future development managed by the South Fitzroy Heritage Overlay. The interface to the south and east of the Campus is particularly sensitive given the intact nature of much of the Napier Street heritage properties.



Attachment 2 - ACU Development Plan - Part 1(2)



FIGURE 1.1. Local Context



Attachment 2 - ACU Development Plan - Part 1(2)



2.6 Location and Layout

The ACU St Patrick's Campus comprises a number of historic and contemporary buildings, the Campus has the following buildings and facilities which are mapped on the adjacent plan, noting that the car park (No. 6) is not part of the ACU campus:

- ① The Mary Glowery Building (115 Victoria Parade): Main teaching space, reception, library, student facilities, the Office of Student Success, specialist laboratories for Speech Pathology, Biology, Occupational Therapy, Media Labs, gymnasium and Moot Court (5 storeys)
- ② The Daniel Mannix Building: Philippa Brazil rsm Lecture Theatre, rooftop garden, St. Mary of the Cross Chapel, Nursing, Paramedics, Exercise Science, Midwifery, Occupational Therapy and Psychology, specialist laboratories. (7 storeys + 2 plant)
- ③ Cathedral Hall: Auditorium and tutorial rooms (3 storeys)
- ④ Modular Buildings (81-89 Victoria Parade) (2 storeys): Tutorial rooms
- ⑤ Young Street multi-level carpark (4 storeys)
- ⑥ Victoria Parade multi-level carpark
- ⑦ Christ Lecture Theatre
- ⑧ Mercy Lecture Theatre
- ⑨ Drake House
- ⑩ Visual Arts Building
- ⑪ Early Childhood Centre
- ⑫ Rectal Room
- ⑬ Mac Labs
- ⑭ St Mary of the Cross Square
- ⑮ 3 Graham Street
- ⑯ 32 Brunswick Street



Attachment 2 - ACU Development Plan - Part 1(2)

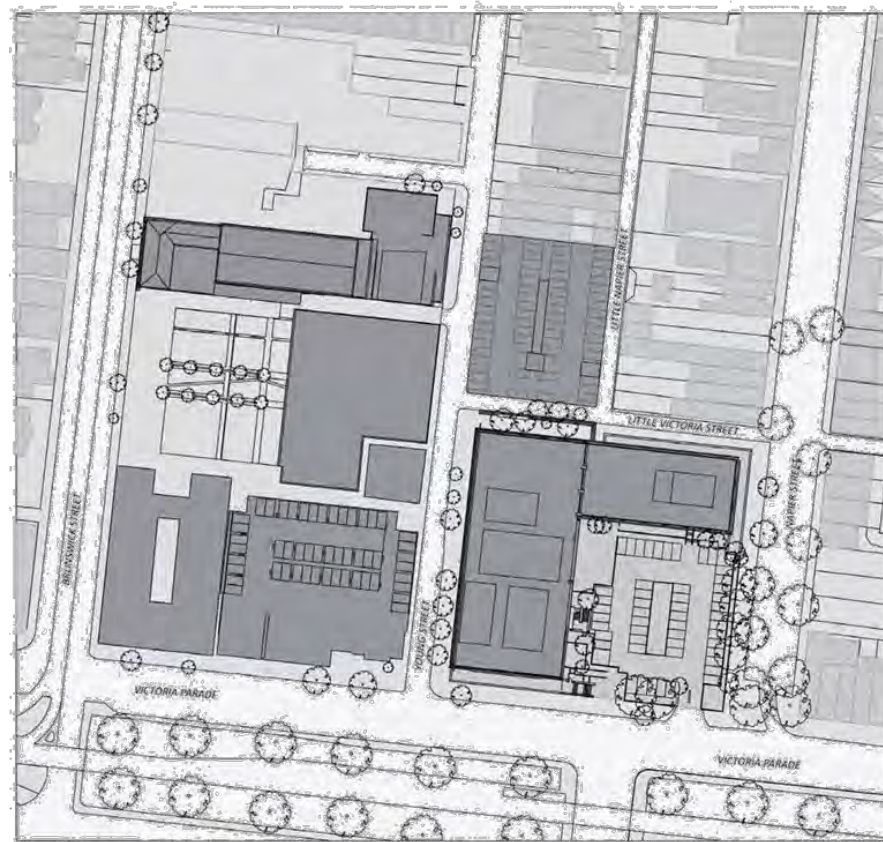


2.7 Existing Buildings

The main buildings on the campus that are subject of redevelopment under this Development Plan are:

- 115B Victoria which is currently used as an at grade car park
- The Mary Glowrey Building
- 28 - 42 Young Street

It is noted that 81 - 89 Victoria Parade is subject to a separate approvals process.



Attachment 2 - ACU Development Plan - Part 1(2)



Attachment 2 - ACU Development Plan - Part 1(2)



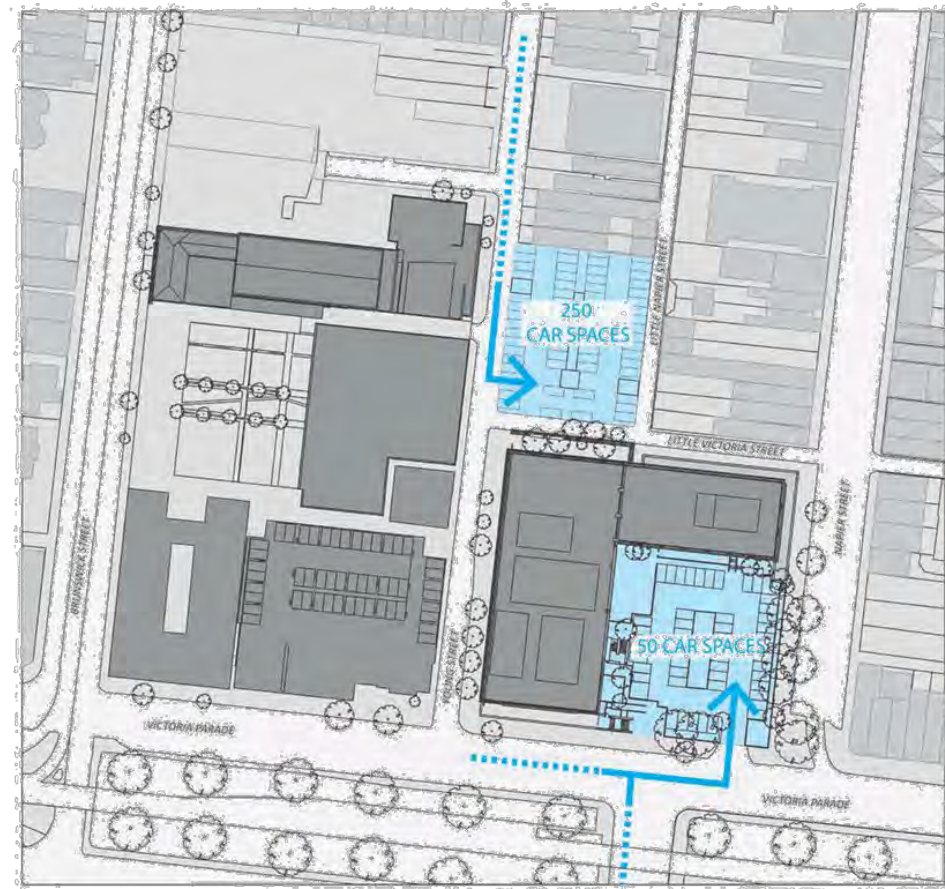
2.8 Existing open space network

The existing open space areas are generally limited to use by ACU students and staff and the Development Plan seeks improvements to open up the campus and open space areas to the wider community.



2.9 Existing access arrangement

Existing access arrangements are provided to the at grade car park on the corner of Victoria Street and Napier Street and to a multi level car park in Young Street.



Attachment 2 - ACU Development Plan - Part 1(2)



2.10 Access & Movement Analysis (pedestrian and vehicle)

The main buildings on campus are proximate to a wide array of transport options including major vehicular routes, all types of public transport, cycling routes and walking linkages. The intended campus consolidation and eventual growth will harness the excellent access available to future staff, students and visitors while also seeking to prioritise sustainable modes of transport by improving access to public transport and integrating pedestrian linkages into the wider pedestrian network.

Victoria Parade

Victoria Parade is one of the major thoroughfares of inner Melbourne. Between Spring Street and Hoddle Street it is a divided road with a wide reservation down the middle. Victoria Street is the extension of the Parade to the east and west that runs for over six kms, between North Melbourne and the Yarra River.

Victoria Street and Parade form a part of the borders of several inner Melbourne suburbs including West Melbourne, North Melbourne, Melbourne, Carlton, East Melbourne, Fitzroy, Collingwood, Richmond and Abbotsford. In addition to the Carlton Gardens, several other Melbourne landmarks are located on Victoria Parade, including RMIT University, the Royal Exhibition Building, St Vincent's Hospital, Queen Victoria Market, the Eastern Hill Fire Station and the Victorian Trades Hall.

Public Transport

The site is very well serviced by trams, trains and buses. Diagonally opposite the Campus is the St Vincent's Plaza super stop which is on the corner of Victoria Parade and Brunswick Street. Trams from this stop service the CBD and travel to North Balwyn, Docklands, Box Hill, Port Melbourne, Victoria Gardens, St Kilda and West Preston. The Nicholson Street Tram is approximately 250m from the Campus servicing Bundoora and East Brunswick.

Parliament Station can be accessed via a short tram ride and it is within a 10 minute walk from the campus for an able bodied person.

Victoria Parade is a priority Bus Route with buses servicing the Northern and Eastern suburbs including Bundoora, Kew, Doncaster, Ringwood and Healesville.

Cycling Access

The ACU campus has good access to the adjacent Napier Street priority cycling route and the recently completed Copenhagen style cycling link to the east of the site along Wellington Street.

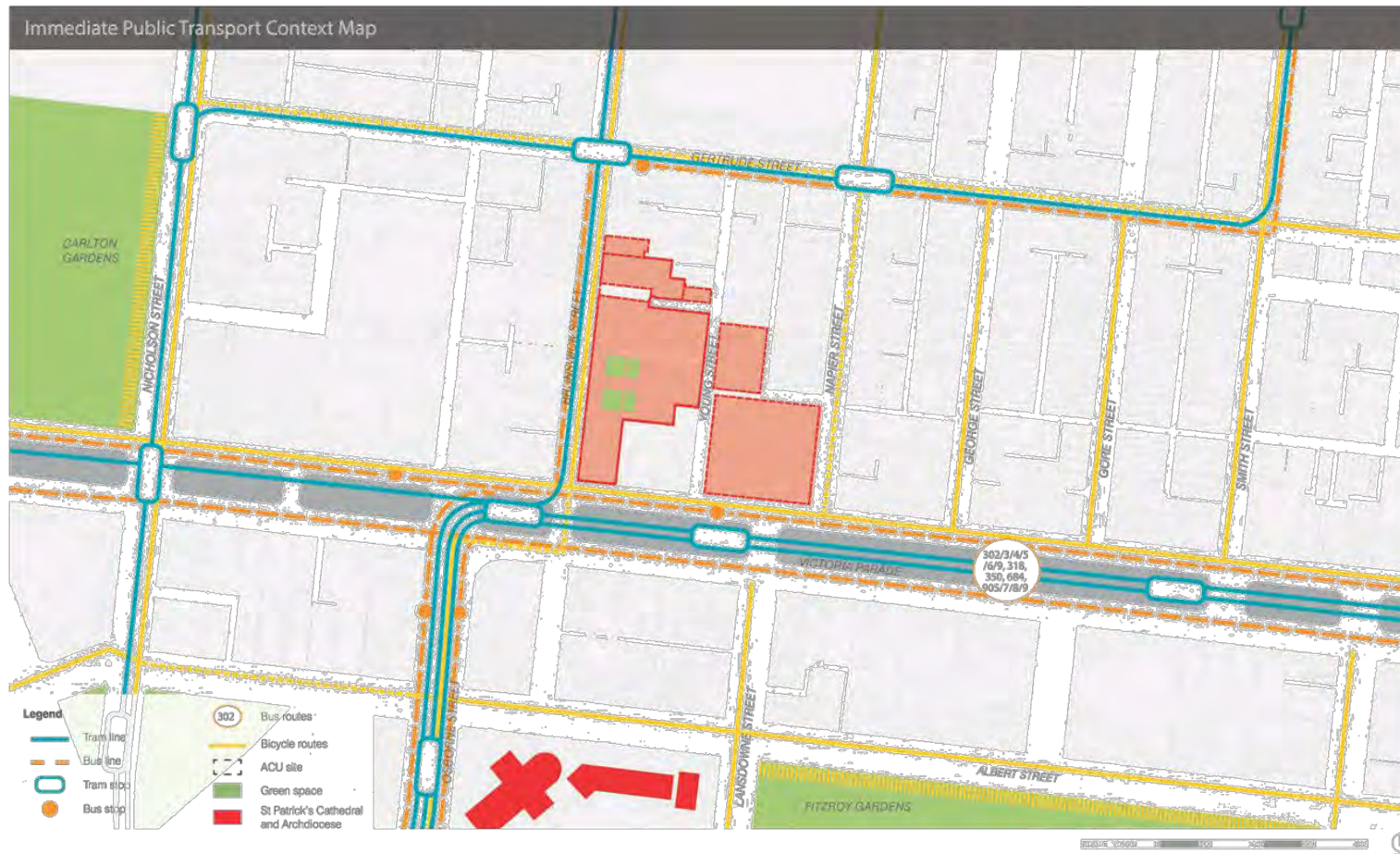
Campus Pedestrian Access

The Campus surrounds has a high number of pedestrians with people moving between the nearby bus and tram stops, large car parks and street car parking areas.

Public Transport Context Overview Map



Attachment 3 - ACU Development Plan - Part 2



Attachment 3 - ACU Development Plan - Part 2



2.11 Existing Urban Character Analysis

The ACU main campus is located within the block bounded by Brunswick Street, Napier Street, Victoria Parade and Gertrude Street, with Young Street running north-east through this wider block. ACU manages 15 buildings within this space as previously demonstrated in Section 2.6.

Victoria Parade is a major vehicular boulevard with distinctive trees creating a central avenue. Victoria Parade plays host to a range of larger buildings, more suitable to face this robust physical interface than the low scale residential areas beyond. Such buildings include various health and research facilities such as St Vincent's Private Hospital, Centre for Eye Research Australia, Epworth Freemason's Maternity Unit, and numerous others. Built form ranges widely in height from single and double storey heritage or period homes, to multi storey commercial facilities. Located within the blocks behind Victoria Parade (to both north and south) are a wide range of fine grain heritage or period dwellings interspersed with other commercial or larger residential developments.

The lower end of Brunswick Street exhibits a wide array of built form including the large social housing developments located between Gertrude and Duke Streets, with further single to triple storey developments of varying heritage or modern value between the social housing and the university.

Within Young Street, which itself is a narrow local access street, beyond the existing ACU campus buildings and the two car parks, are varying single to triple storey housing in period to modern architectural styles.

Napier Street, like Young Street, a local access street, is largely low-rise with single to triple storey residential and small scale commercial uses. The street is similar to many others in the wider area, demonstrating a fine-grain heritage streetscape interspersed with some more modern development.

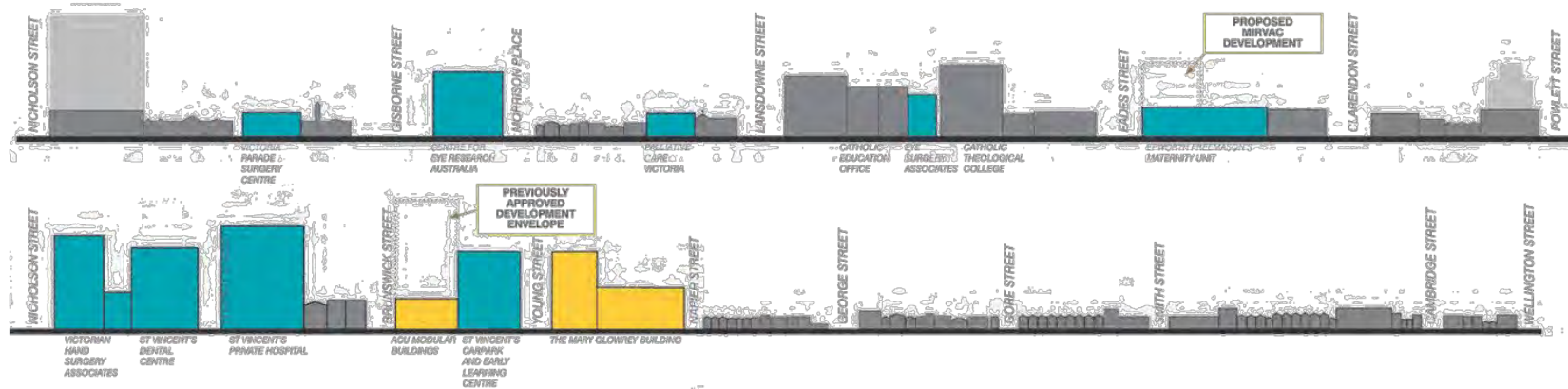
The interface between the university buildings and the lower scale built form to the north and east is softened at present through the use of varying architectural treatments and design styles, designed to complement the heritage nature of some of the building stock, softened further by landscaping which fits the treed avenues.



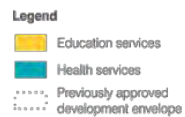
Victoria Street opposite to the ACU



St Vincent's Private Hospital, Fitzroy



Victoria Street facade adjacent ACU



Built form joining ACU along Brunswick Street

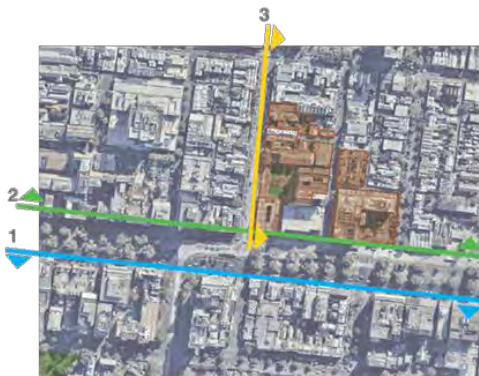
Attachment 3 - ACU Development Plan - Part 2



Victoria Street, south view away from the Australian Catholic University



Victoria Street, north view towards Australian Catholic University



- View 1 - Hard commercial edge along Victoria Street opposite to ACU
- View 2 - Mixed use Victoria Street facade adjacent the ACU
- View 3 - Finer grain heritage protected built form joining the ACU along Brunswick Street

Attachment 3 - ACU Development Plan - Part 2



Brunswick Street, view to the left of the Australian Catholic University

Attachment 3 - ACU Development Plan - Part 2



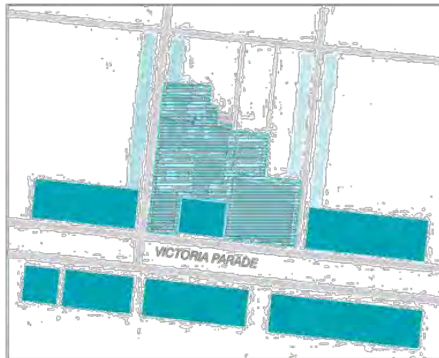
3.0 DEVELOPMENT PLAN PRINCIPLES

3.1 Key Drivers

The following section sets out the key drivers of the Development Plan, campus design drivers and ideas and design principles for new buildings.

Responding to Victoria Parade through appropriate scale and mass.

Responding to the established and future character of the Victoria Parade Health and Education Corridor as a higher density mixed use urban zone, the sites location on a major civic boulevard provides the opportunity to accommodate greater scale and height similar to buildings on St Kilda Road



Improving the legibility of the Campus through the development of well designed larger buildings for visitors to the site and as the north east entrance to the CBD

Higher Buildings will be oriented toward Victoria Parade to strengthen the presence of the University as part of this important gateway to the central city. New well designed buildings on Victoria Parade will improve the presentation of the Campus when viewed from its surrounds.



Responding to the range of local scale and heritage character conditions to the north and east of the Campus.

The size and orientation of the Campus provides the opportunity for higher scale development to be located on the Victoria Parade frontage with height transitioning down to the north to assist in reducing the impact of bulk on the sensitive residential uses to the north of the Campus



Attachment 3 - ACU Development Plan - Part 2

Improving access to **public transport** along Victoria Parade, Brunswick Street and Gertrude Street from and to the Campus.

The development proposes overall improvements to the quality and safety of the public realm by improving the integration of pedestrian access and the surrounding local precinct with a priority to improve safety and accessibility to public transport stops adjacent to the Campus



St Vincent's Plaza tram stop, Victoria Parade

Ensuring **pedestrian links** from the Campus integrate and form part of the wider pedestrian network.

St Patrick's Campus will become a pedestrian oriented space that is safe for students and prioritises sustainable modes of transport. The Campus will be developed to improve the quality of walking infrastructure on Campus and better integrate these pathways with the pedestrian routes adjacent to the Campus



Pedestrian crossing along Victoria Parade

Improving the provision and access to **additional and upgraded open space** for the ACU Campus community, visitors to the site and the local community

Incrementally, the Campus will have a network of different kinds of interlinked open spaces that serve different functions and offer flexible spaces for break out, social and structured interaction and rest and relaxation for both the University and local community. Green landscape elements will be incorporated throughout the Campus where ever possible



Pedestrian access in and around the campus

Attachment 3 - ACU Development Plan - Part 2



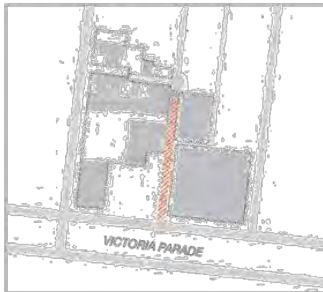
3.2 Campus Design Drivers

The Campus design drivers reflect the ACU's vision to improve the quality and accessibility of the Campus' public realm and as a well-designed city campus, to better integrate the Campus within its local environment.

The Campus open space and pedestrian networks will be improved and expanded to better connect the Campus to its local surroundings, increase access to open space in the South Fitzroy area and provide greater accessibility and sense of safety for all students, academic staff and visitors to the Campus.

The planning and design principles have been developed in accordance with the City of Yarra's commitment to reduce car dependence by promoting walking, cycling and public transport use and improving the quality of walking and cycling infrastructure within and adjacent to the St Patrick's Campus.

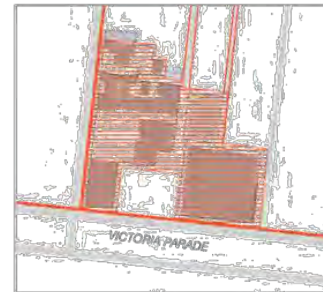
- 1** A shared zone along **Young Street** will create a **pedestrian environment** that contributes to public open space and amenity.



- 2** Two **Campus spines** provides access for pedestrians through and around the Campus and opens up the Campus for improved public access.



- 3** A **pedestrian focused** Campus provides a safer environment for students and strengthens connections and relationships between campus buildings and the adjacent campus uses and activities.



- 4** A series of **open spaces** will increase public realm access and Campus amenity.



- 5** A **hierarchy** of open spaces will serve a multitude of passive and active functions.



- 6** Establishing **Campus hearts**, that work together as communal spaces will serve the various buildings within the Campus.



- 7** An **activated building edge** on the ground level allows for a connection between the built form and open spaces.



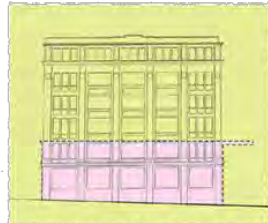
- 8** The **Campus green** is both a public and Campus zone, connecting the Campus back to the wider community.



Attachment 3 - ACU Development Plan - Part 2



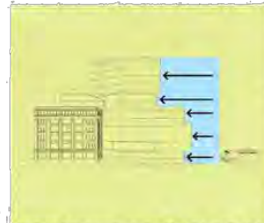
Design Ideas – New Buildings



The existing glass facade and ramp arrangement of the Mary Glowrey Building on Victoria Pde is removed to re-establish the heritage facade.



Consolidates a new entry for both the Mary Glowrey Building and the new IISB Victoria Parade building.



The new building consciously steps back from Napier Street, keeping the primary height and mass of the new buildings recessed away from the more residential scale of the Napier Street context.



The new building is formally composed such that it allows the two primary ends of the Mary Glowrey Building to be fully expressed in the primary streetscapes. This is achieved by revealing and preserving corners of the Mary Glowrey Building.



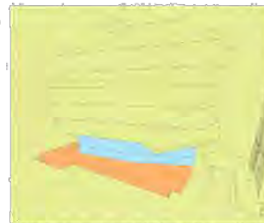
The building form of IISB is chamfered to reveal a greater portion of the Mary Glowrey facade as well as softening the impact of the new building on the Victoria Parade streetscape.



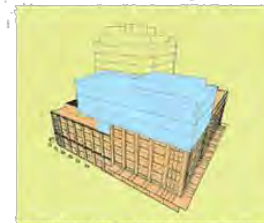
The form of the new building consists of three distinct parts that respond directly to their individual context. The podium responds to the steps in form of the Mary Glowrey Building. The hub sits back to form a bridging element between the Mary Glowrey building and the new building. The tower aligns and steps back significantly from Napier Street.



An atrium between Mary Glowrey and IISB has been established to provide connections between the two buildings as well as preserving access to natural light for both buildings. The existing glass pop-out of the Mary Glowrey is removed to restore the heritage facade.



The ground floor is undercut away from the site boundary on Victoria Pde and Napier Street, revealing valuable urban space at the ground plane as well as providing generous means of circulation around the building from Napier Street and Victoria Parade.



The addition above the Mary Glowrey building is set back from the line of the new IISB building and tapers away from Napier Street to minimise its visual impact on the Mary Glowrey building and on the residential context of Napier Street.



The built form on top of the Mary Glowrey building extends across the atrium as a bridging element, visually tying the main building to the 3-storey addition.



A new canopy will extend beyond the entrance to IISB along the face of the Mary Glowrey Building on Victoria Parade continuing into Young Street. The canopy will improve the pedestrian experience for the new IISB inhabitants and link the new building with the heart of the campus.



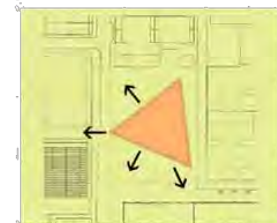
An inclusive ground plane strategy around IISB seeks to activate the zone around the Mary Glowrey and Little Victoria Street through the breaking down of the edge wall of the Mary Glowrey. Elevating a majority of the light court to street level widens the streetscape, providing opportunities to give back to the urban realm.



The car park entry and exit have been planned to be located on Napier Street which ensures the preservation of a strong active urban corner on Victoria Parade and enables a more harmonious relationship between cars, cyclists and pedestrians around IISB.



The triangular form of the new building creates two distinct open spaces to its north and south, creating valuable urban space that will offer a combination of conditions according to the position of the sun throughout the day.



The new building as a building in the round, means that it has no distinct front or back, enabling it to open itself up to all parts of the ACU campus and its surrounding context, whilst ensuring no overlooking of neighbouring properties from habitable spaces.

Attachment 3 - ACU Development Plan - Part 2



4.0 DEVELOPMENT PLAN COMPONENTS

4.1 Campus Design Principles

The Campus Design Principles have been developed in response to the important role and location of the St Patrick's Campus and to improve the Campus amenity and accessibility for students, academic staff, visitors to the Campus and the local community. The proposed developments provide the opportunity for the Campus to fulfil its role as part of the capital city knowledge economy and reflect its location at the north east entrance to the CBD that is part of the cluster of higher health and education buildings including the Eye and Ear Hospital and St Vincent's Public and Private Hospitals.

The vision of the ACU is for the St Patrick's Campus to optimise its place making potential through the development of well designed higher buildings that improve the legibility of the Campus while respecting the important interface it has with the finer grain heritage buildings and streetscapes.

As the Campus is developed, the open space and pedestrian networks will be improved and expanded to better connect the Campus to its local surroundings, increase open space in the South Fitzroy area and provide greater accessibility for students, academic staff, visitors to the Campus and the local community.

LAND USE

The Development Plan is required to enable the ACU to accommodate growing community educational and research needs and to consolidate all of the ACU's teaching, academic and research activities to the St Patrick's Campus.

The Development Plan has been developed for delivery over a fifteen year period.



BUILT FORM

The ACU St Patrick's Campus will deliver built form that relates to the surrounding context and creates an optimal environment for the sustainable operation of the University. This will include:

- height and mass that relates to the established and future character of the Victoria Parade corridor as a health and education focused mixed use urban zone.
- buildings with identity oriented toward Victoria Parade and the city to serve a marker to strengthen the presence of the University within the city.
- transitions in height and intensity to respond to the character of the inner Fitzroy precinct.
- the ground level of buildings interact with the public realm and help activate and enliven open spaces within the Campus.

THE CAMPUS PUBLIC REALM

The Campus public realm will provide a hierarchy of spaces that are activated and well used by the University and the wider community. This will be achieved through the following key initiatives:

- St Patrick's Campus will become a pedestrian oriented space that is safe for students and prioritises sustainable modes of transport.
- the creation of a pedestrian focussed zone that encompasses University buildings will create strong pedestrian linkages through the Campus and to adjacent uses, activities and transport networks.
- a number of different kinds of open spaces that serve different functions and offer flexible spaces for break out, social and structured interaction for both the University and the community and places for rest and relaxation.
- green landscape elements will be incorporated where ever possible including enhancing any blank walls.
- the public realm will be activated by built form boundaries where inside and outside activities cross over with uses that extend into open spaces from within buildings.

ENVIRONMENTAL SUSTAINABILITY

The built form and public realm is designed to maximise environmental sustainable development (ESD) initiatives and showcase sustainability to the wider community. This includes:

- ensure efficient use of energy and reduce operating greenhouse gas emissions.
- ensure efficient use of water, reducing potable water use and encourage the collection and reuse of stormwater and alternative water sources.
- improve indoor environment quality including by providing fresh air intake and cross ventilation, where feasible natural and appropriate levels of lighting and external views, and thermal comfort.
- reduce the impact and improve the quality of stormwater runoff through water sensitive urban design.
- promote sustainable transport including walking, cycling and public transport to minimise car dependency.
- manage waste through waste avoidance, reuse and recycling and the creation of adaptable spaces and buildings formed with durable materials.
- enhance biodiversity and natural habitats and reduce the urban heat island effect through the use of indigenous vegetation.

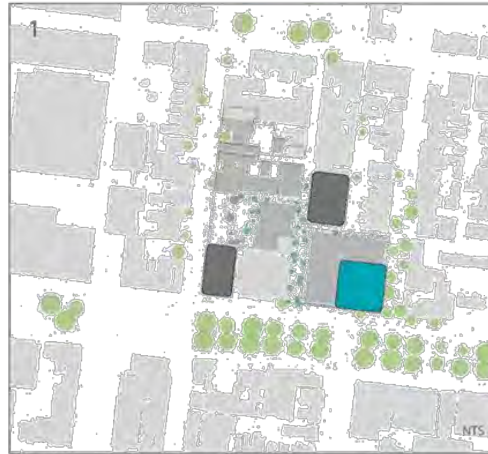
Attachment 3 - ACU Development Plan - Part 2



4.2 Staging

The campus will be gradually consolidated allowing the leases to be released and a consolidated campus achieved.

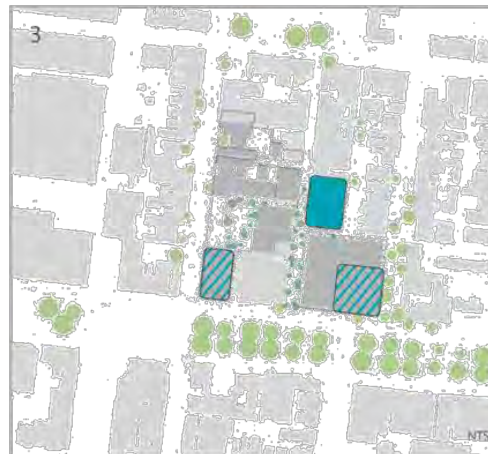
The indicative staging is indicated in the diagrams below with the new building at 115B Victoria Parade being the first priority project. Future stages will depend on future education needs, growth and funding availability, as the university continues to plan for evolving educational trends.



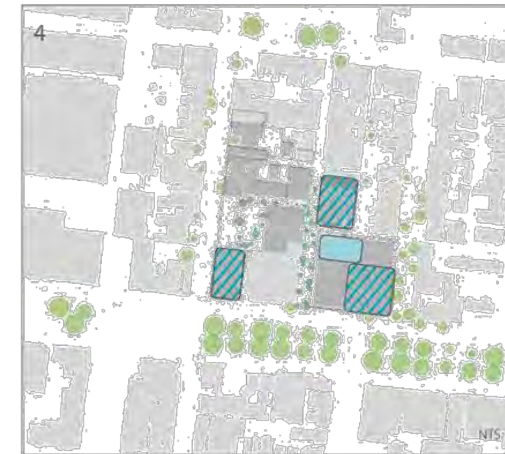
Stage 1 - Development of 115B Victoria Parade



Stage 2 - Development of 81-89 Victoria Parade
*subject to a separate application process



Stage 3 - Development of 28-42 Young Street



Additional floor space to Mary Glowrey Building

- Legend**
- Campus building for development
 - Current development within stage
 - Completed development
 - Additional floor space to existing buildings

Attachment 3 - ACU Development Plan - Part 2



4.3 Demolition Plan

The following diagram indicates areas and buildings requiring demolition to allow for the new buildings sought to achieve the campus objectives.



Demolition Plan

4.4 Built Form Plans

- The likely building envelopes of the key development projects are set out in this section and include:
- 115B (117) Victoria Parade with underground car parking
- 28-42 Young Street and campus hub

4.4.1 115B (117) VICTORIA PARADE

The first stage of development is to construct a new building on the corner of Napier Street and Victoria Parade. The existing car park will be replaced with new underground car parking and a new building constructed. This will be the first important step in achieving the campus consolidation objectives sought. The following drawings indicate the indicative layout and building envelope.

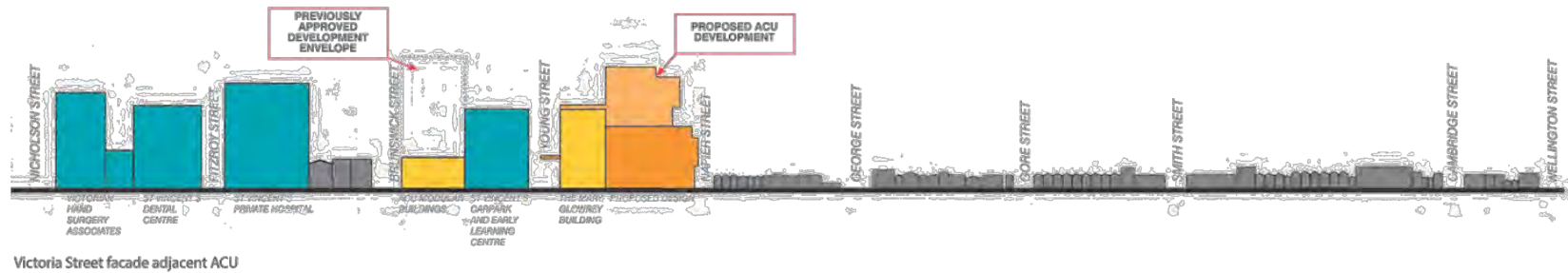
The extension to the Mary Glowrey building will be a later stage of development and provide a connection to the new building at 115B and additional learning space on the campus.








Proposed Plan



4.4.2 PROPOSED STREETScape



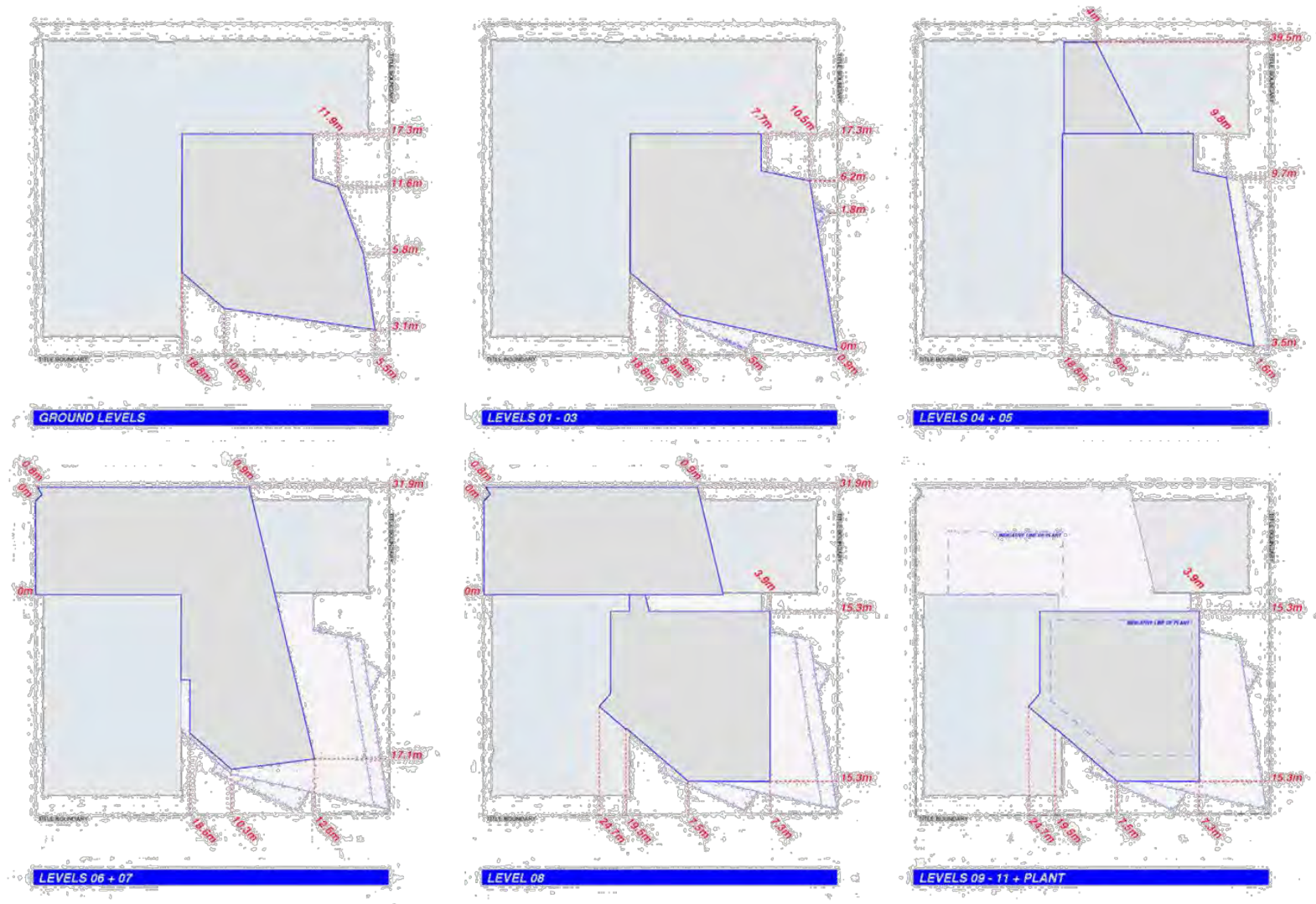
Legend

-  Education services
-  Proposed ACU building
-  Proposed ACU building (increased setback)
-  Health services
-  Previously approved development envelope

Attachment 3 - ACU Development Plan - Part 2



4.4.3 INDICATIVE SETBACKS AND FLOOR LAYOUT



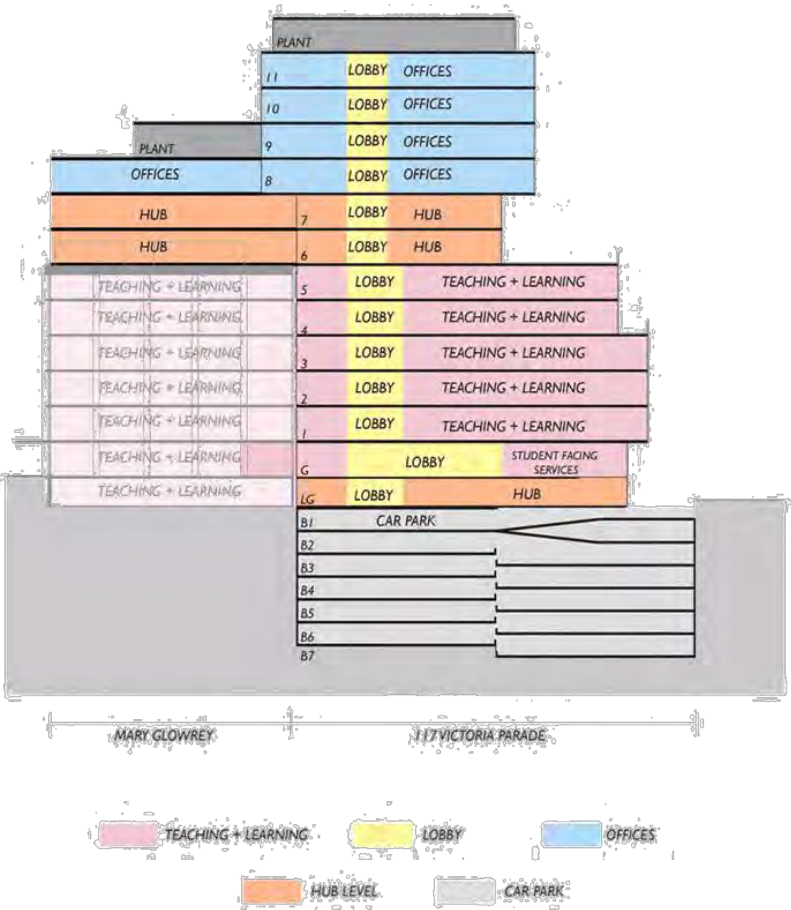
Attachment 3 - ACU Development Plan - Part 2

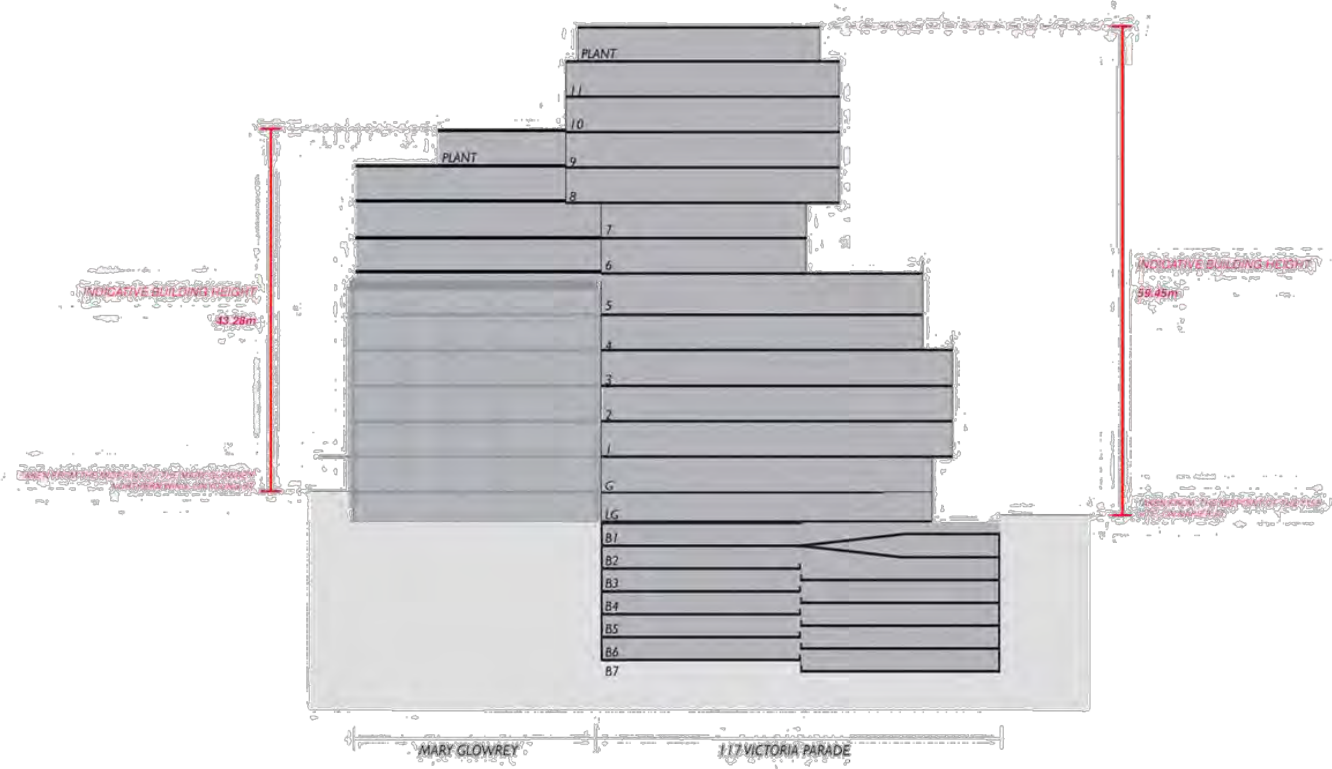
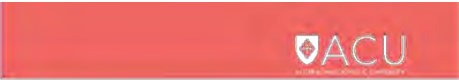


4.4.4 INDICATIVE SECTION, LAYOUT AND HEIGHT

The following diagrams indicate the likely teaching and learning, office, student hub and car parking spaces of the proposed building and the indicative building height.

The internal layout will be developed in accordance with ACU's teaching, student and staff needs, at the planning permit stage.





Attachment 3 - ACU Development Plan - Part 2



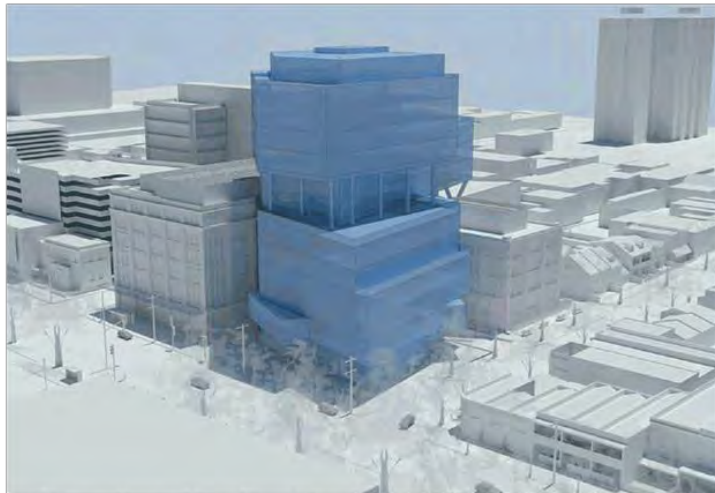
4.4.5 INDICATIVE BUILDING ENVELOPES



Looking south down Napier Street



Looking north-east from Victoria Parade

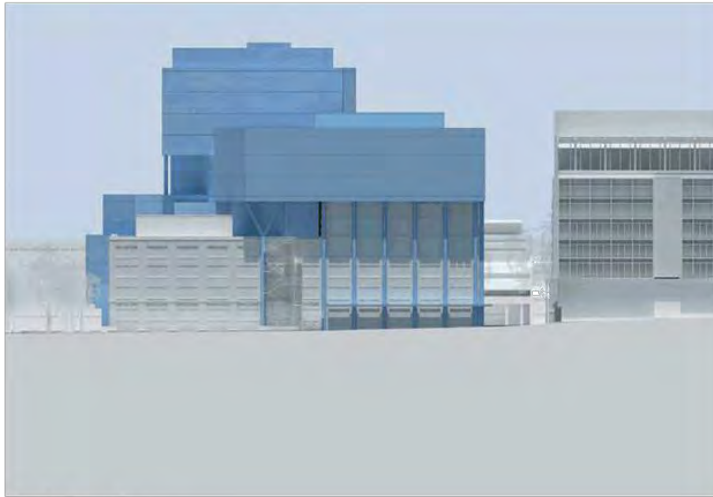


Looking north-west from Victoria Parade

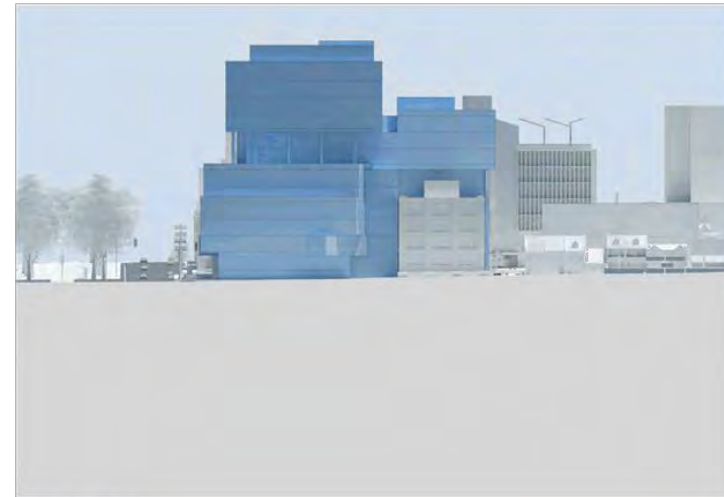


Looking north up Young Street

Attachment 3 - ACU Development Plan - Part 2



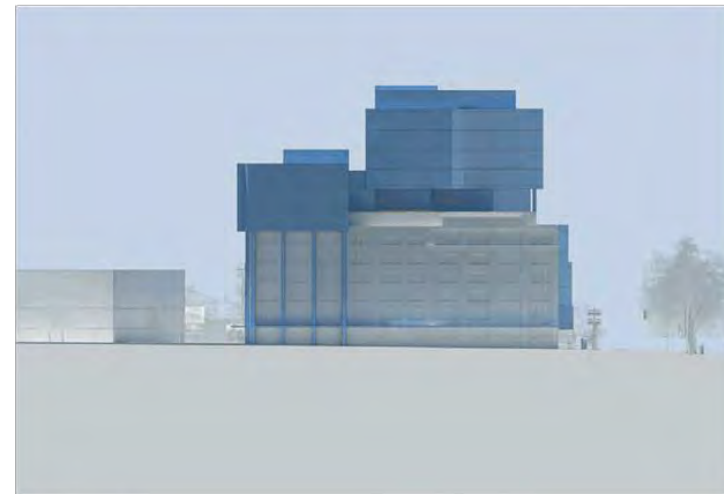
North Elevation



East Elevation



South Elevation



West Elevation

Attachment 3 - ACU Development Plan - Part 2



4.4.6 INDICATIVE BUILDING APPEARANCE

The following diagrams give an indication of the general appearance of the proposed building. This will be subject to further review and refinement at the detailed design and planning permit stage.



Aerial view over Napier Street looking west.



Aerial view above Napier Street residences.

Attachment 3 - ACU Development Plan - Part 2



Street view from the corner of Brunswick Street and Victoria Parade.



Street view from the end of Landsdowne Street looking north.



Street view from Victoria Parade looking towards Young Street.



Street view looking west up Victoria Parade.

Attachment 3 - ACU Development Plan - Part 2



Street view looking south along Napier Street.



Street view from the Landsdowne Street tram stop.



Street view from eastern side of Napier Street.



Street view at the entry of the 115b building.

Attachment 3 - ACU Development Plan - Part 2



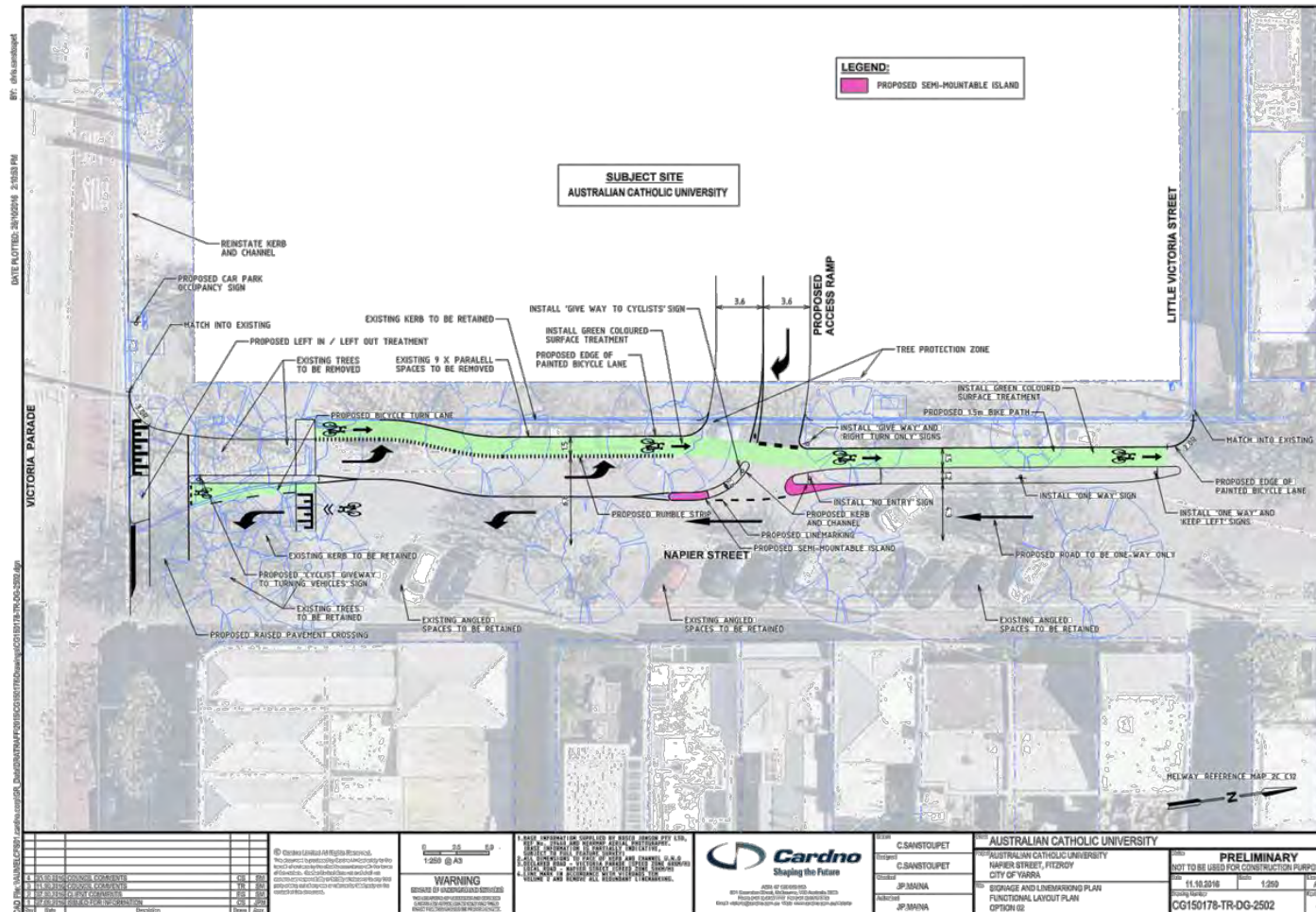
Street view from Napier Street.



Street view from the corner of Victoria Parade and Napier Street.



4.4.7 PROPOSED ACCESS 115B VICTORIA PARADE



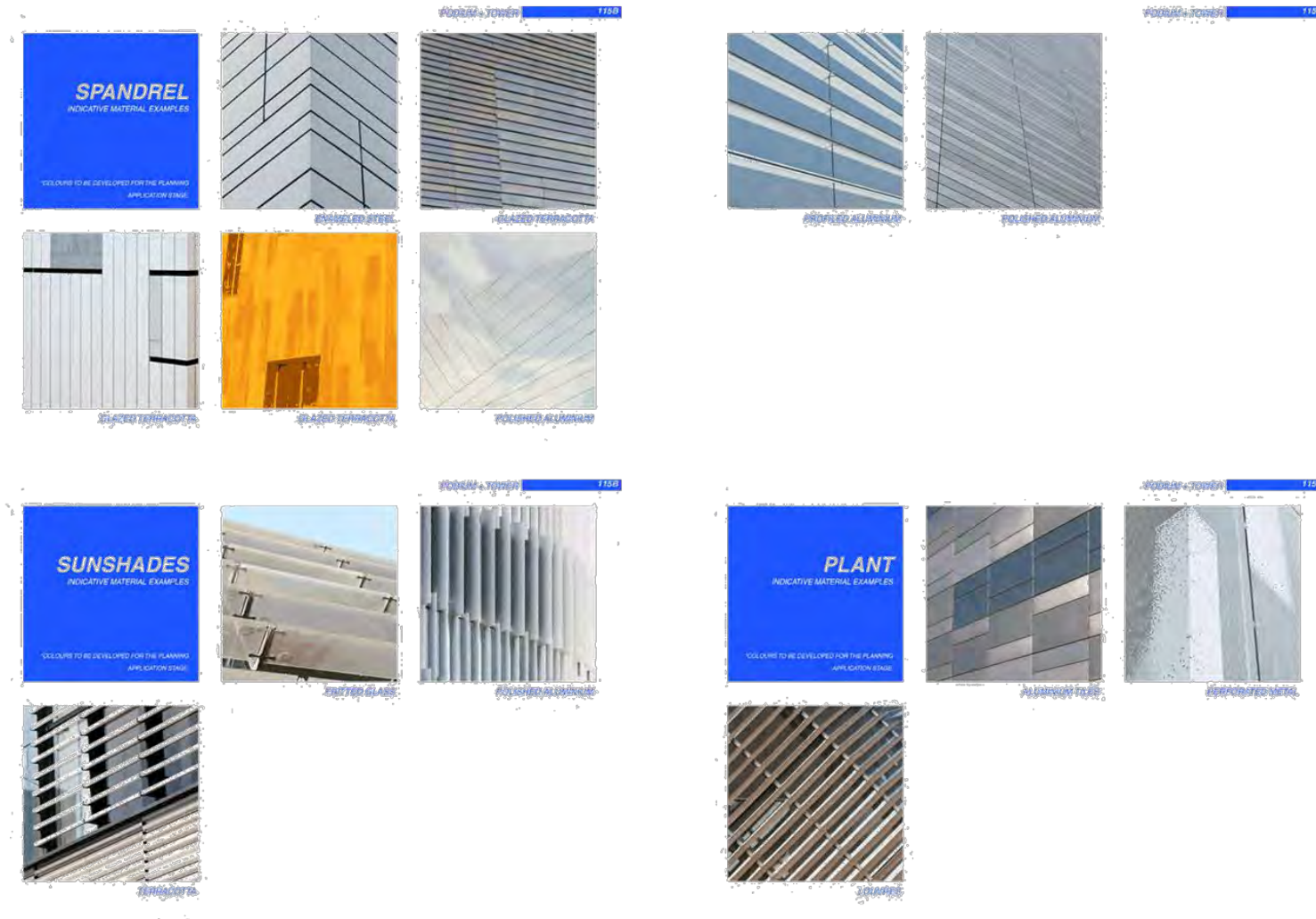
Attachment 3 - ACU Development Plan - Part 2

4.4.8 POSSIBLE MATERIALS PALETTE

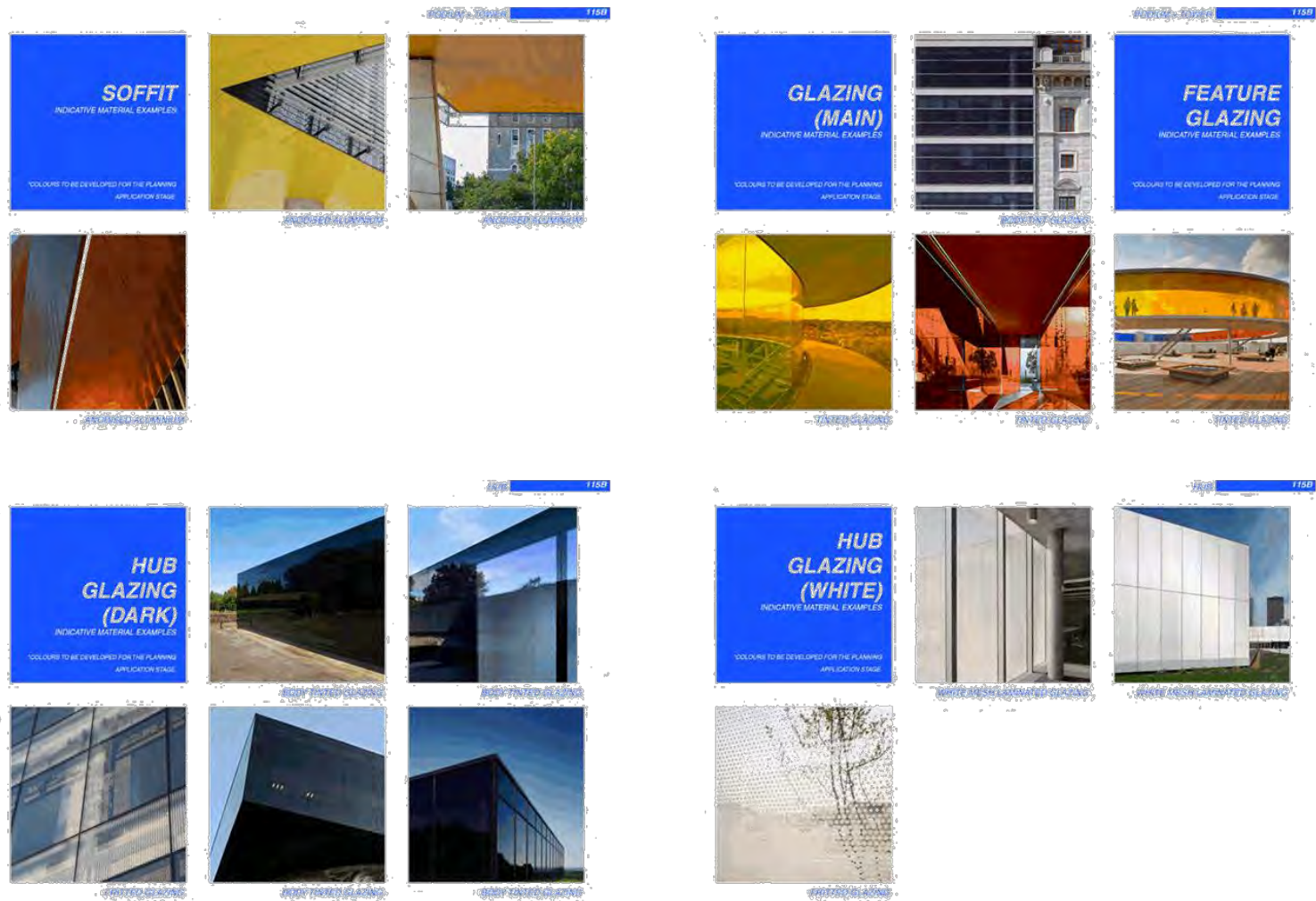
The new building designed by Lyons will be constructed of high quality materials and finishes to provide an appropriate contemporary response to the surrounding area. Some examples of the possible materials are provided below.



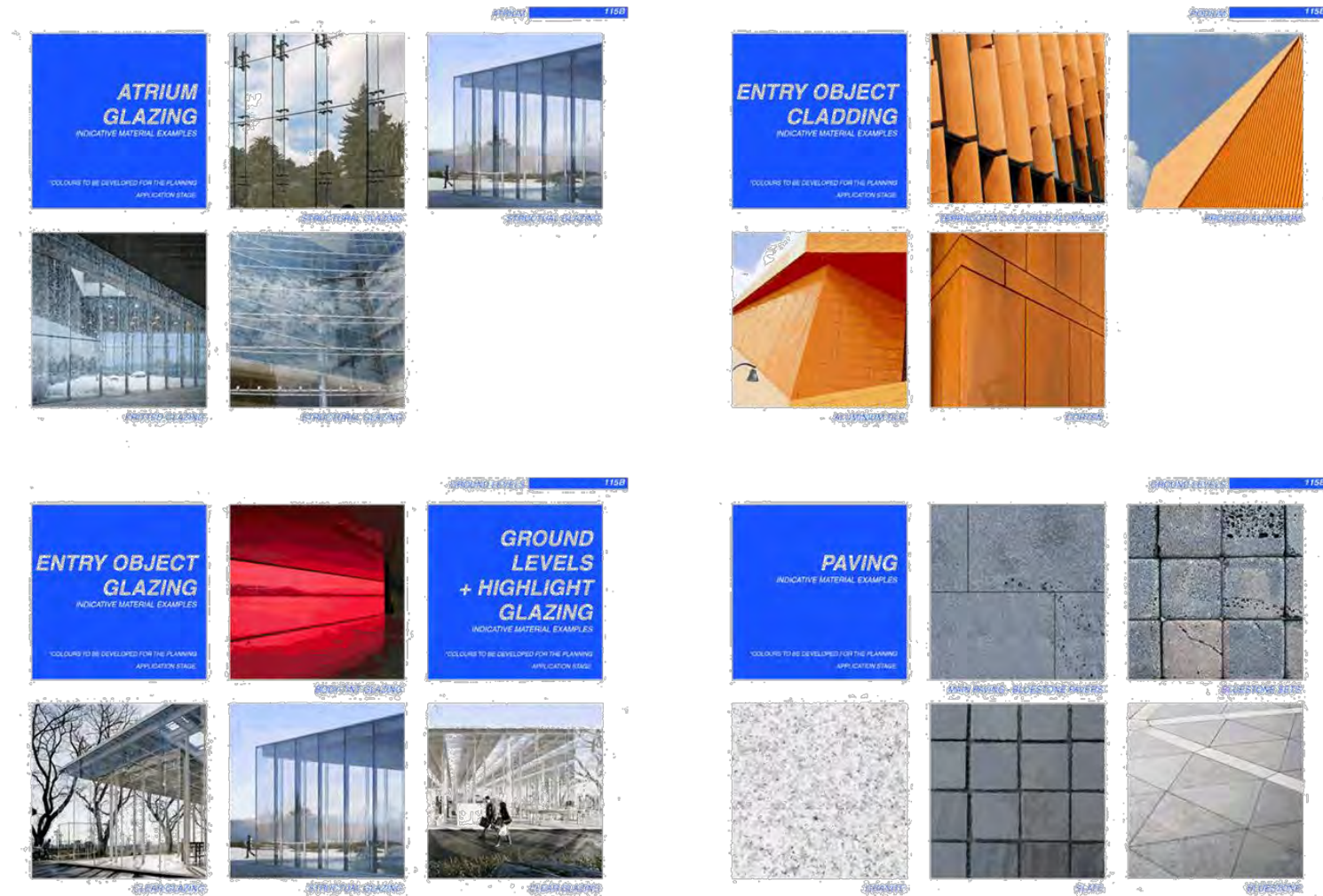
Attachment 3 - ACU Development Plan - Part 2



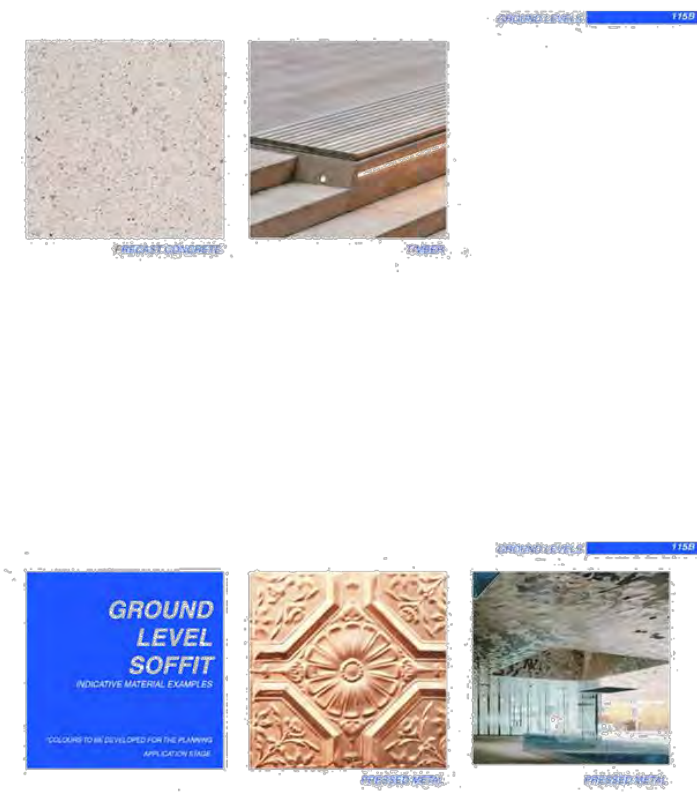
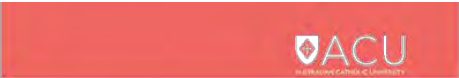
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Attachment 4 - ACU Development Plan - Part 3



Attachment 4 - ACU Development Plan - Part 3



Attachment 4 - ACU Development Plan - Part 3



4.4.9 DESIGN GUIDELINES - 115B VICTORIA PARADE

The proposed building form for 115B provides the following key design outcomes:

<ul style="list-style-type: none"> The existing glass facade and ramp arrangement of the Mary Glowrey Building on Victoria Parade is removed to re-establish the heritage facade; 	<ul style="list-style-type: none"> The form of the new building consists of three distinct parts that respond directly to their individual context; The podium responds to the steps in form of the Mary Glowrey Building; The hub sits back to form a bridging element between the Mary Glowrey building and the new building; The tower aligns and steps back significantly from Napier street.
<ul style="list-style-type: none"> Consolidates a new entry for the both the Mary Glowrey Building and the new 115B Victoria Parade building; 	<ul style="list-style-type: none"> An atrium between Mary Glowrey and 115B has been established to provide connections between the two buildings as well as preserving access to natural light for both buildings; the existing glass pop-out of the Mary Glowrey is removed to restore the heritage facade.
<ul style="list-style-type: none"> The new building consciously steps back from Napier Street, keeping the primary height and mass of the new buildings recessed away from the more residential scale of the Napier Street context; 	<ul style="list-style-type: none"> The ground floor is undercut away from the site boundary on Victoria Parade and Napier Street, revealing valuable urban space at the ground plane as well as providing generous means of circulation around the building from Napier Street and Victoria Parade;
<ul style="list-style-type: none"> The new building is formally composed such that it allows the two primary ends of the Mary Glowrey Building to be fully expressed in the primary streetscapes. This is achieved by revealing and preserving corners of the Mary Glowrey Building; 	<ul style="list-style-type: none"> The addition above the Mary Glowrey building is set back from the line of the new 115B building and tapers away from Napier Street to minimise its visual impact on the Mary Glowrey building and on the residential context of Napier Street;
<ul style="list-style-type: none"> The building form of 115B is chamfered to reveal a greater portion of the Mary Glowrey facade as well as softening the impact of the new building on the Victoria Parade streetscape; 	<ul style="list-style-type: none"> A new canopy will extend beyond the entrance to 115B along the face of the Mary Glowrey Building on Victoria Parade continuing into Young street. The canopy will improve the pedestrian experience for the new 115B inhabitants and link the new building with the heart of the campus.

Attachment 4 - ACU Development Plan - Part 3



- An inclusive ground plane strategy around 115B seeks to activate the zone around the Mary Glowrey and Little Victoria Street through the breaking down of the edge wall of the Mary Glowrey. Elevating a majority of the light court to street level widens the streetscape providing opportunities to give back to the urban realm.
- The car park entry and exit have been planned to be located on Napier Street which ensures the preservation of a strong active urban corner on Victoria Parade and enables a more harmonious relationship between cars, cyclists and pedestrians around 115B;
- The triangular form of the new building creates two distinct open spaces to its' north and south, creating valuable urban space that will offer a combination of conditions according to the position of the sun throughout the day.
- The new building, as a building in the round, means that it has no distinct front or back, enabling it to open itself up to all parts of the ACU campus and its' surrounding context, whilst ensuring no overlooking of neighbouring properties from habitable spaces.

Attachment 4 - ACU Development Plan - Part 3



4.4.10 28 - 42 YOUNG STREET

The existing multi level car park will be removed to make way for a new student facility and community space with public open space. The following drawings indicate the indicative layout and building envelopes.

4.4.11 DESIGN GUIDELINES - 28-42 YOUNG STREET

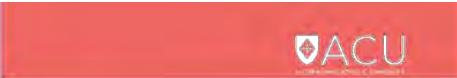
The proposed building form for the Young Street Building provides the following key design outcomes:

- The triangular form of the new building creates two distinct open spaces to its north and south, creating valuable urban space that will offer a combination of conditions according to the position of the sun throughout the day.
- The new building, as a building in the round means that it has no distinct front or back, enabling it to open itself up to all parts of the ACU campus and its surrounding context, whilst ensuring no overlooking of neighbouring properties from habitable spaces.
- The spaces around the new building act as an open campus hub that connect to all areas of the campus forming a singular consolidated space for students and staff at the centre of the campus.



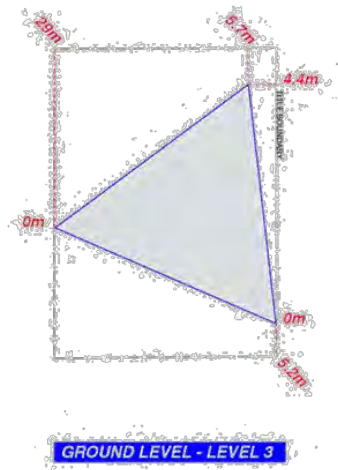
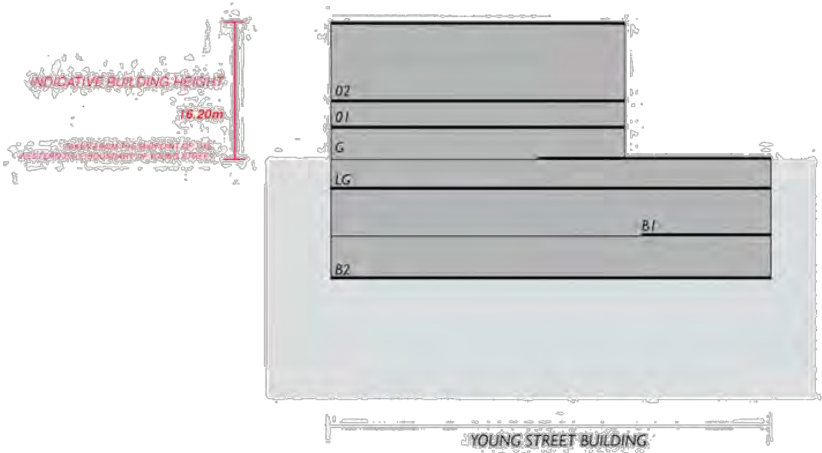
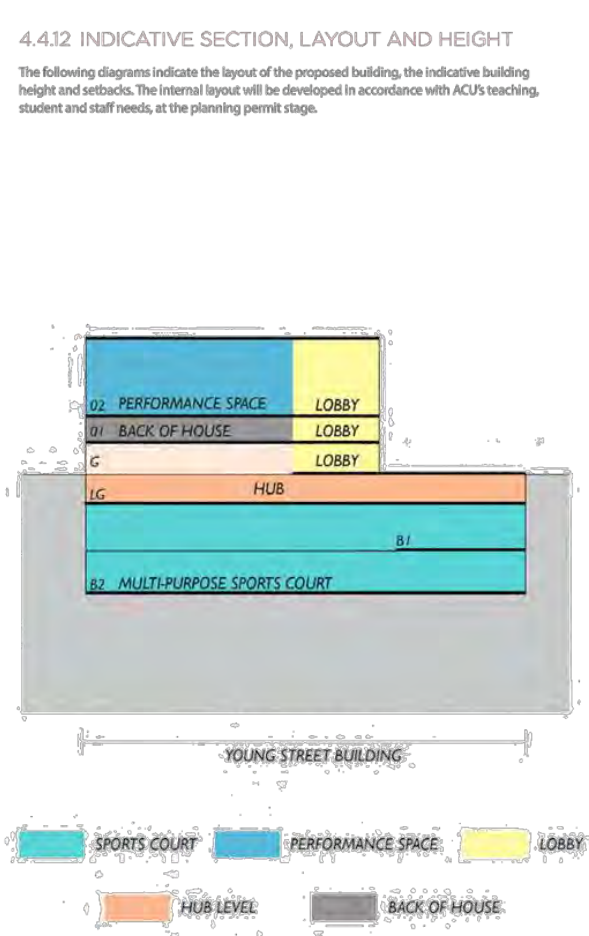
Proposed Plan - Young Street

Attachment 4 - ACU Development Plan - Part 3



4.4.12 INDICATIVE SECTION, LAYOUT AND HEIGHT

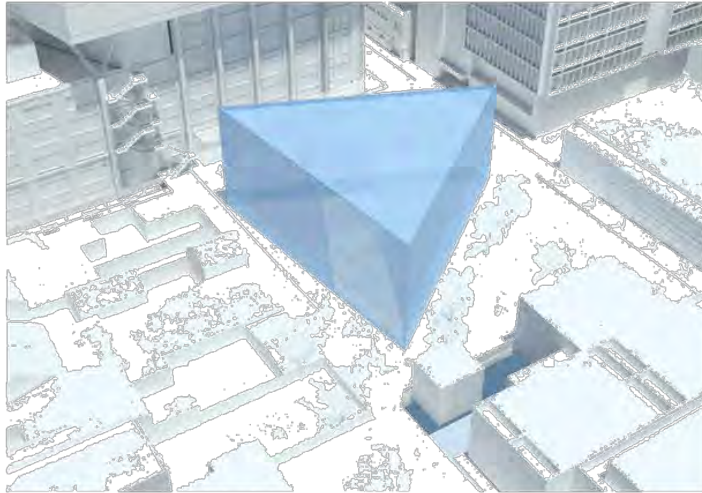
The following diagrams indicate the layout of the proposed building, the indicative building height and setbacks. The internal layout will be developed in accordance with ACU's teaching, student and staff needs, at the planning permit stage.



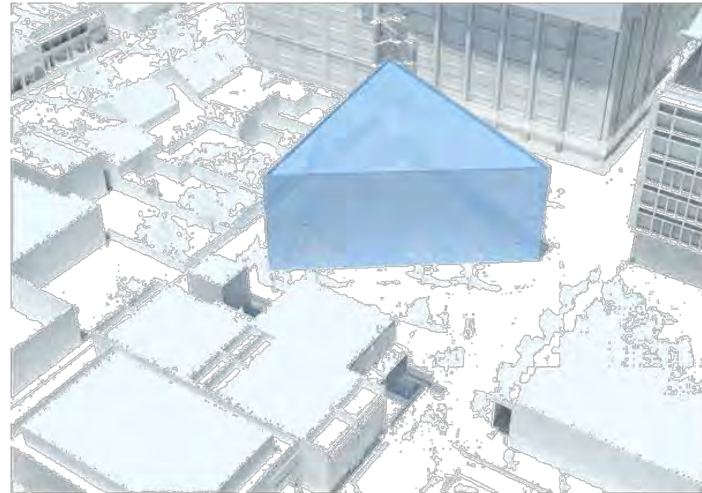
Attachment 4 - ACU Development Plan - Part 3



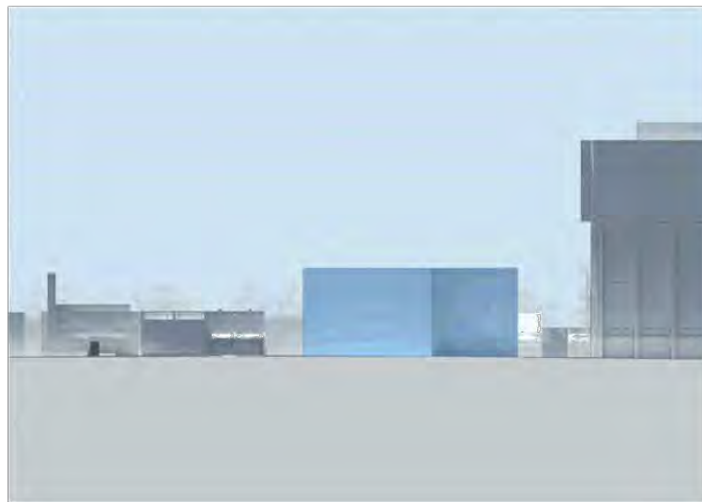
4.4.13 INDICATIVE BUILDING ENVELOPES



Aerial view from above Little Napier Street looking towards Young Street.



Aerial view above Graham Street looking towards the Mary Glowrey Building



West Elevation

Attachment 4 - ACU Development Plan - Part 3

4.4.14 INDICATIVE BUILDING APPEARANCE

The following diagrams give an indication of the general appearance of the proposed building. This will be subject to further review and refinement at the detailed design and planning permit stage.



Aerial view above Graham Street looking towards the Young Street site.



Street view from the corner of Graham Street and Young Street.



Street view from the end of Duke Street



Street view from little Napier Street looking towards the Young Street site.



Street view from the corner of Little Napier and Little Victoria Streets.



View from the proposed landscape of the Young Street site.

Attachment 4 - ACU Development Plan - Part 3

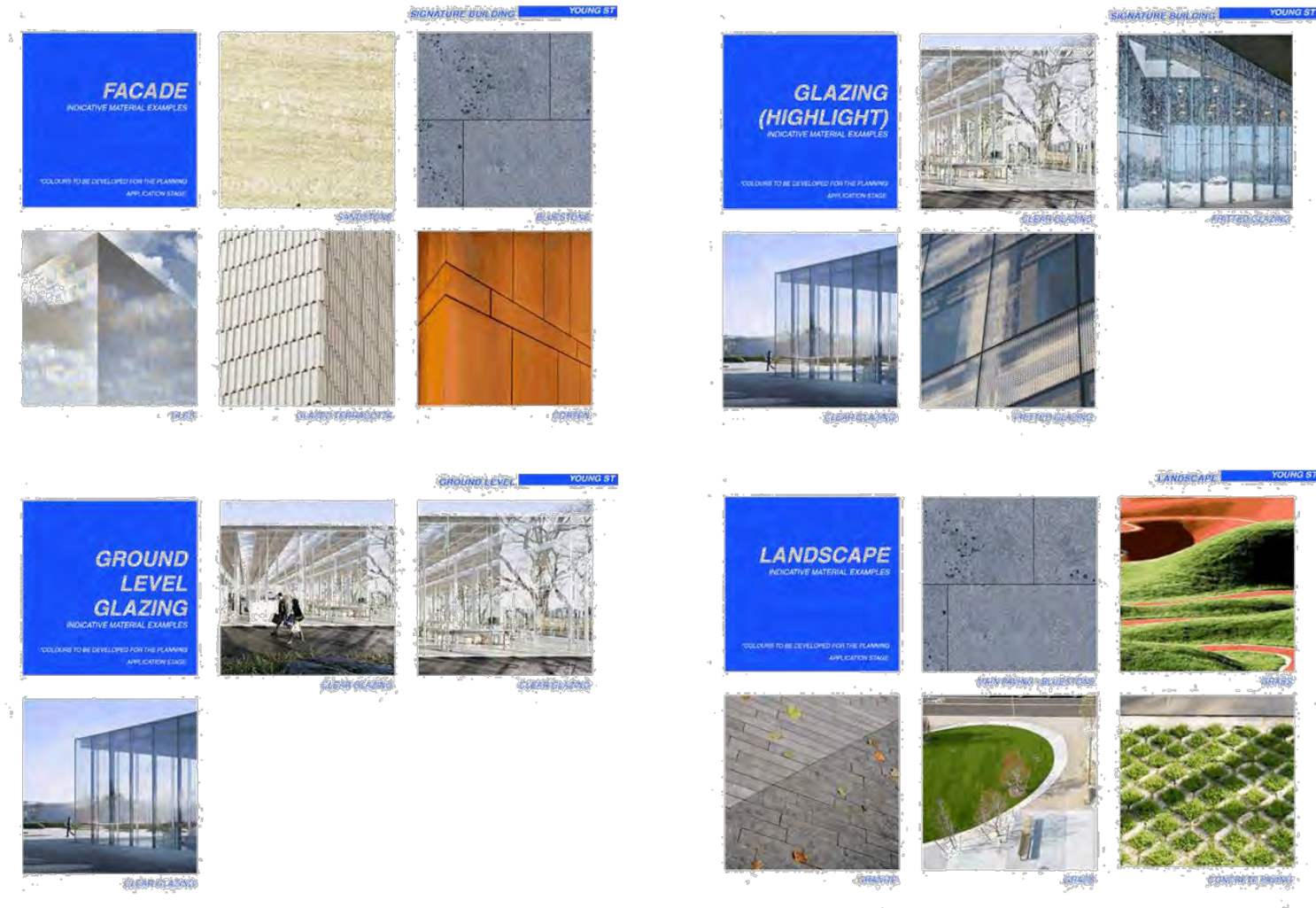
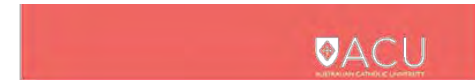


4.4.15 YOUNG STREET INDICATIVE MATERIAL PALETTE

YOUNG ST



Attachment 4 - ACU Development Plan - Part 3



Attachment 4 - ACU Development Plan - Part 3



4.5 Campus Hub

The new buildings will create a new campus hub which will provide an attractive and active space, bringing the campus together.



Proposed Plan - Hub Diagram

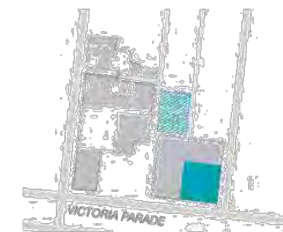
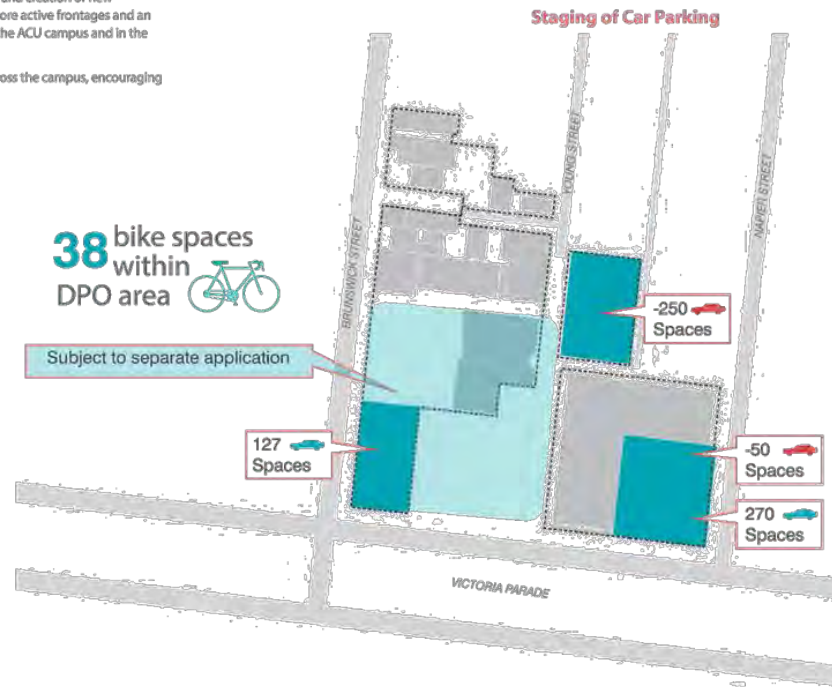
Attachment 4 - ACU Development Plan - Part 3



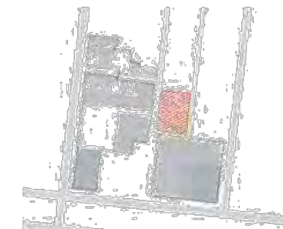
4.6 Proposed Vehicle and Bicycle Parking

The removal of the Young Street Car Park and creation of new underground car parking will allow for more active frontages and an improved urban design outcome across the ACU campus and in the public realm.

New bicycle facilities will be provided across the campus, encouraging students and staff to cycle.



Development of 115 B (117) Victoria Parade with 270 car parking spaces



Demolition of existing car park, 250 car parking spaces

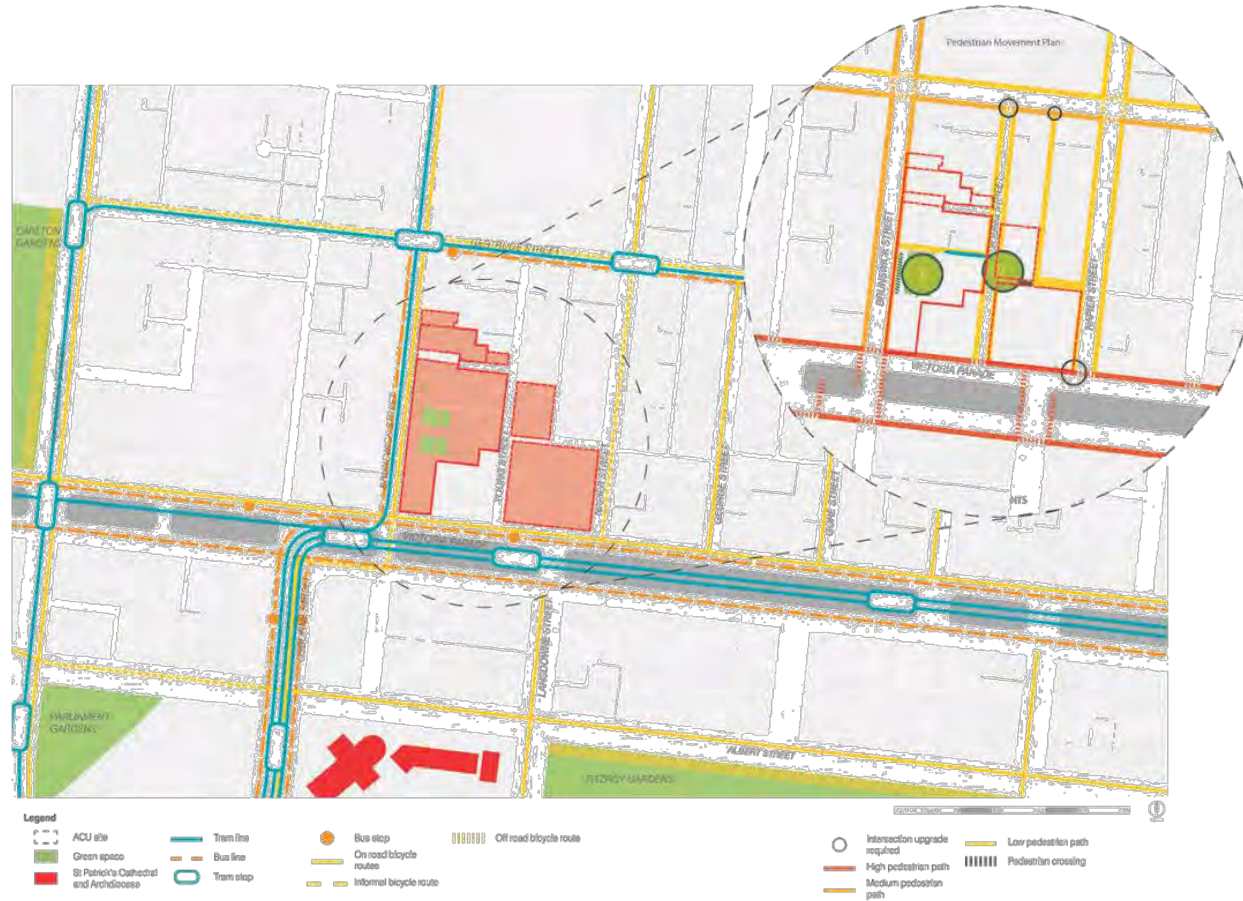
80 additional bike spaces in total on campus

Attachment 4 - ACU Development Plan - Part 3



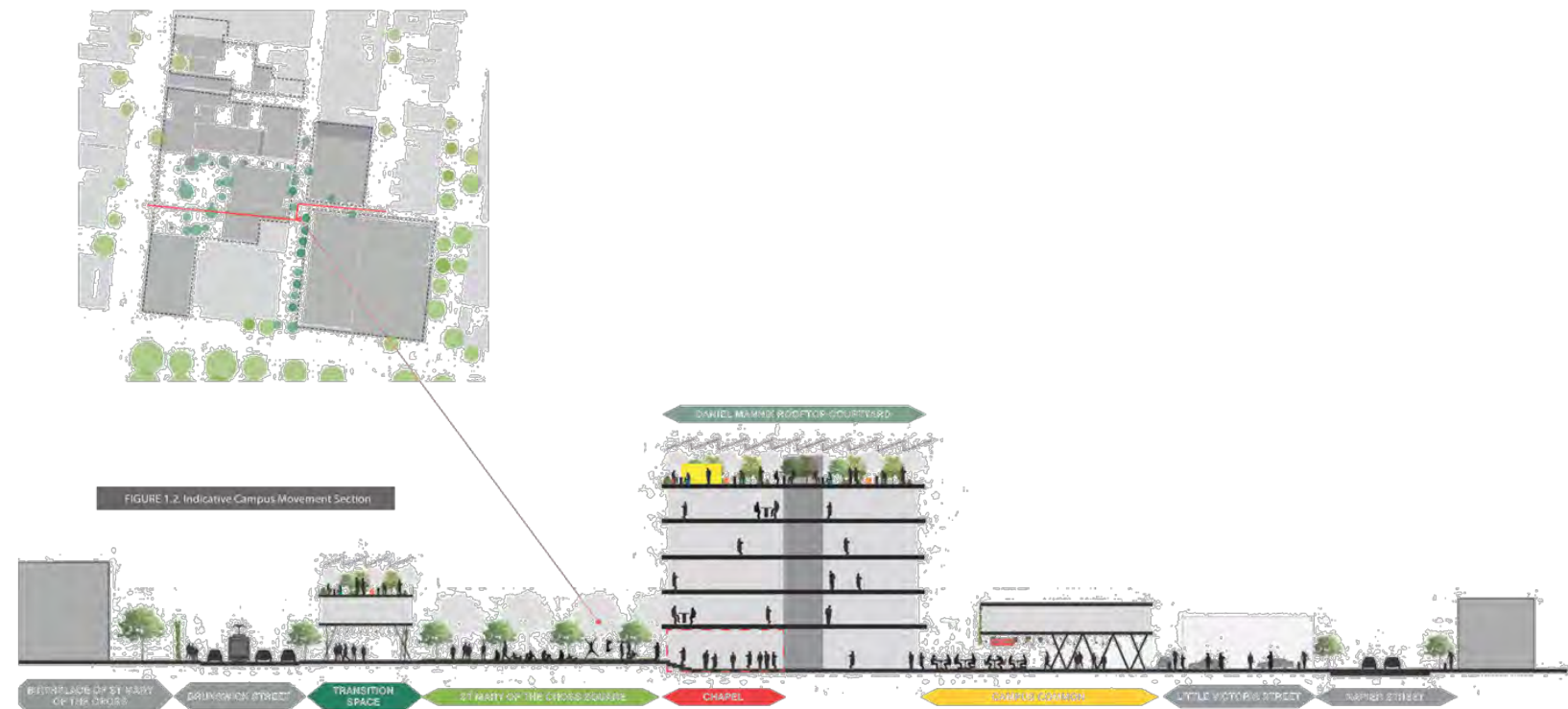
4.7 Overall Pedestrian Movement Plan

The new campus hub and new buildings with access edges will improve pedestrian movements around the campus and link the campus together and to the surrounding street network.



4.8 Pedestrian and Public Realm

It is proposed to make consequential improvements to pedestrian access throughout the Campus as well as the quality of the public realm to provide an attractive and safe environment that will integrate with the surrounding local precinct.



Attachment 4 - ACU Development Plan - Part 3



4.9 Overall Landscape and Open Space Plan and Principles

The proposed landscaping will connect the campus and meet safety principles to provide areas that are well used, have passive surveillance and provide attractive spaces for the public.

It is noted that any future public realm works will be undertaken in consultation with Council and that additional approvals from relevant authorities, including Council department outside of planning are required for public realm works. ACU acknowledges that public realm works associated with each future planning permit are likely to be subject to a condition that requires the works to be undertaken at the University's cost in consultation with Council.



Attachment 4 - ACU Development Plan - Part 3



4.10 Landscape Master Plan

CLIMATE AND SUSTAINABILITY FRAMEWORK

There are numerous linear corridors leading from the surrounding streets to the ACU Campus that may experience wind effects. Along with the over shadowing created by the surrounding buildings, this may result in an uncomfortable microclimate at times. There are however some open areas with better solar access and that are less likely to experience wind tunnel effects.

Design opportunities:

- Take advantage of the areas that are most likely to have a desirable microclimate and establish them as places for people to stop and stay.

There is also extensive hardscaping typical of university environments. Future design should minimise stormwater run-off and moderate discharge flows through on-site absorption and retention.

Provide opportunities for Water Sensitive Urban Design (WSUD) such as:

- Use of permeable channels and paving
- decorative garden beds that act as rain gardens
- collection and retention of roof rainwater run-off and re-use on canopy trees

ACCESS AND MOBILITY

The ACU campus will become a vibrant part of the local urban realm. It is presently fragmented from the surrounding road network with sections of the campus isolated and 'off limits'. The future design will have a high level of accessibility and mobility, and encourage pedestrian use through prominent and cohesive visual way-finding opportunities.

PRIMARY PEDESTRIAN PROMENADE (SUBJECT TO SEPARATE APPROVAL)

Young Street, Duke Street and St. Mary of the Cross Square will become the primary pedestrian area that will have provision for maintenance and service vehicle access. This promenade will act as the main circulation spine through the campus.

Design opportunities include:

- Medium canopy deciduous trees
- Feature paving
- Feature lighting including canopy lighting

- Short term seating areas
- WSUD opportunities including permeable paving and raingardens

SECONDARY PEDESTRIAN LINKS

The north end of Young Street and the west half of Little Victoria Street will become the secondary pedestrian area that will also have provision for maintenance and service vehicle access. These routes will act as links to the main circulation spine through the campus.

Design opportunities include:

- Planter boxes
- Vertical green walls
- Feature paving
- Feature lighting in pavement

TRANSITION ZONES

Transition zones are pedestrian cross over entry points that signify a change in urban neighbourhood such as from street realm to campus environment which could be either pedestrian only or shared conditions.

Design opportunities include:

- Use of pavement material to produce change
- Signage
- Colour
- Lighting

VIEWLINES

There are long narrow vistas to the campus from surrounding main roads such as Victoria Parade, Napier Street and Gertrude Street. ACU identity and way finding should be the primary design feature to make the campus legible and accessible.

Design opportunities include:

- Innovative features on campus building walls such as murals, vertical sculptures and green walls
- Use of signature signage
- Lighting
- Consolidation of wall materials used at pedestrian level to uniform space that act as a visual symbol representing the identity of the ACU campus

OPEN SPACE AND LANDSCAPE FRAMEWORK

ACU FORECOURT - YOUNG STREET

The generous open area surrounding the new Campus Hub will be composed of large flexible spaces and smaller, more intimate spaces. This forecourt will become the key social hub for the campus and will also be accessible to the public, making a positive contribution to the local public realm.

Design opportunities include:

- Active café precinct
- Creative use of paving lighting
- Landscape details such as seats, bins, lights and feature paving
- Retention of existing trees on Young Street, along with the addition of new trees (subject to soil volume constraints)
- Urban art such as sculptures
- Space for active recreation such as basketball half court
- Large and small grassed spaces

VICTORIA PARADE / NAPIER STREET ENTRY AREA

At the corner of Victoria Parade and Napier Street there is an urban edge that will reinforce the presence of the campus from commuters travelling from the east via a creative entry design.

Design opportunities include:

- Integrated seating areas that cater for short stops/meeting points
- Retention of most existing canopy trees, along with the addition of new trees
- Feature paving
- Signage
- Lighting

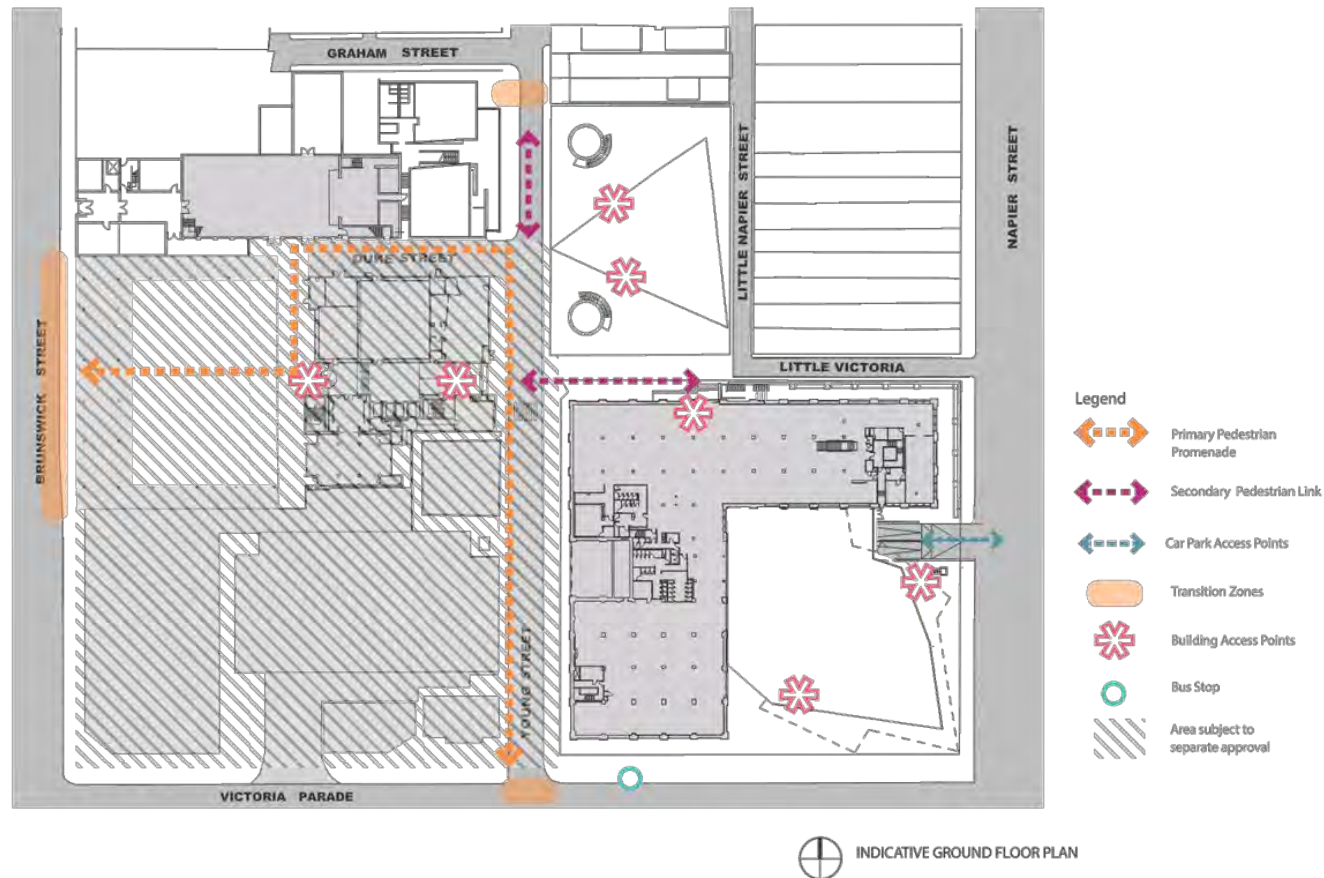
SAFER SPACES

The approach to the landscape design is to increase activity and maximise visual and physical connectivity through the spaces. Lighting will provide an inviting after hours environment and the structure of planting will ensure direct sight lines through low ground level planting and high canopy trees.

Attachment 4 - ACU Development Plan - Part 3



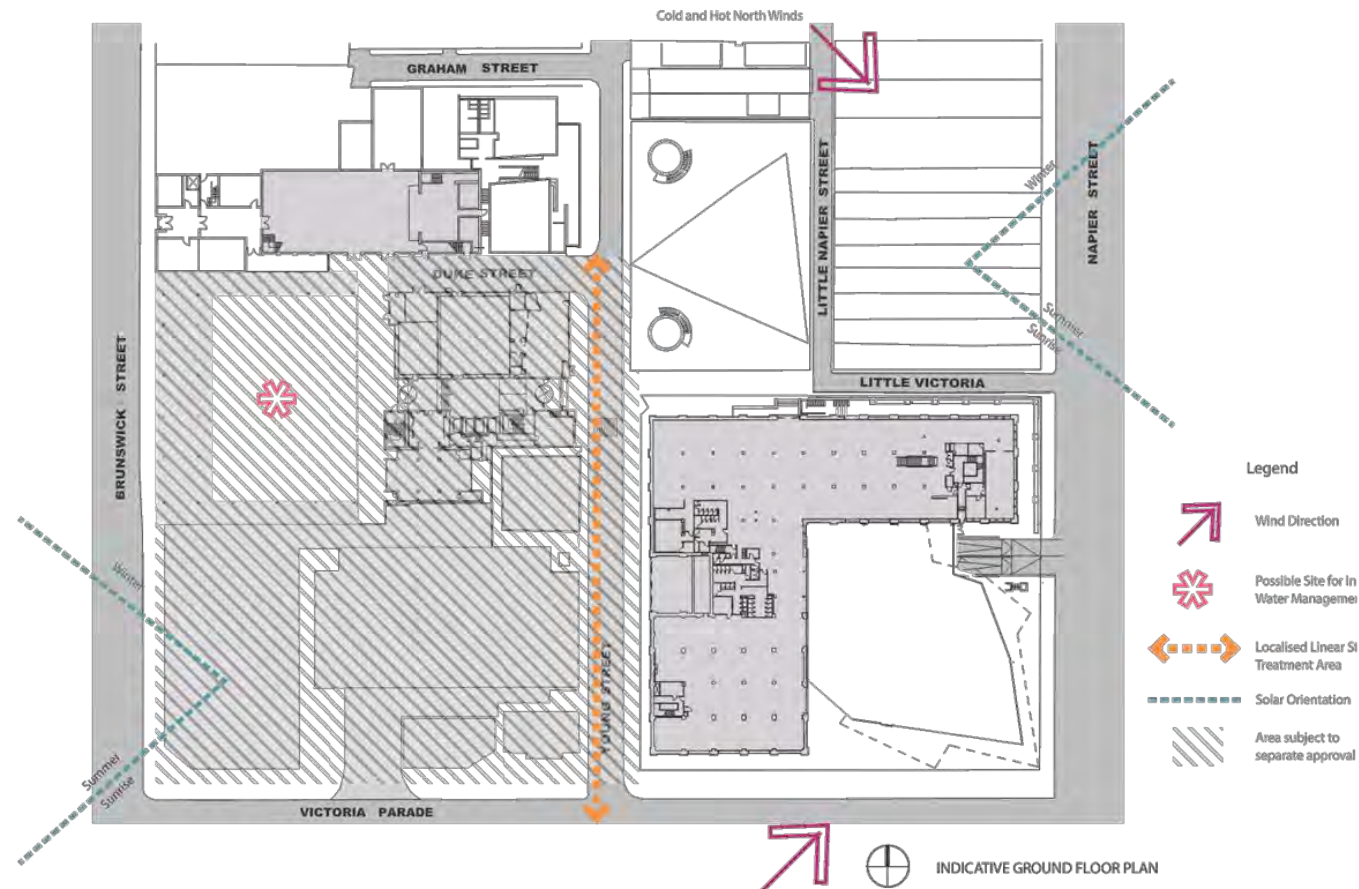
4.10.1 ANALYSIS PLAN 1 : ACCESS AND MOBILITY PLAN



Attachment 4 - ACU Development Plan - Part 3



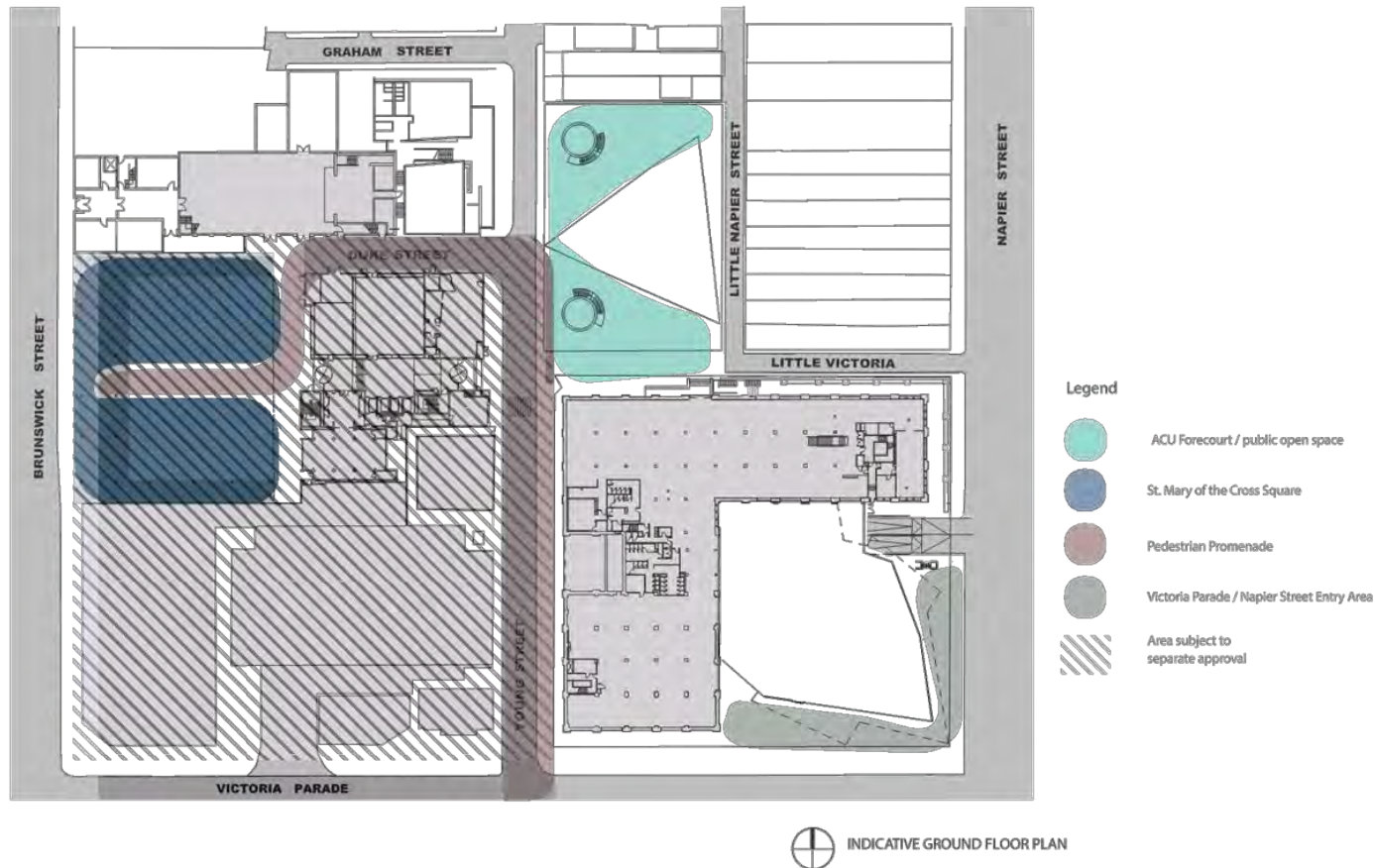
4.10.2 ANALYSIS PLAN 2 : CLIMATE AND SUSTAINABLE FRAMEWORK PLAN



Attachment 4 - ACU Development Plan - Part 3



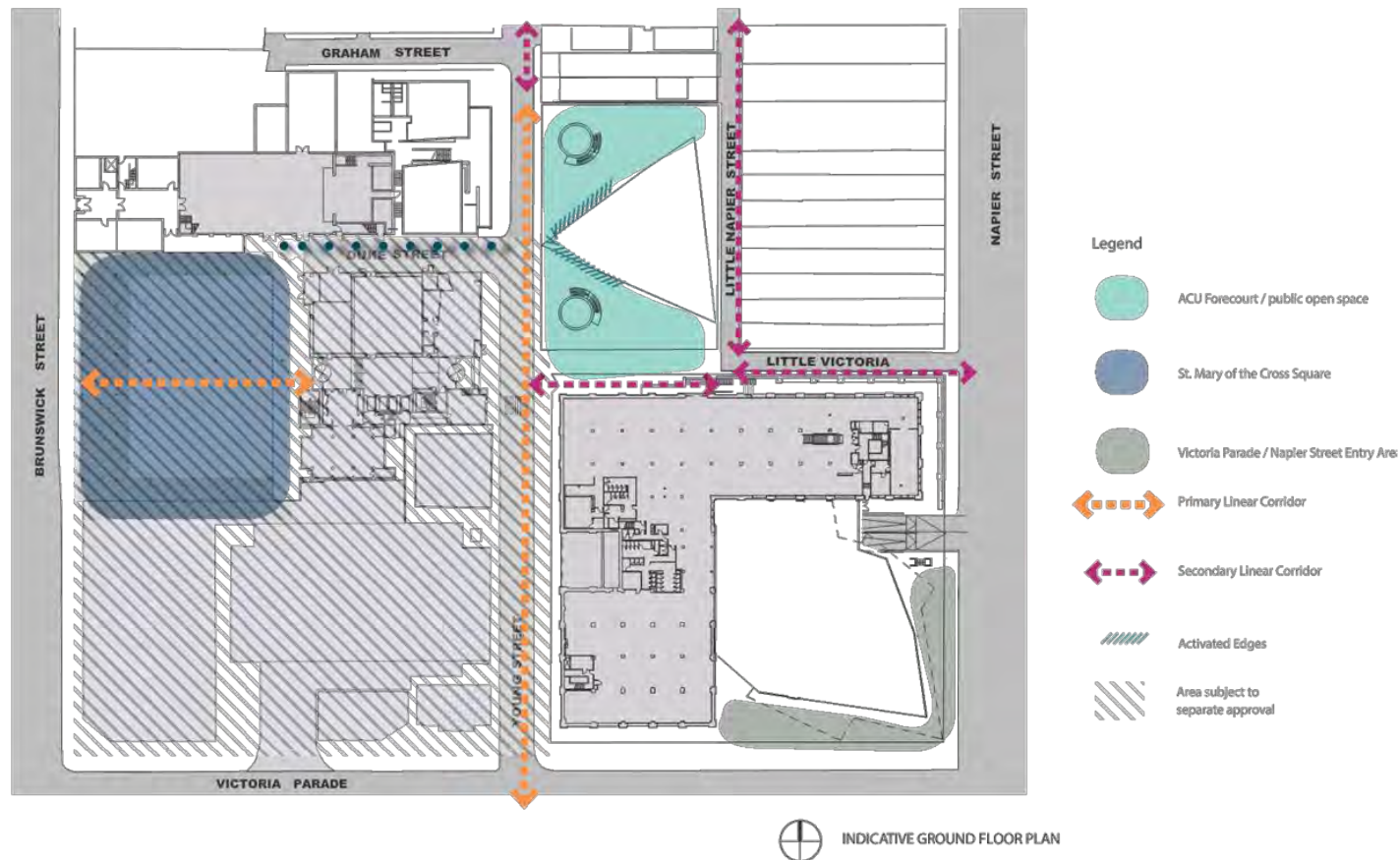
4.10.3 ANALYSIS PLAN 3 : INDICATIVE LANDSCAPE PLAN



Attachment 4 - ACU Development Plan - Part 3



4.10.4 ANALYSIS PLAN 4 : OPEN SPACE AND LANDSCAPE FRAMEWORK PLAN



Attachment 4 - ACU Development Plan - Part 3



4.10.5 LANDSCAPE CHARACTER

ACU FORECOURT

Intent

- To provide a welcoming, high technology hub in a campus style environment with distinctive landscaping.

Objectives

- To provide good, clear access from surrounding links using signage and lighting.
- To compliment the active edge with distinctive landscape features such as feature paving, urban art and thematic landscaping.
- To provide good, clear access from surrounding links.
- To provide views to the hub from the surrounding streets to reinforce its presence.
- To allow for active and passive recreation
- To be the key social hub for students as well as a public open space providing amenity for the local community.



PEDESTRIAN PROMENADE

Intent

To provide for a clear way finding identity through the campus set in a well landscaped environment and prominent connections to the surrounding streets.

Objectives

- To provide distinctive landscape features such as feature paving and thematic vertical landscaping including deciduous canopy trees.
- To incorporate WSUD opportunities including raingardens.
- To provide good, clear access from surrounding links.
- Provide views to the hub from the surrounding streets to reinforce its presence



Attachment 4 - ACU Development Plan - Part 3

VICTORIA PARADE / NAPIER STREET ENTRY AREA

Intent

To enhance the existing entry to the campus from Victoria Parade

Objectives

- To provide integrated seating areas that caters for short stops/meeting points.
- To provide distinctive landscape features such as feature paving and deciduous canopy trees.
- Retain the existing tree character of the streets



Attachment 4 - ACU Development Plan - Part 3



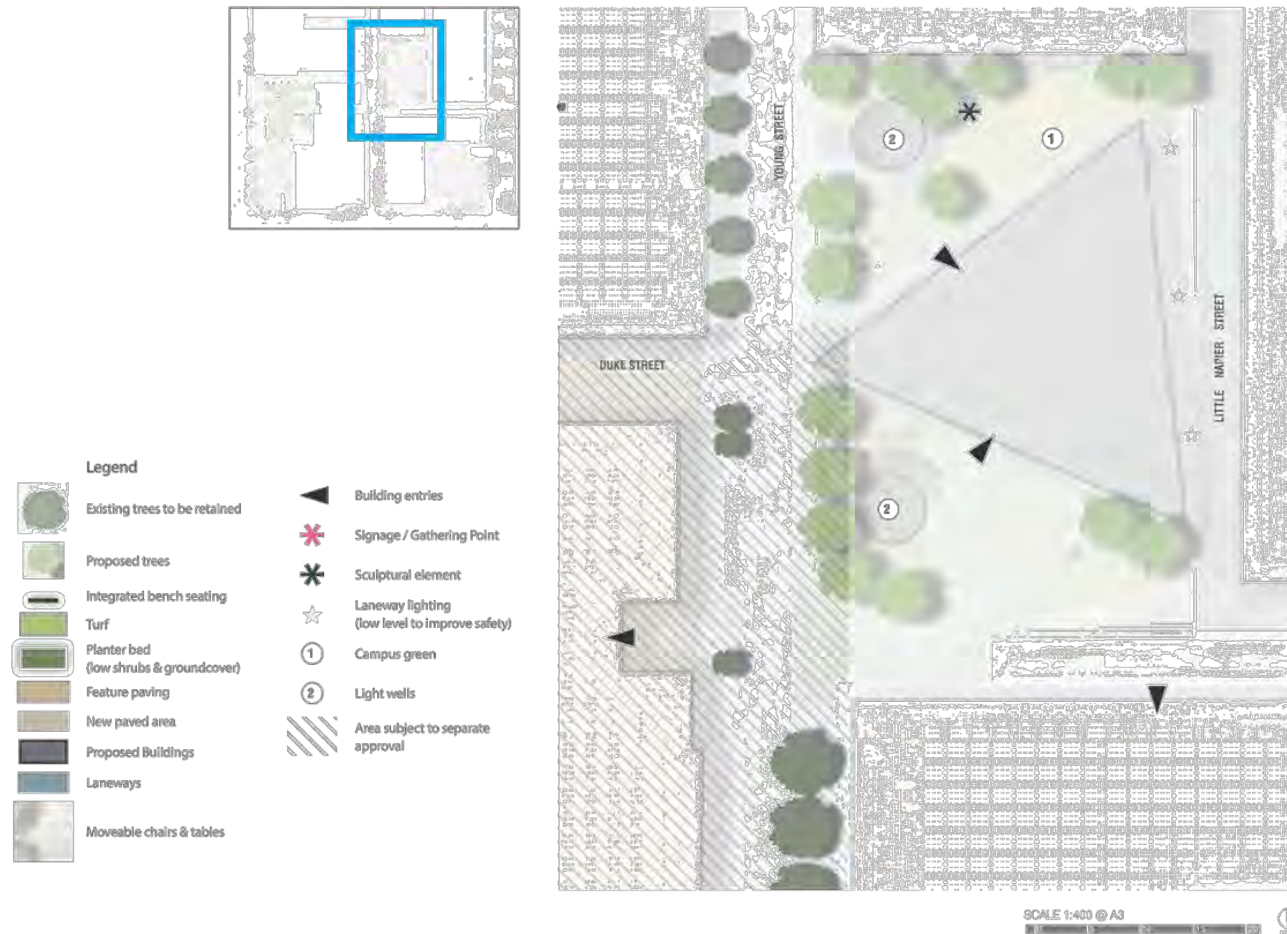
4.10.6 INDICATIVE LANDSCAPE MASTER PLAN



Attachment 4 - ACU Development Plan - Part 3



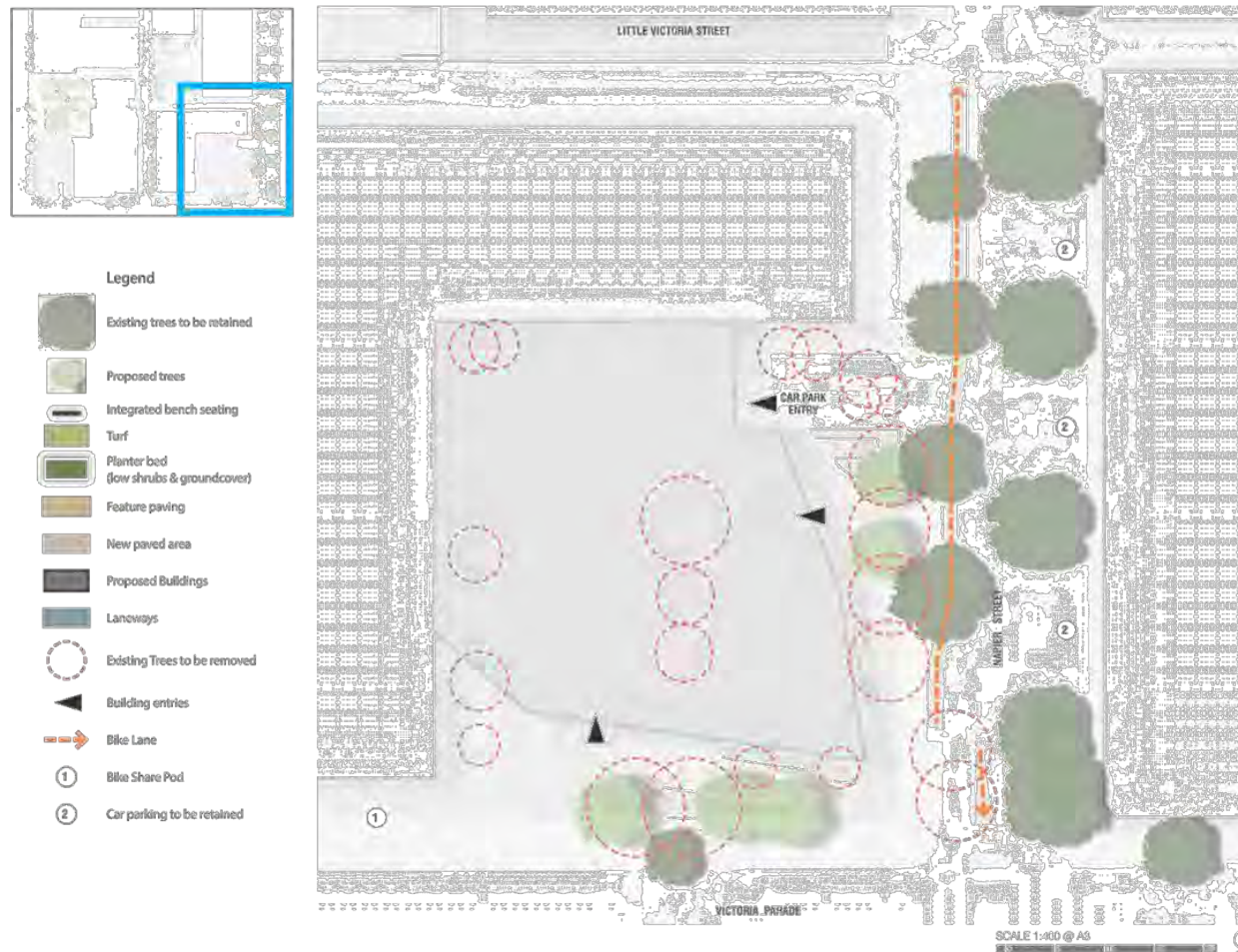
4.10.7 ACU FORECOURT / STUDENT HUB DETAIL PLAN



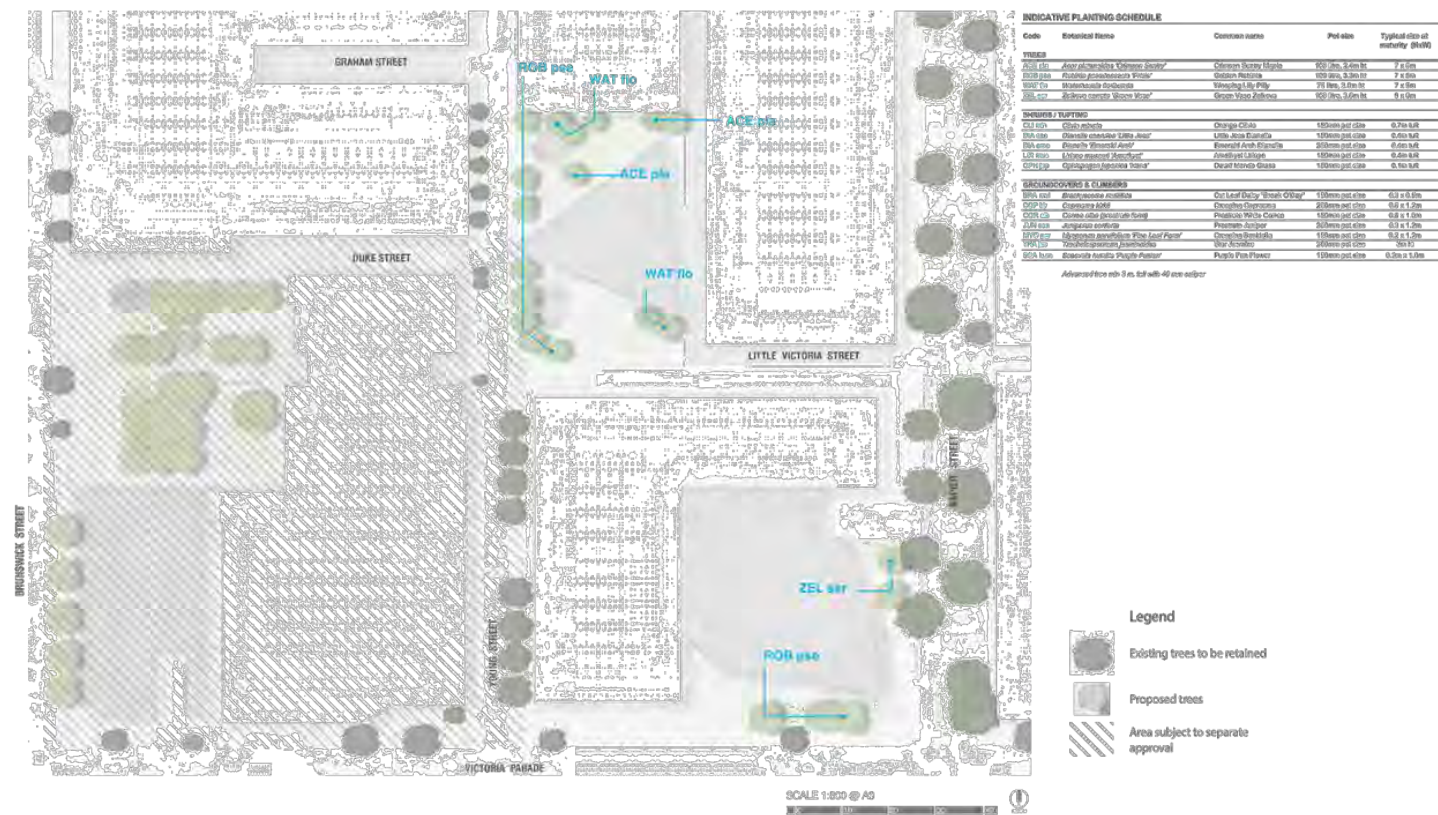
Attachment 4 - ACU Development Plan - Part 3



4.10.8 INDICATIVE VICTORIA PARADE ENTRY PLAN



4.10.9 INDICATIVE TREE SPECIES



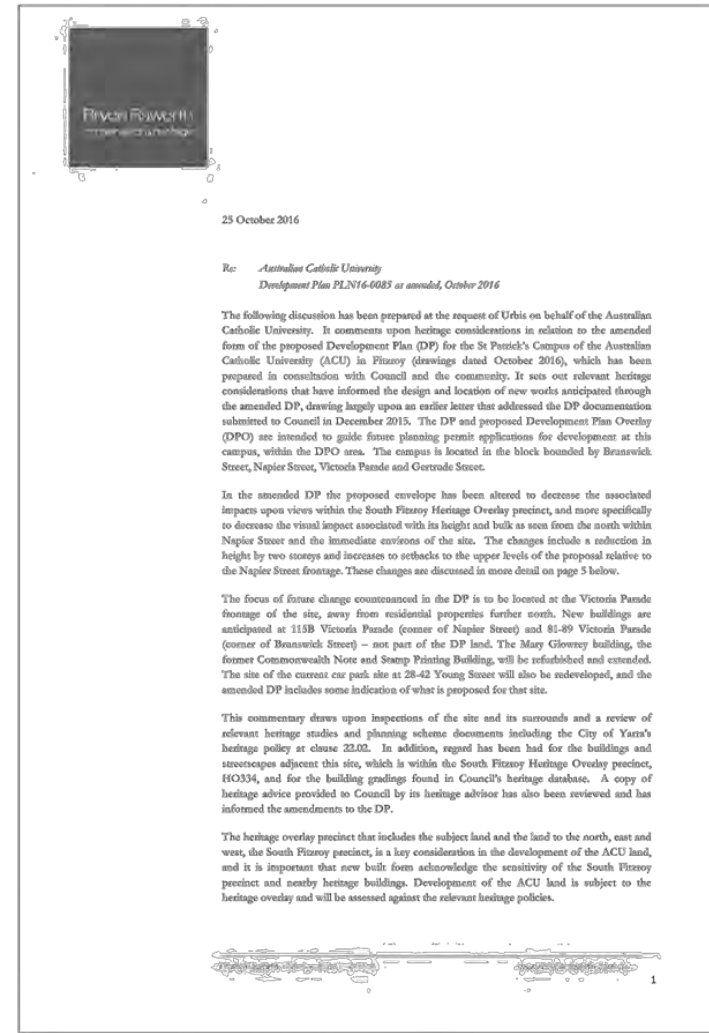
Attachment 4 - ACU Development Plan - Part 3



4.11 Heritage Matters

The heritage nature of particular built form both on campus and within the surrounding streetscape is an important consideration. Appropriate integration, design characteristics, and the interface to neighbouring heritage built form and streetscapes will be integral to the development.

The proposed building envelopes have been reviewed by heritage consultant Bryce Raworth who has provided the following assessment.



Attachment 4 - ACU Development Plan - Part 3



The statement of significance for HO334 in the *City of Yarra Heritage Review* (2007) states that:

The South Fitzroy Heritage Overlay Area is significant:

- *As the earliest urban area outside the Melbourne City grid to be settled in the Melbourne municipality, with several buildings from the mid nineteenth century surviving as testimony to its early establishment;*
- *For the unusually high number of early Victorian-era and some Regency period buildings, being generally simply detailed and a clear reflection of the early date of Fitzroy's settlement;*
- *As evidence of early government planning controls or Acts of Parliament, from the 1850s, that aimed to solve street alignment problems in this primarily planned suburb, arising from a hitherto lack of co-ordination between neighbouring allotment owners;*
- *As a good example of the successful application of the Act for Regulating Building and Party Walls, and for Preventing Mischief by Fire in the City of Melbourne (Melbourne Building Act 1849), which forced the use of fireproof construction and gave South Fitzroy a character distinct from other inner suburbs such as Richmond and Collingwood, that have a greater proportion of Victorian-era timber buildings;*
- *As a substantially intact collection of predominantly mid to late nineteenth and early twentieth century building stock, interspersed with well preserved inter-war residential, commercial, retail and industrial buildings that contribute to the historical character of the area;*
- *For the relatively large number of individually significant buildings, being predominantly solid masonry rather than clad with timber, largely as a result of the Melbourne Building Act, 1849;*
- *For the ornate and exuberant detail of many late nineteenth and early twentieth century buildings in the suburb, reflecting the affluence of many of the inhabitants of this area, particularly in the late 19th century;*
- *For the early street, lane and allotment layouts, some original bluestone kerbs, paving and guttering, and some mature exotic street trees, providing an appropriate setting for this collection of residential, retail, commercial and industrial buildings;*
- *For the landmark qualities of some large factory and warehouse buildings from the late 19th and earlier 20th century, such as the MacRobertson confectiory complex which are significant features in the skyline of this predominantly low rise suburb; and*
- *For the major early institutions that developed on its fringes, in particular, St Vincent's Hospital and The Convent of Mercy, as closely linked with the area's history, education and welfare within the metropolitan area.*

The statement of significance for the former Commonwealth Note and Stamp Printing Building at 115 Victoria Parade (now known as the Mary Glowrey building) is as follows:

What is significant?

The Commonwealth Note and Stamp Printing department ... was created in 1929-1931 for the Australian Commonwealth Government and has close historical associations with that important function.

The place has a fair integrity to its creation date (now altered occasionally, as conversion to university use took at rear and side).

Fabric from the creation date at the Commonwealth Note and Stamp Printing department, former is locally significant within the City of Yarra, compared to other similar places from a similar era.

How is it significant?

The Commonwealth Note and Stamp Printing department, former at 115 Victoria Parade, Fitzroy is historically significant to the locality of Fitzroy and the City of Yarra.

Why is it significant?

The Commonwealth Note and Stamp Printing department is significant as the former origin of all legal tender in Australia, reaching extensive publicity on its completion and with notably monumental Greek Revival styling.

It is to be noted that alterations to the former Commonwealth Note and Stamp Printing Building have been somewhat more than cosmetic – this statement of significance seems to predate a number of works that have been completed within the site, the additions to the southern facade and to the east elevation of the main, southern wing in particular. In addition, most of the windows have been replaced with simple modern glazing in place of the original steel framed windows. The northern wing has been similarly altered.



2

Yarra's heritage policy at Clause 22.02-5.7.1 provides detailed guidelines for new development and alterations and additions heritage overlay area, with key policy including the following:

Encourage the design of new development and alterations and additions to a heritage place or a contributory element to a heritage place to:

- *Respect the pattern, rhythm, orientation to the street, spatial characteristics, form, roof form, materials and heritage character of the surrounding historic streetscape;*
- *Be articulated and massed to correspond with the prevailing building form of the heritage place or contributory element to the heritage place;*
- *Be visually recessive and not dominate the heritage place;*
- *Be distinguishable from the original historic fabric;*
- *Not remove, cover, damage or change original historic fabric;*
- *Not obscure views of principal features;*
- *Consider the architectural integrity and context of the heritage place or contributory element.*

Encourage setbacks from the principal street frontage to be similar to those of adjoining contributory buildings where there are differing adjoining setbacks, the greater setback will apply.

Encourage similar facade heights in the adjoining contributory elements in the street. Where there are differing facade heights, the design should adopt the lower height.

Clause 22.02-5.7.2 encourages the following specific requirements with regard to corner sites and sites with dual frontages:

Encourage new building and additions on a site with frontages to two streets, being either a corner site or a site with dual street frontages, to respect the built form and character of the heritage place and adjoining or adjacent contributory elements to the heritage place.

Encourage new buildings on corner sites to reflect the setbacks of buildings that occupy other corners of the intersection.

This Clause also includes specific requirements for upper level additions to Industrial, Commercial and Retail Heritage Places or Contributory Elements:

Encourage new upper level additions and works to:

- *Respect the scale and form of the existing heritage place or contributory elements to the heritage place by being set back from the lower built form elements. Each higher element should be set further back from lower heritage built forms.*
- *Integrate treatments which make them less apparent.*

The extent of development proposed under the DP is substantial and will lead to appreciable change in the built form of the campus and associated change to views and character within adjoining streets. This said, the DP seeks to realise this substantial change in such a manner as to achieve an acceptable level of compliance with the heritage policy.

The VCAT approval for a fourteen storey development at the corner of Brunswick Street and Victoria Parade, ie 81-89 Victoria Parade (Premier Projects Pty Ltd v Yarra CC [2007]; VCAT reference no. P2540/2006), establishes a relevant benchmark for redevelopment along this part of Victoria Parade and within the ACU campus. While there are buildings along Victoria Parade within the context of the site that are of individual heritage significance, the streetscape to this part of Victoria Parade is of mixed character, is not of particular significance, and lends itself to higher development. The scale of current and proposed development to the west of ACU along Victoria Parade, at the St Vincent Hospital sites, and the scale of buildings to the south side of Victoria Parade, also suggest that built form of substantial scale may be considered in the Victoria Parade context.

The amended DP contemplates a new building of 12 storeys on the carpark site at the corner of Victoria Parade and Napier Street, known as 115B Victoria Parade. This is to be attached to the east side of the former Commonwealth Note and Stamp Printing Department Building at 115 Victoria Parade, and also to the south side of the northern wing of the Commonwealth Note and Stamp Printing Department Building.



3

Attachment 4 - ACU Development Plan - Part 3



The new building is to provide a new major point of entry to the campus, and will utilize the lift and stair 'core' attached to the east side of the heritage building. The existing glazed entry and disabled access ramp addition to the front of 115 Victoria Parade will be removed and that part of the facade reinstated.

The residential streetscapes to Napier Street to the east and north of the site at 115B Victoria Parade include heritage buildings of one and two storeys, with two storeys being predominant. It is a fine heritage street, particularly to the north of ACU. On this basis it is appropriate that new built form be very carefully designed to limit impact upon the character, appearance and significance of the streetscapes to Napier Street.

Young Street, located more easterly in relation to the ACU land holdings, is less significant at its southern end, already containing built form of scale, but includes a heritage building at the Victoria Parade corner, west side, and the former Commonwealth Note and Stamp Printing Department Building on the east corner.

The DP concept for ACU at 115B Victoria Parade proposes to:

- Remove the existing glazed entry and disabled access ramp from the front facade of 115 Victoria Parade.
- Consolidate the entry for both 115 and 117 Victoria Parade to create an active and prominent entry.
- Angle the entry and south elevation to 115B Victoria Parade back from the alignment of 115 Victoria Parade in order to reveal the robust corner detailing of 115 Victoria Parade. This corner will remain visible in views from Victoria Parade, with the angled form of the new works providing a 'setback' against the heritage building.
- Use a similar strategy to provide views to the eastern wing of the Mary Glowrey building (former Commonwealth Note and Stamp Printing Department).
- Integrate the floor plans of 115 Victoria Parade and 115B Victoria Parade, the new building being attached to a side of 115 Victoria Parade that has already been the subject of considerable modification and additions.
- The new built form will in part overhang 115 Victoria Parade, albeit with a degree of vertical separation that will enable the two forms to read separately.
- The building adopts a stepped profile to create a height relationship with the built form along Napier Street.
- An addition of three levels to the rear part of the northern wing of the Mary Glowrey building (former Commonwealth Note and Stamp Printing Department), will set back from Napier Street in accordance with heritage policy for upper level additions to industrial buildings.

The existing car park building at 28-42 Young Street will be demolished and replaced with a low rise building that incorporates an open plaza or community space at ground level. The development of this part of the campus is not seen to raise heritage issues insofar as it is in a highly altered environment with negligible heritage fabric to its streetscape.

Having regard for the DP, it is apparent that the anticipated works will result in substantial change and to some extent impacts upon heritage values and views, relative to both the former Commonwealth Note and Stamp Printing Department Building and the streetscapes extending to the north along Napier Street and to the east along Victoria Parade. This said, the DP seeks to limit these impacts through focusing change within the already altered environment of Victoria Parade, at the very southern edge of the South Fitzroy precinct.

As noted, a number of changes introduced in the amended design seek to limit the appearance of scale and built form in terms of views from the north, particularly in Napier Street. The view analysis provided for the amended DP compares the massing of the form as originally contemplated in 2015, the massing of the form as revised in September 2016, and a indication of the architectural treatment that may accompany the revised massing.



The key amendments to the DP scheme in relation to heritage considerations are as follows.

Reduction in height

The DP scheme submitted to Council in December 2015 was of 14 storeys plus plant. The present scheme is reduced to 12 storeys plus plant. The reduction in height result in a lessening of the visibility and visual impact of the proposal in terms of the view analysis provided in the DP documentation. This is apparent in each of the views assessed within the documentation, including views along Napier Street from the north. The documentation shows that in these views from the north the building is not only apparently lower in terms of the number of storeys per se, but that the plant level is also less visible, further diminishing the apparent bulk of the envelope in these views.

Podium design along Napier Street

The podium to Napier Street is effectively of 6 storeys, albeit with the Ground and Lower Ground floors being relatively open and setback. The treatment of the podium to Napier Street is not substantially changed, although the setback has increased at ground floor relative to the south face of the northern wing of the former Commonwealth Note and Stamp Printing Department Building, and marginally decreased at the floors above. However, it is acknowledged that the podium should respond to some degree to the streetscape condition of buildings that are aligned with the property boundaries.

Setbacks of higher levels from Napier Street

The upper levels of the proposal have generally been increased relative to Napier Street, particularly in terms of levels 6 & 7 (and above). At levels 6 & 7 the setbacks are essentially twice those of levels 7-9 in the previous concept, with the minimum dimension from Napier Street increasing from 8.28m to 17.1m. The various alignments of the faces of the upper levels have also changed, further reducing the visual bulk in terms of views along Napier Street. The built form remains visible, but is viewed as being appreciably further from the alignment of the street than in the earlier scheme.

Additional structure to support new levels above northern wing of the former Commonwealth Note and Stamp Printing Department Building

The DP concept has consistently included an addition of 3 storeys (plus plant and undercroft space) above the northern wing of the former Commonwealth Note and Stamp Printing Department Building. The setbacks of this built form have been increased relative to Napier Street, and the modelling has altered. These changes help limit visual impact in terms of views from the north.

This said, an element of the scheme that was not previously contemplated is the provision of additional structure to the exterior of the host building to assist in supporting the new levels. The additional structure will comprise new piers or columns that will be attached to the existing vertical structure of the host building, in a manner that is intended to complement rather than disrupt the articulation and character of that building. The customised pier carries angled struts that support an overhanging section of the new envelope. This new structure is to be sited well back from Napier Street and will result in minimal change to the character and appearance of the additions as seen from the north within Napier Street.

Details and materials

The DP scheme includes an indication of how the architecture of the proposal might be resolved. This is essentially limited to an indicative arrangement of glazing and walls, with some suggestion of a pale colour or white as the primary finish to greater part of the building. The architects have documented a range of indicative materials illustrating options that may be considered as the design is further refined. This said, the materials, finishes and colours have not been determined at this stage, and will be subject to further amendment and refinement as part of a future permit application process.



Attachment 4 - ACU Development Plan - Part 3



Conclusion

The original DP scheme of December 2015 was developed having regard for the potential for impacts upon the setting of the former Commonwealth Note and Stamp Printing Department Buildings and upon views within Napier Street and Victoria Parade, having regard for the location at Victoria Parade and taking a 'whole of planning' view that accepts the importance of ACU within this precinct and within the City of Yarra. In the present amendments to the form and scale of the built form anticipated in the DP, a concerted attempt has been made to further limit impacts upon buildings, views and the broader heritage overlay precinct, while still seeking to achieve ACU's requirements.

Bryce Raworth and Carolynne Baker



Attachment 5 - ACU Development Plan - Part 4



4.12 Possible ESD Principles

ACU promotes a holistic view of sustainability and embraces the need to be a leader, a good neighbour and a global citizen. The following principles summarise the way that ACU and their project team will progress the projects within the Development Plan.

PURSUE THE COMMON GOOD

Part of ACU's mission is to pursue the common good. Within the context of this Development Plan, this is translated as the following:

- Demonstrating environmental stewardship through the application of recognised frameworks, such as the City of Yarra's Built Environment Sustainability Scorecard (BESS) and Green Star.
- Being a place-maker by providing spaces for community use
- Protecting heritage value through considered and sensitive design

ENHANCING USER EXPERIENCE

The success of education buildings relies in part on the user experience they provide. In this case, users include students, academic and administrative staff, and the community. Research has shown that our local environment can influence our health, wellbeing and productivity. As such, the Development Plan will enhance the user experience through:

- High quality daylight and artificial lighting
- Good indoor air quality
- Good acoustics
- High levels of thermal comfort
- Desirable external spaces and microclimates (wind, rain, sun, heat island etc)
- Opportunities to connect to nature
- Cyclist facilities

PROGRESSING WITH CONFIDENCE

We live in an uncertain world, with a changing climate, increasing rate of change in pedagogy and student needs, and rising utility prices. Through design and project processes such as those listed below, the projects within the Development Plan will give ACU, the City of Yarra and surrounding community confidence.

- Design for future climate
- Flexible and adaptable buildings and spaces
- Efficient consumption, onsite generation of energy, and capture and reuse of water
- Quality control in design, construction and operation (e.g. planning and building code compliance, as-built green star rating, commissioning, tuning, metering and monitoring)

SAVING MONEY

Sustainability initiatives provide an opportunity for ACU to save money upfront and in the long term. The projects within the Development Plan will be vigilant for opportunities to reduce costs, enabling ACU to focus its spending in higher value areas.

- Energy efficiency leading to smaller plant (chiller, substation etc) size
- Durable materials and finishes
- Appropriate access for maintenance
- Efficient consumption, onsite generation of energy, and capture and reuse of water

BESS CATEGORY	CREDIT	CREDIT SUMMARY	DEVELOPMENT PLAN RESPONSE
Sustainability	Pre-application meeting		To be part of building design stage.
	Thermal performance modelling	Preliminary Section J glazing assessment undertaken.	To be part of building design stage.
	Building users guide	A building users' guide be produced and issued to occupants.	To be part of building design stage.
Energy	Greenhouse Gas Emissions	Reduction in emissions compared to a code compliant reference building	To be part of building design stage. The development plan notes that appropriate solar controls, such as external shading and screens, expect to be added to the façade as part of design optimisation.
	Peak Demand	Has the instantaneous (peak-hour) demand been reduced by >20%.	As for Greenhouse Gas Emissions above.
	Energy consumption (Electricity and Gas)	Is the annual electricity consumption >10% below the benchmark Is the annual gas consumption >10% below the benchmark?	As for Greenhouse Gas Emissions above.
	Car Park Ventilation	If you have a basement carpark is it either: (a) fully naturally ventilated (no mechanical ventilation system), or (b) use Carbon Monoxide monitoring to control the operation and speed of the ventilation fans?	To be part of building design stage. Car park ventilation will use carbon monoxide control
	Domestic Hot Water	Does the hot water system use >10% less energy (gas and electricity) than the reference case?	To be part of building design stage.
	External lighting	The external lighting is controlled by a motion detector.	To be part of building design stage.
	Internal lighting	The maximum illumination power density (W/m2) in at least 90% of the relevant Building Class is at least 20% lower than required by Table J6.2a of the NCC BCA (2013) Volume 1 Section J (Class 2 to 9).	To be part of building design stage. All external lighting to be controlled by PE sensors and time clocks.
Water	Rainwater capture and reuse	Installation of rainwater tanks and reticulation of captured rainwater for reuse (e.g. toilet flushing, irrigation).	To be part of building design stage. Space for rain water storage has been allowed for within the basement as part of the building massing.
	Fixtures and fittings	WELS rating of water fixtures, fittings and connections.	To be part of building design stage.
	Water efficient landscaping	Water efficient landscaping is installed.	To be part of building design stage.
	Fire test system water	Measures have been taken to reduce water consumption when testing fire safety systems.	To be part of building design stage. Space for capture and reuse of fire system test water has been allowed for within the basement as part of the building massing.
Sustainability	Stormwater treatment	Best practice stormwater management is demonstrated.	As for Rainwater capture and reuse above. The overflow from the rainwater tank will have appropriate treatment to improve the quality of discharged stormwater quality.
Daylight	Daylight access (non-residential)	Percentage of the nominated area has at least 2% daylight factor.	To be part of the building design stage.

Attachment 5 - ACU Development Plan - Part 4



BESS CATEGORY	CREDIT	CREDIT SUMMARY	DEVELOPMENT PLAN RESPONSE
Transport	Bicycle parking – non-residential	The planning scheme requirements for employee bicycle parking been exceeded by at least 50%.	Refer to section 4.6 for proposed bicycle parking facilities.
	Bicycle parking – non-residential visitor	The planning scheme requirements for visitor bicycle parking been exceeded by at least 50%.	Refer to section 4.6 for proposed bicycle parking facilities.
	End of trip facilities – non-residential	End of trip facilities provided with: 1 shower for the first 5 bicycle spaces plus 1 for each 10 bicycles spaces thereafter. Changing facilities adjacent to showers. One secure locker per required bicycle space in the vicinity of the changing / shower facilities.	End of trip facilities will be provided across the campus, the detail will follow at the planning permit stage.
	Electric vehicle infrastructure	Facilities for the charging of electric vehicles are provided.	Refer to Integrated Transport and Access Plan in appendices
	Car share scheme	A formal car sharing scheme has been integrated into the development.	Refer to Integrated Transport and Access Plan in appendices
	Motorbikes/mopeds	A minimum of 5% of vehicle parking spaces are designed and labelled for motorbikes (must be at least five motorbike space).	Refer to Integrated Transport and Access Plan in appendices
Waste	Construction waste management	There is a commitment to re-use and recycle construction & demolition waste.	To be part of building design stage.
	Building re-use	If the development is on a site that has been previously developed, at least 30% of the existing building has been re-used.	Existing building is being retained in full.
	Food & garden waste	Facilities are provided for on-site management of food and garden waste.	To be considered as part of building design stage.
	Convenience of recycling	The recycling facilities are at least as convenient for occupants as facilities for general waste.	Spatial allowance has been considered as part of the Development Plan
Urban Ecology	Communal spaces	There is at least the following amount of common space (m ²): 1m ² for each of the first 50 occupants, an additional 0.5m ² for each occupant between 51 and 250, and an additional 0.25m ² for each occupant above 251.	To be part of building design stage.
	Vegetation	Percentage of the ground area of the site that is covered with vegetation.	Refer to landscape master plan
	Green roof	The development includes outdoor terraces.	To be considered as part of the building design stage.
	Green wall or façade	The development incorporates a green wall or façade.	To be considered as part of building design stage.
	Food production – non-residential	There is at least 0.25m ² of space per occupant dedicated to food production.	To be considered as part of building design stage.

Attachment 5 - ACU Development Plan - Part 4



5.0 CONCLUSION

The Development Plan has been developed in line with ACU's Vision for the St Patrick's Campus and will guide key developments within the Development Plan Overlay area for the coming decade.


The Development Plan will guide development of the parts of ACU subject to the Development Plan Overlay in accordance with the Yarra Planning Scheme.

Through the Development Plan the projects will improve the public realm through providing new landscaping opportunities, open space and pedestrian connectivity through the campus. The new building projects will continue ACU's tradition of providing excellent teaching and learning spaces.


Attachment 5 - ACU Development Plan - Part 4



Attachment 5 - ACU Development Plan - Part 4




Integrated Transport and Access Plan Australian Catholic University, St. Patricks Campus, Fitzroy



Integrated Transport and Access Plan

Australian Catholic University, St. Patricks Campus, Fitzroy

Prepared for Australian Catholic University
27 October 2016



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Contact Information

Cardno Victoria Pty Ltd
Trading as Cardno
ABN 47 106 610 913

Level 4
501 Swanston Street
Melbourne
Victoria 3000 Australia

Telephone: (03) 8415 7777
Facsimile: (03) 8415 7788
International: +61 3 8415 7777

victoria@cardno.com.au
www.cardno.com

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- Appendix A Concept Functional Layout Plans
- Appendix B Bicycle Parking Products

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Integrated Transport and Access Plan Australian Catholic University, St. Patrick's Campus, Fitzroy

1 Introduction


1.1 Background

A Development Plan is being prepared for the Australian Catholic University (ACU) St Patrick's Campus to deliver a mixed use development to accommodate the planned growth in students, teachers and research activities, improve the quality of teaching and learning environments and improve the pedestrian and public environments for students and the wider community.

To accompany the Development Plan, Cardno was engaged by Australian Catholic University to prepare an Integrated Transport and Access Plan (ITAP). The subject site comprises a number of buildings generally located to the north of Victoria Street between Brunswick Street and Napier Street, as shown in Figure 1-1.

The site is located within the suburb of Fitzroy and is situated approximately 500 metres to the east of the Melbourne Central Activities District (CAD).

Figure 1-1 Site Location




Schedule 2 to the Development Plan Overlay (DPO) (or Schedule 2 to Clause 43.04 of the Yarra Planning Scheme) applies to the subject site and provides criteria for any future use and development of the land at 115 Victoria Parade and 20-23 Brunswick Street, Fitzroy.

Schedule 2 to the DPO is dated 19 January 2006 and provides the following requirements for the Development Plan which are considered relevant to transport:

- > The development plan must show:
 - The number, location, dimensions and layout of all car parks and access ways to and from them;
 - A management plan for the operation and maintenance of the car park areas;
 - The location and dimensions of all bicycle, vehicle and pedestrian ways;
 - A traffic management plan which must show any traffic management and traffic control works considered necessary in adjoining and nearby roads when the development is completed; and
 - The means of vehicular and pedestrian ingress to and egress from the land.

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Integrated Transport and Access Plan Australian Catholic University, St. Patrick's Campus, Fitzroy

1.2 Purpose

The purpose of this report is to provide an integrated transport approach to any future use and development at ACU.

This report considers all modes of transport, with a view to updating and incorporating a more integrated transport approach to the transport related requirements in Schedule 2 to the DPO.

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Integrated Transport and Access Plan Australian Catholic University, St. Patrick's Campus, Fitzroy

2 Existing Conditions

2.1 Population and Employment

ACU currently operates with 10,000 effective full time (EFT) students and approximately 800 staff.

2.2 Mode Share and Journey to Work

Relevant journey to work and journey to education data has been sourced from the 2011 ABS Census and the Victorian Integrated Survey of Travel and Activity (VISTA) which is an ongoing survey of travel and activity that helps the government make better transport and land-use planning decisions.

Table 2-1 shows journey to work data based on work destinations for the suburb of Fitzroy from the 2011 ABS Census and for the City of Yarra from the VISTA 09 surveys.

Table 2-1 Journey to Work Data Based on Work Destination

Travel Mode	2011 ABS Census Suburb of Fitzroy	VISTA 09 City of Yarra
Car Based	53%	57%
Public Transport	32%	30%
Bicycle/Walking	15%	13%

Table 2-2 shows journey to education data based on education destinations for RMIT in Melbourne and the Kangan Institute of TAFE in Crenorne. The results are noticeably different, with significantly higher public transport use at RMIT and higher car and bicycle/walking use at Kangan Institute of TAFE.

Table 2-2 Journey to Education Data Based on Education Destination

Travel Mode	VISTA 09 RMIT, Melbourne City of Melbourne	VISTA 09 Kangan Institute of TAFE, Crenorne City of Yarra
Car Based	10%	37%
Public Transport	67%	30%
Bicycle/Walking	23%	33%

The site is more similarly located to RMIT with respect to its public transport accessibility and proximity to the CAD, however on-site parking at ACU is more readily available.

In this respect, it is anticipated that car use would be in between the results for RMIT and Kangan Institute of TAFE.

This is confirmed by results from a questionnaire survey that was distributed by ACU staff in May and August 2009.

Approximately 800 surveys were completed and returned, with the results of the survey summarised in Table 2-3, Table 2-4 and Table 2-5 overleaf.

The data collected by the University reveals that a large contingent of the existing student and staff population either catch public transport or walk or cycle to the facility.

Table 2-3 Student Travel Modes

Travel Mode	Number	Percentage
Car (Driver)	122	19%
Car (Passenger)	38	6%
Public Transport	445	68%
Walked	31	5%
Cycled	17	2%
Total	653	100%

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Table 2-4 Staff Travel Modes

Travel Mode	Number	Percentage
Car (Driver)	62	34%
Car (Passenger)	12	7%
Public Transport	80	44%
Walked	15	8%
Cycled	8	4%
Other	3	2%
Total	180	100%

Table 2-5 University Travel Modes

Travel Mode	Number	Percentage
Car (Driver)	184	22%
Car (Passenger)	50	6%
Public Transport	525	63%
Walked	46	6%
Cycled	25	3%
Other	3	0%
Total	833	100%

2.3 Pedestrian Network

The existing pedestrian network in the vicinity of the site includes footpaths on both sides of Brunswick Street, Young Street and Napier Street providing access towards Gertrude Street and Victoria Street to the north and south respectively, both of which are also provided with footpaths along both sides.

Victoria Street is also provided with footpaths along both sides of the central median.

Pedestrian crossings are provided at all signalised intersections, including:

- > Brunswick Street / Gertrude Street;
- > Brunswick Street / Victoria Street / Gisborne Street; and
- > Victoria Street / Lansdowne Street.

At all other intersections, pedestrian crossings are facilitated by pram ramps.

Tram stops associated with tram routes 11, 30, 86, 109 and 120 are readily accessible from the subject site via the pedestrian network, whilst Parliament Train Station is also readily accessible via footpaths along both sides of Gisborne Street.

The pedestrian network in the vicinity of the site also provides access towards neighbouring suburbs, including Collingwood, Fitzroy North, Carlton, East Melbourne and the Central Activities District (CAD).

Key pedestrian links in the vicinity of the site is shown graphically in Figure 2-1, whilst Figure 2-2 shows the pedestrian links within the campus and their connections to the key pedestrian links.

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Integrated Transport and Access Plan Australian Catholic University, St. Patrick's Campus, Fitzroy

Figure 2-1 Key Pedestrian Links



Figure 2-2 Campus Pedestrian Links



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Integrated Transport and Access Plan Australian Catholic University, St. Patrick's Campus, Fitzroy

2.4 Bicycle Network

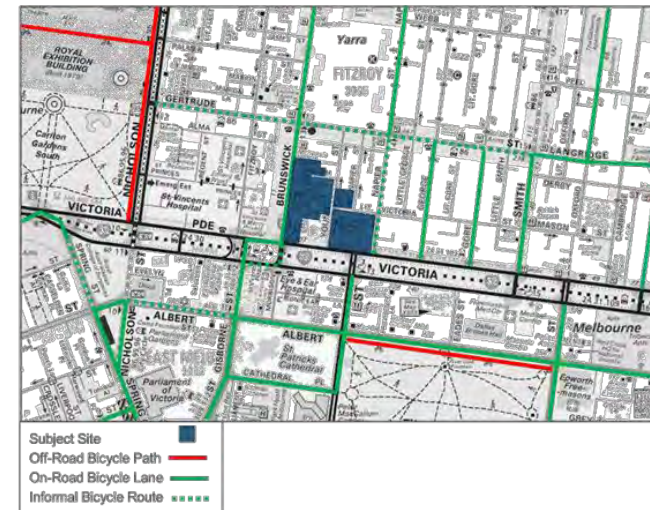
The subject site is well serviced by Melbourne's extensive bicycle network, with numerous off-road paths, on-road bicycle lanes and informal bicycle routes (generally designated by painted bicycle symbols and the provision of bicycle storage boxes at signalised intersections).

As shown in Figure 2-3, on-road bicycle lanes on Brunswick Street provide access towards Fitzroy North to the north and towards the CAD to the south via Gisborne Street.

The informal route along Gertrude Street provides access towards Collingwood to the east and towards Carlton to the west via the off-road path between the Royal Exhibition Building and the Melbourne Museum.

Additional on-road bicycle lanes along key streets, including Napier Street, Albert Street, Clarendon Street and Wellington Street provide further connections to surrounding suburbs, including East Melbourne and Richmond.

Figure 2-3 Existing Bicycle Network



It is noted that to improve safety for cyclists that traverse between Napier Street and Lansdowne Street, new traffic signals have been installed on the departure side (eastbound) of the Victoria Parade / Lansdowne Street intersection.

Council have also recently installed bike share pods along Napier Street, proximate to its intersection with Victoria Parade, and we understand that they also plan to install an additional bike share pod at the Fitzroy Town Hall.

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Integrated Transport and Access Plan Australian Catholic University, St. Patrick's Campus, Fitzroy

2.5 Public Transport

The subject site has excellent access to public transport, with all train lines accessible via Parliament Station approximately 550 metres walking distance from the subject site and numerous bus and tram routes operating either along the site frontage or in close proximity to the subject site.

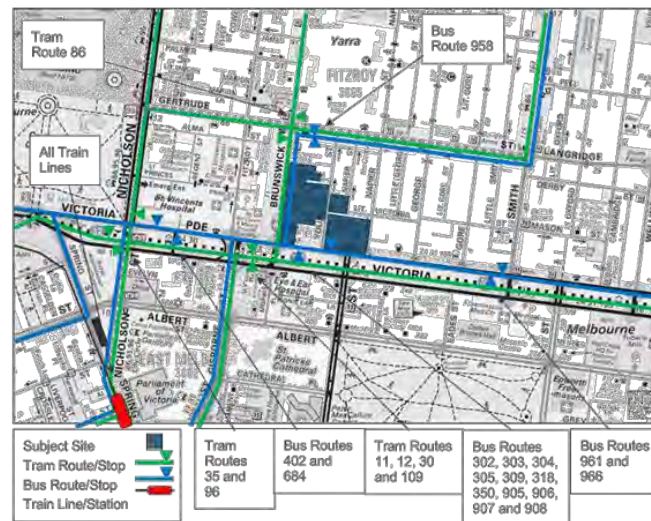
The St Vincents Plaza / Victoria Parade tram stop and the Australian Catholic University / Victoria Parade bus stop are located directly adjacent to the site and provide access to a total of 4 tram routes and 11 bus routes. Parliament Station is also accessible via tram routes 11, 12 and 109 which are serviced by the abovementioned St Vincents Plaza / Victoria Parade tram stop.

Tram lines along Victoria Street, Nicholson Street and Gisborne Street are separated from the traffic lanes, whilst the tram lines along Gertrude Street, Brunswick Street and Smith Street are shared with a traffic lane.

Notably, bus lanes have recently been installed along Victoria Parade to provide bus priority through this corridor. It is also understood that plans are being progressed to upgrade the Brunswick Street tram corridor, with the works including the provision of DDA compliant stops.

The public transport services in the vicinity of the site are summarised in Table 2-6 and illustrated graphically in Figure 2-4.

Figure 2-4 Public Transport Map



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Integrated Transport and Access Plan Australian Catholic University, St. Patrick's Campus, Fitzroy

Table 2-6 Public Transport Services

Service	Route No's	Route	Nearest Stop	Approximate Walking Distance
Tram	11	West Preston – Victoria Harbour Docklands	St Vincents Plaza/Victoria Pde	Adjacent Site
	12	Victoria Gardens – St Kilda		
	30	St Vincents Plaza – Docklands via La Trobe St		
	109	Box Hill – Port Melbourne		
Bus	88	Bundoora RMIT – Waterfront City Docklands	Brunswick St/Gertrude St	100 m
	35	City Circle (Free Tourist Tram)	Nicholson St/Victoria Pde	300 m
	96	East Brunswick – St Kilda Beach		
	302	Box Hill via Belmont Rd and Eastern Fwy	Australian Catholic University / Victoria Pde	Adjacent Site
	303	City – Ringwood North via Park Rd		
	304	City – Doncaster SC via Belmont Rd and Eastern Fwy		
	305	City – The Pines SC via Eastern Fwy		
	309	City – Donvale via Reynolds Rd	St Vincents Hospital / Victoria Pde	200 m
	318	City – Deep Creek		
	350	City – La Trobe University via Eastern Fwy		
	905	City – The Pines SC via Eastern Fwy, Templestowe (SMARTBUS Service)		
	906	City – Warrandyte via The Pines SC (SMARTBUS Service)	Brunswick St / Gertrude St	100 m
	907	City – Mitcham via Doncaster Road (SMARTBUS Service)		
	908	City – The Pines SC via Eastern Fwy (SMARTBUS Service)	Smith St / Victoria Pde	300 m
	402	Footscray – East Melbourne via North Melbourne		
	684	Eldon – Melbourne via Lilydale Station	Parliament Station	550 m
	958	NightRider – City – Eltham via Smith Street, Darebin Road		
	961	NightRider – City – Doncaster via Eastern Freeway, James Street		
	966	NightRider – City – Croydon – Lilydale via Victoria Street, Maroondah Highway		
Tram	All Train Lines		Parliament Station	550 m

A consolidated public transport and bicycle network plan is provided as Figure 2-5 overleaf.

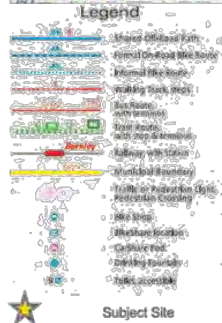
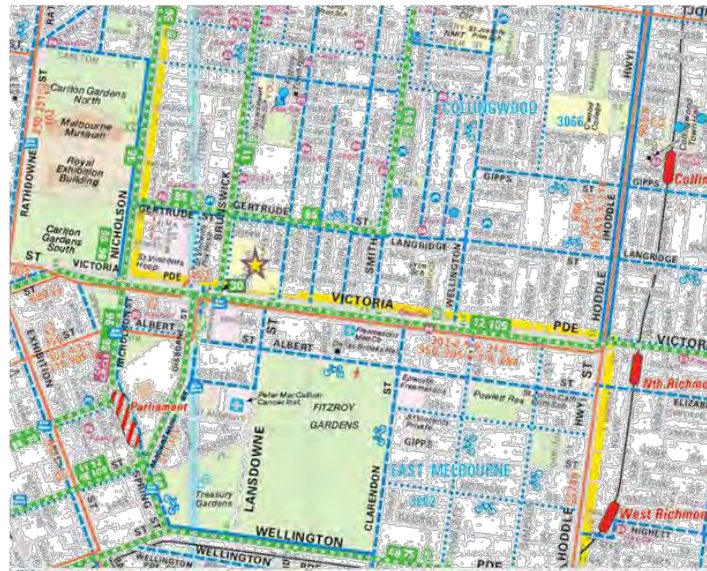
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Figure 2-5 Public Transport and Bicycle Network



2.6 Traffic Network

2.6.1 General

The subject site is primarily accessible via Victoria Parade and Young Street. Various additional crossovers and roller doors provide secondary access, generally for loading and other authorised vehicles.

The key road links in the vicinity of the site are shown in Figure 2-6, providing access to surrounding suburbs including Collingwood and Richmond to the east, Fitzroy North to the north, Carlton and the CAD to the west and East Melbourne to the south.

Both the Eastern Freeway and CityLink are readily accessible from Hoddle Street via Victoria Parade. A detailed review of the streets in the immediate vicinity of the site is provided in the following sections.

Figure 2-6 Key Road Links in the Vicinity of the Site



2.6.2 Brunswick Street

Brunswick Street is generally aligned in a north-south direction from St Georges Road in Fitzroy North to Victoria Parade in Fitzroy. North of Alexandra Parade, Brunswick Street is a Declared Main Road. ACU is located on the east side of Brunswick Street.

Adjacent the subject site, Brunswick Street operates with two lanes in each direction, with the central lanes shared between tram lines and traffic and the kerbside lanes generally operate as shared parking and bicycle lanes, with the exception of between 7:00am – 9:00am Monday to Friday when 'No Stopping' restrictions apply on the eastern kerb.

Both the Brunswick Street / Gertrude Street and Brunswick Street / Victoria Parade intersections are controlled by traffic signals.

In the vicinity of the site, Brunswick Street operates with a posted speed limit of 40 kilometres per hour. A view of Brunswick Street in the vicinity of the site is provided in Figure 2-7.

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Figure 2-7 Cross Section of Brunswick Street in the Vicinity of the Site



2.6.3 Young Street

Young Street is a local road aligned in a north-south direction from Gertrude Street to Victoria Parade. Young Street primarily serves an access function, with speed humps used along its length for traffic calming purposes. ACU is located on both the east and west sides of Young Street, with vehicle access provided to an ACU car park and an ACU private road named Duke Street.

Along its length, Young Street operates with a varying cross section. Between Victoria Parade and the entrance to the ACU car park, Young Street operates with a single traffic lane in each direction. This section of Young Street operates with a posted speed limit of 40 kilometres per hour. Beyond this point, Young Street operates as one way, northbound only.

Between Little Victoria Street and Duke Street, Young Street operates as a shared zone, within a carriageway which allows for a single lane of northbound traffic. As required for shared zones, this section of Young Street operates with a speed limit of 10 kilometres per hour. There is generally no provision for kerbside parking, with the exception of a single indented parallel disabled car space.

Between Duke Street and Graham Street, Young Street operates with a single northbound traffic lane, with no provision for kerbside parking. North of the shared zone, Young Street operates with a default speed limit of 50 kilometres per hour.

Between Graham Street and Gertrude Street, Young Street allows for a single lane of northbound traffic, with kerbside parallel parking permitted on the west kerb clear of traffic.

The Young Street / Victoria Parade intersection operates as a left in/left out 'Give Way' controlled T-intersection, with priority given to Victoria Parade.

The Young Street / Gertrude Street intersection operates as a 'Stop' controlled T-intersection, with priority given to Gertrude Street and allows for outbound movements out of Young Street only. Right turns are banned between 7:00am – 9:00am Monday to Friday.

The various cross sections of Young Street are shown in Figure 2-8 through to Figure 2-11.

Figure 2-8 Cross Section of Young Street Between Victoria Parade and ACU Car Park Entrance



Figure 2-9 Cross Section of Young Street Between ACU Car Park Entrance and Duke Street



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Figure 2-10 Cross Section of Young Street Between Duke Street and Graham Street

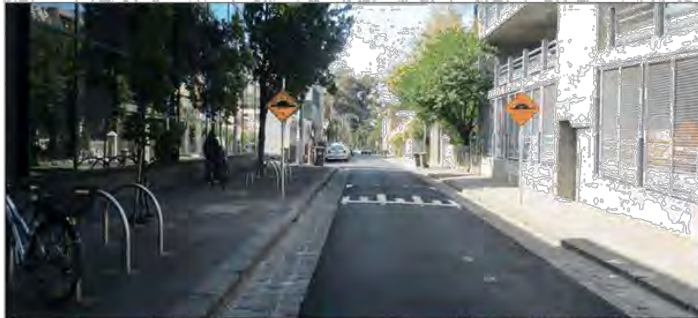


Figure 2-11 Cross Section of Young Street Between Graham Street and Gertrude Street



2.6.4 Napier Street

Napier Street is a local road aligned in a north-south direction from Cecil Street to Victoria Parade. Various traffic calming treatments are used along the length of Napier Street, including speed humps and no through roads. ACU is located on the west side of Napier Street. No vehicle access to ACU is provided via Napier Street.

In the vicinity of the site, Napier Street allows for a single lane of traffic in each direction, with angle parking permitted alongside one kerb and parallel parking permitted on the other kerb and a bicycle symbol identifying Napier Street in the vicinity of the site as an informal bicycle route.

The Napier Street / Gertrude Street intersection operates as a 'Stop' controlled cross intersection, with priority given to Gertrude Street. Right turns out of the southern Napier Street approach are not permitted between 7:30am – 9:00am Monday to Friday.

The Napier Street / Victoria Parade intersection is a left out only 'Stop' controlled intersection, with priority given to Victoria Parade. Inbound movements into Napier Street are not permitted.

In the vicinity of the site, Napier Street operates with a posted speed limit of 40 kilometres per hour.

A view of Napier Street in the vicinity of the site is provided in Figure 2-12.

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Figure 2-12 Cross Section of Napier Street in the Vicinity of the Site



2.6.5 Victoria Parade

Victoria Parade is a Declared Main Road which extends generally in an east-west direction from Hoddle Street to La Trobe Street. At both ends, Victoria Parade continues as Victoria Street. ACU is located on the north side of Victoria Parade, with access provided to an on-site car park.

In the vicinity of the site, Victoria Parade operates with three traffic lanes and a bus lane in each direction, separated by a wide central median which accommodates tram lines, tram stops and footpaths. Left and right turn deceleration lanes are provided at key intersections.

Along its length, parallel parking is intermittently permitted on both the north and south kerbs and as well as both kerbs of the central median, however in the vicinity of the site, parking is only permitted on the south kerb.

Victoria Parade operates with a posted speed limit of 60 kilometres per hour. A view of Victoria Parade in the vicinity of the site is provided in Figure 2-13.

Figure 2-13 View of Victoria Parade in the Vicinity of the Site



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2.6.6 Little Victoria Street

Little Victoria Street is a right of way (ROW) which extends in an east-west direction from George Street to Little Napier Street. Little Victoria Street terminates at the eastern boundary of ACU, however does not provide vehicle access to the ACU.

In the vicinity of the site, Little Victoria Street operates as one way westbound only within a single traffic lane and provides access to Little Napier Street which services dwellings fronting Napier Street. Kerbside parking is not permitted.

A view of Little Victoria Street in the vicinity of the site is provided in Figure 2-14.

Figure 2-14 Cross Section of Little Victoria Street in the Vicinity of the Site



2.6.7 Little Napier Street

Little Napier Street is a right of way (ROW) which extends in a north-south direction from Gertrude Street to Little Victoria Street. Little Napier Street is aligned along part of ACU's eastern boundary, however does not provide vehicle access.

Little Napier Street operates as one way northbound only within a single traffic lane and services dwellings fronting Napier Street.

A view of Little Napier Street is provided in Figure 2-15.

Figure 2-15 Cross Section of Little Napier Street in the Vicinity of the Site



2.6.8 Graham Street

Graham Street is a local road which extends west from Young Street for approximately 45 metres before terminating. Graham Street extends to the boundary of ACU.

Graham Street operates as two way, however allows for a single direction of travel at any one time. Parallel parking is permitted on the northern kerb, as shown in Figure 2-16.

Figure 2-16 Cross Section of Graham Street in the Vicinity of the Site



2.6.9 Duke Street

Duke Street is a private road within ACU which extends west from Young Street for approximately 40 metres.

Duke Street operates as a shared zone, within a carriageway which allows for a single direction of travel at any one time. As required for shared zones, this section of Young Street operates with a speed limit of 10 kilometres per hour with a speed limit of 10 kilometres per hour.

No entry to Duke Street is permitted with the exception of authorised vehicles. The cross section of Duke Street in the vicinity of the site is provided in Figure 2-17.

Figure 2-17 Cross Section of Duke Street in the Vicinity of the Site



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2.7 Road Network Safety

Casualty accident history for the roads and intersections adjoining the ACU, specifically, roads bound by Victoria Parade to the south, Gertrude Street to the north, Brunswick Street to the west and Napier Street to the east have been sourced from VicRoads CrashStats accident database.

A summary of the accidents in the area defined above for the last available five year period (1 July 2008 to 30 June 2013) are summarised in Figure 2-18.

Figure 2-18 Casualty Accident History - 1 July 2008 to 30 June 2013



The CrashStats review indicates that a total of 33 casualty accidents have been reported within the nominated area within the five year period. Table 2-7 provides a broad summary of the accident categories.

Table 2-7 Accident Categories

Accident Type	Number
Involving Bicyclists	16
Involving Motor Cyclists	8
Involving Pedestrian	5
Involving Motorists Only	4
Total	33

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2.8 Car Parking

The site is located in an area with constrained long term and short term on street parking opportunities with observations revealing that on street parking opportunities in the vicinity of the site are generally ticketed and or marked as permit zones with the limited number of long term (4 hours or greater) parking opportunities observed to be well utilised.

Notwithstanding the above, commercial off-street parking facilities are available and include the onsite parking spaces provided by the University, specifically, a 250 space deck car park accessible via Young Street, and a 50 space at grade car park accessible via Victoria Parade.

The location of these onsite parking areas are illustrated in Figure 2-19.

Figure 2-19 ACU Onsite Car Parking Spaces



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3 State and Local Policies

3.1 State Policies

3.1.1 Plan Melbourne

Plan Melbourne outlines the vision for Melbourne's growth to the year 2050. The document was developed through extensive collaboration across government and a sustained dialogue with professional, industry and community stakeholders. In October 2013, Plan Melbourne was released for comment, with the final Plan Melbourne released in May 2014 following submissions.

Plan Melbourne specifies seven outcomes and objectives for Melbourne, summarised as follows:

- > **Delivering jobs and investment:** Create a city structure that drives productivity, supports investment through certainty and creates more jobs;
- > **Housing choice and affordability:** Provide a diversity of housing in defined locations that cater for different households and are close to jobs and services;
- > **A more connected Melbourne:** Provide an integrated transport system connecting people to jobs and services, and goods to market;
- > **Liveable communities and neighbourhoods:** Create healthy and active neighbourhoods and maintain Melbourne's identity as one of the world's most liveable cities;
- > **Environment and water:** Protect our natural assets and better plan our water, energy and waste management systems to create a sustainable city;
- > **A state of cities:** Maximise the growth potential of Victoria by developing a state of cities which delivers choice, opportunity and global competitiveness; and
- > **Implementation:** Delivering better governance: Achieve clear results and deliver outcomes through better governance, planning, regulation and funding mechanisms.

Plan Melbourne provides a number of directions to address the abovementioned outcomes and objectives. Those considered relevant to this report include:

- > **Direction 1.1:** Define a new city structure to deliver an integrated land use and transport strategy for Melbourne's changing economy;
- > **Direction 3.1:** Transform the transport system to support a more productive central city;
- > **Direction 3.2:** Improve access to job-rich areas across Melbourne and strengthen transport networks in existing suburbs; and
- > **Direction 3.4:** Improve local travel options to increase social and economic participation.

A discussion paper called Plan Melbourne Refresh was prepared in October 2015 to build upon the work and consultation undertaken in preparing Plan Melbourne, with Plan Melbourne 2016 to be released in the first half of 2016.

It is understood that Plan Melbourne 2016 will maintain the key priorities of Plan Melbourne 2014 but strengthen its focus on a number of areas.

3.1.2 Transport Integration Act

The Transport Integration Act 2010 creates a new framework for the provision of an integrated and sustainable transport system in Victoria. The Act provides a vision statement, reproduced below:

"The Parliament recognises the aspirations of Victorians for an integrated and sustainable transport system that contributes to an inclusive, prosperous and environmentally responsible State."

The Transport Integration Act recognises that the transport system should be considered as a single system and ensures that all transport agencies work together.

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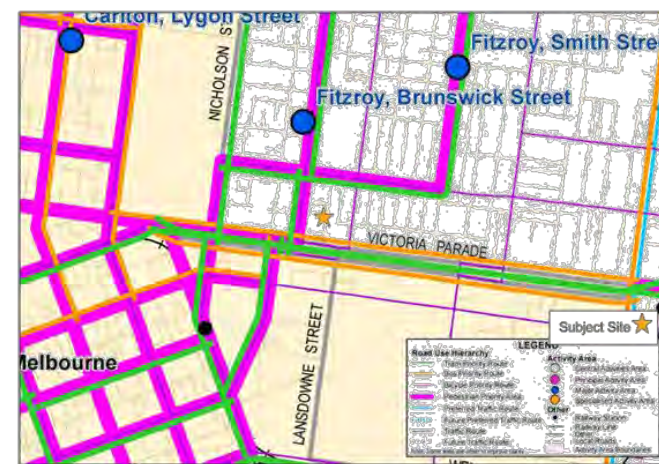
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3.1.3 VicRoads' SmartRoads

SmartRoads is a policy implemented by VicRoads to manage competing interests for limited road space by giving priority use of the road to different transport modes at particular times of the day. Road Use Hierarchy Maps show the priority modes on each road for each Council area and form the foundation for the network operating plan.

The Road Use Hierarchy Map in the vicinity of the site is provided in Figure 3-1 and shows Victoria Parade as a tram priority, bus priority and traffic route, whilst Brunswick Street is shown as a tram priority and bicycle priority route as well as a pedestrian priority area.

Figure 3-1 SmartRoads Road Use Hierarchy Map



3.1.4 Victorian Cycling Action Plan 2013 & 2014

The Victorian Cycling Action Plan 2013 & 2014 was prepared for the Victorian Government and was released in December 2012. The document aims to grow and support cycling in Victoria by making it easier for more people to cycle and to make it safer for people who already ride as well as improving the cycling experience for all types of bike riders.

Six directions are identified within the Victorian Cycling Action Plan to achieve the above aims, reproduced as follows:

- > **Build evidence:** build a stronger evidence base for the Victorian Government to make more informed decisions;
- > **Enhance governance and streamline processes:** clarify accountability and improve co-ordination, planning and delivery;
- > **Reduce safety risks:** reduce conflicts and risks to make cycling safer;
- > **Encourage cycling:** help Victorians feel more confident about cycling and make cycling more attractive;
- > **Grow the cycling economy:** support opportunities to grow and diversify Victoria's economy through cycling; and
- > **Plan networks and prioritise investment:** plan urban cycling networks to improve connectivity and better target infrastructure investment for urban networks, regional trails and specialist cycle sport infrastructure.

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3.1.5 Pedestrian Access Strategy

The Pedestrian Access Strategy was prepared in 2010 and aims to increase walking for transport in Victoria and notes that more people walking has the potential to help ease congestion, reduce greenhouse emissions, improve the health of Victorians and promote social connections.

The document lists five strategic directions for walking, reproduced as follows:

- > **Encourage people to walk by changing attitudes and behaviour:** This aims to make walking the top-of-mind choice for Victorians – especially for short trips – by making walking for transport a visible and valued part of daily life;
- > **Collaborate to improve provision for walking:** This aims to clarify the roles and responsibilities of both state and local governments in providing for walking. The Victorian Government will work with local governments to ensure they have the capacity and information they need to provide better pedestrian facilities;
- > **Create pedestrian-friendly built environments, streets and public spaces:** This aims to ensure built environments across Victoria facilitate easy and efficient pedestrian movements;
- > **Increase the safety of walking:** This will identify and address risks to pedestrians across the transport system and give pedestrians the skills to negotiate road environments; and
- > **Continue integrating walking with public transport:** This aims to ensure more Victorians walk in combination with public transport. Walkers need to find it easy to get to major public transport hubs across Victoria and easy walking access should be provided at public transport stops.

3.2 Local Policy

3.2.1 Clause 21.06 of the Yarra Planning Scheme

Clause 21.06 of the Yarra Planning Scheme details the City of Yarra's transport related objectives and strategies. Clause 21.06 acknowledges that the City of Yarra needs to reduce car dependence by promoting walking, cycling and public transport use as viable and preferable alternatives. Clause 21.06 lists four objectives as follows:

- > **Objective 30:** To provide safe and convenient pedestrian and bicycle environments;
- > **Objective 31:** To facilitate public transport usage;
- > **Objective 32:** To reduce the reliance on the private motor car; and
- > **Objective 33:** To reduce the impact of traffic.

Of particular relevance to this report is Strategy 32.2 in response to objective 32, reproduced below:

- > **Strategy 32.2:** Require all new large developments to prepare and implement integrated transport plans to reduce the use of private cars and to encourage walking, cycling and public transport.

3.2.2 Strategic Transport Statement 2006 - City of Yarra

The Strategic Transport Statement is a document prepared by the City of Yarra in 2006 which addresses the access needs of Yarra's community whilst minimising the impact of cars on Yarra's community. The Strategic Transport Statement has a stated vision as follows:

"To create a city which is accessible to everyone irrespective of levels of personal mobility and where a fulfilling life can be had without the need for a car."

The Strategic Transport Statement lists seven key Strategic Transport Objectives to achieve the above vision as follows:

- > Create a city which is a great and safe place to walk and increase the numbers of those walking in Yarra.
- > Create the most bicycle friendly city in Australia and increase the numbers of those cycling in Yarra.
- > Advocate for increased performance of public transport across Melbourne and thereby reduce the number of car trips and through traffic by both Yarra and non-Yarra residents.
- > Ensure that any new road construction is not in conflict with encouraging more sustainable transport use.
- > Ensure Council's response to parking demand is based on Yarra's parking hierarchy and sustainable transport principles.

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- > Work to limit freight movement to arterial roads and freeways within Yarra and work to reduce freight movement through Yarra.
- > Encourage Council staff to use more sustainable transport for their travel and increase the capacity of the Council as a whole to respond to and initiate positive actions to further strategic transport objectives 1 to 6.

3.2.3 City of Yarra Bicycle Strategy 2010 – 2015

The City of Yarra Bicycle Strategy 2010 – 2015 details the status on bicycle initiatives and lays out future plans for bicycle facilities, with an aim to establish cycling as a legitimate first choice of transport by people of all ages and cycling abilities.

The document lists 11 strategies to achieve the above, listed as follows:

- > **Strategy 1:** Better on-road bicycle network;
- > **Strategy 2:** Better local streets for cycling;
- > **Strategy 3:** Better off-road bicycle network;
- > **Strategy 4:** Better bicycle network maintenance;
- > **Strategy 5:** Better end of trip facilities – bicycle parking;
- > **Strategy 6:** Better bicycle network accountability;
- > **Strategy 7:** Better bicycle safety by reducing conflict;
- > **Strategy 8:** Better Council use of bicycles;
- > **Strategy 9:** Better recruitment and retention of cyclists;
- > **Strategy 10:** Better policies; and
- > **Strategy 11:** Better innovation and relationships.

3.2.4 Inner Melbourne Action Plan (IMAP)

The Inner Melbourne Action Plan (IMAP) was prepared by the Cities of Melbourne, Yarra, Port Phillip, and Stonnington in partnership with VicUrban and was adopted in December 2005. Maribymong City Council also became a member of the IMAP in 2013.

The IMAP lists a number of strategies to achieve its vision, including the following which are considered relevant to this report:

- > **Strategy 2:** Effectively link transport routes so that the Inner Melbourne Region is accessible throughout by walking, cycling and public transport;
- > **Strategy 3:** Minimise the growing impact of traffic congestion;
- > **Strategy 4:** Increase public transport use.

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Attachment 5 - ACU Development Plan - Part 4

4 Development Proposal

4.1 Development Plan

The Development Plan has been developed to accommodate the ACU's projected growth in students, teaching staff and research from 2015 to 2025, whilst also allowing the consolidation of teaching and research activities onto the Campus.

Specifically it is anticipated that by 2020, ACU will increase their EFT student numbers to 10,700 and staff numbers to 850. A summary of existing and proposed student and staff numbers is provided in Table 4-1.

Table 4-1 Student and Staff Numbers

Existing	Proposed	Change
10,000 EFT Students	10,700 EFT Students	+700 EFT Students
800 Staff	850 Staff	+50 Staff

The key principle that will inform the Development Plan from a transport perspective is based on the City of Yarra's commitment to reduce car dependency by promoting walking, cycling and public transport use.

Specifically, the St Patrick's Campus Development Plan seeks to become a pedestrian oriented space that is safe for students and prioritises sustainable modes of transport by:

- > Improving access to public transport along Victoria Parade, Brunswick Street and Gertrude Street;
- > Ensuring pedestrian links from the Campus integrate and form part of the wider pedestrian network;

The above will be achieved through the creation of strong pedestrian linkages through the Campus and to adjacent uses, activities and transport networks. Notably, the facade treatment to Victoria Parade on the Mary Glowrey Building is to be removed and by so doing, substantially increasing the footpath width along Victoria Parade in the section between Young Street and Napier Street. This will be particularly beneficial at the interface of the Mary Glowrey Building and the existing bus stop.

4.2 Individual Projects

The individual projects within the Development Plan will include:

4.2.1 115 B Victoria Parade

This project will comprise of the subdivision of 115 Victoria Parade to create a new building site. Within this site, a multi storey building is contemplated, and will include the provision of basement level car parking for 270 spaces.

Vehicular access to this building is planned via Napier Street. Mitigation works will be required at the intersection of Victoria Parade and Napier Street to accommodate direct access via Victoria Parade.

4.2.2 28 - 42 Young Street (Existing Deck Car Park)

The existing 250 space deck car park is to be demolished and in its place a new building is proposed.

This new building will have no car parking spaces provided, instead, the development of this site will provide the opportunity to make substantial improvements to pedestrian access and connectivity between the campus buildings, and the adjacent campus uses and activities.

5 Transport Network Interventions

5.1 Pedestrian Network

The Development Plan contemplates improvements to pedestrian access throughout the Campus, with the aim of providing an attractive and safe pedestrian environment that will integrate with the surrounding local precinct.

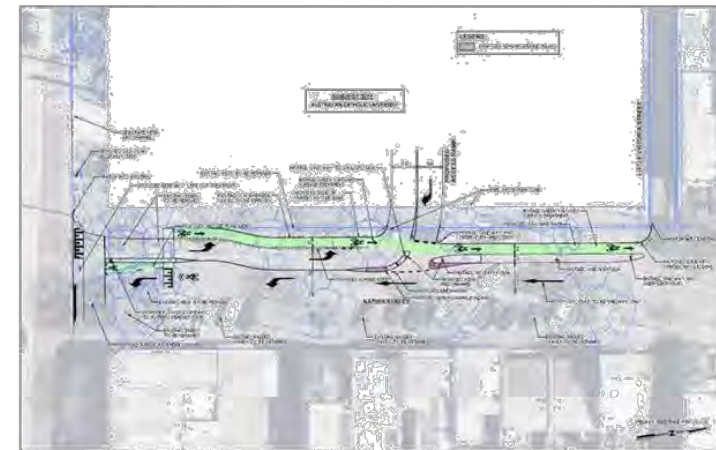
The key initiatives in this regard will be the creation of strong pedestrian linkages through the Campus and to adjacent uses, activities and transport networks. Notably, the facade treatment to Victoria Parade on the Mary Glowrey Building is to be removed and by so doing, substantially increasing the footpath width along Victoria Parade in the section between Young Street and Napier Street. This will be particularly beneficial at the interface of the Mary Glowrey Building and the existing bus stop.

5.2 Bicycle Network

The mitigation works required at the intersection of Victoria Parade and Napier Street to accommodate vehicular access to the 115 B Victoria Parade project, provides the opportunity to improve cyclist amenity at this intersection.

The concept plan shown as Figure 5-1 illustrates the proposed mitigation works and improvements to cyclist amenity.

Figure 5-1 Victoria Parade / Napier Street – Concept Design



Attachment 5 - ACU Development Plan - Part 4



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5.3 Local Area Traffic Management

The City of Yarra have over the years undertaken Local Area Traffic Management (LATM) studies to improve traffic conditions and road safety in local streets.

The studies looked at issues such as traffic speed and volume, pedestrian safety and comfort, and how to calm traffic so that neighbourhoods are more liveable. Notably, as part of these studies residents and businesses were surveyed about the traffic issues in the area and proposed treatments.

The ACU St Patrick's Campus is located within the bounds of Council's LATMS11 – Fitzroy area. An extract of LATMS11 – Fitzroy is provided as Figure 5-2

Figure 5-2 Extract of LATMS 11 – Fitzroy

LATMS11 identifies the intersection of Victoria Parade and Napier Street as an intersection that requires improvement, furthermore, the section of Young Street between Little Victoria Street and Duke Street has also been identified as an area that is subject to detailed investigation.

Noting the foregoing, the proposed pedestrian and bicycle network interventions could be considered as options consistent with the aspirations of LATMS 11 – Fitzroy.

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6 Statutory Considerations

6.1 Schedule 2 to the Development Plan Overlay

The statutory requirements applicable to the Development Plan are contained within Schedule 2 to the Development Plan Overlay.

The traffic specific requirements are reproduced below.

The development plan must show:

- > The number, location, dimensions, and layout of all car parks and access ways to and from them;
- > A management plan for the operation and maintenance of the car park areas;
- > The location and dimensions of all bicycle, vehicle and pedestrian ways;
- > A traffic management plan which must show any traffic management and traffic control works considered necessary in adjoining and nearby roads when the development is completed; and
- > The means of vehicular and pedestrian ingress to and egress from the land.

An assessment of the Development Plans against the above noted statutory requirements follows:

6.2 Design Response

6.2.1 Car Parking and Accessways

A total of 270 car spaces are proposed. These spaces are to be provided within basement levels on the 115 B Victoria Parade project.

Specific to the DPO Requirements:

- > Parking spaces are to be provided at a minimum 4.9m long and 2.6m wide;
- > Accessways within the car park are to be provided at a minimum 6.4m wide where they serve car parking spaces and a minimum 5.5m wide where they do not abut parking. The main accessway off Napier Street is provided at a width of 7.6m;
- > A minimum head height of 2.1 metres is proposed within the basement car park, increasing to 2.5m where provision is made for disabled spaces and Small Rigid Vehicles.

These dimensions are consistent with the requirements contained within Clause 52.06 of the City of Yarra Planning Scheme.

6.2.2 Bicycle Ways

An additional 80 bicycle spaces are to be provided as part of the Development Plan.

These bicycles are to be spread throughout the campus and designed to meet the dimension requirements set out by Bicycle Network and or AS2890.3:2015, and would comprise a mix as appropriate of the products as per the spec sheets provided as Appendix B.

6.2.3 Pedestrian Ways

These pedestrian linkages through the site are to be designed to meet and or exceed the relevant standards.

6.2.4 Means of Vehicular and Pedestrian Access

For the 115B Victoria Parade project, vehicular access is to be facilitated via Napier Street, as illustrated in the accompanying concept functional layout plan, Cardno drawing number CG150178-TR-DG-2505-3 provided as Appendix A.

Pedestrian access is to be provided via Victoria Parade, and Napier Street. The new building will also be accessible via existing pedestrian access points provided for the adjacent Mary Glowrey Building.

With regard to the 28 - 42 Young Street project, no car parking is proposed. Accordingly, no vehicular access is contemplated. Pedestrian access will be via Little Victoria Street and Young Street.

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6.2.5 Traffic Management Plan

Victoria Parade / Napier Street

Vehicular access to the 115B Victoria Parade project will be via Napier Street. Ingress and egress will be restricted to the intersection of Victoria Parade and Napier Street.

This intersection will require mitigation works to allow access via Victoria Parade. The traffic management and traffic control works considered necessary to mitigate the impact of the development at this intersection are illustrated in Cardno drawing number CG150178-TR-DG-2502-3, provided as Appendix A.

Young Street

Tube count surveys undertaken on Young Street in October 2015, at a location north of the existing access to the deck car park indicates that on average Young Street carries about 550 vehicles per day northbound, with the car park contributing about 100 vehicles per day to this total figure. On weekends northbound traffic flows reduce to 250 vehicles per day.

On this basis, Young Street can be classified as an Access Place, noting:

- > An Access Place as defined by Clause 56.06 of the City of Yarra Planning Scheme, is a minor street providing local residential access with shared traffic, pedestrian and recreation use, but with pedestrian priority;
- > This road can accommodate an indicative maximum traffic volume of between 300 – 1000 vehicles over a 24-hour period, within a 5.5m wide carriageway with parking on one side of the carriageway.

The proposed demolition of the existing deck car park will remove traffic currently generated by the car park, thus reducing daily traffic flows along this road to the benefit of pedestrian movements via the reduction in the potential for vehicle and pedestrian conflicts.

6.2.6 Car Park Management Plan

The following conditions detail the operation and management of the car parking spaces proposed within the 115B Victoria Parade project.

- > The parking area will be secured by boom gates, activated by card readers and ticket machine on entry and departure during the facility's operating hours, anticipated to be between 7:00am – 10:30pm Weekdays and 8:00am – 10:30pm weekends consistent with current operational practises for the existing car park. During afterhours the car park will be secured by roller doors.
- > To alert casual parkers entering the car park and ensure ease of use, clearly visible advisory signs will be posted on the car park entry and also within the car park. Additionally, to avoid conflict between long term (staff) and short term (students) users, spaces will be designated to individual staff members once they are determined, with signage posted at the end of each bay.
- > Operation of the public car park will be limited to the hours listed above, with its fee structure expected to be based on an hourly charge rates in-line with other commercial car park facilities in the vicinity of the site. The actual charge rates will however be determined once an operator is appointed. The car park is to be operated by ticket collection on entry, and then payment at a pay station likely to be located adjacent to the lifts.
- > Dynamic signage on Victoria Parade will be provided to indicate if the car park is full, which will reduce the number of vehicles turning into Napier Street during high occupancy periods. Notwithstanding, U-turn movements can be undertaken within the ROW located on the east side of Napier Street midblock between the site access and Victoria Parade.

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6.3 Car Parking - Clause 52.06

6.3.1 Car Parking Requirement

Table 1 to Clause 52.06 of the City of Yarra Planning Scheme sets out the car parking provision rates that apply to the Development Plan.

Specifically, Clause 52.06 requires that parking for the University is provided at a minimum rate of:

- > 0.4 spaces to each student that is part of the maximum number of students on site at any one time.

The student population on the campus is projected to increase by 700 students, in the period between 2015 – 2020.

Application of this rate to the proposed increase in students reveals a statutory requirement to provide a minimum 280 spaces.

6.3.2 Car Parking Provision

A total of 270 spaces are proposed as part of the 115B Victoria Parade Project, whilst the 28 - 42 Young Street project contemplates the demolition of the 250 space deck car park, with no additional parking provided on this site.

The building at 115B Victoria Parade will also be built on the existing at-grade car park site currently providing 50 spaces.

Accordingly the Development Plan results in a net decrease of 30 spaces, against a statutory requirements of 280 spaces.

This provision is considerably lower than the statutory requirements, and as such is in line with the sustainable transport objectives set out in local and state level policies.

6.4 Bicycle Parking – Clause 52.34

6.4.1 Bicycle Parking Requirement

Table 1 to Clause 52.34 of the City of Yarra Planning Scheme sets out the bicycle parking provision rates that apply to the Development Plan.

Specifically, Clause 52.34 requires that bicycle parking for the University is provided at a minimum rate of:

- > 1 space to each 20 employees; plus
- > 1 space to each 20 full time students.

The student population on the campus is projected to increase by 700 students, with a corresponding increase in staff numbers of 50 in the period between 2015 – 2020.

Application of these rates to the proposed increase in staff and students reveals a statutory requirement to provide a minimum 38 bicycle spaces comprising, 3 staff spaces and 35 student spaces.

6.4.2 Bicycle Parking Provision

It is intended to provide an additional 80 bicycle spaces. These spaces will be provided across the campus, not contained within the DPO area.

This provision is double the statutory requirements, and as such is in line with the sustainable transport objectives set out in local and state level policies.

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7 Traffic Impact

7.1 Projected Traffic Volumes

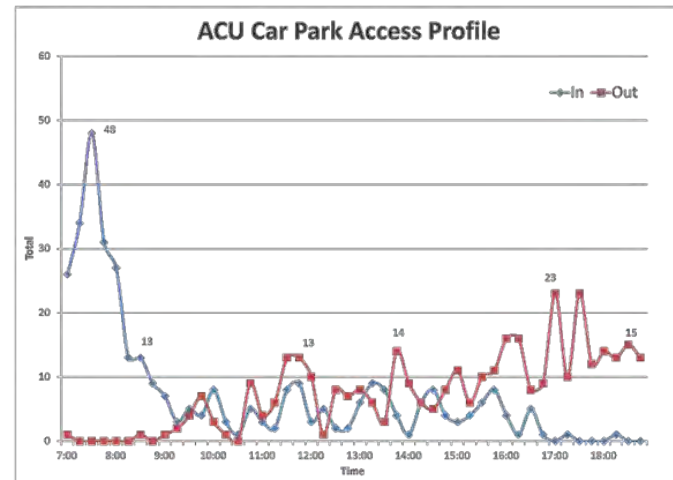
The existing ACU deck car park with 250 spaces used by both staff and students provides a suitable reference to inform consideration about projected traffic volumes at the proposed 270 space car park at the 115B Victoria Parade project.

The existing car park was surveyed on

- Wednesday 21st May 2014 between 7am to 7pm; and
- Wednesday 7th October 2015 between 7am to 7pm

Figure 7-1 and Figure 7-2 provide a summary of the traffic profile at the ACU car park access points.

Figure 7-1 ACU Car Parking Access Profile – Wednesday 21st May 2014



The key findings were as follows:

- > There were a total of 355 inbound movements and 360 outbound movements; and
 - > The AM and PM peak hour was found to occur between 7:15am - 8:15am and 5:00pm-6:00pm respectively, when a total of 140 and 68 vehicle movements were recorded at the site access points respectively;
- The above equates to about 60 % of the car parking spaces filling during the AM peak period, and about 30% of the spaces emptying during the PM peak. Traffic flows during the peak periods was observed as being heavily biased towards the peak direction.

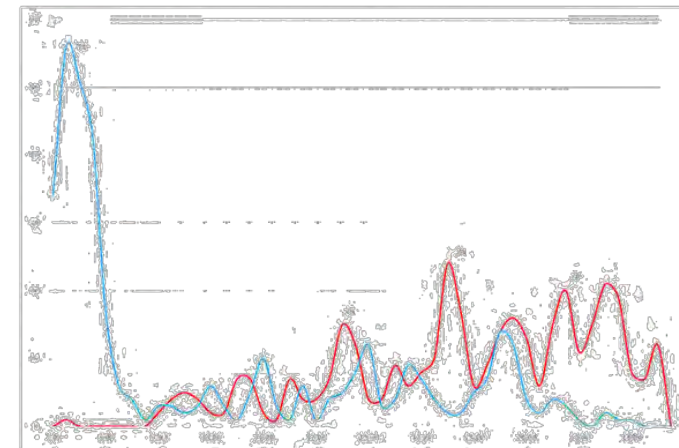
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Figure 7-2 ACU Car Parking Access Profile – Wednesday 7th October 2015



The key findings were as follows:

- > There were a total of 375 inbound movements and 360 outbound movements; and
 - > The AM and PM peak hour was found to occur between 7:00am - 8:00am and 3:15pm-4:15pm respectively, when a total of 185 and 45 vehicle movements were recorded at the site access points respectively.
- The above equates to about 75 % of the car parking spaces filling during the AM peak period, and about 20% of the spaces emptying during the PM peak. Traffic flows during the peak periods was observed as being heavily biased towards the peak direction.

Conservatively adopting a rate of 75 % of the car spaces filling during the AM peak and 30% emptying during the PM peak, the proposed 270 space car park would be projected to generate:

- > 203 vehicle movements during the AM peak period; and
- > 81 vehicle movements during the PM peak period.

For the purpose of this assessment, traffic flows will be biased 95% in the peak direction and 5% in the non-peak direction.

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Attachment 5 - ACU Development Plan - Part 4

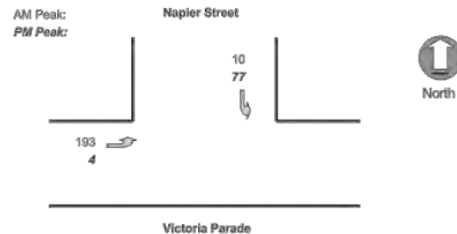


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7.2 Traffic Distribution

Traffic to and from the proposed car park will be restricted to the Victoria Parade / Napier Street intersection. This intersection will operate as a left in / left out, with all traffic arriving from the west and departing towards the east. Figure 7-3 illustrates the projected additional traffic flows at this intersection.

Figure 7-3 Projected Additional Traffic Flows – Victoria Parade / Napier Street



Based on historic traffic data collected on Napier Street in 2010, Napier Street at a location north of Little Victoria Street carries on average about 700 southbound vehicles per day, inclusive of about 100 vehicle movements during the AM peak period and 50 vehicle movements during the PM peak period.

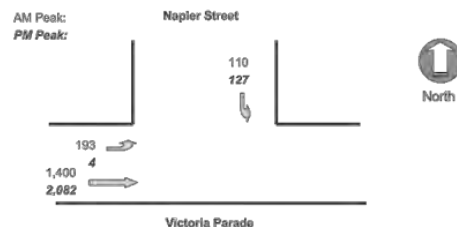
7.3 Post Development Traffic Flows

SCATS data sourced from VicRoads for Thursday 8th October 2015 at the intersection of Victoria Parade / Lansdowne Street, show that during the AM peak periods occurs between 11:00am – 12:00pm, when 1,400 vehicles are recorded past Napier Street. The PM peak period occurs between 5:00pm – 6:00pm, with 2,082 vehicles recorded past this intersection.

Noting that Napier Street is located a short distance downstream of the signalised intersection of Victoria Parade / Lansdowne Street, platooning of traffic during the peak periods has been observed which assists motorists seeking to depart from Napier Street.

Allowing for the estimated existing traffic flows on Napier Street and the recorded traffic flows along Victoria Parade, the anticipated post development traffic flows are illustrated as Figure 7-4.

Figure 7-4 Anticipated Post Development Traffic Flows – Victoria Parade / Napier Street



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7.4 Intersection Analysis

The operation of the Victoria Parade / Napier Street intersection was analysed using SIDRA Intersection. This computer package, originally developed by the Australian Road Research Board, provides information about the capacity of an intersection in terms of a range of parameters, as described below.

Degree of Saturation (D.O.S.) is the ratio of the volume of traffic observed making a particular movement compared to the maximum capacity for that movement. Various values of degree of saturation and their rating are shown in Table 7-1.

Table 7-1 Rating of Degrees of Saturation

D.O.S.	Rating
Up to 0.6	Excellent
0.6 to 0.7	Very Good
0.7 to 0.8	Good
0.8 to 0.9	Fair
0.9 to 1.0	Poor
Above 1.0	Very Poor

It is considered acceptable for some critical movements in an intersection to operate in the range of 0.9 to 1.0 during the high peak periods, reflecting actual conditions in a significant proportion of suburban signalised intersections.

The **95th Percentile (95thile) Queue** represents the maximum queue length, in metres, that can be expected in 95% of observed queue lengths in the peak hour; and

Average Delay is the delay time, in seconds, which can be expected over all vehicles making a particular movement in the peak hour.

The results of the SIDRA Intersection analysis are summarised in Table 7-2.

Table 7-2 SIDRA Intersection Analysis Summary

Approach	Degree of Saturation	95 th ile Queue	Average Delay
AM Peak			
Napier Street (North)	0.174	5 metres	12 seconds
Victoria Parade (West)	0.252	0 metres	1 second
PM Peak			
Napier Street (North)	0.298	8 metres	16 seconds
Victoria Parade (West)	0.375	0 metres	0 seconds

Based on the foregoing the intersection of Victoria Parade and Napier Street is expected to operate under excellent conditions, with motorists experiencing minimal queues and delays.

7.4.2 Operation of Site Access

An assessment of the operation of the site access has been undertaken to determine likely queues at the control point, especially during the AM peak period.

The analysis shows that during the AM peak period when 193 ingress vehicle movements are projected, 95th %ile queues of 4 vehicles are anticipated.

The boom gates are to be located at the bottom of the ramp, and will therefore afford motorists a queuing distance of about 24 metres, measured from the title boundary.

This setback can accommodate about 4 vehicles and as such queues on to Napier Street are not anticipated.

The design and location of the proposed car park access is therefore not expected to result in conditions that obstruct traffic flows along Napier Street or Victoria Parade.

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Attachment 5 - ACU Development Plan - Part 4



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8 Green Travel Initiatives

8.1 Preamble

Green Travel Initiatives are aimed at encouraging people to choose sustainable travel alternatives such as cycling, walking or public transport, over private car usage. It is the view of the State Government that tertiary institutions are typically considered positive environments for the application of travel behaviour change programs as many students and staff already support sustainable transport for environmental, financial and social reasons.

Accordingly, and in consultation with Council, the University could develop a Green Travel Plan that would seek to encourage staff and students to choose sustainable travel alternatives such as public transport, cycling and walking.

8.2 Objective

As a guide, the objective of this travel plan would be to increase the proportion of alternative modes of transport users to 90% within 5 years.

The co-ordination and implementation of the Green Travel Plan would be the responsibility of the University in consultation with Council to:

- > Increase awareness of sustainable transport options to new and ongoing staff and students at the Campus;
- > Encourage behavioural change in both new and on-going staff and students from single-occupant motor vehicle use, to alternative methods of transport i.e. trams, buses and bicycle/walking paths;
- > Implement travel/behaviour change strategies and use pre/post surveys to monitor changes in attitudes and reported behaviour;
- > Identify infrastructure changes / improvements to alternative travel mode infrastructure within the locality that will facilitate take-up of sustainable transport behaviours by the University community.

8.3 Action Plan (Guide Only)

Various strategies can be adopted to encourage the use of non-private motor vehicles, as described in action plans below.

8.3.1 General Actions

Action	Timeline	By whom	Approx cost
Strategic Marketing			
Promotion of Green travel initiatives during orientation and enrolment & re-enrolment periods.	Ongoing - from time of occupation	University	
Toilet talk - a series of posters behind toilet doors where people can read them in private!			
An events calendar: 3-4 events per year. Best in conjunction with state wide events such as Ride to Work Day, World Environment Day, National Walk to Work Day, Plan for lunch, morning teas or breakfasts, guest speakers, demonstrations etc.			
Display boards in prominent locations to showcase green travel initiatives.			

8.3.2 Walking

Action	Timeline	By whom	Approx cost
Produce a map showing safe walking routes to and from the University with indicative walking times, not distances, to local facilities, such as shops and bus stops (people often have an unrealistic idea of how long it takes to walk).	On Occupation	University	
Provide umbrellas in staff rooms to encourage staff members to consider walking during adverse weather periods.			
Create a sustainable travel tab on the University web page with links to appropriate external and internal websites.			
Negotiate with council for improvements to footpaths in the vicinity of the site.	Ongoing	University & Council	

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8.3.3 Cycling

Action	Timeline	By whom	Approx cost
Establish a University "Bicycle Users Group" (BUG). BUGs are formed by people who want to work together to improve facilities for cyclists and encourage cycling.	On Occupation	University	
Participate in annual events such as 'Ride to Work Day.'	Ongoing		
Provide sufficient bicycle parking and end of trip facilities (lockers / showers) to meet staff and student needs, which is easily accessible and secure.	On Occupation		
Ensure bicycle parking is clearly visible or provide signage to direct people to cycle bays.			
Produce a map showing quiet cycle routes to the University.			
Create a sustainable travel tab on the University web page with links to appropriate external and internal websites.			
Improvements to Bicycle infrastructure as identified in Yarra City Council's Draft Bicycle Strategy, such as modifications to the adjacent traffic signals along Victoria Parade to facilitate connections to the bicycle routes in the City of Melbourne.		Council	

8.3.4 Public Transport

Action	Timeline	By whom	Approx cost
Develop a map showing public transport routes to the University.	On occupation	University	
Put up a noticeboard with leaflets and maps showing the main public transport routes to and from the University.	On occupation		
Create a sustainable travel tab on the University web page with links to appropriate external and internal websites.	On occupation		
Encourage public transport use for business travel. This could be done by ensuring shared tickets are available at the University for work travel during the day.	Ongoing		
Participate in the Public Transport Victoria (PTV), 'Commuter Club' scheme, which allows employers to purchase discounted yearly MYKI cards on behalf of their employees.			

8.3.5 Car Parking / Car Pooling

Action	Timeline	By whom	Approx cost
Set up a car pooling database.	Within 12 Months of Occupation	University	
Allocate priority parking spaces for car poolers.			
Provide sustainable transport allowances for staff who surrender car parking permits.			
Review pricing policy for existing ACU off street car park.			

The above draft action plans are an outline of the actions and incentives that could be undertaken / provided and is subject to agreement between the University and Council.

8.4 Monitoring and Review

In order to monitor the success of the aforementioned Green Travel Initiatives a 3 stage monitoring system could be implemented by the University.

Elected University representatives, both staff and students would be assigned as Travel Co-ordinators of the Travel Plan. The co-ordinators would be responsible for:

- > Organising the monitoring of the performance of the Travel Plan against the targets that have been agreed.
- > Reviewing the occupancy / use (and abuse) of the facilities that are provided on site, for example, cycle racks, motorcycle and motorcar parking spaces, clothes lockers, and travel information etc.

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Attachment 5 - ACU Development Plan - Part 4



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- > Reviewing the operational success or failure of the plan and produce a review report highlighting successful initiatives and also areas that require improvement. This review report would be issued to Council on an annual basis and would form the platform of an updated Travel Plan, with sign off input from Council sought on a regular basis, the duration of which would be determined by the relevant stake holders.

The suggested 3 stage monitoring program is outlined below:

Stage 1

- > Stage 1, would involve a questionnaire survey of students and staff during the enrolment process. The survey will be useful to collect information on the travel characteristics of the staff and students and assist in gauging interest in the various Green Travel initiatives and to seek ideas for other Green Travel initiatives.

Stage 2

- > Stage 2, would involve a questionnaire and feedback form to be filled out by Students and Staff 3 months after implementation, in order to determine what Green Travel initiatives are working and which are not. A pro forma for the Stage 2 questionnaire is shown attached as Annex 1.

Stage 3

- > Stage 3, would be the monitoring component of the plan which would be undertaken 6 months after occupation. This questionnaire would test the success rate of the various initiatives and help rework programs to suit the needs of staff and students. A pro forma for the Stage 3 questionnaire is shown attached as Appendix 1.

It is expected that the above stages would form the basis of a continuous monitoring program to gauge the effectiveness of the travel plan.

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Integrated Transport and Access Plan Australian Catholic University, St. Patrick's Campus, Fitzroy

9 Conclusions

- > The Development Plan has been developed to accommodate the ACU's projected growth in students, teaching staff and research from 2015 to 2025, whilst also allowing the consolidation of teaching and research activities onto the Campus.

Specifically it is anticipated that by 2020, ACU will increase their EFT student numbers to 10,700 and staff numbers to 850

- > The individual projects within the Development Plan will include:

115 B Victoria Parade

This project will comprise of the subdivision of 115 Victoria Parade to create a new building site. Within this site, a multi storey building is contemplated, and will include the provision of basement level car parking for 270 spaces.

Vehicular access to this building is planned via Napier Street. Mitigation works will be required at the intersection of Victoria Parade and Napier Street to accommodate two-way vehicle flow within the southern portion of Napier Street.

In principle approval has been secured from VicRoads and Council to the suite of mitigation works proposed.

28 - 42 Young Street (Existing Deck Car Park)

The existing 250 space deck car park is to be demolished and in its place a new building is proposed.

This new building will have no car parking spaces provided, instead, the development of this site will provide the opportunity to make improvements to pedestrian access and connectivity between the campus buildings, and the adjacent campus uses and activities.

- > The key principle that will inform the Development Plan from a transport perspective is based on the City of Yarra's commitment to reduce car dependency by promoting walking, cycling and public transport use.

Specifically, the St Patrick's Campus Development Plan seeks to become a pedestrian oriented space that is safe for students and prioritises sustainable modes of transport by:

- Improving access to public transport along Victoria Parade, Brunswick Street and Gertrude Street;
- Ensuring pedestrian links from the Campus integrate and form part of the wider pedestrian network;

The above will be achieved through the creation of strong pedestrian linkages through the Campus and to adjacent uses, activities and transport networks. Notably, the facade treatment to Victoria Parade on the Mary Glowrey Building is to be removed and by so doing, substantially increasing the footpath width along Victoria Parade in the section between Young Street and Napier Street. This will be particularly beneficial at the interface of the Mary Glowrey Building and the existing bus stop.

- > A total of 270 spaces are proposed as part of the 115B Victoria Parade Project and results in the loss of 50 existing at-grade spaces, whilst the 28 - 42 Young Street project contemplates the demolition of the 250 space deck car park, with no additional parking provided on this site. Accordingly the Development Plan seeks to provide a net decrease of 30 spaces, against a statutory requirements of 280 spaces.

- > The proposed 115B Victoria Parade Car Park will be operated and managed as follows:

- The parking area will be secured by boom gates, activated by card readers and ticket machine on entry and departure during the facility's operating hours, anticipated to be between 7:00am – 10:30pm Weekdays and 8:00am – 10:30pm weekends consistent with current operational practices for the existing car park. During afterhours the car park will be secured by roller doors.
- To alert casual parkers entering the car park and ensure ease of use, clearly visible advisory signs will be posted on the car park entry and also within the car park. Additionally to avoid conflict between long term (staff) and short term (students) users, spaces will be designated to individual staff members once they are determined, with signage posted at the end of each bay.
- Operation of the public car park will be limited to the hours listed above, with its fee structure expected to be based on an hourly charge rate in-line with other commercial car park facilities in the vicinity of the site. The actual charge rates will however be determined once an operator is appointed. The car park is to be operated by ticket collection on entry, and then payment at a pay station likely to be located adjacent to the lifts.

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Attachment 5 - ACU Development Plan - Part 4



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- Dynamic signage on Victoria Parade will indicate if the car park is full, which will reduce the number of vehicles turning into Napier Street. Notwithstanding, U-turn movements can be undertaken within the ROW located on the east side of Napier Street midblock between the site access and Victoria Parade.
- > The proposal triggers a statutory requirement to provide 38 bicycle spaces. A total of 80 bicycle parking spaces are proposed. This provision is double the statutory requirements, and as such is in line with the sustainable transport objectives set out in local and state level policies.
- > An assessment of the post development intersection operating conditions shows that with the mitigation works proposed, the Victoria Parade and Napier Street intersection will to operate under excellent conditions, with motorists experiencing limited queues and delays.

Additionally, an assessment of the operation of the site access has revealed 95th %ile queues of 4 vehicles are anticipated during the critical AM peak period. The design of the car park access and proposed location of the control points is such that motorists will be afforded a queuing distance of about 24 metres, measured from the title boundary.

This setback can accommodate about 4 vehicles and as such queues on to Napier Street are not anticipated. The design and location of the proposed car park access is therefore not expected to result in conditions that obstruct traffic flows along Napier Street or Victoria Parade.

- > ACU will also actively pursue Green Travel initiatives aimed at encouraging people to choose sustainable travel alternatives such as cycling, walking or public transport, over private car usage.

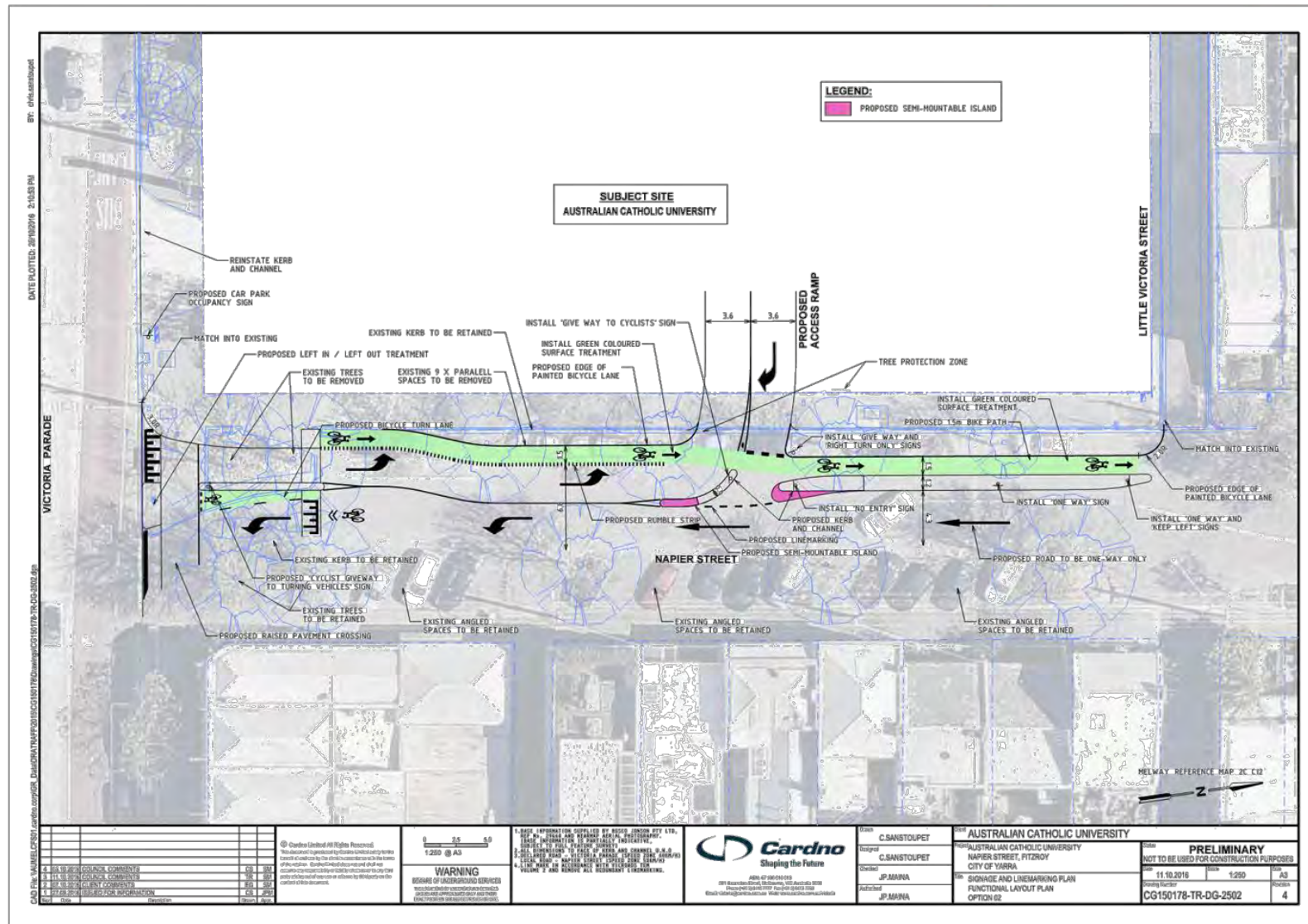
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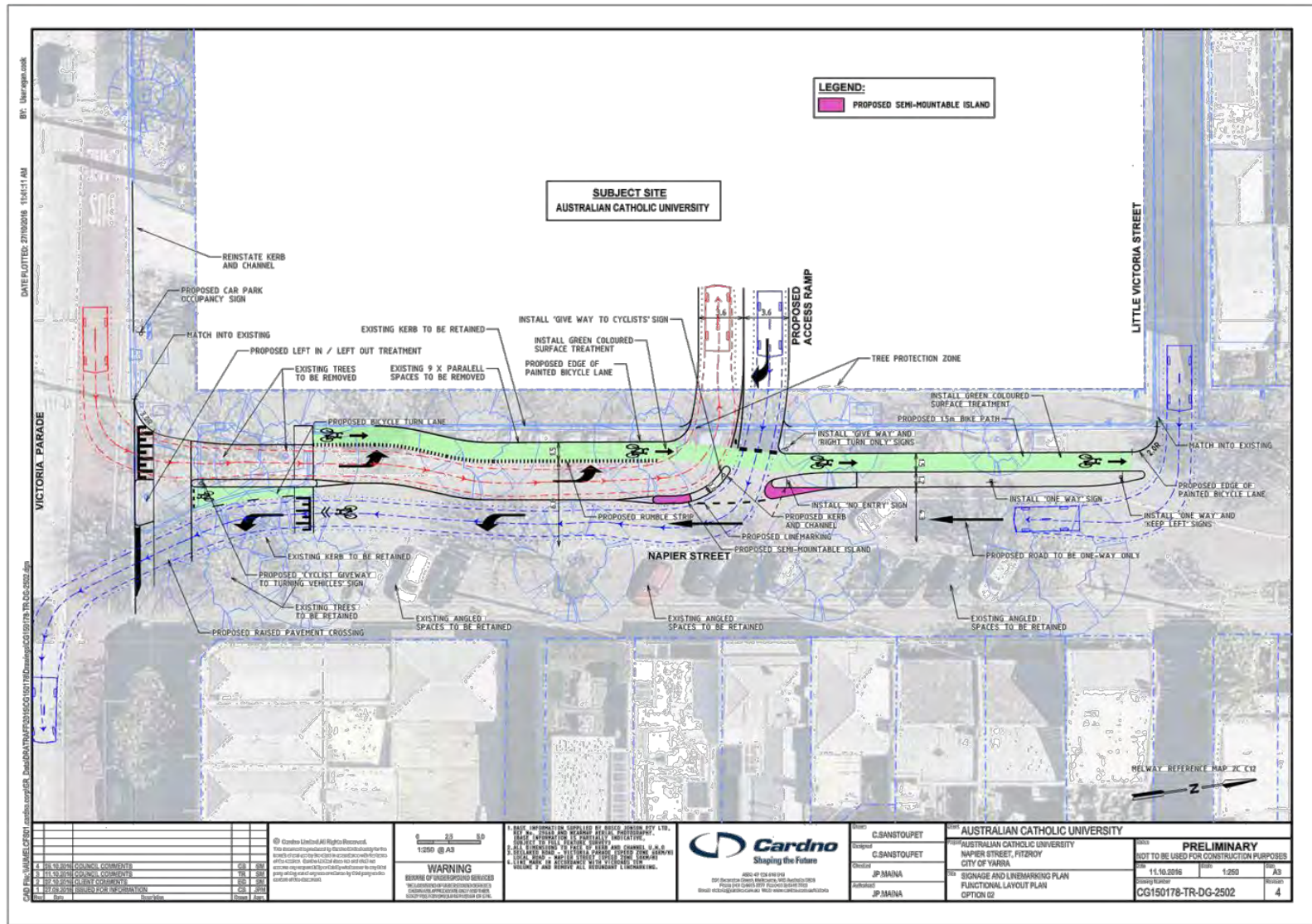
Australian Catholic University,
St. Patrick's Campus, Fitzroy

APPENDIX
A
CONCEPT
FUNCTIONAL
LAYOUT PLANS

Shaping the Future



Attachment 6 - ACU Development Plan - Part 5



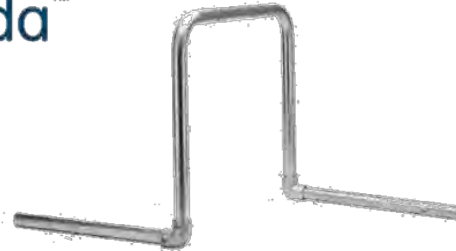
Attachment 6 - ACU Development Plan - Part 5



Attachment 6 - ACU Development Plan - Part 5



Anaconda™



Galvanised finish

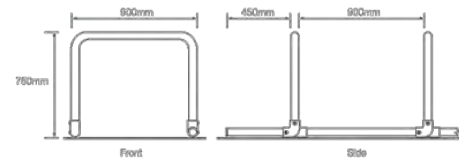
Features



Anaconda rails are a freestanding version of the Flat Top. These are an excellent solution for use on asphalt or for temporary event parking. Available in a variety of materials and finishes in both adult and junior sizes.

- Easy to use with any bike lock
- Freestanding, self-supporting and can be fastened to all surfaces
- Using clamp-on elbows to join the rails
- Powder coated, galvanised, alloy or stainless steel rail tubing available
- Parks two bicycles per rail in either parallel or diagonal set up
- Supports all styles of bicycles in an upright position

Dimensions



Specifications

Material options

- 316 Marine grade stainless steel
- Galvanised
- Powder coated
- Alloy

Fixing options

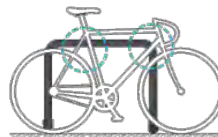
- Clamp on

Recommended fasteners

- Zinc plated Dynabolts (M10 x 65mm)
- Stainless Dynabolts (M10 x 65mm)

Dimensions
900mm [w] x 750mm [h]

Locking points



V1.5 - 10/04/2016 | Specifications may be subject to change without notice. ©2016 Bicycle Network

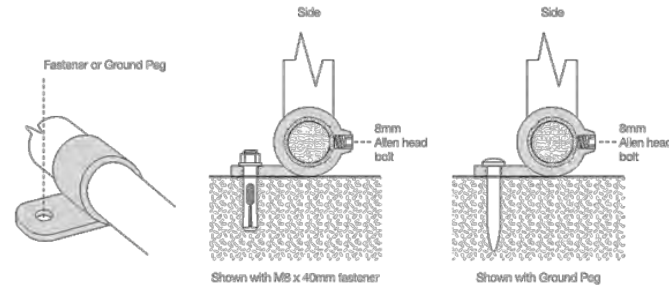
Design. Supply. Install.

Bicycle Network ABN 41 028 835 908
p. 1900 727 689 e. parking@bicycletnetwork.com.au bikeparking.com.au
VIC Level 4, 248 Bourke St, Melbourne 3000 NSW 234 Crown St, Darlinghurst 2010
TAS 210 Collins St, Hobart 7000

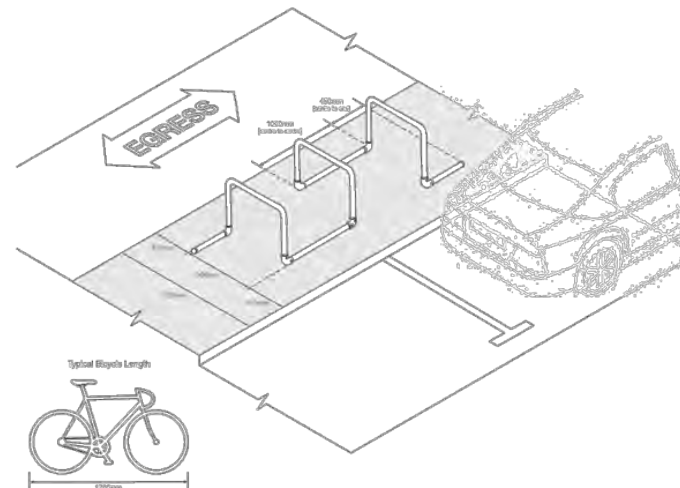


Fixing options

Fix to the ground with Ground Tabs fitted over the piping at each end and secured with 8mm Allen bolts. Use either a Fastener or Ground Peg to secure the Anaconda to the ground.



Layout guidelines



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Attachment 6 - ACU Development Plan - Part 5





Arc de Triomphe™



Stainless steel finish

Features



- Each rail supports two adult bikes in an upright position
- Can be either bolted to a concrete slab or concreted in situ
- Available in stainless steel or galvanised steel
- Provides the ability to lock both wheels and frame
- Suitable for foyers and entry areas

Dimensions



Welded base plate

Concrete footing

Specifications

Material options

- 316 Marine grade stainless steel
- Galvanised

Fixing options

- Welded flange
- In situ

Recommended fasteners

- Galvanised Dynabolts (M10 x 65mm)
- Stainless Dynabolts (M10 x 65mm)
- Shear Nut security fasteners

Dimensions

1000mm [w] x 850mm [h]

Locking points





V1.1 - 16/09/2010 | Arc de Triomphe may be subject to change without notice. ©2010 Bicycle Network

Design. Supply. Install.

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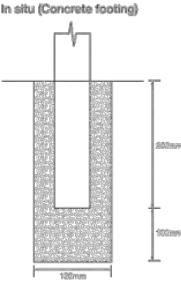




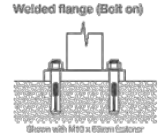
Arc de Triomphe™

Fixing options

In situ (Concrete footing)

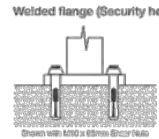


Welded flange (Bolt on)



Shown with M10 x 65mm fastener

Welded flange (Security heads)

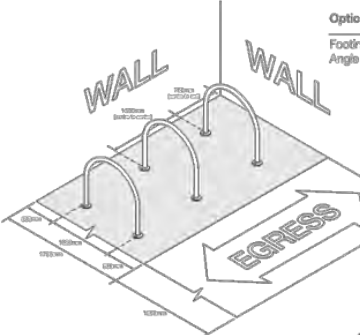


Shown with M10 x 65mm Security Head

Layout guidelines


Option 1:

Footing Width 1700mm
Angle 0°

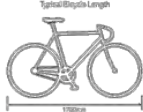


Option 2:

Footing Width 1200mm
Angle 45°




Typical Bicycle Length



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Attachment 6 - ACU Development Plan - Part 5



BICYCLE
SYSTEMS

Flat Top™

Stainless steel finish

Features

- Each rail supports two adult bikes in an upright position
- Can be either bolted to a concrete slab or concreted in situ
- Available in stainless steel or galvanised steel
- Provides the ability to lock both wheels and frame
- Suitable for interior use including storage cages

Specifications

Material options

- 316 Marine grade stainless steel
- Galvanised

Fixing options

- Welded flange
- In situ

Recommended fasteners

- Galvanised Dynabolts (M10 x 65mm)
- Stainless Dynabolts (M10 x 65mm)
- Shear Nut security fasteners

Dimensions
950mm (w) x 750mm (h)

Locking points

Dimensions

900mm
750mm
1200mm
200mm
Concrete footing

Design, Supply, Install.

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TAS 210 Collins St, Hobart 7000

BIKE PARKING EXPERTS™

Fixing options

In situ (Concrete footing)

Welded flange (Bolt on)

Shown with M10 x 65mm fastener

Welded flange (Security heads)

Shown with M10 x 65mm Shear Nuts

Layout guidelines

Option 1:
Footing Width 1700mm
Angle 0°

Option 2:
Footing Width 1200mm
Angle 45°

Typical Bicycle Length
1200mm

Design, Supply, Install.

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TAS 210 Collins St, Hobart 7000

BIKE PARKING EXPERTS™

Ned Kelly[®]™

Galvanised finish

Black powder coat finish

Features

- Each rail provides storage for a single bike
- Suits bikes with full length mud guards
- Available in galvanised or powder coat over mild steel
- Provides the ability to lock the main frame and one wheel
- Support prongs with protective coating prevent damage to rim
- Can be used with custom framing – no wall needed

Dimensions

Specifications

Material options

- Galvanised
- Powder coat over mild steel
- Stainless steel*

Fixing options

- Bolt on to wall
- Fixed to support framing

Recommended fasteners – wall

- Dynabolt (M8 x 40mm)
- Steiner Nut security fasteners

Recommended fasteners – framing

- Bolt and nut (M10 x 60mm)
- Tek screws

Dimensions

125mm [w] x 700mm [h] x 600mm [d]

* Pre-order only

Locking points

Design. Supply. Install.

Bicycle Network ABN 41 028 835 903

p. 1900 737 888 e. parking@bicycletnetwork.com.au bisparking.com.au

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TAS 210 Collins St, Hobart 7000

V1.1 - 16/09/2019 | Spec2019-09-16 for callout to change to final release. ©2019 Bicycle Network

**BIKE
PARKING
EXPERTS[®]**

Bike Parking Experts | Ned Kelly

Fixing options

Fix to a wall using 4x fasteners or Shear Nuts

Shown with M8 x 40mm fastener

Shown with M8 x 40mm Shear Nut

Fix to a frame using 4x bolts or Tek Screws

Shown with M10 x 60mm Bolt, Washer & Nut

Shown with Tek Screw

Layout guidelines

Option 1:

Fitting 4 Ned Kellys to a wall

Option 2:

Fitting 6 Ned Kellys to a Duragal steel frame

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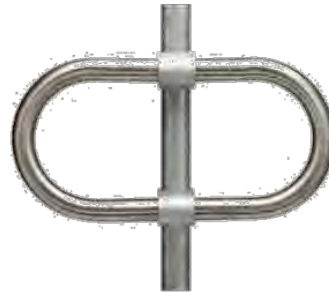
Design. Supply. Install.

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p. 1300 727 669 e. parking@bikeparkingnetwork.com.au bikeparking.com.au
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Pole Vault™



Stainless finish

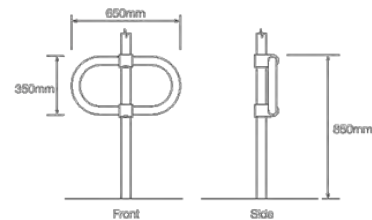
Features



The Pole Vault is an innovative street sign mounted bike parking rail that can be fitted in minutes with no drilling or concrete required.

- Can be retro-fitted to existing sign poles
- Accommodates a wide range of sizes and styles of bikes
- Can be moved and re-fitted to different locations
- Comes in standard galvanised, powdercoat or stainless material
- Easy to use with any bike lock
- Supports the entire bike so it won't slip or fall over

Dimensions



Specifications

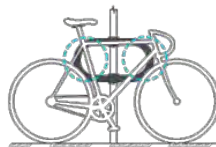
Material options
 - 316 Marine grade stainless steel
 - Galvanised

Fixing options
 - Clamp on

Recommended fasteners
 - 8mm Allen head bolts

Dimensions
 650mm [w] x 350mm [h]

Locking points

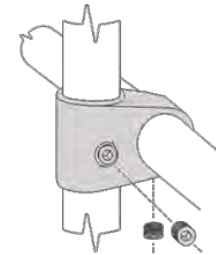


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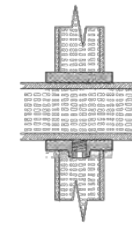
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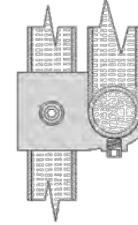
Fixing options



2 x 8mm Allen head bolts per clamp



Front

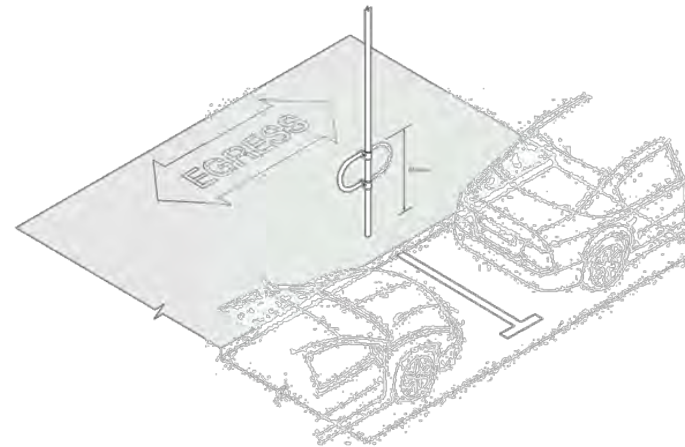


Side



Bike Parking Experts | Pole Vault™

Layout guidelines



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Towel Hitching™



Zinc treated finish

Features



The Towel Rail is a space-efficient solution for parking single bikes against a wall, useful in narrow corridors.

- Space-efficient, can be fitted to narrow corridors
- Can be supplied in powdercoat or galvanised finish
- Accommodates all types and sizes of bicycle
- Is easy to use with any bike lock
- Requires no lifting

Dimensions



TOP

SIDE

Specifications

Material options

- Powder coated
- Stainless steel
- Zinc treated
- Galvanised

Fixing options

- Welded flange - Bolt On

Recommended fasteners

- Zinc plated dynabolts (M8 x 40mm)

Dimensions

900mm (w) x 150mm (d)

Locking points





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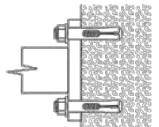




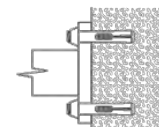
Towel Hitching™

Fixing options

Fix to a wall using 4x fasteners or Shear Nuts

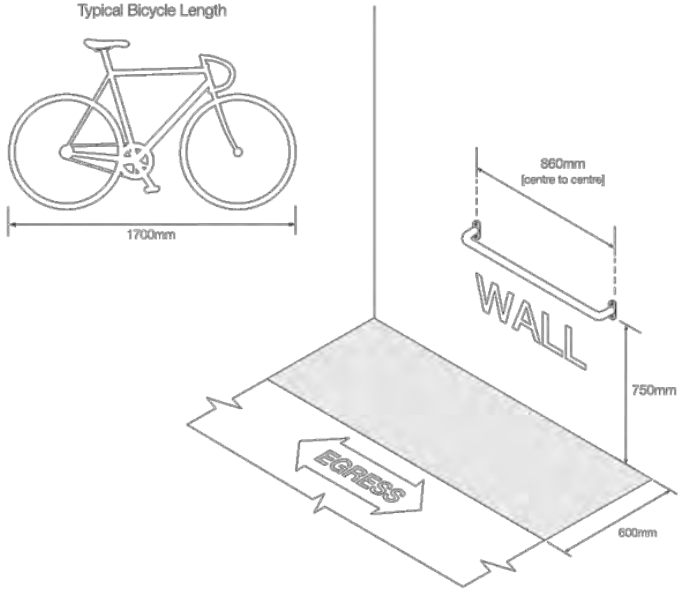


Shown with M8 x 40mm fastener



Shown with M8 x 40mm Shear Nuts

Layout guidelines




WALL

FLOOR

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Pedestrian comfort at 115B Victoria Parade



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Wind Microclimate Assessment
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Wind Microclimate Assessment

Pedestrian comfort at 115B Victoria Parade

Client: Australian Catholic University

ABN: 15050192660

Prepared by

AECOM Australia Pty Ltd
Level 10, Tower Two, 727 Collins Street, Melbourne VIC 3008, Australia
T +61 3 9853 1234 F +61 3 9854 7117 www.aecom.com
ABN 20 083 040 925

19-Oct-2016

Job No.: 60519200

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Date 19-Oct-2016
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Reviewed by Sian Willmott

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Revision	Revision Date	Details	Authorised	
			Name/Position	Signature
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1

Executive Summary

This report assesses the likelihood that wind-induced discomfort may be felt by patrons of various outdoor spaces around the proposed 115B Victoria Parade development for the Australian Catholic University. The assessment draws on a large local meteorological data set adjusted for location and height.

Winds are assessed against the Lawson comfort criteria, which sets upper wind speeds for comfort during a range of typical activities — long-term sedentary (e.g. sitting at café tables), short-term sedentary (e.g. queuing, talking), slow movement (e.g. window shopping, strolling), and rapid movement (e.g. fast walking).

Based on hourly wind speed and direction data collected from the Melbourne Regional Office (located approximately 1km to the west of the site), predominant winds are west south westerly and north easterly throughout the year and during business hours, as well as outside of business hours. These two key wind directions have therefore been the focus of the analysis provided for this submission.

Seven locations within and around the site have been identified as being sensitive to wind speed. These are summarised in the table below, along with the desired wind comfort and activity category. Potential issues have been highlighted.

Table 1: Wind assessment summary and impacts

Location	Desired wind comfort and activity category	Potential wind microclimate impacts
1 Ground level outdoor seating around within the site boundary	Comfortable for pedestrians sitting or standing for longer periods of time	Channelling of winds along Victoria Parade – some local mitigation may be required.
2 Along Napier Street	Comfortable for pedestrians leisure walking	Unlikely to be significant impacts caused by the proposed building.
3 Along Victoria Parade	Comfortable for pedestrians leisure walking	Unlikely to be significant impacts caused by the proposed building.
4 Level 4 Terrace	Comfortable for pedestrians sitting or standing for shorter periods of time	Exposed to predominant winds – balustrades likely to provide sufficient protection.
5 Level 6 Terrace	Comfortable for pedestrians sitting or standing for shorter periods of time	Exposed to predominant winds – balustrades likely to provide some protection. Additional local mitigation may be required.
6 Along Young Street	Comfortable for pedestrians leisure walking	Unlikely to be significant impacts caused by the proposed building.
7 Along Little Victoria Street	Comfortable for pedestrians leisure walking	Unlikely to be significant impacts caused by the proposed building.

Due to the relatively low wind speeds experienced in this area of Melbourne, there are unlikely to be any significant adverse impacts at Ground Level within or surrounding the site due to the construction of the proposed 115B Victoria Parade development. The staggered, non-uniform shape of the building envelope is likely to minimise wind acceleration, with horizontal shading further assisting in minimising potential issues.

A detailed computational analysis will be undertaken during the next design stage which will quantify expected wind speeds and compare these against the Lawson comfort criteria.

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1.0 Introduction

It is somewhat inevitable that, with the construction of a new development, the wind microclimate in the vicinity will be changed. Where new buildings are significantly different in size and form, orientation or height from those in the immediate vicinity, winds can be introduced which may cause discomfort to pedestrians. The design of a development should therefore consider the provision of a quality outdoor environment, which is appropriate for its designated use for the majority of the year. Due to the height and layout of the proposed building at 115B Victoria Parade, a number of potentially adverse wind effects may be experienced.

This study is an experience based qualitative review of the pedestrian level wind environment around the proposed 115B Victoria Parade development. The assessment of wind conditions is based upon our experience with other similar schemes and our knowledge of the interaction of the wind with the built environment. The wind conditions around the proposed development in relation to planned pedestrian activities have been considered and an assessment of the potential impact that the proposed development may have on the surrounding area's wind microclimate is provided. Areas where wind is likely to be accelerated by geometrical features are highlighted and ways to mitigate these effects are recommended.

Local authority planning guidelines typically focus on user comfort and safety. Here, users are usually taken to be at ground level (or the main podium level etc.) of a particular space. They may be passing through on foot or bicycle; wandering slowly (e.g. talking); sitting or standing for a short period (e.g. at a bus stop) or for a long period (e.g. outdoor dining). Planning criteria focus on the frequency of high winds, which are known to be uncomfortable in various situations and for various durations of exposure.

Upper-level users of balconies etc. may also be affected by winds, but exposure is generally considered to be by choice and easily avoidable. In rare cases, outdoor dining or other regular outdoor activity may take place relatively high above ground level, in which case special assessments must be made.

As this is a desk based study, quantification of likely increases or decreases in wind speed cannot be given and only an indication of likely conditions that pedestrians will experience is presented here.

1.1 Purpose, basis and limitations of this report

The purpose of this report is to provide evidence as to the likelihood of wind-induced discomfort to ground-level users of the spaces around the building.

This report presents probabilistic estimates of the likelihood of events which may have comfort implications. These are based on historical wind data and measured against commonly available wind effects criteria using accepted estimated methods. Given these limitations, AECOM cannot guarantee with certainty that the development will not adversely impact upon safety and comfort in the public realm.

This report is based on drawings and other information supplied; a statistical analysis of data; published methodologies for wind assessment; and experience with assessing wind flows around buildings. The statistical analysis does not include an allowance for rare high-wind events such as severe storms. Also note that future wind patterns may not reflect past wind patterns. For example, changes in wind climate due to global warming are not accounted for. This report does not address structural aspects of wind phenomena.

All advice is provided with best intent and to the accuracy limits of the nature of the assessment undertaken.

2.0 Assessment criteria

2.1 Basis

Wind speed and gustiness are the primary measurable factors affecting people's comfort. Other factors such as air temperature and humidity, clothing, sun exposure, etc. are also significant, but these can often be addressed by a modification of effective wind speeds (Twidell, 2006).

Wind speed is understood to mean the average wind speed taken over a time of one hour or so. Gustiness refers to the rate of change of wind speed, usually identified with the turbulent intensity defined by ratio of the standard deviation of the mean wind speed to the mean itself. The important wind gusts are those lasting 2–3 seconds, being the time taken to perform a simple act such as a few walking steps, opening a door etc.

Gustiness is a difficult factor to assess on the urban micro-scale. Fortunately, the implied turbulent intensity may be related to the underlying means in order to recast gustiness criteria in terms of mean wind speed (Twidell, 2006), (Melbourne, 1978), (ASHRAE, 2001), (Blocken, 2004). Estimates of turbulent intensity in urban situations range from 15% to 30% (Twidell, 2006), implying that gust wind speeds are generally 1.5–2.0 times greater than mean wind speeds.

2.2 Comfort

In general, comfort criteria relate to both the thermal effects of wind on people, and the mechanical effects of wind on their activities.

The comfort criteria used in this study is the Lawson criteria (Lawson, 1978), based on the probability of exceeding certain mean wind speeds. The criteria are presented in Table 2. Wind conditions are unacceptable when the probability of the mean wind speed exceeding the given number is greater than 5%.

Table 2: The Lawson wind comfort criteria

Threshold wind speed (m/s)	Activity
4	Uncomfortable for pedestrians in the vicinity of entrance doors or sitting outside for long periods of time, such as outdoor cafes.
6	Uncomfortable for pedestrians standing or sitting for shorter periods of time, such as queuing or talking.
8	Uncomfortable for pedestrians 'leisure walking' e.g. strolling, window shopping and sightseeing.
10	Uncomfortable for pedestrians walking quickly e.g. walking to a destination, and cycling.

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3.0 The local wind climate

3.1 Meteorological data

The wind data was taken from the Bureau of Meteorology automatic weather station at Melbourne Regional Office (RO)¹, which is located on the corner of La Trobe Street and Victoria Parade, approximately 1km to the west of 115B Victoria Parade.

The wind speed data was rescaled to account for the difference in land surface structure between the meteorological station and the development site, and the height difference between the anemometer and the level at which people are affected (assumed to be 1.5m above ground level). The rescaling was accomplished using a logarithmic-law approximation to a neutrally stable atmospheric boundary layer profile (Pasquill-Gifford Class D) (Oke, 2006) using the equation:

$$u_z = \frac{u_*}{\kappa} \ln \frac{z}{z_0}$$

In which u_z is the wind speed at height z (1.5m for pedestrian height), u_* is the friction velocity which is based on the reference wind speed from Melbourne RO, κ is von Karman's constant (≈ 0.4) and z_0 is the roughness height (taken as 2m for this site to account for physical obstructions such as cars).

Wind speeds below 0.5 m/s are registered by the anemometer as zero (calm).

3.2 Summary statistics

The wind data was analysed to assess the likelihood of uncomfortable winds, without allowing for the presence of the development. Local wind effects due to the development will be discussed in the next section.

The analysis was carried out using:

- The entire data set, representing wind conditions 24 hours a day
- A subset restricted to the hours of 7am to 7pm (business hours) when outdoor areas would be most active

3.2.1 Wind speed

Calm conditions occur only rarely (5%) during business hours, slightly more frequently (8%) during winter. Wind speeds at pedestrian height are low compared to the comfort criteria (Figure 1 on the following page). Based on wind speeds experienced at the Melbourne RO, i.e. not taking in to account acceleration caused by the built environment local to 115B Victoria Parade, wind speeds are likely to be comfortable for long term sedentary activities for the majority of the time.

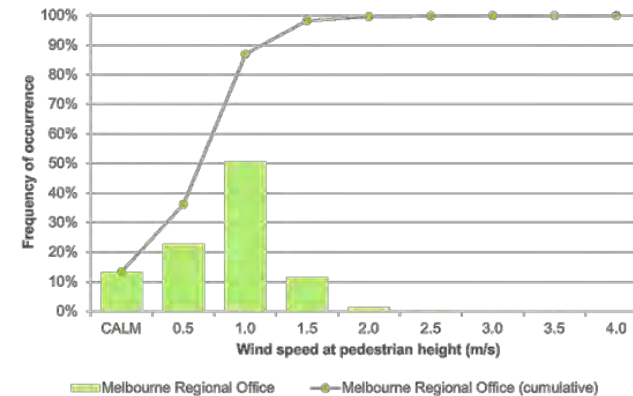


Figure 1: Distribution of wind speeds by band (bars) and cumulatively (line).

¹ Station number 085071. The data consisted of hourly wind speed and direction, maximum gust speed, temperature and other variables from 3/3/1997 to 31/8/2009. After quality checks, there were a total of 94,026 suitable records.

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3.2.2 Wind direction

Figure 2 shows the frequency of winds from each direction (divided in to 10° increments) for all hours and seasons (top left), divided by season for all times of the day (top right) and divided by season during business hours (bottom). Within the CBD, there is very little variation in predominant winds between seasons, and during business hours and outside of business hours, with west south westerly winds occurring most frequently, closely followed by north easterly winds. Hence west south westerly and north easterly winds will be the focus of the assessment in section 4.0.

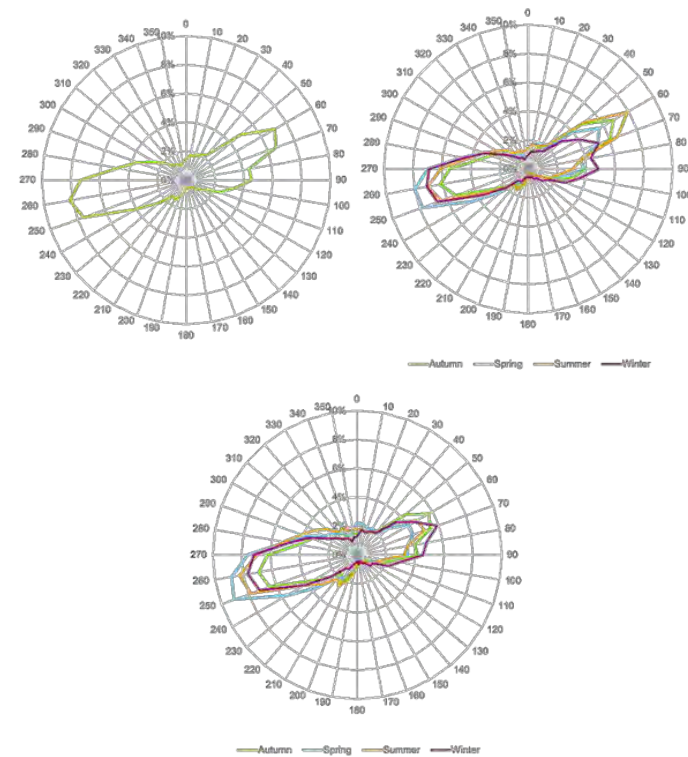


Figure 2: Distribution of wind heading for all times and seasons (top left), all times divided by season (top right), and by season during business hours (bottom).

19-Oct-2016

4.0 Comfort assessment

4.1 Wind-sensitive locations

Figure 3 and Figure 4 identify the locations that are potentially sensitive to wind flows, both within the site boundary, and the impact that the proposed building may have on adjacent streets. Each location is discussed in the following sections.



Figure 3: View from south east

1. Ground level outdoor seating area
2. Along Napier Street
3. Along Victoria Parade
4. Level 4 Terrace
5. Level 6 Terrace

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- 6. Along Young Street
- 7. Along Little Victoria Street



Figure 4: View from north west

The terrace area is exposed to north easterly winds with no shelter provided by surrounding buildings as they are all considerably lower than this level. The balustrades (currently shown at around 1.6m high) will provide protection to users of this space, and an estimate of likely wind speeds will be provided in the detailed analysis in the next stage of design.

4.1.5 Level 6 Terrace

As with Level 4, the Level 6 Terrace should be suitable for shorter periods of sitting or standing, however exposure is generally considered to be by choice and easily avoidable.

This terrace is exposed to both west south westerly winds and north easterly winds, and so has the potential to experience elevated winds compared to the Level 4 Terrace, due to the depth of the terrace. Again, the balustrades (currently shown at 1.6m high) will provide protection to users, and further analysis will be provided in the next design stage.

4.1.6 Along Young Street

Conditions along Young Street should be suitable for leisure walking as a minimum. The proposed building is unlikely to accelerate wind speeds above this threshold, and more detailed analysis will be undertaken during the next stage of design in order to confirm this.

4.1.7 Along Little Victoria Street

Conditions along Little Victoria Street should be suitable for leisure walking as a minimum. The proposed building is unlikely to accelerate wind speeds above this threshold, and more detailed analysis will be undertaken during the next stage of design in order to confirm this.

4.1.1 Ground level outdoor seating area

This area is required to be comfortable for pedestrians sitting or standing for longer periods of time.

West south westerly winds are likely to channel along Victoria Parade from the city. Buildings on the opposite side of Victoria Parade are likely to provide little shelter, however winds are unlikely to be excessive in speed. Local mitigation features may be required, and this will be confirmed during the detailed analysis that will be carried out during the next design stage.

The adjacent buildings across Napier Street are likely to provide some shelter from north easterly winds, with down was on the eastern façade unlikely due to the staggered floor plate of the buildings and horizontal fins used for provide shade.

4.1.2 Along Napier Street

Conditions along Napier Street should be suitable for leisure walking as a minimum. The proposed building is unlikely to accelerate wind speeds above this threshold, and more detailed analysis will be undertaken during the next stage of design in order to confirm this.

4.1.3 Along Victoria Parade

Conditions along Victoria Parade should be suitable for leisure walking as a minimum. The proposed building is unlikely to accelerate wind speeds above this threshold, and more detailed analysis will be undertaken during the next stage of design in order to confirm this.

4.1.4 Level 4 Terrace

The Level 4 Terrace should be suitable for shorter periods of sitting or standing, however exposure is generally considered to be by choice and easily avoidable.

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5.0 Summary

5.1 Likelihood of discomfort

A high level desktop study of likely wind conditions around the proposed 115B Victoria Parade building and surrounding streets has been undertaken. Due to the relatively low wind speeds experienced in this area of Melbourne, there are unlikely to be any significant adverse impacts at ground level within or surrounding the site due to the construction of this building. The staggered, non-uniform shape of the building envelope is likely to minimise wind acceleration, with horizontal shading further assisting in minimising potential issues.

The terraces on Levels 4 and 6 are relatively exposed to prevailing winds. Although the current balustrades will provide some protection, additional local mitigation measures may be required.

A detailed computational analysis will be undertaken during the next design stage which will quantify expected wind speeds and compared these against the Lawson comfort criteria.

5.2 Mitigation

No additional mitigation requirements are proposed at this stage, however smaller local features may need to be incorporated following the detailed analysis.

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Attachment 6 - ACU Development Plan - Part 5



**ARBORICULTURAL REPORT
ARBORICULTURAL IMPACT ASSESSMENT
AUSTRALIAN CATHOLIC UNIVERSITY
115B VICTORIA PARADE, FITZROY**

PREPARED BY
Simon Howe
Consultant Arborist
B.AppSci(Hort)
GradDip Plan&Des (LandscapeArch) MELB

October 2016



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PAGE 1

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1 INTRODUCTION

- 1.1 Trees within and adjacent to the Australian Catholic University, 115B Victoria Parade, Fitzroy were assessed on the 15th September, 2016. Trees within the subject site as well as trees in adjacent road reserves (Young and Napier Streets, Victoria Parade) were assessed as part of the study.

2 DISCUSSION

- 2.1 32 trees or tree groups were assessed as part of this study:

- 10 trees or tree groups within the subject site have been assessed of medium retention value;
- 8 trees within the subject site have been assessed of low retention value;
- 14 trees have been assessed outside the subject site.

- 2.2 Of the ten trees assessed within the site of medium retention value, nine are semi-mature Pin Oaks within the car park in the east of the site. These are generally developing well within a highly urbanised environment. A pair of Bangalow Palms (Tree 1) are located adjacent to the north- west corner of the car park.

TABLE 1 Trees assessed of medium retention value

No.	Species	Common Name
1	<i>Archontophoenix cunninghamiana</i>	Bangalow Palm
2	<i>Quercus palustris</i>	Pin Oak
3	<i>Quercus palustris</i>	Pin Oak
4	<i>Quercus palustris</i>	Pin Oak
5	<i>Quercus palustris</i>	Pin Oak
6	<i>Quercus palustris</i>	Pin Oak
10	<i>Quercus palustris</i>	Pin Oak
11	<i>Quercus palustris</i>	Pin Oak
12	<i>Quercus palustris</i>	Pin Oak
13	<i>Quercus palustris</i>	Pin Oak

- 2.3 The balance of trees assessed within the site are of low retention value, Silver Birch located to the periphery of the car park. The low retention value of these trees is a reflection of their overall condition and limited existing and potential amenity value.

TABLE 2 Trees assessed of low retention value

No.	Species	Common Name
7	<i>Betula pendula</i>	Silver Birch
8	<i>Betula pendula</i>	Silver Birch
9	<i>Betula pendula</i>	Silver Birch
14	<i>Betula pendula</i>	Silver Birch
15	<i>Betula pendula</i>	Silver Birch
16	<i>Betula pendula</i>	Silver Birch
17	<i>Betula pendula</i>	Silver Birch
18	<i>Betula pendula</i>	Silver Birch

- 2.4 Trees assessed outside the site are street trees in the Napier Street, Young Street and Victoria Parade road reserves to the east, west and south respectively. Young Street is planted with developing Golden Robinia (Trees 19-25), their overall form reflecting the limited light between tall buildings in this section of Young Street. An older Locust (Tree 26) is located in the south west of the study area within Victoria Parade.

- 2.5 The west side of Napier Street is planted with a pair of semi-mature Pin Oaks (Trees 27 and 28) located in a roadside bed at the intersection of Victoria Parade, with a row of established Elms (Trees 29-32) planted in roadside cut-outs to the north of the intersection. The Elms are part of a larger avenue plantation that provide a high level of amenity to the streetscape.

TABLE 3 Trees assessed outside the site

No.	Species	Common Name
19	<i>Robinia pseudacacia</i> 'Frisia'	Golden Robinia
20	<i>Robinia pseudacacia</i> 'Frisia'	Golden Robinia
21	<i>Robinia pseudacacia</i> 'Frisia'	Golden Robinia
22	<i>Robinia pseudacacia</i> 'Frisia'	Golden Robinia
23	<i>Robinia pseudacacia</i> 'Frisia'	Golden Robinia
24	<i>Robinia pseudacacia</i> 'Frisia'	Golden Robinia
25	<i>Robinia pseudacacia</i> 'Frisia'	Golden Robinia
26	<i>Robinia pseudacacia</i>	Locust
27	<i>Quercus palustris</i>	Pin Oak
28	<i>Quercus palustris</i>	Pin Oak
29	<i>Ulmus procera</i>	English Elm
30	<i>Ulmus procera</i>	English Elm
31	<i>Ulmus procera</i>	English Elm
32	<i>Ulmus procera</i>	English Elm

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<p>Australian Catholic University, 115B Victoria Parade Fitzroy ARBORICULTURAL REPORT ARBORICULTURAL IMPACT ASSESSMENT</p> <p>October 2016</p> <p>3 IMPACT OF PROPOSED DEVELOPMENT</p> <p>3.1 Development of the site is proposed, including construction of a new 13 level building over basement car park in the existing car parking area. A new vehicular access is proposed from Napier Street, with changes proposed to the road functional layout of Napier Street. The following drawings have been reviewed in the preparation of these notes:</p> <p><i>Lyons Architects ACU Development Plan October 2016;</i></p> <p><i>&</i></p> <p><i>Signage and Linemarking Plan. Functional Layout Plan Option 02 CG150178-TR-DG-2502 Rev 3 11.10.2016</i> <i>Australian Catholic University, Napier Street, Fitzroy. City Of Yarra</i> <i>Prepared by Cardno</i></p> <p>Site Trees</p> <p>3.2 All trees assess within the site will require removal to facilitate development. None are considered to be of sufficiently high value to require redesign to ensure retention.</p> <p>Trees Outside the Site</p> <p>3.3 Two trees outside the site, a pair of Pin Oaks (Trees 27 and 28) at the south western end of Napier Street will require removal for the revised road functional layout.</p> <p>3.4 The balance of trees to the west side of Napier Street outside the development area can be retained, subject to appropriate protection conforming to AS4970-2009 <i>Protection of Trees on Development Sites</i>. The only noted encroachments by works are for:</p> <ul style="list-style-type: none"> ▪ The basement, <1% of the tree protection zone of Tree 29. ▪ The basement ramp crossover, <= 3% for Tree 30, <1% for Tree 31. <p>3.5 All are minor encroachments under the provisions of AS4970-2009.</p> <p>3.6 The road functional layout has been developed so that new kerb and channel is limited to the crossover within the tree protection zones of trees to be retained, and traffic separation within TPZs can be accomplished with line-marking and surface texturing.</p> <p>3.7 There may be the potential to provide permeability through removal of bituminous concrete to portions of the west side of Napier Street (currently sealed to the kerb) to improve growing conditions for existing trees.</p> <p>JOHN PATRICK LANDSCAPE ARCHITECTS PTY LTD OFFICE REF: 16-794TS-01.docx</p> <p>PAGE 3</p>	<p>Australian Catholic University, 115B Victoria Parade Fitzroy ARBORICULTURAL REPORT ARBORICULTURAL IMPACT ASSESSMENT</p> <p>October 2016</p> <p>3.8 A full survey of all trees is included below.</p> <p>3.9 The location of each tree is shown in 7 Appendix 1 – Tree Location Plan.</p> <p>JOHN PATRICK LANDSCAPE ARCHITECTS PTY LTD OFFICE REF: 16-794TS-01.docx</p> <p>PAGE 4</p>
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4 SITE PHOTOGRAPHS



Figure 1 From right, Trees 2, 3 and 4 in the car park.

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Figure 2 Tree 1, a pair of Bangalow Palms.

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Figure 3 Tree 32 in the Napier Street road reserve.

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Figure 4 Golden Robins in the Young Street road reserve.

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5 RESULTS OF TREE SURVEY

Tree-1 *Archontophoenix cunninghamiana*, Bangalow Palm

Origin: Australian native	Type: Evergreen Broadleaf	Age: Semi-mature
DBH (cm): 16	Height: 10m	Width: 5m
Crown class: Symmetrical	Health: Fair-Good	Structure: Fair-Good
Amenity value: Medium	Comments: Can be transplanted	SULE: 20years
Retention Value: Medium	Reason:	
Impact of Development: Remove		

Tree-2 *Quercus palustris*, Pin Oak

Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature
DBH (cm): 35.5	Height: 13m	Width: 6m
Crown class: Symmetrical	Health: Dormant	Structure: Fair-Good
Amenity value: Medium	Comments: Good developing specimen	SULE: 20years
Retention Value: Medium	Reason:	
Impact of Development: Remove		

Tree-3 *Quercus palustris*, Pin Oak

Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature
DBH (cm): 20.5	Height: 6m	Width: 5m
Crown class: Symmetrical	Health: Dormant	Structure: Fair
Amenity value: Low	Comments:	SULE: 20years
Retention Value: Medium	Reason:	
Impact of Development: Remove		

Tree-4 *Quercus palustris*, Pin Oak

Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature
DBH (cm): 23	Height: 11m	Width: 6m
Crown class: Symmetrical	Health: Dormant	Structure: Fair-Good
Amenity value: Medium	Comments: Slightly kinked trunk, otherwise good developing specimen	SULE: 20years
Retention Value: Medium	Reason:	
Impact of Development: Remove		

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Tree-5 *Quercus palustris*, Pin Oak

Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature
DBH (cm): 30	Height: 12m	Width: 6m
Crown class: Symmetrical	Health: Dormant	Structure: Fair
Amenity value: Medium	Comments:	SULE: 20years
Retention Value: Medium	Reason:	
Impact of Development: Remove		

Tree-6 *Quercus palustris*, Pin Oak

Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature
DBH (cm): 31.5	Height: 12m	Width: 7m
Crown class: Symmetrical	Health: Dormant	Structure: Fair
Amenity value: Medium	Comments: Conforted trunk. Codominant pruned out.	SULE: 20years
Retention Value: Medium	Reason:	
Impact of Development: Remove		

Tree-7 *Betula pendula*, Silver Birch

Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature
DBH (cm): 17	Height: 6m	Width: 4m
Crown class: Symmetrical	Health: Dormant	Structure: Fair
Amenity value: Low	Comments:	SULE: 10-20years
Retention Value: Low	Reason:	
Impact of Development: Remove		

Tree-8 *Betula pendula*, Silver Birch

Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature
DBH (cm): 25.5	Height: 10m	Width: 6m
Crown class: Symmetrical	Health: Dormant	Structure: Fair
Amenity value: Medium	Comments: Narrow primary union	SULE: 10-20years
Retention Value: Low	Reason:	
Impact of Development: Remove		

Tree-9 *Betula pendula*, Silver Birch

Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature
DBH (cm): 18	Height: 9m	Width: 6m
Crown class: Symmetrical	Health: Dormant	Structure: Fair-Poor
Amenity value: Low	Comments: Dead wood evident	SULE: 0-10years
Retention Value: Low	Reason:	
Impact of Development: Remove		

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Tree-10			
Quercus palustris, Pin Oak			
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 23.5	Height: 9m	Width: 6m	TPZ: 2.8m
Crown class: Symmetrical	Health: Dormant	Structure: Fair-Good	SULE: 20years
Amenity value: Medium	Comments:		
Retention Value: Medium	Reason:		
Impact of Development: Remove			
Tree-11			
Quercus palustris, Pin Oak			
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 22.5	Height: 8m	Width: 6m	TPZ: 2.7m
Crown class: Symmetrical	Health: Dormant	Structure: Fair-Good	SULE: 20years
Amenity value: Medium	Comments:		
Retention Value: Medium	Reason:		
Impact of Development: Remove			
Tree-12			
Quercus palustris, Pin Oak			
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 23.5	Height: 8m	Width: 7m	TPZ: 2.8m
Crown class: Symmetrical	Health: Dormant	Structure: Fair-Good	SULE: 20years
Amenity value: Medium	Comments:		
Retention Value: Medium	Reason:		
Impact of Development: Remove			
Tree-13			
Quercus palustris, Pin Oak			
Origin: Exotic	Type: Deciduous Broadleaf	Age:	
DBH (cm): 31	Height: 8m	Width: 7m	TPZ: 3.7m
Crown class: Symmetrical	Health: Dormant	Structure: Fair-Good	SULE: years
Amenity value: Medium	Comments:		
Retention Value: Medium	Reason:		
Impact of Development: Remove			
Tree-14			
Betula pendula, Silver Birch			
Origin: Exotic	Type: Deciduous Broadleaf	Age: Senescent	
DBH (cm): 0	Height: 5m	Width: 2m	TPZ: 2.0m
Crown class: Symmetrical	Health: Poor	Structure: Poor	SULE: 0years
Amenity value: Very Low	Comments: Cracks in trunk, possibly dead.		
Retention Value: Low	Reason:		
Impact of Development: Remove			

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Tree-15			
<i>Betula pendula</i>, Silver Birch			
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 13.5	Height: 5m	Width: 2m	TPZ: 2.0m
Crown class: Symmetrical	Health: Dormant	Structure: Fair	SULE: 10-20years
Amenity value: Low	Comments:		
Retention Value: Low	Reason:		
Impact of Development: Remove			
Tree-16			
<i>Betula pendula</i>, Silver Birch			
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 19.5	Height: 7m	Width: 5m	TPZ: 2.3m
Crown class: Symmetrical	Health: Dormant	Structure: Fair	SULE: 10-20years
Amenity value: Medium	Comments:		
Retention Value: Low	Reason:		
Impact of Development: Remove			
Tree-17			
<i>Betula pendula</i>, Silver Birch			
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 15	Height: 10m	Width: 4m	TPZ: 2.0m
Crown class: Symmetrical	Health: Dormant	Structure: Fair-Good	SULE: 10-20years
Amenity value: Medium	Comments:		
Retention Value: Low	Reason:		
Impact of Development: Remove			
Tree-18			
<i>Betula pendula</i>, Silver Birch			
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 20	Height: 10m	Width: 6m	TPZ: 2.4m
Crown class: Asymmetrical	Health: Dormant	Structure: Fair-Good	SULE: 10-20years
Amenity value: Medium	Comments:		
Retention Value: Low	Reason:		
Impact of Development: Remove			
Tree-19			
<i>Robinia pseudoacacia</i> 'Frisia', Golden Robinia			
Origin: Exotic weed	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 20	Height: 12m	Width: 6m	TPZ: 2.4m
Crown class: Symmetrical	Health: Dormant	Structure: Fair	SULE: 20years
Amenity value: Medium	Comments: Street tree		
Retention Value: Outside property	Reason:		
Impact of Development: Retain			

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Tree-20			
<i>Robinia pseudoacacia</i> 'Frisia', Golden Robinia			
Origin: Exotic weed	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 17.5	Height: 12m	Width: 6m	TPZ: 2.1m
Crown class: Symmetrical	Health: Dormant	Structure: Fair	SULE: 20years
Amenity value: Medium	Comments: Street tree		
Retention Value: Outside property	Reason:		
Impact of Development: Retain			
Tree-21			
<i>Robinia pseudoacacia</i> 'Frisia', Golden Robinia			
Origin: Exotic weed	Type: Deciduous Broadleaf	Age: Juvenile	
DBH (cm): <15	Height: 2m	Width: 1m	TPZ: 2.0m
Crown class: Symmetrical	Health: Fair-Good	Structure: Fair-Good	SULE: 20years
Amenity value: Low	Comments: Newly planted Street tree		
Retention Value: Outside property	Reason:		
Impact of Development: Retain			
Tree-22			
<i>Robinia pseudoacacia</i> 'Frisia', Golden Robinia			
Origin: Exotic weed	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): <15	Height: 2m	Width: 1m	TPZ: 2.0m
Crown class: Symmetrical	Health: Dormant	Structure: Fair-Good	SULE: 20years
Amenity value: Low	Comments: Newly planted Street tree		
Retention Value: Outside property	Reason:		
Impact of Development: Retain			
Tree-23			
<i>Robinia pseudoacacia</i> 'Frisia', Golden Robinia			
Origin: Exotic weed	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 13	Height: 12m	Width: 7m	TPZ: 2.0m
Crown class: Symmetrical	Health: Dormant	Structure: Fair	SULE: 20years
Amenity value: Low	Comments: Street tree		
Retention Value: Outside property	Reason:		
Impact of Development: Retain			
Tree-24			
<i>Robinia pseudoacacia</i> 'Frisia', Golden Robinia			
Origin: Exotic weed	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 23	Height: 12m	Width: 7m	TPZ: 2.8m
Crown class: Symmetrical	Health: Dormant	Structure: Fair-Good	SULE: 20years
Amenity value: Medium	Comments: Street tree		
Retention Value: Outside property	Reason:		
Impact of Development: Retain			

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Tree-25			
<i>Robinia pseudoacacia</i> 'Frisia', Golden Robinia			
Origin: Exotic weed	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 30	Height: 12m	Width: 7m	TPZ: 3.6m
Crown class: Symmetrical	Health: Dormant	Structure: Fair	SULE: 0-10years
Amenity value: Medium	Comments: Basal trunk canker and frass. Street tree		
Retention Value: Outside property	Reason:		
Impact of Development: Retain			
Tree-26			
<i>Robinia pseudoacacia</i> , Locust			
Origin: Exotic weed	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 19.5	Height: 6m	Width: 5m	TPZ: 2.3m
Crown class: Symmetrical	Health: Fair	Structure: Fair	SULE: 10-20years
Amenity value: Medium	Comments: Fungal bodies on trunk. Street tree		
Retention Value: Outside property	Reason:		
Impact of Development: Retain			
Tree-27			
<i>Quercus palustris</i> , Pin Oak			
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 32	Height: 10m	Width: 9m	TPZ: 3.8m
Crown class: Symmetrical	Health: Dormant	Structure: Fair	SULE: 20years
Amenity value: Medium	Comments: Canyon pruned.		
Retention Value: Outside property	Reason:		
Impact of Development: Remove			
Tree-28			
<i>Quercus palustris</i> , Pin Oak			
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 34	Height: 13m	Width: 10m	TPZ: 4.1m
Crown class: Intermediate	Health: Dormant	Structure: Fair	SULE: 20years
Amenity value: Medium	Comments: Canyon pruned and asymmetric		
Retention Value: Outside property	Reason:		
Impact of Development: Remove			
Tree-29			
<i>Ulmus procera</i> , English Elm			
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 34	Height: 13m	Width: 10m	TPZ: 4.1m
Crown class: Symmetrical	Health: Dormant	Structure: Fair-Good	SULE: 20years
Amenity value: High	Comments: Basal wounding.		
Retention Value: Outside property	Reason:		
Impact of Development: Retain			

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Tree-30			
<i>Ulmus procera</i> , English Elm			
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 29	Height: 12m	Width: 6m	TPZ: 3.5m
Crown class: Symmetrical	Health: Dominant	Structure: Fair	SULE: 10-20years
Amenity value: High	Comments: Large tear-out wound east side.		
Retention Value: Outside property		Reason:	
Impact of Development: Retain			
<hr/>			
Tree-31			
<i>Ulmus procera</i> , English Elm			
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 36.5	Height: 13m	Width: 7m	TPZ: 4.4m
Crown class: Symmetrical	Health: Dominant	Structure: Fair	SULE: 20years
Amenity value: Medium	Comments: Lost co-dominant. Trunk wounding.		
Retention Value: Outside property		Reason:	
Impact of Development: Retain			
<hr/>			
Tree-32			
<i>Ulmus procera</i> , English Elm			
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 33	Height: 11m	Width: 7m	TPZ: 4.0m
Crown class: Symmetrical	Health: Dominant	Structure: Fair	SULE: 20years
Amenity value: High	Comments: Some trunk wounds		
Retention Value: Outside property		Reason:	
Impact of Development: Retain			

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6 DESCRIPTORS

Tree Number: Refers to location of tree as per the plan at Appendix 1.

Botanical Name: Botanical name of species, based on nomenclature and spelling used by Spencer in *Horticultural Flora of South Eastern Australia* (vols 1-5). Where *Eucalyptus* spp. are not found in this source, nomenclature is based on *Euclid: Eucalypts of Australia* (2006). Eucalypt subspecies information is also based on this source.

While accurate tree identification is attempted, and uncertainties are indicated, some inaccuracies in tree identification may still be present – especially in certain, difficult to determine, genera (e.g. *Cotoneaster* and *Ulmus*) and with cultivars which can have similar characteristics.

Where a doubt as to exact species is indicated, the common name and origin are based on the listed species, and would change if the species were found to be incorrect.

From time to time taxonomists revise plant classification, and name changes are assigned. If it is known names have been revised post the publication of the relevant above listed source, the new nomenclature has been used.

Common Name: Common names are based primarily on names and spelling used by Spencer in *Horticultural Flora of South Eastern Australia* (vols 1-5). The source of common names is taken in the following order:

1. Single name supplied in *Horticultural Flora of South Eastern Australia*;
2. First in list of names supplied in *Horticultural Flora of South Eastern Australia*, unless another name in the list is deemed more appropriate;
3. As per name supplied in *Trees of Victoria and Adjoining Areas*;
4. Then by best known common name if not available in either source.

Common names are provided for thoroughness; the botanical name should be used when referring to the tree taxon.

Origin: Exotic: Tree origin is from outside the Australian mainland, Tasmania or near islands.

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Type:	Australian Native: Origin is from within the Australian mainland or near islands, but <u>outside</u> Victoria.	
	Victorian Native: Origin is from within Victoria but <u>outside</u> the Melbourne region. This includes trees whose native range extends beyond Victoria into other states.	
	Melbourne: Origin is from within Melbourne, as defined by plants listed in the <i>Flora of Melbourne</i> . This includes trees also found outside Melbourne, and those only within the area at the far extent of their range.	
	Locally Indigenous: Tree's range includes the local area.	
	Weed: Trees known to show tendencies to weediness within Victoria. Based on the City of Knox weed list, Department of Primary Industries (Victoria) weed list and past experience. Trees with the addition of "(nox)" indicate a declared noxious weed; refer to the Department of Primary Industries website for further information.	
Age:	Broadleaf: Tree is a dicotyledon flowering plant.	
	Conifer: Tree is a cone bearing non-flowering plant.	
	Palm: Tree is a monocotyledon Palm (that is <i>Arecaceae</i>).	
	Palm Like: Tree is a monocotyledon, but is not a palm (that is not <i>Arecaceae</i>).	
	Deciduous: Tree seasonally loses its leaves in Victoria.	
Health:	Evergreen: Tree maintains its leaves throughout the year.	
	Semi-deciduous: Tree may or may not lose its leaves, or may only partially lose them.	
	Juvenile: Tree is actively growing and is still in its establishment phase. Tree currently makes little contribution to the amenity of the landscape. Trees of this age are possible candidates for relocation during development.	
	Semi-mature: Tree is still actively growing but has reached an age and size where it is starting to make a contribution to the landscape.	
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Height:	The size of the tree would still be expected to increase considerably given no significant changes to the current situation.	
	Mature: Tree growth has slowed, and the size of the tree would not be expected to increase considerably without significant changes to the current situation (e.g. vegetation removal). Tree is not exhibiting any major signs of health or structural weakness as a result of age.	
	Over mature: Tree is no longer actively putting out extension growth, and is starting to show decline in health or structural stability as a result of age.	
	Senescent: Tree is senescing. Trees in this category may not be especially large or old, but are reaching the end of their expected life, often indicated by extreme poor health.	
	Estimate of the tree's height in metres	
DBH:	The tree's trunk Diameter at Breast Height (1.4m above ground) unless specified as having been taken lower. This can be either estimated or measured as specified in the report.	
	Stems of multi-stemmed trees may be listed individually, or a measurement given at a lower point where the tree still has one stem. In some cases, especially where trees are not considered worthy of retention or stems are too numerous the DBH may simply be listed as "multi-stemmed".	
Health:	The tree's health is rated as Good , Fair and Poor as listed below. Tree ratings of Fair-Good and Fair-Poor indicate that the tree falls between the two categories. Dead trees are not given a rating, but are listed as Dead .	
	Ratings generally meet the following descriptions:	
	Good: Tree is showing no obvious signs of poor health or stress with a dense canopy that is free of dieback. Rot or pathogens are not obvious or are not considered to be a threat to the tree. Growth rates are acceptable.	
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<p>Australian Catholic University, 115B Victoria Parade Fitzroy ARBORICULTURAL REPORT ARBORICULTURAL IMPACT ASSESSMENT</p>	<p>October 2016</p>
<p>Fair: Tree is showing signs of reduced health or stress. This is apparent through moderate foliage density, minor dieback, moderate stress response growth, minor to moderate rot, moderate pathogen infestation, stunted growth or a combination of the above symptoms.</p> <p>Poor: Tree is showing signs of poor health and/or severe stress. This is apparent through either low foliage density, moderate to large-scale dieback, severe stress response growth, severe rot, severe pathogen infestation, failure of wounds to heal, overall tree decline or a combination of the above symptoms.</p> <p>Note on Deciduous Species: Assessment of deciduous species can be problematic and results may vary depending on the time of year of assessment. Descriptor comments in relation to foliage density do not apply to deciduous trees assessed when dormant or entering or exiting dormancy. Time of leaf drop or bud burst and extent of bud swell may be considered in the health rating of these trees.</p> <p>The ratings indicate that certain characteristics listed have, or have not been observed. Inspections do not assess the whole tree in detail for each characteristic. The comments category should be referred to for further information.</p> <p>Structure: The tree's structure is rated as Good, Fair and Poor. Tree ratings of Fair-Good and Fair-Poor indicate that the tree falls between the two categories.</p> <p>As a general rule, the structure rating is based on the tree's likelihood of failure. However, it must be noted that this is not a full hazard or failure assessment of the tree.</p> <p>Good: Tree has no obvious structural defects and is therefore not considered likely to fail.</p> <p>Fair: Tree has at least one obvious structural defect, but this is considered to be manageable and of only moderate failure risk or the piece likely to fail may be small. Structural defects that may contribute to a fair rating are as follows:</p>	<p>PAGE 19</p>

<p>Australian Catholic University, 115B Victoria Parade Fitzroy ARBORICULTURAL REPORT ARBORICULTURAL IMPACT ASSESSMENT</p>	<p>October 2016</p>
<ul style="list-style-type: none"> • Poor branch attachment (including deadwood and large epicormics); • Bifurcated, but with a join that is considered to be solid; • Moderate trunk lean but without other defects; • Minor damage to the trunk base; • Rot or other damage starting to compromise the structure; • History of shedding minor branches. <p>Poor: Tree has at least one structural defect that is severe and considered to have a relatively high risk of failure. If targets are present then defect(s) require treatment, or alternatively the tree should be removed. In some cases removal may be the only option for these trees. Structural defects that may contribute to a poor rating are as follows:</p> <ul style="list-style-type: none"> • Poor branch attachment (including deadwood and large epicormics); • Bifurcated with swelling and/or included bark; • Severe trunk lean associated with other defects such as injury in the plane of lean or root plate lift; • Major damage to the trunk base or root system; • Rot or other damage severely compromising the structure; • History of shedding large branches. <p>The ratings indicate that certain characteristics listed have, or have not been observed. Inspections do not assess the whole tree in intense detail for each characteristic. The comments category should be referred to for further information.</p> <p>Crown class: Symmetrical: For the most part canopy received light from all four sides and has to potential for even foliage distribution. Canopy may or may not be symmetrical, but is not suppressed.</p> <p>Asymmetrical: Canopy is shaded or suppressed with one or more sides and dominant when compared to the remainder of the tree. Also includes crowns damaged by previous shading.</p>	<p>PAGE 20</p>

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<p>Australian Catholic University, 1155 Victoria Parade Fitzroy ARBORICULTURAL REPORT ARBORICULTURAL IMPACT ASSESSMENT</p> <p>October 2016</p> <p>Intermediate: Canopy is only receiving light from top, and while shape may be even the upper portions of the canopy dominate over the lower.</p> <p>Suppressed: Canopy is completely shaded by surrounding vegetation, buildings etc.</p> <p>Regrowth: Canopy comprised of regrowth. This can be from the base, but also includes branches covered with small, stress related epicormics.</p> <p>Trained: Canopy has been specifically trained. This may include trees that are pollarded, coppiced or espaliered.</p> <p>Trees may exhibit a combination of the characteristics above (e.g. a symmetrical canopy of basal regrowth), or may fall between two categories. The characteristic listed is considered to be the best fit at the time.</p> <p>Amenity value:</p> <p>Very Low: Tree makes little or no contribution to the amenity value of the site or surrounding area. In some cases the tree may be detrimental to the area's amenity value (e.g. unsightly, risk of weed spread).</p> <p>Low: Tree makes some contribution to the amenity value of the site, but makes no contribution to the amenity value of the surrounding area. Removal of the tree would result in little loss of amenity. Juvenile trees (including street trees) are generally included in this category, however they may have the potential to supply increased amenity in the future.</p> <p>Medium: Tree makes a moderate contribution to the amenity of the site and/or may contribute to the amenity of the surrounding area.</p> <p>High: Tree makes a significant contribution to the amenity value of the site, or tree makes a moderate to significant contribution to the amenity value of the larger landscape.</p> <p>The amenity value rating considers the impact the tree has on any neighbouring sites as being of equal importance to that supplied to the</p> <p>JOHN PATRICK LANDSCAPE ARCHITECTS PTY LTD OFFICE REF: 16-794TS-01.docx</p> <p>PAGE 21</p>	<p>Australian Catholic University, 1155 Victoria Parade Fitzroy ARBORICULTURAL REPORT ARBORICULTURAL IMPACT ASSESSMENT</p> <p>October 2016</p> <p>subject site. However, trees that contribute to the amenity of the general area (e.g. streetscape) are given greater weight.</p> <p>Comments: Any additional comments in relation to the above categories.</p> <p>SULE:</p> <p>The Safe, Useful, Life Expectancy of the tree from a health, structure, amenity and weediness viewpoint given no significant changes to the current situation. This category is difficult to determine, and should be taken as an estimate only, in addition to this, factors not observed at the time of inspection can lead to tree decline.</p> <p>0: Tree is a hazard or a weed and should be removed immediately.</p> <p>0-10: Estimated SULE of less than 10 years.</p> <p>10-20: Estimated SULE of 10 to 20 years.</p> <p>20: Estimated SULE of 20 years or greater.</p> <p>Recommendation:</p> <p>Remove: Tree is either not worthy of retention or requires removal (e.g. weed species).</p> <p>Retain or Remove: Tree does not require removal, but is of low retention value.</p> <p>Retain if practical: Tree has a moderate retention value and should be retained if possible during any development of the site.</p> <p>Notes:</p> <p>Dead: Tree is dead and should therefore be removed.</p> <p>Good condition: Tree is worthy of retention based on its condition. Trees may still have some structural or health problems, but are generally worth retaining.</p> <p>Good development potential: Tree is of a small size, but is considered to have a high potential to develop well. Retention of these trees should be considered as they should develop more quickly than new plantings.</p> <p>Hazardous: Tree should be removed as it is hazardous.</p> <p>JOHN PATRICK LANDSCAPE ARCHITECTS PTY LTD OFFICE REF: 16-794TS-01.docx</p> <p>PAGE 22</p>
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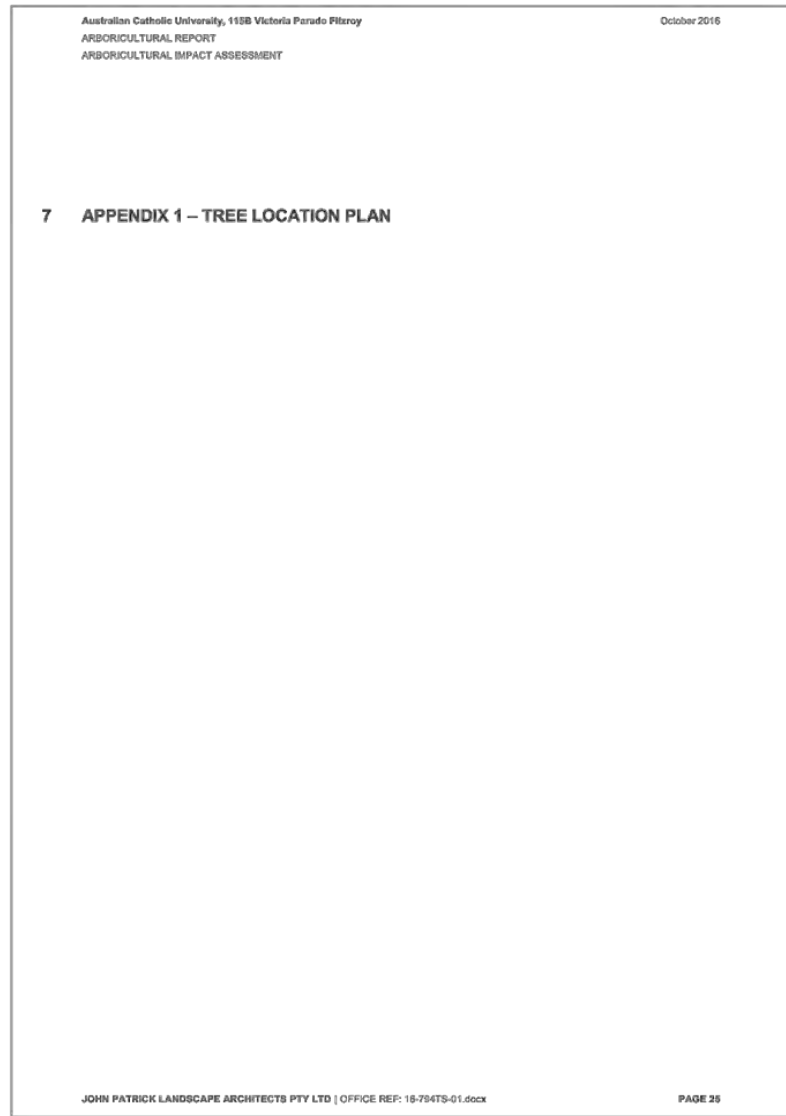
Attachment 6 - ACU Development Plan - Part 5



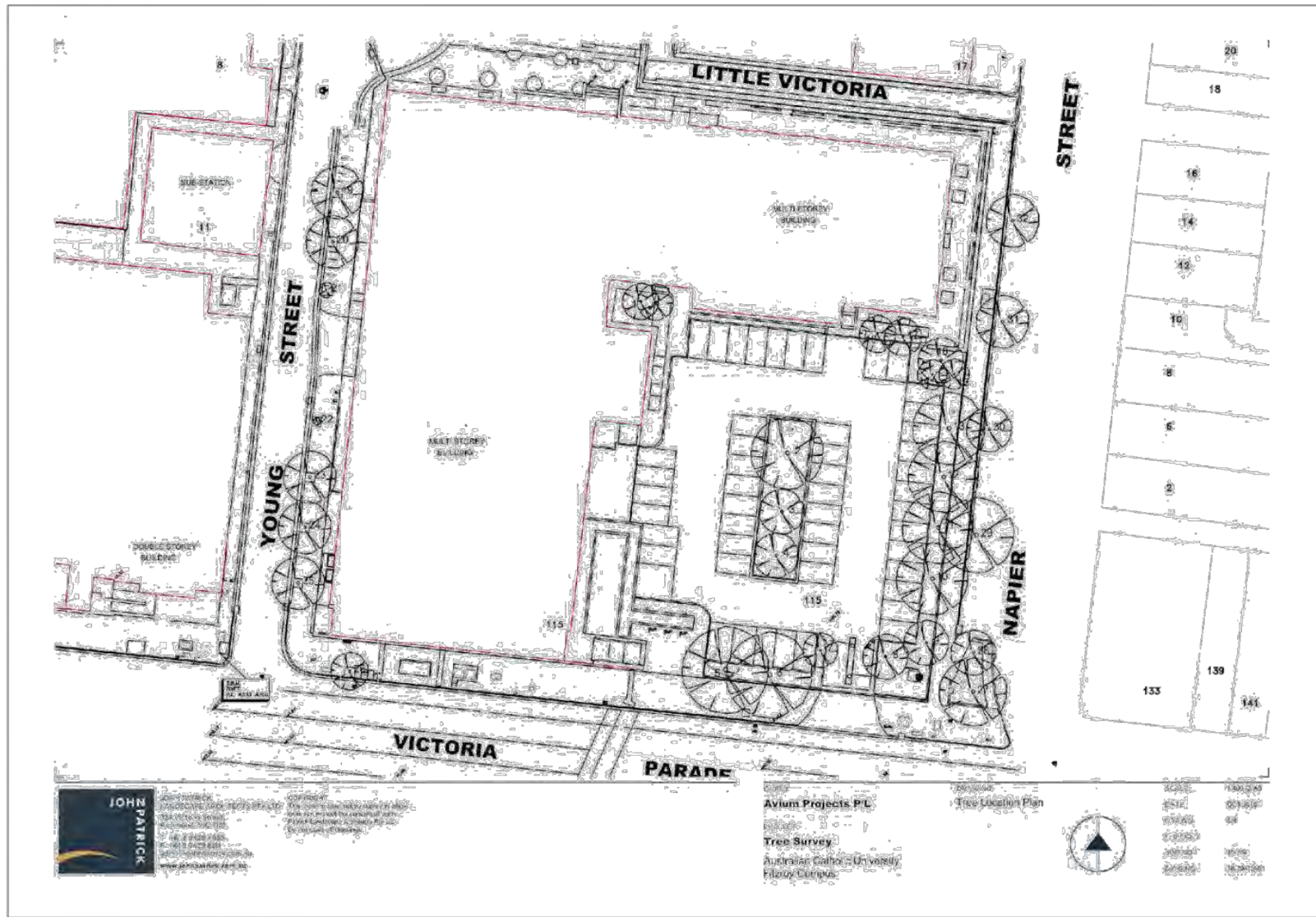
<p>Australian Catholic University, 115B Victoria Parade Fitzroy ARBORICULTURAL REPORT ARBORICULTURAL IMPACT ASSESSMENT</p> <p>October 2016</p> <p>Heritage tree: Tree is of heritage significance. Refer to the introduction for further information on any trees of heritage significance.</p> <p>High landscape contribution: Tree is worthy of retention based on its contribution to the site or landscape (associated with amenity value).</p> <p>Inappropriate location: The tree is not in an appropriate location for its species, size etc. Includes trees too large for their current location.</p> <p>Juvenile – simple to replace: Tree does not have a high retention value as a similarly sized replacement specimen could be obtained. Alternatively, the tree is a candidate for relocation.</p> <p>Limited life expectancy: Tree is in decline, or is expected to start to decline within a relatively short time period. As a result, it is not sensible to implement extensive tree protection measures to save the tree unless there are extenuating circumstances (e.g. outside ownership).</p> <p>Low Amenity Value: Tree is unsightly, or has little potential to add to site amenity (e.g. a non-canopy fruit tree).</p> <p>Outside ownership: Tree is located outside the subject site, and is therefore owned by another party. The tree may be in a neighbouring private property or fall within the council managed nature strip/road reserve.</p> <p>It is assumed that the owner of the tree wishes to retain it, and the trees are listed as retain for that reason. The owner should be contacted for discussions if the removal of the tree is wanted. Recommendation of retention of any of these trees is based solely on the above mentioned reason, and is no indication of the tree's general worthiness for retention.</p> <p>Poor condition: Tree's poor condition makes it unworthy of retention.</p> <p>Rare / unusual species: Tree is of a species, cultivar or form (trained or otherwise) which is unusual, at least in the local area, and which has some retention value (usually amenity value). Trees of this nature may also classify as a "heritage tree".</p> <p>JOHN PATRICK LANDSCAPE ARCHITECTS PTY LTD OFFICE REF: 16-794TS-01.docx</p> <p>PAGE 23</p>	
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<p>Australian Catholic University, 115B Victoria Parade Fitzroy ARBORICULTURAL REPORT ARBORICULTURAL IMPACT ASSESSMENT</p> <p>October 2016</p> <p>Remnant Indigenous: The tree is a remnant indigenous specimen and therefore has environmental value. Trees of this nature, in reasonable condition are usually recommended for retention.</p> <p>Senescent: Tree should be removed as it is dying.</p> <p>Significant tree: The tree has been declared a significant tree by the local council, and retention is likely to be a permit requirement.</p> <p>Unlikely to develop well: Tree is immature with a severe defect which will prevent its form developing as it should or tree has a severe defect, the correction of which will result in a tree shape that is unlikely to redevelop well.</p> <p>Weed species: Tree should be removed due to weedy nature of the species.</p> <p>TPZ: The Tree Protection Zone of the tree, measured as a radial distance in metres from the centre of the trunk. The TPZ is calculated using the method specified in <i>Australian Standard AS4970-2009 Protection of trees on development sites</i>.</p> <p>TPZs are not listed for trees that are recommended for removal.</p> <p>JOHN PATRICK LANDSCAPE ARCHITECTS PTY LTD OFFICE REF: 16-794TS-01.docx</p> <p>PAGE 24</p>	
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Sydney

Tower 2, Level 23, Darling Park
201 Sussex Street
Sydney, NSW 2000
Tel: +612 8233 9900
Fax: +612 8233 9966

Brisbane

Level 7, 123 Albert Street
Brisbane, QLD 4000
Tel: +617 3007 3800
Fax: +617 3007 3811

Melbourne

Level 12, 120 Collins Street
Melbourne, VIC 3000
Tel: +613 8663 4888
Fax: +613 8663 4999

Perth

Level 1, 55 St Georges Terrace
Perth, WA 6000
Tel: +618 9346 0500
Fax: +618 9221 1779

Australia - Asia - Middle East
www.urbis.com.au
info@urbis.com.au

Attachment 7 - ACU Development Plan - Heritage



25 October 2016

Re: *Australian Catholic University*
Development Plan PLN16-0085 as amended, October 2016

The following discussion has been prepared at the request of Urbis on behalf of the Australian Catholic University. It comments upon heritage considerations in relation to the amended form of the proposed Development Plan (DP) for the St Patrick's Campus of the Australian Catholic University (ACU) in Fitzroy (drawings dated October 2016), which has been prepared in consultation with Council and the community. It sets out relevant heritage considerations that have informed the design and location of new works anticipated through the amended DP, drawing largely upon an earlier letter that addressed the DP documentation submitted to Council in December 2015. The DP and proposed Development Plan Overlay (DPO) are intended to guide future planning permit applications for development at this campus, within the DPO area. The campus is located in the block bounded by Brunswick Street, Napier Street, Victoria Parade and Gertrude Street.

In the amended DP the proposed envelope has been altered to decrease the associated impacts upon views within the South Fitzroy Heritage Overlay precinct, and more specifically to decrease the visual impact associated with its height and bulk as seen from the north within Napier Street and the immediate environs of the site. The changes include a reduction in height by two storeys and increases to setbacks to the upper levels of the proposal relative to the Napier Street frontage. These changes are discussed in more detail on page 5 below.

The focus of future change countenanced in the DP is to be located at the Victoria Parade frontage of the site, away from residential properties further north. New buildings are anticipated at 115B Victoria Parade (corner of Napier Street) and 81-89 Victoria Parade (corner of Brunswick Street) – not part of the DP land. The Mary Glowrey building, the former Commonwealth Note and Stamp Printing Building, will be refurbished and extended. The site of the current car park site at 28-42 Young Street will also be redeveloped, and the amended DP includes some indication of what is proposed for that site.

This commentary draws upon inspections of the site and its surrounds and a review of relevant heritage studies and planning scheme documents including the City of Yarra's heritage policy at clause 22.02. In addition, regard has been had for the buildings and streetscapes adjacent this site, which is within the South Fitzroy Heritage Overlay precinct, HO334, and for the building gradings found in Council's heritage database. A copy of heritage advice provided to Council by its heritage advisor has also been reviewed and has informed the amendments to the DP.

The heritage overlay precinct that includes the subject land and the land to the north, east and west, the South Fitzroy precinct, is a key consideration in the development of the ACU land, and it is important that new built form acknowledge the sensitivity of the South Fitzroy precinct and nearby heritage buildings. Development of the ACU land is subject to the heritage overlay and will be assessed against the relevant heritage policies.

Attachment 7 - ACU Development Plan - Heritage

The statement of significance for HO334 in the *City of Yarra Heritage Review* (2007) states that:

The South Fitzroy Heritage Overlay Area is significant:

- *As the earliest urban area outside the Melbourne City grid to be settled in the Melbourne municipality, with several buildings from the mid nineteenth century surviving as testimony to its early establishment;*
- *For the unusually high number of early Victorian-era and some Regency period buildings, being generally simply detailed and a clear reflection of the early date of Fitzroy's settlement.*
- *As evidence of early government planning controls or Acts of Parliament, from the 1850s, that aimed to solve street alignment problems in this privately planned suburb, arising from a hitherto lack of co-ordination between neighbouring allotment owners;*
- *As a good example of the successful application of the Act for Regulating Buildings and Party Walls, and for Preventing Mischief by Fire in the City of Melbourne (Melbourne Building Act 1849), which forced the use of fireproof construction and gave South Fitzroy a character distinct from other inner suburbs such as Richmond and Collingwood, that have a greater proportion of Victorian-era timber buildings.*
- *As a substantially intact collection of predominantly mid to late nineteenth and early twentieth century building stock, interspersed with well preserved inter-war residential, commercial, retail and industrial buildings that contribute to the historical character of the area.*
- *For the relatively large number of individually significant buildings, being predominantly solid masonry rather than clad with timber, largely as a result of the Melbourne Building Act, 1849.*
- *For the ornate and exuberant detail of many late nineteenth and early twentieth century buildings in the suburb, reflecting the affluence of many of the inhabitants of this area, particularly in the late 19th century.*
- *For the early street, lane and allotment layouts, some original bluestone kerbs, paving and guttering, and some mature exotic street trees, providing an appropriate setting for this collection of residential, retail, commercial and industrial buildings.*
- *For the landmark qualities of some large factory and warehouse buildings from the late 19th and earlier 20th century, such as the MacRobertson confectionary complex which are significant features in the skyline of this predominantly low rise suburb; and*
- *For the major early institutions that developed on its fringes, in particular, St Vincent's Hospital and The Convent of Mercy, as closely linked with the area's history, education and welfare within the metropolitan area.*

The statement of significance for the former Commonwealth Note and Stamp Printing Building at 115 Victoria Parade (now known as the Mary Glowrey building) is as follows:

What is significant?

The Commonwealth Note and Stamp Printing department ... was created in 1929-1931 for the Australian Commonwealth Government and has close historical associations with that important function.

The place has a fair integrity to its creation date (now altered cosmetically, as conversion to university; car park at rear and side).

Fabric from the creation date at the Commonwealth Note and Stamp Printing department, former is locally significant within the City of Yarra, compared to other similar places from a similar era.

How is it significant?

The Commonwealth Note and Stamp Printing department, former at 115 Victoria Parade, Fitzroy is historically significant to the locality of Fitzroy and the City of Yarra.

Why is it significant?

The Commonwealth Note and Stamp Printing department is significant as the former origin of all legal tender in Australia, receiving extensive publicity on its completion and with suitably monumental Greek Revival styling.

It is to be noted that alterations to the former Commonwealth Note and Stamp Printing Building have been somewhat more than cosmetic – this statement of significance seems to predate a number of works that have been completed within the site, the additions to the southern facade and to the east elevation of the main, southern wing in particular. In addition, most of the windows have been replaced with simple modern glazing in place of the original steel framed windows. The northern wing has been similarly altered.

Attachment 7 - ACU Development Plan - Heritage

Yarra's heritage policy at Clause 22.02-5.7.1 provides detailed guidelines for new development and alterations and additions heritage overlay areas, with key policy including the following:

Encourage the design of new development and alterations and additions to a heritage place or a contributory element to a heritage place to:

- *Respect the pattern, rhythm, orientation to the street, spatial characteristics, fenestration, roof form, materials and heritage character of the surrounding historic streetscape.*
- *Be articulated and massed to correspond with the prevailing building form of the heritage place or contributory elements to the heritage place.*
- *Be visually recessive and not dominate the heritage place.*
- *Be distinguishable from the original historic fabric.*
- *Not remove, cover, damage or change original historic fabric.*
- *Not obscure views of principle façades.*
- *Consider the architectural integrity and context of the heritage place or contributory element.*

Encourage setbacks from the principal street frontage to be similar to those of adjoining contributory buildings; where there are differing adjoining setbacks, the greater setback will apply.

Encourage similar façade heights to the adjoining contributory elements in the street. Where there are differing façade heights, the design should adopt the lesser height.

Clause 22.02-5.7.2 encourages the following specific requirements with regard to corner sites and sites with dual frontages:

Encourage new building and additions on a site with frontages to two streets, being either a corner site or a site with dual street frontages, to respect the built form and character of the heritage place and adjoining or adjacent contributory elements to the heritage place.

Encourage new buildings on corner sites to reflect the setbacks of buildings that occupy other corners of the intersection.

This Clause also includes specific requirements for upper level additions to Industrial, Commercial and Retail Heritage Places or Contributory Elements:

Encourage new upper level additions and works to:

- *Respect the scale and form of the existing heritage place or contributory elements to the heritage place by being set back from the lower built form elements. Each higher element should be set further back from lower heritage built forms.*
- *Incorporate treatments which make them less apparent.*

The extent of development proposed under the DP is substantial and will lead to appreciable change in the built form of the campus and associated change to views and character within adjoining streets. This said, the DP seeks to realise this substantial change in such a manner as to achieve an acceptable level of compliance with the heritage policy.

The VCAT approval for a fourteen storey development at the corner of Brunswick Street and Victoria Parade, ie 81-89 Victoria Parade (Premier Projects Pty Ltd v Yarra CC [2007]; VCAT reference no. P2540/2006), establishes a relevant benchmark for redevelopment along this part of Victoria Parade and within the ACU campus. While there are buildings along Victoria Parade within the context of the site that are of individual heritage significance, the streetscape to this part of Victoria Parade is of mixed character, is not of particular significance, and lends itself to higher development. The scale of current and proposed development to the west of ACU along Victoria Parade, at the St Vincent Hospital sites, and the scale of buildings to the south side of the Victoria Parade, also suggest that built form of substantial scale may be considered in the Victoria Parade context.

The amended DP contemplates a new building of 12 storeys on the carpark site at the corner of Victoria Parade and Napier Street, known as 115B Victoria Parade. This is to be attached to the east side of the former Commonwealth Note and Stamp Printing Department Building at 115 Victoria Parade, and also to the south side of the northern wing of the Commonwealth Note and Stamp Printing Department Building.

Attachment 7 - ACU Development Plan - Heritage

The new building is to provide a new major point of entry to the campus, and will utilize the lift and stair 'core' attached to the east side of the heritage building. The existing glazed entry and disabled access ramp addition to the front of 115 Victoria Parade will be removed and that part of the facade reinstated.

The residential streetscapes to Napier Street to the east and north of the site at 115B Victoria Parade include heritage buildings of one and two storeys, with two storeys being predominant. It is a fine heritage street, particularly to the north of ACU. On this basis it is appropriate that new built form be very carefully designed to limit impact upon the character, appearance and significance of the streetscapes to Napier Street.

Young Street, located more centrally in relation to the ACU land holdings, is less significant at its southern end, already containing built form of scale, but includes a heritage building at the Victoria Parade corner, west side, and the former Commonwealth Note and Stamp Printing Department Building on the east corner.

The DP concept for ACU at 115B Victoria Parade proposes to:

- Remove the existing glazed entry and disabled access ramp from the front facade of 115 Victoria Parade.
- Consolidate the entry for both 115 and 117 Victoria Parade to create an active and prominent entry.
- Angle the entry and south elevation to 115B Victoria Parade back from the alignment of 115 Victoria Parade in order to reveal the robust corner detailing of 115 Victoria Parade. This corner will remain visible in views from Victoria Parade, with the angled form of the new works providing a 'setback' against the heritage building.
- Use a similar strategy to provide views to the eastern wing of the Mary Glowrey building (former Commonwealth Note and Stamp Printing Department).
- Integrate the floor plans of 115 Victoria Parade and 115B Victoria Parade, the new building being attached to a side of 115 Victoria Parade that has already been the subject of considerable modification and additions.
- The new built form will in part overhang 115 Victoria Parade, albeit with a degree of vertical separation that will enable the two forms to read separately.
- The building adopts a stepped profile to create a height relationship with the built form along Napier Street.
- An addition of three levels to the rear part of the northern wing of the Mary Glowrey building (former Commonwealth Note and Stamp Printing Department), well set back from Napier Street in accordance with heritage policy for upper level additions to industrial buildings.

The existing car park building at 28-42 Young Street will be demolished and replaced with a low rise building that incorporates an open plaza or community space at ground level. The development of this part of the campus is not seen to raise heritage issues insofar as it is in a highly altered environment with negligible heritage fabric to its streetscape.

Having regard for the DP, it is apparent that the anticipated works will result in substantial change and to some extent impacts upon heritage values and views, relative to both the former Commonwealth Note and Stamp Printing Department Building and the streetscapes extending to the north along Napier Street and to the east along Victoria Parade. This said, the DP seeks to limit these impacts through focusing change within the already altered environment of Victoria Parade, at the very southern edge of the South Fitzroy precinct.

As noted, a number of changes introduced in the amended design seek to limit the appearance of scale and bulk in terms of views from the north, particularly in Napier Street. The view analysis provided for the amended DP compares the massing of the form as originally contemplated in 2015, the massing of the form as revised in September 2016, and a indication of the architectural treatment that may accompany the revised massing.

Attachment 7 - ACU Development Plan - Heritage

The key amendments to the DP scheme in relation to heritage considerations are as follows.

Reduction in height

The DP scheme submitted to Council in December 2015 was of 14 storeys plus plant. The present scheme is reduced to 12 storeys plus plant. The reduction in height result in a lessening of the visibility and visual impact of the proposal in terms of the view analysis provided in the DP documentation. This is apparent in each of the views assessed within the documentation, including views along Napier Street from the north. The documentation shows that in these views from the north the building is not only apparently lower in terms of the number of storeys per se, but that the plant level is also less visible, further diminishing the apparent bulk of the envelope in these views.

Podium design along Napier Street

The podium to Napier Street is effectively of 6 storeys, albeit with the Ground and Lower Ground floors being relatively open and setback. The treatment of the podium to Napier Street is not substantially changed, although the setback has increased at ground floor relative to the south face of the northern wing of the former Commonwealth Note and Stamp Printing Department Building, and marginally decreased at the floors above. However, it is acknowledged that the podium should respond to some degree to the streetscape condition of buildings that are aligned with the property boundaries.

Setbacks of higher levels from Napier Street

The upper levels of the proposal have generally been increased relative to Napier Street, particularly in terms of levels 6 & 7 (and above). At levels 6 & 7 the setbacks are essentially twice those of levels 7-9 in the previous concept, with the minimum dimension from Napier Street increasing from 8.28m to 17.1m. The various alignments of the faces of the upper levels have also changed, further reducing the visual bulk in terms of views along Napier Street. The built form remains visible, but is viewed as being appreciably further from the alignment of the street than in the earlier scheme.

Additional structure to support new levels above northern wing of the former Commonwealth Note and Stamp Printing Department Building

The DP concept has consistently included an addition of 3 storeys (plus plant and undercroft space) above the northern wing of the former Commonwealth Note and Stamp Printing Department Building. The setbacks of this built form have been increased relative to Napier Street, and the modelling has altered. These changes help limit visual impact in terms of views from the north.

This said, an element of the scheme that was not previously contemplated is the provision of additional structure to the exterior of the host building to assist in supporting the new levels. The additional structure will comprise new piers or columns that will be attached to the existing vertical structure of the host building, in a manner that is intended to complement rather than disrupt the articulation and character of that building. The easternmost pier carries angled struts that support an overhanging section of the new envelope. This new structure is to be sited well back from Napier Street and will result in minimal change to the character and appearance of the additions as seen from the north within Napier Street.

Details and materials

The DP scheme includes an indication of how the architecture of the proposal might be resolved. This is essentially limited to an indicative arrangement of glazing and walls, with some suggestion of a pale colour or white as the primary finish to greater part of the building. The architects have documented a range of indicative materials illustrating options that may be considered as the design is further refined. This said, the materials, finishes and colours have not been determined at this stage, and will be subject to further amendment and refinement as part of a future permit application process.

Attachment 7 - ACU Development Plan - Heritage

Conclusion

The original DP scheme of December 2015 was developed having regard for the potential for impacts upon the setting of the former Commonwealth Note and Stamp Printing Department Buildings and upon views within Napier Street and Victoria Parade, having regard for the location at Victoria Parade and taking a 'whole of planning' view that accepts the importance of ACU within this precinct and within the City of Yarra. In the present amendments to the form and scale of the built form anticipated in the DP, a concerted attempt has been made to further limit impacts upon buildings, views and the broader heritage overlay precinct, while still seeking to achieve ACU's requirements.

Bryce Raworth and Carolynne Baker

Attachment 8 - ACU Development Plan - Traffic Part 1



Integrated Transport and Access Plan

Australian Catholic
University, St.
Patricks Campus,
Fitzroy

Prepared for Australian
Catholic University
27 October 2016



Attachment 8 - ACU Development Plan - Traffic Part 1



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Contact Information

Cardno Victoria Pty Ltd
Trading as Cardno
 ABN 47 106 610 913

Level 4
 501 Swanston Street
 Melbourne
 Victoria 3000 Australia

Telephone: (03) 8415 7777
 Facsimile: (03) 8415 7788
 International: +61 3 8415 7777

victoria@cardno.com.au
 www.cardno.com

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Attachment 8 - ACU Development Plan - Traffic Part 1

Appendices

Appendix A Concept Functional Layout Plans

Appendix B Bicycle Parking Products

Attachment 8 - ACU Development Plan - Traffic Part 1



Integrated Transport and Access Plan Australian Catholic University, St. Patricks Campus, Fitzroy

1 Introduction

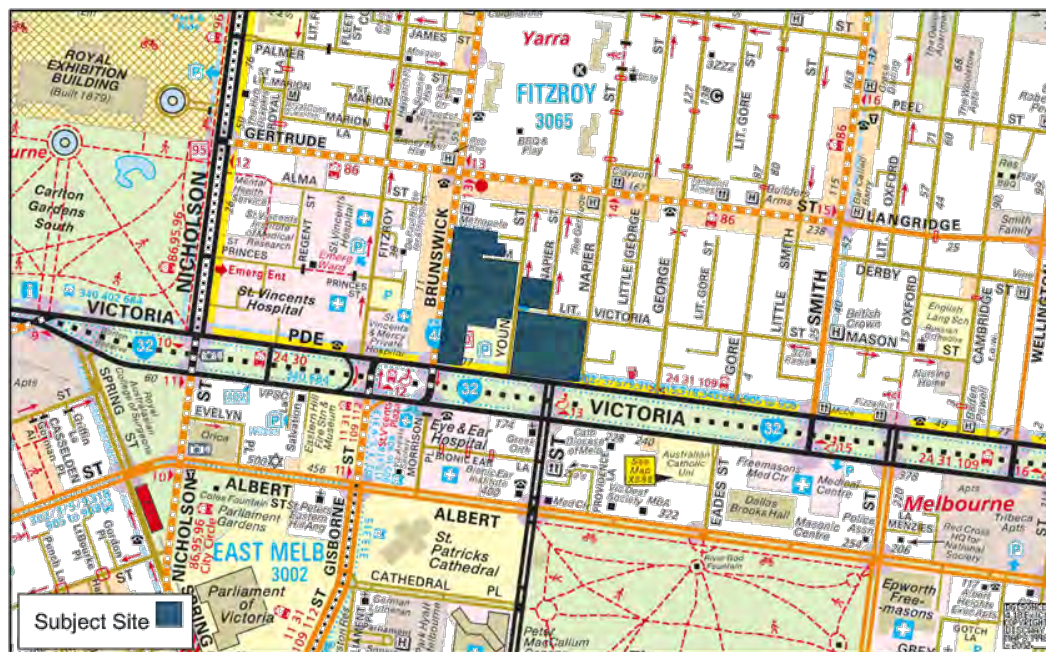
1.1 Background

A Development Plan is being prepared for the Australian Catholic University (ACU) St Patrick's Campus to deliver a mixed use development to accommodate the planned growth in students, teachers and research activities, improve the quality of teaching and learning environments and improve the pedestrian and public environments for students and the wider community.

To accompany the Development Plan, Cardno was engaged by Australian Catholic University to prepare an Integrated Transport and Access Plan (ITAP). The subject site comprises a number of buildings generally located to the north of Victoria Street between Brunswick Street and Napier Street, as shown in Figure 1-1.

The site is located within the suburb of Fitzroy and is situated approximately 500 metres to the east of the Melbourne Central Activities District (CAD).

Figure 1-1 Site Location



Schedule 2 to the Development Plan Overlay (DPO) (or Schedule 2 to Clause 43.04 of the Yarra Planning Scheme) applies to the subject site and provides criteria for any future use and development of the land at 115 Victoria Parade and 20-23 Brunswick Street, Fitzroy.

Schedule 2 to the DPO is dated 19 January 2006 and provides the following requirements for the Development Plan which are considered relevant to transport:

- > The development plan must show:
 - The number, location, dimensions and layout of all car parks and access ways to and from them;
 - A management plan for the operation and maintenance of the car park areas;
 - The location and dimensions of all bicycle, vehicle and pedestrian ways;
 - A traffic management plan which must show any traffic management and traffic control works considered necessary in adjoining and nearby roads when the development is completed; and
 - The means of vehicular and pedestrian ingress to and egress from the land.

Attachment 8 - ACU Development Plan - Traffic Part 1



Integrated Transport and Access Plan Australian Catholic University, St. Patricks Campus, Fitzroy

1.2 Purpose

The purpose of this report is to provide an integrated transport approach to any future use and development at ACU.

This report considers all modes of transport, with a view to updating and incorporating a more integrated transport approach to the transport related requirements in Schedule 2 to the DPO.

Attachment 8 - ACU Development Plan - Traffic Part 1



2 Existing Conditions

2.1 Population and Employment

ACU currently operates with 10,000 effective full time (EFT) students and approximately 800 staff.

2.2 Mode Share and Journey to Work

Relevant journey to work and journey to education data has been sourced from the 2011 ABS Census and the Victorian Integrated Survey of Travel and Activity (VISTA) which is an ongoing survey of travel and activity that helps the government make better transport and land-use planning decisions.

Table 2-1 shows journey to work data based on work destinations for the suburb of Fitzroy from the 2011 ABS Census and for the City of Yarra from the VISTA 09 surveys.

Table 2-1 Journey to Work Data Based on Work Destination

Travel Mode	2011 ABS Census Suburb of Fitzroy	VISTA 09 City of Yarra
Car Based	53%	57%
Public Transport	32%	30%
Bicycle/Walking	15%	13%

Table 2-2 shows journey to education data based on education destinations for RMIT in Melbourne and the Kangan Institute of TAFE in Cremorne. The results are noticeably different, with significantly higher public transport use at RMIT and higher car and bicycle/walking use at Kangan Institute of TAFE.

Table 2-2 Journey to Education Data Based on Education Destination

Travel Mode	VISTA09 RMIT, Melbourne City of Melbourne	VISTA 09 Kangan Institute of TAFE, Cremorne City of Yarra
Car Based	10%	37%
Public Transport	67%	30%
Bicycle/Walking	23%	33%

The site is more similarly located to RMIT with respect to its public transport accessibility and proximity to the CAD, however on-site parking at ACU is more readily available.

In this respect, it is anticipated that car use would be in between the results for RMIT and Kangan Institute of TAFE.

This is confirmed by results from a questionnaire survey that was distributed by ACU staff in May and August 2009.

Approximately 800 surveys were completed and returned, with the results of the survey summarised in Table 2-3, Table 2-4 and Table 2-5 overleaf.

The data collected by the University reveals that a large contingent of the existing student and staff population either catch public transport or walk or cycle to the facility.

Table 2-3 Student Travel Modes

Travel Mode	Number	Percentage
Car (Driver)	122	19%
Car (Passenger)	38	6%
Public Transport	445	68%
Walked	31	5%
Cycled	17	2%
Total	653	100%

Attachment 8 - ACU Development Plan - Traffic Part 1



Table 2-4 Staff Travel Modes

Travel Mode	Number	Percentage
Car (Driver)	62	34%
Car (Passenger)	12	7%
Public Transport	80	44%
Walked	15	8%
Cycled	8	4%
Other	3	2%
Total	180	100%

Table 2-5 University Travel Modes

Travel Mode	Number	Percentage
Car (Driver)	184	22%
Car (Passenger)	50	6%
Public Transport	525	63%
Walked	46	6%
Cycled	25	3%
Other	3	0%
Total	833	100%

2.3 Pedestrian Network

The existing pedestrian network in the vicinity of the site includes footpaths on both sides of Brunswick Street, Young Street and Napier Street providing access towards Gertrude Street and Victoria Street to the north and south respectively, both of which are also provided with footpaths along both sides.

Victoria Street is also provided with footpaths along both sides of the central median.

Pedestrian crossings are provided at all signalised intersections, including:

- > Brunswick Street / Gertrude Street;
- > Brunswick Street / Victoria Street / Gisborne Street; and
- > Victoria Street / Lansdowne Street.

At all other intersections, pedestrian crossings are facilitated by pram ramps.

Tram stops associated with tram routes 11, 30, 86, 109 and 120 are readily accessible from the subject site via the pedestrian network, whilst Parliament Train Station is also readily accessible via footpaths along both sides of Gisborne Street.

The pedestrian network in the vicinity of the site also provides access towards neighbouring suburbs, including Collingwood, Fitzroy North, Carlton, East Melbourne and the Central Activities District (CAD).

Key pedestrian links in the vicinity of the site is shown graphically in Figure 2-1, whilst Figure 2-2 shows the pedestrian links within the campus and their connections to the key pedestrian links.

Attachment 8 - ACU Development Plan - Traffic Part 1



Integrated Transport and Access Plan Australian Catholic University, St. Patricks Campus, Fitzroy

Figure 2-1 Key Pedestrian Links



Figure 2-2 Campus Pedestrian Links



Prepared for Australian Catholic University

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2.4 Bicycle Network

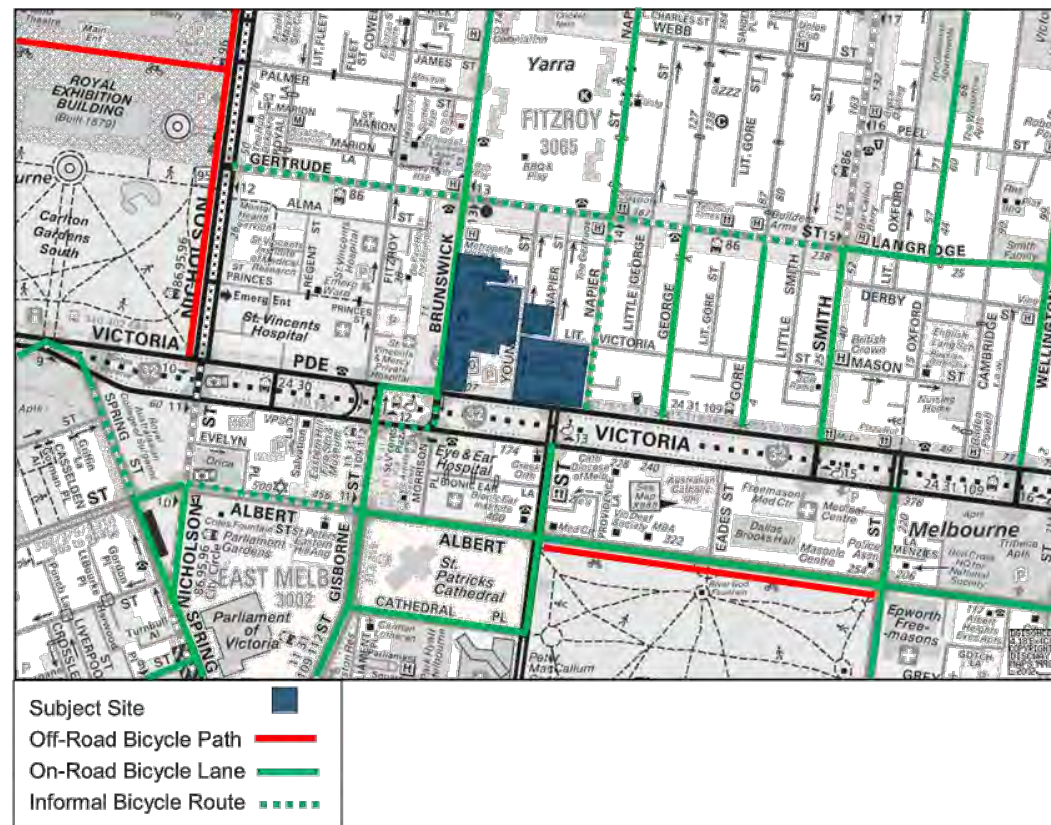
The subject site is well serviced by Melbourne's extensive bicycle network, with numerous off-road paths, on-road bicycle lanes and informal bicycle routes (generally designated by painted bicycle symbols and the provision of bicycle storage boxes at signalised intersections).

As shown in Figure 2-3, on-road bicycle lanes on Brunswick Street provide access towards Fitzroy North to the north and towards the CAD to the south via Gisborne Street.

The informal route along Gertrude Street provides access towards Collingwood to the east and towards Carlton to the west via the off-road path between the Royal Exhibition Building and the Melbourne Museum.

Additional on-road bicycle lanes along key streets, including Napier Street, Albert Street, Clarendon Street and Wellington Street provide further connections to surrounding suburbs, including East Melbourne and Richmond.

Figure 2-3 Existing Bicycle Network



It is noted that to improve safety for cyclists that traverse between Napier Street and Lansdowne Street, new traffic signals have been installed on the departure side (eastbound) of the Victoria Parade / Lansdowne Street intersection.

Council have also recently installed bike share pods along Napier Street, proximate to its intersection with Victoria Parade, and we understand that they also plan to install an additional bike share pod at the Fitzroy Town Hall.

Attachment 8 - ACU Development Plan - Traffic Part 1



Integrated Transport and Access Plan Australian Catholic University, St. Patricks Campus, Fitzroy

2.5 Public Transport

The subject site has excellent access to public transport, with all train lines accessible via Parliament Station approximately 550 metres walking distance from the subject site and numerous bus and tram routes operating either along the site frontage or in close proximity to the subject site.

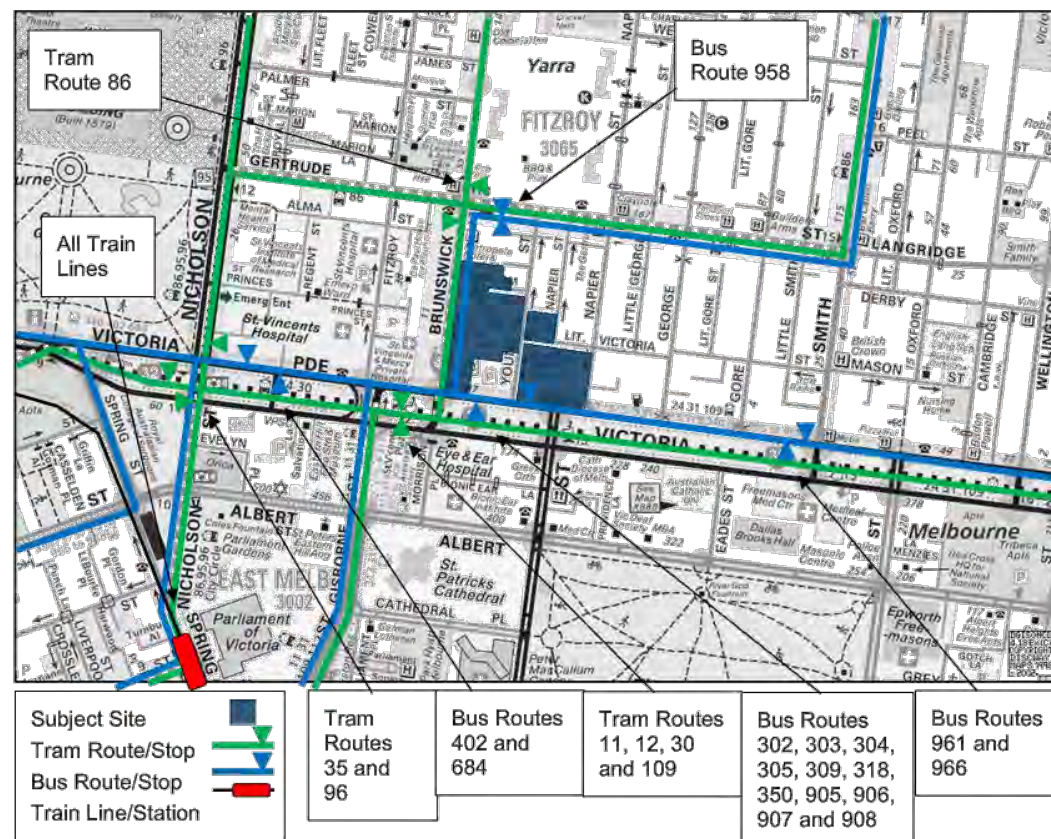
The St Vincents Plaza / Victoria Parade tram stop and the Australian Catholic University / Victoria Parade bus stop are located directly adjacent to the site and provide access to a total of 4 tram routes and 11 bus routes. Parliament Station is also accessible via tram routes 11, 12 and 109 which are serviced by the abovementioned St Vincents Plaza / Victoria Parade tram stop.

Tram lines along Victoria Street, Nicholson Street and Gisborne Street are separated from the traffic lanes, whilst the tram lines along Gertrude Street, Brunswick Street and Smith Street are shared with a traffic lane.

Notably, bus lanes have recently been installed along Victoria Parade to provide bus priority through this corridor. It is also understood that plans are being progressed to upgrade the Brunswick Street tram corridor, with the works including the provision of DDA compliant stops.

The public transport services in the vicinity of the site are summarised in Table 2-6 and illustrated graphically in Figure 2-4.

Figure 2-4 Public Transport Map



Attachment 8 - ACU Development Plan - Traffic Part 1



Table 2-6 Public Transport Services

Service	Route No's	Route	Nearest Stop	Approximate Walking Distance
Tram	11	West Preston – Victoria Harbour Docklands	St Vincents Plaza/Victoria Pde	Adjacent Site
	12	Victoria Gardens – St Kilda		
	30	St Vincents Plaza – Docklands via La Trobe St		
	109	Box Hill – Port Melbourne		
	86	Bundoora RMIT – Waterfront City Docklands	Brunswick St/Gertrude St	100 m
	35	City Circle (Free Tourist Tram)	Nicholson St/Victoria Pde	300 m
	96	East Brunswick – St Kilda Beach		
Bus	302	Box Hill via Belmore Rd and Eastern Fwy	Australian Catholic University / Victoria Pde	Adjacent Site
	303	City – Ringwood North via Park Rd		
	304	City – Doncaster SC via Belmore Rd and Eastern Fwy		
	305	City – The Pines SC via Eastern Fwy		
	309	City – Donvale via Reynolds Rd		
	318	City – Deep Creek		
	350	City – La Trobe University via Eastern Fwy		
	905	City – The Pines SC via Eastern Fwy, Templestowe (SMARTBUS Service)		
	906	City – Warrandyte via The Pines SC (SMARTBUS Service)	St Vincents Hospital / Victoria Pde	200 m
	907	City – Mitcham via Doncaster Road (SMARTBUS Service)		
	908	City – The Pines SC via Eastern Fwy (SMARTBUS Service)		
	402	Footscray – East Melbourne via North Melbourne		
	684	Eildon – Melbourne via Lilydale Station		
	958	NightRider – City – Eltham via Smith Street, Darebin Road	Brunswick St / Gertrude St	100 m
	961	NightRider – City – Doncaster via Eastern Freeway, James Street	Smith St / Victoria Pde	300 m
	966	NightRider – City – Croydon – Lilydale via Victoria Street, Maroondah Highway		
Train	All Train Lines		Parliament Station	550 m

A consolidated public transport and bicycle network plan is provided as Figure 2-5 overleaf.

Attachment 8 - ACU Development Plan - Traffic Part 1



Integrated Transport and Access Plan Australian Catholic University, St. Patricks Campus, Fitzroy

Figure 2-5 Public Transport and Bicycle Network



Legend

- Shared Off-Road Path
- Formal On-Road Bike Route
- Informal Bike Route
- Walking Track, steps
- Bus Route, with terminus
- Tram Route, with stop & terminus
- Railway, with Station
- Municipal Boundary
- Traffic or Pedestrian Light, Pedestrian Crossing
- Bike Shop
- BikeShare location
- CarShare Pod
- Drinking Fountain
- Toilet, accessible



Subject Site

Attachment 8 - ACU Development Plan - Traffic Part 1



Integrated Transport and Access Plan Australian Catholic University, St. Patricks Campus, Fitzroy

2.6 Traffic Network

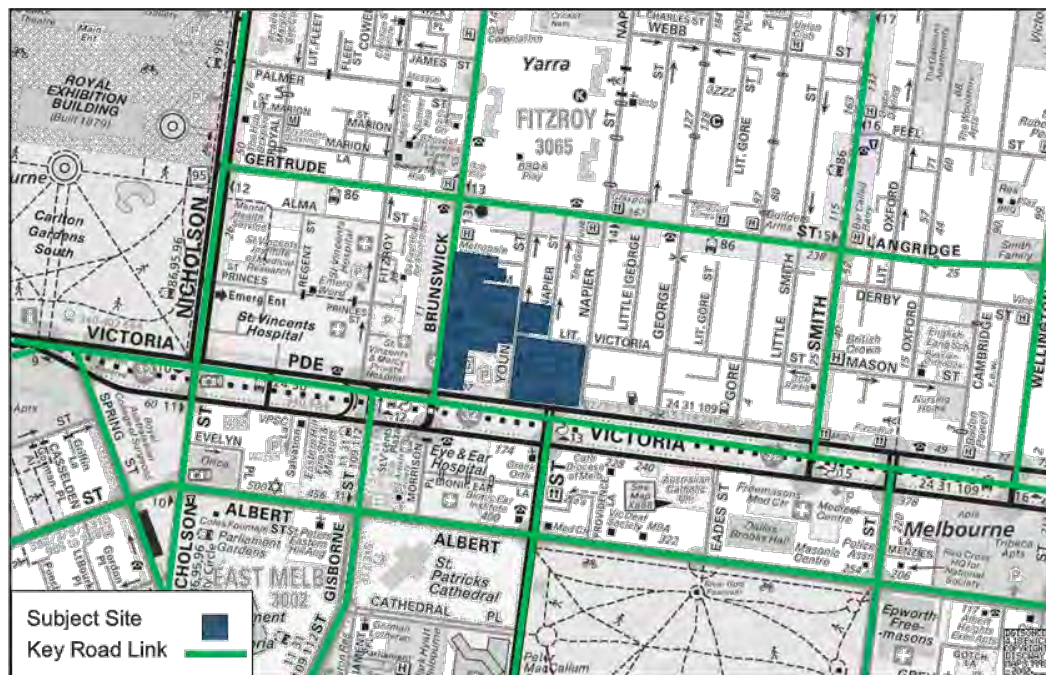
2.6.1 General

The subject site is primarily accessible via Victoria Parade and Young Street. Various additional crossovers and roller doors provide secondary access, generally for loading and other authorised vehicles.

The key road links in the vicinity of the site are shown in Figure 2-6, providing access to surrounding suburbs including Collingwood and Richmond to the east, Fitzroy North to the north, Carlton and the CAD to the west and East Melbourne to the south.

Both the Eastern Freeway and CityLink are readily accessible from Hoddle Street via Victoria Parade. A detailed review of the streets in the immediate vicinity of the site is provided in the following sections.

Figure 2-6 Key Road Links in the Vicinity of the Site



2.6.2 Brunswick Street

Brunswick Street is generally aligned in a north-south direction from St Georges Road in Fitzroy North to Victoria Parade in Fitzroy. North of Alexandra Parade, Brunswick Street is a Declared Main Road. ACU is located on the east side of Brunswick Street.

Adjacent the subject site, Brunswick Street operates with two lanes in each direction, with the central lanes shared between tram lines and traffic and the kerbside lanes generally operate as shared parking and bicycle lanes, with the exception of between 7:00am – 9:00am Monday to Friday when 'No Stopping' restrictions apply on the eastern kerb.

Both the Brunswick Street / Gertrude Street and Brunswick Street / Victoria Parade intersections are controlled by traffic signals.

In the vicinity of the site, Brunswick Street operates with a posted speed limit of 40 kilometres per hour. A view of Brunswick Street in the vicinity of the site is provided in Figure 2-7.

Attachment 8 - ACU Development Plan - Traffic Part 1



Figure 2-7 Cross Section of Brunswick Street in the Vicinity of the Site



2.6.3 Young Street

Young Street is a local road aligned in a north-south direction from Gertrude Street to Victoria Parade. Young Street primarily serves an access function, with speed humps used along its length for traffic calming purposes. ACU is located on both the east and west sides of Young Street, with vehicle access provided to an ACU car park and an ACU private road named Duke Street.

Along its length, Young Street operates with a varying cross section. Between Victoria Parade and the entrance to the ACU car park, Young Street operates with a single traffic lane in each direction. This section of Young Street operates with a posted speed limit of 40 kilometres per hour. Beyond this point, Young Street operates as one way, northbound only.

Between Little Victoria Street and Duke Street, Young Street operates as a shared zone, within a carriageway which allows for a single lane of northbound traffic. As required for shared zones, this section of Young Street operates with a speed limit of 10 kilometres per hour. There is generally no provision for kerbside parking, with the exception of a single indented parallel disabled car space.

Between Duke Street and Graham Street, Young Street operates with a single northbound traffic lane, with no provision for kerbside parking. North of the shared zone, Young Street operates with a default speed limit of 50 kilometres per hour.

Between Graham Street and Gertrude Street, Young Street allows for a single lane of northbound traffic, with kerbside parallel parking permitted on the west kerb clear of traffic.

The Young Street / Victoria Parade intersection operates as a left in/left out 'Give Way' controlled T-intersection, with priority given to Victoria Parade.

The Young Street / Gertrude Street intersection operates as a 'Stop' controlled T-intersection, with priority given to Gertrude Street and allows for outbound movements out of Young Street only. Right turns are banned between 7:00am – 9:00am Monday to Friday.

The various cross sections of Young Street are shown in Figure 2-8 through to Figure 2-11.

Attachment 8 - ACU Development Plan - Traffic Part 1



Integrated Transport and Access Plan Australian Catholic University, St. Patricks Campus, Fitzroy

Figure 2-8 Cross Section of Young Street Between Victoria Parade and ACU Car Park Entrance



Figure 2-9 Cross Section of Young Street Between ACU Car Park Entrance and Duke Street



Attachment 8 - ACU Development Plan - Traffic Part 1



Integrated Transport and Access Plan Australian Catholic University, St. Patricks Campus, Fitzroy

Figure 2-10 Cross Section of Young Street Between Duke Street and Graham Street



Figure 2-11 Cross Section of Young Street Between Graham Street and Gertrude Street



2.6.4 Napier Street

Napier Street is a local road aligned in a north-south direction from Cecil Street to Victoria Parade. Various traffic calming treatments are used along the length of Napier Street, including speed humps and no through roads. ACU is located on the west side of Napier Street. No vehicle access to ACU is provided via Napier Street.

In the vicinity of the site, Napier Street allows for a single lane of traffic in each direction, with angle parking permitted alongside one kerb and parallel parking permitted on the other kerb and a bicycle symbol identifying Napier Street in the vicinity of the site as an informal bicycle route.

The Napier Street / Gertrude Street intersection operates as a 'Stop' controlled cross intersection, with priority given to Gertrude Street. Right turns out of the southern Napier Street approach are not permitted between 7:30am – 9:00am Monday to Friday.

The Napier Street / Victoria Parade intersection is a left out only 'Stop' controlled intersection, with priority given to Victoria Parade. Inbound movements into Napier Street are not permitted.

In the vicinity of the site, Napier Street operates with a posted speed limit of 40 kilometres per hour.

A view of Napier Street in the vicinity of the site is provided in Figure 2-12.

Attachment 8 - ACU Development Plan - Traffic Part 1



Integrated Transport and Access Plan Australian Catholic University, St. Patrick's Campus, Fitzroy

Figure 2-12 Cross Section of Napier Street in the Vicinity of the Site



2.6.5 Victoria Parade

Victoria Parade is a Declared Main Road which extends generally in an east-west direction from Hoddle Street to La Trobe Street. At both ends, Victoria Parade continues as Victoria Street. ACU is located on the north side of Victoria Parade, with access provided to an on-site car park.

In the vicinity of the site, Victoria Parade operates with three traffic lanes and a bus lane in each direction, separated by a wide central median which accommodates tram lines, tram stops and footpaths. Left and right turn deceleration lanes are provided at key intersections.

Along its length, parallel parking is intermittently permitted on both the north and south kerbs and as well as both kerbs of the central median, however in the vicinity of the site, parking is only permitted on the south kerb.

Victoria Parade operates with a posted speed limit of 60 kilometres per hour. A view of Victoria Parade in the vicinity of the site is provided in Figure 2-13.

Figure 2-13 View of Victoria Parade in the Vicinity of the Site



Attachment 8 - ACU Development Plan - Traffic Part 1



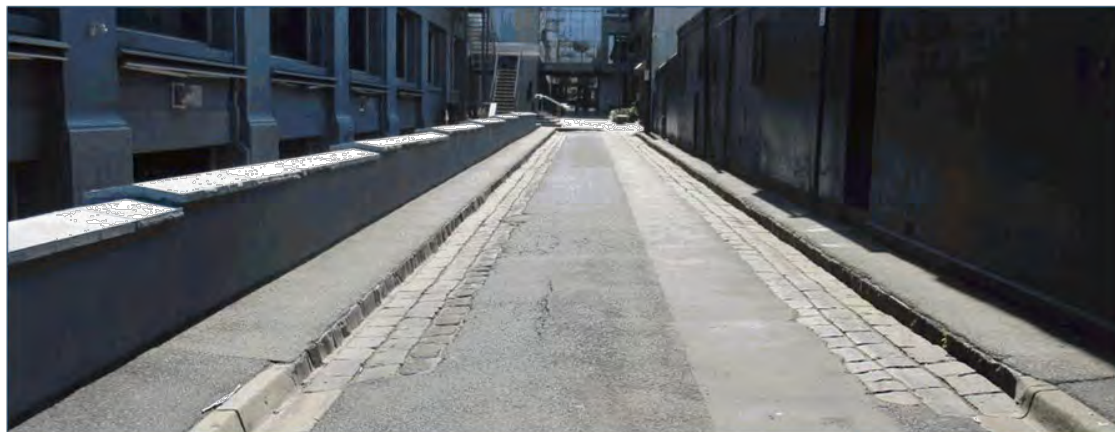
2.6.6 Little Victoria Street

Little Victoria Street is a right of way (ROW) which extends in an east-west direction from George Street to Little Napier Street. Little Victoria Street terminates at the eastern boundary of ACU, however does not provide vehicle access to the ACU.

In the vicinity of the site, Little Victoria Street operates as one way westbound only within a single traffic lane and provides access to Little Napier Street which services dwellings fronting Napier Street. Kerbside parking is not permitted.

A view of Little Victoria Street in the vicinity of the site is provided in Figure 2-14.

Figure 2-14 Cross Section of Little Victoria Street in the Vicinity of the Site



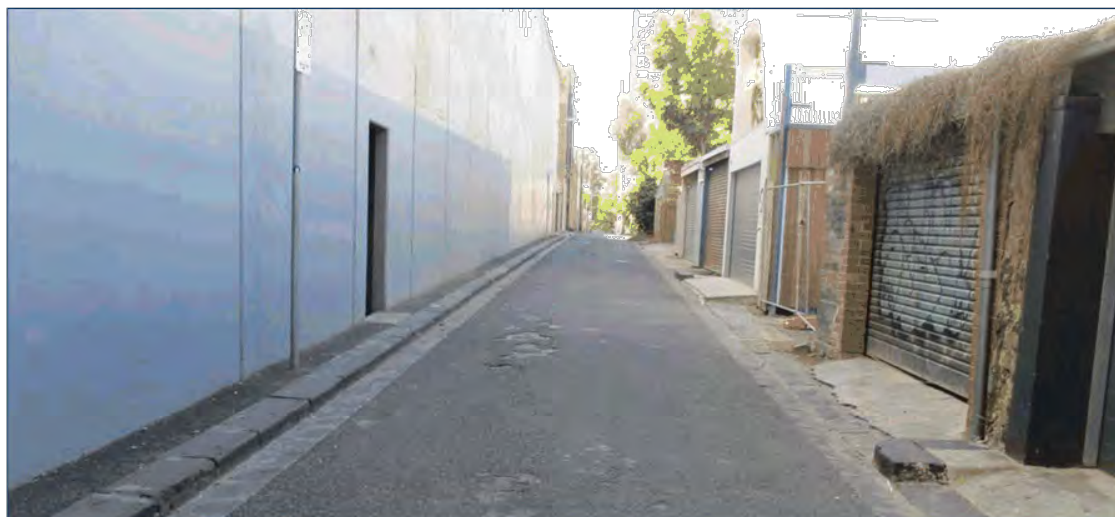
2.6.7 Little Napier Street

Little Napier Street is a right of way (ROW) which extends in a north-south direction from Gertrude Street to Little Victoria Street. Little Napier Street is aligned along part of ACU's eastern boundary, however does not provide vehicle access.

Little Napier Street operates as one way northbound only within a single traffic lane and services dwellings fronting Napier Street.

A view of Little Napier Street is provided in Figure 2-15.

Figure 2-15 Cross Section of Little Napier Street in the Vicinity of the Site



Attachment 8 - ACU Development Plan - Traffic Part 1



2.6.8 Graham Street

Graham Street is a local road which extends west from Young Street for approximately 45 metres before terminating. Graham Street extends to the boundary of ACU.

Graham Street operates as two way, however allows for a single direction of travel at any one time. Parallel parking is permitted on the northern kerb, as shown in Figure 2-16.

Figure 2-16 Cross Section of Graham Street in the Vicinity of the Site



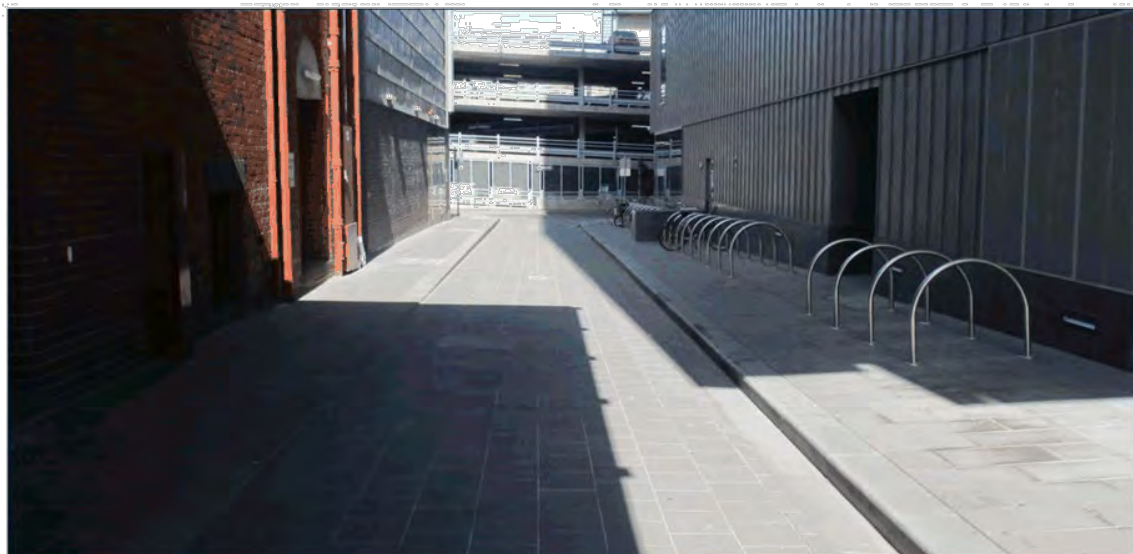
2.6.9 Duke Street

Duke Street is a private road within ACU which extends west from Young Street for approximately 40 metres.

Duke Street operates as a shared zone, within a carriageway which allows for a single direction of travel at any one time. As required for shared zones, this section of Young Street operates with a speed limit of 10 kilometres per hour with a speed limit of 10 kilometres per hour.

No entry to Duke Street is permitted with the exception of authorised vehicles. The cross section of Duke Street in the vicinity of the site is provided in Figure 2-17.

Figure 2-17 Cross Section of Duke Street in the Vicinity of the Site



Attachment 8 - ACU Development Plan - Traffic Part 1



Integrated Transport and Access Plan Australian Catholic University, St. Patricks Campus, Fitzroy

2.7 Road Network Safety

Casualty accident history for the roads and intersections adjoining the ACU, specifically, roads bound by Victoria Parade to the south, Gertrude Street to the north, Brunswick Street to the west and Napier Street to the east have been sourced from VicRoads CrashStats accident database.

A summary of the accidents in the area defined above for the last available five year period (1 July 2008 to 30 June 2013) are summarised in Figure 2-18.

Figure 2-18 Causality Accident History - 1 July 2008 to 30 June 2013



The CrashStats review indicates that a total of 33 casualty accidents have been reported within the nominated area within the five year period. Table 2-7 provides a broad summary of the accident categories.

Table 2-7 Accident Categories

Accident Type	Number
Involving Bicyclists	16
Involving Motor Cyclists	8
Involving Pedestrian	5
Involving Motorists Only	4
Total	33

Attachment 8 - ACU Development Plan - Traffic Part 1



2.8 Car Parking

The site is located in an area with constrained long term and short term on street parking opportunities with observations revealing that on street parking opportunities in the vicinity of the site are generally ticketed and or marked as permit zones with the limited number of long term (4 hours or greater) parking opportunities observed to be well utilised.

Notwithstanding the above, commercial off-street parking facilities are available and include the onsite parking spaces provided by the University, specifically, a 250 space deck car park accessible via Young Street, and a 50 space at grade car park accessible via Victoria Parade.

The location of these onsite parking areas are illustrated in Figure 2-19.

Figure 2-19 ACU Onsite Car Parking Spaces



Attachment 8 - ACU Development Plan - Traffic Part 1



3 State and Local Policies

3.1 State Policies

3.1.1 Plan Melbourne

Plan Melbourne outlines the vision for Melbourne's growth to the year 2050. The document was developed through extensive collaboration across government and a sustained dialogue with professional, industry and community stakeholders. In October 2013, Plan Melbourne was released for comment, with the final Plan Melbourne released in May 2014 following submissions.

Plan Melbourne specifies seven outcomes and objectives for Melbourne, summarised as follows:

- > **Delivering jobs and investment:** Create a city structure that drives productivity, supports investment through certainty and creates more jobs;
- > **Housing choice and affordability:** Provide a diversity of housing in defined locations that cater for different households and are close to jobs and services;
- > **A more connected Melbourne:** Provide an integrated transport system connecting people to jobs and services, and goods to market;
- > **Liveable communities and neighbourhoods:** Create healthy and active neighbourhoods and maintain Melbourne's identity as one of the world's most liveable cities;
- > **Environment and water:** Protect our natural assets and better plan our water, energy and waste management systems to create a sustainable city;
- > **A state of cities:** Maximise the growth potential of Victoria by developing a state of cities which delivers choice, opportunity and global competitiveness; and
- > **Implementation:** Delivering better governance: Achieve clear results and deliver outcomes through better governance, planning, regulation and funding mechanisms.

Plan Melbourne provides a number of directions to address the abovementioned outcomes and objectives. Those considered relevant to this report include:

- > **Direction 1.1:** Define a new city structure to deliver an integrated land use and transport strategy for Melbourne's changing economy;
- > **Direction 3.1:** Transform the transport system to support a more productive central city;
- > **Direction 3.2:** Improve access to job-rich areas across Melbourne and strengthen transport networks in existing suburbs; and
- > **Direction 3.4:** Improve local travel options to increase social and economic participation.

A discussion paper called Plan Melbourne Refresh was prepared in October 2015 to build upon the work and consultation undertaken in preparing Plan Melbourne, with Plan Melbourne 2016 to be released in the first half of 2016.

It is understood that Plan Melbourne 2016 will maintain the key priorities of Plan Melbourne 2014 but strengthen its focus on a number of areas.

3.1.2 Transport Integration Act

The Transport Integration Act 2010 creates a new framework for the provision of an integrated and sustainable transport system in Victoria. The Act provides a vision statement, reproduced below:

"The Parliament recognises the aspirations of Victorians for an integrated and sustainable transport system that contributes to an inclusive, prosperous and environmentally responsible State."

The Transport Integration Act recognises that the transport system should be considered as a single system and ensures that all transport agencies work together.

Attachment 8 - ACU Development Plan - Traffic Part 1

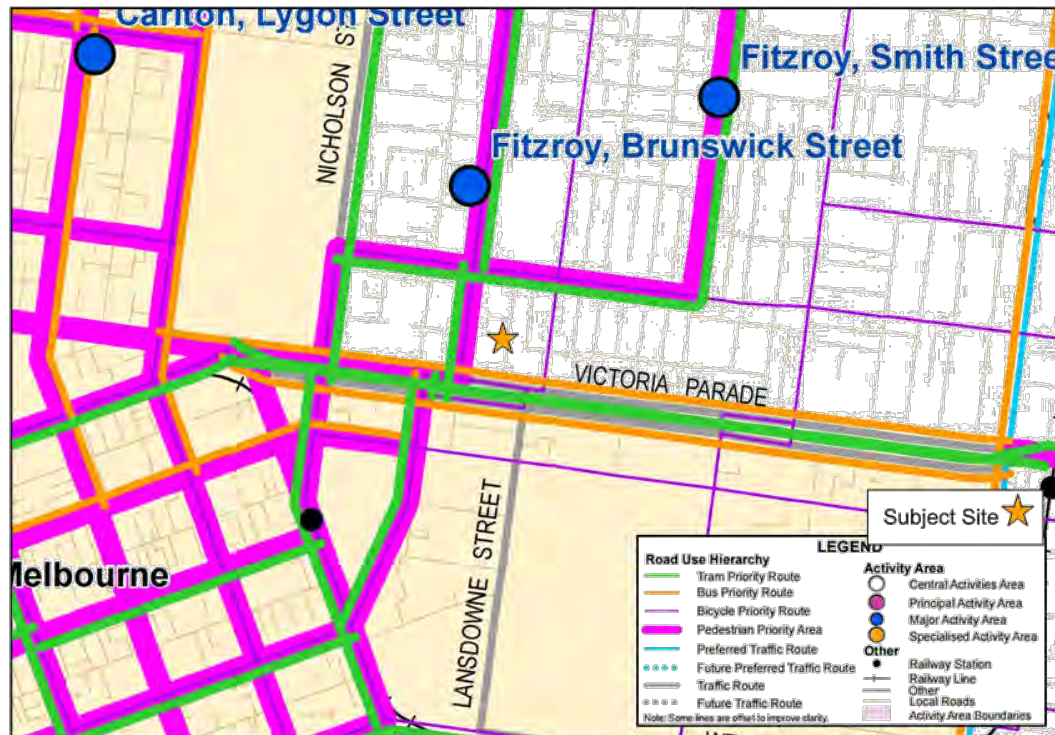


3.1.3 VicRoads' SmartRoads

SmartRoads is a policy implemented by VicRoads to manage competing interests for limited road space by giving priority use of the road to different transport modes at particular times of the day. Road Use Hierarchy Maps show the priority modes on each road for each Council area and form the foundation for the network operating plan.

The Road Use Hierarchy Map in the vicinity of the site is provided in Figure 3-1 and shows Victoria Parade as a tram priority, bus priority and traffic route, whilst Brunswick Street is shown as a tram priority and bicycle priority route as well as a pedestrian priority area.

Figure 3-1 SmartRoads Road Use Hierarchy Map



3.1.4 Victorian Cycling Action Plan 2013 & 2014

The Victorian Cycling Action Plan 2013 & 2014 was prepared for the Victorian Government and was released in December 2012. The document aims to grow and support cycling in Victoria by making it easier for more people to cycle and to make it safer for people who already ride as well as improving the cycling experience for all types of bike riders.

Six directions are identified within the Victorian Cycling Action Plan to achieve the above aims, reproduced as follows:

- > **Build evidence:** build a stronger evidence base for the Victorian Government to make more informed decisions;
- > **Enhance governance and streamline processes:** clarify accountability and improve co-ordination, planning and delivery;
- > **Reduce safety risks:** reduce conflicts and risks to make cycling safer;
- > **Encourage cycling:** help Victorians feel more confident about cycling and make cycling more attractive;
- > **Grow the cycling economy:** support opportunities to grow and diversify Victoria's economy through cycling; and
- > **Plan networks and prioritise investment:** plan urban cycling networks to improve connectivity and better target infrastructure investment for urban networks, regional trails and specialist cycle sport infrastructure.

Attachment 8 - ACU Development Plan - Traffic Part 1



3.1.5 Pedestrian Access Strategy

The Pedestrian Access Strategy was prepared in 2010 and aims to increase walking for transport in Victoria and notes that more people walking has the potential to help ease congestion, reduce greenhouse emissions, improve the health of Victorians and promote social connections.

The document lists five strategic directions for walking, reproduced as follows:

- > **Encourage people to walk by changing attitudes and behaviour:** This aims to make walking the top-of-mind choice for Victorians – especially for short trips – by making walking for transport a visible and valued part of daily life;
- > **Collaborate to improve provision for walking:** This aims to clarify the roles and responsibilities of both state and local governments in providing for walking. The Victorian Government will work with local governments to ensure they have the capacity and information they need to provide better pedestrian facilities;
- > **Create pedestrian-friendly built environments, streets and public spaces:** This aims to ensure built environments across Victoria facilitate easy and efficient pedestrian movements;
- > **Increase the safety of walking:** This will identify and address risks to pedestrians across the transport system and give pedestrians the skills to negotiate road environments; and
- > **Continue integrating walking with public transport:** This aims to ensure more Victorians walk in combination with public transport. Walkers need to find it easy to get to major public transport hubs across Victoria and easy walking access should be provided at public transport stops.

3.2 Local Policy

3.2.1 Clause 21.06 of the Yarra Planning Scheme

Clause 21.06 of the Yarra Planning Scheme details the City of Yarra's transport related objectives and strategies.

Clause 21.06 acknowledges that the City of Yarra needs to reduce car dependence by promoting walking, cycling and public transport use as viable and preferable alternatives. Clause 21.06 lists four objectives as follows:

- > **Objective 30:** To provide safe and convenient pedestrian and bicycle environments;
- > **Objective 31:** To facilitate public transport usage;
- > **Objective 32:** To reduce the reliance on the private motor car; and
- > **Objective 33:** To reduce the impact of traffic.

Of particular relevance to this report is Strategy 32.2 in response to objective 32, reproduced below:

- > **Strategy 32.2:** Require all new large developments to prepare and implement integrated transport plans to reduce the use of private cars and to encourage walking, cycling and public transport.

3.2.2 Strategic Transport Statement 2006 - City of Yarra

The Strategic Transport Statement is a document prepared by the City of Yarra in 2006 which addresses the access needs of Yarra's community whilst minimising the impact of cars on Yarra's community. The Strategic Transport Statement has a stated vision as follows:

"To create a city which is accessible to everyone irrespective of levels of personal mobility and where a fulfilling life can be had without the need for a car."

The Strategic Transport Statement lists seven key Strategic Transport Objectives to achieve the above vision as follows:

- > Create a city which is a great and safe place to walk and increase the numbers of those walking in Yarra.
- > Create the most bicycle friendly city in Australia and increase the numbers of those cycling in Yarra.
- > Advocate for increased performance of public transport across Melbourne and thereby reduce the number of car trips and through traffic by both Yarra and non-Yarra residents.
- > Ensure that any new road construction is not in conflict with encouraging more sustainable transport use.
- > Ensure Council's response to parking demand is based on Yarra's parking hierarchy and sustainable transport principles.

Attachment 8 - ACU Development Plan - Traffic Part 1



- > Work to limit freight movement to arterial roads and freeways within Yarra and work to reduce freight movement through Yarra.
- > Encourage Council staff to use more sustainable transport for their travel and increase the capacity of the Council as a whole to respond to and initiate positive actions to further strategic transport objectives 1 to 6.

3.2.3 City of Yarra Bicycle Strategy 2010 – 2015

The City of Yarra Bicycle Strategy 2010 – 2015 details the status on bicycle initiatives and lays out future plans for bicycle facilities, with an aim to establish cycling as a legitimate first choice of transport by people of all ages and cycling abilities.

The document lists 11 strategies to achieve the above, listed as follows:

- > **Strategy 1:** Better on-road bicycle network;
- > **Strategy 2:** Better local streets for cycling;
- > **Strategy 3:** Better off-road bicycle network;
- > **Strategy 4:** Better bicycle network maintenance;
- > **Strategy 5:** Better end of trip facilities – bicycle parking;
- > **Strategy 6:** Better bicycle network accountability;
- > **Strategy 7:** Better bicycle safety by reducing conflict;
- > **Strategy 8:** Better Council use of bicycles;
- > **Strategy 9:** Better recruitment and retention of cyclists;
- > **Strategy 10:** Better policies; and
- > **Strategy 11:** Better innovation and relationships.

3.2.4 Inner Melbourne Action Plan (IMAP)

The Inner Melbourne Action Plan (IMAP) was prepared by the Cities of Melbourne, Yarra, Port Phillip, and Stonnington in partnership with VicUrban and was adopted in December 2005. Maribyrnong City Council also became a member of the IMAP in 2013.

The IMAP lists a number of strategies to achieve its vision, including the following which are considered relevant to this report:

- > **Strategy 2:** Effectively link transport routes so that the Inner Melbourne Region is accessible throughout by walking, cycling and public transport;
- > **Strategy 3:** Minimise the growing impact of traffic congestion;
- > **Strategy 4:** Increase public transport use.

Attachment 8 - ACU Development Plan - Traffic Part 1



4 Development Proposal

4.1 Development Plan

The Development Plan has been developed to accommodate the ACU's projected growth in students, teaching staff and research from 2015 to 2025, whilst also allowing the consolidation of teaching and research activities onto the Campus.

Specifically it is anticipated that by 2020, ACU will increase their EFT student numbers to 10,700 and staff numbers to 850. A summary of existing and proposed student and staff numbers is provided in Table 4-1.

Table 4-1 Student and Staff Numbers

Existing	Proposed	Change
10,000 EFT Students	10,700 EFT Students	+700 EFT Students
800 Staff	850 Staff	+50 Staff

The key principle that will inform the Development Plan from a transport perspective is based on the City of Yarra's commitment to reduce car dependency by promoting walking, cycling and public transport use.

Specifically, the St Patrick's Campus Development Plan seeks to become a pedestrian oriented space that is safe for students and prioritises sustainable modes of transport by:

- > Improving access to public transport along Victoria Parade, Brunswick Street and Gertrude Street;
- > Ensuring pedestrian links from the Campus integrate and form part of the wider pedestrian network;

The above will be achieved through the creation of strong pedestrian linkages through the Campus and to adjacent uses, activities and transport networks. Notably, the facade treatment to Victoria Parade on the Mary Glowrey Building is to be removed and by so doing, substantially increasing the footpath width along Victoria Parade in the section between Young Street and Napier Street. This will be particularly beneficial at the interface of the Mary Glowrey Building and the existing bus stop.

4.2 Individual Projects

The individual projects within the Development Plan will include:

4.2.1 115 B Victoria Parade

This project will comprise of the subdivision of 115 Victoria Parade to create a new building site. Within this site, a multi storey building is contemplated, and will include the provision of basement level car parking for 270 spaces.

Vehicular access to this building is planned via Napier Street. Mitigation works will be required at the intersection of Victoria Parade and Napier Street to accommodate direct access via Victoria Parade.

4.2.2 28 - 42 Young Street (Existing Deck Car Park)

The existing 250 space deck car park is to be demolished and in its place a new building is proposed.

This new building will have no car parking spaces provided, instead, the development of this site will provide the opportunity to make substantial improvements to pedestrian access and connectivity between the campus buildings, and the adjacent campus uses and activities.

Attachment 9 - ACU Development Plan - Traffic Part 2



5 Transport Network Interventions

5.1 Pedestrian Network

The Development Plan contemplates improvements to pedestrian access throughout the Campus, with the aim of providing an attractive and safe pedestrian environment that will integrate with the surrounding local precinct.

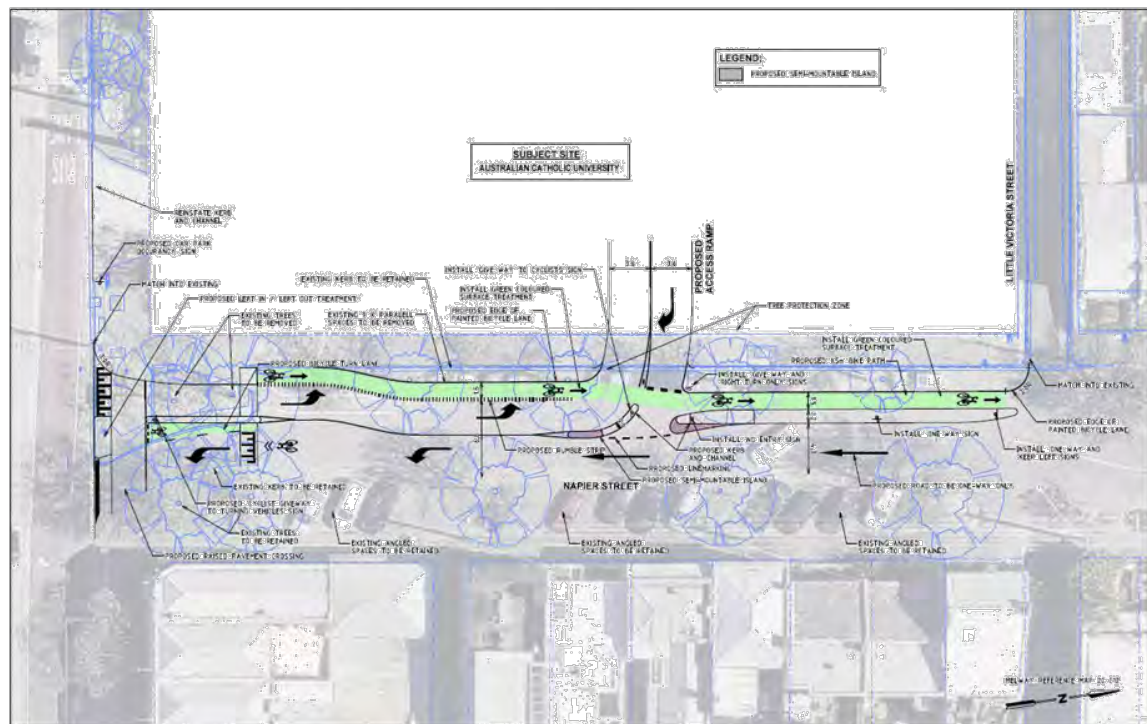
The key initiatives in this regard will be the creation of strong pedestrian linkages through the Campus and to adjacent uses, activities and transport networks. Notably, the facade treatment to Victoria Parade on the Mary Glowrey Building is to be removed and by so doing, substantially increasing the footpath width along Victoria Parade in the section between Young Street and Napier Street. This will be particularly beneficial at the interface of the Mary Glowrey Building and the existing bus stop.

5.2 Bicycle Network

The mitigation works required at the intersection of Victoria Parade and Napier Street to accommodate vehicular access to the 115 B Victoria Parade project, provides the opportunity to improve cyclist amenity at this intersection.

The concept plan shown as Figure 5-1 illustrates the proposed mitigation works and improvements to cyclist amenity.

Figure 5-1 Victoria Parade / Napier Street – Concept Design



Attachment 9 - ACU Development Plan - Traffic Part 2



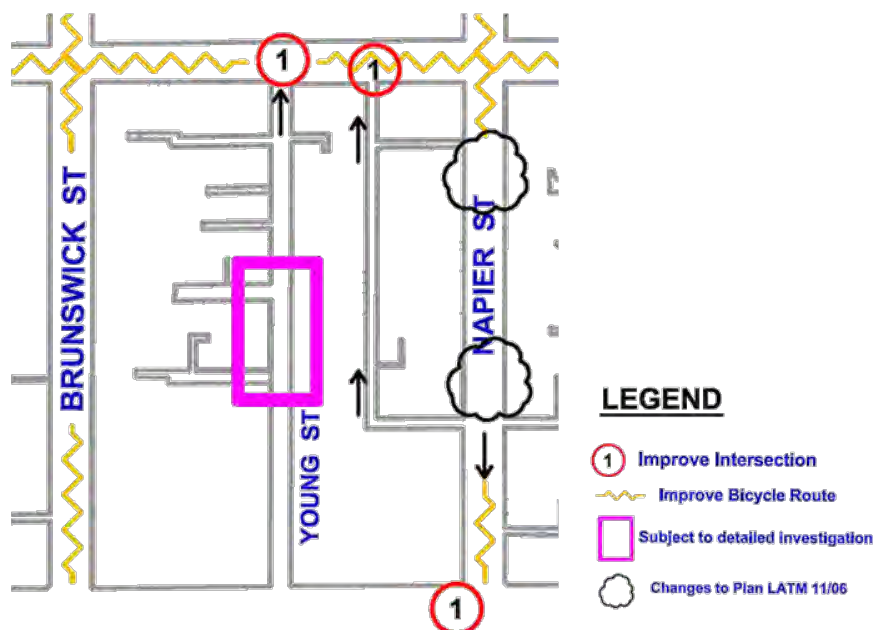
5.3 Local Area Traffic Management

The City of Yarra have over the years undertaken Local Area Traffic Management (LATM) studies to improve traffic conditions and road safety in local streets.

The studies looked at issues such as traffic speed and volume, pedestrian safety and comfort, and how to calm traffic so that neighbourhoods are more liveable. Notably, as part of these studies residents and businesses were surveyed about the traffic issues in the area and proposed treatments.

The ACU St Patrick's Campus is located within the bounds of Council's LATMS11 – Fitzroy area. An extract of LATMS11 – Fitzroy is provided as Figure 5-2

Figure 5-2 Extract of LATMS 11 – Fitzroy



LATMS11 identifies the intersection of Victoria Parade and Napier Street as an intersection that requires improvement, furthermore, the section of Young Street between Little Victoria Street and Duke Street has also been identified as an area that is subject to detailed investigation.

Noting the foregoing, the proposed pedestrian and bicycle network interventions could be considered as options consistent with the aspirations of LATMS 11 – Fitzroy.

Attachment 9 - ACU Development Plan - Traffic Part 2

6 Statutory Considerations

6.1 Schedule 2 to the Development Plan Overlay

The statutory requirements applicable to the Development Plan are contained within Schedule 2 to the Development Plan Overlay.

The traffic specific requirements are reproduced below.

The development plan must show:

- > The number, location, dimensions, and layout of all car parks and access ways to and from them;
- > A management plan for the operation and maintenance of the car park areas;
- > The location and dimensions of all bicycle, vehicle and pedestrian ways;
- > A traffic management plan which must show any traffic management and traffic control works considered necessary in adjoining and nearby roads when the development is completed; and
- > The means of vehicular and pedestrian ingress to and egress from the land.

An assessment of the Development Plans against the above noted statutory requirements follows:

6.2 Design Response

6.2.1 Car Parking and Accessways

A total of 270 car spaces are proposed. These spaces are to be provided within basement levels on the 115 B Victoria Parade project.

Specific to the DPO Requirements:

- > Parking spaces are to be provided at a minimum 4.9m long and 2.6m wide;
- > Accessways within the car park are to be provided at a minimum 6.4m wide where they serve car parking spaces and a minimum 5.5m wide where they do not abut parking. The main accessway off Napier Street is provided at a width of 7.6m;
- > A minimum head height of 2.1 metres is proposed within the basement car park, increasing to 2.5m where provision is made for disabled spaces and Small Rigid Vehicles.

These dimensions are consistent with the requirements contained within Clause 52.06 of the City of Yarra Planning Scheme.

6.2.2 Bicycle Ways

An additional 80 bicycle spaces are to be provided as part of the Development Plan.

These bicycles are to be spread throughout the campus and designed to meet the dimension requirements set out by Bicycle Network and or AS2890.3:2015, and would comprise a mix as appropriate of the products as per the spec sheets provided as Appendix B.

6.2.3 Pedestrian Ways

These pedestrian linkages through the site are to be designed to meet and or exceed the relevant standards.

6.2.4 Means of Vehicular and Pedestrian Access

For the 115B Victoria Parade project, vehicular access is to be facilitated via Napier Street, as illustrated in the accompanying concept functional layout plan, Cardno drawing number CG150178-TR-DG-2505-3 provided as Appendix A.

Pedestrian access is to be provided via Victoria Parade, and Napier Street. The new building will also be accessible via existing pedestrian access points provided for the adjacent Mary Glowrey Building.

With regard to the 28 - 42 Young Street project, no car parking is proposed. Accordingly, no vehicular access is contemplated. Pedestrian access will be via Little Victoria Street and Young Street.

Attachment 9 - ACU Development Plan - Traffic Part 2



6.2.5 Traffic Management Plan

Victoria Parade / Napier Street

Vehicular access to the 115B Victoria Parade project will be via Napier Street. Ingress and egress will be restricted to the intersection of Victoria Parade and Napier Street.

This intersection will require mitigation works to allow access via Victoria Parade. The traffic management and traffic control works considered necessary to mitigate the impact of the development at this intersection are illustrated in Cardno drawing number CG150178-TR-DG-2502-3, provided as Appendix A.

Young Street

Tube count surveys undertaken on Young Street in October 2015, at a location north of the existing access to the deck car park indicates that on average Young Street carries about 550 vehicles per day northbound, with the car park contributing about 100 vehicles per day to this total figure. On weekends northbound traffic flows reduce to 250 vehicles per day.

On this basis, Young Street can be classified as an Access Place, noting:

- > An Access Place as defined by Clause 56.06 of the City of Yarra Planning Scheme, is a minor street providing local residential access with shared traffic, pedestrian and recreation use, but with pedestrian priority;
- > This road can accommodate an indicative maximum traffic volume of between 300 – 1000 vehicles over a 24-hour period, within a 5.5m wide carriageway with parking on one side of the carriageway.

The proposed demolition of the existing deck car park will remove traffic currently generated by the car park, thus reducing daily traffic flows along this road to the benefit of pedestrian movements via the reduction in the potential for vehicle and pedestrian conflicts.

6.2.6 Car Park Management Plan

The following conditions detail the operation and management of the car parking spaces proposed within the 115B Victoria Parade project.

- > The parking area will be secured by boom gates, activated by card readers and ticket machine on entry and departure during the facility's operating hours, anticipated to be between 7:00am – 10:30pm Weekdays and 8:00am – 10:30pm weekends consistent with current operational practises for the existing car park. During afterhours the car park will be secured by roller doors.
- > To alert casual parkers entering the car park and ensure ease of use, clearly visible advisory signs will be posted on the car park entry and also within the car park. Additionally, to avoid conflict between long term (staff) and short term (students) users, spaces will be designated to individual staff members once they are determined, with signage posted at the end of each bay.
- > Operation of the public car park will be limited to the hours listed above, with its fee structure expected to be based on an hourly charge rates in-line with other commercial car park facilities in the vicinity of the site. The actual charge rates will however be determined once an operator is appointed. The car park is to be operated by ticket collection on entry, and then payment at a pay station likely to be located adjacent to the lifts.
- > Dynamic signage on Victoria Parade will be provided to indicate if the car park is full, which will reduce the number of vehicles turning into Napier Street during high occupancy periods. Notwithstanding, U-turn movements can be undertaken within the ROW located on the east side of Napier Street midblock between the site access and Victoria Parade.

Attachment 9 - ACU Development Plan - Traffic Part 2



6.3 Car Parking - Clause 52.06

6.3.1 Car Parking Requirement

Table 1 to Clause 52.06 of the City of Yarra Planning Scheme sets out the car parking provision rates that apply to the Development Plan.

Specifically, Clause 52.06 requires that parking for the University is provided at a minimum rate of:

- > 0.4 spaces to each student that is part of the maximum number of students on site at any one time.

The student population on the campus is projected to increase by 700 students, in the period between 2015 – 2020.

Application of this rate to the proposed increase in students reveals a statutory requirement to provide a minimum 280 spaces.

6.3.2 Car Parking Provision

A total of 270 spaces are proposed as part of the 115B Victoria Parade Project, whilst the 28 - 42 Young Street project contemplates the demolition of the 250 space deck car park, with no additional parking provided on this site.

The building at 115B Victoria Parade will also be built on the existing at-grade car park site currently providing 50 spaces.

Accordingly the Development Plan results in a net decrease of 30 spaces, against a statutory requirements of 280 spaces.

This provision is considerably lower than the statutory requirements, and as such is in line with the sustainable transport objectives set out in local and state level policies.

6.4 Bicycle Parking – Clause 52.34

6.4.1 Bicycle Parking Requirement

Table 1 to Clause 52.34 of the City of Yarra Planning Scheme sets out the bicycle parking provision rates that apply to the Development Plan.

Specifically, Clause 52.34 requires that bicycle parking for the University is provided at a minimum rate of:

- > 1 space to each 20 employees; plus
- > 1 space to each 20 full time students.

The student population on the campus is projected to increase by 700 students, with a corresponding increase in staff numbers of 50 in the period between 2015 – 2020.

Application of these rates to the proposed increase in staff and students reveals a statutory requirement to provide a minimum 38 bicycle spaces comprising, 3 staff spaces and 35 student spaces.

6.4.2 Bicycle Parking Provision

It is intended to provide an additional 80 bicycle spaces. These spaces will be provided across the campus, not contained within the DPO area.

This provision is double the statutory requirements, and as such is in line with the sustainable transport objectives set out in local and state level policies.

Attachment 9 - ACU Development Plan - Traffic Part 2



7 Traffic Impact

7.1 Projected Traffic Volumes

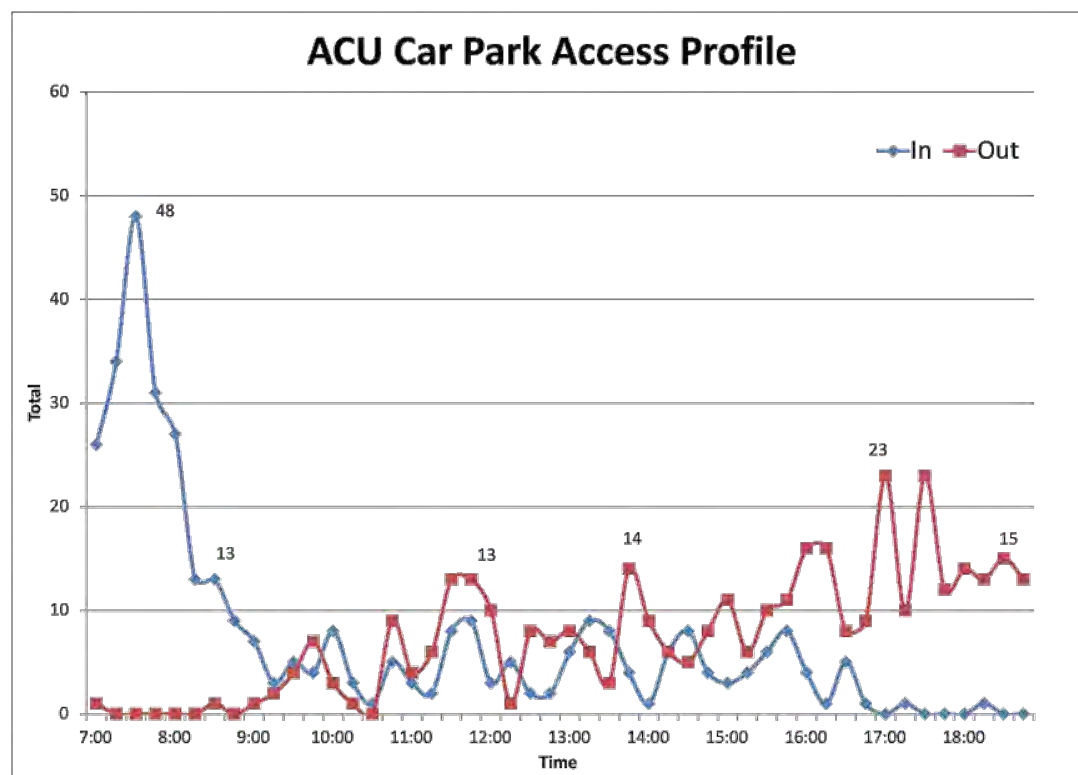
The existing ACU deck car park with 250 spaces used by both staff and students provides a suitable reference to inform consideration about projected traffic volumes at the proposed 270 space car park at the 115B Victoria Parade project.

The existing car park was surveyed on

- Wednesday 21st May 2014 between 7am to 7pm; and
- Wednesday 7th October 2015 between 7am to 7pm

Figure 7-1 and Figure 7-2 provide a summary of the traffic profile at the ACU car park access points.

Figure 7-1 ACU Car Parking Access Profile – Wednesday 21st May 2014



The key findings were as follows:

- > There were a total of 355 inbound movements and 360 outbound movements; and
- > The AM and PM peak hour was found to occur between 7:15am - 8:15am and 5:00pm-6:00pm respectively, when a total of 140 and 68 vehicle movements were recorded at the site access points respectively;

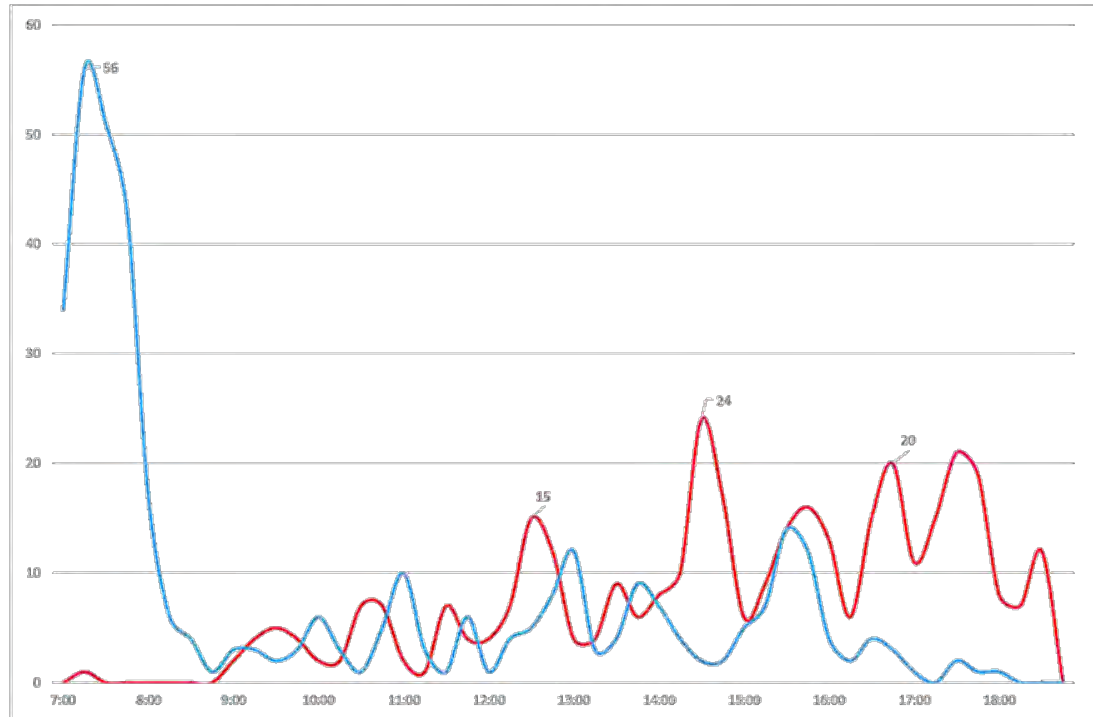
The above equates to about 60 % of the car parking spaces filling during the AM peak period, and about 30% of the spaces emptying during the PM peak. Traffic flows during the peak periods was observed as being heavily biased towards the peak direction.

Attachment 9 - ACU Development Plan - Traffic Part 2



Integrated Transport and Access Plan Australian Catholic University, St. Patricks Campus, Fitzroy

Figure 7-2 ACU Car Parking Access Profile – Wednesday 7th October 2015



The key findings were as follows:

- > There were a total of 375 inbound movements and 360 outbound movements; and
- > The AM and PM peak hour was found to occur between 7:00am - 8:00am and 3:15pm-4:15pm respectively, when a total of 185 and 45 vehicle movements were recorded at the site access points respectively.

The above equates to about 75 % of the car parking spaces filling during the AM peak period, and about 20% of the spaces emptying during the PM peak. Traffic flows during the peak periods was observed as being heavily biased towards the peak direction.

Conservatively adopting a rate of 75 % of the car spaces filling during the AM peak and 30% emptying during the PM peak, the proposed 270 space car park would be projected to generate:

- > 203 vehicle movements during the AM peak period; and
- > 81 vehicle movements during the PM peak period.

For the purpose of this assessment, traffic flows will be biased 95% in the peak direction and 5% in the non-peak direction.

Attachment 9 - ACU Development Plan - Traffic Part 2

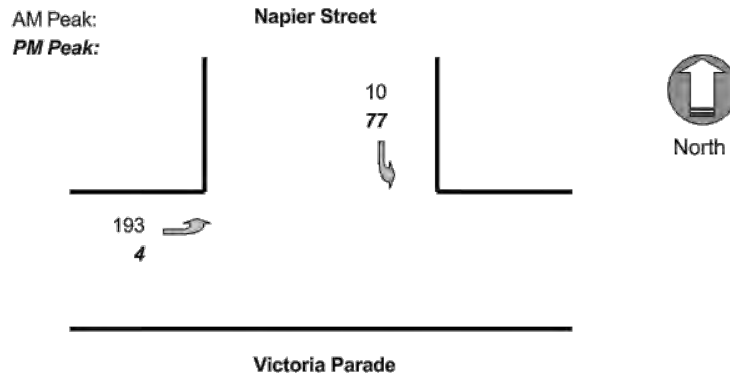


7.2 Traffic Distribution

Traffic to and from the proposed car park will be restricted to the Victoria Parade / Napier Street intersection.

This intersection will operate as a left in / left out, with all traffic arriving from the west and departing towards the east. Figure 7-3 illustrates the projected additional traffic flows at this intersection.

Figure 7-3 Projected Additional Traffic Flows – Victoria Parade / Napier Street



Based on historic traffic data collected on Napier Street in 2010, Napier Street at a location north of Little Victoria Street carries on average about 700 southbound vehicles per day, inclusive of about 100 vehicle movements during the AM peak period and 50 vehicle movements during the PM peak period.

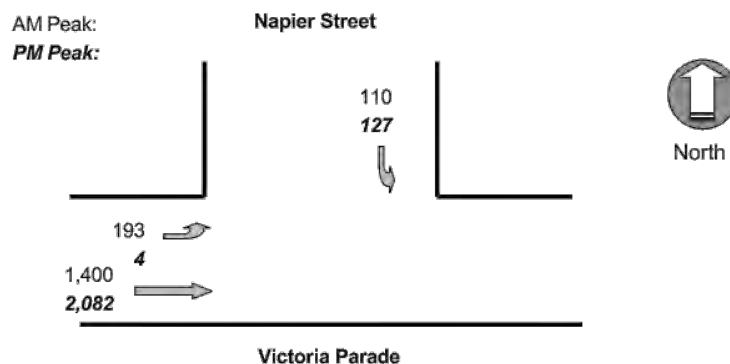
7.3 Post Development Traffic Flows

SCATS data sourced from VicRoads for Thursday 8th October 2015 at the intersection of Victoria Parade / Lansdowne Street, show that during the AM peak periods occurs between 11:00am – 12:00pm, when 1,400 vehicles are recorded past Napier Street. The PM peak period occurs between 5:00pm – 6:00pm, with 2,082 vehicles recorded past this intersection.

Noting that Napier Street is located a short distance downstream of the signalised intersection of Victoria Parade / Lansdowne Street, platooning of traffic during the peak periods has been observed which assists motorists seeking to depart from Napier Street.

Allowing for the estimated existing traffic flows on Napier Street and the recorded traffic flows along Victoria Parade, the anticipated post development traffic flows are illustrated as Figure 7-4.

Figure 7-4 Anticipated Post Development Traffic Flows – Victoria Parade / Napier Street



Attachment 9 - ACU Development Plan - Traffic Part 2



7.4 Intersection Analysis

The operation of the Victoria Parade / Napier Street intersection was analysed using SIDRA Intersection. This computer package, originally developed by the Australian Road Research Board, provides information about the capacity of an intersection in terms of a range of parameters, as described below:

Degree of Saturation (D.O.S.) is the ratio of the volume of traffic observed making a particular movement compared to the maximum capacity for that movement. Various values of degree of saturation and their rating are shown in Table 7-1.

Table 7-1 Rating of Degrees of Saturation

D.O.S.	Rating
Up to 0.6	Excellent
0.6 to 0.7	Very Good
0.7 to 0.8	Good
0.8 to 0.9	Fair
0.9 to 1.0	Poor
Above 1.0	Very Poor

It is considered acceptable for some critical movements in an intersection to operate in the range of 0.9 to 1.0 during the high peak periods, reflecting actual conditions in a significant proportion of suburban signalised intersections.

The **95th Percentile (95thile) Queue** represents the maximum queue length, in metres, that can be expected in 95% of observed queue lengths in the peak hour; and

Average Delay is the delay time, in seconds, which can be expected over all vehicles making a particular movement in the peak hour.

The results of the SIDRA Intersection analysis are summarised in Table 7-2.

Table 7-2 SIDRA Intersection Analysis Summary

	Approach	Degree of Saturation	95thile Queue	Average Delay
AM Peak	Napier Street (North)	0.174	5 metres	12 seconds
	Victoria Parade (West)	0.252	0 metres	1 second
PM Peak	Napier Street (North)	0.298	8 metres	16 seconds
	Victoria Parade (West)	0.375	0 metres	0 seconds

Based on the foregoing the intersection of Victoria Parade and Napier Street is expected to operate under excellent conditions, with motorists experiencing minimal queues and delays.

7.4.2 Operation of Site Access

An assessment of the operation of the site access has been undertaken to determine likely queues at the control point, especially during the AM peak period.

The analysis shows that during the AM peak period when 193 ingress vehicle movements are projected, 95th %ile queues of 4 vehicles are anticipated.

The boom gates are to be located at the bottom of the ramp, and will therefore afford motorists a queuing distance of about 24 metres, measured from the title boundary.

This setback can accommodate about 4 vehicles and as such queues on to Napier Street are not anticipated.

The design and location of the proposed car park access is therefore not expected to result in conditions that obstruct traffic flows along Napier Street or Victoria Parade.

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8 Green Travel Initiatives

8.1 Preamble

Green Travel initiatives are aimed at encouraging people to choose sustainable travel alternatives such as cycling, walking or public transport, over private car usage. It is the view of the State Government that tertiary institutions are typically considered positive environments for the application of travel behaviour change programs as many students and staff already support sustainable transport for environmental, financial and social reasons.

Accordingly, and in consultation with Council, the University could develop a Green Travel Plan that would seek to encourage staff and students to choose sustainable travel alternatives such as public transport, cycling and walking.

8.2 Objective

As a guide, the objective of this travel plan would be to increase the proportion of alternative modes of transport users to 90% within 5 years.

The co-ordination and implementation of the Green Travel Plan would be the responsibility of the University in consultation with Council to:

- > Increase awareness of sustainable transport options to new and ongoing staff and students at the Campus;
- > Encourage behavioural change in both new and on-going staff and students from single-occupant motor vehicle use, to alternative methods of transport i.e. trams, buses and bicycle/walking paths;
- > Implement travel/behaviour change strategies and use pre/post surveys to monitor changes in attitudes and reported behaviour;
- > Identify infrastructure changes / improvements to alternative travel mode infrastructure within the locality that will facilitate take-up of sustainable transport behaviours by the University community.

8.3 Action Plan (Guide Only)

Various strategies can be adopted to encourage the use of non-private motor vehicles, as described in action plans below.

8.3.1 General Actions

Action	Timeline	By whom	Approx cost
Strategic Marketing			
Promotion of Green travel initiatives during orientation and enrolment & re enrolment periods.	Ongoing - from time of occupation	University	
Toilet talk – a series of posters behind toilet doors where people can read them in private!			
An events calendar: 3-4 events per year. Best in conjunction with state wide events such as Ride to Work Day, World Environment Day, National Walk to Work Day. Plan for lunch, morning teas or breakfasts, guest speakers, demonstrations etc.			
Display boards in prominent locations to showcase green travel initiatives.			

8.3.2 Walking

Action	Timeline	By whom	Approx cost
Produce a map showing safe walking routes to and from the University with indicative walking times, not distances, to local facilities, such as shops and bus stops (people often have an unrealistic idea of how long it takes to walk).	On Occupation	University	
Provide umbrellas in staff rooms to encourage staff members to consider walking during adverse weather periods.			
Create a sustainable travel tab on the University web page with links to appropriate external and internal websites.			
Negotiate with council for improvements to footpaths in the vicinity of the site.	Ongoing	University & Council	

Attachment 9 - ACU Development Plan - Traffic Part 2



8.3.3 Cycling

Action	Timeline	By whom	Approx cost
Establish a University "Bicycle Users Group" (BUG). BUGs are formed by people who want to work together to improve facilities for cyclists and encourage cycling.	On Occupation	University	
Participate in annual events such as 'Ride to Work Day.'	Ongoing		
Provide sufficient bicycle parking and end of trip facilities (lockers / showers) to meet staff and student needs, which is easily accessible and secure.	On Occupation		
Ensure bicycle parking is clearly visible or provide signage to direct people to cycle bays.			
Produce a map showing quiet cycle routes to the University.			
Create a sustainable travel tab on the University web page with links to appropriate external and internal websites			
Improvements to Bicycle Infrastructure as identified in Yarra City Council's Draft Bicycle Strategy, such as modifications to the adjacent traffic signals along Victoria Parade to facilitate connections to the bicycle routes in the City of Melbourne.		Council	

8.3.4 Public Transport

Action	Timeline	By whom	Approx cost
Develop a map showing public transport routes to the University.	On occupation	University	
Put up a noticeboard with leaflets and maps showing the main public transport routes to and from the University.	On occupation		
Create a sustainable travel tab on the University web page with links to appropriate external and internal websites.	On occupation		
Encourage public transport use for business travel. This could be done by ensuring shared tickets are available at the University for work travel during the day.	Ongoing		
Participate in the Public Transport Victoria (PTV), 'Commuter Club' scheme, which allows employers to purchase discounted yearly MYKI cards on behalf of their employees.			

8.3.5 Car Parking / Car Pooling

Action	Timeline	By whom	Approx cost
Set up a car pooling database.	Within 12 Months of Occupation	University	
Allocate priority parking spaces for car poolers.			
Provide sustainable transport allowances for staff who surrender car parking permits.			
Review pricing policy for existing ACU off street car park.			

The above draft action plans are an outline of the actions and incentives that could be undertaken / provided and is subject to agreement between the University and Council.

8.4 Monitoring and Review

In order to monitor the success of the aforementioned Green Travel Initiatives a 3 stage monitoring system could be implemented by the University.

Elected University representatives, both staff and students would be assigned as Travel Co-ordinators of the Travel Plan. The co-ordinators would be responsible for:

- > Organising the monitoring of the performance of the Travel Plan against the targets that have been agreed.
- > Reviewing the occupancy / use (and abuse) of the facilities that are provided on site, for example, cycle racks, motorcycle and motorcar parking spaces, clothes lockers, and travel information etc.

Attachment 9 - ACU Development Plan - Traffic Part 2



- > Reviewing the operational success or failure of the plan and produce a review report highlighting successful initiatives and also areas that require improvement. This review report would be issued to Council on an annual basis and would form the platform of an updated Travel Plan, with sign off/ input from Council sought on a regular basis, the duration of which would be determined by the relevant stake holders.

The suggested 3 stage monitoring program is outlined below:

Stage 1

- > Stage 1, would involve a questionnaire survey of students and staff during the enrolment process. The survey will be useful to collect information on the travel characteristics of the staff and students and assist in gauging interest in the various Green Travel initiatives and to seek ideas for other Green Travel initiatives.

Stage 2

- > Stage 2, would involve a questionnaire and feedback form to be filled out by Students and Staff 3 months after implementation, in order to determine what Green Travel initiatives are working and which are not. A pro forma for the Stage 2 questionnaire is shown attached as Annex 1.

Stage 3

- > Stage 3, would be the monitoring component of the plan which would be undertaken 6 months after occupation. This questionnaire would test the success rate of the various initiatives and help rework programs to suit the needs of staff and students. A pro forma for the Stage 3 questionnaire is shown attached as Appendix 1.

It is expected that the above stages would form the basis of a continuous monitoring program to gauge the effectiveness of the travel plan.

Attachment 9 - ACU Development Plan - Traffic Part 2



9 Conclusions

- > The Development Plan has been developed to accommodate the ACU's projected growth in students, teaching staff and research from 2015 to 2025, whilst also allowing the consolidation of teaching and research activities onto the Campus.

Specifically it is anticipated that by 2020, ACU will increase their EFT student numbers to 10,700 and staff numbers to 850

- > The individual projects within the Development Plan will include:

115 B Victoria Parade

This project will comprise of the subdivision of 115 Victoria Parade to create a new building site. Within this site, a multi storey building is contemplated, and will include the provision of basement level car parking for 270 spaces.

Vehicular access to this building is planned via Napier Street. Mitigation works will be required at the intersection of Victoria Parade and Napier Street to accommodate two-way vehicle flow within the southern portion of Napier Street.

In principle approval has been secured from VicRoads and Council to the suite of mitigation works proposed.

28 - 42 Young Street (Existing Deck Car Park)

The existing 250 space deck car park is to be demolished and in its place a new building is proposed.

This new building will have no car parking spaces provided, instead, the development of this site will provide the opportunity to make improvements to pedestrian access and connectivity between the campus buildings, and the adjacent campus uses and activities.

- > The key principle that will inform the Development Plan from a transport perspective is based on the City of Yarra's commitment to reduce car dependency by promoting walking, cycling and public transport use.

Specifically, the St Patrick's Campus Development Plan seeks to become a pedestrian oriented space that is safe for students and prioritises sustainable modes of transport by:

- Improving access to public transport along Victoria Parade, Brunswick Street and Gertrude Street;
- Ensuring pedestrian links from the Campus integrate and form part of the wider pedestrian network;

The above will be achieved through the creation of strong pedestrian linkages through the Campus and to adjacent uses, activities and transport networks. Notably, the facade treatment to Victoria Parade on the Mary Glowrey Building is to be removed and by so doing, substantially increasing the footpath width along Victoria Parade in the section between Young Street and Napier Street. This will be particularly beneficial at the interface of the Mary Glowrey Building and the existing bus stop.

- > A total of 270 spaces are proposed as part of the 115B Victoria Parade Project and results in the loss of 50 existing at-grade spaces, whilst the 28 - 42 Young Street project contemplates the demolition of the 250 space deck car park, with no additional parking provided on this site. Accordingly the Development Plan seeks to provide a net decrease of 30 spaces, against a statutory requirements of 280 spaces.

- > The proposed 115B Victoria Parade Car Park will be operated and managed as follows:

- The parking area will be secured by boom gates, activated by card readers and ticket machine on entry and departure during the facility's operating hours, anticipated to be between 7:00am – 10:30pm Weekdays and 8:00am – 10:30pm weekends consistent with current operational practises for the existing car park. During afterhours the car park will be secured by roller doors.
- To alert casual parkers entering the car park and ensure ease of use, clearly visible advisory signs will be posted on the car park entry and also within the car park. Additionally to avoid conflict between long term (staff) and short term (students) users, spaces will be designated to individual staff members once they are determined, with signage posted at the end of each bay.
- Operation of the public car park will be limited to the hours listed above, with its fee structure expected to be based on an hourly charge rate in-line with other commercial car park facilities in the vicinity of the site. The actual charge rates will however be determined once an operator is appointed. The car park is to be operated by ticket collection on entry, and then payment at a pay station likely to be located adjacent to the lifts.

Attachment 9 - ACU Development Plan - Traffic Part 2



- Dynamic signage on Victoria Parade will indicate if the car park is full, which will reduce the number of vehicles turning into Napier Street. Notwithstanding, U-turn movements can be undertaken within the ROW located on the east side of Napier Street midblock between the site access and Victoria Parade.
- > The proposal triggers a statutory requirement to provide 38 bicycle spaces. A total of 80 bicycle parking spaces are proposed. This provision is double the statutory requirements, and as such is in line with the sustainable transport objectives set out in local and state level policies.
- > An assessment of the post development intersection operating conditions shows that with the mitigation works proposed, the Victoria Parade and Napier Street intersection will to operate under excellent conditions, with motorists experiencing limited queues and delays.

 Additionally, an assessment of the operation of the site access has revealed 95th %ile queues of 4 vehicles are anticipated during the critical AM peak period. The design of the car park access and proposed location of the control points is such that motorists will be afforded a queuing distance of about 24 metres, measured from the title boundary.

 This setback can accommodate about 4 vehicles and as such queues on to Napier Street are not anticipated. The design and location of the proposed car park access is therefore not expected to result in conditions that obstruct traffic flows along Napier Street or Victoria Parade.
- > ACU will also actively pursue Green Travel initiatives aimed at encouraging people to choose sustainable travel alternatives such as cycling, walking or public transport, over private car usage.

Attachment 9 - ACU Development Plan - Traffic Part 2



Integrated Transport and Access Plan Australian Catholic University, St. Patricks Campus, Fitzroy

Australian Catholic University,
St. Patricks Campus, Fitzroy

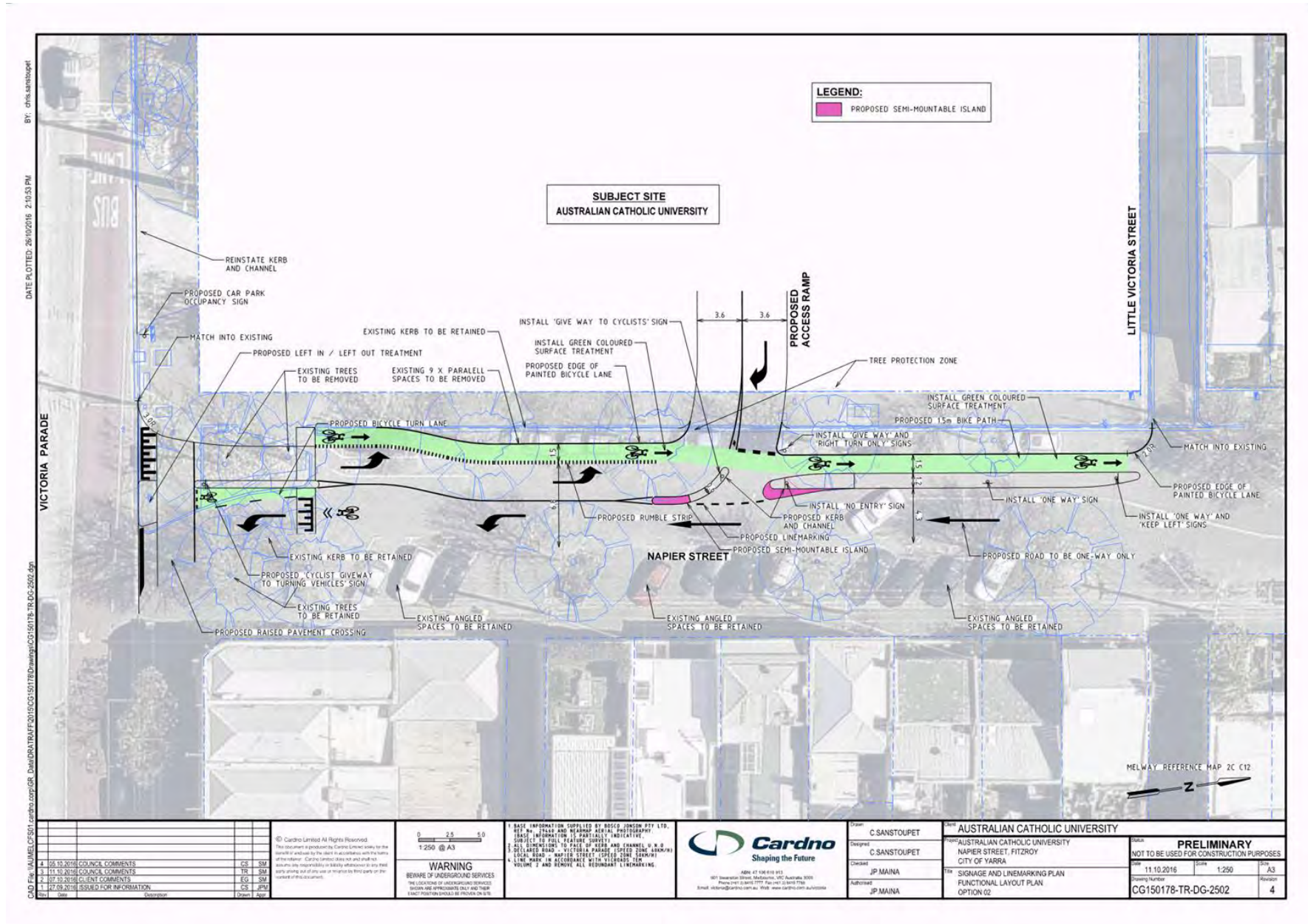
APPENDIX

A

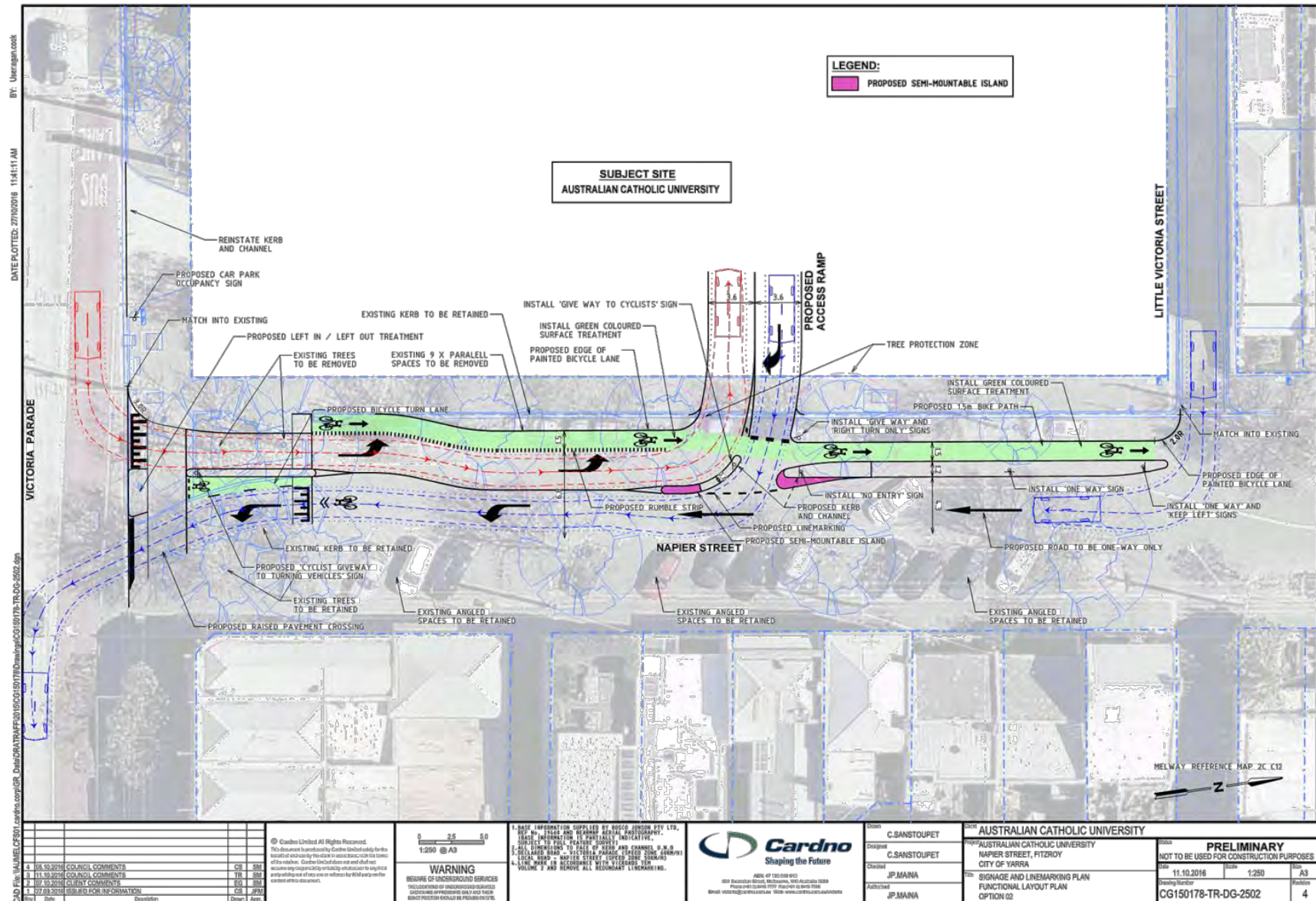
CONCEPT
FUNCTIONAL
LAYOUT PLANS



Attachment 9 - ACU Development Plan - Traffic Part 2



Attachment 9 - ACU Development Plan - Traffic Part 2



Attachment 9 - ACU Development Plan - Traffic Part 2



Integrated Transport and Access Plan Australian Catholic University, St. Patricks Campus, Fitzroy

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St. Patricks Campus, Fitzroy

APPENDIX

B

BICYCLE PARKING
PRODUCTS



Attachment 9 - ACU Development Plan - Traffic Part 2



Anaconda™



Galvanised finish

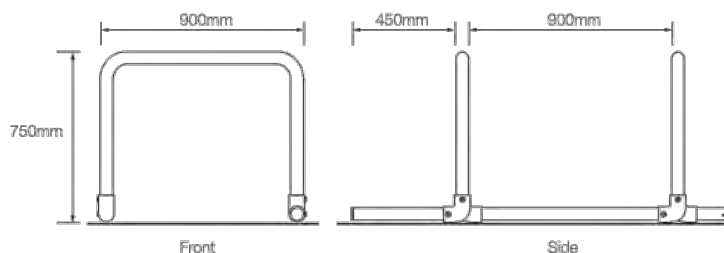
Features



Anaconda rails are a freestanding version of the Flat Top. These are an excellent solution for use on asphalt or for temporary event parking. Available in a variety of materials and finishes in both adult and junior sizes.

- Easy to use with any bike lock
- Freestanding, self-supporting and can be fastened to all surfaces
- Using clamp-on elbows to join the rails
- Powder coated, galvanised, alloy or stainless steel rail tubing available
- Parks two bicycles per rail in either parallel or diagonal set up
- Supports all styles of bicycles in an upright position

Dimensions



Specifications

Material options

- 316 Marine grade stainless steel
- Galvanised
- Powder coated
- Alloy

Fixing options

- Clamp on

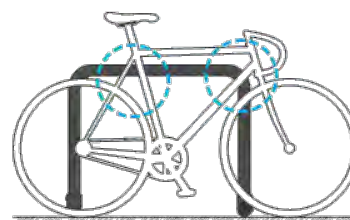
Recommended fasteners

- Zinc plated Dynabolts (M10 x 65mm)
- Stainless Dynabolts (M10 x 65mm)

Dimensions

900mm [w] x 750mm [h]

Locking points



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Design. Supply. Install.

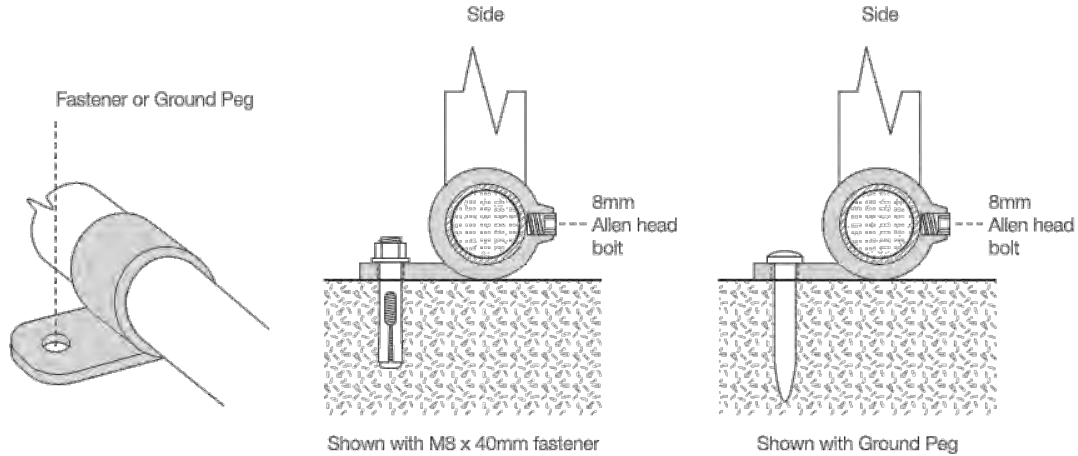
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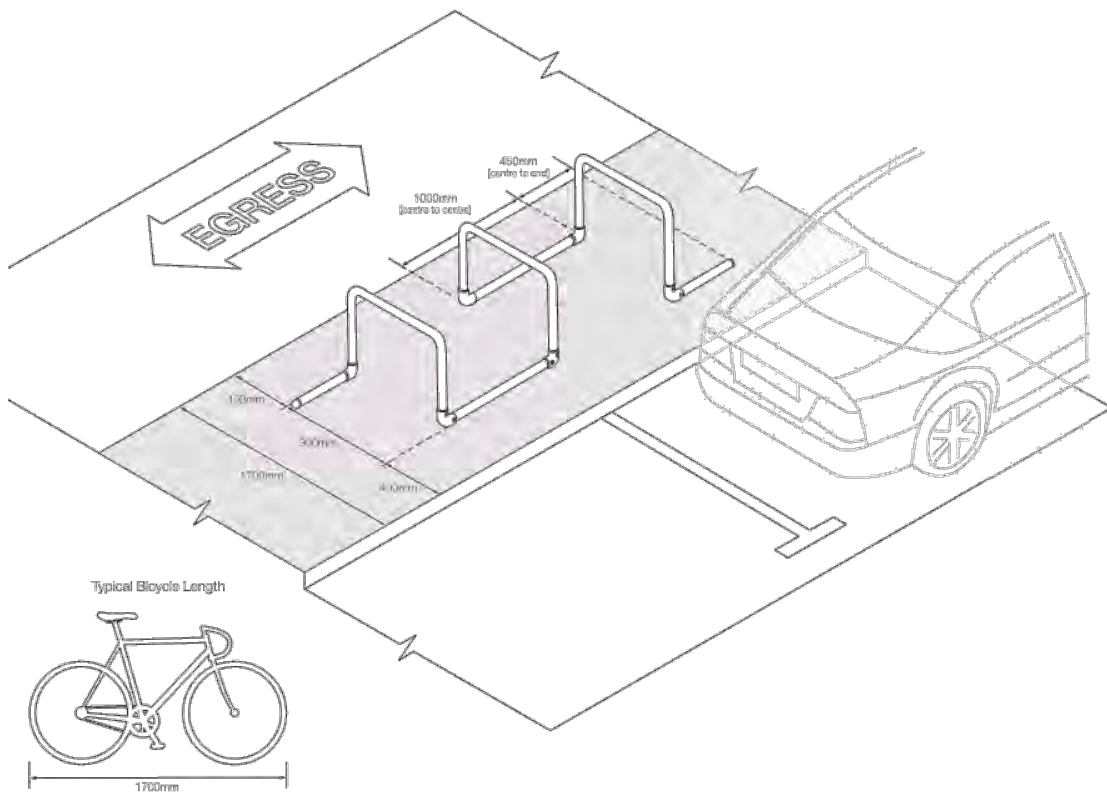
Attachment 9 - ACU Development Plan - Traffic Part 2

Fixing options

Fix to the ground with Ground Tabs fitted over the piping at each end and secured with 8mm Allen bolts. Use either a Fastener or Ground Peg to secure the Anaconda to the ground.



Layout guidelines



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Arc de Triomphe™



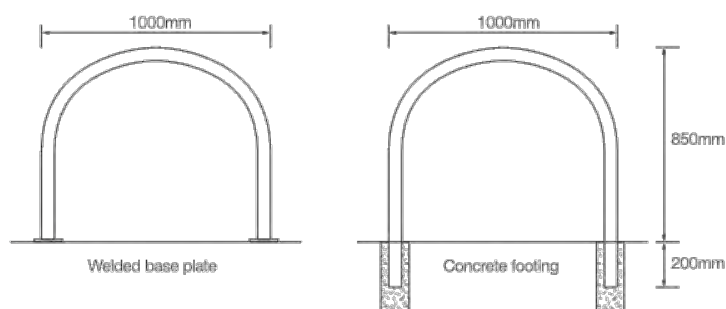
Stainless steel finish

Features



- Each rail supports two adult bikes in an upright position
- Can be either bolted to a concrete slab or concreted in situ
- Available in stainless steel or galvanised steel
- Provides the ability to lock both wheels and frame
- Suitable for foyers and entry areas

Dimensions



Specifications

Material options

- 316 Marine grade stainless steel
- Galvanised

Fixing options

- Welded flange
- In situ

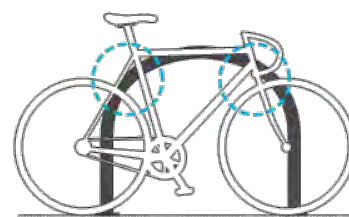
Recommended fasteners

- Galvanised Dynabolts (M10 x 65mm)
- Stainless Dynabolts (M10 x 65mm)
- Shear Nut security fasteners

Dimensions

1000mm [w] x 850mm [h]

Locking points



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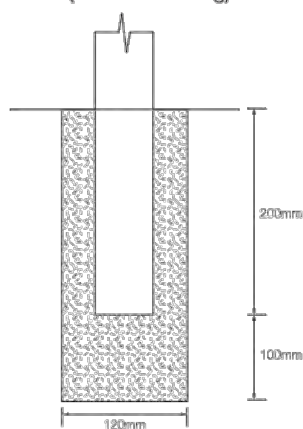
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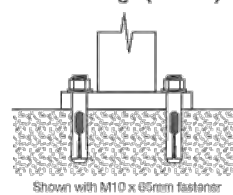


Fixing options

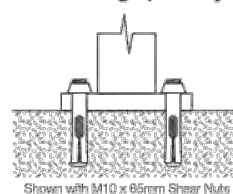
In situ (Concrete footing)



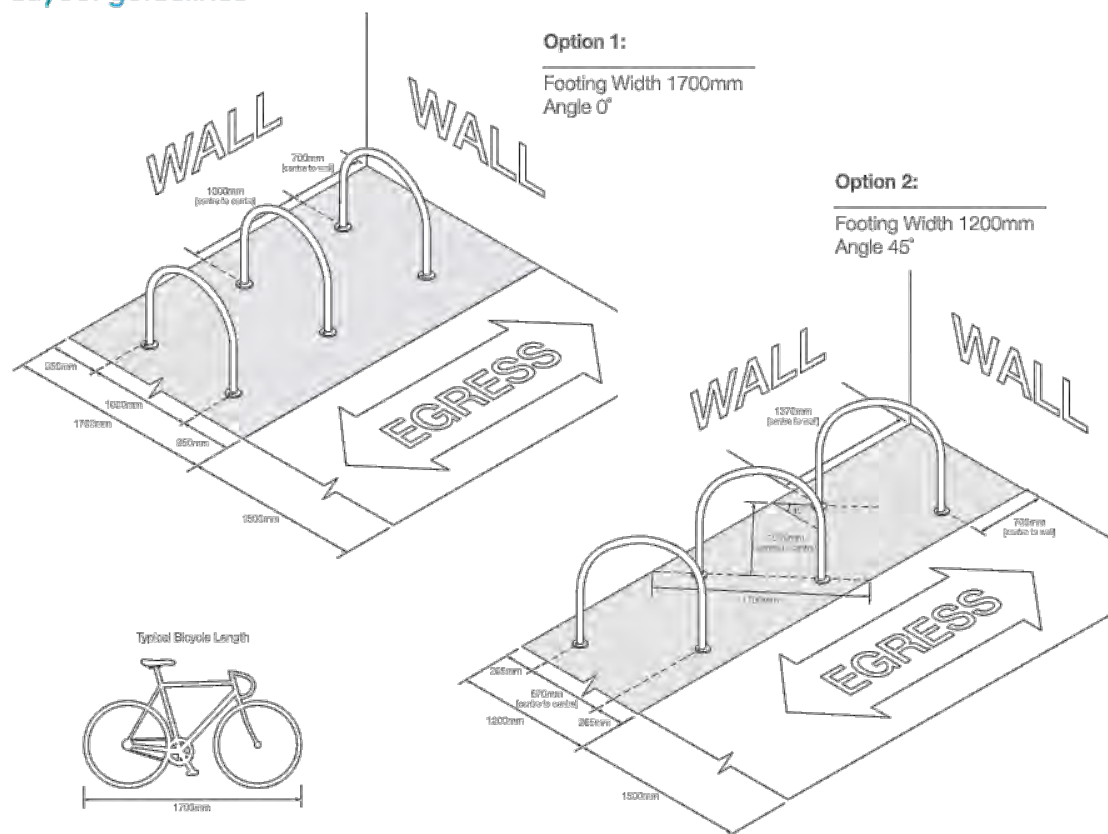
Welded flange (Bolt on)



Welded flange (Security heads)



Layout guidelines



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Attachment 9 - ACU Development Plan - Traffic Part 2



Flat Top™



Stainless steel finish

Features



- Each rail supports two adult bikes in an upright position
- Can be either bolted to a concrete slab or concreted in situ
- Available in stainless steel or galvanised steel
- Provides the ability to lock both wheels and frame
- Suitable for interior use including storage cages

Specifications

Material options

- 316 Marine grade stainless steel
- Galvanised

Fixing options

- Welded flange
- In situ

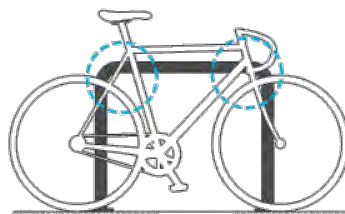
Recommended fasteners

- Galvanised Dynabolts (M10 x 65mm)
- Stainless Dynabolts (M10 x 65mm)
- Shear Nut security fasteners

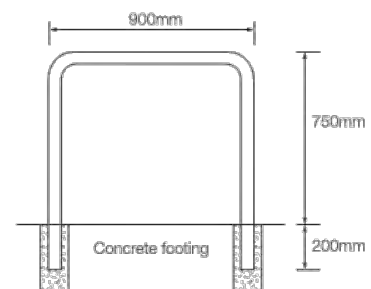
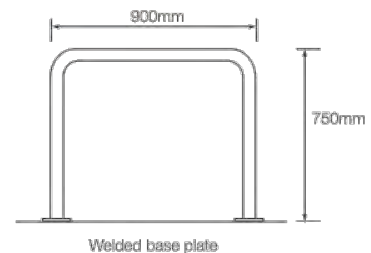
Dimensions

950mm [w] x 750mm [h]

Locking points



Dimensions



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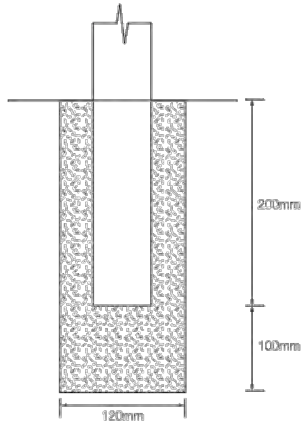
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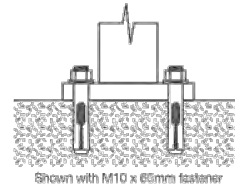
Attachment 9 - ACU Development Plan - Traffic Part 2

Fixing options

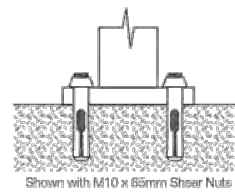
In situ (Concrete footing)



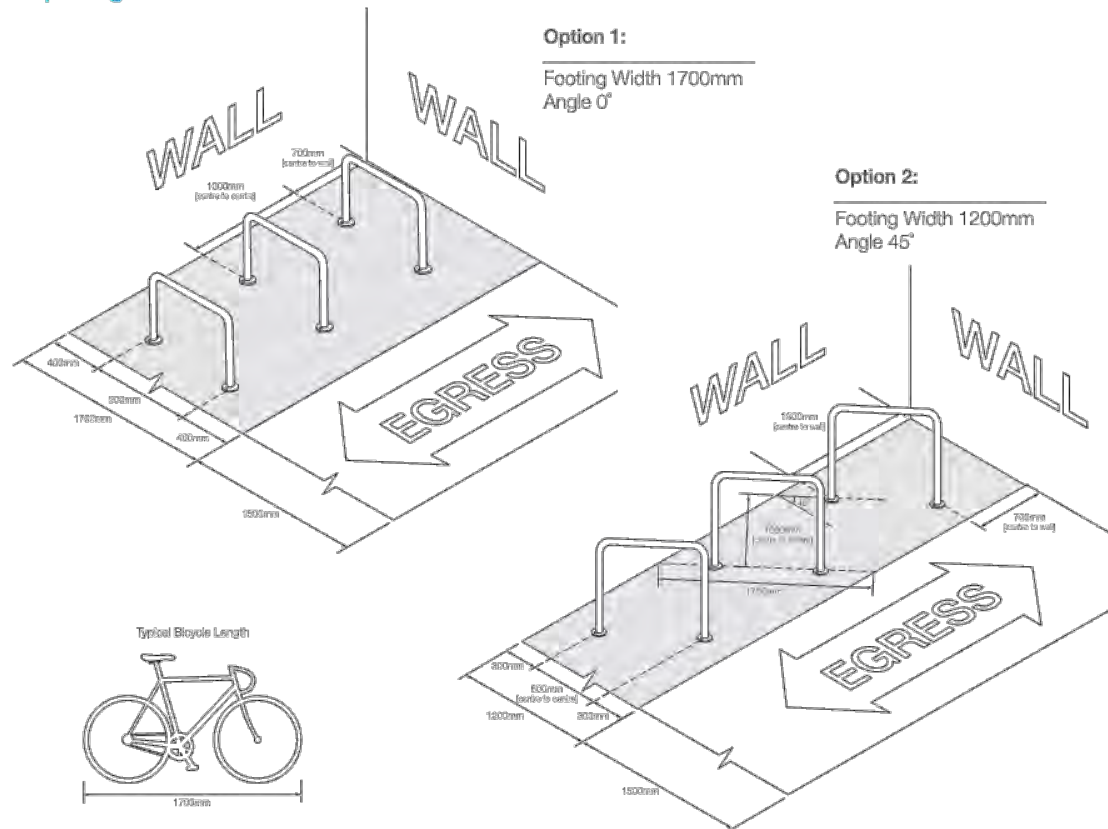
Welded flange (Bolt on)



Welded flange (Security heads)



Layout guidelines



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Attachment 9 - ACU Development Plan - Traffic Part 2



Ned Kelly[®]™



Galvanised finish



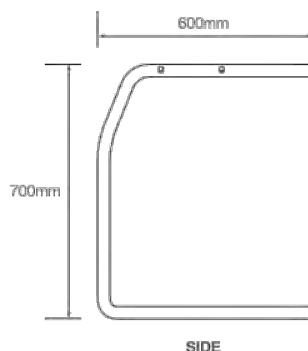
Black powder coat finish

Features



- Each rail provides storage for a single bike
- Suits bikes with full length mud guards
- Available in galvanised or powder coat over mild steel
- Provides the ability to lock the main frame and one wheel
- Support prongs with protective coating prevent damage to rim
- Can be used with custom framing – no wall needed

Dimensions



SIDE



FRONT

Specifications

Material options

- Galvanised
- Powder coat over mild steel
- Stainless steel*

Fixing options

- Bolt on to wall
- Fixed to support framing

Recommended fasteners – wall

- Dynabolts (M8 x 40mm)
- Shear Nut security fasteners

Recommended fasteners – framing

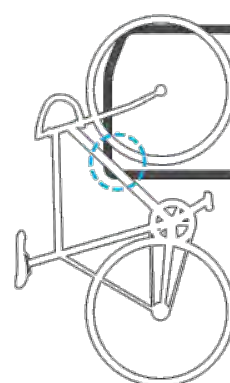
- Bolt and nut (M10 x 60mm)
- Tek screws

Dimensions

125mm [w] x 700mm [h] x 600mm [d]

* Pre-order only

Locking points



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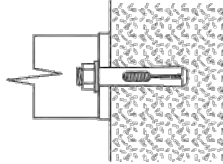
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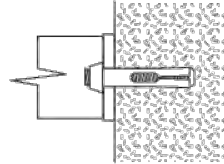
Attachment 9 - ACU Development Plan - Traffic Part 2

Fixing options

Fix to a wall using 4x fasteners or Shear Nuts

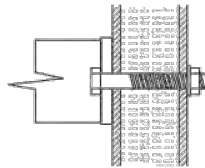


Shown with M8 x 40mm fastener

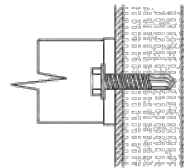


Shown with M8 x 40mm Shear Nuts

Fix to a frame using 4x bolts or Tek Screws

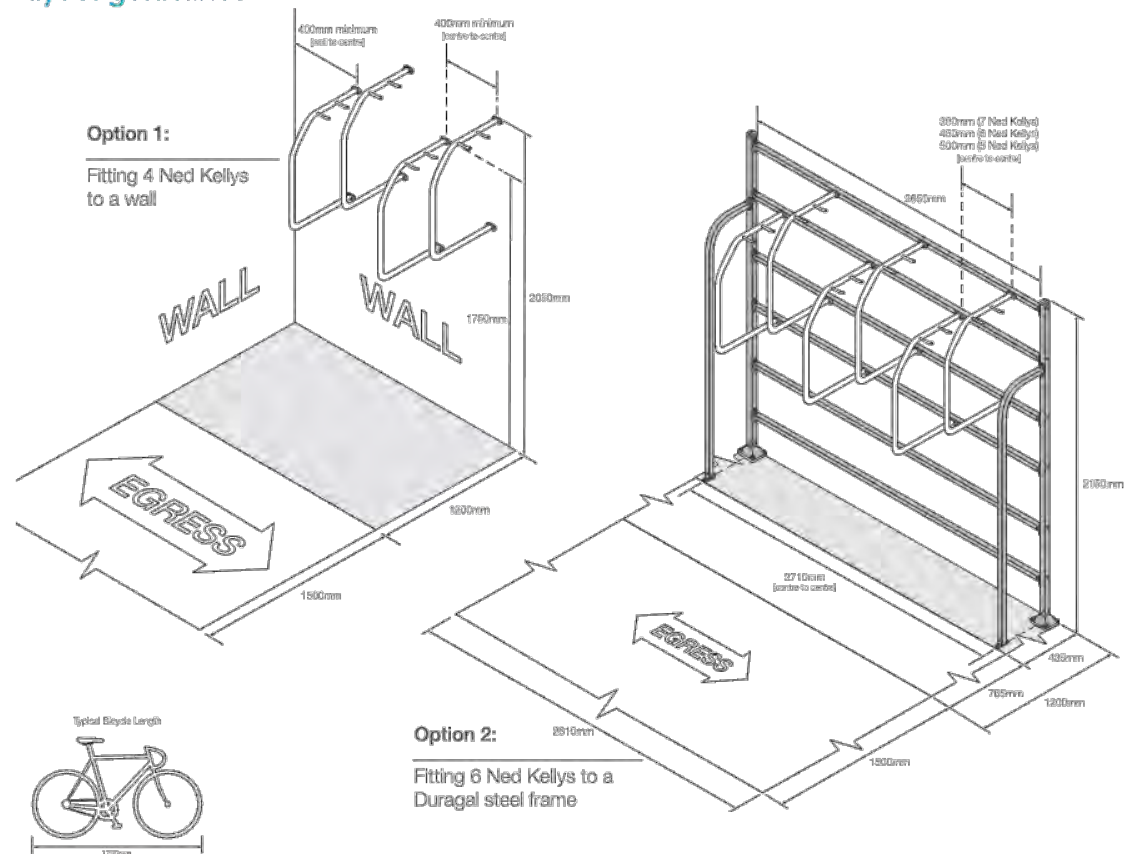


Shown with M10 x 60mm Bolt, Washer & Nut



Shown with Tek Screw

Layout guidelines



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Attachment 9 - ACU Development Plan - Traffic Part 2



Pole Vault™



Stainless finish

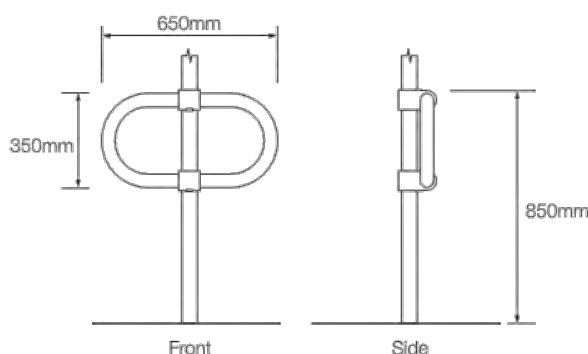
Features



The Pole Vault is an innovative street sign mounted bike parking rail that can be fitted in minutes with no drilling or concrete required.

- Can be retro-fitted to existing sign poles
- Accommodates a wide range of sizes and styles of bikes
- Can be moved and re-fitted to different locations
- Comes in standard galvanised, powdercoat or stainless material
- Easy to use with any bike lock
- Supports the entire bike so it won't slip or fall over

Dimensions



Specifications

Material options

- 316 Marine grade stainless steel
- Galvanised

Fixing options

- Clamp on

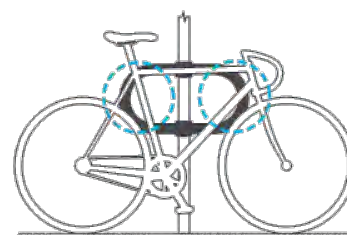
Recommended fasteners

- 8mm Allen head bolts

Dimensions

650mm [w] x 350mm [h]

Locking points



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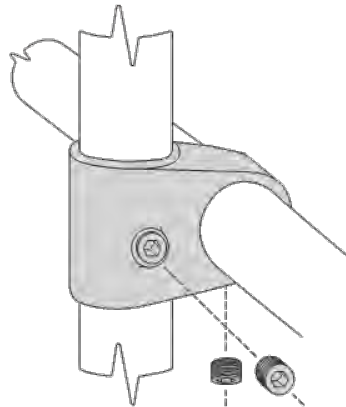


Attachment 10 - ACU Development Plan - Traffic Part 3

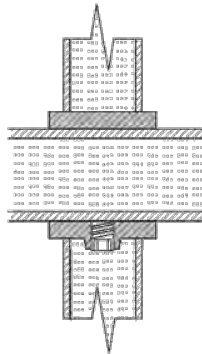


Bike Parking Experts | Pole Vault™

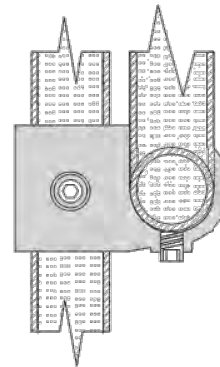
Fixing options



2 x 8mm Allen head bolts per clamp

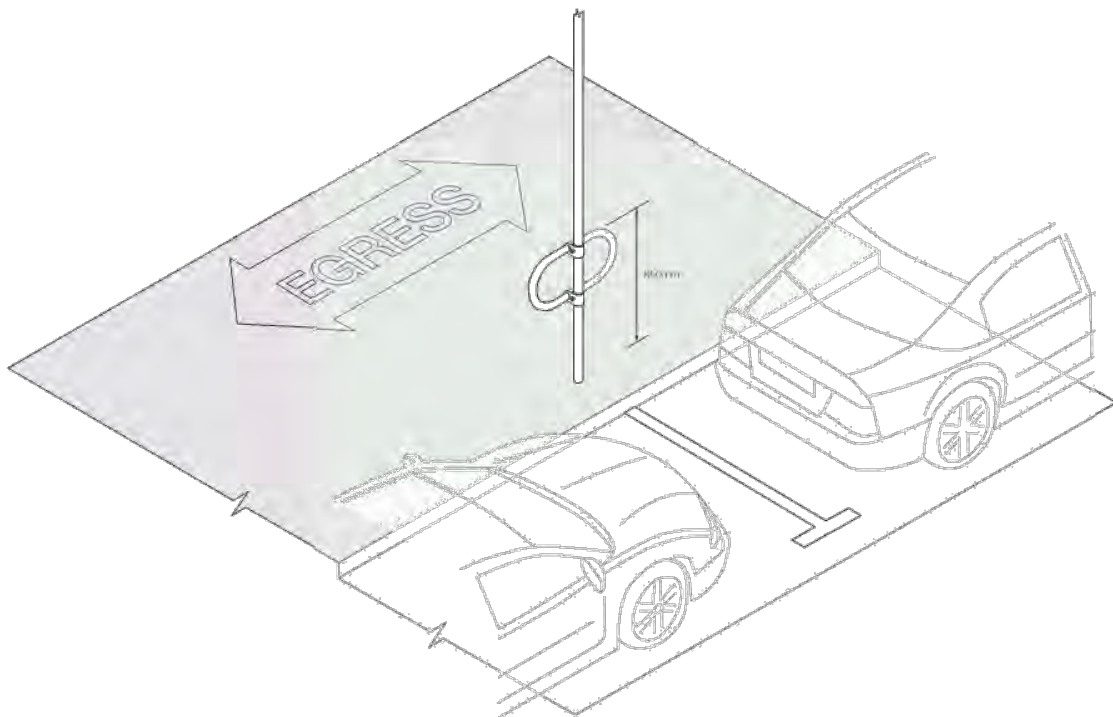


Front



Side

Layout guidelines



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Attachment 10 - ACU Development Plan - Traffic Part 3



Towel Hitching™



Zinc treated finish

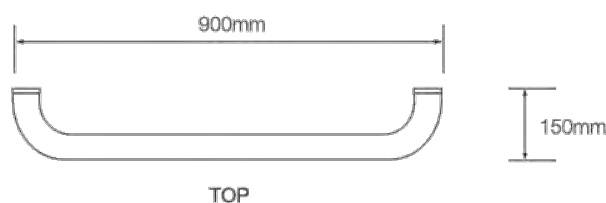
Features



The Towel Rail is a space-efficient solution for parking single bikes against a wall, useful in narrow corridors.

- Space-efficient, can be fitted to narrow corridors
- Can be supplied in powdercoat or galvanised finish
- Accommodates all types and sizes of bicycle
- Is easy to use with any bike lock
- Requires no lifting

Dimensions



Specifications

Material options

- Powder coated
- Stainless steel
- Zinc treated
- Galvanised

Fixing options

- Welded flange - Bolt On

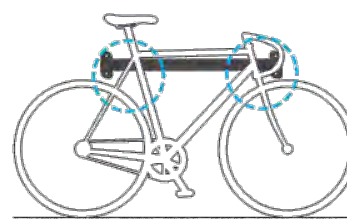
Recommended fasteners

- Zinc plated dynabolts (M8 x 40mm)

Dimensions

900mm [w] x 150mm [d]

Locking points



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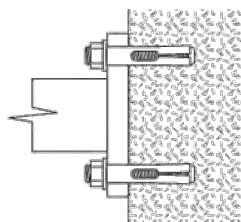
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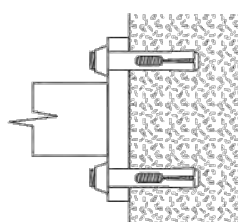
Attachment 10 - ACU Development Plan - Traffic Part 3

Fixing options

Fix to a wall using 4x fasteners or Shear Nuts

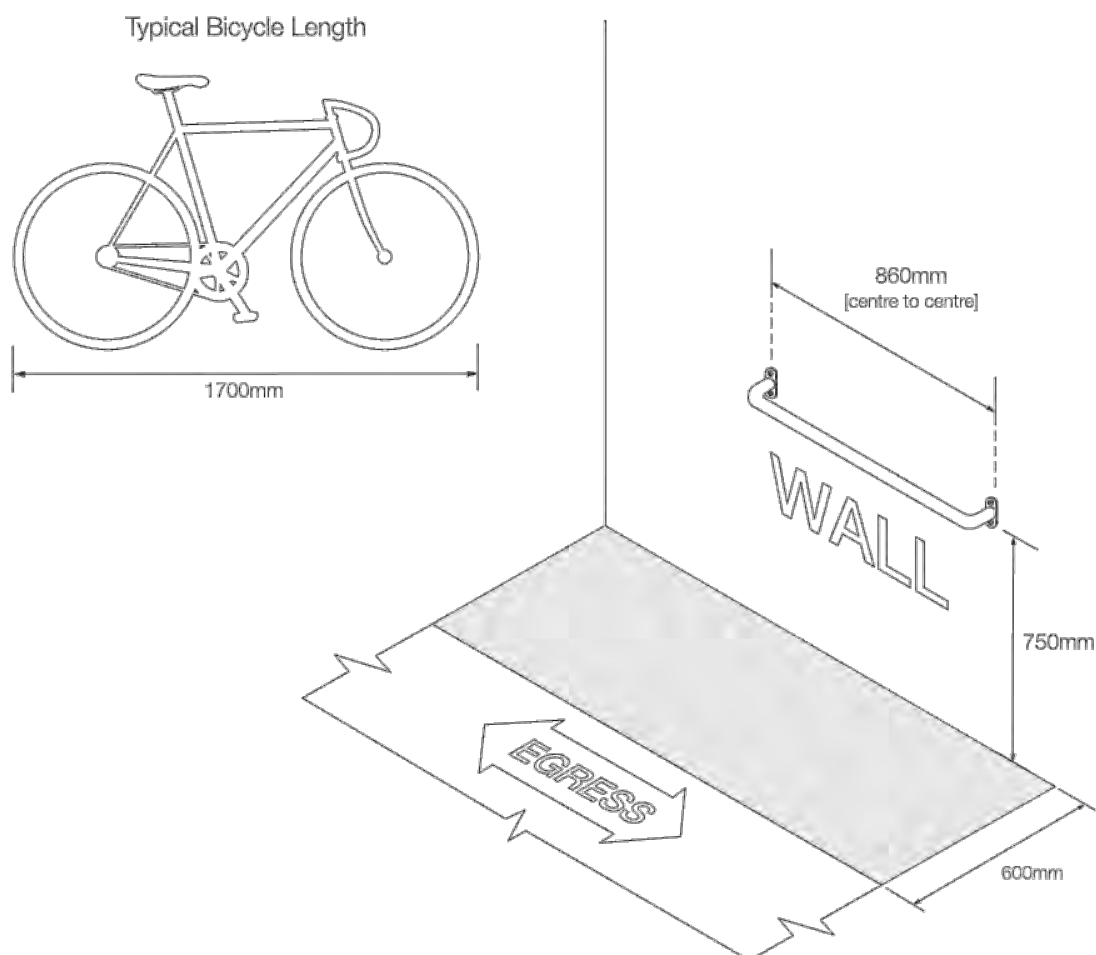


Shown with M8 x 40mm fastener



Shown with M8 x 40mm Shear Nuts

Layout guidelines



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Attachment 11 - ACU Development Plan - Wind

AECOM

Australian Catholic University Melbourne Campus Development
Australian Catholic University
19-Oct-2016

Commercial-in-Confidence

Wind Microclimate Assessment

Pedestrian comfort at 115B Victoria Parade



Attachment 11 - ACU Development Plan - Wind

AECOM

Australian Catholic University Melbourne Campus Development
Wind Microclimate Assessment
Commercial-In-Confidence

Wind Microclimate Assessment

Pedestrian comfort at 115B Victoria Parade

Client: Australian Catholic University

ABN: 15050192860

Prepared by

AECOM Australia Pty Ltd
Level 10, Tower Two, 727 Collins Street, Melbourne VIC 3008, Australia
T +61 3 9653 1234 F +61 3 9654 7117 www.aecom.com
ABN 20 093 846 923

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19-Oct-2016
Prepared for – Australian Catholic University – ABN: 15050192860

Quality Information

Document Wind Microclimate Assessment
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Ref \\aume11p001\projects\605x\60519200\4. tech work area\4.3
esd\01_assessments\wind microclimate\161019_acu_wind microclimate
assessment_final.docx
Date 19-Oct-2016
Prepared by Nicki Parker
Reviewed by Sian Willmott

Revision History

Revision	Revision Date	Details	Authorised	
			Name/Position	Signature
0	19-Oct-2016	Issued for inclusion to Development Plan	Russell Evans Technical Director	

Attachment 11 - ACU Development Plan - Wind

AECOM

Australian Catholic University Melbourne Campus Development
Wind Microclimate Assessment
Commercial-in-Confidence

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Attachment 11 - ACU Development Plan - Wind

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I

Executive Summary

This report assesses the likelihood that wind-induced discomfort may be felt by patrons of various outdoor spaces around the proposed 115B Victoria Parade development for the Australian Catholic University. The assessment draws on a large local meteorological data set adjusted for location and height.

Winds are assessed against the Lawson comfort criteria, which sets upper wind speeds for comfort during a range of typical activities — long-term sedentary (e.g. sitting at café tables), short-term sedentary (e.g. queuing, talking), slow movement (e.g. window shopping, strolling), and rapid movement (e.g. fast walking).

Based on hourly wind speed and direction data collected from the Melbourne Regional Office (located approximately 1km to the west of the site), predominant winds are west south westerly and north easterly throughout the year and during business hours, as well as outside of business hours. These two key wind directions have therefore been the focus of the analysis provided for this submission.

Seven locations within and around the site have been identified as being sensitive to wind speed. These are summarised in the table below, along with the desired wind comfort and activity category. Potential issues have been highlighted.

Table 1: Wind assessment summary and impacts

Location		Desired wind comfort and activity category	Potential wind microclimate impacts
1	Ground level outdoor seating around within the site boundary	Comfortable for pedestrians sitting or standing for longer periods of time	Channelling of winds along Victoria Parade – some local mitigation may be required.
2	Along Napier Street	Comfortable for pedestrians leisure walking	Unlikely to be significant impacts caused by the proposed building.
3	Along Victoria Parade	Comfortable for pedestrians leisure walking	Unlikely to be significant impacts caused by the proposed building.
4	Level 4 Terrace	Comfortable for pedestrians sitting or standing for shorter periods of time	Exposed to predominant winds – balustrades likely to provide sufficient protection.
5	Level 6 Terrace	Comfortable for pedestrians sitting or standing for shorter periods of time	Exposed to predominant winds – balustrades likely to provide some protection. Additional local mitigation may be required.
6	Along Young Street	Comfortable for pedestrians leisure walking	Unlikely to be significant impacts caused by the proposed building.
7	Along Little Victoria Street	Comfortable for pedestrians leisure walking	Unlikely to be significant impacts caused by the proposed building.

Due to the relatively low wind speeds experienced in this area of Melbourne, there are unlikely to be any significant adverse impacts at Ground Level within or surrounding the site due to the construction of the proposed 115B Victoria Parade development. The staggered, non-uniform shape of the building envelope is likely to minimise wind acceleration, with horizontal shading further assisting in minimising potential issues.

A detailed computational analysis will be undertaken during the next design stage which will quantify expected wind speeds and compare these against the Lawson comfort criteria.

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1.0 Introduction

It is somewhat inevitable that, with the construction of a new development, the wind microclimate in the vicinity will be changed. Where new buildings are significantly different in size and form, orientation or height from those in the immediate vicinity, winds can be introduced which may cause discomfort to pedestrians. The design of a development should therefore consider the provision of a quality outdoor environment, which is appropriate for its designated use for the majority of the year. Due to the height and layout of the proposed building at 115B Victoria Parade, a number of potentially adverse wind effects may be experienced.

This study is an experience based qualitative review of the pedestrian level wind environment around the proposed 115B Victoria Parade development. The assessment of wind conditions is based upon our experience with other similar schemes and our knowledge of the interaction of the wind with the built environment. The wind conditions around the proposed development in relation to planned pedestrian activities have been considered and an assessment of the potential impact that the proposed development may have on the surrounding area's wind microclimate is provided. Areas where wind is likely to be accelerated by geometrical features are highlighted and ways to mitigate these effects are recommended.

Local authority planning guidelines typically focus on user comfort and safety. Here, users are usually taken to be at ground level (or the main podium level etc.) of a particular space. They may be passing through on foot or bicycle; wandering slowly (e.g. talking); sitting or standing for a short period (e.g. at a bus stop) or for a long period (e.g. outdoor dining). Planning criteria focus on the frequency of high winds, which are known to be uncomfortable in various situations and for various durations of exposure.

Upper-level users of balconies etc. may also be affected by winds, but exposure is generally considered to be by choice and easily avoidable. In rare cases, outdoor dining or other regular outdoor activity may take place relatively high above ground level, in which case special assessments must be made.

As this is a desk based study, quantification of likely increases or decreases in wind speed cannot be given and only an indication of likely conditions that pedestrians will experience is presented here.

1.1 Purpose, basis and limitations of this report

The purpose of this report is to provide evidence as to the likelihood of wind-induced discomfort to ground-level users of the spaces around the building.

This report presents probabilistic estimates of the likelihood of events which may have comfort implications. These are based on historical wind data and measured against commonly available wind effects criteria using accepted estimated methods. Given these limitations, AECOM cannot guarantee with certainty that the development will not adversely impact upon safety and comfort in the public realm.

This report is based on drawings and other information supplied; a statistical analysis of data; published methodologies for wind assessment; and experience with assessing wind flows around buildings. The statistical analysis does not include an allowance for rare high-wind events such as severe storms. Also note that future wind patterns may not reflect past wind patterns. For example, changes in wind climate due to global warming are not accounted for. This report does not address structural aspects of wind phenomena.

All advice is provided with best intent and to the accuracy limits of the nature of the assessment undertaken.

2.0 Assessment criteria

2.1 Basis

Wind speed and gustiness are the primary measurable factors affecting people's comfort. Other factors such as air temperature and humidity, clothing, sun exposure, etc. are also significant, but these can often be addressed by a modification of effective wind speeds (Twidell, 2006).

Wind speed is understood to mean the average wind speed taken over a time of one hour or so. Gustiness refers to the rate of change of wind speed, usually identified with the turbulent intensity defined by ratio of the standard deviation of the mean wind speed to the mean itself. The important wind gusts are those lasting 2–3 seconds, being the time taken to perform a simple act such as a few walking steps, opening a door etc.

Gustiness is a difficult factor to assess on the urban micro-scale. Fortunately, the implied turbulent intensity may be related to the underlying means in order to recast gustiness criteria in terms of mean wind speed (Twidell, 2006), (Melbourne, 1978), (ASHRAE, 2001), (Blocken, 2004). Estimates of turbulent intensity in urban situations range from 15% to 30% (Twidell, 2006), implying that gust wind speeds are generally 1.5–2.0 times greater than mean wind speeds.

2.2 Comfort

In general, comfort criteria relate to both the thermal effects of wind on people, and the mechanical effects of wind on their activities.

The comfort criteria used in this study is the Lawson criteria (Lawson, 1978), based on the probability of exceeding certain mean wind speeds. The criteria are presented in Table 2. Wind conditions are unacceptable when the probability of the mean wind speed exceeding the given number is greater than 5%.

Table 2: The Lawson wind comfort criteria

Threshold wind speed (m/s)	Activity
4	Uncomfortable for pedestrians in the vicinity of entrance doors or sitting outside for long periods of time, such as outdoor cafes.
6	Uncomfortable for pedestrians standing or sitting for shorter periods of time, such as queuing or talking.
8	Uncomfortable for pedestrians 'leisure walking' e.g. strolling, window shopping and sightseeing.
10	Uncomfortable for pedestrians walking quickly e.g. walking to a destination, and cycling.

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3.0 The local wind climate

3.1 Meteorological data

The wind data was taken from the Bureau of Meteorology automatic weather station at Melbourne Regional Office (RO)¹, which is located on the corner of La Trobe Street and Victoria Parade, approximately 1km to the west of 115B Victoria Parade.

The wind speed data was rescaled to account for the difference in land surface structure between the meteorological station and the development site, and the height difference between the anemometer and the level at which people are affected (assumed to be 1.5m above ground level). The rescaling was accomplished using a logarithmic-law approximation to a neutrally stable atmospheric boundary layer profile (Pasquill-Gifford Class D) (Oke, 2006) using the equation:

$$u_z = \frac{u_*}{\kappa} \ln \frac{z}{z_0}$$

In which u_z is the wind speed at height z (1.5m for pedestrian height), u_* is the friction velocity which is based on the reference wind speed from Melbourne RO, κ is von Karman's constant (≈ 0.4) and z_0 is the roughness height (taken as 2m for this site to account for physical obstructions such as cars).

Wind speeds below 0.5 m/s are registered by the anemometer as zero (calm).

3.2 Summary statistics

The wind data was analysed to assess the likelihood of uncomfortable winds, without allowing for the presence of the development. Local wind effects due to the development will be discussed in the next section.

The analysis was carried out using:

- ☐ The entire data set, representing wind conditions 24 hours a day
- ☐ A subset restricted to the hours of 7am to 7pm (business hours) when outdoor areas would be most active

3.2.1 Wind speed

Calm conditions occur only rarely (5%) during business hours, slightly more frequently (8%) during winter. Wind speeds at pedestrian height are low compared to the comfort criteria (Figure 1 on the following page). Based on wind speeds experienced at the Melbourne RO, i.e. not taking in to account acceleration caused by the built environment local to 115B Victoria Parade, wind speeds are likely to be comfortable for long term sedentary activities for the majority of the time.

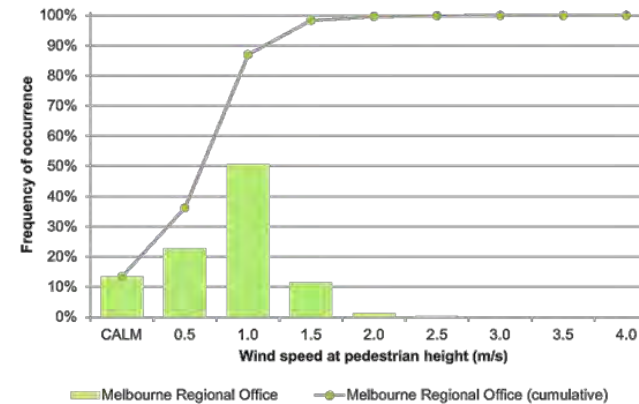


Figure 1: Distribution of wind speeds by band (bars) and cumulatively (line).

¹ Station number 086071. The data consisted of hourly wind speed and direction, maximum gust speed, temperature and other variables from 3/3/1997 to 31/8/2009. After quality checks, there were a total of 94,026 suitable records.

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3.2.2 Wind direction

Figure 2 shows the frequency of winds from each direction (divided in to 10° increments) for all hours and seasons (top left), divided by season for all times of the day (top right) and divided by season during business hours (bottom). Within the CBD, there is very little variation in predominant winds between seasons, and during business hours and outside of business hours, with west south westerly winds occurring most frequently, closely followed by north easterly winds. Hence west south westerly and north easterly winds will be the focus of the assessment in section 4.0.

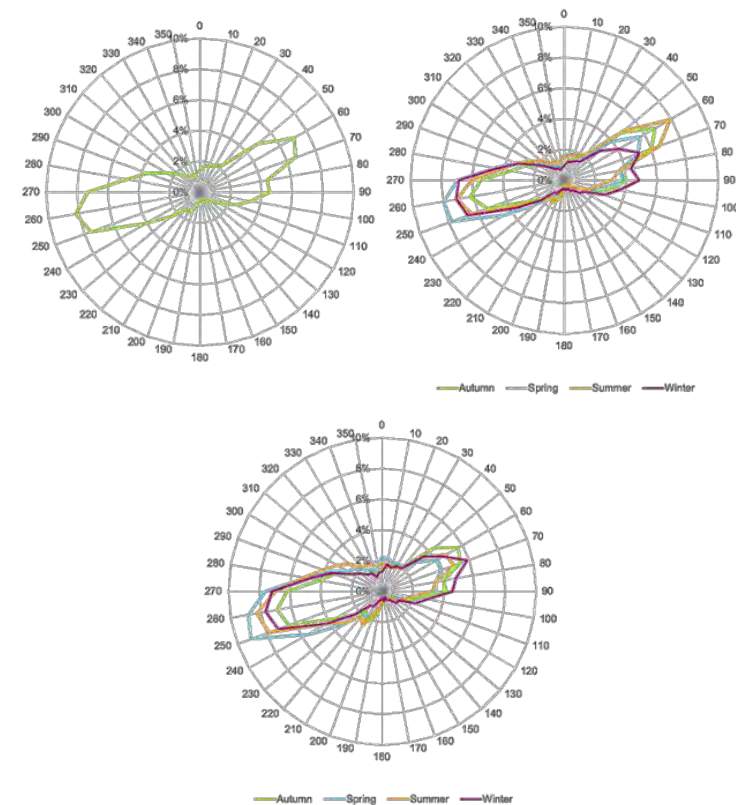


Figure 2: Distribution of wind heading for all times and seasons (top left), all times divided by season (top right), and by season during business hours (bottom).

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4.0 Comfort assessment

4.1 Wind-sensitive locations

Figure 3 and Figure 4 identify the locations that are potentially sensitive to wind flows, both within the site boundary, and the impact that the proposed building may have on adjacent streets. Each location is discussed in the following sections.



Figure 3: View from south east

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- 6. Along Young Street
- 7. Along Little Victoria Street



Figure 4: View from north west

The terrace area is exposed to north easterly winds with no shelter provided by surrounding buildings as they are all considerably lower than this level. The balustrades (currently shown at around 1.6m high) will provide protection to users of this space, and an estimate of likely wind speeds will be provided in the detailed analysis in the next stage of design.

4.1.5 Level 6 Terrace

As with Level 4, the Level 6 Terrace should be suitable for shorter periods of sitting or standing, however exposure is generally considered to be by choice and easily avoidable.

This terrace is exposed to both west south westerly winds and north easterly winds, and so has the potential to experience elevated winds compared to the Level 4 Terrace, due to the depth of the terrace. Again, the balustrades (currently shown at 1.6m high) will provide protection to users, and further analysis will be provided in the next design stage.

4.1.6 Along Young Street

Conditions along Young Street should be suitable for leisure walking as a minimum. The proposed building is unlikely to accelerate wind speeds above this threshold, and more detailed analysis will be undertaken during the next stage of design in order to confirm this.

4.1.7 Along Little Victoria Street

Conditions along Little Victoria Street should be suitable for leisure walking as a minimum. The proposed building is unlikely to accelerate wind speeds above this threshold, and more detailed analysis will be undertaken during the next stage of design in order to confirm this.

4.1.1 Ground level outdoor seating area

This area is required to be comfortable for pedestrians sitting or standing for longer periods of time.

West south westerly winds are likely to channel along Victoria Parade from the city. Buildings on the opposite side of Victoria Parade are likely to provide little shelter, however winds are unlikely to be excessive in speed. Local mitigation features may be required, and this will be confirmed during the detailed analysis that will be carried out during the next design stage.

The adjacent buildings across Napier Street are likely to provide some shelter from north easterly winds, with down wash on the eastern façade unlikely due to the staggered floor plate of the buildings and horizontal fins used for provide shade.

4.1.2 Along Napier Street

Conditions along Napier Street should be suitable for leisure walking as a minimum. The proposed building is unlikely to accelerate wind speeds above this threshold, and more detailed analysis will be undertaken during the next stage of design in order to confirm this.

4.1.3 Along Victoria Parade

Conditions along Victoria Parade should be suitable for leisure walking as a minimum. The proposed building is unlikely to accelerate wind speeds above this threshold, and more detailed analysis will be undertaken during the next stage of design in order to confirm this.

4.1.4 Level 4 Terrace

The Level 4 Terrace should be suitable for shorter periods of sitting or standing, however exposure is generally considered to be by choice and easily avoidable.

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5.0 Summary

5.1 Likelihood of discomfort

A high level desktop study of likely wind conditions around the proposed 115B Victoria Parade building and surrounding streets has been undertaken. Due to the relatively low wind speeds experienced in this area of Melbourne, there are unlikely to be any significant adverse impacts at ground level within or surrounding the site due to the construction of this building. The staggered, non-uniform shape of the building envelope is likely to minimise wind acceleration, with horizontal shading further assisting in minimising potential issues.

The terraces on Levels 4 and 6 are relatively exposed to prevailing winds. Although the current balustrades will provide some protection, additional local mitigation measures may be required.

A detailed computational analysis will be undertaken during the next design stage which will quantify expected wind speeds and compared these against the Lawson comfort criteria.

5.2 Mitigation

No additional mitigation requirements are proposed at this stage, however smaller local features may need to be incorporated following the detailed analysis.

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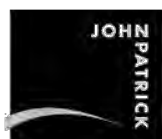
**ARBORICULTURAL REPORT
ARBORICULTURAL IMPACT ASSESSMENT
AUSTRALIAN CATHOLIC UNIVERSITY
115B VICTORIA PARADE, FITZROY**

PREPARED BY

Simon Howe

Consultant Arborist
B.AppSci(Hort)
GradDip Plan&Des (LandscpArch) *MELB*

October 2016



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324 Victoria Street, Richmond, VIC 3121, Australia
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Australian Catholic University, 115B Victoria Parade Fitzroy
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1 INTRODUCTION

- 1.1 Trees within and adjacent to the Australian Catholic University, 115B Victoria Parade, Fitzroy were assessed on the 15th September, 2016. Trees within the subject site as well as trees in adjacent road reserves (Young and Napier Streets, Victoria Parade) were assessed as part of the study.

2 DISCUSSION

- 2.1 32 trees or tree groups were assessed as part of this study:
- 10 trees or tree groups within the subject site have been assessed of medium retention value;
 - 8 trees within the subject site have been assessed of low retention value;
 - 14 trees have been assessed outside the subject site.
- 2.2 Of the ten trees assessed within the site of medium retention value, nine are semi-mature Pin Oaks within the car park in the east of the site. These are generally developing well within a highly urbanised environment. A pair of Bangalow Palms (Tree 1) are located adjacent to the north-west corner of the car park.

TABLE 1 Trees assessed of medium retention value

No	Species	Common Name
1	<i>Archontophoenix cunninghamiana</i>	Bangalow Palm
2	<i>Quercus palustris</i>	Pin Oak
3	<i>Quercus palustris</i>	Pin Oak
4	<i>Quercus palustris</i>	Pin Oak
5	<i>Quercus palustris</i>	Pin Oak
6	<i>Quercus palustris</i>	Pin Oak
10	<i>Quercus palustris</i>	Pin Oak
11	<i>Quercus palustris</i>	Pin Oak
12	<i>Quercus palustris</i>	Pin Oak
13	<i>Quercus palustris</i>	Pin Oak

- 2.3 The balance of trees assessed within the site are of low retention value, Silver Birch located to the periphery of the car park. The low retention value of these trees is a reflection of their overall condition and limited existing and potential amenity value.

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TABLE 2 Trees assessed of low retention value

No	Species	Common Name
7	<i>Betula pendula</i>	Silver Birch
8	<i>Betula pendula</i>	Silver Birch
9	<i>Betula pendula</i>	Silver Birch
14	<i>Betula pendula</i>	Silver Birch
15	<i>Betula pendula</i>	Silver Birch
16	<i>Betula pendula</i>	Silver Birch
17	<i>Betula pendula</i>	Silver Birch
18	<i>Betula pendula</i>	Silver Birch

- 2.4 Trees assessed outside the site are street trees in the Napier Street, Young Street and Victoria Parade road reserves to the east, west and south respectively. Young Street is planted with developing Golden Robinia (Trees 19-25), their overall form reflecting the limited light between tall buildings in this section of Young Street. An older Locust (Tree 26) is located in the south west of the study area within Victoria Parade.
- 2.5 The west side of Napier Street is planted with a pair of semi-mature Pin Oaks (Trees 27 and 28) located in a roadside bed at the intersection of Victoria Parade, with a row of established Elms (Trees 29-32) planted in roadside cut-outs to the north of the intersection. The Elms are part of a larger avenue plantation that provide a high level of amenity to the streetscape.

TABLE 3 Trees assessed outside the site

No	Species	Common Name
19	<i>Robinia pseudoacacia</i> 'Frisia'	Golden Robinia
20	<i>Robinia pseudoacacia</i> 'Frisia'	Golden Robinia
21	<i>Robinia pseudoacacia</i> 'Frisia'	Golden Robinia
22	<i>Robinia pseudoacacia</i> 'Frisia'	Golden Robinia
23	<i>Robinia pseudoacacia</i> 'Frisia'	Golden Robinia
24	<i>Robinia pseudoacacia</i> 'Frisia'	Golden Robinia
25	<i>Robinia pseudoacacia</i> 'Frisia'	Golden Robinia
26	<i>Robinia pseudoacacia</i>	Locust
27	<i>Quercus palustris</i>	Pin Oak
28	<i>Quercus palustris</i>	Pin Oak
29	<i>Ulmus procera</i>	English Elm
30	<i>Ulmus procera</i>	English Elm
31	<i>Ulmus procera</i>	English Elm
32	<i>Ulmus procera</i>	English Elm

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3 IMPACT OF PROPOSED DEVELOPMENT

- 3.1 Development of the site is proposed, including construction of a new 13 level building over basement car park in the existing car parking area. A new vehicular access is proposed from Napier Street, with changes proposed to the road functional layout of Napier Street. The following drawings have been reviewed in the preparation of these notes:

Lyons Architects ACU Development Plan October 2016;

&

Signage and Linemarking Plan. Functional Layout Plan Option 02 CG150178-TR-DG-2502 Rev 3 11.10.2016

Australian Catholic University, Napier Street, Fitzroy. City Of Yarra

Prepared by Cardno

Site Trees

- 3.2 All trees assess within the site will require removal to facilitate development. None are considered to be of sufficiently high value to require redesign to ensure retention.

Trees Outside the Site

- 3.3 Two trees outside the site, a pair of Pin Oaks (Trees 27 and 28) at the south western end of Napier Street will require removal for the revised road functional layout.
- 3.4 The balance of trees to the west side of Napier Street outside the development area can be retained, subject to appropriate protection conforming to AS4970-2009 *Protection of Trees on Development Sites*. The only noted encroachments by works are for :
- The basement, <1% of the tree protection zone of Tree 29.
 - The basement ramp crossover, <= 3% for Tree 30, <1% for Tree 31.
- 3.5 All are minor encroachments under the provisions of AS4970-2009.
- 3.6 The road functional layout has been developed so that new kerb and channel is limited to the crossover within the tree protection zones of trees to be retained, and traffic separation within TPZs can be accomplished with line-marking and surface texturing.
- 3.7 There may be the potential to provide permeability through removal of bituminous concrete to portions of the west side of Napier Street (currently sealed to the kerb) to improve growing conditions for existing trees.

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- 3.8 A full survey of all trees is included below.
- 3.9 The location of each tree is shown in 7 Appendix 1 – Tree Location Plan.

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4 SITE PHOTOGRAPHS



Figure 1 From right, Trees 2, 3 and 4 in the car park.

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Figure 2 Tree 1, a pair of Bangalow Palms .

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Figure 3 Tree 32 in the Napier Street road reserve.

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Figure 4 Golden Robinias in the Young Street road reserve.

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5 RESULTS OF TREE SURVEY

Tree-1		Archontophoenix cunninghamiana, Bangalow Palm	
Origin: Australian native	Type: Evergreen Broadleaf	Age: Semi-mature	
DBH (cm): 16	Height: 10m	Width: 5m	TPZ: 3.5m
Crown class: Symmetrical	Health: Fair-Good	Structure: Fair-Good	SULE: 20years
Amenity value: Medium	Comments: Can be transplanted		
Retention Value: Medium		Reason:	
Impact of Development: Remove			
Tree-2		Quercus palustris, Pin Oak	
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 35.5	Height: 13m	Width: 6m	TPZ: 4.3m
Crown class: Symmetrical	Health: Dormant	Structure: Fair-Good	SULE: 20years
Amenity value: Medium	Comments: Good developing specimen		
Retention Value: Medium		Reason:	
Impact of Development: Remove			
Tree-3		Quercus palustris, Pin Oak	
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 20.5	Height: 8m	Width: 5m	TPZ: 2.5m
Crown class: Symmetrical	Health: Dormant	Structure: Fair	SULE: 20years
Amenity value: Low	Comments:		
Retention Value: Medium		Reason:	
Impact of Development: Remove			
Tree-4		Quercus palustris, Pin Oak	
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 23	Height: 11m	Width: 6m	TPZ: 2.8m
Crown class: Symmetrical	Health: Dormant	Structure: Fair-Good	SULE: 20years
Amenity value: Medium	Comments: Slightly kinked trunk, otherwise good developing specimen		
Retention Value: Medium		Reason:	
Impact of Development: Remove			

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Tree-5	Quercus palustris, Pin Oak		
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 30	Height: 12m	Width: 6m	TPZ: 3.6m
Crown class: Symmetrical	Health: Dormant	Structure: Fair	SULE: 20years
Amenity value: Medium	Comments:		
Retention Value: Medium	Reason:		
Impact of Development: Remove			
Tree-6	Quercus palustris, Pin Oak		
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 31.5	Height: 12m	Width: 7m	TPZ: 3.8m
Crown class: Symmetrical	Health: Dormant	Structure: Fair	SULE: 20years
Amenity value: Medium	Comments: Contorted trunk. Codominant pruned out.		
Retention Value: Medium	Reason:		
Impact of Development: Remove			
Tree-7	Betula pendula, Silver Birch		
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 17	Height: 6m	Width: 4m	TPZ: 2.0m
Crown class: Symmetrical	Health: Dormant	Structure: Fair	SULE: 10-20years
Amenity value: Low	Comments:		
Retention Value: Low	Reason:		
Impact of Development: Remove			
Tree-8	Betula pendula, Silver Birch		
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 25.5	Height: 10m	Width: 6m	TPZ: 3.1m
Crown class: Symmetrical	Health: Dormant	Structure: Fair	SULE: 10-20years
Amenity value: Medium	Comments: Narrow primary union		
Retention Value: Low	Reason:		
Impact of Development: Remove			
Tree-9	Betula pendula, Silver Birch		
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 18	Height: 9m	Width: 6m	TPZ: 2.2m
Crown class: Symmetrical	Health: Dormant	Structure: Fair-Poor	SULE: 0-10years
Amenity value: Low	Comments: Dead wood evident		
Retention Value: Low	Reason:		
Impact of Development: Remove			

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Tree-10	Quercus palustris, Pin Oak		
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 23.5	Height: 9m	Width: 6m	TPZ: 2.8m
Crown class: Symmetrical	Health: Dormant	Structure: Fair-Good	SULE: 20years
Amenity value: Medium	Comments:		
Retention Value: Medium	Reason:		
Impact of Development: Remove			
Tree-11	Quercus palustris, Pin Oak		
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 22.5	Height: 8m	Width: 6m	TPZ: 2.7m
Crown class: Symmetrical	Health: Dormant	Structure: Fair-Good	SULE: 20years
Amenity value: Medium	Comments:		
Retention Value: Medium	Reason:		
Impact of Development: Remove			
Tree-12	Quercus palustris, Pin Oak		
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 23.5	Height: 8m	Width: 7m	TPZ: 2.8m
Crown class: Symmetrical	Health: Dormant	Structure: Fair-Good	SULE: 20years
Amenity value: Medium	Comments:		
Retention Value: Medium	Reason:		
Impact of Development: Remove			
Tree-13	Quercus palustris, Pin Oak		
Origin: Exotic	Type: Deciduous Broadleaf	Age:	
DBH (cm): 31	Height: 8m	Width: 7m	TPZ: 3.7m
Crown class: Symmetrical	Health: Dormant	Structure: Fair-Good	SULE: years
Amenity value: Medium	Comments:		
Retention Value: Medium	Reason:		
Impact of Development: Remove			
Tree-14	Betula pendula, Silver Birch		
Origin: Exotic	Type: Deciduous Broadleaf	Age: Senescent	
DBH (cm): 0	Height: 5m	Width: 2m	TPZ: 2.0m
Crown class: Symmetrical	Health: Poor	Structure: Poor	SULE: 0years
Amenity value: Very Low	Comments: Cracks in trunk, possibly dead.		
Retention Value: Low	Reason:		
Impact of Development: Remove			

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Tree-15	Betula pendula, Silver Birch		
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 13.5	Height: 5m	Width: 2m	TPZ: 2.0m
Crown class: Symmetrical	Health: Dormant	Structure: Fair	SULE: 10-20years
Amenity value: Low	Comments:		
Retention Value: Low	Reason:		
Impact of Development: Remove			
Tree-16	Betula pendula, Silver Birch		
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 19.5	Height: 7m	Width: 5m	TPZ: 2.3m
Crown class: Symmetrical	Health: Dormant	Structure: Fair	SULE: 10-20years
Amenity value: Medium	Comments:		
Retention Value: Low	Reason:		
Impact of Development: Remove			
Tree-17	Betula pendula, Silver Birch		
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 15	Height: 10m	Width: 4m	TPZ: 2.0m
Crown class: Symmetrical	Health: Dormant	Structure: Fair-Good	SULE: 10-20years
Amenity value: Medium	Comments:		
Retention Value: Low	Reason:		
Impact of Development: Remove			
Tree-18	Betula pendula, Silver Birch		
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 20	Height: 10m	Width: 6m	TPZ: 2.4m
Crown class: Asymetrical	Health: Dormant	Structure: Fair-Good	SULE: 10-20years
Amenity value: Medium	Comments:		
Retention Value: Low	Reason:		
Impact of Development: Remove			
Tree-19	Robinia pseudoacacia 'Frisia', Golden Robinia		
Origin: Exotic weed	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 20	Height: 12m	Width: 6m	TPZ: 2.4m
Crown class: Symmetrical	Health: Dormant	Structure: Fair	SULE: 20years
Amenity value: Medium	Comments: Street tree		
Retention Value: Outside property	Reason:		
Impact of Development: Retain			

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Tree-20	<i>Robinia pseudoacacia</i> 'Frisia', Golden Robinia		
Origin: Exotic weed	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 17.5	Height: 12m	Width: 6m	TPZ: 2.1m
Crown class: Symmetrical	Health: Dormant	Structure: Fair	SULE: 20years
Amenity value: Medium	Comments: Street tree		
Retention Value: Outside property	Reason:		
Impact of Development: Retain			
Tree-21	<i>Robinia pseudoacacia</i> 'Frisia', Golden Robinia		
Origin: Exotic weed	Type: Deciduous Broadleaf	Age: Juvenile	
DBH (cm): <15	Height: 2m	Width: 1m	TPZ: 2.0m
Crown class: Symmetrical	Health: Fair-Good	Structure: Fair-Good	SULE: 20years
Amenity value: Low	Comments: Newly planted Street tree		
Retention Value: Outside property	Reason:		
Impact of Development: Retain			
Tree-22	<i>Robinia pseudoacacia</i> 'Frisia', Golden Robinia		
Origin: Exotic weed	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): <15	Height: 2m	Width: 1m	TPZ: 2.0m
Crown class: Symmetrical	Health: Dormant	Structure: Fair-Good	SULE: 20years
Amenity value: Low	Comments: Newly planted Street tree		
Retention Value: Outside property	Reason:		
Impact of Development: Retain			
Tree-23	<i>Robinia pseudoacacia</i> 'Frisia', Golden Robinia		
Origin: Exotic weed	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 13	Height: 12m	Width: 7m	TPZ: 2.0m
Crown class: Symmetrical	Health: Dormant	Structure: Fair	SULE: 20years
Amenity value: Low	Comments: Street tree		
Retention Value: Outside property	Reason:		
Impact of Development: Retain			
Tree-24	<i>Robinia pseudoacacia</i> 'Frisia', Golden Robinia		
Origin: Exotic weed	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 23	Height: 12m	Width: 7m	TPZ: 2.8m
Crown class: Symmetrical	Health: Dormant	Structure: Fair-Good	SULE: 20years
Amenity value: Medium	Comments: Street tree		
Retention Value: Outside property	Reason:		
Impact of Development: Retain			

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Tree-25	<i>Robinia pseudoacacia</i> 'Frisia', Golden Robinia		
Origin: Exotic weed	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 30	Height: 12m	Width: 7m	TPZ: 3.6m
Crown class: Symmetrical	Health: Dormant	Structure: Fair	SULE: 0-10years
Amenity value: Medium	Comments: Basal trunk canker and frass. Street tree		
Retention Value: Outside property	Reason:		
Impact of Development: Retain			
Tree-26	<i>Robinia pseudoacacia</i>, Locust		
Origin: Exotic weed	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 19.5	Height: 6m	Width: 5m	TPZ: 2.3m
Crown class: Symmetrical	Health: Fair	Structure: Fair	SULE: 10-20years
Amenity value: Medium	Comments: Fungal bodies on trunk. Street tree		
Retention Value: Outside property	Reason:		
Impact of Development: Retain			
Tree-27	<i>Quercus palustris</i>, Pin Oak		
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 32	Height: 10m	Width: 9m	TPZ: 3.8m
Crown class: Symmetrical	Health: Dormant	Structure: Fair	SULE: 20years
Amenity value: Medium	Comments: Canyon pruned.		
Retention Value: Outside property	Reason:		
Impact of Development: Remove			
Tree-28	<i>Quercus palustris</i>, Pin Oak		
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 34	Height: 13m	Width: 10m	TPZ: 4.1m
Crown class: Intermediate	Health: Dormant	Structure: Fair	SULE: 20years
Amenity value: Medium	Comments: Canyon pruned and asymmetric		
Retention Value: Outside property	Reason:		
Impact of Development: Remove			
Tree-29	<i>Ulmus procera</i>, English Elm		
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 34	Height: 13m	Width: 10m	TPZ: 4.1m
Crown class: Symmetrical	Health: Dormant	Structure: Fair-Good	SULE: 20years
Amenity value: High	Comments: Basal wounding.		
Retention Value: Outside property	Reason:		
Impact of Development: Retain			

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Tree-30	Ulmus procera, English Elm		
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 29	Height: 12m	Width: 6m	TPZ: 3.5m
Crown class: Symmetrical	Health: Dormant	Structure: Fair	SULE: 10-20years
Amenity value: High	Comments: Large tear-out wound east side.		
Retention Value: Outside property	Reason:		
Impact of Development: Retain			
Tree-31	Ulmus procera, English Elm		
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 36.5	Height: 13m	Width: 7m	TPZ: 4.4m
Crown class: Symmetrical	Health: Dormant	Structure: Fair	SULE: 20years
Amenity value: Medium	Comments: Lost co-dominant. Trunk wounding.		
Retention Value: Outside property	Reason:		
Impact of Development: Retain			
Tree-32	Ulmus procera, English Elm		
Origin: Exotic	Type: Deciduous Broadleaf	Age: Semi-mature	
DBH (cm): 33	Height: 11m	Width: 7m	TPZ: 4.0m
Crown class: Symmetrical	Health: Dormant	Structure: Fair	SULE: 20years
Amenity value: High	Comments: Some trunk wounds		
Retention Value: Outside property	Reason:		
Impact of Development: Retain			

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6 DESCRIPTORS

Tree Number: Refers to location of tree as per the plan at Appendix 1.

Botanical Name: Botanical name of species, based on nomenclature and spelling used by Spencer in *Horticultural Flora of South Eastern Australia* (vols 1-5). Where *Eucalyptus* spp. are not found in this source, nomenclature is based on *Euclid: Eucalypts of Australia* (2006). Eucalypt subspecies information is also based on this source.

While accurate tree identification is attempted, and uncertainties are indicated, some inaccuracies in tree identification may still be present – especially in certain, difficult to determine, genera (e.g. *Cotoneaster* and *Ulmus*) and with cultivars which can have similar characteristics.

Where a doubt as to exact species is indicated, the common name and origin are based on the listed species, and would change if the species were found to be incorrect.

From time to time taxonomists revise plant classification, and name changes are assigned. If it is known names have been revised post the publication of the relevant above listed source, the new nomenclature has been used.

Common Name: Common names are based primarily on names and spelling used by Spencer in *Horticultural Flora of South Eastern Australia* (vols 1-5). The source of common names is taken in the following order:

1. Single name supplied in *Horticultural Flora of South Eastern Australia*;
2. First in list of names supplied in *Horticultural Flora of South Eastern Australia*, unless another name in the list is deemed more appropriate;
3. As per name supplied in *Trees of Victoria and Adjoining Areas*;
4. Then by best known common name if not available in either source.

Common names are provided for thoroughness; the botanical name should be used when referring to the tree taxon.

Origin: **Exotic:** Tree origin is from outside the Australian mainland, Tasmania or near islands.

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Australian Native: Origin is from within the Australian mainland or near islands, but outside Victoria.

Victorian Native: Origin is from within Victoria but outside the Melbourne region. This includes trees whose native range extends beyond Victoria into other states.

Melbourne: Origin is from within Melbourne, as defined by plants listed in the *Flora of Melbourne*. This includes trees also found outside Melbourne, and those only within the area at the far extent of their range.

Locally Indigenous: Tree's range includes the local area.

Weed: Trees known to show tendencies to weediness within Victoria. Based on the City of Knox weed list, Department of Primary Industries (Victoria) weed list and past experience. Trees with the addition of "(nox.," indicate a declared noxious weed; refer to the Department of Primary Industries website for further information.

Type:

Broadleaf: Tree is a dicotyledon flowering plant.

Conifer: Tree is a cone bearing non-flowering plant.

Palm: Tree is a monocotyledon Palm (that is *Arecaceae*).

Palm Like: Tree is a monocotyledon, but is not a palm (that is not *Arecaceae*).

Deciduous: Tree seasonally loses its leaves in Victoria.

Evergreen: Tree maintains its leaves throughout the year.

Semi-deciduous: Tree may or may not lose its leaves, or may only partially lose them.

Age:

Juvenile: Tree is actively growing and is still in its establishment phase. Tree currently makes little contribution to the amenity of the landscape. Trees of this age are possible candidates for relocation during development.

Semi-mature: Tree is still actively growing but has reached an age and size where it is starting to make a contribution to the landscape.

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The size of the tree would still be expected to increase considerably given no significant changes to the current situation.

Mature: Tree growth has slowed, and the size of the tree would not be expected to increase considerably without significant changes to the current situation (e.g. vegetation removal). Tree is not exhibiting any major signs of health or structural weakness as a result of age.

Over mature: Tree is no longer actively putting out extension growth, and is starting to show decline in health or structural stability as a result of age.

Senescent: Tree is senescing. Trees in this category may not be especially large or old, but are reaching the end of their expected life, often indicated by extreme poor health.

Height: Estimate of the tree's height in metres

DBH: The tree's trunk Diameter at Breast Height (1.4m above ground) unless specified as having been taken lower. This can be either estimated or measured as specified in the report.

Stems of multi-stemmed trees may be listed individually, or a measurement given at a lower point where the tree still has one stem. In some cases, especially where trees are not considered worthy of retention or stems are too numerous the DBH may simply be listed as "multi-stemmed".

Health: The tree's health is rated as **Good**, **Fair** and **Poor** as listed below. Tree ratings of **Fair-Good** and **Fair-Poor** indicate that the tree falls between the two categories. Dead trees are not given a rating, but are listed as **Dead**.

Ratings generally meet the following descriptions:

Good: *Tree is showing no obvious signs of poor health or stress with a dense canopy that is free of dieback. Rot or pathogens are not obvious or are not considered to be a threat to the tree. Growth rates are acceptable.*

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Fair: *Tree is showing signs of reduced health or stress.* This is apparent through moderate foliage density, minor dieback, moderate stress response growth, minor to moderate rot, moderate pathogen infestation, stunted growth or a combination of the above symptoms.

Poor: *Tree is showing signs of poor health and/or severe stress.* This is apparent through either low foliage density, moderate to large-scale dieback, severe stress response growth, severe rot, severe pathogen infestation, failure of wounds to heal, overall tree decline or a combination of the above symptoms.

Note on Deciduous Species: Assessment of deciduous species can be problematic and results may vary depending on the time of year of assessment. Descriptor comments in relation to foliage density do not apply to deciduous trees assessed when dormant or entering or exiting dormancy. Time of leaf drop or bud burst and extent of bud swell may be considered in the health rating of these trees.

The ratings indicate that certain characteristics listed have, or have not been observed. Inspections do not assess the whole tree in detail for each characteristic. The comments category should be referred to for further information.

Structure:

The tree's structure is rated as **Good**, **Fair** and **Poor**. Tree ratings of **Fair-Good** and **Fair-Poor** indicate that the tree falls between the two categories.

As a general rule, the structure rating is based on the tree's likelihood of failure. However, it must be noted that this is not a full hazard or failure assessment of the tree.

Good: Tree has no obvious structural defects and is therefore not considered likely to fail.

Fair: Tree has at least one obvious structural defect, but this is considered to be manageable and of only moderate failure risk or the piece likely to fail may be small. Structural defects that may contribute to a fair rating are as follows:

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- Poor branch attachment (including deadwood and large epicormics);
- Bifurcated, but with a join that is considered to be solid;
- Moderate trunk lean but without other defects;
- Minor damage to the trunk base;
- Rot or other damage starting to compromise the structure;
- History of shedding minor branches.

Poor: Tree has at least one structural defect that is severe and considered to have a relatively high risk of failure. If targets are present then defect(s) require treatment, or alternatively the tree should be removed. In some cases removal may be the only option for these trees. Structural defects that may contribute to a poor rating are as follows:

- Poor branch attachment (including deadwood and large epicormics);
- Bifurcated with swelling and/or included bark;
- Severe trunk lean associated with other defects such as injury in the plane of lean or root plate lift;
- Major damage to the trunk base or root system;
- Rot or other damage severely compromising the structure;
- History of shedding large branches.

The ratings indicate that certain characteristics listed have, or have not been observed. Inspections do not assess the whole tree in intense detail for each characteristic. The comments category should be referred to for further information.

Crown class:

Symmetrical: For the most part canopy received light from all four sides and has the potential for even foliage distribution. Canopy may or may not be symmetrical, but is not suppressed.

Asymmetrical: Canopy is shaded or suppressed with one or more sides and dominant when compared to the remainder of the tree. Also includes crowns damaged by previous shading.

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Intermediate: Canopy is only receiving light from top, and while shape may be even the upper portions of the canopy dominate over the lower.

Suppressed: Canopy is completely shaded by surrounding vegetation, buildings etc.

Regrowth: Canopy comprised of regrowth. This can be from the base, but also includes branches covered with small, stress related epicormics.

Trained: Canopy has been specifically trained. This may include trees that are pollarded, coppiced or espaliered.

Trees may exhibit a combination of the characteristics above (e.g. a symmetrical canopy of basal regrowth), or may fall between two categories. The characteristic listed is considered to be the best fit at the time.

Amenity value:

Very Low: Tree makes little or no contribution to the amenity value of the site or surrounding area. In some cases the tree may be detrimental to the area's amenity value (e.g. unsightly, risk of weed spread).

Low: Tree makes some contribution to the amenity value of the site, but makes no contribution to the amenity value of the surrounding area. Removal of the tree would result in little loss of amenity. Juvenile trees (including street trees) are generally included in this category, however they may have the potential to supply increased amenity in the future.

Medium: Tree makes a moderate contribution to the amenity of the site and/or may contribute to the amenity of the surrounding area.

High: Tree makes a significant contribution to the amenity value of the site, or tree makes a moderate to significant contribution to the amenity value of the larger landscape.

The amenity value rating considers the impact the tree has on any neighbouring sites as being of equal importance to that supplied to the

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subject site. However, trees that contribute to the amenity of the general area (e.g. streetscape) are given greater weight.

Comments: Any additional comments in relation to the above categories.

SULE: The Safe, Useful, Life Expectancy of the tree from a health, structure, amenity and weediness viewpoint given no significant changes to the current situation. This category is difficult to determine, and should be taken as an estimate only, in addition to this, factors not observed at the time of inspection can lead to tree decline.

0: Tree is a hazard or a weed and should be removed immediately.

0-10: Estimated SULE of less than 10 years.

10-20: Estimated SULE of 10 to 20 years.

20: Estimated SULE of 20 years or greater.

Recommendation: **Remove:** Tree is either not worthy of retention or requires removal (e.g. weed species).

Retain or Remove: Tree does not require removal, but is of low retention value.

Retain if practical: Tree has a moderate retention value and should be retained if possible during any development of the site.

Notes: **Dead:** Tree is dead and should therefore be removed.

Good condition: Tree is worthy of retention based on its condition. Trees may still have some structural or health problems, but are generally worth retaining.

Good development potential: Tree is of a small size, but is considered to have a high potential to develop well. Retention of these trees should be considered as they should develop more quickly than new plantings.

Hazardous: Tree should be removed as it is hazardous.

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Heritage tree: Tree is of heritage significance. Refer to the introduction for further information on any trees of heritage significance.

High landscape contribution: Tree is worthy of retention based on its contribution to the site or landscape (associated with amenity value).

Inappropriate location: The tree is not in an appropriate location for its species, size etc. Includes trees too large for their current location.

Juvenile – simple to replace: Tree does not have a high retention value as a similarly sized replacement specimen could be obtained. Alternatively, the tree is a candidate for relocation.

Limited life expectancy: Tree is in decline, or is expected to start to decline within a relatively short time period. As a result, it is not sensible to implement extensive tree protection measures to save the tree unless there are extenuating circumstances (e.g. outside ownership).

Low Amenity Value: Tree is unsightly, or has little potential to add to site amenity (e.g. a non-canopy fruit tree).

Outside ownership: Tree is located outside the subject site, and is therefore owned by another party. The tree may be in a neighbouring private property or fall within the council managed nature strip/road reserve.

It is assumed that the owner of the tree wishes to retain it, and the trees are listed as retain for that reason. The owner should be contacted for discussions if the removal of the tree is wanted. Recommendation of retention of any of these trees is based solely on the above mentioned reason, and is no indication of the tree's general worthiness for retention.

Poor condition: Tree's poor condition makes it unworthy of retention.

Rare / unusual species: Tree is of a species, cultivar or form (trained or otherwise) which is unusual, at least in the local area, and which has some retention value (usually amenity value). Trees of this nature may also classify as a "heritage tree".

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Remnant Indigenous: The tree is a remnant indigenous specimen and therefore has environmental value. Trees of this nature, in reasonable condition are usually recommended for retention.

Senescent: Tree should be removed as it is dying.

Significant tree: The tree has been declared a significant tree by the local council, and retention is likely to be a permit requirement.

Unlikely to develop well: Tree is immature with a severe defect which will prevent its form developing as it should or tree has a severe defect, the correction of which will result in a tree shape that is unlikely to redevelop well .

Weed species: Tree should be removed due to weedy nature of the species.

TPZ: The Tree Protection Zone of the tree, measured as a radial distance in metres from the centre of the trunk. The TPZ is calculated using the method specified in *Australian Standard AS4970-2009 Protection of trees on development sites*.

TPZs are not listed for trees that are recommended for removal.

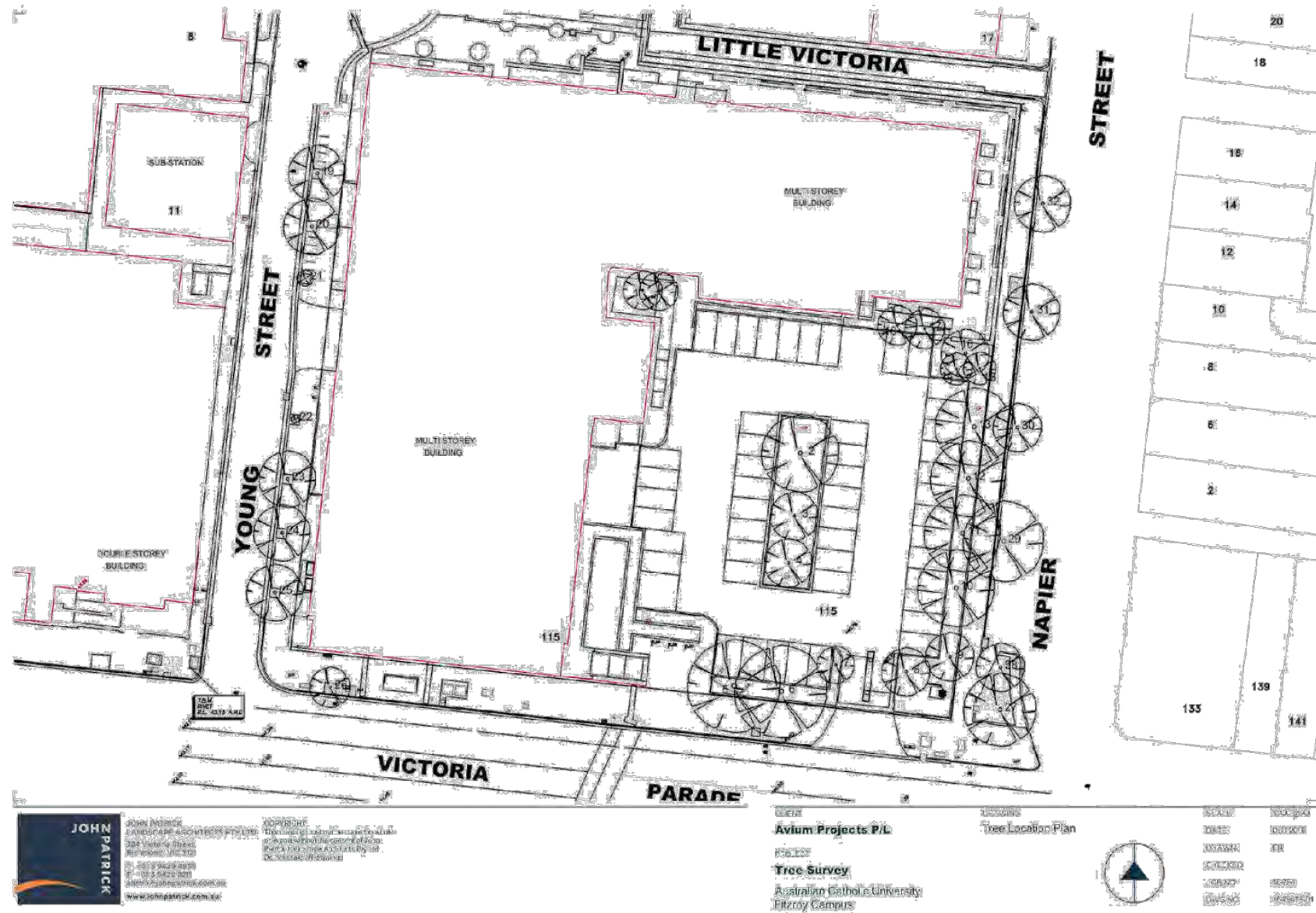
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7 APPENDIX 1 – TREE LOCATION PLAN

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***ACU - 115B + YOUNG ST MASSING
COMPARISONS***

115B MASSING COMPARISONS

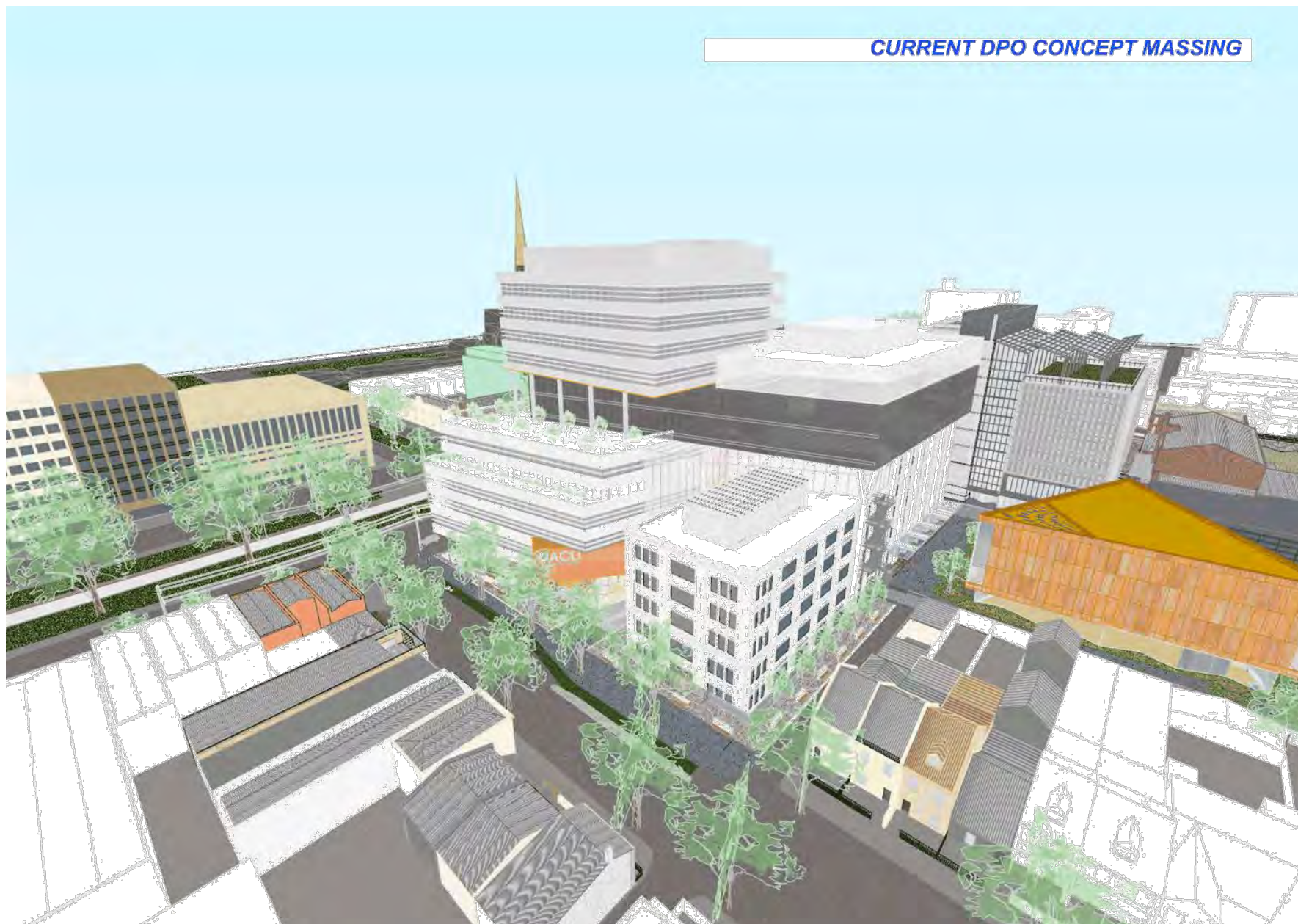
Attachment 13 - ACU Development Plan - Massing Comparison



Attachment 13 - ACU Development Plan - Massing Comparison



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YOUNG STREET MASSING COMPARISONS

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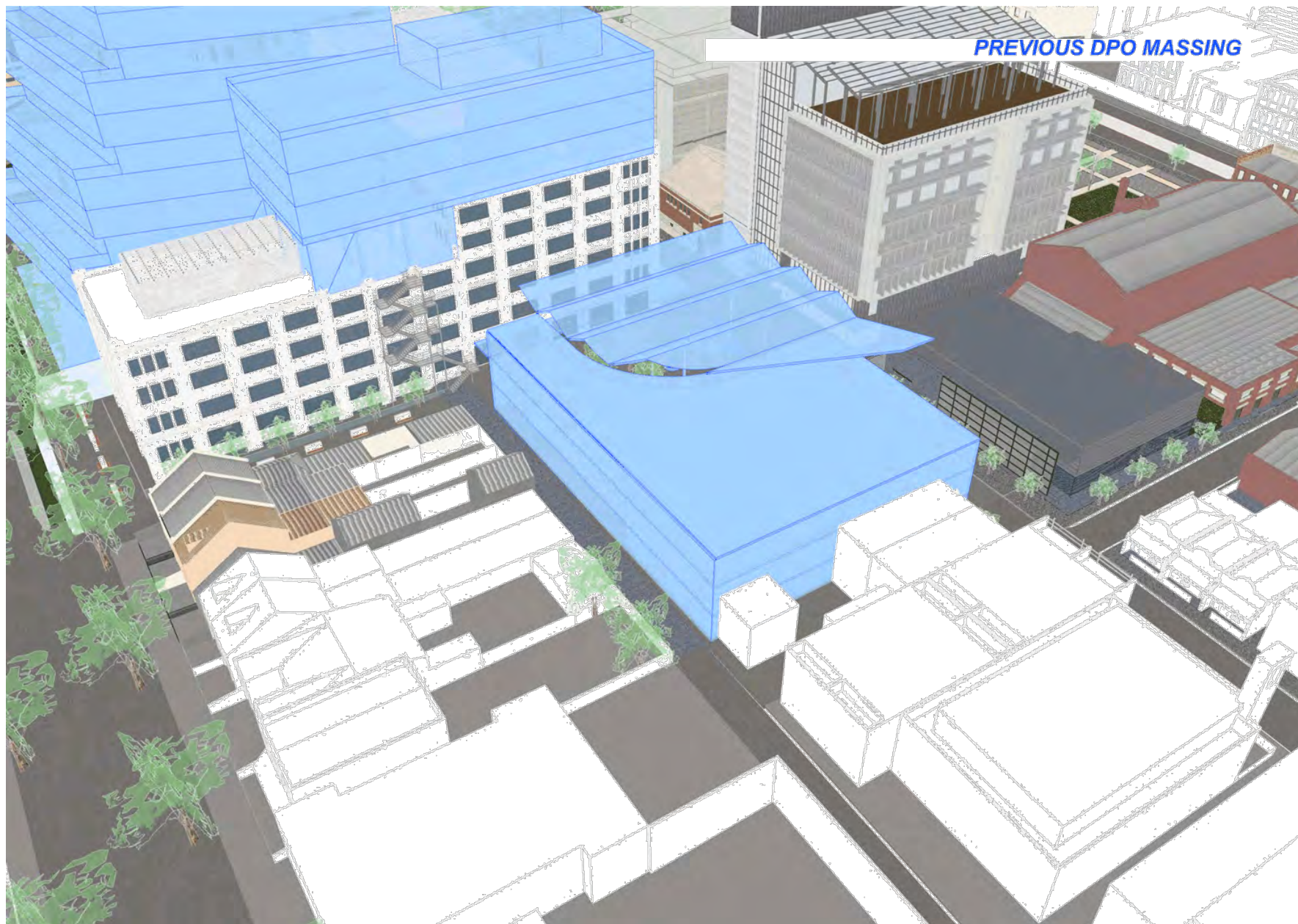
Attachment 13 - ACU Development Plan - Massing Comparison



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Attachment 13 - ACU Development Plan - Massing Comparison



Attachment 13 - ACU Development Plan - Massing Comparison



Attachment 13 - ACU Development Plan - Massing Comparison



Attachment 14 - ACU Development Plan - Applicant response to Referrals



ACU AMENDED DEVELOPMENT PLAN

REFERRAL RESPONSE

SUMMARY OF COUNCIL REFERRAL	ACU RESPONSE	REFERENCE
Traffic Engineer		
<i>The car park entrance should provide a pedestrian sight triangle at the exit lane.</i>	Cardno have advised that this can be achieved. This detail will be shown at the planning permit stage when the full design detail is provided.	N/A
<i>The width of the vehicular access point is supported.</i>	N/A	N/A
<i>The painted roundabout treatment in Napier Street is not supported.</i>	The design has been amended following consultation with Council's Traffic Engineers.	N/A
<i>Dimensions of the car parking modules and internal ramps are consistent with design standard requirements.</i>	N/A	N/A
<i>The two-way accessways where they do not abut parking are adequate provided they have 300mm wide kerbs either side.</i>	Cardno have advised that this can be achieved and the detail will come at the planning permit stage.	N/A
<i>Detailed traffic study and community consultation required before Young Street closure can be considered further.</i>	The Young Street closure has been removed from proposal.	Development Plan
Open Space and Landscaping		
<i>Arborist report on street trees to be removed and impact from basement, Napier Street works. At this stage tree removal not supported.</i>	Arborist report provided	Arborist report
<i>The proposed Robinia pseudoacacia should be relocated inside the property boundary along Victoria Parade.</i>	Relocated as requested shown in Landscape Master Plan	Landscape Master Plan
<i>Change proposed species (Waterhousia florihunda and Cleditsea triacanthos var. inermis) along Young Street to match the existing Rohinia species in Young Street.</i>	Young Street planting removed following removal of Young Street closure. All existing trees will be retained.	Landscape Master Plan

Attachment 14 - ACU Development Plan - Applicant response to Referrals



ACU AMENDED DEVELOPMENT PLAN

REFERRAL RESPONSE

<i>Reconfiguration of the proposed bike lanes at the end of Napier Street should be considered in order to retain significant existing street trees.</i>	Design amended, refer to Arborist report and updated Cardno plan / report.	Cardno Design Arborist report
<i>More information requested - showing exact location of existing and proposed trees, trees to be removed.</i>	Landscape Master Plan and Arborist report	Arborist report
<i>Overshadowing of streets and open space areas from the new buildings. Recommended reduction in building height to minimise impact on existing street trees.</i>	Building height has been discussed with Urban Design and Council and is supportable in this urban context.	Amended building envelopes.
Young Street / ACU Forecourt <i>More detail requested in relation to canopy of planting, sunlight to this area, will proposed planting be successful etc.</i>	Young Street design amended to allow for greater open space area, to the north side which will have good sunlight access	Landscape Master Plan
<i>Recommended that the developer considers solar access where the vertical green walls are proposed.</i>	Vertical green walls have been removed and replaced with more suitable planting	Landscape Master Plan
<i>Vertical green walls and lighting to private (non ACU) properties needs owners permission.</i>	These have been removed from the DP	Landscape Master Plan
<i>The need for feature lighting along the north south laneway running off little Victoria Street is questioned</i>	We understand this is supported by Urban Design therefore indicative lighting has been retained, this will be subject to further Council approval at the planning application stage.	Landscape Master Plan
<i>Seems to be a barrier at the junction of little Victoria and little Napier street, which can be better connected</i>	There is no barrier indicated on the Landscape Plan. The Landscape Plans have been updated for clarity	Landscape Master Plan
<i>Support ground plane improvements at corner of Napier and Victoria Parade. More detail requested to ensure the public realm is enhanced. Planter boxes location is questioned.</i>	The updated design shows the intention for a high quality public realm. This detail will come at the planning permit stage.	White box images showing ground plane work. Landscape Master Plan
<i>Detail requested and comments on St Mary of the Cross Square.</i>	This is outside the DPO area and therefore is not included.	N/A
<i>Further information showing locations of the bike parking spaces within the DPO area.</i>	This detail will come at the planning permit stage.	N/A

Attachment 14 - ACU Development Plan - Applicant response to Referrals



ACU AMENDED DEVELOPMENT PLAN

REFERRAL RESPONSE

Urban Design (Hansen)		
<i>Support consolidated entry between the Mary Glowrey building and new 115B</i>	This design element has been retained in the amended design.	N/A
<i>The circulation within the building on the ground floor supported</i>	This design element has been retained in the amended design.	N/A
Air bridges breach the DPO extent and impact will draw pedestrians away from the street.	The air-bridges have been removed from the Development Plan as requested	Development Plan
<i>The setback of the ground floor from Napier Street is encouraged.</i>	This design element has been retained in the amended design.	
<i>The provision for a new open space (Young Street Building) in the centre of the precinct is strongly encouraged.</i>	Agree and further open space has been provided in the amended Young Street design.	
<i>The retention of the 4 storey walls of the existing Young Street carpark is encouraged.</i>	The Young Street building has been amended and the car park will be demolished. We understand this is supported by the Urban Designers following meetings post the referral comments.	
<i>Support road closure of Young Street.</i>	The Young Street closure has been removed from the Development Plan following feedback from Council officers.	
<i>The raising of the laneway to the north side of 115B is supported.</i>	Note	
<i>The entry on Napier Street to the underground carpark enables an active frontage along Victoria Parade.</i>	Agree, this has been retained in the amended design	
<i>To decide whether the frontages are sufficiently activated and to understand the streetscape treatment, the drawings should provide a higher level of detail.</i>	Further information has been provided to show the appearance and anticipated materiality of the building, including glazed frontage at ground level to activate the streetscape.	Indicative Building Appearance Indicative Material Schedule
115B Building Height needs to better consider the character of the surrounding area.	The height has been reduced to 12 storey (plus plant)	115B amended design Lyons comparison booklet

Attachment 14 - ACU Development Plan - Applicant response to Referrals



ACU AMENDED DEVELOPMENT PLAN

REFERRAL RESPONSE

<i>Transition to Napier Street in terms of height between the building and Napier Street (in response to fine grain character), mass, upper level prominence and upper level setbacks to Napier Street</i>	The height and setbacks have been amended in response to this comment.	115B amended design Lyons comparison booklet
<i>The existing built form and grain of Napier Street, should be referenced in facade.</i>	The further detailing of the façade and anticipated materials will provide a modern response that is respectful of building materials found in the area. Noting that the site is located along the commercial spine of Victoria Parade.	Indicative building appearance Indicative Material Schedule
<i>Contrary to the heritage overlay, height overwhelms the Mary Glowrey Building.</i>	The height has been reduced and upper levels setbacks increased so the building does not overwhelm the Mary Glowrey building	115B Victoria Parade amended building Lyons comparison booklet Bryce Raworth letter
<i>Victoria Parade - Consider increasing setback of upper levels (above 5 levels), to be consistent with the surrounding context.</i>	The amended design shows a clear podium to Level 5 with increased upper level setbacks, particularly at the corner of Victoria Parade and Napier Street. The upper level setbacks now begin at Level 6 (previously Level 7) and the setback from Victoria Parade has increased from 2.5m to 7.3m at the upper levels. The shape of the upper level has been amended to read as a separate and more recessive element along Victoria Parade rather than the previous stepped design.	115B amended building envelopes. Lyons comparison booklet
<i>Eastern Transition - This site should provide a transition in scale to the fine grain and shallow allotments to the immediate east</i>	The upper level setback has been increased on the east so that the tallest building element is more central to the site and further away from allotments to the east.	115B amended building envelopes. Lyons comparison
<i>The chamfered edge of the building form to reveal the flanks of the Mary Glowrey Building is supported.</i>	This element of the building design has been retained.	
<i>The built form attached to the northern corner of the Mary Glowrey Building is supported. Further refinement is required to determine a clearer relationship between the two buildings.</i>	This corner will be attached with clear glazing to show a clear distinction between the old and new building forms.	Indicative building appearance

Attachment 14 - ACU Development Plan - Applicant response to Referrals



ACU AMENDED DEVELOPMENT PLAN

REFERRAL RESPONSE

<i>The massing and scale of the Young Street Building is encouraged.</i>	The massing has been further reduced which we understand is supported in principal.	Young Street amended massing
<i>The addition of the sky bridges will negatively impact on the use of the proposed public realm additions.</i>	The sky bridges have been removed from the Development Plan	Development Plan
<i>Opportunities to establish retail/cafe tenancies within the proposal should be considered to enable higher activation of the internal streets and spaces</i>	This will be considered at the detailed planning permit stage.	N/A
<i>The proposal for a public space flanked by the new Young Street Building is a strong addition to the public realm.</i>	Agree, further more open space has been provided in the amended Young Street design.	Amended Young Street design Open Space Diagram
<i>We support the elevation of the existing sunken walkway along Little Victoria Street.</i>	Agree this element has been retained in the amended design.	
<i>The proposed canopy extending between building 115B and the Mary Glowrey Building is encouraged.</i>	The amended design has retained a canopy between the two buildings.	
<i>A more detailed depiction of landscaping and public seating is encouraged to better understand the public realm propositions.</i>	The landscape master plan provides a high level indication of landscaping and seating which will be subject to further detail at the planning permit stage. To assist to understand the intention the indicative appearance 3D images show this for Young Street and 115B	Landscape Master Plan Indicative appearance of 115B and Young Street.
<i>More information regarding further provision for storage of bicycles.</i>	General information is provided in the traffic report and more detailed information will come at the planning permit stage.	Traffic report by Cardno
<i>More detail description of the Young Street buildings appearance, height, dimensions, and floor area, material palette, finishes</i>	This information has been provided which shows the indicative appearance and examples of high quality materials of the Young Street building.	Indicative building appearance Indicative materials examples.
<i>There are concerns that the elevations of the Victoria Street Façade (pg. 23 & 34 of ACU package) do not truly represent the scale of the built form to the east of 115B Victoria Parade.</i>	This diagram has been amended to reflect the existing built form to the east of 115B Victoria Parade	Amended streetscape drawing.

Attachment 14 - ACU Development Plan - Applicant response to Referrals



ACU AMENDED DEVELOPMENT PLAN

REFERRAL RESPONSE

Drainage Engineer		
<i>Napier Street Design Detail</i>	This will be subject to separate process with Council's engineers at the planning permit stage.	N/A
<i>Council drain under the west side channel of Napier Street where footpath is proposed to be widened. Drain should be upgraded when these works are done.</i>	This could be a condition at the planning permit stage.	
<i>Napier Street west side footpath – new kerb will create reverse fall which is undesirable.</i>	This will be addressed at the detailed design stage through conditions and separate Council approval.	
<i>Proposed kerb extension at the northern end of the roundabout on Napier Street will prevent stormwater flow. A new drain will be required on the east side of Napier Street.</i>	The roundabout has been removed following advice from Council's traffic engineers.	Amended Napier Street Design
<i>Design needs to consider 1 in 10 Year storm events</i>	This will be considered at the detailed design stage and subject to further Council approval.	
<i>All new or reconstructed kerbs should be bluestone and the channel due to the heritage overlay.</i>	This can be required by condition of permit.	
<i>Proposed bike station should be repositioned so that the bikes back wheels will not drop off the kerb when extracting a bike.</i>	The bike station has been relocated to Victoria Parade as requested.	Amended Landscape Plan showing bike location.
<i>Full width of road pavement in Napier Street from Victoria Street to Victoria Land should be resheeted in accordance with Council requirements .</i>	This will be done at the detailed design stage.	
<i>The footpaths on Victoria Parade and Napier Street should also be resheeted with asphalt in accordance with Council requirements.</i>	As above.	
<i>Street lighting in Napier Street between Victoria Parade and Victoria Land should be reviewed and upgraded by the Developer.</i>	This can be subject to discussion at the planning permit stage.	

Attachment 14 - ACU Development Plan - Applicant response to Referrals



ACU AMENDED DEVELOPMENT PLAN

REFERRAL RESPONSE

Strategic Transport		
<u>Napier Street Design and Bicycle Access</u> <ol style="list-style-type: none"> 1. Define bicycle / pedestrian spaces between Lansdown Street/ Vic Parade and Napier Street 2. No through road signage on Victoria and Napier St entrance 3. Resolve conflict point between bicycles and cars at Napier/ Victoria St intersection where cars will be entering the Napier St car park 4. Dimension bike storage area 5. Alternative treatment to car park entrance to give priority to cyclists 6. Widen Copenhagen bike lane north of the car park entrance to 2.5m 7. Design car park entrance to remove potential for drivers to access the car park from North (Gertrude Street approach) 8. Narrow 'necking' north of car park entrance between bike lane and kerb to prevent northbound entry to Napier Street 	<p>The Napier Design and Bicycle Access has been amended to address these issues following further consultation with Council's Traffic Engineers.</p>	<p>Amended Napier Street design.</p>
<u>Bicycle Parking on site</u> Further detail regarding type and location Provide bicycle parking on Napier Street side of development	<p>This detail will come at the planning permit stage. Some examples of bicycle parking typologies are provided in the traffic report</p>	<p>Cardno Traffic report.</p>
<u>Developer Contribution</u> Suggest contributing to upgrade or relocation of traffic signals	<p>ACU would like the opportunity to discuss this further with Council at the planning permit stage, if required.</p>	

Attachment 14 - ACU Development Plan - Applicant response to Referrals



ACU AMENDED DEVELOPMENT PLAN

REFERRAL RESPONSE

Heritage		
Height – 115B Proposed building will dominate southern end of Napier Street and Young Street streetscape Should be reduced so that is no visible, or not noticeably visible from Napier Street in a sightline above the Mary Glowrey building north east wind (i.e. the part abutting Little Victoria Street) A sightline should be taken southwards and to the south-west down Napier Street from the opposite kerb and the carriageway say at the mid-point between Gertrude Street and Little Victoria Street	The building height has been reduced, however not to the extent suggested by Council's heritage advice. There is a need to balance heritage with the land zoning and policies which support providing increased educational facilities. In our view, the amended proposal strikes the right balance between these policies, by providing reduced building height and increased setbacks to Napier Street and providing accommodation required by the university. Further, Bryce Raworth has reviewed the amended plans and supports the proposal.	Bryce Raworth report
Height - Addition to Mary Glowrey Building The additional levels are unacceptable as they will be similar in height to the Daniel Mannix building	The heritage advice by Bryce Raworth supports the proposed additions to the Mary Glowrey building which have been designed in a way to provide a clear distinction between the new building and the Mary Glowrey.	Bryce Raworth report
Young Street building – Sightline from Napier Street required to confirm view. Recognises better outcome than current car park.	The Young Street building has been reduced further and will have no impact on the heritage streetscape. The new high quality architectural building with landscaping areas is a vast improvement on the current multi deck car park.	N/A
Setback to Napier Street Should be increased to reflect the open garden streetscape of the existing residential heritage precinct Angled section of the elevation will impact on the sense of space and place Setback should be parallel to the property line and be in the same alignment as the eastern elevation of the north-east wing abutting Little Victoria Street.	The reduced height and amended setbacks to Napier Street improve the relationship of the proposed building to the Mary Glowrey building. We understand the angles section is supported by Council's Urban Design advisers and this will allow greater views through to the Mary Glowrey building.	Bryce Raworth report
Clarify whether building will cantilever over the east elevation	Yes the intention is to have a cantilever.	N/A

Attachment 14 - ACU Development Plan - Applicant response to Referrals



ACU AMENDED DEVELOPMENT PLAN

REFERRAL RESPONSE

Materials – requests a sample board and that timber and metal mesh or pre-corroded metal are not to be used	The materials detail has been provided which demonstrates example of high quality materials. The proposed materials are supported by Bryce Raworth heritage consultant.	Indicative Materials examples
Wind turbines – clarify and confirm that they will not be added to the building as adds additional height	There are no wind turbines shown on the plans and it is not intended to add them.	N/A
Street trees – should not be removed particularly the Dutch Elms trees as they are historically significant	The street trees are not covered by the Heritage Overlay, however in response to this issue the amount of tree removal has been reduced so that only the two tree that are planted on the corner of Victoria Parade and Napier Street are to be removed. The remaining trees, which are older and provide the Napier Street canopy, will be retained.	Amended Napier Street design. Landscape Master Plan

Attachment 15 - ACU Development Plan - Applicant response to Objections



ACU DEVELOPMENT PLAN OBJECTION RESPONSE

Community Issue	ACU Response	Reference Document
Campus site planning		
Inappropriate location for consolidation of ACU campus	115B is in a commercial zone (not residential), education is an as of right use.	N/A
Intentions for remaining ACU land unknown; residents would like to see a full master plan of all ACU sites proposed for the next 10-15 years	The DPO applies to only part of the campus. The DP documents refer to the campus vision over the next 10-15 years, and the vision documents are available on the ACU website.	Section 1.1 ACU Fitzroy of the Development Plan October 2016 (DP) shows the extent of the DPO.
ACU has a Master Plan that has not been disclosed	Campus Vision prepared and community invited to 3 x consultation sessions in November, which outlined ACU's vision for the campus. The documents are available on ACU's website. List of properties to notify provided by Council, letter drop also followed mail out.	Campus Vision Document is available on ACU website.
Timing for the Young Street car park to be closed	ACU development intent is for 115B, 81 Victoria Parade, and the Young Street site, as made clear on all the ACU documents.	Section 4.2 Staging of DP provides a staging diagram.
Query the accuracy of student number projections	Student numbers are based on Government figures for the education sector. Figures beyond 2020-2021 do not exist as they cannot be modelled in the rapidly changing tertiary education environment.	Page 1.5 ACU Vision, Expansion and Consolidations Plans of the DP provides projected student numbers.
Why is 81-89 Victoria Parade not included?	81-89 Victoria Parade is not covered by the DPO. This process is the statutory process required by the DPO that applies to only part of the ACU campus. Any future applications will be subject to a separate planning permit application process, including public notification.	The table on page 9 of the DP outlines staging explains that 81-89 Victoria Parade is subject to a separate process.
What is the cumulative effect of the ACU as it continues to buy property?	ACU is part of the community and will continue to be into the future.	N/A
Over Development		
Height Why should this building be allowed to be higher than the Mary Glowrey Building, and its set-backs?	The height has been reduced to 12 storeys in response to community feedback. The building height is driven by ACU's needs for education floor space, however the floor area has been reduced in light of the community comments.	Section 4.4 Built Form Plans of the DP shows the amended 115B design Lyons comparison package shows reduction in building

Attachment 15 - ACU Development Plan - Applicant response to Objections



ACU DEVELOPMENT PLAN

OBJECTION RESPONSE

Community Issue	ACU Response	Reference Document
	The additional floor space is required to consolidate the campus and provide improved teaching and learning spaces for students in much needed areas such as teaching and nursing. We believe the design takes into account the surrounding context and heritage and the location on Victoria Parade, in a commercial zone, an at-grade car park is an appropriate design response.	height. <i>Section 4.11 Heritage Matters</i> contains Bryce Raworth's letter dated 25 October 2016 assesses heritage issues.
What height limit applies?	There is no height control for the site, under the planning scheme. Residential properties are in a different zone (residential) which has a height limit. ACU is in a commercial zone and mixed use zone.	N/A
Setbacks	The lower level setbacks / splay have been deliberately designed to reveal the Mary Glowrey building. The upper level setbacks have increased in response to this issue. To Napier Street the setback have increased as follows: <ul style="list-style-type: none"> - Minimum setback of 17.1m tapering to 31.9m at Level 6 and 7 (previously 3.7m – 9.8m at Level 6 and 8.2m – 14.3m at Level 7) - Consistent 15.3m setback for Levels 8 – 11 (previously 8.2m – 14.3m at Level 8 and 9 and 11.82m to 17.85m at Level 10 – 13 noting that Level 12 and 13 have been deleted). 	<i>Section 4.4.3 Indicative Setbacks and Floor Layout</i> shows the setbacks of the amended 115B Design. Lyons comparison package shows increased setbacks.
Building footprint	The above ground floor area has been reduced by 2300 sq m in response to this issue. Both buildings have been reduced.	Lyons comparison package
Where are the open spaces?	Young Street building has been amended to provide additional open space. Increased from 635 sq m to 1205 sq m. Tree planting and green spaces are proposed throughout the campus.	<i>Section 4.4.11 28-42 Young Street</i> <i>Section 4.9 Overall Landscape and Open Space Plan and Principles</i> <i>Section 4.10 Landscape Master Plan</i>

Attachment 15 - ACU Development Plan - Applicant response to Objections



ACU DEVELOPMENT PLAN

OBJECTION RESPONSE

Community Issue	ACU Response	Reference Document
Height limit should be set for the Young Street building	A building envelope is included in the DP for the Young Street building and the height will be generally similar to the existing car park structure.	Section 4.4.11 28-42 Young Street Amended Young Street drawing
Heritage		
Design and appearance	Further detail has been provided with indicative appearance and material examples. This demonstrates the high quality architectural and material quality of the buildings.	Indicative Building Appearance Indicative Materials examples
Impacts on the Mary Glowrey building	The building height and setbacks have been reviewed and amended to reduce any impacts on the Mary Glowrey building.	Section 4.11 Heritage Matters contains Bryce Raworth's assessment of the revised proposal
Impacts on the broader heritage precinct	The building height has been reduced along with the need to balance heritage with the land zoning and policies which support providing increased educational facilities. In our view, the amended proposal strikes the right balance between these policies, by providing reduced building height and increased setbacks to Napier Street and providing accommodation required by the University.	Section 4.11 Heritage Matters contains Bryce Raworth's revised assessment
Retention of existing streetscapes and heritage buildings	There is no significant demolition proposed. The Mary Glowrey building will be refurbished and extended.	N/A
Yarra is considering that Mary Glowrey Building be referred to Heritage Victoria for listing as a building of heritage significance	ACU understands that Council is currently considering this and will respond at the appropriate time, most likely when the formal request is made to Heritage Victoria.	N/A
Traffic, parking and access		
Napier Street impacts	The Napier Design and Bicycle Access has been amended to address these issues following further consultation with Council's Traffic Engineers. The design will prevent cars travelling past the ACU site north into Napier Street, this will limit traffic impacts. The traffic impacts have been reviewed in detail in the accompanying traffic report.	Section 4.4.7 Proposed Access 115b Victoria Parade shows the amended Napier Street design Updated Integrated Transport Access Plan by Cardno
Young Street impacts	The proposal to close Young Street has been deleted from the	Updated diagrams throughout

Attachment 15 - ACU Development Plan - Applicant response to Objections



ACU DEVELOPMENT PLAN OBJECTION RESPONSE

Community Issue	ACU Response	Reference Document
	Development Plan in response to this issue.	DP.
Pedestrian safety	The footpath location in Napier Street has been reviewed and amended in consultation with Council's traffic engineers.	<i>Section 4.4.7 Proposed Access 115b Victoria Parade</i> Integrated Transport Access Plan by Cardno dated 27 October 2016
Cyclist safety	The raised bicycle footpath at the car park entry priorities cyclists over motorists. The proposed access point to the 115B site has been amended to prioritise cycling. Refer to traffic report for further detail.	<i>Section 4.4.7 Proposed Access 115b Victoria Parade</i> Integrated Transport Access Plan by Cardno dated 27 October 2016
Major cycling route in Napier Street The disruption to the existing bike route	The amended design provides a safe and direct bicycle route down Napier Street and will not disrupt the existing bicycle route.	<i>Section 4.4.7 Proposed Access 115b Victoria Parade</i> Integrated Transport Access Plan by Cardno dated 27 October 2016
Loss of trees	The amended proposed retains reduces the tree removal to only 2 trees located in the garden bed near the corner of Napier and Victoria Parade (previously 7 trees to be removed)	<i>Section 4.10 Landscape Master Plan</i>
Why is so much parking required?	The car parking is required to accommodate the eventual removal of the Young Street car park and the current at grade car parking provided at 115B.	Integrated Transport Access Plan by Cardno dated 27 October 2016
Will ACU consider putting the car park under the St Mary of the Cross Square, and removing parking from 115 Victoria Parade?	This is not part of the DPO or current DP. This is likely to have impacts on Yarra trams.	N/A
Loss of revenue from Council parking meters	The on- street permit parking on the east side of Napier Street will be retained. Revenue from parking meters is a matter for Council.	<i>Section 4.4.7 Proposed Access 115b Victoria Parade</i> Integrated Transport Access Plan by Cardno dated 27 October 2016
Needs to be a balance between car parking, bicycles, and pedestrians	Agree, the relatively low amount of car parking located towards Victoria Parade, will make up for the loss of parking from Young	<i>Section 4.6 Proposed Vehicle and Bicycle Spaces</i>

Attachment 15 - ACU Development Plan - Applicant response to Objections



ACU DEVELOPMENT PLAN

OBJECTION RESPONSE

Community Issue	ACU Response	Reference Document
	Street and the at grade parking. Bicycle parking provision has been increase to 80 (double the statutory requirement) in response to this issue.	Integrated Transport Access Plan by Cardno dated 27 October 2016
Construction impact on traffic and parking	The planning permit phase will request that a construction and traffic management plan be submitted for the construction phase	N/A
Bottleneck in Young Street due to car park and location of ACU loading dock	The removal of the Young Street car park in the long term will assist with this.	N/A
Privatisation of public space		
Air bridges	ACU is seeking to improve the public realm for the benefit of the community, not only its students. This is of course Council land and requires the relevant approvals from Council, some of these require additional further approval outside of the current planning permission sought. Therefore, the air bridges have been removed from the Development Plan in response to this issue.	The air bridges are no longer shown in the Development Plan.
Footpaths/streets	The Young Street closure has been removed from the Development Plan.	The reference to Young Street closure has been removed from the Development Plan.
Amenity Impacts		
Noise	The current application does not seek to change the existing use and is as of right in the Commercial Zone (115B), the proposal seeks approval for built form and car parking only	N/A
Privacy	Overlooking / screening is required where open space and windows are within 9m of an adjoining properties. All new windows will be further than this. ACU can discuss this with individual residential properties closest to the site at the detailed design stage.	N/A
Student behaviour	ACU understands that the main complaint is related to smoking. ACU adheres to the no-smoking policy that all Melbourne Universities abide by. Students should not smoke in the street, and ACU does not have any issue with Council Officers fining	N/A

Attachment 15 - ACU Development Plan - Applicant response to Objections



ACU DEVELOPMENT PLAN

OBJECTION RESPONSE

Community Issue	ACU Response	Reference Document
	students who smoke in spaces where it is illegal to do so.	
Overshadowing	The orientation of the land avoids shadow impacts to residential properties.	N/A
Head lights	The location of properties on the opposite side of the street and the design of the new basement entry which is splayed will prevent headlight spill, other than that usually caused by vehicles currently using the road.	N/A
Tree removal		
Unnecessary	The amount of tree removal has been reduced in response to this issue. Only 2 trees located in the garden bed near the corner of Napier and Victoria Parade (previously 7 trees to be removed).	Arborist report <i>Section 4.10 Landscape Master Plan</i> <i>Section 4.4.7 Proposed Access 115b Victoria Parade</i>
Will ACU replace any trees that are cut down?	Yes, the landscape plan shows new planting (5 trees) on site on the 115B site plus additional planting and tree within the new open space areas around the Young Street building.	<i>Section 4.10 Landscape Master Plan</i>
Infrastructure impact		
Drainage impacts	This will be addressed through standard permit conditions (following the approval of the DP)	N/A

Attachment 16 - ACU Development Plan - Referral Memo - Open Space, Streetscapes and Arboriculture



Memo

To: Mary Osman

Cc: Carrie Lindsay; Glen Williames

From: Christa Mitchell

Date: 1/12/2016

**Subject: 115 Victoria Parade and 28-42 Young Street, Fitzroy
(PLN16/0085)**

diverse

vibrant

exciting

inclusive

Attachment 16 - ACU Development Plan - Referral Memo - Open Space, Streetscapes and Arboriculture

We have received the updated Plans for the ACU development in Fitzroy (PLN16/0085) and provide the following feedback:

Arboriculture and Streetscape comment:

Referring to the provided arborist report on the existing trees and the Cardno Signage and Linemarking Plan:

- The proposed reconfiguration of the bike lanes along Napier Street has been changed in the updated plans; we support the reconfiguration to retain five existing *Ulmus Procera* street trees, removing only two *Quercus palustris* trees at the corner of Victoria and Napier Streets.

Trees

The updated submission includes built-form modelled perspective images, which provide further information of the development proposal. However in order to make a proper assessment of this application, the following require attention:

- Photographs of all trees to be removed should be included in report;
- Plan of proposed development and existing vegetation showing TPZ of all trees and encroachment is required;
- Shadow diagrams, particularly of the existing and proposed buildings in relation to the existing and proposed streetscape and landscaped areas;
- The proposed removal of trees numbered 12 and 13 is not supported, and there is insufficient detail supporting the removal of Trees 5 – 13;
- There are 3 trees proposed for removal on the western side of the carpark as shown on the ACU 4.10.8 Indicative Victoria Parade Entry Plan which are not shown in the Arborist report; and
- The street tree adjacent Trees 5 and 6 is not shown in the Arborist report

Open Space comment:

ACU forecourt

The reconfigured building at 28-42 Young Street and subsequent additional open space surrounding this building is supported.

- The reconfiguring of the 28-42 Young Street building and its orientation within the open space has resulted in increased open space and access to sunlight which is supported.
- The proposed tree and plant species are supported, however in order to fully assess the proposed planting plan, we require shadow diagrams of the newly configured building, to assess the impact upon proposed and existing trees.

Attachment 16 - ACU Development Plan - Referral Memo - Open Space, Streetscapes and Arboriculture

Lighting

- Proposed safety lighting along Little Napier Street remains within the proposal. It is no longer represented in the plans as catenary lighting, but it is not clear as to the type of lighting proposed as it is described as Laneway lighting (low level to improve safety). Further detail is required to show the type of lighting proposed to ensure that it does not negatively impact on the neighbouring residential properties, and is not located on public land (unless otherwise approved by Council).

Victoria Parade campus entry

- The removal of two *Quercus palustris* trees (numbered 12 & 13 in the arboricultural report) is not supported. These trees are performing well in restricted conditions. The proposed replacement *Zelkova serrata* trees will not be required if the existing trees are protected during construction.
- The proposed single large planter bed, replacing the three small planter beds will provide improved growing conditions to proposed trees and shrub plantings.
- More detail re. proposed planting and planters will be required for Council to provide a comprehensive review.

Summary of further details required:

- Shadow diagrams of the built form showing how overshadowing affects the open space and streetscapes is still required.
- More detail required on the proposed lighting to Young Street.
- More detail required on the proposed public space area on the corner of Napier Street and Victoria Parade.
- More detail required in the Arborist Report.

Please note: There are no open space capital works planned in the area.

If you require any further information, or would like to discuss these comments in more detail, please don't hesitate to contact me on 9426 1427.

Regards

Christa Mitchell

Open Space Planner

Open Space Planning and Design

Attachment 17 - ACU Development Plan - Proposed Traffic Management in Napier Street Fitzroy

From: Demosthenous, Nick
Sent: Tuesday, 29 November 2016 2:11 PM
To: Osman, Mary
Subject: RE: ACU Development - Proposed Traffic Management in Napier Street Fitzroy (TRIM REF: D16/158496)

Mary,

Further to comments made by Alistair and Mark, my comments are:

Infrastructure

Cardno plan (CG150178-TR-DG-2502 rev 4) on page 88 of the report indicates that the bike lane is a painted bike lane there are no notations indicating there will be kerb extensions/footpath widenings. However on page 29 of the report under referral response, the responses to various issues raised previously such as footpath widening and drainage suggest that it is planned to widen the footpath and various technical issues will be resolved at the detailed design stage. It needs to be clarified whether there will be kerb extensions/footpath widening or not. Footpath widening is likely to create reverse fall on the footpath which may be difficult to address to Council's satisfaction at the design stage. Typical cross-section across the road should be provided at critical locations to clearly show that the concept is practical. Dimensions should be provided on the typical sections.

If there are to be proposed kerb extensions/footpath widening on the west side of Napier Street from the development's vehicle crossing to Little Victoria Street There will an adverse effect on stormwater flow in the channel. The channel on the west side of Napier Street falls to the south. There is a Council stormwater drain under the west side channel that extends from Victoria Parade and stops approximately 20m south of Little Victoria Street This drain is a 225mm diameter pipe which is less than Council's current minimum pipe size standard of 300mm diameter. Therefore the existing stormwater drain from Victoria Parade will need to be reconstructed as a 300mm diameter concrete drain and extended to the north side of Victoria Lane.

The kerb and channel in Napier Street between Victoria Parade and Little Victoria Street is concrete kerb and channel. The location is within a heritage overlay. If a kerb extension/footpath widening is to be constructed the existing kerb and channel between Victoria Parade and the development's driveway will need to be removed as part of the drainage works and a new kerb and channel constructed on the new alignment. Council's current Infrastructure-Road Materials Policy requires that the kerb and channel be reconstructed as a bluestone kerb and channel. However in order to facilitate the bicycle lane the kerb and channel should be constructed as 300mm wide sawn bluestone. Similarly if the kerb and channel from the proposed vehicle crossing to Little Victoria Street is realigned it should be reconstructed as 300mm wide sawn bluestone.

The proposal shows a raised pavement across Napier at the intersection of Victoria Parade. On the east side of Napier street at the kerb extension the road falls towards Victoria Parade. There may be pooling of water against the raised pavement once it is built. Drainage or pavement adjustment on the east side may be required.

The proposed vehicle crossing into the development must have the footpath at grade with the footpath on each side in accordance with Council's standards. It should not be designed as an intersection. A spoon drain will be required across the vehicle crossing to carry channel flow.

Attachment 17 - ACU Development Plan - Proposed Traffic Management in Napier Street Fitzroy

The raised pavement ramps at both ends of the raised pavements are to be constructed using sawn bluestone pitchers cut in half and laid smooth side up. Bluestone pitchers are to be used for any kerb or kerb and channel construction.

The median island that to be constructed between the proposed driveway and Little Victoria Street will affect surface stormwater flow on the road pavement. Provision must be made to ensure surface stormwater does not pool along the edge of the island.

The median island and the left turn island should be constructed using sawn bluestone 300mm wide.

Traffic Movement

The turning movement diagrams in the Cardno report (Drawing No CG150178-TR-2502 Rev 4) are for a 99 percentile car. However clause 6.2.1 of the report refers to an increase in basement height to 2.5m to allow for small rigid vehicles. If vehicles larger than the 99 percentile car will use the basement carpark then the appropriate turning circles should be used to verify the on street traffic movement.

Victorian road law allows that you may cross a solid white line when entering or leaving a road or parking, it does not allow U-turns. Cardno should confirm that the proposed line marking will allow a car that mistakenly enters Napier Street with a view to proceeding up the street can legally turn around to exit via Victoria Parade. Similarly the legality of doing U-turns/3 point turns to the right from a lane marked with left turn arrows should be verified. Reflective pavement markers or other delineators should be considered for lane delineation.

At the proposed vehicle crossing the dashed line may encourage cars to stop at that point before entering Napier Street. Also the painted island across the footpath infers that the footpath is part of the driveway. These line markings may distract drivers from pausing at the edge of the footpath to check for pedestrians. Council's normal driveway standard does not use a dashed line or a painted island on the footpath.

The plan shows there is 4.3m between the 60 degree angle parking on the east side of Napier Street and the proposed median island north of the driveway. However it is noted that the median island at the driveway provides less space between it and the angle parking bays. The minimum reversing space specified in Australian standard 2890.5 -1993 for 60 degree angle parking is 4.3m. Reversing swept path for cars parking opposite this island should be shown to verify that the reversing manoeuvre can be achieved.

The width of the left turn lane from Napier Street to the driveway should be shown on the plan. It appears to be 2.8m at its widest narrowing to 2.6m adjacent to the left turn island. Council's Traffic unit should confirm these dimensions are satisfactory.

A "No Through Road" sign should be located in Victoria Parade and/or at the entry into Napier Street to discourage through traffic entering Napier Street.

Using Semi-mountable kerb in the left turn island and the splitter island will encourage illegal turns into the driveway.

The rumble strip at the edge of the bike lane will be noisy if vehicles drive on it. This may generate resident complaints.

Attachment 17 - ACU Development Plan - Proposed Traffic Management in Napier Street Fitzroy

Other Items

Tree removals should be referred to Council's Open Space Unit for comment.

Street Lighting Should be reviewed and upgraded to ensure it is in accordance with Council's standards. Comments should be sought from Council's Construction Management Officer.

Removal of the parking spaces along the west kerb should include removal of the line marking, removal or modification to parking signs and the removal of any parking ticket machines. This should be indicated on the plan These costs should be met by ACU. Comment and agreement should be obtained from Council's Parking Enforcement Unit.

A Road Safety Audit should be undertaken on the design and at the end of any roadworks and traffic management changes.

Regards,

Nick Demosthenous

Senior Project Engineer
City of Yarra, PO Box 168, Richmond 3121
T (03) 9205 5745 F (03) 8417 6666
E Nick.Demosthenous@yarracity.vic.gov.au
W www.yarracity.vic.gov.au

From: Osman, Mary

Sent: Wednesday, 16 November 2016 3:12 PM

To: Pisani, Mark <Mark.Pisani@yarracity.vic.gov.au>; McDonald, Alistair <Alistair.McDonald@yarracity.vic.gov.au>

Cc: Exon, Simon <Simon.Exon@yarracity.vic.gov.au>; Ghasperidis, John <John.Ghasperidis@yarracity.vic.gov.au>; Demosthenous, Nick <Nick.Demosthenous@yarracity.vic.gov.au>

Subject: ACU

Dear All

ACU have now formally lodged their amended plans can you please review the document (TRIM REF: D16/158496) and provide any comments to me within the next 2 weeks please.

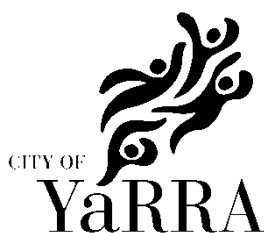
Mark/Alistair the plans look pretty much like the preliminary plans we saw a little while ago, but just want the final comments.

Thanks

Mary Osman

Manager Statutory Planning
City of Yarra PO Box 168 Richmond 3121
T (03) 9205 5334 F (03) 8417 6666
E Mary.Osman@yarracity.vic.gov.au
W www.yarracity.vic.gov.au

Attachment 18 - ACU Development Plan - Engineering Comments



MEMO

To: Mary Osman
From: Mark Pisani
Date: 24 November 2016
Subject: Application No: PLN16/0085
 Description: Amendment to Development Plan Overlay Schedule 2 (DPO2)
 Site Address: 115 Victoria Parade & 28-42 Young Street, Fitzroy

I refer to the above Amendment to the Development Plan and the accompanying *Integrated Transport and Access Plan* prepared by Cardno dated 27 October 2016 for the Australian Catholic University. Council's Engineering Services unit provides the following information:

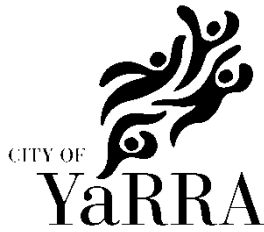
CAR PARK ENTRANCE AND NAPIER STREET

Item	Assessment
Access Arrangements	
Development Entrance	The entrance to the car park has a two-way width of 7.2 metres, which satisfies <i>Design standard 1 – Accessways</i> of Clause 52.06-8.
Vehicle Turning Movements via Napier Street	The swept path diagrams showing two B99 design vehicles entering and exiting the development entrance are considered satisfactory.
Visibility at Entrance	The previous revision of the Cardno drawing (Rev 3) had shown a sight triangle for the exit lane from the car park. This sight triangle should be maintained.
Vehicle Crossing	To be constructed to Council's Heritage Standard. The construction of the new vehicle crossing should be clear of any drainage pits.
Napier Street Works	
Channelised Treatment – Use of Semi Mountable Kerb	A conventional concrete semi-mountable kerb is not supported as this would allow vehicles to easily mount and potentially turn left into Napier Street. Since the works are located within a heritage overlay, Council's bluestone semi-mountable kerb is to be used. With Council's bluestone semi mountable kerb, cars are less likely to drive over it. The detailed design of the Napier Street works would be assessed and approved by Engineering Services.
Vehicle Reversing Movements out of Angle On-Street Parking Bays – East Side of Napier Street	Swept path diagrams should be prepared for cars exiting critical spaces (the angled spaces with the least amount of reversing room) on the east side of Napier Street, due to the island and separator.

Attachment 18 - ACU Development Plan - Engineering Comments

Regards

Mark Pisani
Senior Development Engineer
Engineering Services Unit

Attachment 19 - ACU Development Plan - Strategic Transport Planning Memo

MEMO

To:**From:** Alistair McDonald**Date:** 22/11/16

Subject: Application No:
Description: ACU
Site Address: Corner of Napier Street/Victoria Parade

I refer to the above Planning Application received on 16/11/16 in relation to the proposed development at ACU Victoria Parade, Fitzroy Council's Strategic Transport unit provides the following information:

Napier Street and carpark access

It's likely that there will be conflict with northbound bikes and vehicles at the new carpark entrance. It is not clear from a legal perspective which user has right of way. The 'Give Way to Cyclists' sign is not regulatory (and enforceable) and cyclists are required to give way to left turning vehicles that are indicating under the Victoria Road Rules. The developer should consider raising the carpark entrance and bike lane and/or converting to a 'priority crossing' with a give way sign at this location. This could help to reduce the legal ambiguity at the location between turning vehicles and cyclists.

The other comments I have are

- Additional traffic management works will be required to prevent cars from entering the one-way section including
 - Additional flared island at the northern side of the carpark entrance
 - Continuation of linemarking or a raised median island to the south of the carpark entrance
 - Additional one-way arrows within the traffic lane
 - Installed of "No Through Road" signage at Victoria
 - Dynamic parking vacancy indicator sign facing Victoria Parade
 - Swept paths to show that vehicles can't enter/exit the carpark access via the northern section of Napier
- Council's arborist has requested offset trees be planted for those lost at the Napier/Victoria intersection, which is compounded with the loss of trees within the ACU site

Due to the unusual arrangement at the site and the high vehicle numbers a Road Safety Audit would need to be provided to the satisfaction of Council officers.

Bike Share

Napier Street currently has a Melbourne RACV Bike Share pod on the corner of Napier Street/Victoria Parade. The proposal offers only a cursory indication that the pod will be replaced onto Victoria Street. There are no plans showing the specific location and its interactions with pedestrians on the footpath, existing above and below ground services or whether the operators approves. Based on previous experience from Council Officers it is very difficult in finding suitable locations. The permit should state that the station needs to be relocated to a similar high profile location to the satisfaction of Council and the RACV Bike Share operator.

Attachment 19 - ACU Development Plan - Strategic Transport Planning Memo

Bicycle Parking Provisions

Under the provisions of Clause 52.06-5 of the Yarra Planning Scheme, the development's parking requirements are as follows:

Proposed Use	Quantity/Size	Statutory Parking Rate	No. of Spaces Required
Student	700	1 space per 20 Students	35
Staff	50	1 space per dwelling 20 staff	3
Total			38 Spaces

The development exceeds the statutory bicycle parking requirements with a total of 80 bike parking spaces to be provided through the wider ACU site and not directly located within the DPO site. The applicant should provide information on the locations of these spaces to Council Officer's satisfaction and to meet Australian Standards as part of the planning permit requirements.

Regards
Alistair McDonald
Strategic Transport Engineer
Strategic Transport Unit

Attachment 20 - ACU Development Plan - Heritage Advice

City of Yarra

Heritage Advice - Supplementary

Application No.: Development Plan Overlay

Address of Property: 115 Victoria Parade, Fitzroy

Planner: Mary Osman

Yarra Planning Scheme References: Clauses 43.01 and 22.02-4, 22.02-5.7.

Heritage Overlay No.: HO334

Precinct: South Fitzroy.

Level of significance

No. 115 (115B) Victoria Parade, (Mary Glowrey Building) constructed 1929-1931, is listed as being "Individually significant". -
(Appendix 8, *City of Yarra Review of Heritage Overlay Areas 2007, Rev. Sep. 2015*)

It is important to remember that generally the building is intact to its original design, the principal change to its original fabric being the removal of all of the original steel-framed windows, some other minor alterations at ground level and to a section of the Young Street (west) elevation and glazed additions which are reversible as they have been constructed outside of the main masonry structure which remains intact behind. If the windows were reinstated and the additions were removed the building would be substantially as per its original appearance.

Neither of the car park sites is considered to be significant.

Revised Proposal

To construct a new 12-storey, plus plant, building on the car park site at the corner of Victoria Parade and Napier Street and to add 3 levels, including plant to the Mary Glowrey Building.

To demolish the existing car park at the corner of Young and Little Victoria Streets and to create an outdoor student hub and construct a 3 storey building.

Scope of Advice

Advice has been provided previously in relation to the application relating to the DPO. This advice is intended to be read in conjunction with that advice and for that reason many of the previous points have not been repeated here even though they may remain relevant.

Specifically, insofar as heritage is concerned, I have been asked to comment on:

Reduction in height of Napier Street building from 14 storey plus plant to 12 storey plus plant and increased setbacks (corner of Napier Street on the existing at-grade car park) with basement car park accessed from Napier Street;

... removal of 2 trees and modified design to improve bicycle priority and prevent vehicles from travelling north past ACU;

A 4 storey plus plant addition to the rear Mary Glowrey building (Napier Street frontage) – NO change from previously advertised.

Attachment 20 - ACU Development Plan - Heritage Advice

Replacement of the existing Young Street car park with a new 3 level, triangular shaped building with basements located within a landscaped park. (please note the ACU still wish to pursue the Young Street car park (approved under a separate DP amendment) in the short term.

Deletion of the proposed air bridges.

Deletion of the proposed Young Street Road closure; and

New trees along street frontages and landscaping and lighting throughout the campus. No substantial change from previously advertised.

Drawing Numbers

Australian Catholic University. St Patrick's Campus. Development Plan. October 2016 with Council date stamp 28 Oct 2016. Contains revised transport information *inter alia* specifically referencing Napier Street, prepared by Cardno and a tree survey prepared by John Patrick Pty Ltd.

ACU – 115B + Young St Massing Comparisons. A4 sheets with Council date stamp 28 Oct 2016.

Heritage assessment, prepared by Bryce Raworth dated 25 October, 2016 included in St Patrick's Campus Development Plan and separately with Council date stamp 28 Oct 2016.

Context

The context of the Victoria Parade, Napier and Brunswick Street interfaces within the context of HO334 has been discussed in the previous heritage advice.

Assessment of Proposed Works

The proposal has been considered in relation to Yarra's Heritage Policy (Cl. 22.02).

Proposed works

Built form (height/setbacks)

Height

It is proposed now to construct a new 12 storey building plus a rooftop plant area on the car park at the corner of Victoria Parade and Napier Street.

The Development Plan recognizes the need to locate large scale development along Victoria Parade and to transition the height "down to the north to assist in minimising the impact of bulk on the sensitive residential uses to the north of the campus". (p. 13) It is not only the residential uses which are sensitive – the one- and two-storey heritage buildings and streetscape are even more sensitive in terms of impact.

The diagram on p.37 of the Development Plan clearly shows that the greatest height, i.e. 59.45 metres in Napier Street, will be focussed at the Victoria Parade/Napier Street corner. Along Young Street the height will be 43.228 metres. The south section of Young Street is less sensitive principally because of the existing impact of the Mannix Building and the St Vincent's car park. The effect of the proposal is clearly shown in the massing diagrams on pp. 38 – 42. In comparison to the Mary Glowrey Building the proposed height will be more than double the height of the north wing and will be similar to the height Mary Glowrey west. A better heritage outcome would be to take advantage of the height of the Mannix Building and the higher built-forms along Victoria Parade west of Brunswick Street and the under-

Attachment 20 - ACU Development Plan - Heritage Advice

developed site on the corner of Victoria Parade, where the impact of height would be more responsive to those contexts. The built-form diagrams on p. 23 and photomontages on p. 24 of the Development Plan show that east of Napier Street, Victoria Parade is almost exclusively double-storey, whereas westwards to Nicholson Street there is greater height **and bulk principally due to the St Vincent's Hospital buildings. A more balanced distribution of height across the St Patrick's Campus would result in a better heritage and a streetscape outcome while enabling the ACU to have a "signature" building along Victoria Parade.** The reduction in height has done nothing to reduce the impacts of bulk in relation to the residential uses and heritage nature of the area (South Fitzroy Precinct HO 334) north of the campus in Napier and Little Victoria Streets in particular and also Young Street.

I note that the site at Nos. 81 - 89 Victoria Parade, on the corner of Brunswick Street, is proposed to be redeveloped 2020 - 2025, and it is proposed to accommodate 12,500 m² gross floor area whereas the 115B Victoria Road site is proposed to have 25,000m². Whether this is due to differences in floor plates or height has not been indicated. (Development Plan, p.9) This site is not subject to the Development Plan (p. 16) and therefore it is unfortunate that it is not part of the equation at this time as a more appropriate outcome could be achieved if more aspects were factored into the proposal at this time..

It is evident from the revised Development Plan (p. 30) that the proposed building form has been considered still overwhelmingly in the context of Victoria Parade with insufficient weight being given to the impacts on Napier and Little Victoria Streets and HO334. The new built form will dominate the southern end of the Napier Street and Little Victoria Street streetscape and that of Young Street. As stated previously, any new built form on the car park site should not be visible, or not noticeably visible, from Napier Street in a sightline above the Mary Glowrey Building north wing. A guideline would be that any new building on the car park site should not be higher than the Mannix Building.

I would also be useful to receive some information as to any likely impacts there might be on views of the new wing in relation to the spires **of St Patrick's Cathedral when viewed from** Gisborne Street. Refer to previous comments regarding wind turbines - I note that these do not appear on the current drawings.)

Setbacks

The two heritage concerns with the proposed setbacks are: the depth of the setbacks and the abstract angled form which is not parallel to the property line and is out-of-keeping with the typical pattern of the streetscape.

The proposed façade to Napier Street is an abstract shape and setbacks vary. The minimum setback at the south end is 3.1 metres which is slightly forward of the east elevation of the north wing of the Mary Glowrey Building. Further north the setbacks range from 5.8 metres to 11.6 metres. However while this might leave some open space immediately south of the north wing and might also keep the new façade back from the typical setback of the dwellings to the north in Napier Street, it is negated by the zero setback at Levels 01 - 03 and the minimal setback of 3.5 metres at Levels 04 - 05 (without the open cutbacks of the ground level). The stack of blocks at oblique angles is not an appropriate response to the characteristics of the heritage streetscape which must be respected. Either all of the setbacks should be parallel to the Napier Street property line and in accord with the setbacks of the dwellings or else, if angled, the upper levels should not come further forward than the setbacks of the Ground levels proposed on p. 35 of the Development Plan. In summary, deeper setbacks may compensate for angled forms without resulting in any adverse effect on the heritage significance of Napier Street.

Attachment 20 - ACU Development Plan - Heritage Advice

Additions to the Mary Glowrey Building

My previous advice that the effect on the immediate environs will be similar to that of the Daniel Mannix building in terms of spatial impacts on the dwellings on the west side of Napier Street remains unchanged.

Young Street Building

This is now triangular in shape. Because of the relatively secluded and location and immediate non-heritage context in Young Street the shape is probably acceptable. However, as advised previously, from Napier Street it appears out-of-proportion with the double and single storey dwellings on the west side of Napier Street but this might be rectified with some adjustments including height and materiality. It appears that it would be visible in a standard sightline from Napier Street. In any event a new building would be a better addition to the heritage streetscapes than the existing car park and concrete wall to Little Napier Street.

Airbridge and Canopy

Deletion of the air bridges is supported.

The new canopy from the Mary Glowrey building around and into Young Street is acceptable.

Colours/materials/Signage

These appear to be unchanged from the previous proposal and my earlier comments are still applicable.

It appears that a large orange north-facing sign is now proposed near the car park entry from Napier Street – is this the same sign as proposed for Victoria Parade?. The Development Plan clearly indicates that the focus is on Victoria Parade so the reason for such a large and visually intrusive sign in Napier Street is not evident and this needs clarification. In any event such a sign would not be in-keeping with the heritage streetscape of Napier Street and should be deleted.

Trees

On the west side of Napier Street, between Victoria Parade and Little Victoria Street are *Ulmus sp.* (Dutch or English elms) of a good size (height and trunk circumference) and which appear to have been planted in accord with the tree layout shown on the MMBW detail plan, 1207, City of Fitzroy. 1899. I note that none of the elms are proposed to be removed as part of the Development Plan and that principally the only street trees proposed to be removed are Pinoaks and Silver birches, which were planted by the ACU. This is acceptable. However, removal of any elms during the construction phase to provide access and the like should not occur. Tree protection zones should be set out and maintained.

It is proposed to plant new trees along street frontages and to improve landscaping and lighting throughout the campus. There is no heritage concern with landscape improvements and additional trees. There is no issue with lighting other than as it might be a disturbance to the surrounding area at nighttime.

Vehicle and Bicycle Access and Historical Urban Infrastructure.

The vehicle access from Napier Street to the car park will be south of Little Victoria Street and with access from Victoria Parade to further north in Napier Street being prevented other than for bicycles, as Napier Street is a cycle route already. Given the activities of the City of Fitzroy in blocking off streets to through traffic as a response to community requests which

Attachment 20 - ACU Development Plan - Heritage Advice

has resulted in a better setting for the heritage buildings and streetscapes it is critical that that Napier Street is not opened up to higher through traffic volumes in the future.

A green cycle path is proposed to be installed on the west side of the Napier Street carriageway. The original Batemans Patent Melbourne Waterworks plate in the carriageway, which is part of the original water supply system to the area, is a significant element of historical urban infrastructure should not be removed or covered over.

Given that the St Vincent's Hospital staff car park and the existing ACU and Young Street car parks are accessed from Victoria Parade it preferred that vehicle access is from Victoria Parade and that the car park entrance is becomes an integral part of the street edge design of any new building.

Recommendation / Comments:

This advice has focussed specifically on the new issues as a consequence of some revisions made to the development Plan. Generally, unless stated to the contrary otherwise, the changes are insufficient to satisfy the heritage concerns expressed in the previous advice and they remain valid.

Signed:

A handwritten signature in black ink, appearing to read 'Riddett', with a stylized flourish at the end.

Robyn Riddett

Director – Anthemion Consultancies

Date: 2 December, 2016

Attachment 21 - ACU Development Plan - Urban Design Assessment Memo



urban design memo

to:	Mary Osman	date:	5 th December 2016
company:	Yarra City Council	from:	Hansen Urban Design
re:	Australian Catholic University – Urban Design Advice		

Further to our on-going peer review role, Hansen Partnership is pleased to provide the following final Urban Design Advice for works pertaining to the **Australian Catholic University's (ACU) - St Patrick's Campus**, based on the October 2016 Development Plan 'package' and associated massing comparisons.

We trust the following information will provide critical urban design insight into both the development plan and the proposed built works for the St Patrick's Campus.

If you have any questions please don't hesitate to contact us for further discussions.



site location

Attachment 21 - ACU Development Plan - Urban Design Assessment Memo



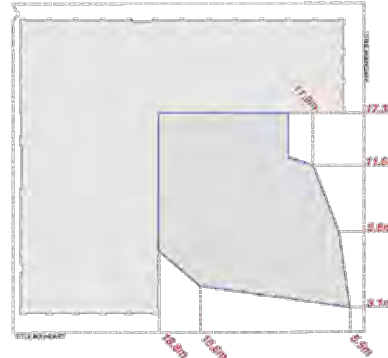
URBAN DESIGN MATTERS

The following is an urban design assessment of the Australian Catholic University – St Patrick's Campus Development Plan dated October 2016 and the associated drawing package submitted by built form massing comparison, prepared by Lyons Architecture. The advice follows an on-going independent peer review role which has involved numerous meetings with the applicant's representatives to discuss and refine the proposed design.

Site Planning:

Generally the proposal is well laid out in response to the inherent opportunities and constraints of the varied built form and movement network of the existing campus and its immediate and surrounding context.

- We support the consolidated entry between the Mary Glowrey building and the new 115B Victoria Parade building which provides a shared entry between the new and the old.
- The circulation within the building on the ground floor is sufficient, the lift core and stair arrangement is legible and the internal space is welcoming and generous.
- The highly glazed and transparent ground plain to both Victoria Parade and 'wrapping' around the corner into the southern portion of Napier Street is supported as it provides an activated and inviting public frontage to the campus, with opportunities for flexible spaces and clear sightlines into the building and through to retained elements of the Mary Glowrey building.
- The sky bridges (across Young Street) are no longer proposed and we strongly support their removal.
- The proposed setback arrangement to street level from Napier Street is encouraged due to the proximity to more fine grain streetscape frontages on the eastern side of Napier Street.
- The provision for new open spaces (at 28-42 Young Street) in the centre of the campus is strongly encouraged as this will create a network of open spaces that are well surveyed by the surrounding existing and proposed campus buildings. Further, the provision of a new central and sunken 'hub' building set within new 'wedges' of open space will provide additional pedestrian movements across the campus while providing an appropriate level of activation to the Little Napier Street (laneway) interface. This represents a more integrated and outwardly orientated 'gesture' than the previous design, in relation to Little Napier Street.
- The raising of the currently sunken laneway to the north side of 115B is encouraged to enhance connectivity and pedestrian links.
- The new access to the proposed underground carpark beneath 115B Victoria Parade (off Napier Street) is sufficient and enables an active frontage along the whole of the Victoria Parade frontage.



Ground floor setbacks to Victoria Pde and Napier St



Highly glazed frontage to Victoria Pde and Napier St



New Campus hub and wedge shaped campus greens to either side

Attachment 21 - ACU Development Plan - Urban Design Assessment Memo

**Massing, Scale and Grain:**

The proposal rises to an overall height of 12 storeys has responded to initial urban design comments and has reduced in both overall height as well as increased setbacks to upper levels and revised massing arrangements to result in a more considered and simplified presentation. While still a large and bulky form the proposal now responds more appropriately to its context. In particular the built form transition to Napier Street has been simplified in terms of the number of 'steps', while the setbacks to the upper levels have been increased.

- The revised built form at 115B now presents a clear 3 part massing response to Napier Street. This reflects our initial comments and on-going discussion in relation to providing a more sensitive built form transition to the existing low scale and fine-grain built form to the east. As demonstrated by the massing comparisons the proposal now presents a clear distinction between the low (streetwall) levels and the rising form. Further the proposed streetwall to Napier Street as a clear relationship with the existing streetwall height of the north wing of the Mary Glowrey building. Further, the provision of materials and architectural expression assist reducing the overall scale and massing of the building.
- However, we are concerned about the volume of the 'bridging' element as it transverse between the proposed 115B form and the addition atop the north wing. While, we appreciate and support this element and its dark, recessive treatment we consider that some recession or sculpting in the form to create a central vertical rebate is required to reference the space 'break' between the new 115B and south retained façade of the northern wing.
- The revision to the height and massing to 115B are supported, as the proposal no longer overwhelms the Mary Glowrey building. While the 12 storey form does considerably exceed the height of the existing built form it now presents as a less dominant form which in oblique views (from within Victoria Parade) will be visible above the Mary Glowrey profile but as a separate and recessive upper element. Further, the revised massing strategy, introduces a 2 level horizontal rebate (negative level/s) helps to create the impression of a 'floating' upper form emphasised through the use of darker finishes and the creation over a shadow-line cast by upper levels. This architectural treatment also clearly 'breaks up' the proposed massing, clearly distinguishing the lower streetscape element from the upper levels.
- The chamfered edge of the building form to reveal the flanks of the Mary Glowrey Building is supported, as it clearly reveals the contrast between heritage and new elements.
- The revised massing strategy for 115B has also resulted in the creation of a more success 'bridging' element which integrates the proposed new building and additions together as one cohesive form. However, we note that the extent of the 'piggy back' arrangement now pushes further to the north and cantilevers over the existing northern façade of the Mary Glowrey (north wing) and this element is also now supported by external columns which project forward to the existing façade. This results in the visual dominance of the upper level additions above the existing façade. This is further emphasised by the dark finish which will further add to the prominence of this addition, creating a 'top-heavy' arrangement which is to be supported by visible external additions. We therefore, recommend that the visual prominence of the additional built form be reduced and that the external columns be removed.



Previous massing arrangement atop the north wing



Current massing arrangement at the north wing



Recommended sculpting of bridging element



Cantilevered element atop north wing and external columns (emphasised)

Attachment 21 - ACU Development Plan - Urban Design Assessment Memo



- We believe that any addition atop an existing built form should rest comfortably as a new element perched on top and therefore, should not require new visible supports to do so. In short, we consider the previous height and massing proposed above the north wing to be a more appropriate massing.
- The massing and scale of the revised Young Street Building (Campus Hub) is encouraged. The central and triangular shaping of the built form to create an open space that will be well surveyed by the surrounding buildings is thoughtful and responds well to the existing scale of the internal spaces of the University. While, this new 'Hub' building is proposed to rise to approximately 16.2m (4-5m higher than the existing multi-level car park) it is to be setback between 0m and 4.4m from Little Napier Street. While, we support the response, and acknowledge it will be a superior outcome for the site and a vast improvement over the existing condition, we seek more information in relation to the massing arrangement (particularly to the south end of Little Napier Street) and comparison to the impacts of the existing blank concrete wall.
- Therefore, we support this response in an overall sense, but seek clarification that the height and massing results in an appropriate response to the rear POS of abutting neighbourhood residential properties which front Napier Street. Ideally the visual bulk and overshadowing impacts should be the equivalent to the existing car park structure.



Photo existing car park interface looking south along Little Napier Street



Proposed presentation to Little Napier Street, looking south

Public Realm Layout:

- The presentation and new 'front door' of the ACU campus to Victoria Parade and southern portion of Napier Street is supported. It appears and a highly glazed and activated façade, which contrasts with the abutting robust presentation of the Mary Glowrey building. Further, it offer views into the lower levels and through to the lift cores, providing a clearly legible entrance to first time visitors.
- The proposal for public spaces (Campus greens) to either side of the new Young Street Building (Campus Hub) is a strong addition to the public realm within ACU and the broader local movement network. The siting of these public spaces enables a network of pedestrian links throughout the campus. Further, their 'wedge' shape and the inclusion of circular light wells will provide a dynamic interplay between the 'sunken' levels of the hub and new passive open space at street level.
- The thin wedge of space, between the Campus Hub and existing Little Napier Street is an interesting interface, and needs to be treated appropriately. Presently this interface is a clear 'back of house' condition given the 'sheer' blank concrete wall presentation of the multi-level car park. However, while the triangular Campus Hub and associated 'wedge' greens are strongly supported and clearly a vast improvement on the existing condition an appropriate rear interface conditions is needed, given the current rear laneway nature of Little Napier Street. Therefore, we seek further information into the intended nature and use of this small public space between the Hub building and property boundary.
- We support the elevation of the existing sunken walkway along Little Victoria Street.
- The proposed canopy extending between building 115B and the Mary Glowrey Building is encouraged. This will provide shelter and enhance the legibility of the entrance as well unifying the two buildings.
- A more detailed depiction of landscaping and public seating/ informal seating is encouraged in order to better understand the public realm propositions.

Access and Mobility:

- We support the restriction of vehicular access to the intersection of Victoria Parade and Napier Street.
 - The provision for carparks, turning circles, pedestrian egress and ingress appears to be sufficient.
- We support the inclusion of the two 'wedge' campus greens and their associated desire lines within the centre of the campus and their role in encouraging through campus pedestrian movements resulting in a more permeable local movement network.

Attachment 21 - ACU Development Plan - Urban Design Assessment Memo



Summary:

We are generally supportive of the October Development Plan 'package' and acknowledge that the majority of our initial comments have now been addressed through the revised built form massing and inclusion of more information in relation to architectural treatments and materials.

However, we consider that some further refinement is required as follows:

- The 'bridging' element which traverses the built form requires the inclusion of a sculpted central rebate in order to 'break up' its massing and appropriately articulate the space between new 115B form and the existing southern façade of the north wing of the Mary Glowery;
- The arrangement of the additional levels at the north wing should be refined, in order to be more consistent with the previous massing arrangement to Little Victoria Street; and
- The massing and setback arrangements of the proposed 'Campus hub' should result in equivalent off-site amenity impacts on the residential properties fronting Napier Street.

We hope this assists Council in formulating a position on the proposed development. Should you have any further enquiries, please don't hesitate to contact us on 9664 9838.

Yours faithfully,
urban design team

05/12/2016

11.2 Council Submission on State Government Fast-track Amendment Affecting 35 Johnston Street and 50 Perry Street, Collingwood (Collingwood Arts Precinct)

Executive Summary

Purpose

To seek Council's endorsement of a submission on the proposed Planning Scheme Amendment changes requested by Creative Victoria for the former Collingwood TAFE site at 35 Johnston Street and 50 Perry Street, which is now known as the Collingwood Arts Precinct.

Key Issues

The Amendment is being undertaken by the Fast Track Government Land Service (FTGL Service). This submission will be considered by the Government Land Standing Advisory Committee (GLSAC), who provides independent advice on the planning proposal to the Minister for Planning.

The Minister for Planning has referred the proposal to the GLSAC to assess and make recommendations on the proposed amendment. Relevant stakeholders and authorities, including Council, have been invited to make a submission to the Advisory Committee.

Creative Victoria are proposing to rezone the site to a Special Use Zone (SUZ) which provides site specific planning controls that will enable the site to transition towards becoming a creative arts precinct. The Amendment proposes the following changes to the Yarra Planning Scheme:

- Rezone the site from Mixed Use Zone (MUZ) to Special Use Zone (SUZ);
- Insert a new schedule to the SUZ (that specifies land use and built form provisions under the SUZ);
- Insert a new schedule to the Car Parking Overlay to provide a waiver of the car parking requirements for most uses on the site;
- Insert a new schedule to Clause 52.43 (Live Music and Entertainment Noise) that identifies the site as an established live music venue;
- Retains the existing Heritage and Environmental Audit Overlays on the site; and
- Removes the current Design and Development Overlay (DDO12) for the site.

Officers have reviewed the proposed amendment and sought legal and technical advice to inform Council's submission to the amendment process.

Financial Implications

The seeking of legal and technical advice is covered by the Governance and Strategic Planning budgets.

PROPOSAL

That Council endorse the submission put forward by officers to be submitted to the Government Land Standing Advisory Committee process for the site.

11.2 Council Submission on State Government Fast-track Amendment Affecting 35 Johnston Street and 50 Perry Street, Collingwood (Collingwood Arts Precinct)

Trim Record Number: D17/23997

Responsible Officer: Manager City Strategy

Purpose

1. To seek Council's endorsement of a submission on the proposed Planning Scheme Amendment changes requested by Creative Victoria for the former Collingwood TAFE site at 35 Johnston Street and 50 Perry Street, which is now known as the Collingwood Arts Precinct.
2. This report explains the proposed planning controls and provides Council with the opportunity to make a submission to the GLSAC and to present the issues outlined in the submission to an Advisory Committee hearing in April.

Background

3. The Minister for Planning has referred the proposal to the GLSAC to assess and make recommendations on the proposed amendment. Relevant stakeholders and authorities, including Council, have been invited to make a submission to the Advisory Committee.
4. The Amendment is being undertaken by the Fast Track Government Land Service (FTGL Service). This submission will be considered by the Government Land Standing Advisory Committee (GLSAC), which provides independent advice on the planning proposal to the Minister for Planning
5. The land at 35 Johnston Street and 50 Perry Street, Collingwood is owned and managed by Creative Victoria (Department of Economic Development, Jobs, Transport and Resources) and is being established as a creative arts precinct, now referred to as the *Collingwood Arts Precinct*.

The Subject Site

6. The site is currently zoned Mixed Use Zone (MUZ) and was previously zoned Public Use Zone 2 (Education). Amendment C103 to the Yarra Planning Scheme (gazetted in 2011) included the site in the Mixed Use Zone after it was declared surplus to government needs, to enable it to be sold. The site is now proposed to be retained in public ownership.
7. Creative Victoria took over the management of the site in 2010 to build a new home for Circus Oz and develop the site as a creative arts precinct.
8. The activity occurring on the site was acknowledged in the Johnston Street Local Area Plan (Precinct 6) which provides strategic support for the site to be used for arts and creative industries, as well as educational and institutional uses across the broader precinct in which it is located.

Current Planning Controls

9. The current planning controls for the site are as follows:
 - (a) Mixed Use Zone;
 - (b) Design Development Overlay (Schedule 12);
 - (c) Environmental Audit Overlay; and
 - (d) Heritage Overlay(s).
10. It is proposed to retain the current Heritage Overlays on the site (which apply to the prominent buildings on the site which form the former Collingwood Technical School and the Keith Haring mural), as well as the Environmental Audit Overlay.

The Proposed Amendment

11. The Amendment documents proposed by Creative Victoria are found as **Attachment 1** and a fact sheet and planning report accompanying the Amendment are found as **Attachment 2**. The supporting technical reports are found as **Attachment 3**.
12. The Amendment proposes the following changes to the Yarra Planning Scheme:
 - (a) rezone the site from Mixed Use Zone (MUZ) to Special Use Zone (SUZ);
 - (b) insert a new schedule to the SUZ (that specifies land use and built form provisions under the SUZ);
 - (c) insert a new schedule to the Car Parking Overlay to provide a waiver of the car parking requirements on the site;
 - (d) insert a new schedule to Clause 52.43 (Live Music and Entertainment Noise) that identifies the site as an established live music venue;
 - (e) retains the existing Heritage and Environmental Audit Overlays on the site; and
 - (f) removes the current Design and Development Overlay (DDO12) for the site.

Proposed Schedule to the Special Use Zone

13. The Special Use Zone allows a site specific planning control that is tailored towards supporting and encouraging the proposed community-based, artistic and creative activities on the site, whilst managing other issues such as noise management under the SUZ schedule.
14. The proposed Schedule has been drafted to allow a number of uses as “permit not required” by listing them under Section 1 within the schedule. This means that they would not be assessed at any stage. Some uses have a condition that they must be associated with creative and artistic uses on the site.
15. Planning applications for Section 2 uses would be assessed by Council as the Responsible Authority. Notice of application requirements and third party rights would still apply.
16. The Schedule addresses the potential for noise management issues on the site with specific requirements that ensure that activities are limited to specific noise levels, measured in accordance with SEPP N-1 and SEPP N-2 criteria (State Environment Protection Policy).
17. The Schedule also provides for the creation of a site Masterplan that would be approved by the Responsible Authority in order to create a framework with objectives for the site, as well specifying how the site would be used and/or developed in the future, in accordance with that Masterplan.

Schedule to the Car Parking Overlay

18. The proposed schedule to the Car Parking Overlay exempts some land uses on the site from the standard requirements of Clause 52.06, effectively requiring zero parking for Section 1 uses, whilst for Section 2 uses lower parking rates would apply by using the lower Column B rate in Table 1 of Clause 52.06.

Schedule to Clause 52.43 – Live Music and Entertainment Noise

19. Noise management has been addressed through requirements outlined in the Schedule to the SUZ, as well as identifying the site in the schedule to Clause 52.43 (Live Music and Entertainment Noise), ensuring that the requirements of the schedule apply to the site.
20. Noise sensitive uses within 50m of the site would need to ensure that they are designed to protect inhabitants from external noise from live music and entertainment venues.

Deletion of DDO12

21. DDO12 currently applies to the site and was introduced when the site was rezoned to the Mixed Use Zone in 2011. The DDO simply provides design objectives for the site (no height limits) and identifies part of the site as suitable for an open space opportunity. DDO12 was applied when it was thought that the site would be sold to a private developer for residential/mixed use development. The requirements of the DDO are not suited to the current and future intended use of the site.

Summary of Proposed Controls

22. In summary, the suite of proposed planning controls achieves the following for the site:
 - (a) allows a number of activities on the site that would not require a planning permit and are not subject to any conditions;
 - (b) exempts the site from the standard car parking requirements as listed under Clause 52.06, requiring zero car-parking on the site for Section 1 (permit not required) uses; and
 - (c) manages noise through specific requirements within the Schedule to the SUZ, as well as Clause 52.43, which require noise levels to be kept below specific levels in accordance with SEPP N-1 and SEPP N-2 criteria.

Planning Permit Application (PLN15/1032)

23. Creative Victoria applied for a planning permit in 2015 that would establish a number of activities associated with the desired future use of the site as an arts and creative precinct.
24. An updated planning permit application was lodged in 2016.
25. The permit would allow:
 - (a) *use of land as a place of assembly (for the purposes of a hall, exhibition centre, function centre, cinema and carnival), an education centre (that includes educational talks, lectures and panel discussions), a dancing school and an art and craft centre, including a reduction in the car parking and loading bay requirements of the Yarra Planning Scheme.*
26. The Minister “called in” the application at the request of the Minister for Creative Arts as well as Council, who resolved at its meeting on 20 December 2016, to request that the Minister determine the application.
27. The application has been considered by a Planning Panel and Council officers are now waiting for the Panel’s report, which is due in a number of weeks (usually within 6 weeks).

Planning Permit Application for 23-33 Johnston Street (PLN16/0845)

28. A planning permit application process is currently underway for 23-33 Johnston Street, which proposes a twelve storey apartment building at the north-western interface of the CAP site.
29. In terms of that application, the “agent of change” principle (which Clause 52.43 applies) requires that a new noise sensitive use must be designed to protect itself against existing noise sources. Conversely, the activities occurring on the CAP site must be designed and managed to achieve noise criteria specified in the Schedule to the SUZ and Clause 52.43.

Response to the Amendment proposal and Council’s submission

30. It is considered that the Special Use Zone (SUZ) is the most appropriate zone to use for the site in order to recognise the state significance of the site and to facilitate the desired activity on the site, whilst managing noise and interface issues.
31. Council officers have sought legal and technical advice on the proposed amendment and have identified a number of aspects of the amendment that should be addressed (improved) to ensure some level of control, or accountability, in managing the appropriateness of activities occurring on the site:

Uses listed under the Schedule to the SUZ

- (a) The list of “permit not required” uses in Section 1 is extensive. Uses that are not considered core activities (as identified in the Purpose to the Schedule) should either be transferred to Section 2 and/or have conditions that relate the activity with the purpose of the zone.

Requirement for a Masterplan

- (b) The requirement for a Masterplan to be prepared, should be strengthened to ensure that activities that are peripheral or associated uses (to the intended core activities on the site), and buildings and works, are in accordance with an approved Masterplan for the site.
- (c) A number of Section 1 uses should be conditional to be in accordance with an approved Masterplan (as outlined in the track changes at **Attachment 4**).

Parking Overlay

- 32. Minor changes are suggested to be made to the schedule to the Parking Overlay to reflect the intention expressed in the amendment material, based on the advice of the traffic consultant engaged by Council officers (Traffix Group) to review both the technical report and Parking Overlay forming parts of the proposed.
- 33. The suggested changes to the amendment are identified as track changed versions at **Attachments 4** and **5**. This would form part of Council's submission at the Advisory Committee hearing.
- 34. The covering letter to Council's submission is found as **Attachment 6**.

External Consultation

- 35. The amendment process is a state government run process and the exhibition period is from 13 February to 24 March.
- 36. The GLSAC held an information session on site on the 1st March and two Council officers attended the session. Some local residents were also in attendance at the session. Key issues related to:
 - (a) concerns about noise management; and
 - (b) the ability to object to planning permit applications for uses listed under Section 1 of the schedule to the zone.

Internal Consultation (One Yarra)

- 37. Council's Strategic Planning Unit is taking the lead in responding to the amendment with the assistance of the Statutory Planning Unit which has had close involvement in the process.

Financial Implications

- 38. There have been some financial implications in assessing and responding to the proposed amendment, namely obtaining legal and technical expert advice on aspects of the amendment.

Economic Implications

- 39. The proposal has positive implications for the subject site and the broader area in terms of creating a significant creative arts precinct that will help to foster Yarra's existing creative arts industries which form an important element in Yarra's economy.

Sustainability Implications

- 40. The proposed Car Parking Overlay encourages sustainable transport use.

Social Implications

- 41. The proposal has positive social implications related to the creative and social activities occurring on the site.

Human Rights Implications

42. There are no known human rights implications associated with the proposed amendment, nor Council's submission.

Communications with CALD Communities Implications

43. The state government has managed the process of consultation and information has been available on Council's web site.

Council Plan, Strategy and Policy Implications

44. Council's response to the proposed amendment is in accordance with the following objective outlined in the Council Plan:
- (a) *Supporting Yarra's community.*

Legal Implications

45. Council officers have sought legal advice on the content and implications of the proposed amendment.

Other Issues

46. The properties at 23-33 Johnston Street, immediately to the west of the subject land, are currently subject to a planning permit application for a proposed 12 storey building. The proposed amendment has had regard to the existing and potential future noise sensitive uses in close proximity to the site, through the noise management requirements included in both the schedule to SUZ and the inclusion of the site in schedule to Clause 52.43 (Live Music and Entertainment Noise).

Options

47. Council can consider a number of response options to the proposed planning scheme changes. The following options are available for Council to consider:
- (a) Council supports the amendment proposal with no changes;
- (b) Council objects to the rezoning of the site (and/or the other aspects of the amendment), preferring that it remain within the current Mixed Use Zone with the current DDO12; or
- (c) Council supports the amendment proposal subject to changes to both the Schedule to the Special Use Zone and Car Parking Overlay.
48. Council officers recommend Option (c) as the preferred option.

Conclusion

49. The proposed future use of the site as the Collingwood Arts Precinct is strategically supported and identified in the Johnston Street Local Area Plan, encouraging arts and creative activities within the precinct.
50. The proposed amendment is supported subject to the changes outlined in **Attachments 4 and 5** which would ensure that the proposed activities on the site are appropriately assessed by Council as the Responsible Authority and that a Masterplan would be produced if a number of uses listed within the Schedule were applied for.

RECOMMENDATION

1. That:
 - (a) Council notes the officer's report summarising the proposed state government amendment to rezone the subject land at 35 Johnston Street and 50 Perry Street, Collingwood; and
 - (b) Council endorse the submission to GSLAC which supports the proposed amendment subject to the changes outlined in Attachment 2;
 - (c) Officers engage legal representation and experts to attend the Advisory Committee hearing which is scheduled for the 10 and 11 April; and
 - (d) the report of the Advisory Committee is released publicly.

CONTACT OFFICER: Evan Burman
TITLE: Strategic Planner
TEL: 9205 5075

Attachments

- 1** Proposed Amendment Documents Collingwood Arts Precinct
- 2** Information Sheet and Planning Report CAP
- 3** Noise and Traffic Reports CAP
- 4** GSLAC Submission - 35 Johnston Street SUZ track changed
- 5** Parking Overlay Schedule Track Changes
- 6** Council submission to GSLAC amendment for 35 Johnston Street and 50 Perry Street

Attachment 1 - Proposed Amendment Documents Collingwood Arts Precinct

37.01

19/01/2006
VC37

SPECIAL USE ZONE

Shown on the planning scheme map as **SUZ** with a number.

Purpose

To implement the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.

To recognise or provide for the use and development of land for specific purposes as identified in a schedule in this zone.

37.01-1

19/01/2006
VC37

Table of uses

Section 1 - Permit not required

USE	CONDITION
Any use in Section 1 of the schedule to this zone	Must comply with any condition in Section 1 of the schedule to this zone

Section 2 - Permit required

USE	CONDITION
Any use in Section 2 of the schedule to this zone	Must comply with any condition in Section 2 of the schedule to this zone.

Any other use not in Section 1 or 3 of the schedule to this zone

Section 3 - Prohibited

USE
Any use in Section 3 of the schedule to this zone

37.01-2

19/01/2006
VC37

Use of land

Any requirement in the schedule to this zone must be met.

Application requirements

An application to use land must be accompanied by any information specified in the schedule to this zone.

Exemption from notice and review

The schedule to this zone may specify that an application is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

Decision guidelines

Before deciding on an application, in addition to the decision guidelines in Clause 65, the responsible authority must consider, as appropriate:

Attachment 1 - Proposed Amendment Documents Collingwood Arts Precinct

- The State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.
- Any guidelines in the schedule to this zone.

37.01-3

19/01/2008
VC37

Subdivision

Permit requirement

A permit is required to subdivide land.

Any requirement in the schedule to this zone must be met.

Application requirements

An application to subdivide land must be accompanied by any information specified in the schedule to this zone.

Exemption from notice and review

The schedule to this zone may specify that an application is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

Decision guidelines

Before deciding on an application, in addition to the decision guidelines in Clause 65, the responsible authority must consider, as appropriate:

- The State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.
- Any guidelines in the schedule to this zone.

37.01-4

19/01/2008
VC37

Buildings and works

Permit requirement

A permit is required to construct a building or construct or carry out works unless the schedule to this zone specifies otherwise.

Any requirement in the schedule to this zone must be met.

Application requirements

An application to construct a building or construct or carry out works must be accompanied by any information specified in the schedule to this zone.

Exemption from notice and review

The schedule to this zone may specify that an application is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

Attachment 1 - Proposed Amendment Documents Collingwood Arts Precinct

Decision guidelines

Before deciding on an application, in addition to the decision guidelines in Clause 65, the responsible authority must consider, as appropriate:

- The State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.
- Any guidelines in the schedule to this zone.

37.01-5

19/01/2008
VC37

Advertising signs

Advertising sign requirements are at Clause 52.05. This zone is in Category 3 unless a schedule to this zone specifies a different category.

Notes:

Refer to the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement, for strategies and policies which may affect the use and development of land

Check whether an overlay also applies to the land.

Other requirements may also apply. These can be found at Particular Provisions.

Attachment 1 - Proposed Amendment Documents Collingwood Arts Precinct

YARRA PLANNING SCHEME

—/—/20—
C—

SCHEDULE [NUMBER] TO THE SPECIAL USE ZONE

Shown on the planning scheme map as **SUZ[number]**.

COLLINGWOOD ARTS PRECINCT**Purpose**

To recognise the Collingwood Arts Precinct as a significant arts and cultural precinct of State and Metropolitan significance.

To provide for the use and development of the site as an integrated hub for the arts and creative industries.

To provide for a broad range of arts and cultural activities including studios, workshops, galleries, rehearsal, performances and events spaces, and spaces for training, production and community engagement activities and associated accommodation for artists and workers in the creative industries.

To facilitate the activation of the site and provide for public access and the use of outdoor spaces, including the western courtyard, for informal outdoor recreation, events and performances including live music entertainment events managed in accordance with a Noise Management Plan approved under this schedule.

To encourage the adaptive re-use and re-purposing of existing vacant buildings including provision for contemporary design responses.

To ensure that the future use and development of the Collingwood Arts Precinct does not unduly impact on the amenity of surrounding areas.

To recognise the site's proximity to the Principal Public Transport Network and to reduce car parking demand by encouraging use of sustainable and active transport modes to and from the site.

1.0**Table of uses**

—/—/20—
C—

Section 1 - Permit not required

Use	Condition
Accommodation	Must be used in conjunction with the use of the site for arts and creative industries, or must be accommodation for artists or other workers in the creative industries.
Art and craft centre	
Cabaret	
Caretaker's house	
Cinema based entertainment facility	
Dancing school	
Education centre (other than Primary School or Secondary School)	
Food and drink premises	
Home occupation	
Leisure and recreation (other than Major sports and recreation facility and Motor racing track)	
Market	
Office	Must be used in conjunction with the use of the site for arts and creative industries

Attachment 1 - Proposed Amendment Documents Collingwood Arts Precinct

YARRA PLANNING SCHEME

Place of assembly (other than Amusement Parlour, and Nightclub)	
Rehearsal studio	
Art studio	
Shop	Must be used in conjunction with the use of the site for arts and creative industries.
Store	
Utility installation (other than reservoir)	
Any use listed in Clause 62.01	Must meet the requirements of Clause 62.01.

Section 2 - Permit required

Use	Condition
Accommodation (if the condition in Section 1 is not met)	
Child care centre	
Nightclub (other than Cabaret)	
Industry (other than Refuse disposal and Transfer station)	Must not be a purpose listed in the table to Clause 52.10.
Manufacturing sales	
Office (if the condition in Section 1 is not met)	
Shop (if the condition in Section 1 is not met)	
Warehouse (other than Store)	Must not be a purpose listed in the table to Clause 52.10.
Any other use not in Section 1 or 3	

Section 3 - Prohibited

Use
Animal husbandry
Aquaculture
Brothel
Corrective institution
Service station
Transport terminal
Veterinary centre
Corrective institution
Intensive animal husbandry

2.0 Use of land**2.1 Application Requirements**

Attachment 1 - Proposed Amendment Documents Collingwood Arts Precinct

YARRA PLANNING SCHEME

An application to use land must be accompanied by the following information, as appropriate:

- Any Master Plan approved for the site.
- The purpose of the use and the types of activities which will be carried out.
- The likely effects, if any, on nearby uses including noise levels, traffic, parking, the hours of delivery and dispatch of goods and materials, hours of operation.
- The compatibility of the proposed use with the purpose of the zone and the use of the site as an arts and creative industries precinct.
- Maintenance of areas associated with the use.

2.2 Decision Guidelines

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C—

Before deciding on an application for a planning permit or a request to approve a Master Plan, the responsible authority must consider as appropriate how the proposal would contribute to:

- The delivery of an arts and creative industries precinct of State and Metropolitan importance.
- Facilitating an evolving physical and cultural development of the site which reflects the changing needs of the arts and creative community.
- Integrating the precinct into its context and maximising opportunities for community access and use.
- Recognising the importance of accommodating a variety of events in integrating the precinct with the surrounding community.
- Maintaining the heritage values of the site and providing protection to and appreciation of the Keith Haring mural, while providing for contemporary design responses and adaptive re-use of the site.
- Organising access and circulation to maximise accessibility by pedestrians and cyclists and minimising conflicts with vehicles.
- Encouraging sustainable transport choices by maximising opportunities for access by means other than private cars and minimising on site car parking.
- The integrated planning of the precinct.

2.3 Noise Management

The use of the land must be managed so as to limit noise in accordance with the following:

- State Environment Protection Policy (Control of Noise from Industry, Commerce, and Trade) No. N-1 (“SEPP N-1”).
- State Environment Protection Policy (Control of Music Noise from Public Premises) No. N-2 (“SEPP N-2”).
- The following Patron Noise Criteria:

For L_{Aeq} noise levels, the less stringent of the following:

- if noise is assessed external to a Noise Sensitive Building, noise emissions must comply with noise limits determined according to SEPP N-1 (as though patron noise were a noise source regulated by this Policy, but with no other modification to the application of the Policy).
- if noise is assessed internal to a Noise Sensitive Building, noise emissions must achieve the lower of the design sound level range for (whichever is relevant) houses and apartments in inner city areas, entertainment districts, or near major roads, as provided in Australian Standard AS2107:2016 ‘Acoustics—Recommended design sound levels and reverberation times for building interiors’.

For L_{Amax} noise levels, during the hours of 10pm-7am:

- ‘Sleep Disturbance Criteria’, being a noise level of 55dB L_{Amax} , assessed inside a habitable room of a Noise Sensitive Building normally used for the purpose of sleeping.

Attachment 1 - Proposed Amendment Documents Collingwood Arts Precinct

YARRA PLANNING SCHEME

- For the purpose of the Patron Noise Criteria, 'Noise Sensitive Building' means a building referred to in the definition of 'Noise sensitive area' in SEPP N-1, being a Dwelling, Residential Building, Caretaker's House, Hospital, Hotel, Institutional Home, Motel, Reformatory Institution, Tourist Establishment or Work Release Hostel.

When deciding on an application (including an application for approval of a Master Plan under this schedule) the responsible authority must have regard to, as appropriate:

- The Noise Management Plan prepared by Noise Consulting & Management Pty Ltd dated 21 December 2016. This Noise Management Plan may be amended to the satisfaction of the Responsible Authority.

For noise that is not subject to SEPP N-1, SEPP N-2 or patron noise criteria (as specified in this clause), EPA publication no. 1254, 'Noise Control Guidelines' 1254 and any other relevant noise guidelines published by EPA from time to time.

2.4 Management of waste

—/—20—
C—

Before any use commences (other than uses for which a planning permit has already been granted) a Waste Management Plan must be prepared to the satisfaction of the Responsible Authority. This requirement does not apply if waste management arrangements are provided for under a Masterplan approved by the Responsible Authority under clause 4.3.

The use of the land must be generally in accordance with an approved Waste Management Plan under this clause, or generally in accordance with an approved Master Plan under clause 4.3, as appropriate.

If in the opinion of the responsible authority a Waste Management Plan is not relevant to the evaluation of an application, or if the responsible authority considers that an existing approved Waste Management Plan adequately manages the waste anticipated to be generated by a new use, the responsible authority may waive the requirements of this clause.

3.0 Subdivision

3.1 Application requirements

An application to subdivide land must be accompanied by the following information, as appropriate:

- Any Masterplan approved for the site.
- The purpose of the subdivision.
- The likely effects the subdivision will have on adjacent land.
- The effects of the subdivision on the use of the site as an integrated creative arts precinct.

3.2 Decision guidelines

Before deciding on an application, the responsible authority must consider, as appropriate:

- The interface with adjoining land and any existing residential properties adjoining the site.
- The effect the subdivision will have on the potential of the land to accommodate existing and potential future uses in accordance with the purpose of this zone.

3.3 Exemption from notice and review

An application to subdivide land is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

Attachment 1 - Proposed Amendment Documents Collingwood Arts Precinct

YARRA PLANNING SCHEME

4.0 Buildings and Works

4.1 Application Requirements

An application to construct a building or construct or carry out works must be accompanied by the following information, as appropriate:

- Any Masterplan approved for the site
- A plan, drawn to scale, that shows:
 - The boundaries and dimensions of the site.
 - Relevant ground levels.
 - The location, height and purpose of buildings and works on adjoining land.
 - The layout and use of existing and proposed buildings and works, accessways, and car parking and loading areas.
 - Elevation drawings to scale indicating the colour and materials of all proposed buildings and works.
 - Details of proposed landscaping.
 - Details of any heritage fabric to be altered or removed.
 - Vehicle and pedestrian entry and exit points for the site.

4.2 Decision Guidelines

Before deciding on an application, the responsible authority must consider, as appropriate:

- Any Masterplan approved for the site.
- The development of the site as an integrated arts precinct and the purposes of the zone.
- The interface with adjoining land, especially the relationship with existing residential properties.
- The location and type of access to the site.
- The provision and location of car parking.
- The appearance and bulk of buildings having regard to the adjoining land, especially the relationship with residential areas.
- The provision for landscaping.
- The movement of pedestrians and cyclists and vehicles providing for supplies, waste removal, emergency services and public transport.
- The effect of the proposed buildings and works on the amenity of the neighbourhood, including the effects of noise, lighting, overshadowing, building bulk and privacy.

4.3 Masterplan

A Masterplan may be prepared for the precinct (or part of the precinct) and approved by the Responsible Authority.

An approved Masterplan may include, but is not limited to, the following:

- The arrangement of land uses and activities across the precinct.
- Access and circulation arrangements.
- Measures to manage traffic, car parking and loading.
- Proposed buildings and works including the location, extent and design of buildings.
- Landscaping and the treatment of external areas.
- Measures to manage noise.
- Measures to protect the heritage features of the site while providing for contemporary design responses.
- Waste management arrangements.

Once approved, the Masterplan may be amended to the satisfaction of the responsible authority.

4.4 Exemption from notice and review

Attachment 1 - Proposed Amendment Documents Collingwood Arts Precinct

YARRA PLANNING SCHEME

An application to construct a building or construct or carry out works for a use in Section 1 of this schedule is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

5.0 Advertising signs

Advertising sign requirements are at Clause 52.05. This zone is in Category 1.

Attachment 1 - Proposed Amendment Documents Collingwood Arts Precinct

45.09

19/04/2013
VC95

PARKING OVERLAY

Shown on the planning scheme map as **PO** with a number.

Purpose

To implement the State Planning Policy Framework and Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.

To facilitate an appropriate provision of car parking spaces in an area.

To identify areas and uses where local car parking rates apply.

To identify areas where financial contributions are to be made for the provision of shared car parking.

45.09-1

19/04/2013
VC95

Operation

This overlay operates in conjunction with Clause 52.06.

A schedule to this overlay may:

- Vary the requirements of Clause 52.06 as allowed by this overlay.
- Specify additional requirements to the requirements of Clause 52.06 as allowed by this overlay.
- Specify requirements for the provision of a financial contribution as a way of meeting the car parking requirements of Clause 52.06 or this overlay.

45.09-2

19/04/2013
VC95

Parking objectives

A schedule to this overlay must specify the parking objectives to be achieved for the area affected by the schedule.

45.09-3

19/04/2013
VC95

Permit requirement

A schedule to this overlay may specify that:

- The exemption from the requirement for a permit in Clause 52.06-3A does not apply. If the exemption does not apply, a permit is required for any of the matters set out in Clause 52.06-3.
- A permit must not be granted to reduce (including reduce to zero) the number of car parking spaces required under Clause 52.06-5 or this overlay.
- A permit must not be granted to provide some or all of the car parking spaces required under Clause 52.06-5 or this overlay on another site.
- A permit must not be granted to provide more than the maximum parking provision specified in a schedule to this overlay.
- A permit is not required under Clause 52.06-3.

45.09-4

19/04/2013
VC95

Number of car parking spaces required

A schedule to this overlay may:

- Vary the car parking rate and measure for any use listed in Table 1 of Clause 52.06-5.
- Specify the car parking requirements for any use of land not listed in Table 1 of Clause 52.06-5.

Attachment 1 - Proposed Amendment Documents Collingwood Arts Precinct

- Specify maximum and minimum car parking requirements for any use of land.
- For any use listed in Table 1 of Clause 52.06-5, apply Column B in the Table to that use.

45.09-5 Application requirements and decision guidelines for permit applications

19/04/2013
VC95

Before deciding on an application under Clause 52.06-3, in addition to the relevant decision guidelines in Clause 52.06-6, the responsible authority must consider, as appropriate:

- The parking objectives of the relevant schedule to this overlay.
- Any application requirements and decision guidelines specified in a schedule to this overlay.

45.09-6 Financial contribution requirement

19/04/2013
VC95

A schedule to this overlay may allow a responsible authority to collect a financial contribution in accordance with the schedule as a way of meeting the car parking requirements that apply under this overlay or Clause 52.06.

A schedule must specify:

- The area to which the provisions allowing the collection of financial contributions applies.
- The amount of the contribution that may be collected in lieu of each car parking space that is not provided, including any indexation of that amount.
- When any contribution must be paid.
- The purposes for which the responsible authority must use the funds collected under the schedule. Such purposes must be consistent with the objectives in section 4 of the Act.

45.09-7 Requirements for a car parking plan

19/04/2013
VC95

A schedule to this overlay may specify additional matters that must be shown on plans prepared under Clause 52.06-7.

45.09-8 Design standards for car parking

19/04/2013
VC95

A schedule to this overlay may specify:

- Additional design standards.
- Other requirements for the design and management of car parking.

Plans prepared in accordance with Clause 52.06-7 must meet any design standards and requirements specified in a schedule to this overlay.

45.09-9 Decision guidelines for car parking plans

19/04/2013
VC95

Before deciding whether a plan prepared under Clause 52.06-7 is satisfactory, in addition to the decision guidelines in Clause 52.06-9, the responsible authority must consider, as appropriate, any other matter specified in a schedule to this overlay.

Attachment 1 - Proposed Amendment Documents Collingwood Arts Precinct

YARRA PLANNING SCHEME

.../20...
C-

SCHEDULE [NUMBER] 1 TO THE PARKING OVERLAY

Shown on the planning scheme map as P0[NUMBER]

COLLINGWOOD ARTS PRECINCT

1.0 Parking objectives to be achieved

- To encourage the re-use of buildings within the Collingwood Arts Precinct for arts and creative industries as identified in Schedule [NUMBER] to the Special Use Zone.
- To acknowledge the existing built form and context of the site and the constrained ability to provide on-site car parking.
- To recognise the site's proximity to the Principal Public Transport Network and to reduce car parking demand by encouraging use of sustainable and active transport modes to and from the site.
- To improve amenity for pedestrians around and through the site by minimising vehicle access to and through the site.

2.0 Permit requirement

A permit is not required under Clause 52.06-3 to reduce (including reduce to zero) the number of car parking spaces required under Clause 52.06-5 for any use listed in Section 1 of the Table of Uses in Schedule [Number] to the Special Use Zone.

3.0 Number of car parking spaces to be provided

For the purpose of Clause 52.06-5A, the car parking requirement for a use that:

- is listed in Section 1 of the Table of Uses in Schedule [Number] to the Special Use Zone; and
- is not listed in Table 1 of clause 52.06;

is zero.

For a use that:

- is not listed in Section 1 of the Table of Uses in Schedule [Number] to the Special Use Zone; and
- is listed in Table 1 of Clause 52.06-5;

the car parking rate in Column B of Table 1 in Clause 52.06 applies.

4.0 Decision guidelines for permit applications

Before deciding on an application vary the number of car parking spaces required, the Responsible Authority must consider as appropriate the decision guidelines set out in Clause 52.06-6.

Attachment 1 - Proposed Amendment Documents Collingwood Arts Precinct

52.43

04/09/2014
VC120

LIVE MUSIC AND ENTERTAINMENT NOISE

Purpose

To recognise that live music is an important part of the State's culture and economy.

To protect live music entertainment venues from the encroachment of noise sensitive residential uses.

To ensure that noise sensitive residential uses are satisfactorily protected from unreasonable levels of live music and entertainment noise.

To ensure that the primary responsibility for noise attenuation rests with the agent of change.

52.43-1

04/09/2014
VC120

Scope

This clause applies to an application required under any zone of this scheme to use land for, or to construct a building or construct or carry out works associated with:

- a live music entertainment venue.
- a noise sensitive residential use that is within 50 metres of a live music entertainment venue.

This clause does not apply to:

- the extension of an existing dwelling.
- a noise sensitive residential use that is in an area specified in clause 1.0 of the schedule to this clause.

52.43-2

04/09/2014
VC120

Definitions

In this clause:

- *live music entertainment venue* means:
 - a food and drink premises, nightclub, function centre or residential hotel that includes live music entertainment
 - a rehearsal studio
 - any other venue used for the performance of music and specified in clause 2.0 of the schedule to this clause, subject to any specified condition or limitation.
- *noise sensitive residential use* means a boarding house, dependent person's unit, dwelling, nursing home, residential aged care facility, residential village or retirement village.

52.43-3

04/09/2014
VC120

Requirements to be met

A live music entertainment venue must be designed, constructed and managed to minimise noise emissions from the premises and provide acoustic attenuation measures that would protect a noise sensitive residential use within 50 metres of the venue.

A noise sensitive residential use must be designed and constructed to include acoustic attenuation measures that will reduce noise levels from any:

Attachment 1 - Proposed Amendment Documents Collingwood Arts Precinct

- indoor live music entertainment venue to below the noise limits specified in State Environment Protection Policy (Control of Music Noise from Public Premises) No. N-2 (SEPP N2).
- outdoor live music entertainment venue to below 45dB(A), assessed as an L_{eq} over 15 minutes.

For the purpose of assessing whether the above noise standards are met, the noise measurement point may be located inside a habitable room of a noise sensitive residential use with windows and doors closed (Schedule B1 of SEPP N2 does not apply).

A permit may be granted to reduce or waive these requirements if the responsible authority is satisfied that an alternative measure meets the purpose of this clause.

52.43-4 Application requirements

04/09/2014
VC120

An application must be accompanied by the following information, as appropriate:

- A site analysis, including plans detailing:
 - the existing and proposed layout of the use, buildings or works, including all external windows and doors
 - the location of any doors, windows and open space areas of existing properties in close proximity to the site.
- If the application is associated with a noise sensitive residential use:
 - the location of any live music entertainment venues within 50 metres of the site
 - the days and hours of operation of identified venues.
- If the application is associated with a live music entertainment venue:
 - the location of any noise sensitive residential uses within 50 metres of the site
 - the days and hours of operation of that venue
 - the times during which live music will be performed.
- Details of existing and proposed acoustic attenuation measures.

If in the opinion of the responsible authority an application requirement is not relevant to the evaluation of an application, the responsible authority may waive or reduce the requirement.

52.43-5 Decision guidelines

04/09/2014
VC120

Before deciding on an application, in addition to the decision guidelines in Clause 65, the responsible authority must consider, as appropriate:

- the extent to which the siting, layout, design and construction minimise the potential for noise impacts.
- whether existing or proposed noise sensitive residential uses will be satisfactorily protected from unreasonable live music and entertainment noise.
- whether the proposal adversely affects any existing uses.

Attachment 1 - Proposed Amendment Documents Collingwood Arts Precinct

YARRA PLANNING SCHEME

~~1.0~~
C--

SCHEDULE TO CLAUSE 52.43**1.0 Areas to which Clause 52.43 does not apply**

~~1.0~~
C--

Name of area	Description
--------------	-------------

2.0 Other venues to which Clause 52.43 applies

~~1.0~~
C--

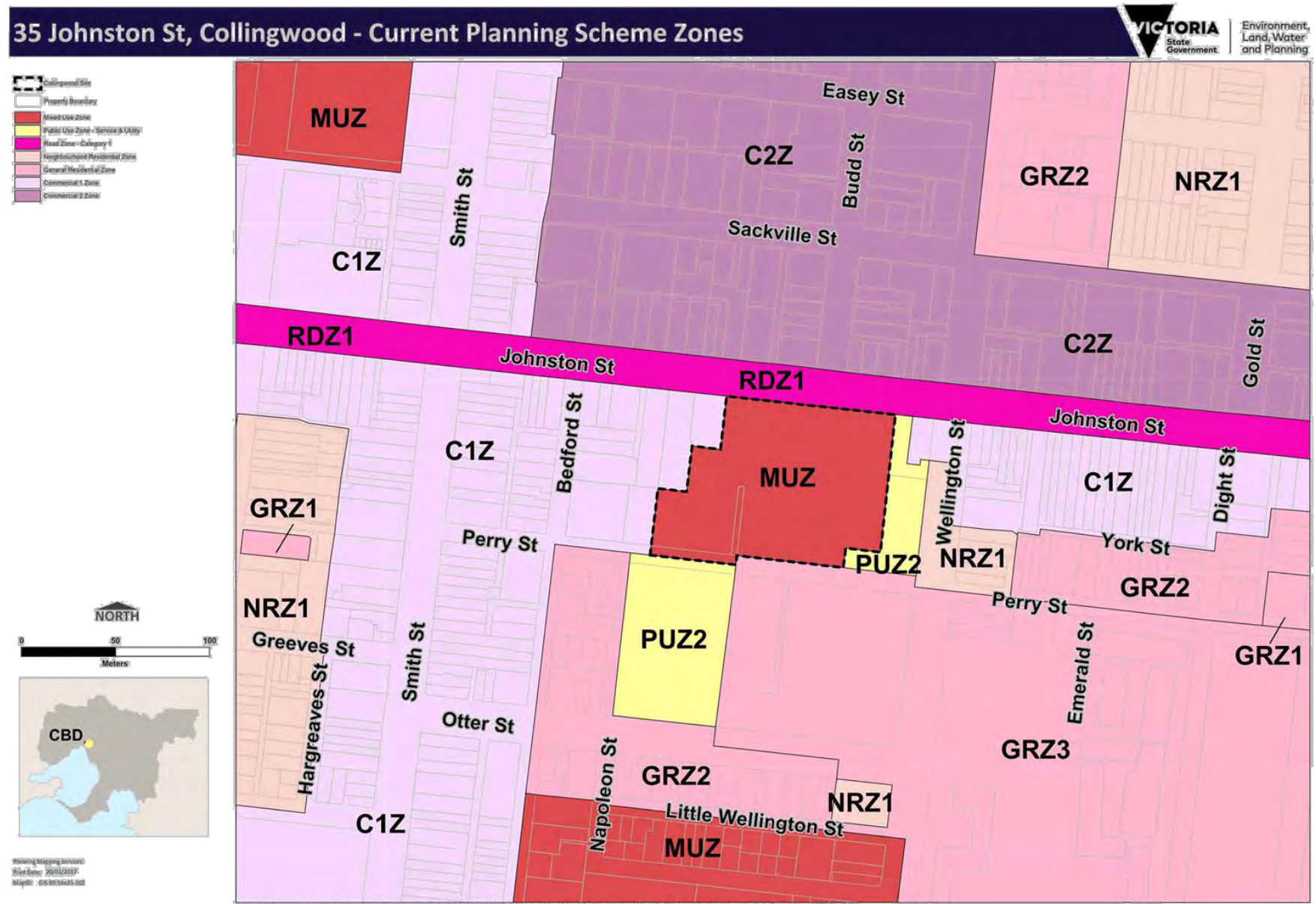
Name of venue	Address	Condition or limitation
Collingwood Arts Precinct	35 Johnston Street and 241 Wellington Street, Collingwood	None

Attachment 1 - Proposed Amendment Documents Collingwood Arts Precinct

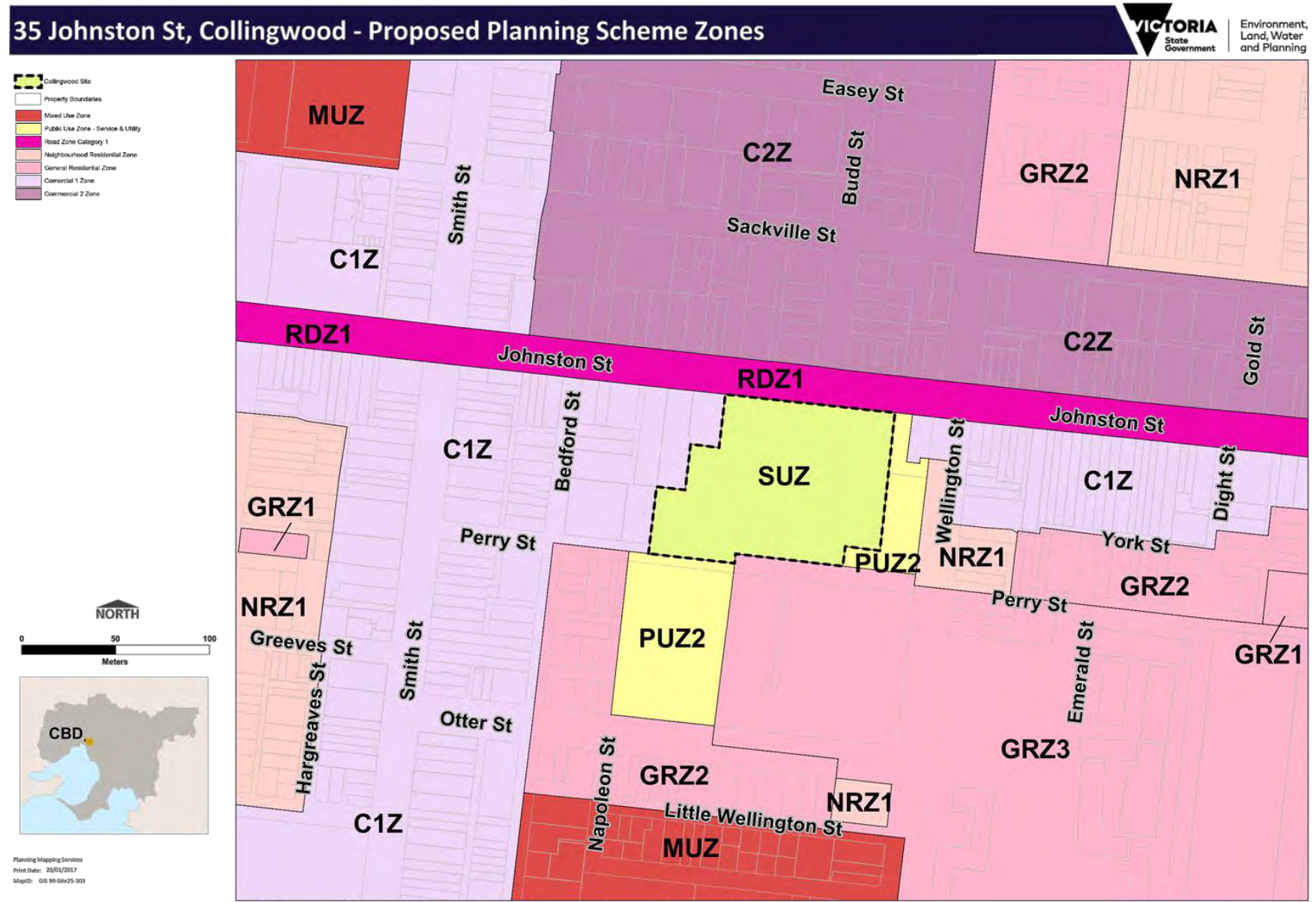
35 Johnston St, Collingwood - Location Map



Attachment 1 - Proposed Amendment Documents Collingwood Arts Precinct



Attachment 1 - Proposed Amendment Documents Collingwood Arts Precinct



Attachment 2 - Information Sheet and Planning Report CAP



Proposed Planning Changes for 35 Johnston and 50 Perry Streets, Collingwood

Information Sheet

13 February 2017

35 Johnston St, Collingwood

Creative Victoria (Department of Economic Development, Jobs, Transport and Resources) has requested that the planning provisions at 35 Johnston and 50 Perry Streets, Collingwood be changed in the Yarra Planning Scheme to reflect this site is an arts precinct.

What is proposed to change?

Creative Victoria took over the management of the site in 2010 to build a new home for Circus Oz and develop the site as a creative arts precinct.

Creative Victoria is proposing the land be rezoned from **Mixed Use Zone** to the **Special Use Zone**, with a new schedule that will specify the uses that can occur on site.

Creative Victoria is also proposing to introduce a new Parking Overlay on the site, and to identify the Arts Precinct as a venue to which the live music and entertainment clause of the Victoria Planning Provisions applies (Clause 52.43).

Creative Victoria is proposing to delete Design and Development Overlay 12, as it considers it to not be relevant to the use of the site as an arts precinct.

As part of Creative Victoria's proposal, it is seeking to retain the two existing Heritage Overlays and the Environmental Audit Overlay.

HAVE YOUR SAY

To **make a submission**, visit our website:
www.delwp.vic.gov.au/fast-track-government-land
and lodge your submission
by **5pm Friday 24 March 2017**.

The Advisory Committee will also host a
public information session on
Wednesday 1 March 2017, 6:30 – 7:30 pm,
The Melba Spiegeltent, 35 Johnston St,
Collingwood

How will the proposal be assessed?

The Minister for Planning, the Hon Richard Wynne MP, has referred the proposal to the Government Land Standing Advisory Committee (Advisory Committee) to assess and make a recommendation on the proposed planning provision changes requested by Creative Victoria.

The Advisory Committee is made up of planning and development experts who will provide independent advice to the Minister.

All local residents, land owners, servicing authorities, the local council and other interested parties are invited to make a submission.

Site: 35 Johnston and 50 Perry Streets, Collingwood



Site Details

The site is the former Collingwood Technical Collage. It is partly occupied by Circus Oz (50 Perry Street, Collingwood) and a number of vacant buildings. The site is irregular in shape and has an overall area of approximately 8,396 square metres.

delwp.vic.gov.au/fast-track-government-land



Environment,
Land, Water
and Planning

Attachment 2 - Information Sheet and Planning Report CAP

Proposed Planning Changes for 35 Johnston and 50 Perry Streets, Collingwood

Making changes to a planning scheme

A planning scheme is a statutory document which sets out objectives, policies and provisions for the use, development and protection of land. A planning scheme regulates the use and development of land through planning provisions to achieve those objectives and policies.

Zones indicate the primary character of the land, whether it is residential, industrial or rural, and determine the types of uses that may occur in that zone. Some local areas have special planning controls (known as overlays), to address issues such as areas of significant vegetation or special heritage significance. These controls are in addition to the zone controls and ensure that important aspects of the land are recognised.

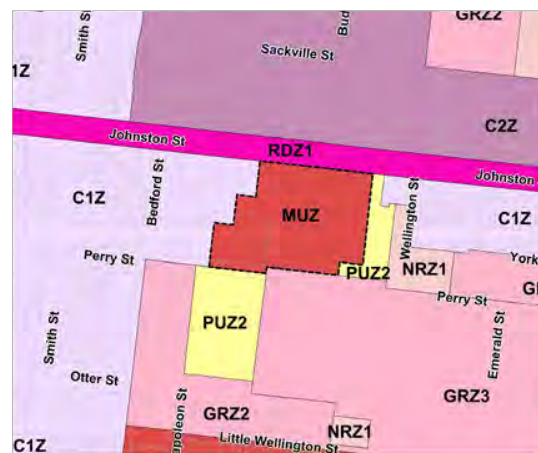
Requested planning scheme changes for the site

Creative Victoria has requested the following changes to the planning provisions for 35 Johnston and 50 Perry Streets, Collingwood.

	Current	Proposed
Zoning:	Mixed Use Zone	Special Use Zone – New Schedule (new)
Overlays and other provisions:	Design and Development Overlay 12 Environmental Audit Overlay Heritage Overlay 324 Heritage Overlay 354 Note – Victorian Heritage Register – VHR Number H2055	Environmental Audit Overlay (retained) Heritage Overlay 324 (retained) Heritage Overlay 354 (retained) Parking Overlay – New Schedule (new) Victorian Heritage Register – VHR Number H2055 (retained)
Particular Provisions		Live music and entertainment – identify the site as a venue to which 52.43 applies (new)

More Information including copies of the current and proposed planning scheme provisions can be found at:
www.delwp.vic.gov.au/fast-track-government-land

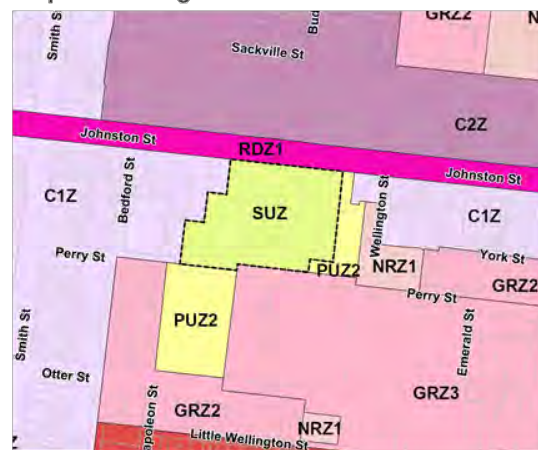
Current Zoning



Mixed Use Zone

The Mixed Use Zone (MUZ) enables a range of residential, commercial, industrial and other uses which complement the mixed use function of the locality. It also provides housing at higher densities which responds to the existing or preferred character of an area.

Proposed Zoning



Special Use Zone

The Special Use Zone (SUZ) and Schedule recognises or provides for the use and development of land for specific purposes as identified in a schedule in this zone. In this case, it is intended to reflect its use as the Collingwood Arts Precinct.

Attachment 2 - Information Sheet and Planning Report CAP

Proposed Planning Changes for 35 Johnston and 50 Perry Streets, Collingwood

Government Land Standing Advisory Committee

The Minister for Planning has established the Government Land Standing Advisory Committee (Advisory Committee) to provide expert advice and recommendations on changes to planning provisions for government land. The Advisory Committee comprises independent experts in statutory and strategic planning, land development, economics and social and environmental issues.

The aim of the Advisory Committee is to provide a consistent, transparent and timely process for the consideration of changes to planning scheme provisions of government land. The matters that must be addressed by the Advisory Committee in its report to the Minister for Planning are set out in the Terms of Reference, which can be found on the website.

Referral of a site to the Advisory Committee

The Minister for Planning has referred the proposed planning provision changes to the Advisory Committee for its consideration. The Advisory Committee will visit the site as part of its deliberations.

The Advisory Committee will also host a public information session on Wednesday 1 March 2017, 6:30 – 7:30 pm, The Melba Spiegeltent, 35 Johnston St, Collingwood.

The public information session will provide an opportunity to ask questions about the Advisory Committee process and timing.

Have your say

The proposed changes for the site are now open for public comment. All land owners, occupiers and other interested stakeholders are invited to make a written submission on the proposed changes to the planning provisions for the site. **Written submissions are due by 5pm, Friday 24 March 2017.**

Submissions must be made online at:

www.delwp.vic.gov.au/fast-track-government-land

Submissions must be directed to the Advisory Committee through Planning Panels Victoria. All submissions are treated as public documents.

Submissions can address any matter the submitter considers relevant including whether the proposal is supported or objected to or if an alternative zone or overlay may be appropriate.

The Advisory Committee cannot consider the use of public land zones, unless specifically requested by the Fast Track Government Land Service.

Public Hearings

You can also make a verbal presentation at the Public Hearing that will be held by the Advisory Committee. The Public Hearings will be held in April 2017.

If you would like to make a presentation at the Public Hearing, you must make a written submission and complete the relevant section of the online form by 24 March 2017.

Further details regarding the public hearing will be made available after the exhibition period has finished and once scheduling has been finalised.

What will the Advisory Committee do?

The Advisory Committee will take into account all submissions (written and verbal) before making a recommendation to the Minister for Planning.

The Advisory Committee has 20 business days from the last day of the Public Hearing to submit its report and recommendations to the Minister.

After the Advisory Committee reports

The Minister for Planning will consider the recommendations of the Advisory Committee and will make the final decision on the proposed changes to the planning provisions for the site.

Timeframe and next steps

Step 1: Site is referred to the Advisory Committee

Step 2: Exhibition starts – 13 February 2017

Step 3: Information Session – Wednesday 1 March 2017 6:30 – 7:30 pm, The Melba Spiegeltent, 35 Johnston St, Collingwood

Step 4: Exhibition Closes – 5pm, 24 March 2017

Step 5: Public Hearing – April 2017

Step 6: Advisory Committee Report submitted to Minister for Planning – May 2017

Attachment 2 - Information Sheet and Planning Report CAP

Proposed Planning Changes for 35 Johnston and 50 Perry Streets, Collingwood

Information Pack

An information pack has been prepared that provides more detailed information about the proposed changes to the planning provisions on the site. The information pack includes:

- A planning report which provides details on the site and why the proposed planning provisions were selected.
- Background reports that helped inform the preparation of the planning report.
- Planning provisions - which provide information on the current and proposed planning provisions (zones and overlays).

The information pack is available on our website.

Hard copies of the information pack are also available for viewing at Yarra City Council offices:

- Richmond Town Hall, 333 Bridge Road, Richmond
- Collingwood Town Hall, 140 Hoddle St, Abbotsford
- Fitzroy Library, 128 Moor Street, Fitzroy



Inside vacant building along Johnston St

Photos provided by Creative Victoria

Contact Information

If you have questions about the site and the selection of the **proposed planning provisions**, please contact **Creative Victoria** on (03) 8683 3180 or email anna.huggins@ecodev.vic.gov.au.

If you have questions about the **Advisory Committee process or timing**, please contact **Planning Panels Victoria** on (03) 8392 5120 or email planning.panels@delwp.vic.gov.au.

All other questions should be directed to the **Fast Track Government Land Service** on (03) 8683 0901 or email ftgl.service@delwp.vic.gov.au.



Fascade of 35 Johnston St



Internal courtyard

Attachment 2 - Information Sheet and Planning Report CAP



Town Planning Report

Collingwood Arts Precinct

35 Johnston Street & 241
Wellington Street,
Collingwood

Proposed re-zoning to the Special
Use Zone

February 2017

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MESSAGE CONSULTANTS
AUSTRALIA PTY LTD
ABN 33 530 423 273

2/398 Smith Street, Collingwood 3066 AUSTRALIA
PO Box 1222 Collingwood 3066
Telephone +61 3 9934 6500 Fax +61 3 9934 6555
info@messageconsultants.com www.messageconsultants.com

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1 Introduction

This report has been prepared by Message Consultants on behalf of Creative Victoria and relates to the land at 35 Johnston Street and 241 Wellington Street, Collingwood (the site).

The site is located on the south side of Johnston Street west of Wellington Street as shown at **Figure 1**. It was formerly home to the Collingwood Technical College and is now managed by Creative Victoria, which is part of the State Government's Department of Economic Development, Jobs, Transport and Resources (DEDJTR).

Part of the site has been used since 2014 as the home base of Circus Oz. The remainder of the site is currently largely unoccupied and will be developed as a creative arts hub for small and medium creative organisations as well as artists and complementary businesses. The resultant precinct will be known as the Collingwood Arts Precinct (the Precinct).

This report supports a request to rezone the site using the Fast Track Government Land Service (FTGL Service).

The proposed amendment seeks to rezone the site from the Mixed Use Zone to the Special Use Zone with a new schedule that recognises its importance as an evolving creative arts precinct and facilitates the development and ongoing operation of the site for this purpose. The amendment also seeks to apply a Parking Overlay to the site, remove an existing Design and Development Overlay (DD012) and add a Schedule to Clause 52.43 (Live music and entertainment noise).

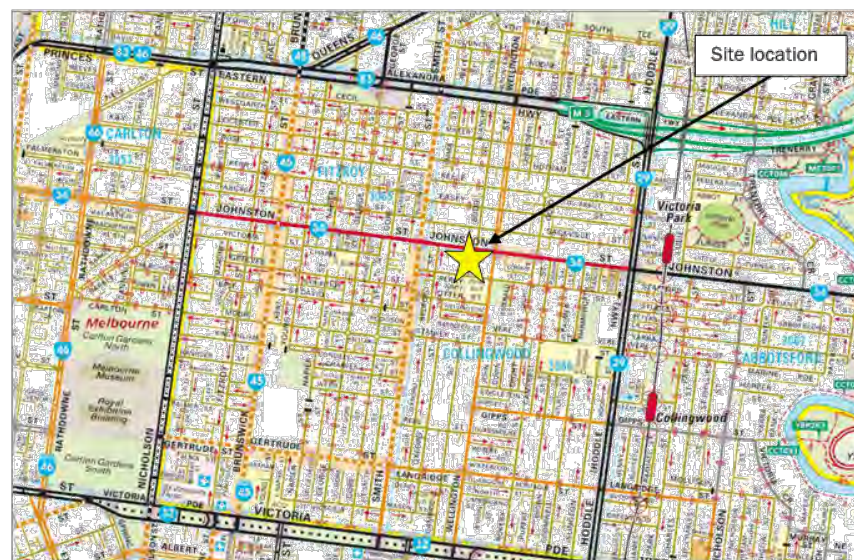


Figure 1 Location Plan

The report summarises existing conditions and planning controls of the subject land and describes the proposed amendment. It provides a strategic justification for the proposed amendment against the

Attachment 2 - Information Sheet and Planning Report CAP



State and local planning policy framework and explains the proposed approach to managing potential amenity impacts which may arise from the use of the land.

A concept plan of the site prepared by Fieldwork is attached at **Appendix 1**.

The proposed schedule to the SUZ is attached at **Appendix 2** to this report. The proposed Parking Overlay is attached at **Appendix 3** and the proposed Clause 52.43 Schedule is attached at **Appendix 4**.

The report should be read in conjunction with:

- The Traffic and Parking Assessment prepared by GTA consultants dated 16.12.16; and
- The proposed Noise Management Plan for the site prepared by Noise Consulting & Management dated 21.12.16.

Attachment 2 - Information Sheet and Planning Report CAP



2 Background

2.1 Creative Victoria

Creative Victoria is a government body dedicated to supporting, championing and growing the state's creative industries, spanning arts, culture, screen and design.

Creative Victoria invests in the ideas, talent, organisations, events and projects that make Victoria a 'Creative State'. Creative Victoria fosters new opportunities for innovation, collaboration, cross-promotion and economic growth. Creative Victoria works to raise the profile, reach and impact of Victoria's creative industries, support the career development of local artists and creative professionals, and ensure that all Victorians benefit from creative and cultural opportunities – from school kids to diverse communities to businesses.

Creative Victoria oversees a portfolio of State-owned cultural facilities and state collections worth \$7 billion; manages a suite of funding programs and initiatives that contribute to the development of Victoria's arts and cultural sector; delivers programs that help build Victoria's capability and international profile as a centre for screen and design; develops international engagement opportunities and promotes creative industries and programs to raise awareness and appreciation of Victoria's creative industries.

2.2 The Collingwood Arts Precinct

The Precinct first opened in 1871 as the Collingwood School of Design and School of Art, was transformed into the Collingwood Technical School in 1921 and later became the Collingwood Technical College. Thousands of students were educated there, but in 2005 the school was closed and the site was abandoned. For many years, it collected dust and fell into disrepair.

Creative Victoria took over management of the site in 2010 as part of the project to build a new home for Circus Oz, which now takes up approximately half of the former school grounds. This is the first time in Circus Oz's history that they have been housed in a facility designed specifically to support their work and the development of contemporary circus and physical theatre. The \$15 million redevelopment funded by the Victorian State Government is complete and Circus Oz moved into the Precinct during February 2014 following the issue of a planning permit in 2012.

After various investigations – from architectural feasibility studies to economic modelling – it was determined that the best future use for the unoccupied portion of the site would be as a contemporary creative precinct – one that will operate independently and provide a home for small and medium creative organisations as well as artists and complementary businesses.

Contemporary Arts Precincts Ltd (CAP Ltd) has been appointed to redevelop the site into a vibrant and diverse centre for the contemporary arts with assistance from the Victoria State Government which has provided over \$7 million in funding to date. CAP Ltd is social enterprise that will work in partnership with Government to deliver an integrated arts hub.

CAP's Ltd's strategic objectives are to:

- Establish CAP as a major contemporary arts and cultural precinct for Melbourne;
- Support and nurture Melbourne's arts community by providing a home for high quality arts practice;
- Create a multi-arts tenant mix that works cooperatively and collaboratively;

Attachment 2 - Information Sheet and Planning Report CAP



- Create a place of community, co-creation and participation;
- Be a high performing, partnership focused organisation;
- Generate resources to invest back into the arts and creative community; and
- Design infrastructure that is flexible and can adapt to changing creative arts practice and community needs over time

Planning for the unoccupied parts of the site has been underway for some time and planning permits have been issued for preparatory works and initial activation of some areas within the existing buildings.

The overall vision is for the Precinct to provide for a broad range of arts and cultural activities including studios, workshops, galleries, rehearsal, performances and events, and spaces for training, production and community engagement and to provide associated accommodation for artists and workers in the creative industries. The vision envisages an active and diverse precinct where the outdoor spaces are used for informal recreation, events and performances which draw the community into the site.

The overall vision for the Precinct is illustrated on the 'Concept Plan' prepared by Fieldwork which is attached at **Appendix 1**.

Attachment 2 - Information Sheet and Planning Report CAP



3 The Site and Context

3.1 The Site

The site is irregularly shaped and is located on the south side of Johnston Street, east of Wellington Street as shown at Figure 2 below.



Figure 2 – Aerial photo showing site location (Source: Nearmap)

The land comprises a series of lots described as Allotments 2643, 2644, 2645 and 2646 Parish of Jika Jika and Lot 1 on TP868459.

It has frontages of approximately 90m to Johnston Street (north) and 100m to Perry Street (south) with stepped boundaries to the east and west. The overall site area is approximately 8,396sqm.

The site currently contains a series of buildings that previously formed part of the Collingwood Technical College, as well as more recent structures developed to accommodate Circus Oz. An existing site plan is shown at Figure 3 (next page).

Circus Oz's operations are concentrated to the east and south of the site. They encompass the Melba Spiegeltent as well as 'Block C' and 'Building D' which have been adapted and extended to provide purpose built rehearsal and performance areas, education and training spaces, workshops for set and prop building, costume making, administration and storage of equipment. Outdoor spaces in the eastern part of the site around the Spiegeltent are used by Circus Oz for car parking, loading and outdoor events in line with existing planning permit (PLN11/0600).

The remainder of the site comprises three separate buildings (Buildings C, E and F) to the west and north which are currently vacant and intended to accommodate a range of arts and creative industry activities to be managed by CAP Ltd.

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The existing buildings previously accommodated the operations of the former technical college and comprise substantial three-storey structures with large windows and generous floor to ceiling heights which are well suited to re-use for a range of creative activities. In the western part of the site is a substantial raised courtyard area which contains mature trees and is intended to perform a key role in the Precinct as a space for outdoor passive recreation, events and performances.

The site is located on sloping land which falls generally towards the east. The main vehicle access is currently via a crossover from Johnston Street in the north-east corner of the site, two further crossovers exist on the Perry Street frontage of the site. A number of mature trees are located within the site.

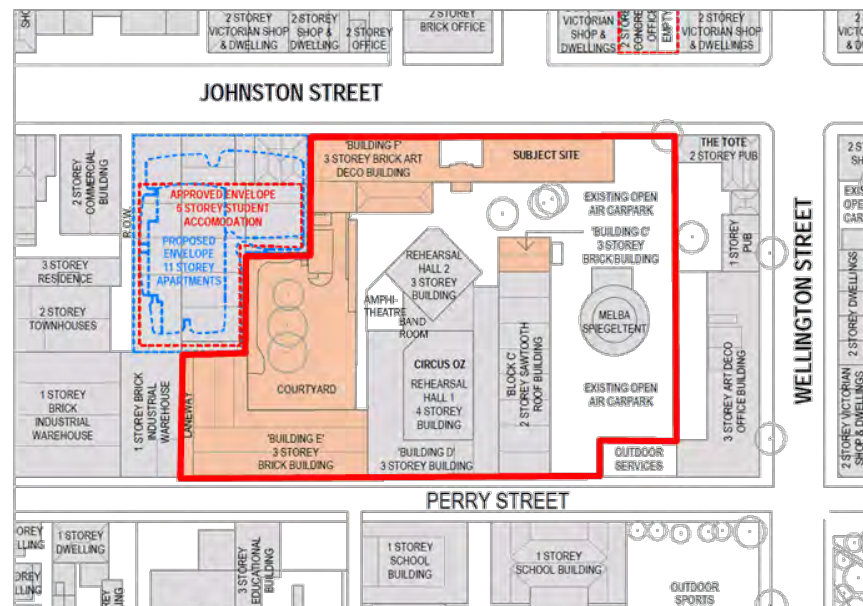


Figure 3 - Existing site plan prepared by Fieldwork



Figure 4 - Building F frontage to Johnston Street looking west



Figure 5 - Spiegeltekt and Building C looking south-west

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Figure 6 – View across western courtyard looking north-east



Figure 7 – View looking east between Buildings C and F

3.2 Context

The site is located in a highly accessible and well-serviced location in Collingwood characterised by a range of land uses and varied built form.

The site has the following direct abutments:

- **To the north** the site abuts Johnston Street (a RDZ1), a major east-west traffic and bus route and commercial strip which is identified as a Neighbourhood Activity Centre within the City of Yarra's MSS (Clause 21.08). Land opposite the site in Johnston Street is occupied by a range of retail and office premises within two storey buildings
- **To the south** is Perry Street, a local road which provides a connection between Smith Street and Wellington Street. Land opposite the site in Perry Street is occupied by educational uses in the form of St Joseph's Primary School and the Collingwood campus of Melbourne Polytechnic.
- **To the west** the site abuts the Tote Hotel which is located at the corner of Johnston and Wellington Street. South of this is the Neighbourhood Justice Centre which extends south to Perry Street.
- **To the west** the site abuts two properties, 23-33 Johnston Street to the north and 10 Perry Street to the south.

23-33 Johnston Street is currently occupied by commercial premises which provide shopfronts to Johnston Street. This site benefits from an existing planning permit (PLN08/0894) for ground floor retail with student accommodation above within a six storey building. A planning permit application has recently been lodged with the City of Yarra for an alternative redevelopment of this site comprising a twelve storey residential apartment building with ground level retail space. At the time of writing this report, the application had not been advertised. 10 Perry Street is occupied by single and double storey commercial buildings which occupy the majority of the site area.

In its broad context the site benefits from proximity to a range of services and amenities within this part of Collingwood. It site has a number of locational attributes which include:

- Proximity to the Smith Street Activity Centre (approximately 140m to the west) with its diverse retail, food and drink, entertainment and cultural offer.

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- Proximity to public transport services including Tram Route No.86 along Smith Street, Bus Routes Nos.200 and 207 and night bus No. 966 along Johnston Street and train services from Victoria Park Railway Station approximately 650m east of the site along Johnston Street.
- Accessibility to a growing local residential population within Collingwood which is being accommodated within a range of residential developments on former commercial and industrial sites.

These attributes make this a highly accessible location which is easily reached by transport modes other than the private car. The site is located within an elongated commercial strip along Johnston Street which is evolving with a wider range of land uses and increased activity in line with the City of Yarra's adopted Johnston Street Local Area Plan.

The site's location in relation to the public transport network is shown at Figure 8.



Figure 8 – Public transport map (Source: PTV)

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Existing Planning Controls

The site is affected by the provisions of the Yarra Planning Scheme. The existing zone and overlay controls that apply to the site are set out below.

3.3 Zoning

The site is located in the Mixed Use Zone (MUZ) pursuant to Clause 32.04 of the Yarra Planning Scheme. A zone map extract has been provided at Figure 9.

The purpose of the MUZ is:

- "To implement the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.
- To provide for a range of residential, commercial, industrial and other uses which complement the mixed- use function of the locality.
- To provide for housing at higher densities.
- To encourage development that responds to the existing or preferred neighbourhood character of the area.
- To facilitate the use, development and redevelopment of land in accordance with the objectives specified in a schedule to this zone."

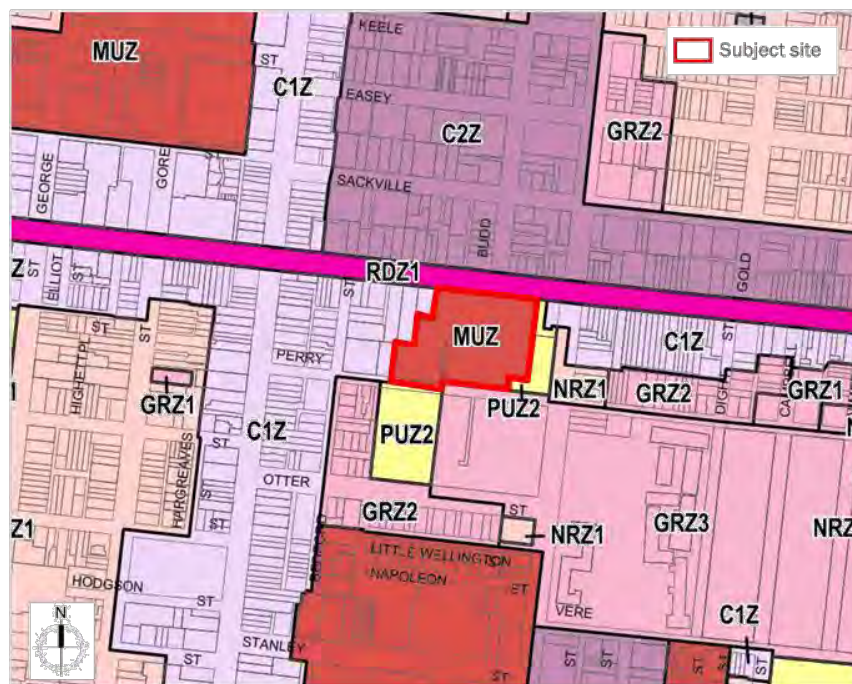


Figure 9 - Zone map extract

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3.4 Overlays

The site is affected by the following overlays:

- Design and Development Overlay - Schedule 12
- Heritage Overlay - Schedules 324 and 354
- Environmental Audit Overlay

Design and Development Overlay – Schedule 12 (DDO12) specifically relates to the subject site, referred to as 'Former NMIT Site 35 Johnston Street, Collingwood' as shown on the DDO map extract at **Figure 10**.

Pursuant to Clause 42.03, the purpose of the DDO is:

- “To implement the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.
- To identify areas which are affected by specific requirements relating to the design and built form of new development.”

Schedule 12 contains a series of design objectives, application requirements and decision guidelines for the site.

In essence, the schedule seeks to encourage the re-use and regeneration of the site in a manner that respects heritage, avoids adverse amenity impacts, ensures effective management of traffic, ensures pedestrian permeability of the site and encourages the provision of public open space within the existing courtyard.

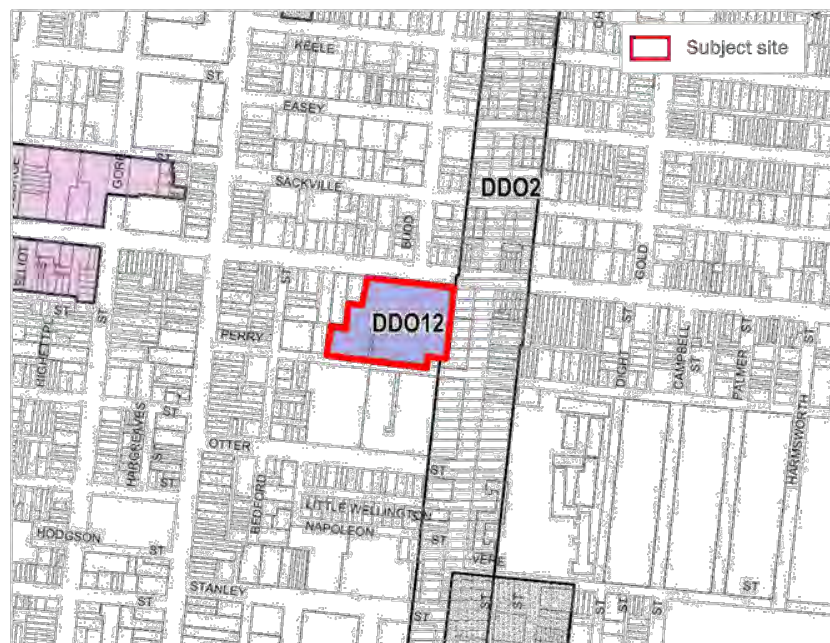


Figure 10 – DDO map extract

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The **Environmental Audit Overlay (EAO)** affects the subject site and neighbouring land to the west as shown on the map at **Figure 11**.

Pursuant to Clause 45.03 the purpose of the EAO is:

- *"To implement the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies."*
- *"To ensure that potentially contaminated land is suitable for a use which could be significantly adversely affected by any contamination."*

Clause 45.03-1 requires that before a sensitive use (residential use, childcare centre, pre-school centre or primary school) commences or before the construction or carrying out of buildings and works in association with a sensitive use commences, either:

- A certificate of environmental audit must be issued for the land in accordance with Part IXD of the Environment Protection Act 1970, or
- An environmental auditor appointed under the Environment Protection Act 1970 must make a statement in accordance with Part IXD of that Act that the environmental conditions of the land are suitable for the sensitive use.

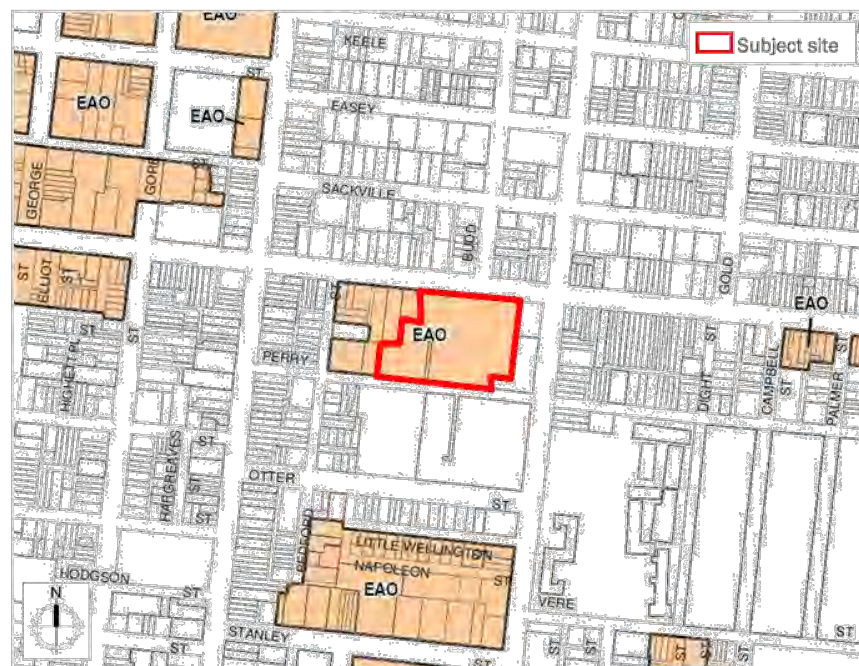


Figure 11 – EAO map extract

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Heritage Overlay - Schedule 324 'Johnston Street Precinct' (HO324) and Schedule 354 'Keith Haring Mural' (HO354) affect the site as shown on the HO map extract at Figure 12.

Pursuant to the Clause 43.01, the purpose of the HO is:

- "To implement the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.
- To conserve and enhance heritage places of natural or cultural significance.
- To conserve and enhance these elements which contribute to the significance of heritage places.
- To ensure that development does not adversely affect the significance of heritage places.
- To conserve specifically identified heritage places by allowing a use that would otherwise be prohibited if this will demonstrably assist with the conservation of the significance of the heritage place."

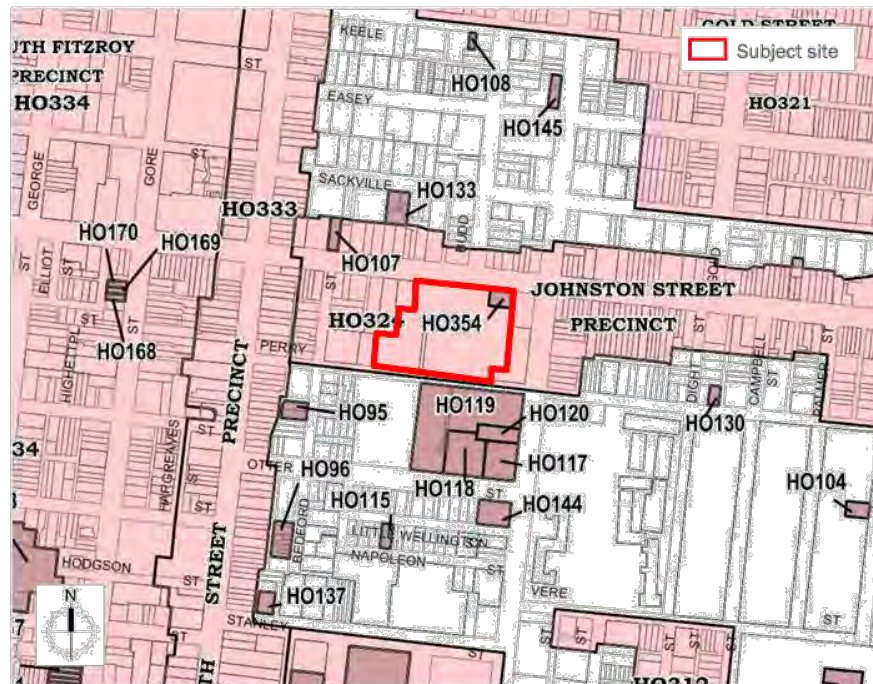


Figure 12 - HO map extract

HO324 is a precinct overlay which affects land along the Johnston Street corridor as shown in the above map.

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H0354 is a site-specific overlay which affects the Keith Haring Mural on the subject site. The Keith Haring Mural (H0354) is registered on the Victorian Heritage Register (VHR Number: H2055) as being of historical, aesthetic and social significance to the State of Victoria.

The mural is painted onto a cement panel located on the bottom half of the red brick wall at the eastern end of the former Collingwood Technical School building fronting Johnston Street. The cement panel and mural measure 7.4 x 11.5 metres.

3.5 Planning History

3.5.1 Planning Controls

The site was previously zoned Public Use Zone Schedule 2 (Education) which reflected its historic use for education which continued until the Northern Melbourne Institute of TAFE vacated the site in around 2005.

The site was subsequently declared surplus to the Government's requirements and was rezoned to the MUZ to facilitate its sale and redevelopment for a mix of uses (residential and commercial) through Amendment C103 (Am C103) to the Yarra Planning Scheme. DD012 was applied to the site as part of this amendment to guide the form of any redevelopment. The EAO was also applied to the site as part of Am C103. Am C103 was considered by an Independent Panel in 2009 and subsequently gazetted in March 2011.

3.5.2 Circus Oz Planning Permit

Following the Gazettal of Am C103, the site was not sold by the Government as envisaged. Instead, it was identified as a location for a new home base for Circus Oz who had outgrown their existing premises in Port Melbourne.

Planning Permit PLN11/0600 was granted in January 2012 and allows:

Use of part of the site for training and performance space including place of assembly, on premises liquor license and development of the land for the demolition of existing buildings and construction of a new central building, associated works, car parking, a reduction in loading bay requirements, signage and alterations to access to a road in a Road Zone (all associated with Circus Oz).

This permit has been acted on and Circus Oz moved into their new facilities in the southern and eastern parts of the site in early 2014. A current application to amend this permit to extend the operating hours of the Spiegeltent is subject to a VCAT Application for Review brought by the owner of a neighbouring property to the west, with a hearing scheduled for April 2017.

3.5.3 Creative Victoria Planning Permits

Creative Victoria has been responsible for the site since 2010. Together with facilitating the new home for Circus Oz, it has been pursuing initiatives to activate and re-use the remaining parts of the site. It has been determined that the best future use for the unoccupied portion of the site would be as a contemporary creative precinct - one that could operate independently and provide a home for small and medium creative organisations as well as artists and complementary businesses. This part of the site will be managed by CAP Ltd.

The following planning permits have been issued for the CAP Ltd parts of the site:

- PLN14/1207 was issued in February 2015 to enable 'make safe' works to the existing buildings

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and allows:

'Part demolition and construction of access ramps, entry doors and new roofing and fencing'

- PLN14/0660 was issued in May 2015 and enables limited occupation of Building C and the ground floors of Buildings E and F as part of the initial activation of the CAP Ltd parts of the site. It allows:

'Change of use to an arts and crafts centre, including a reduction in the car parking and waiver of loading bay requirements of the Yarra Planning Scheme.'

A planning permit application (PLN15/1032) has also been lodged for further activation of Buildings C, E and F and the internal (western) courtyard for the purposes of a place of assembly, dancing school, art and craft centre and education centre and a reduction of car parking requirements. This application has been called in by the Minister for Planning and is due to be considered by an Independent Panel at a hearing commencing on 6 March 2017.

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4 The Proposed Amendment

4.1 What is proposed

The proposed amendment seeks to:

- Rezone the site at 35 Johnston Street Collingwood from the Mixed Use Zone (MUZ) to the Special Use Zone (SUZ) with a site-specific SUZ Schedule which relates to the ongoing and planned use and development of the site as the Collingwood Arts Precinct;
- Apply the Parking Overlay to the site with a site-specific schedule that relates to the Collingwood Arts Precinct to establish particular parking requirements for the site;
- Introduce a Schedule to Clause 52.43 (Live music and entertainment noise) to refer to the Collingwood Arts Precinct as a venue to which Clause 52.43 applies.
- Remove the existing Design and Development Overlay (DDO12) from the land.

The existing Heritage Overlays and Environmental Audit Overlay are to be retained. The proposed amendment is summarised in the table below.

ZONE	Proposed Zone	Special Use Zone & new Schedule
	Zone to be replaced	Mixed Use Zone
OVERLAYS	Proposed new Overlay	Parking Overlay & new Schedule
	Overlay to be deleted	Design and Development Overlay – Schedule 12
	Overlays to be retained	Heritage Overlay – Schedules H0324 & H0354 Environmental Audit Overlay
PARTICULAR PROVISIONS	Clause 52.43	Add Collingwood Arts Precinct site to Section 2.0 of Schedule to Clause 52.43

4.2 Why the amendment is required

The proposed rezoning is required in order to encourage and facilitate the development and use of the site as an integrated hub for arts and cultural activities. This is in line with the objectives in *Creative State*, the State Government's Creative Industries Strategy 2016-2020, to activate co-working spaces and hubs across the state.

The current MUZ enables the range of land uses envisaged for the site to be considered. However, it does not promote, support or protect the development or ongoing evolution of the site as an arts precinct in any way, nor does it recognise the key role the site is intended to play as a cultural resource and facility within the City of Yarra, Melbourne or Victoria.

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The MUZ was applied to the site through Am C103 specifically to facilitate the sale of the site and its redevelopment for a mix of residential and commercial uses. As can be read from the Panel Report that accompanied Am C103, at the date of this Amendment, there was no clear vision for the future use of the site. Accordingly, less tailored planning controls (specifically the MUZ and DD012) were applied to the site compared with the more tailored controls now proposed.

The application of the Special Use Zone will embed the strategic importance of the arts precinct within the planning scheme. It will deliver a bespoke control that facilitates the range of activities existing and planned for the site by avoiding the need for use permits for the key uses intended for the Precinct, while providing measures to ensure that potential amenity effects are managed effectively.

The re-zoning of the site is considered by Creative Victoria to be an important strategic planning step in the development of an arts and cultural precinct of Metropolitan and State significance.

The application of the Parking Overlay to the site will allow maximum parking rates to be specified for Section 1 (permit not required) uses which reflect the fact that parking availability on the site is, and will remain, limited and will reduce the need for a series of permit applications for parking reductions and/or waivers to be sought to enable the precinct to develop.

The inclusion of the site at 2.0 in the Schedule to Clause 52.43 reflects the importance to the precinct of both performance and rehearsal of live music. The nomination of the site in the Schedule to Clause 52.43 will ensure noise conflicts are managed in a way that will allow the use of the site for music performance to continue if new residential development occurs nearby, whilst requiring new development to implement design measures that will protect future residents from music noise impacts

4.3 Strategic Justification

4.3.1 Policy Context

The development of the site for an integrated arts and cultural precinct is supported by policy directions at the State and local level.

Creative Victoria as the government body dedicated to championing, growing and supporting Victoria's creative industries has identified the development of hubs across the state generally, and the Collingwood Arts Precinct specifically, as a key strategic initiative. This is reflected in the substantial funding provided to support Circus Oz's move to the site (\$1.5m approx.) and the significant funding provided to CAP Ltd (\$7m approx.) to develop the precinct further.

In terms of planning policy, State and metropolitan planning policies recognise the importance of the arts to the cultural life of the city and State and the contribution of the creative industries to the wider economy.

In particular the Metropolitan Strategy *Plan Melbourne* recognises the need for a co-ordinated approach to the delivery of education, health, recreation and cultural facilities. Of particular relevance are the references to:

- the importance of maintaining the cultural, sporting and convention infrastructure and adding to it with new world-class facilities to maintain the city's reputation and global competitiveness (p.115);

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- The need for all neighbourhoods to have good access to a range of services including arts infrastructure (p.125);
- The aim to ensure that Melbourne's distinctiveness as a leading cultural and sporting city is maintained (p.128); and
- The commitment to continuing to support Melbourne's multicultural and creative identity and to reinforce this through cultural events and programs and associated infrastructure (p.129).

State Planning policies of relevance to this proposal include:

- Clause 11 (settlement) which seeks to anticipate and respond to the needs of existing and future communities through the provision of, amongst other things, community facilities. This clause seeks to ensure that land within activity centres is developed for a range of uses to maximise communities' choices and encourage social interaction.
- Clause 17.03-2 (tourism in metropolitan Melbourne) seeks to maintain and develop Melbourne as a destination for visitors by 'nurturing artistic and cultural life'.
- Clause 19.02-3 (cultural facilities) which aims to 'develop a strong cultural environment and increase access to arts, recreation and other cultural facilities'.

Local planning policy directions of relevance include:

- Clause 21.02 (Municipal profile) which highlights Yarra's cultural role and the number and diversity of commercial and community arts and cultural facilities located in the municipality.
- Clause 21.04 (Land use) which encourages the arts and arts venues and includes a strategy to 'support a diversity of uses such as live music venues, performance spaces, galleries and artist studios in appropriate and accessible locations'.
- Clause 21.06 (transport) which states that Yarra needs to reduce car dependence by promoting walking, cycling and public transport use as viable and preferable alternatives.
- Clause 21.08 (neighbourhoods) which locates the site within the Johnston Street Neighbourhood Activity Centre, an elongated commercial centre which extends along the Johnston Street corridor.

In addition, Council has adopted the Johnston Street Local Area Plan which is intended to guide future land use, urban design and public realm improvements for the Johnston Street activity centre and study area. The Strategic Framework Plan at Figure 13 of this plan identifies the Circus Oz/Arts Precinct site as a location for arts, education and cultural activities. Land use strategies within the plan seek to 'foster and support education, arts and community based activities at key locations within the study area (such as the Abbotsford Convent Precinct & the emerging Collingwood Arts Precinct)'.

Taken together the policy context is one in which there is strong support for the development and protection of cultural facilities and infrastructure.

More particularly, the site is in an inner city location which is gentrifying rapidly, resulting in many in the artistic communities of Collingwood and Fitzroy being 'priced out' due to increasing land values and residential development that is capitalising on the growing desirability of inner city living.

This amendment seeks to facilitate and protect a unique opportunity to contribute to the artistic, cultural and community life of the area and the city more generally. This initiative also has the

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potential to contribute positively to the ongoing renewal taking place in Collingwood and the beneficial reactivation of the Johnston Street Neighbourhood Activity Centre.

4.3.2 Application of the Special Use Zone

The Special Use Zone is one of the special purpose zones within the Victorian Planning Provisions and enables a tailored zone control to be prepared that recognises or provides for the use of land for a specific purpose through the incorporation of a bespoke Schedule.

Planning Practice Note 3 (PPN3) (DELWP June 2015) provides guidance on applying the Special Use Zone. It establishes four principles to be born in mind when considering the application of the SUZ, namely that:

- The complexity of planning requirements is reduced by keeping the number of zones used to a minimum.
- Planning scheme maps identify statutory requirements which apply to land, not particular land uses which happen to exist there.
- Detailed and complex site specific zones are discouraged in preference for clear policy guidelines as the primary tool for decision making on planning matters
- The planning permit should be the principle method for land use or development approval.

The Practice Note affirms that the SUZ can be considered when either:

- An appropriate combination of the other available zones, overlays and local policies cannot give effect to the desired objectives or requirements; or
- The site adjoins more than one zone and the strategic intent of the site, if redeveloped, is not known and it is therefore not possible to determine which zone is appropriate.

A general strategic planning approach that encourages the use of standard zones where they are appropriate and which seeks generally to avoid complex site specific zones is logical and sound. However, there are cases across our city and State where unique circumstances exist and where a site specific zone is the most effective way of fostering a desired planning outcome. This can be seen in the application of the SUZ to a number of unique sites and cultural precincts such as the Abbotsford Convent site in the City of Yarra as well as sites of strategic importance such as the Royal Melbourne Showgrounds and Flemington Racecourse in the City of Melbourne.

Collingwood Arts Precinct is an example of a unique precinct which is the subject of a strategic State Government initiative to promote arts and creative industries and is a case where the application of the SUZ is considered appropriate.

While the use of a standard zone (such as the existing MUZ) with new local policy guidelines could have some effect in allowing the development and use of the arts precinct, such an approach would not embed, encourage and protect the importance of the precinct in the planning framework in the same way as an SUZ, nor provide the same statutory weight in facilitating development of the Precinct.

It is submitted that reliance on the MUZ (or application of another standard zone) would not necessarily give effect to the desired objectives or requirements for the site. Neither could the relevant policy guidance be incorporated into the planning scheme in the expeditious manner that can occur with a rezoning through the FTGL Service. On this basis, it is considered that the application of the

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SUZ with a site-specific schedule is the best use of the statutory planning tools available to provide an appropriate planning framework for the Precinct.

Consideration was given to the use of the Public Use Zone (PUZ), given the site is publicly owned. However, the Special Use Zone is considered to provide greater flexibility in the future planning of a precinct which may ultimately accommodate a wide range of tenants and in the ways in which the land will be held, leased and sub-leased in the future.

It is noted that the existing Mixed Use Zone extends into Perry Street along this frontage of the site. While this area is outside the boundaries of the site, it would be logical for this portion of the road to be rezoned to the SUZ along with the site.

4.3.3 Proposed SUZ Schedule

The SUZ provides flexibility within the prescribed schedule to establish the particular purposes of the zone, to specify Section 1 (no permit required), Section 2 (permit required) and Section 3 (prohibited uses) and to set out particular permit requirements, conditions and exemptions and decision guidelines.

The proposed Schedule attached at **Appendix 2** has been tailored to deliver the strategic vision for the arts precinct while managing potential impacts on neighbouring land. Key aspects of the proposed Schedule are summarised below.

Zone Purposes

The proposed Schedule includes a set of site-specific purposes which seek to facilitate and support the development and use of the site as an integrated arts and creative industries hub. These purposes are intended to reinforce the importance of this arts and culture precinct and support a range of activities on the site while managing amenity and heritage impacts. The purposes of the proposed SUZ Schedule are:

- To recognise the Collingwood Arts Precinct as a significant arts and cultural precinct of State and Metropolitan significance.
- To provide for the use and development of the site as an integrated hub for the arts and creative industries.
- To provide for a broad range of arts and cultural activities including studios, workshops, galleries, rehearsal performances and events spaces, and spaces for training, production and community engagement activities and associated accommodation for artists and workers in the creative industries.
- To facilitate the activation of the site and provide for public access and the use of outdoor spaces, including the western courtyard, for informal outdoor recreation, events and performances including live music entertainment events managed in accordance with a Noise Management Plan approved under this schedule.
- To encourage the adaptive re-use and re-purposing of existing vacant buildings including provision for contemporary design responses.
- To ensure that the future use and development of the Collingwood Arts Precinct does not unduly impact on the amenity of surrounding areas.
- To recognise the site's proximity to the Principal Public Transport Network and to reduce car parking

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demand by encouraging use of sustainable and active transport modes to and from the site.

Land Uses

The Schedule includes within Section 1 of the Table of Uses all of the uses existing, planned and envisaged as potentially taking place within the arts precinct as it develops and evolves.

In essence the Section 1 uses have been drawn to encompass the vision for the site to provide art studios, workshops and galleries, rehearsal and performance spaces, training and rehearsal space, and enable theatrical and musical production and community engagement, as well as associated activities such as accommodation, administration, retail and storage.

Of particular note are the following:

- 'Accommodation is included in Section 1 on the condition that it is used in conjunction with the use of the site for arts and creative industries or is accommodation provided to artists or workers in the creative industries. This allows for the provision of accommodation for artists and creative industry workers on the site, but means that any future proposal for independent residential use on the site would require a planning permit.
- Office uses are similarly limited so that they can be used for the administration of arts and creative industries organisations but a permit would be required for any independent office use.
- Shop use is also required to be associated with the use of the site for the arts and creative industries. A permit would be required for any independent shop use.

The remaining uses in Section 1 do not have specific conditions attached. However, the management of potential impacts arising from these uses has been carefully considered as discussed in the following section.

Management of potential impacts

The Schedule contains general requirements relating to the use of land to ensure it does not detrimentally affect the amenity of the neighbourhood.

In addition, regard has been had to the management of potential impacts from the particular uses envisaged within this zone. These relate in particular to noise and waste management. Parking and traffic are discussed separately in relation to the proposed Parking Overlay at Section 5.3.4.

Amenity issues have been considered in relation to the likely day-to-day operation of the site as well as the potential for events that attract a significant number of patrons or members of the public to the precinct. Key aspects are addressed below:

▪ Noise

The range of activities envisaged for the site include indoor and outdoor events that may involve live music, amplified speech as well as operations within the site such as workshops, loading etc. which generate noise.

Previous noise assessments have been by undertaken on the site by Arup and Noise Consulting and Management in relation to the Circus Oz's operations and the current CAP Ltd permit application. These assessments have informed a proposed Noise Management Plan (NMP) which sets out the proposed measures to manage noise for all uses to ensure compliance with relevant noise limits, protect existing noise sensitive areas and address Clause 52.43 'Agent of Change' provisions.

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The Noise Management Plan is intended to be a reference document at Section 2.3 of the SUZ Schedule. This clause also provides for amendments to the NMP in the future to reflect changed circumstance subject to the satisfaction of the responsible authority.

▪ Waste

Section 2.4 of the Schedule requires that a Waste Management Plan (WMP) be prepared for the site before the commencement of any use (other than a use for which a planning permit has already been granted) or that alternatively waste management arrangements may form part of a Master Plan approved under the SUZ.

On this basis the responsible authority can be assured that adequate waste management arrangements can be resolved for any use including those which do not require a permit. The Schedule also provides that the requirement for a WMP can be waived by the responsible authority where not considered relevant or where an existing approved WMP adequately manages waste anticipated to be generated by a new use.

Other relevant considerations for the site relate to heritage and potentially contaminated land.

▪ Heritage

It is not anticipated that the proposed amendment will have any heritage impacts. The existing Heritage Overlays that affect the site will be retained and the inclusion of the Keith Haring Mural on the VHR will be unaffected.

The evolution and development of the arts precinct is anticipated to include new buildings and works including, potentially, additions to existing buildings or new stand-alone structures. The existing Heritage Overlays will trigger the need for buildings and works permits for these proposals as they come forward. A permit for buildings and works will also be required under the proposed SUZ, unless the proposed buildings and works are generally in accordance with an approved Masterplan.

The purposes set out in the Schedule, as well as the proposed decision guidelines refer to the maintenance of the site's heritage values as well as allowing for contemporary design responses.

▪ Contaminated land

The site is affected by an Environmental Audit Overlay (EAO) and this is proposed to be retained as part of this amendment. This will ensure that a certificate of environmental audit or a statement by a qualified environmental auditor must be prepared before any sensitive use (e.g. accommodation for artists) is established on the site.

Potential for a Masterplan

Section 4.3 of the Schedule has been drafted to allow for a Masterplan to be prepared and approved by the responsible authority, but does not require a Masterplan to be prepared. If a Masterplan is prepared and approved, there is provision for such a plan to provide exemptions from permit requirements for buildings and works for proposals generally in accordance with the approved plan. This would not obviate the need for buildings and works permits under the Heritage Overlay.

This approach recognises that the planning and development of the CAP Ltd parts of the site is still at an early stage. The intention of Section 4.3 is to provide flexibility for a Masterplan to be prepared but to avoid the need for a Masterplan before particular uses are established or permit applications made.

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Advertising signs

The proposed SUZ Schedule places the site within a Category 1 area under the advertising sign provisions at Clause 52.05. This is consistent with the signage control category that applies within the Commercial 1 Zone and Commercial 2 Zone which cover the majority of the Johnston Street corridor.

Exemptions from notice and review

The proposed SUZ Schedule provides an exemption from notice requirements and review rights for applications for subdivision and buildings and works for a use in Section 1 in the Table of Uses. This is intended to provide a streamlined process for permit applications which are associated with the uses that are envisaged for the arts precinct and deliver on the purposes set out in the Schedule.

Decision guidelines

The decision guidelines of the proposed Schedule seek to guide decision making on permit applications within the proposed SUZ in line with the strategic vision for the site.

This includes delivering an arts and creative precinct of State significance, maximising community access, facilitating events, maintaining heritage alongside contemporary design, encouraging sustainable transport choices and promoting the integrated planning of the precinct.

4.3.4 Traffic matters and the proposed Parking Overlay

GTA Consultants, traffic engineers, have undertaken a traffic engineering assessment of the proposed amendment and the proposed application of the Parking Overlay to the site with a Schedule which exempt the need to obtain a permit to reduce or waive parking required under clause 52.06 of the planning scheme for Section 1 (Permit not required) uses in the proposed SUZ Schedule.

The proposed Parking Overlay Schedule is attached at **Appendix 3**.

The assessment concludes that:

- The site is proposed to be rezoned to a Special Use Zone to facilitate the reuse and development of the site as an arts and cultural precinct known as the Collingwood Arts Precinct.
- The proposed schedule to the parking overlay will assist in the efficient re-use of the site for the intended purposes by removing the need to:
 - provide on-site car parking; and
 - seek a planning permit to reduce the statutory parking requirement for the uses listed in Section 1 (permit not required) of the Table of Uses within the Schedule to the Special Use Zone.
- The Parking Overlay will enable on-going use and re-use of the buildings with a variety of arts and cultural uses that may change over time, therefore avoiding on-going planning permit applications for parking dispensation when a new Section 1 use commences.
- All other uses not listed in Section 1 (permit not required) of the Table of Uses within the Schedule to the Special Use Zone would be required to provide parking as per Column B of the Table to Clause 52.06-5 or seek a permit to reduce this requirement as per Clause 52.06-3.
- It is considered that appropriate strategic justification exists for the implementation of the Parking Overlay for the Collingwood Arts Precinct based on:

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- The impracticality of providing car parking on the site due to the desire to retain the existing buildings
- The historical parking deficiency associated with the previous use of the land for Educational purposes
- The positive impact of fewer car parking spaces will have on local amenity including pedestrian amenity
- The availability of alternate transport modes including trains, trams, buses, bicycles and share cars and Council's aim to increase utilisation of alternate transport modes, and in particular bicycles
- The character of the surrounding area and the reduction in car parking provision resulting in a positive urban design outcome
- On-street parking in the vicinity of the site is generally time restricted which will limit the ability for long-term parking to occur on-street
- A range of state and local government policies.

On the basis of GTA's assessment and conclusions it is considered that the introduction of the Parking Overlay is appropriate in delivering the desired strategic planning outcomes for the Precinct.

4.3.5 Removal of DD012

DD012 was applied to the site in conjunction with its rezoning to the MUZ as part of Am C103, in order to guide the form of development for commercial and residential purposes following disposal of the site.

Circumstances have since changed and the nature of development and use now envisaged for the site as an integrated arts precinct means that DD012 now has limited relevance and ought to be removed. Relevant aspects of DDO design objectives have been captured in the purposes of the proposed SUZ Schedule including the maintenance of heritage values alongside contemporary design and the encouragement of public access to the site, while the SPPF and LPPF will continue to guide built form outcomes.

4.3.6 Schedule to Clause 52.43

Performances involving live music, as well as music rehearsal are seen as a key element of the site's activities in the future. These activities already occur on parts of the site as part of Circus Oz's permitted uses.

The site is located within a gentrifying inner city area where residential development is rapidly changing the nature of existing land use. It is therefore considered prudent to introduce a schedule to clause 52.43 which specifically designates the site as a 'live music entertainment venue'. This would recognise live music as a key aspect of the precinct's activities and provide protection of the use of the site for music entertainment as well as protection of future residents nearby the site via the adoption of the noise standards and design measures provided in clause 52.43.

Attachment 2 - Information Sheet and Planning Report CAP



5 Conclusion

In conclusion, it is considered that the proposed Amendment is appropriate for the following reasons:

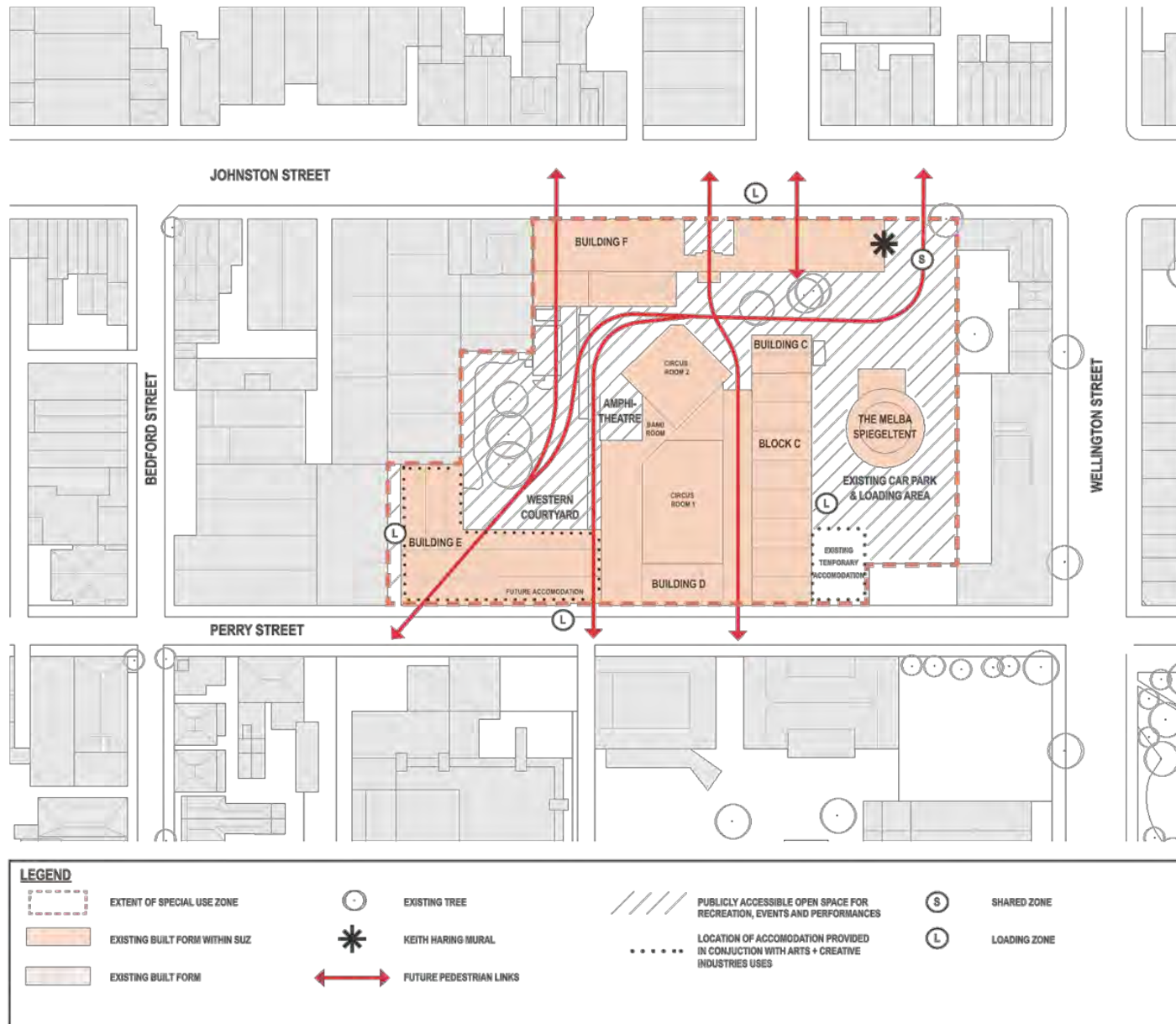
- The development of the Collingwood Arts Precinct is a key State Government initiative and a unique opportunity to deliver an integrated arts and creative industries hub on a partly vacant site through the re-purposing of unused former education buildings.
- The current planning controls for the site were applied prior to the commitment to develop an integrated arts precinct and are not tailored to this purpose.
- The proposed application of the Special Use Zone and the associated planning scheme changes proposed in this amendment will provide the necessary planning framework to facilitate its development and protect its status over time.
- The proposed amendment makes appropriate use of the statutory tools available in the VPPs which will support the development of the arts precinct, an initiative with clear and inherent community benefit, and ensure appropriate management of potential impacts on neighbouring land.

Attachment 2 - Information Sheet and Planning Report CAP



Appendix 1: Concept Plan

Attachment 2 - Information Sheet and Planning Report CAP



Attachment 2 - Information Sheet and Planning Report CAP



Appendix 2: Proposed SUZ Schedule

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YARRA PLANNING SCHEME

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SCHEDULE [NUMBER] TO THE SPECIAL USE ZONE

Shown on the planning scheme map as **SUZ[number]**.

COLLINGWOOD ARTS PRECINCT**Purpose**

To recognise the Collingwood Arts Precinct as a significant arts and cultural precinct of State and Metropolitan significance.

To provide for the use and development of the site as an integrated hub for the arts and creative industries.

To provide for a broad range of arts and cultural activities including studios, workshops, galleries, rehearsal, performances and events spaces, and spaces for training, production and community engagement activities and associated accommodation for artists and workers in the creative industries.

To facilitate the activation of the site and provide for public access and the use of outdoor spaces, including the western courtyard, for informal outdoor recreation, events and performances including live music entertainment events managed in accordance with a Noise Management Plan approved under this schedule.

To encourage the adaptive re-use and re-purposing of existing vacant buildings including provision for contemporary design responses.

To ensure that the future use and development of the Collingwood Arts Precinct does not unduly impact on the amenity of surrounding areas.

To recognise the site's proximity to the Principal Public Transport Network and to reduce car parking demand by encouraging use of sustainable and active transport modes to and from the site.

1.0**Table of uses**

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Section 1 - Permit not required

Use	Condition
Accommodation	Must be used in conjunction with the use of the site for arts and creative industries, or must be accommodation for artists or other workers in the creative industries.
Art and craft centre	
Cabaret	
Caretaker's house	
Cinema based entertainment facility	
Dancing school	
Education centre (other than Primary School or Secondary School)	
Food and drink premises	
Home occupation	
Leisure and recreation (other than Major sports and recreation facility and Motor racing track)	
Market	
Office	Must be used in conjunction with the use of the site for arts and creative industries

Attachment 2 - Information Sheet and Planning Report CAP

YARRA PLANNING SCHEME

Place of assembly (other than Amusement Parlour, and Nightclub)	
Rehearsal studio	
Art studio	
Shop	Must be used in conjunction with the use of the site for arts and creative industries.
Store	
Utility installation (other than reservoir)	
Any use listed in Clause 62.01	Must meet the requirements of Clause 62.01.

Section 2 - Permit required

Use	Condition
Accommodation (if the condition in Section 1 is not met)	
Child care centre	
Nightclub (other than Cabaret)	
Industry (other than Refuse disposal and Transfer station)	Must not be a purpose listed in the table to Clause 52.10.
Manufacturing sales	
Office (if the condition in Section 1 is not met)	
Shop (if the condition in Section 1 is not met)	
Warehouse (other than Store)	Must not be a purpose listed in the table to Clause 52.10.
Any other use not in Section 1 or 3	

Section 3 - Prohibited

Use
Animal husbandry
Aquaculture
Brothel
Corrective institution
Service station
Transport terminal
Veterinary centre
Corrective institution
Intensive animal husbandry

2.0 Use of land

2.1 Application Requirements

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YARRA PLANNING SCHEME

An application to use land must be accompanied by the following information, as appropriate:

- Any Master Plan approved for the site.
- The purpose of the use and the types of activities which will be carried out.
- The likely effects, if any, on nearby uses including noise levels, traffic, parking, the hours of delivery and dispatch of goods and materials, hours of operation.
- The compatibility of the proposed use with the purpose of the zone and the use of the site as an arts and creative industries precinct.
- Maintenance of areas associated with the use.

2.2 Decision Guidelines

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Before deciding on an application for a planning permit or a request to approve a Master Plan, the responsible authority must consider as appropriate how the proposal would contribute to:

- The delivery of an arts and creative industries precinct of State and Metropolitan importance.
- Facilitating an evolving physical and cultural development of the site which reflects the changing needs of the arts and creative community.
- Integrating the precinct into its context and maximising opportunities for community access and use.
- Recognising the importance of accommodating a variety of events in integrating the precinct with the surrounding community.
- Maintaining the heritage values of the site and providing protection to and appreciation of the Keith Haring mural, while providing for contemporary design responses and adaptive re-use of the site.
- Organising access and circulation to maximise accessibility by pedestrians and cyclists and minimising conflicts with vehicles.
- Encouraging sustainable transport choices by maximising opportunities for access by means other than private cars and minimising on site car parking.
- The integrated planning of the precinct.

2.3 Noise Management

The use of the land must be managed so as to limit noise in accordance with the following:

- State Environment Protection Policy (Control of Noise from Industry, Commerce, and Trade) No. N-1 (“SEPP N-1”).
- State Environment Protection Policy (Control of Music Noise from Public Premises) No. N-2 (“SEPP N-2”).
- The following Patron Noise Criteria:

For L_{Aeq} noise levels, the less stringent of the following:

- if noise is assessed external to a Noise Sensitive Building, noise emissions must comply with noise limits determined according to SEPP N-1 (as though patron noise were a noise source regulated by this Policy, but with no other modification to the application of the Policy).
- if noise is assessed internal to a Noise Sensitive Building, noise emissions must achieve the lower of the design sound level range for (whichever is relevant) houses and apartments in inner city areas, entertainment districts, or near major roads, as provided in Australian Standard AS2107:2016 ‘Acoustics—Recommended design sound levels and reverberation times for building interiors’.

For L_{Amax} noise levels, during the hours of 10pm-7am:

- ‘Sleep Disturbance Criteria’, being a noise level of 55dB L_{Amax} , assessed inside a habitable room of a Noise Sensitive Building normally used for the purpose of sleeping.

Attachment 2 - Information Sheet and Planning Report CAP

YARRA PLANNING SCHEME

- For the purpose of the Patron Noise Criteria, 'Noise Sensitive Building' means a building referred to in the definition of 'Noise sensitive area' in SEPP N-1, being a Dwelling, Residential Building, Caretaker's House, Hospital, Hotel, Institutional Home, Motel, Reformatory Institution, Tourist Establishment or Work Release Hostel.

When deciding on an application (including an application for approval of a Master Plan under this schedule) the responsible authority must have regard to, as appropriate:

- The Noise Management Plan prepared by Noise Consulting & Management Pty Ltd dated 21 December 2016. This Noise Management Plan may be amended to the satisfaction of the Responsible Authority.

For noise that is not subject to SEPP N-1, SEPP N-2 or patron noise criteria (as specified in this clause), EPA publication no. 1254, 'Noise Control Guidelines' 1254 and any other relevant noise guidelines published by EPA from time to time.

2.4 Management of waste

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Before any use commences (other than uses for which a planning permit has already been granted) a Waste Management Plan must be prepared to the satisfaction of the Responsible Authority. This requirement does not apply if waste management arrangements are provided for under a Masterplan approved by the Responsible Authority under clause 4.3.

The use of the land must be generally in accordance with an approved Waste Management Plan under this clause, or generally in accordance with an approved Master Plan under clause 4.3, as appropriate.

If in the opinion of the responsible authority a Waste Management Plan is not relevant to the evaluation of an application, or if the responsible authority considers that an existing approved Waste Management Plan adequately manages the waste anticipated to be generated by a new use, the responsible authority may waive the requirements of this clause.

3.0 Subdivision

3.1 Application requirements

An application to subdivide land must be accompanied by the following information, as appropriate:

- Any Masterplan approved for the site.
- The purpose of the subdivision.
- The likely effects the subdivision will have on adjacent land.
- The effects of the subdivision on the use of the site as an integrated creative arts precinct.

3.2 Decision guidelines

Before deciding on an application, the responsible authority must consider, as appropriate:

- The interface with adjoining land and any existing residential properties adjoining the site.
- The effect the subdivision will have on the potential of the land to accommodate existing and potential future uses in accordance with the purpose of this zone.

3.3 Exemption from notice and review

An application to subdivide land is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

Attachment 2 - Information Sheet and Planning Report CAP

YARRA PLANNING SCHEME

4.0 Buildings and Works

4.1 Application Requirements

An application to construct a building or construct or carry out works must be accompanied by the following information, as appropriate:

- Any Masterplan approved for the site
- A plan, drawn to scale, that shows:
 - The boundaries and dimensions of the site.
 - Relevant ground levels.
 - The location, height and purpose of buildings and works on adjoining land.
 - The layout and use of existing and proposed buildings and works, accessways, and car parking and loading areas.
 - Elevation drawings to scale indicating the colour and materials of all proposed buildings and works.
 - Details of proposed landscaping.
 - Details of any heritage fabric to be altered or removed.
 - Vehicle and pedestrian entry and exit points for the site.

4.2 Decision Guidelines

Before deciding on an application, the responsible authority must consider, as appropriate:

- Any Masterplan approved for the site.
- The development of the site as an integrated arts precinct and the purposes of the zone.
- The interface with adjoining land, especially the relationship with existing residential properties.
- The location and type of access to the site.
- The provision and location of car parking.
- The appearance and bulk of buildings having regard to the adjoining land, especially the relationship with residential areas.
- The provision for landscaping.
- The movement of pedestrians and cyclists and vehicles providing for supplies, waste removal, emergency services and public transport.
- The effect of the proposed buildings and works on the amenity of the neighbourhood, including the effects of noise, lighting, overshadowing, building bulk and privacy.

4.3 Masterplan

A Masterplan may be prepared for the precinct (or part of the precinct) and approved by the Responsible Authority.

An approved Masterplan may include, but is not limited to, the following:

- The arrangement of land uses and activities across the precinct.
- Access and circulation arrangements.
- Measures to manage traffic, car parking and loading.
- Proposed buildings and works including the location, extent and design of buildings.
- Landscaping and the treatment of external areas.
- Measures to manage noise.
- Measures to protect the heritage features of the site while providing for contemporary design responses.
- Waste management arrangements.

Once approved, the Masterplan may be amended to the satisfaction of the responsible authority.

4.4 Exemption from notice and review

Attachment 2 - Information Sheet and Planning Report CAP

YARRA PLANNING SCHEME

An application to construct a building or construct or carry out works for a use in Section 1 of this schedule is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

5.0 Advertising signs

Advertising sign requirements are at Clause 52.05. This zone is in Category 1.

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Appendix 3: Proposed Parking Overlay

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YARRA PLANNING SCHEME

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SCHEDULE [NUMBER] 1 TO THE PARKING OVERLAY

Shown on the planning scheme map as P0[NUMBER]

COLLINGWOOD ARTS PRECINCT

1.0 Parking objectives to be achieved

- To encourage the re-use of buildings within the Collingwood Arts Precinct for arts and creative industries as identified in Schedule [NUMBER] to the Special Use Zone.
- To acknowledge the existing built form and context of the site and the constrained ability to provide on-site car parking.
- To recognise the site's proximity to the Principal Public Transport Network and to reduce car parking demand by encouraging use of sustainable and active transport modes to and from the site.
- To improve amenity for pedestrians around and through the site by minimising vehicle access to and through the site.

2.0 Permit requirement

A permit is not required under Clause 52.06-3 to reduce (including reduce to zero) the number of car parking spaces required under Clause 52.06-5 for any use listed in Section 1 of the Table of Uses in Schedule [Number] to the Special Use Zone.

3.0 Number of car parking spaces to be provided

For the purpose of Clause 52.06-5A, the car parking requirement for a use that:

- is listed in Section 1 of the Table of Uses in Schedule [Number] to the Special Use Zone; and
- is not listed in Table 1 of clause 52.06;

is zero.

For a use that:

- is not listed in Section 1 of the Table of Uses in Schedule [Number] to the Special Use Zone; and
- is listed in Table 1 of Clause 52.06-5;

the car parking rate in Column B of Table 1 in Clause 52.06 applies.

4.0 Decision guidelines for permit applications

Before deciding on an application vary the number of car parking spaces required, the Responsible Authority must consider as appropriate the decision guidelines set out in Clause 52.06-6.

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Appendix 4: Proposed Clause 52.43 Schedule

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YARRA PLANNING SCHEME

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SCHEDULE TO CLAUSE 52.43**1.0 Areas to which Clause 52.43 does not apply**

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Name of area	Description
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2.0 Other venues to which Clause 52.43 applies

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Name of venue	Address	Condition or limitation
Collingwood Arts Precinct	35 Johnston Street and 241 Wellington Street, Collingwood	None

Attachment 3 - Noise and Traffic Reports CAP



Noise Consulting & Management Pty Ltd
PO Box 432, Bentleigh, VIC 3204
Suite 118, 283 Glenhuntly Road,
Elsternwick
Ph 03 9557 3799
ABN 74 105 601 321
noise@noises.com.au

COLLINGWOOD ARTS PRECINCT

35 JOHNSTON STREET, COLLINGWOOD

NOISE MANAGEMENT PLAN

Prepared by:

Andrew Rogers B App Sci
Member Australian Acoustics Society
Acoustic Consultant
Noise Consulting & Management Pty Ltd

Ref 3079
21 Dec 2016

Attachment 3 - Noise and Traffic Reports CAP

COLLINGWOOD ARTS PRECINCT - NOISE MANAGEMENT PLAN

Purpose

This Noise Management Plan provides guidance concerning the management of noise from the Collingwood Arts Precinct site, located at 35 Johnston Street, Collingwood (Site). At the date of preparation of this Noise Management Plan, the Site is currently used for circus, training and performance related activities (including music rehearsal and live music performance) and is intended to be used in the future for a range of arts and creative activities, public events and live music entertainment.

The aim of this Noise Management Plan is to protect existing noise sensitive areas from noise emissions from the Collingwood Arts Precinct, to protect the operations within the precinct for arts and creative industries (including live music entertainment) from the encroachment of noise sensitive residential use and to protect future residents in the vicinity of the site from noise.

Special Use Zone noise requirements

The schedule to the Special Use Zone (SUZ) applicable to the Site specifies land uses that can occur on the Site without first obtaining a planning permit. In this Noise Management Plan, these are referred to as 'Section 1 Uses'.

Under the SUZ, all activities must comply with the noise criteria summarised below. However, it is expected that uses on the Site which require a planning permit would be subject to more detailed assessment and controls via the permit application process and permit conditions. Accordingly, the mandatory noise criteria in the SUZ are primarily relevant to Section 1 Uses.

The mandatory noise criteria specified in the SUZ are as follows:

State Environment Protection Policy (Control of Noise from Industry, Commerce, and Trade) No. N-1 ("SEPP N-1").

This policy protects normal domestic and recreational uses and sleep in noise sensitive areas from commercial, industrial or trade noise. The policy sets quantitative noise limits in terms of decibels that vary depending on the time of the day. For example, the noise limits that apply during the evening and night period are generally higher than during the day time. Under SEPP N-1, there are no 'prohibited times' relating to land use, rather, uses of land must comply with the decibel noise limits set according to the Policy. Accordingly, SEPP N-1 will allow the Collingwood Arts Precinct to operate at any time throughout the day, evening and night, provided the relevant noise limits are met. As shown in Table 1 of this Noise Management Plan, there are various activities that can occur on the Site which are predicted to meet the relevant SEPP N-1 noise limits without any further acoustic assessment (referred to as 'Deemed to Satisfy' activities in this Noise Management Plan and Table 1). For activities that are not in accordance with the Deemed to Satisfy requirement in Table 1, a specific acoustic assessment may be required to assess whether the proposed activity can comply with the SEPP N-1 noise limits. Whether or not a specific acoustic assessment should be undertaken should be at the discretion of the Responsible Authority.

Attachment 3 - Noise and Traffic Reports CAP

COLLINGWOOD ARTS PRECINCT - NOISE MANAGEMENT PLAN

State Environment Protection Policy (Control of Music Noise from Public Premises) No. N-2 ("SEPP N-2").

This policy protects normal domestic and recreational uses and sleep in noise sensitive areas from music noise from indoor venues. Similarly to SEPP N-1, SEPP N-2 sets decibel noise limits that must be achieved at all times. SEPP N-2 does not restrict hours of operation, other than for noise from 'outdoor venues'.¹

Compliance with SEPP N-2 will allow the Collingwood Arts Precinct to operate music in indoor venues at any time throughout the day, evening and night, with the noise limits and operational scenarios adjusted according to the different time periods.

In relation to noise from Outdoor Venues, SEPP N-2 protects normal conversation, and sleep after 11:00pm. The policy will allow the Collingwood Arts Precinct to operate music in outdoor venues between noon and 11:00pm, or later with the approval of EPA Victoria.²

Patron Noise Criteria.

The criteria adopted in the SUZ will provide an acceptable degree of amenity at noise sensitive areas at all times and protect sleep between 10:00pm and 7:00am. The Patron Noise Criteria will allow the Collingwood Arts Precinct to have patrons within the precinct at any time throughout the day, evening and night, with the noise limits and operational scenarios adjusted according to the different time periods. The criteria allows the precinct to utilize any sound proofing that is adopted for new residential premises that may be affected.

Different quantitative criteria are specified for the Patron Noise depending on whether the noise is assessed inside or external to a noise sensitive building. Compliance with the less stringent criteria is intended to provide an acceptable level of internal amenity for residents regardless of which criteria is used. To explain this, the noise limits that apply outside a noise sensitive building are higher in absolute terms because these don't take into account noise reduction provided by the noise receiving building. The noise limits applicable inside noise sensitive buildings are lower (in absolute terms), but are a direct indicator of what is considered an acceptable noise environment as experienced by a listener within a building according to relevant Australian Standards. For practical reasons (i.e. to simplify noise assessments) it is expected that compliance with the Patron Noise Criteria will ordinarily be assessed outside a noise sensitive building. However, where the circumstances indicate that an internal noise assessment is appropriate, for example, where a noise sensitive building has been acoustically designed to limit noise ingress, then compliance may be assessed internally. This is specifically allowed under the SUZ which provides that the 'less stringent' of the outdoor or indoor Patron Noise Criteria must be achieved.

However, it is noted that under the SUZ, compliance with the Sleep Disturbance Criteria is always assessed inside a bedroom. This will typically require the amount of noise reduction provided by a noise sensitive building to be predicted to assess the expected maximum internal noise level within bedrooms.

¹ Refer to clauses 26-29 of SEPP N-2 for restrictions on operating times applicable to Outdoor Venues. Typically, music from an Outdoor Venue must stop by 11pm, unless the EPA authorise later operation (see SEPP N-2 clause 27(b)).

² As to later operation, refer to SEPP N-2, clause 27(b).

Attachment 3 - Noise and Traffic Reports CAP

COLLINGWOOD ARTS PRECINCT - NOISE MANAGEMENT PLAN

Complying with the noise requirements

The Noise Management Plan demonstrates that the Collingwood Arts Precinct can comply with the SUZ noise requirements at existing noise sensitive areas and provides guidance on the actions needed for new Section 1 Uses to achieve the requirements. Based on previous noise assessments³ and the existing building envelope, a series of 'Deemed to Satisfy' noise controls have been developed which will ensure compliance with the SUZ noise requirements and be appropriate for many of the future uses of the Site which are Section 1 Uses. It is acknowledged that there are some Section 1 Uses that have not been analysed in this report, as well as uses that will operate outside of the Deemed to Satisfy controls. These uses may comply with the noise emission criteria but further acoustical assessment will be required to demonstrate this.

Clause 52.43 of the Yarra Planning Scheme is applicable to the Collingwood Arts Precinct and surrounding areas. This clause is intended to protect live music entertainment venues from encroachment of noise sensitive residential use. In general, it requires that the "agent of change" is responsible to ensure that there are no adverse consequences due to new residential sites within 50m of a live music venue. The use that causes the change and its location within or outside the Collingwood Arts Precinct will determine what action is required and who is responsible. The requirements of Clause 52.43 are explained further below.

Achieving compliance with SUZ noise requirements at existing noise sensitive areas

The most affected existing noise sensitive areas have been identified as dwellings at 10 Perry Street, 4 Bedford Street and Johnston Street shop top dwellings including 38 Johnston Street and 60 Johnston Street. Compliance with the noise limits at these locations will ensure compliance at all other existing, less affected noise sensitive areas. Table 1, below, identifies the area within the precinct, the applicable Section 1 Use, the applicable SUZ noise requirement, and the corresponding Deemed to Satisfy controls required to achieve the noise limit at existing dwellings. Uses operating outside of the Deemed to Satisfy controls may comply with the SUZ requirements but will require additional acoustical assessment. The Deemed to Satisfy provisions are based on the existing building envelope, assessments undertaken by Noise Consulting and Management between July and November 2016 (Reports 2080, 3008, 3030) and ARUPs 2015 assessment (Attachment A).

Table 1

Area	Use	Noise Limit	Noise Source	Deemed to Satisfy Noise Controls do not required additional assessment
Circus Oz, all areas	All existing operations	Noise assessments show that the existing Circus Oz operations in all inside and outside areas comply with the existing noise requirements. Circus Oz has an existing noise management plan controlling the existing use of the Circus Oz site (Attachment B).		
Circus Oz, all areas	New Section 1 Uses that operate within the existing hours of operation and are acoustically similar in nature to the existing uses	SEPP N-1 SEPP N-2 Patron Noise	Plant Music Patrons	Complies, no controls required. (ARUP) Complies, no controls required. Complies, no controls required.

³ Attached to this Noise Management Plan.

Attachment 3 - Noise and Traffic Reports CAP

COLLINGWOOD ARTS PRECINCT - NOISE MANAGEMENT PLAN

Area	Use	Noise Limit	Noise Source	Deemed to Satisfy Noise Controls do not required additional assessment
Circus Oz indoors, including Spiegeltent	New Section 1 Uses that operate beyond the existing hours of operation or are acoustically different in nature to the existing uses	SEPP N-1 SEPP N-2 Patron Noise	Plant Music Patrons	Complies, no controls required. (ARUP) Data not available, further assessment required Data not available, further assessment required
Outdoor Amphitheatre	New Section 1 Uses that are acoustically different in nature to the existing uses	SEPP N-1 SEPP N-2 Patron Noise	Plant Music Patrons	Complies, no controls required. (ARUP) Outdoor venue time limit of noon to 10:00pm or 11:00pm for events no longer than 5 hours duration applies. Music levels to be limited to Leq 90 dB(A) @ 5m from stage unless event monitoring is undertaken by a qualified Acoustical Consultant. (based on previous "event" measurements) No more than 800 patrons in Amphitheatre between 10:00pm and 7:00am. (based on ARUP) No amplification between 10:00pm and 7:00am, apart from music based events with controls as per SEPP N-2 above.
Circus Oz outdoor areas	New Section 1 Uses that are acoustically different in nature to the existing uses	SEPP N-1 SEPP N-2 Patron Noise	Plant Music Patrons	Complies, no controls required. (ARUP) Data not available, further assessment required Data not available, further assessment required
Outdoor Western Courtyard	Section 1 Uses	SEPP N-1 SEPP N-2 Patron Noise	Plant Music Patrons	Complies, no controls required. (ARUP) Outdoor venue time limit of noon to 10:00pm or 11:00pm for events no longer than 5 hours duration applies. Music levels to be limited to Leq 90 dB(A) @ 5m from stage unless event monitoring is undertaken by a qualified Acoustical Consultant. (based on previous "event" measurements) No more than 800 patrons in Amphitheatre between 10:00pm and 7:00am. (based on ARUP) No amplification between 10:00pm and 7:00am, apart from music based events with controls as per SEPP N-2 above.
CAP - Building E	Section 1 Uses	SEPP N-1 SEPP N-2 Patron Noise	Plant Music Patrons	Complies, no controls required. (ARUP) All speakers inside, all doors and windows closed, Music level to be limited to Leq 90 dB(A), Music operation only during the SEPP N-2 "day/evening" period ⁱ (based on ARUP) No more than 50 patrons in patrons in Building E between 10:00pm and 7:00am without additional Patron Noise Sleep impact assessment. (ARUP).
CAP - Building F	Section 1 Uses	SEPP N-1 SEPP N-2 Patron Noise	Plant Music Patrons	Complies, no controls required. (ARUP) All speakers inside, all doors and windows closed, Music level to be limited to Leq 90 dB(A), Music operation only during the SEPP N-2 "day/evening" period ⁱ (based on ARUP) Complies, no controls required. (ARUP)

Attachment 3 - Noise and Traffic Reports CAP

COLLINGWOOD ARTS PRECINCT - NOISE MANAGEMENT PLAN

Area	Use	Noise Limit	Noise Source	Deemed to Satisfy Noise Controls do not required additional assessment
Other outdoor areas except the western courtyard, amphitheatre and circus oz outdoor areas	Section 1 Uses	SEPP N-1 SEPP N-2 Patron Noise	Plant Music Patrons	Complies, no controls required. (ARUP) Data not available, further assessment required Data not available, further assessment required

Yarra Planning Scheme Clause 52.43

Clause 52.43 of the Yarra Planning Scheme is intended to protect live music entertainment from encroachment of noise sensitive residential use. The clause requires that the “agent of change” is responsible for any controls that are required to satisfactorily protect noise sensitive residential use. A live music venue must provide acoustic attenuation measures to protect existing noise sensitive areas within 50m of the venue. New noise sensitive residential use must include acoustic measures to reduce noise from existing live music venues. Table 2 identifies anticipated uses within and around the Collingwood Arts Precinct that may require 52.43 considerations, the agent responsible for noise attenuation, and the controls needed to satisfy the requirements of the Clause 52.43.

Table 2 Agent of Change

Permitted Use	Agent Responsible	Clause 52.43 Noise Control Required
Accommodation within Collingwood Arts Precinct	New accommodation within precinct is likely to be “agent of change” and responsible for any controls.	Accommodation developer required to perform Noise Impact Assessment in accordance with SEPP N-2 and Clause 52.43 considering the existing noise sources (on and off site) and future acoustic environment due to proposed Collingwood Arts Precinct activities. This will ensure that accommodation is satisfactorily protected.
Live music, rehearsals in existing Circus Oz venues in relation to new noise sensitive development within 50m	Future accommodation likely to be “agent of change” and responsible for any controls.	Accommodation developer required to perform Noise Impact Assessment in accordance with SEPP N-2 and Clause 52.43 considering the existing noise sources (on and off site) and future acoustic environment due to proposed Collingwood Arts Precinct activities. This will ensure that accommodation is satisfactorily protected.
Live music, rehearsals in existing Circus Oz venues in relation to existing noise sensitive areas	Clause 52.43 not applicable as there is no change. Circus Oz complies with current requirements.	None
All new Section 1 Uses on Collingwood Arts Precinct site with live music in relation to new noise sensitive development within 50m	Future accommodation likely to be “agent of change” and responsible for any controls. See note below.	Accommodation developer required to perform Noise Impact Assessment in accordance with SEPP N-2 and Clause 52.43 considering the existing noise sources (on and off site) and future acoustic environment due to proposed Collingwood Arts Precinct activities. This will ensure that accommodation is satisfactorily protected.
All new Section 1 Uses on	Uses likely to be “agent of change” and	Adherence to the individual SEPP N-2 controls listed in Table 2 will ensure that that all existing

Attachment 3 - Noise and Traffic Reports CAP

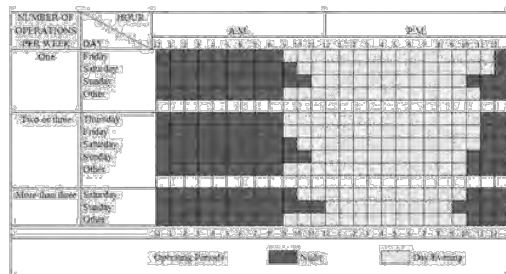
COLLINGWOOD ARTS PRECINCT - NOISE MANAGEMENT PLAN

Permitted Use	Agent Responsible	Clause 52.43 Noise Control Required
Collingwood Arts Precinct site with live music in relation to existing noise sensitive areas.	responsible for any controls.	dwellings in the vicinity are satisfactorily protected from music from Section 1 Uses. Thus any requirements of Clause 52.43, including the requirement to provide noise attenuation, will be met or exceeded.
All other Section 1 Uses without live music	Clause 52.43 not applicable.	None.

The establishment of the Collingwood Arts Precinct as a specified live music venue in Clause 2 of the schedule to 52.43 requires all nearby future residential development to provide the noise controls necessary to protect the proposed noise sensitive residential use in accordance with Clause 52.43. Although outside Collingwood Arts Precinct's control, this will require that nearby future residential development outside the precinct performs a noise impact assessment in accordance with SEPP N-2 and Clause 52.43 considering the existing or likely music noise sources and future acoustic environment due to Collingwood Arts Precinct's Section 1 Uses. Data establishing the existing music noise sources in the vicinity of the Site (as at the date of preparation of this Noise Management Plan) is available in Noise Consulting and Management report 3030, Nov 2016.

Notes:

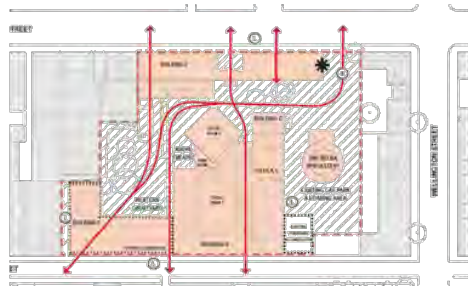
ⁱTable of SEPP N-2 operational periods



ⁱⁱ Sketch of Collingwood Arts Precinct

Attachment 3 - Noise and Traffic Reports CAP

COLLINGWOOD ARTS PRECINCT - NOISE MANAGEMENT PLAN



Attachment 3 - Noise and Traffic Reports CAP

COLLINGWOOD ARTS PRECINCT - NOISE MANAGEMENT PLAN

Attachment A Previous Noise measurements and impact analysis used as the basis for recommendations

Attachment 3 - Noise and Traffic Reports CAP



Noise Consulting & Management Pty Ltd
PO Box 432, Bentleigh, VIC 3204
Suite 118, 283 Glenhuntly Road,
Elsternwick
Ph 03 9557 3799
ABN 74 105 601 321
noise@noises.com.au

**SEPP – N2 NOISE MONITORING AND PLANNING SCHEME AMENDMENT VC120
RECOMMENDATIONS**

OPEN HOUSE EVENT

35 JOHNSTON STREET, COLLINGWOOD

31 JUL 2016

COMMISSIONED BY

**CREATIVE VICTORIA, DEPARTMENT OF ECONOMIC DEVELOPMENT, JOBS,
TRANSPORT & RESOURCES**

LEVEL 31, 121 EXHIBITION ST, MELBOURNE VIC 3000

Prepared by:

Andrew Rogers B App Sci
Member Australian Acoustics Society
Acoustic Consultant
Noise Consulting & Management Pty Ltd

Ref 2061
7 Oct 2016

Attachment 3 - Noise and Traffic Reports CAP

SEPP – N2 Noise Monitoring and Control for Creative Victoria

Summary

An outdoor music concert was held on 31 Jul 2016 at Circus Oz Amphitheatre accessed via 35 Johnston Street, Collingwood between noon and approximately 16:00.

Noise Consulting & Management was contracted by Creative Victoria to monitor, control and report on noise levels in the vicinity of the event and prepare recommendation regarding Planning Scheme Amendment VC120 Clause 52.43 (Agent of Change).

Measurements were performed in accordance with the EPA SEPP N-2 Policy outdoor venue noise limits.

The event was found to comply with the mandatory limits at all existing dwellings. However, if new dwellings are built directly to the west of the site the Planning Scheme Amendment VC120 Clause 52.43 will require the agent of change to take additional actions.

General Information

Measurements were performed in accordance with the EPA SEPP N-2 Policy, which specifies the Government's objectives for the control of music noise from public premises.

This EPA policy states the noise measurement and calculation techniques to be applied for different venue types and operating times. Basically, an "effective noise level" is measured at a "noise sensitive area" while the venue is operating. The effective noise level is then compared with the "noise limits", which have been determined at the same location without the venue operating. Compliance with the policy is achieved if the effective noise level is below the noise limit.

The policy only applies at noise sensitive areas (generally dwellings) and generally does not apply at commercial and industrial sites.

Music from the site must comply with the EPA SEPP N-2 Policy. This policy requires the venue to control music to compliant levels at all dwellings. However, the Planning Scheme Amendment VC120 Clause 52.43 requires that the "agent of change" is responsible to ensure that there are no adverse consequences due to a new residential site within 50m of a music venue.

Due to the relatively high noise limit allowed at outdoor venues, residential amenity is also controlled by limiting the operating hours and frequency of events.

The policy automatically allows operation between noon and 10:00pm for outdoor events (or 11:00pm for events of five hours or less duration). A venue may hold no more than six concerts per year.

Attachment 3 - Noise and Traffic Reports CAP

SEPP – N2 Noise Monitoring and Control for Creative Victoria

Music complying with the policy limit can be expected to be audible inside and outside affected dwellings and may affect normal domestic and recreational activities, however normal conversation should be possible.

Measurements were performed near the closest existing residences in each direction and at the potential development site to the west of the venue.

Measurements were taken outside building at least 2m away from any reflecting surfaces such as walls or fences. The locations that were determined to be the most affected were:

- 1 10 Perry Street (noise sensitive area)
- 2 38 Johnston St (noise sensitive area)
- 3 29 Johnston St, rear of 1st floor office/dwelling? (may not be a noise sensitive area)
- 4 Rear of 27 Johnston St, boundary with Creative Victoria towards Perry St (Bike repair shop, not a noise sensitive area)
- 5 29 Johnston St, boundary with Creative Victoria F Building (Artist studio, not a noise sensitive area)
- 6 Rear of lane near 23 Johnston St (noise sensitive area)
- 7 Stage

These locations include existing dwellings (noise sensitive area) and other commercial areas where the SEPP N-2 policy does not apply.

Music was playing at a level of approximately Leq 92 dB(A) at a distance of 5m in front of the stage. The stage was located near the Circus Oz building facing west.

The locations chosen and the stage location are shown below.

Attachment 3 - Noise and Traffic Reports CAP

SEPP – N2 Noise Monitoring and Control for Creative Victoria



Sound System

2x12" DSC self powered

1x18" Sub

1 stage monitor

Music was playing at a level of approximately Leq 92 dB(A) at a distance of 5m in front of the stage. The stage was located in the Circus Oz amphitheatre (35 Johnston St) facing west,

Metrological Conditions:

Wind - Still

Temperature - 14 degrees Celsius

Personnel Present:

Andrew Rogers

Noise Consulting & Management

Member of the Australian Acoustical Society (Vic)

Sasha Hadjimouratis

Creative Victoria, Department of Economic Development,

Instrumentation:

Svantek 947 Type 1 precision octave band sound and vibration analyzer
Serial No. 4277.

Bruel & Kjaer 4220 Pistophone Serial No. 169898.

Attachment 3 - Noise and Traffic Reports CAP

SEPP – N2 Noise Monitoring and Control for Creative Victoria

A field calibration is performed before and after the analyzer is used. Full calibration traceable to the national standard is completed annually for all instrumentation.

Results

Noise Limits

The noise limits for “concerts” at noise sensitive areas specified by the mandatory SEPP N-2 policy are given below:

Time period	Noise Limit (L_{eq} dB(A))
12:00-23:00	65

For concerts scheduled for more than 5 hours duration the finish time must be no later than 22:00.

According to the policy, this venue can operate no more than 6 concerts per financial year. It is understood that this was the first concert.

The policy does not limit the number of “events” per year where the effective noise level at noise sensitive areas is no higher than L_{eq} 15 min 55 dB(A) during the event i.e. 10 decibels lower than a concert. The time limits for events are the same as for concerts,

Noise Levels

The effective noise level measurements taken throughout the concert are given below.

Measurement start time	L_{eq} 15 min dB(A)	Location	Complies/ Comments
12:15	92	5m in front of stage	DJ starts
12:30	79	5) 29 Johnston artist studio/ F building	NA, not currently a noise sensitive area
12:45	75	4) rear 27 Johnston/ Bike repair	NA, not currently a noise sensitive area
13:00	49	6) Dwelling in lane near 23 Johnston	Yes
13:15	59	2) 38 Johnston dwelling	Yes, traffic noise only, music not audible
13:35	46	1) Dwelling 10 Perry	Yes
13:55	60 (adj)	3) Rear of 1st floor room 29 Johnston	Yes, measurement taken at top of stairs F building and adjusted by -10 dB to allow for distance and barrier due to F building and roof of the rear of 29 Johnston.

The outdoor music concert complied with the noise limit at all times and all noise sensitive locations.

Attachment 3 - Noise and Traffic Reports CAP

SEPP – N2 Noise Monitoring and Control for Creative Victoria

Effect of Planning Scheme Amendment VC120 on future residential development at 27-29 Johnston Street

Planning Scheme Amendment VC120 Clause 52.43 requires that the “agent of change” is responsible to ensure that there are no adverse consequences due to a new residential site within 50m of an existing music venue. To do this the amendment seeks to modify the SEPP N-2 by requiring any assessment of noise to be performed inside new apartments with doors and windows closed, thus allowing noise to be attenuated by a well designed residential building.

The noise levels due to a Circus Oz concert at the façade of any future residential development at 27-29 Johnston Street would be up to 10 decibels above the SEPP N-2 noise limit. In this case, the planning scheme amendment requires that a noise sensitive residential use must be designed and constructed to achieve noise levels below Leq 15 min 45 dB(A) inside all habitable rooms. The wall, window, door, and roof structure of the most affected apartments will have to provide 30 decibels sound reduction. Acoustic analysis of any future residential development at 27-29 Johnston Street, including detailed specification of the building structure, will be needed to ensure that the requirements of Clause 52.43 are met.

Conclusion

The music event held at 35 Johnston Street, Collingwood on 31 Jul 2016 was in compliance with SEPP N-2 outdoor venue noise limits. Any residential development of 27-28 Johnston Street will require further acoustic design consideration.

Attachment 3 - Noise and Traffic Reports CAP



Noise Consulting & Management Pty Ltd
PO Box 432, Bentleigh, VIC 3204
Suite 118, 283 Glenhuntly Road,
Elsternwick
Ph 03 9557 3799
ABN 74 105 601 321
noise@noises.com.au

**SEPP – N2 NOISE MONITORING AND PLANNING SCHEME AMENDMENT VC120
RECOMMENDATIONS**

MELBA SPIEGLETENT EVENT

35 JOHNSTON STREET, COLLINGWOOD

7 OCT 2016

COMMISSIONED BY

**CREATIVE VICTORIA, DEPARTMENT OF ECONOMIC DEVELOPMENT, JOBS,
TRANSPORT & RESOURCES**

LEVEL 31, 121 EXHIBITION ST, MELBOURNE VIC 3000

Prepared by:

Andrew Rogers B App Sci
Member Australian Acoustics Society
Acoustic Consultant
Noise Consulting & Management Pty Ltd

Ref 3008
14 Oct 2016

Attachment 3 - Noise and Traffic Reports CAP

SEPP – N2 Noise Monitoring and Control for Creative Victoria

Summary

An indoor music event was held on 7 October 2016 at Circus Oz Melba Spiegletent accessed via 35 Johnston Street, Collingwood between 7:00pm and midnight.

Noise Consulting & Management was contracted by Creative Victoria to monitor and report on noise levels in the vicinity of the event and to prepare recommendations regarding that application of Planning Scheme Amendment VC120 Clause 52.43 (Agent of Change) to a potential residential development to the west at 29 Johnston Street.

Measurements were performed in accordance with the EPA SEPP N-2 Policy for an indoor venue. It was beyond the scope of this report to calculate and measure noise levels at existing dwellings.

If new dwellings are built directly to the west of the site the Planning Scheme Amendment VC120 Clause 52.43 will require the agent of change to take additional actions.

General Information

Measurements were performed in accordance with the EPA SEPP N-2 Policy, which specifies the Government's objectives for the control of music noise from public premises.

This EPA policy states the noise measurement and calculation techniques to be applied for different venue types and operating times. Basically, an "effective noise level" is measured at a "noise sensitive area" while the venue is operating. The effective noise level is then compared with the "noise limits", which have been determined at the same location without the venue operating. Compliance with the policy is achieved if the effective noise level is below the noise limit.

The policy only applies at noise sensitive areas (generally dwellings) and generally does not apply at commercial and industrial sites.

Music from the site must comply with the EPA SEPP N-2 Policy. This policy requires the venue to control music to compliant levels at all dwellings. However, the Planning Scheme Amendment VC120 Clause 52.43 requires that the "agent of change" is responsible to ensure that there are no adverse consequences due to a new residential site within 50m of a music venue.

The potential residential development site at 29 Johnston Street is 15m from the boundary of the Circus Oz site. The Circus Oz Spiegletent is 70m from the potential development. The Circus Oz indoor band room is 18m from the potential development.

Measurements were performed near the potential development site to the west of the venue.

Attachment 3 - Noise and Traffic Reports CAP

SEPP – N2 Noise Monitoring and Control for Creative Victoria

Measurements were taken outside building at least 2m away from any reflecting surfaces such as walls or fences. The locations that were determined to be the most affected were:

- 1 29 Johnston St, boundary with Creative Victoria F Building (Artist studio, not currently a noise sensitive area)

Band music playing inside the Spiegletent consisted of drums, guitars, keyboard and vocals through an amplified sound system. The stage was located inside the tent facing north.

The venue may operate until midnight on Thur, Fri, Sat and Sun. Some of the operating times occur during the most stringent SEPP N-2 “night” period for indoor venues.

The location chosen and the stage location are shown below:



Metrological Conditions:

Wind – 10kmh East
Temperature - 12 degrees Celsius

Personnel Present:

Andrew Rogers
Noise Consulting & Management

Attachment 3 - Noise and Traffic Reports CAP

SEPP – N2 Noise Monitoring and Control for Creative Victoria

Member of the Australian Acoustical Society (Vic)
Instrumentation:

Svantek 947 Type 1 precision octave band sound and vibration analyzer
Serial No. 4277.

Bruel & Kjaer 4220 Pistophone Serial No. 169898.

A field calibration is performed before and after the analyzer is used. Full calibration traceable to the national standard is completed annually for all instrumentation.

Results

Noise Limits

Based on *Mylonas v Darebin CC* [2016] VCAT 1583 (19 September 2016) and Practice Note 81, the agent of change has the obligation to mitigate noise. It is unnecessary to consider whether existing noise emissions from a live music venue complies with SEPP N-2. It is therefore beyond the scope of this report to calculate the noise limits at existing dwellings.

However, for any potential new dwellings to comply with the Planning Scheme Amendment VC120 Clause 52.43 and SEPP N-2, it is likely that dwellings would have to be constructed achieve the SEPP N-1 base noise limits given below:

Location	Time	Octave Band Centre Frequency (Hz)						
		63	125	250	500	1000	2000	4000
		Base Night Noise Limit						
In bedrooms of potential new dwellings	Night period	40	30	20	20	15	10	10

Noise Levels

The effective noise level measurements taken throughout the event are given below:

Location, Comments	Time	Octave Band Centre Frequency (Hz)						
		63	125	250	500	1000	2000	4000
		Music Level (L _{eq 10} dB)						
1. 29 Johnston St/ Building F, Full band playing in Spiegletent	20:20 Fri 7/10/16	60	60	54	49	47	41	36
1. 29 Johnston St/ Building F, Full band playing in Spiegletent but not measurable due to music from other nearby venue	21:15 Fri 7/10/16	62	68	57	47	44	42	40

Attachment 3 - Noise and Traffic Reports CAP

SEPP – N2 Noise Monitoring and Control for Creative Victoria

Effect of Planning Scheme Amendment VC120 on future residential development at 27-29 Johnston Street

Planning Scheme Amendment VC120 Clause 52.43 requires that the “agent of change” is responsible to ensure that there are no adverse consequences due to a new residential site within 50m of an existing music venue. To do this the amendment seeks to modify the SEPP N-2 by requiring any assessment of noise to be performed inside new apartments with doors and windows closed, thus allowing noise to be attenuated by a well designed residential building. A recent VCAT decision and Practice Note 81 states that the agent of change has the obligation to mitigate noise and that it is unnecessary to consider whether existing noise emissions from a live music venue complies with SEPP N-2.

The wall, window, door, and roof structures at the façade of any future residential development at 29 Johnston Street will have to provide 30 decibels sound reduction at the 125Hz Centre Octave Band to ensure that the requirements of Clause 52.43 are met for Circus Oz Spiegletent operations. 38 decibels sound reduction at the 125Hz Centre Octave Band will be required if the operations of other venues in the vicinity are also considered.

Conclusion

The music event held at Circus Oz Spiegletent 35 Johnston Street, Collingwood on 7 October 2016 was considered in relation to Planning Scheme Amendment VC120 Clause 52.43. Any residential development at 29 Johnston Street will require further acoustic design consideration.

Attachment 3 - Noise and Traffic Reports CAP



Noise Consulting & Management Pty Ltd
PO Box 432, Bentleigh, VIC 3204
Suite 118, 283 Glenhuntly Road,
Elsternwick
Ph 03 9557 3799
ABN 74 105 601 321
noise@noises.com.au

**SEPP – N2 AND PLANNING SCHEME AMENDMENT VC120
RECOMMENDATIONS**

**MELBA SPIEGLETENT EVENT, MUSIC ROOM OPERATIONS, OUTDOOR
COURTYARD**

35 JOHNSTON STREET, COLLINGWOOD

JUL- NOV 2016

COMMISSIONED BY

**CREATIVE VICTORIA, DEPARTMENT OF ECONOMIC DEVELOPMENT, JOBS,
TRANSPORT & RESOURCES**

LEVEL 31, 121 EXHIBITION ST, MELBOURNE VIC 3000

Prepared by:

Andrew Rogers B App Sci
Member Australian Acoustics Society
Acoustic Consultant
Noise Consulting & Management Pty Ltd

Ref 3030
16 Nov 2016

Attachment 3 - Noise and Traffic Reports CAP

SEPP – N2 Recommendations Creative Victoria

Summary

Noise Consulting & Management was contracted by Creative Victoria to monitor and report on noise levels in the vicinity of Circus Oz and to prepare recommendations regarding the application of Planning Scheme Amendment VC120 Clause 52.43 (Agent of Change) to a potential residential development to the west at 29 Johnston Street.

A number of different operations that are currently undertaken by Circus Oz, 35 Johnston St, were measured. The events include; an outdoor music concert 31 Jul 2016 noon - 16:00 at Circus Oz Amphitheatre, an indoor music event on 7 October 2016 7:00pm - midnight at Circus Oz Melba Spiegeltent, and a Music Room practice on 15 Nov 2016 14:00 - 14:25.

Measurements were performed in accordance with the EPA SEPP N-2 Policy. It was beyond the scope of this report to calculate and measure noise levels at existing dwellings.

If new dwellings are built directly to the west of the site the Planning Scheme Amendment VC120 Clause 52.43 will require the agent of change to take additional actions.

General Information

Measurements were performed in accordance with the EPA SEPP N-2 Policy, which specifies the Government's objectives for the control of music noise from public premises.

This EPA policy states the noise measurement and calculation techniques to be applied for different venue types and operating times. Basically, an "effective noise level" is measured at a "noise sensitive area" while the venue is operating. The effective noise level is then compared with the "noise limits", which have been determined at the same location without the venue operating. Compliance with the policy is achieved if the effective noise level is below the noise limit.

The policy only applies at noise sensitive areas (generally dwellings) and usually does not apply at commercial and industrial sites.

Indoor venues may operate during the SEPP N-2 "day" and "night" period. The noise limits are more stringent during the "night" period.

Due to the higher noise limit allowed at Outdoor venues, residential amenity is also controlled by limiting the operating hours and frequency of events.

Attachment 3 - Noise and Traffic Reports CAP

SEPP – N2 Recommendations Creative Victoria

Music from the site must comply with the EPA SEPP N-2 Policy. This policy requires the venue to control music to compliant levels at all dwellings. However, the Planning Scheme Amendment VC120 Clause 52.43 requires that the "agent of change" is responsible to ensure that there are no adverse consequences due to a new residential site within 50m of a music venue.

The potential residential development site at 29 Johnston Street is approximately 15m from the boundary of the Circus Oz site.

Measurements were performed near the potential development site to the west of the venue.

Measurements were taken outside building at least 2m away from any reflecting surfaces such as walls or fences. The locations that were determined to be the most affected were:

- 1 29 Johnston St, boundary with Creative Victoria F Building (Artist studio, not currently a noise sensitive area)

Amphitheatre Operations

Outdoor music may operate in the amphitheatre 6-10 times per year. Typically this venue is used for open days, parties and community events. The Circus Oz house PA is normally used but outside suppliers can also provide equipment. This area operates during the day and occasionally as late as 11:00pm.

Typically the stage will face west as shown in Fig 1. The PA used by the band and DJ during the measurement on 31 Jul was 2x12" DSC self powered speakers, 1x18" Sub, 1 stage monitor.

The Circus Oz outdoor amphitheatre is 17m from the development site.

Spiegletent Operations

Band music playing inside the Spiegletent consisted of drums, guitars, keyboard and vocals through an amplified sound system. The stage was located inside the tent facing north.

The venue may operate between 9:00am and 9:00pm everyday and until midnight on Thur, Fri, Sat and Sun. Some of the operating times occur during the most stringent SEPP N-2 "night" period for indoor venues.

The Circus Oz Spiegletent is 70m from the potential development site.

Attachment 3 - Noise and Traffic Reports CAP

SEPP – N2 Recommendations Creative Victoria

Music Room Operations

The Circus Oz band may rehearse and perform in the Music Room at anytime. The number of musicians and PA system varies. However, operations are normally during the day and evening. The external door to the music room is often left open during warmer weather. The door to the Circus Oz practice area is open during performances.

The Circus Oz indoor band room is 25m from the development site.

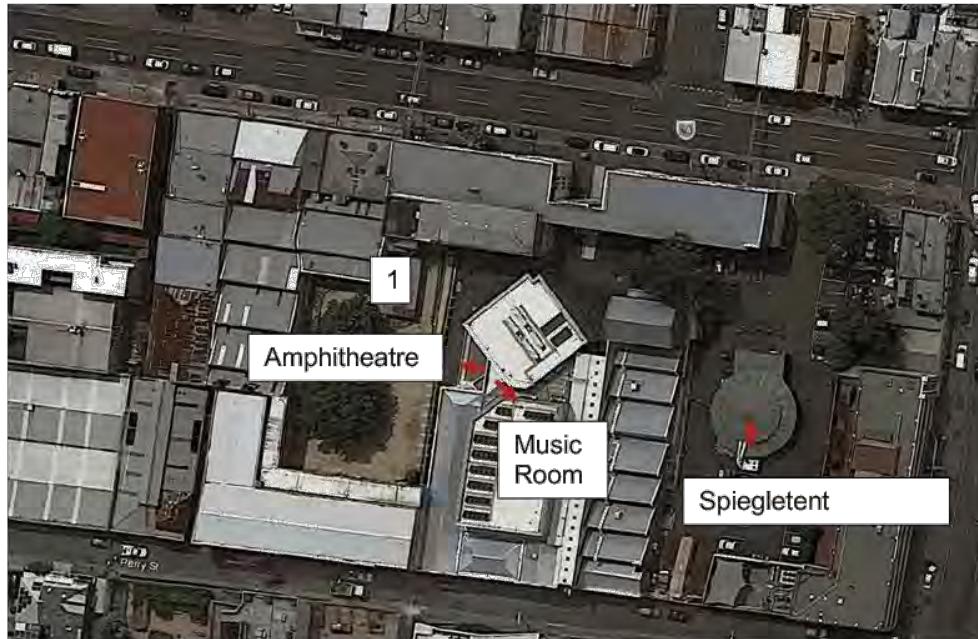


Figure 1 Proposed development site measurement point, 1, and Circus Oz operations and measured PA location and direction ↑

Personnel Present:

Andrew Rogers
Noise Consulting & Management
Member of the Australian Acoustical Society (Vic)

Instrumentation:

Svantek 947 Type 1 precision octave band sound and vibration analyser
Serial No. 4277.

Bruel & Kjaer 4220 Pistophone Serial No. 169898.

A field calibration is performed before and after the analyser is used. Full calibration traceable to the national standard is completed annually for all instrumentation.

Attachment 3 - Noise and Traffic Reports CAP

SEPP – N2 Recommendations Creative Victoria

Results

Noise Limits

Based on *Mylonas v Darebin CC* [2016] VCAT 1583 (19 September 2016) and Practice Note 81, the agent of change has the obligation to mitigate noise. It is unnecessary to consider whether existing noise emissions from a live music venue complies with SEPP N-2. It is therefore beyond the scope of this report to calculate the noise limits at existing dwellings.

However, for any potential new dwellings to comply with the Planning Scheme Amendment VC120 Clause 52.43 and SEPP N-2, it is likely that dwellings would have to be constructed to achieve the SEPP N-2 base noise limits for Indoor venues given below:

Location	Time	Octave Band Centre Frequency (Hz)						
		63	125	250	500	1000	2000	4000
		Base Night Noise Limit						
In bedrooms of potential new dwellings	Night period	40	30	20	20	15	10	10

In addition, noise sensitive residential use must be designed and constructed to achieve noise levels below $L_{eq} 15 \text{ min } 45 \text{ dB(A)}$ inside all habitable rooms for Outdoor venue noise.

Noise Levels

Measurements were performed under locally neutral conditions. The effective noise level measurements taken throughout the various events at the boundary are given below:

Amphitheatre Measurements		
Location	Measurement start time	$L_{eq} 15 \text{ min dB(A)}$
29 Johnston artist studio/ F building	12:30 31 Jul	79

Spiegletent Measurements	Time	Octave Band Centre Frequency (Hz)							Leq dB(A)
		63	125	250	500	1000	2000	4000	
Location, Comments		Music Level (Leq 10 dB)							
29 Johnston St/ Building F, Full band playing in Spiegletent	20:20 Fri 7/10/16	60	60	54	49	47	41	36	50
29 Johnston St/ Building F, Full band playing in Spiegletent but not measurable due to music from other nearby venue	21:15 Fri 7/10/16	62	68	57	47	44	42	40	52

Note: It was not possible to measure the Spiegletent effective noise levels during the "night" period due to dominant music from another venue. The operations of the Spiegletent are identical during the "day" and "night" period.

Attachment 3 - Noise and Traffic Reports CAP

SEPP – N2 Recommendations Creative Victoria

Music Room Measurements	Time	Octave Band Centre Frequency (Hz)							Leq dB(A)
		63	125	250	500	1000	2000	4000	
Location, Comments		Music Level (L _{eq 10} dB)							
29 Johnston St/ Building F, band (drums, keyboard, guitar) playing with external door open	14:10, 15/11/16	64	70	69	71	66	61	58	69
29 Johnston St/ Building F, band playing with external door closed	14:20, 15/11/16	65	58	54	49	44	44	41	50

Effect of Planning Scheme Amendment VC120 on future residential development at 27-29 Johnston Street

Planning Scheme Amendment VC120 Clause 52.43 requires that the “agent of change” is responsible to ensure that there are no adverse consequences due to a new residential site within 50m of an existing music venue. To do this the amendment seeks to modify the SEPP N-2 by requiring any assessment of noise to be performed inside new apartments with doors and windows closed, thus allowing noise to be attenuated by a well designed residential building. A recent VCAT decision and Practice Note 81 states that the agent of change has the obligation to mitigate noise and that it is unnecessary to consider whether existing noise emissions from a live music venue complies with SEPP N-2.

The wall, window, door, and roof structures at the façade of any future residential development at 29 Johnston Street will have to provide significant noise reduction of approximately 40 decibels at low frequencies to ensure that the developer's requirements under Clause 52.43 are met for Circus Oz operations.

Conclusion

The music noise emissions from a series of musical events held at Circus Oz, 35 Johnston Street, Collingwood was considered in relation to Planning Scheme Amendment VC120 Clause 52.43. Any residential development at 29 Johnston Street will require further acoustic design consideration.

Attachment 3 - Noise and Traffic Reports CAP

Breathe Architecture 35 Johnston Street, Collingwood Noise Impact Assessment

AAc/R001.0

Issue | 10 August 2015

This report takes into account the particular instructions and requirements of our client. It is not intended for and should not be relied upon by any third party and no responsibility is undertaken to any third party.




Job number 244784-00

Arup
Arup Pty Ltd ABN 18 000 966 165
Arup
Level 17
1 Nicholson Street
Melbourne VIC 3000
Australia
www.arup.com



Attachment 3 - Noise and Traffic Reports CAP

Document Verification

Job title		35 Johnston Street, Collingwood		Job number		244784-00	
Document title		Noise Impact Assessment		File reference			
Document ref		AAc/R001.0					
Revision	Date	Filename	R01.0 35 Johnston Street Noise Impact Assessment - ISSUE.docx				
Issue	10 Aug 2015	Description	Issue				
			Prepared by	Checked by	Approved by		
		Name	Jon Osborne	Jon Osborne	Frank Butera		
		Signature					
		Filename					
		Description					
			Prepared by	Checked by	Approved by		
		Name					
		Signature					
		Filename					
		Description					
			Prepared by	Checked by	Approved by		
		Name					
		Signature					

Issue Document Verification with Document



Attachment 3 - Noise and Traffic Reports CAP

Breathe Architecture

35 Johnston Street, Collingwood
Noise Impact Assessment

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Appendices

Appendix A

Acoustic Terminology

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Breathe Architecture

35 Johnston Street, Collingwood
Noise Impact Assessment

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Breathe Architecture

35 Johnston Street, Collingwood
Noise Impact Assessment

1 Introduction

Breathe Architecture is working on a Town Planning application for 35 Johnston Street, Collingwood. The development is to be used by Creative Victoria, as part of the Collingwood Contemporary Arts Precinct. It includes a change of ground floor usage (to a 'Place of Assembly') for Buildings C, E and F as indicated in Figure 1, as well as the outdoor courtyard area.

The exact usage is not confirmed at this stage; however, we understand that events to be held within the building are likely to include:

- Dance events with audience of a maximum of 200 people
- Live music events (amplified and unamplified)
- Theatrical performances
- Markets and other community events held in the outdoor courtyard

Arup has been engaged by Breathe Architecture to undertake an acoustic assessment to investigate potential noise emissions from the development and the impact on the surrounding community as part of the planning application.

Noise from the proposed development has the potential to impact the existing residential dwellings in the vicinity of the subject site. Potential noise sources associated with the operation of the venue includes mechanical services, food trucks, music and patron noise.

Arup has reviewed and undertaken the assessment based on

- Breathe Architecture construction drawing set dated 23 March 2015,
- Circus Oz planning documentation and
- Operational information provided by Creative Victoria and Breathe Architecture.

A glossary of acoustic terminology is provided in Appendix A.

Attachment 3 - Noise and Traffic Reports CAP

Breathe Architecture

35 Johnston Street, Collingwood
Noise Impact Assessment

2 Site Description

2.1 Surrounding Area

The subject site is located at 35 Johnston Street, Collingwood and is currently zoned as Commercial Zone 1 (C1Z). The subject site is bounded by the following:

- Johnston Street and commercial buildings to the north, with residences located at first floor level at 38 Johnston Street, and at 62 Johnston Street, Collingwood.
- The Tote Hotel to the east
- Melbourne Polytechnic and St Joseph's Primary School to the south
- 10 Perry Street residences (warehouse conversion) to the west

The nearest noise-sensitive receiver to Building E is located adjacent to the building at 10 Perry Street. The nearest noise sensitive receivers to Building F are 48 and 62 Johnston Street, located opposite Building F.

The subject site, Contemporary Arts Precinct and noise sensitive receivers are shown in Figure 1.

Figure 1: Site Plan Showing 35 Johnston Street site and local noise-sensitive receivers



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Noise Impact Assessment

2.2 Existing Site

The subject site was previously used as for educational use. It is currently unused.

The acoustic environment of the subject site is dominated by vehicle traffic noise from Johnston Street, and noise from the Tote music venue when operational. The site is shared with Circus Oz offices and Spiegel tent (performance space). Spiegel tent events include rehearsals, circus skill classes and circus shows.

2.3 Proposed Usage

No additional significant fixed mechanical plant is proposed at this stage of the development.

The indoor and outdoor areas are proposed to accommodate a total of 400 patrons, with a maximum of 200 patrons in each of Building E, Building F, and the courtyard.

Events at 35 Johnston Street are expected to include:

- Dance events with audience of a maximum of 200 people
- Live music events (amplified and unamplified)
- Theatrical performances
- Market events eg craft markets, which will operate at weekends only
- Other outdoor events in the courtyard such as community gardening events and outdoor theatrical performances
- Up to two food trucks will use the site a maximum of once per week

In addition:

- Licensed events are expected to occur both indoors and outdoors.
- There are no current plans to install a permanent sound system in the courtyard.
- Sound systems are likely to be installed in one or both of Building E and Building F.
- Maximum venue capacity ie simultaneous occupation of all buildings will not exceed 400 people.

It is understood that music and theatrical events held at 35 Johnston Street are expected to be low budget, small scale productions. As a result, it is understood that future events will not involve significant theatrical equipment, or regular bump in/out operations.

No additional mechanical services equipment is planned for installation at this stage.

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Breathe Architecture

35 Johnston Street, Collingwood
Noise Impact Assessment

3 Noise Criteria

The relevant legislation or guidelines applicable for the assessment of each of the identified potential noise impacts are summarised in Table 1 below.

Table 1: Relevant noise legislation and guidance

Potential Noise Impact	Source of assessment criteria	Status
Mechanical Services Noise Delivery movements	SEPP N-1	Legislation
Music Noise	SEPP N-2	Legislation
Patron Noise	VCAT precedence	Guideline

3.1 Commercial Noise (SEPP N-1)

Within the Melbourne metropolitan area, noise from air-conditioning, ventilation, exhaust and refrigeration equipment and deliveries from commercial premises is governed by *State Environment Protection Policy (Control of Noise from Commerce, Industry and Trade) No. N-1* (SEPP N-1).

The objective of SEPP N-1 is to protect residential areas from noise generated by commercial, industrial or trade premises.

The assessment of noise emissions under SEPP N-1 is based on the calculation of a noise limit at a receiver position, taking into account the land use in the surrounding area and the ambient noise level. Under SEPP N-1 the noise limit is the maximum allowable “level of noise emitted from the commercial, industrial or trade premise”, in a “noise sensitive area” as defined by the Policy.

The noise emission from the site, when corrected for duration and character, is referred to as the 'effective noise level' (L_{eff}). The predicted effective noise level is compared to the noise limit to determine if noise controls are required to comply with SEPP N-1. The effective noise level is the adjusted L_{eq} of the noise source or sources measured over a 30-minute period.

The calculation of noise limits requires the calculation of a zoning level that is based on land use in the surrounding area. The zoning level is then adjusted appropriately, depending on the measured background noise level. SEPP N-1 separates the day into three different time periods - day, evening and night as shown in Table 2.

Table 2: SEPP N-1 time periods

Period	Time period
Day	07.00 – 18.00 hrs
Evening	18.00 – 22.00 hrs
Night	22.00 – 07.00 hrs

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Noise Impact Assessment

3.2 Music Noise (SEPP N-2)

Music noise emissions are controlled in the State of Victoria by *State Environment Protection Policy (Control of Music Noise from Public Premises) N-2* (SEPP N-2). Compliance with SEPP N-2 is mandatory in the State of Victoria.

SEPP N-2 prescribes separate day / evening and night-time noise limits for music noise emission based on measured background levels. SEPP N-2 defines the music noise limit for the subject site are as follows:

- Day / evening period as the current measured background $L_{A90} + 5$ dB
- Night period (after 10.00 pm weekdays and Saturday, 9.00 pm Sundays) as $L_{OCT90} + 8$ dB

Day, evening and night periods are defined as follows:

Table 3: SEPP N-1 time periods

Period	Day of week	Time period
Day	Monday – Friday	07.00 – 18.00 hrs
	Saturday	07.00 – 13.00 hrs
Evening	Monday – Friday	18.00 – 22.00 hrs
	Saturday	13.00 – 22.00 hrs
	Sunday, Public Holidays	07.00 – 21.00 hrs
Night	Monday – Saturday	22.00 – 07.00 hrs
	Sunday	21.00 – 07.00 hrs

3.3 Patron Noise

Patron activity noise from the development may be audible from the nearby residential properties.

Victoria does not have specific regulatory requirements for assessing patron noise. In the absence of legislation, it is proposed that patron noise be based on recent Victorian Civil and Administrative Tribunal (VCAT) decisions related to outdoor patron noise.

Noise limits recommended for this project are based on a recent VCAT decision for developments located in the metropolitan Melbourne area. The patron noise limits a quasi-steady state or *average* patron noise limit based on existing background noise levels.

The patron noise criteria are summarised in Table 4 and it is proposed the time periods are based on the SEPP N-1 time periods outlined in Table 2.

Table 4: Proposed Patron Noise Limits at residential receiver

Time Period	Patron Noise Criteria, dBL_{Aeq}
Day	The higher of L_{Aeq} 50 or $L_{A90,BG} + 15$
Evening	The higher of L_{Aeq} 45 or $L_{A90,BG} + 10$
Night	The higher of L_{Aeq} 40 or $L_{A90,BG} + 5$

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35 Johnston Street, Collingwood
Noise Impact Assessment

4 Noise Survey

Arup undertook a noise assessment between 26 March and 1 April 2015 which comprised a site survey, attended noise measurements and continuous noise logging over 5 days.

All equipment held current NATA certification and calibration was checked on-site before and after measurements with no significant drift detected.

Locations of noise monitoring devices are shown in Figure 2.

One noise monitoring device was located approximately 5 m above ground level, in the private laneway between the subject site and the 10 Perry Street residential development. This location was selected to be representative of the background noise levels at the nearest affected noise sensitive receiver to Building E.

The second noise monitoring device was located 1.5m above ground level at the corner of the subject site, approximately 5m south of Johnston Street and approximately 10m from the Tote Hotel to the East. This location was representative of the nearest residential receiver on Johnston Street to Building F.

Based on site observations, road traffic noise on Johnston Street was the dominant noise source in the vicinity of the subject site for weekdays and music noise from The Tote was mostly dominant on weekends.

The measured noise levels were used to inform the noise limits discussed in Section 5.

Figure 2: Site map showing noise monitoring device locations



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35 Johnston Street, Collingwood
Noise Impact Assessment

5 Derived Noise Limits

5.1 Commercial Noise (SEPP N-1) Noise Limits

The SEPP N-1 noise limits have been determined for the day, evening and night-time periods based on the average measured background noise levels. The noise limits at the nearest affected residential properties are presented in Table 5 and Table 6.

Table 5: SEPP N-1 noise limits at 10 Perry Street, dB re 20µPa.

Period	Zoning Level, dBL _{Aeq}	Background Noise Level, dBL _{A90}	SEPP N-1 Background Noise Level Designation (High/Neutral/Low)	Noise Limit, dBL _{Aeq}
Day	51	42	Neutral	51
Evening	45	38	Neutral	45
Night	40	37	Neutral	40

Table 6: SEPP N-1 noise limits at 35 Johnston Street, dB re 20µPa.

Period	Zoning Level, dBL _{Aeq}	Background Noise Level, dBL _{A90}	SEPP N-1 Background Noise Level Designation (High/Neutral/Low)	Noise Limit, dBL _{Aeq}
Day	54	52	High	58
Evening	47	52	High	55
Night	42	44	High	47

5.2 Music Noise (SEPP N-2) Noise Limits

The SEPP N-2 day/evening and night-time limits at the nearest noise sensitive properties to the subject site based on measured background noise levels are presented in Table 7 and Table 8 respectively.

Table 7: SEPP N-2 day and evening noise limit at residential locations, dB re 20µPa.

Location	Period	Background noise level, dBL _{A90}	+ 5 dB adjustment	Noise Limit, dBL _{Aeq}
1 (10 Perry St)	Day	52	5	57
1 (10 Perry St)	Evening	52	5	57
2 (35 Johnston St)	Day	43	5	48
2 (35 Johnston St)	Evening	38	5	43

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Table 8: SEPP N-2 night-time noise limit at residential locations, dB re 20µPa.

Description	Sound Pressure Level, dB Octave Band Centre Frequency, Hz						
	63	125	250	500	1k	2k	4k
10 Perry Street Background noise level, L ₉₀	54	49	44	41	40	32	21
Plus 8 dB adjustment	+ 8	+ 8	+ 8	+ 8	+ 8	+ 8	+ 8
10 Perry Street noise limit, L₁₀	52	57	52	49	48	40	29
35 Johnston Street Background noise level, L ₉₀	41	38	35	30	34	29	20
Plus 8 dB adjustment	+ 8	+ 8	+ 8	+ 8	+ 8	+ 8	+ 8
62 - 70 Johnston Street noise limit, L₁₀	49	46	43	38	42	37	28

Night-time SEPP N-2 noise limits have been based on attended background octave-band noise levels measured at 15 Perry Street and 141 Johnston Street on Thursday 6 August, 2015 between 22.00 hrs – 23.00 hrs (10.00 pm – 11.00 pm) to reflect the background noise levels during the night-time period. The Johnston Street location was considered to assess music noise levels from the operation of the Tote Hotel.

5.3 External Patron Noise

External patron noise limits have been based on the measured background noise levels. Patron noise limits have been based on lowest measured background noise levels for the day, evening and night-time periods, averaged over the entire period per day. The patron noise limits are summarised in

Table 9.

Table 9: External Patron Noise Limits, dB re 20µPa.

Time Period	Average Measured Background Noise Level, dBL _{Aeq}	Patron Noise Limit Adjustment	Patron Noise Criteria, dBL _{Aeq}
10 Perry Street			
Day period	43	+15	58
Evening period	38	+10	48
Night period	37	+5	42
35 Johnston Street			
Day period	52	+15	67
Evening period	52	+10	62
Night period	44	+5	49

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6 Assessment Methodology

6.1 Commercial Noise (SEPP N-1)

The cumulative noise emissions from the fixed mechanical plant, food trucks and deliveries must comply with the SEPP N-1 noise limits are discussed in Sections 6.1.1 to 0 below.

6.1.1 Mechanical Plant

We understand that there are no current plans for significant mechanical services to be installed as part of this development.

Should this change in the future, mechanical services noise shall be assessed against and shall comply with the SEPP N-1 limits provided in this document.

6.1.2 Food Trucks

Mechanical ventilation noise from food trucks is assessable under SEPP N-1.

Noise from food trucks has been assessed for the potentially nearest affected residential properties located at 38 and 62 Johnston Street, and at 10 Perry Street, Collingwood

The predicted noise level of food trucks has been based on Arup noise measurements of food trucks from similar projects. This noise source has been used based on the following assumptions:

- The primary source of noise emission is the exhaust fan
- The food truck is powered by connection to mains (i.e. the vehicle engine was not operating)

The noise prediction considers two trucks operating simultaneously and noise attenuation due to distance and shielding provided by existing buildings.

The food truck sound power level used for the noise prediction is provided in Table 10. Table 10: Sound power level used for food truck noise prediction, dB re 1 pW

Description	Sound Power Level, dB re 1 pW Octave Band Centre Frequency, Hz							
	dB(A)	63	125	250	500	1k	2k	4k
Typical food truck source sound power level	82	80	78	77	79	77	76	71

The predicted noise levels and noise limit compliance are discussed in Section 7.1.

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6.2 Music Noise (SEPP N-2)

Installed sound systems are understood to be likely as part of the development.

- Noise predictions have been undertaken to assess compliance with the SEPP N-2 for the day, evening and night time under the conditions provided in Loudspeakers located inside have been assessed based on the existing structure with windows and doors closed.

Maximum allowable noise limits are provided below :

- Loudspeakers located inside have been assessed based on the existing structure with windows and doors closed.
- Maximum allowable noise limits are provided below in Section 7.2.

6.3 Patron Noise

Noise modelling was undertaken to investigate patron noise emissions to the nearby noise sensitive receivers from the subject site.

Details of noise predictions are as follows:

- Noise emissions from the subject site have been predicted for the subject site for 200 patrons located outdoors, and 200 patrons located in either of Building E or Building F based on the expected worst case operational conditions.
- Patron source noise levels have been calculated using by applying methodology provided in *Prediction of Noise from Small to Medium Sized Crowds*¹. Application of this paper is considered best practice for prediction of patron noise in beer gardens and is accepted by VCAT.

The patron sound power level frequency spectrum used for L_{Aeq} prediction is based on a typical voice spectrum and is provided Table 11 below.

Table 11: Sound power level used for patron noise prediction, dB re 1 pW

Description	Sound Power Level, dB re 1 pW Octave Band Centre Frequency, Hz							
	dB(A)	63	125	250	500	1k	2k	4k
Average 200 patron area source sound power level	99	72	80	89	97	95	91	84

The patron source noise levels were verified using Association of Australian Acoustical Consultants (AAAC) patron noise data and patron noise measurements from other Arup projects.

¹ *Prediction of Noise from Small to Medium Sized Crowds*- MJ Hayne, November 2011.

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Breathe Architecture

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Noise Impact Assessment

7 Results and Assessment

7.1 Commercial Noise (SEPP N-1)

Noise from existing fixed mechanical plant is not anticipated to be a significant noise source at this stage of the project, since no new mechanical services equipment is proposed.

Food trucks therefore comprise the dominant noise source assessable under SEPP N-1. Noise levels from food trucks have been predicted to noise-sensitive residential receivers at 10 Perry Street, 48 and 62 Johnston Street for two trucks operating simultaneously. The predicted noise levels for all time periods are summarised in

Table 9.

Table 12: Food Truck Noise, dB re 20µPa.

Time Period	SEPP N-1 Noise Limit, dBL _{Aeq}	Predicted Noise Level, dBL _{Aeq}	Complies with SEPP N-1 Noise Limit?
Day period	58	46	✓
Evening period	55		✓
Night period	47		✓

Noise from deliveries is expected to occur during daytime hours and is not expected to exceed noise from food trucks.

In summary, the cumulative commercial noise emissions assessable under SEPP N-1 are predicted to comply with the SEPP N-1 noise limits.

7.2 Music Noise (SEPP N-2)

Music noise emitted from the subject site must comply with SEPP N-2 noise limits at the nearby noise sensitive receivers. The subject site must control the emission of noise through the selection, location, and orientation of loudspeakers, internal finishes and resulting music noise levels from internal and external loudspeakers.

In order to comply with SEPP N-2 music noise limits, music noise shall not exceed the levels provided in Table 13.

Table 13: Maximum permissible music noise levels to comply with SEPP N-2

Location	Sound Power Level, dB re 1 pW Octave Band Centre Frequency, Hz							
	dB(A)	63	125	250	500	1k	2k	4k
Building E - Limiting music noise spectrum	100	83	93	92	95	99	84	80
Building F - Limiting music noise spectrum	110	96	98	99	101	109	98	96
Courtyard - Limiting music noise spectrum	45	49	46	43	38	42	37	28

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It is recommended that compliance with the music noise limits be achieved by conducting noise commissioning measurements to control the maximum output levels.

If music noise levels are found to exceed the criteria, a noise limiter that reduces the sound level output of the external loudspeakers may be considered.

7.2.1 Indoor Music Noise - Existing Building Construction

The maximum allowable noise levels for music include consideration of the existing Building E and Building F constructions. The assessment has been completed with all external windows and doors closed.

Based on this, no further acoustic mitigation is recommended at this stage.

7.3 Patron Noise

The predicted patron noise level at the most exposed receiver locations are presented in Table 14.

Table 14: Patron Noise, dB re 20µPa.

Location	Patron Noise Limit, dBL _{Aeq}			Predicted Noise Level, dBL _{Aeq}	Complies with Patron Noise Limit?		
	Day	Evening	Night		Day	Evening	Night
Courtyard	58	48	42	36	✓	✓	✓
Building E	58	48	42	47	✓	✓	x
Building F	67	62	49	30	✓	✓	✓

Patron noise is predicted to meet the most onerous (night-time) noise limit under the proposed worst case (ie maximum capacity) operational conditions for Building F and the courtyard.

Patron noise is predicted to exceed the most onerous (night-time) noise limit under the worst case (ie maximum capacity) operational conditions for Building E and as result it is recommended that events do not operate during this period.

Attachment 3 - Noise and Traffic Reports CAP

Breathe Architecture

35 Johnston Street, Collingwood
Noise Impact Assessment

8 Summary

8.1 Summary of Recommendations for City of Yarra to Consider

All commercial noise emitted from food trucks and mechanical services shall comply with SEPP N-1 noise limits at the nearest residential receivers.

All music noise including internal and external music noise shall comply with SEPP N-2 at the nearest residential receivers.

All external patron noise, including from buildings and external areas, shall meet patron noise limits at the nearest noise-sensitive receivers as provided in Table 15.

Table 15: External Patron Noise Limits, dB re 20µPa.

Time Period	Patron Noise Criteria, dBL _{Aeq}
10 Perry Street	
Day period	58
Evening period	48
Night period	42
35 Johnston Street	
Day period	67
Evening period	62
Night period	49

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Breathe Architecture

35 Johnston Street, Collingwood
Noise Impact Assessment

8.2 Summary of Study

Arup has undertaken an acoustic impact assessment of the noise emissions from the proposed change of use to ground floor Buildings C, E and F at 35 Johnston Street, Collingwood in response to the requirements of SEPP N-1 and SEPP N-2.

The noise assessment has considered commercial, music and patron noise emissions from the subject site to nearby noise sensitive receivers. Noise modelling has been undertaken to assess noise emissions against the relevant legislation.

The noise assessment outcomes are as follows:

- Commercial noise emitted from the subject site is predicted to comply with the SEPP N-1 limits at the nearest noise sensitive receiver; mechanical services noise is not predicted to exceed noise limits.
- Music noise emitted from the subject site shall comply with SEPP N-2 limits at the nearest noise sensitive receiver through compliance with limiting maximum noise levels. Commissioning measurements and the consideration of a noise limiter are required to ensure that these are met.
- Patron noise complies with established noise limits for patron noise to local noise-sensitive receivers based on recent VCAT decisions, with the exception of maximum occupancy patron noise from Building E to 10 Perry Street, which complies for day and evening periods only.
- Prior to commencing use at the subject site a Noise Management Plan shall be submitted to demonstrate compliance with appropriate noise policies.

On this basis the proposed development is predicted to meet the proposed noise criteria based on the existing building construction with windows and doors closed, with the exception of patron noise during the night-time period from Building E to 10 Perry Street.

Should events be operated in Building E during the night-time period, the building construction and internal finishes would require acoustic improvement.

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Appendix A

Acoustic Terminology

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Breathe Architecture

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Background Noise Level

The background noise level is the noise level that is generally present at a location at all or most times. Although the background noise may change over the course of a day, over shorter time periods (e.g. 15 minutes) the background noise is almost-constant. Examples of background noise sources include steady traffic (e.g. motorways or arterial roads), constant mechanical or electrical plant and some natural noise sources such as wind, foliage, water and insects.

Decibel

The decibel scale is a logarithmic scale which is used to measure sound and vibration levels. Human hearing is not linear and involves hearing over a large range of sound pressure levels, which would be unwieldy if presented on a linear scale. Therefore a logarithmic scale, the decibel (dB) scale, is used to describe sound levels.

An increase of approximately 10 dB corresponds to a subjective doubling of the loudness of a noise. The minimum increase or decrease in noise level that can be noticed is typically 2 to 3 dB.

dB(A)

dB(A) denotes a single-number sound pressure level that includes a frequency weighting (“A-weighting”) to reflect the subjective loudness of the sound level.

The frequency of a sound affects its perceived loudness. Human hearing is less sensitive at low and very high frequencies, and so the A-weighting is used to account for this effect. An A-weighted decibel level is written as dB(A).

Some typical dB(A) levels are shown below.

Sound Pressure Level dB(A)	Example
130	Human threshold of pain
120	Jet aircraft take-off at 100 m
110	Chain saw at 1 m
100	Inside nightclub
90	Heavy trucks at 5 m
80	Kerbside of busy street
70	Loud stereo in living room
60	Office or restaurant with people present
50	Domestic fan heater at 1m
40	Living room (without TV, stereo, etc)
30	Background noise in a theatre
20	Remote rural area on still night
10	Acoustic laboratory test chamber
0	Threshold of hearing

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L_{90}

The L_{90} statistical level is often used as the “average minimum” or “background” level of a sound level that varies with time.

Mathematically, L_{90} is the sound level exceeded for 90% of the measurement duration. As an example, 45 dB $L_{A90,15\text{min}}$ is a sound level of 45 dB(A) or higher for 90% of the 15 minute measurement period.

L_{eq}

The ‘equivalent continuous sound level’, L_{eq} , is used to describe the level of a time-varying sound or vibration measurement.

L_{eq} is often used as the “average” level for a measurement where the level is fluctuating over time. Mathematically, it is the energy-average level over a period of time (i.e. the constant sound level that contains the same sound energy as the measured level). When the dB(A) weighting is applied, the level is denoted dB L_{Aeq} . Often the measurement duration is quoted, thus $L_{Aeq,15\text{ min}}$ represents the dB(A) weighted energy-average level of a 15 minute measurement.

Frequency

Frequency is the number of cycles per second of a sound or vibration wave. In musical terms, frequency is described as “pitch”. Sounds towards the lower end of the human hearing frequency range are perceived as “bass” or “low-pitched” and sounds with a higher frequency are perceived as “treble” or “high pitched”.

Sound Power and Sound Pressure

The sound power level (L_w) of a source is a measure of the total acoustic power radiated by a source. The sound pressure level (L_p) varies as a function of distance from a source. However, the sound power level is an intrinsic characteristic of a source (analogous to its mass), which is not affected by the environment within which the source is located.

Attachment 3 - Noise and Traffic Reports CAP

COLLINGWOOD ARTS PRECINCT - NOISE MANAGEMENT PLAN

Attachment B Existing Spiegeltent Noise impact analysis and management plan

Attachment 3 - Noise and Traffic Reports CAP

CIRCUS OZ NOISE and AMENITY ACTION PLAN

Relating to new occupancy at 35 Johnston St., Collingwood, aka 20 Perry St Collingwood.



2 October 2013 – Anni Davey

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Attachment 3 - Noise and Traffic Reports CAP

1. Introduction

Circus Oz, with the support of the State Government, will be moving into a new purpose built rehearsal and training facility at the old TAFE site on Johnston St in Collingwood in early 2014. The new facility will also host all of Circus Oz's administration, set and costume workshops, as well as its public classes program and corporate engagement (sponsor functions and leadership workshops). While the facility is not designed as a performance venue for the company, the Circus Oz Spiegeltent will be housed at this location and will be programmed with community events, circus performances and physical theatre from independent artists for 25 weeks every year.

This Noise and Amenity Action Plan (NAAP) is submitted as a requirement of the Planning Permit (Number PLN11/0600) prior to use commencing (current estimate of use commencing 28 January 2014). The NAAP has been prepared in accordance with the Yarra Planning Scheme, Clause 22.09 – 4.3

Prior to occupancy Circus Oz will establish an existing base noise level chart defining existing noise levels at various positions on and around the proposed site of the Spiegeltent at various times of the day and week using properly calibrated noise measurement instrumentation. This chart will be kept on record.

After Circus Oz has moved into the new premises, noise levels at the various positions around the site will be measured and compared to the existing background noise levels measured. It is proposed to perform the noise level measurements at similar times to the existing base level times to determine the acoustic impact of the Spiegeltent during a performance and allow a direct comparison of the noise emissions from the Spiegeltent to the typical existing background noise levels being experienced at the present. Noise measurements will be performed regularly in the first year of tenancy to inform strategies of compliance with planning permit restrictions.

Noise emissions testing will continue to be regularly conducted by the Circus Oz sound engineer and/or desk operator during sound checks and performances in the Spiegeltent to test and demonstrate ongoing compliance with specified noise emissions levels.

2. Outline of proposed activities within premises

Circus Oz's activities in the new building will include Public Classes providing circus training to children and adults, schools and community groups, the High Flying Teams program providing leadership and team building workshops using circus skills for corporate clients, functions or events for sponsors, rehearsals for the touring Circus Oz show (including some audience attended final rehearsals) and training for the core ensemble of 12 performers and the independent circus sector. It will also house set building and costume construction workshops, and administrative offices for 35 permanent staff.

In the open external area adjoining Tote Hotel and the Neighbourhood Justice Centre, Circus Oz will erect their Spiegeltent. This is proposed as a 200 seat bar and performance venue that will operate as such for 25 weeks of the year (not necessarily

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concurrent). It is proposed to program performances in the Spiegeltent by Circus Oz, independent groups and artists, and community groups. It is also envisaged to occasionally host festivals and community celebrations in and around the rest of the building. Most of these various functions will involve live or amplified music. During Spiegeltent performances or functions only the Johnston St entrance will enable public access to the whole site.

An onsite premises liquor license is currently being prepared to cover the area including and surrounding the Spiegeltent during functions and events, and also public areas inside the building to enable the serving of alcohol for consumption on-site during 'final rehearsals' (attended by up to 100 invited audience) and other corporate or community functions held on the premises or in the Spiegeltent.

Circus Oz is also keen to encourage the community to visit and use the facilities by providing some landscaped areas and equipment (eg. two outdoor ping pong tables).

3. Identification of closest neighbours to property

The closest noise sensitive receivers to the proposed new Circus Oz facility are the Neighbourhood Justice Centre (NJC), who have courtrooms adjoining the boundary of the Circus Oz site at the Eastern boundary, and the Tote Hotel who also share a boundary at the north eastern corner of the site. The nearest residential properties to the proposed site include 15A Perry St (70 -80 metres from the site boundary) and on Johnston Street approximately 60 – 70 metres from the proposed Spiegeltent location)....

Neighbourhood Justice Centre: The NJC court-rooms are approximately 25 metres from the planned site for the Spiegeltent. Circus Oz has made an undertaking with the NJC that sound checks or performances in the Spiegeltent will be scheduled to avoid coinciding with NJC court sessions (see schedule below). Compliance will be monitored through regular noise level surveys conducted by the Circus Oz sound engineer and/or desk operator during sound checks and performances (this is consistent with current practice; noise level output is regularly recorded and included on show reports of performances).

NEIGHBOURHOOD JUSTICE CENTRE COURT SESSIONS (at 9 July 2013)

MONDAY	9.30am– 4pm	Magistrates Court
TUESDAY	9.30am– 4pm	Victorian Civil and Administrative Tribunal (VCAT)
WEDNESDAY	9.30am– 4pm	Magistrates Court
THURSDAY	9.30am– 4pm 4pm – 7pm	- Magistrates Court - VCAT twilight sittings (every second Thursday*)
FRIDAY	9.30am– 4pm	VCAT

* NJC has indicated that there are currently plans that could shift the fortnightly VCAT twilight sittings to Tuesday evenings.

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The Tote Hotel: The Tote's band room is approximately 25 – 28 metres from the Spiegeltent. Circus Oz and The Tote's proprietors (Jon Perring and Andrew Portokallis) have drafted an agreement around the scheduling of sound checks and performances in order to mitigate any potential conflict over noise levels.

Circus Oz will conduct ambient noise tests prior to our tenancy in order to establish a base level from which to compare the effect of our tenancy on ambient noise levels at various times of the day and week.

4. Procedures to be undertaken by staff in the event of complaints by a member of the public, the Victoria Police, an 'authorised officer' of Council or an officer of Liquor Licensing Victoria.

Circus Oz has been responsibly managing major public events for its 35 years of operation. Circus Oz's events staff regularly produce public performance seasons (annually at Birrarung Marr, bi-annually in Sydney and regularly in other capital cities) drawing crowds of up to 1400 people in temporarily licensed premises. The organisation has an excellent record of adherence to regulations and compliances.

During general operations Circus Oz's General Manager will be responsible for ensuring noise from people movement from the premises is kept at a minimum to reduce any nuisance noise to adjoining occupiers or cause detriment to the amenity of the neighbourhood.

During the operation of performances (including Spiegeltent program), festivals, sponsor events and functions, either the Circus Oz event manager, or an authorised person on duty, will be on the premises to receive and deal with complaints.

The event manager will be responsible for ensuring that to their best endeavours no disturbance occurs or emanates from or during the event which would be likely to cause a nuisance to adjoining occupiers or cause detriment to the amenity of the neighbourhood.

Entry to the premises will be made freely available during all operating hours to a member of the Victoria Police, an 'authorised officer' of the Responsible Authority or an officer of Liquor Licensing Victoria, to carry out any investigations associated with the sale or consumption of alcohol on the site.

In the event of receiving a substantive complaint, a representative of Circus Oz will seek to speak personally with the complainant to determine the specific nature of the complaint and to negotiate mutually acceptable measures to mitigate the possibility of reoccurrence.

If complaints relate to a regular breach of conditions regarding noise emissions (ie noise regularly exceeds the noise levels specified by the planning permit) Circus Oz will consider initiating more stringent sound level controls, and/or more, or more effective sound barriers strategically placed.

Circus Oz management will retain a logbook of complaints or incidents for review.

5. Details of staffing arrangements including numbers and working hours of all security staff, bar staff, waiters, on-premises manager, and other staff.

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DAILY OPERATIONS		
	Number of staff	Operating Hours
Office/Administrative	22 - 25	9am – 6pm Monday - Friday
Costume workshop	1 – 6 people depending on level of demand	10am – 6pm Monday – Friday Peak Period April/May/June annually
Set Workshop	1 – 4 people depending on demand	10am – 6pm Monday – Friday Peak Period April/May/June annually
Rehearsals	12 performers 8 technical support 3 artistic direction	Monday – Friday 10am – 6pm 14 - 20 weeks per year Peak period April/May/June
Public Classes	1 – 12 trainers ratio trainers to participants 1/8	Intended 8am – 10am and 4pm – 9.00pm Monday – Friday 9am – 5pm Saturday and Sunday
SPIEGELTENT EVENTS (CORPORATE FUNCTIONS, COMMUNITY EVENTS, PERFORMANCE PROGRAM)		PERMISSABLE OPERATING HOURS 10am – 9pm Monday – Wednesday 1pm – midnight Thursday - Sunday
Security Staff	2, audience < 100 3, audience 100<200	1/2 hour prior to advertised start time until 1/2 hour after finish of performance
Event Manager	1	1 hour prior to advertised start time until doors close (ie event manager locks up)
Box Office	1	1 hour prior to performance until ½ hour after performance commence as required
Bar staff	1 - 2	1 hour prior to performance until 1 hour after performance finishes, or until close as required
Technical staff	1 - 4	2 hours prior to performance until ½ hour after performance finishes as required
Performers	As per event needs	2 hours prior to performance until ½ hour after performance finishes as required
PERFORMANCES, FESTIVALS, SPONSOR EVENTS AND FUNCTIONS		PERMISSABLE OPERATING HOURS 9am – Midnight Monday - Sunday
Security Staff	2, audience < 100 with additional 1 security staff per increase of 100	1 hour prior to advertised start time until 1/2 hour after finish of performance. 1 security staff

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	people or part thereof	remains until close and lock up
Front of House Manager	1	1 hour prior to advertised start time until doors close (ie FOH manager locks up)
Box Office	1	1 hour prior to performance until ½ hour after performance commence, as required
Bar staff	1 - 2	1 hour prior to performance until 1 hour after performance finishes, or until close, as required
Technical staff	1 - 4	2 hours prior to performance until ½ hour after performance finishes, as required
Performers	As per event needs	2 hours prior to performance until ½ hour after performance finishes, as required

6. Details of training provided for bar staff in the responsible serving of alcohol.

All Circus Oz events staff engaged in the service of alcohol are required to have undertaken a 'Responsible Serving of Alcohol' course, as approved by the Director of Liquor Licensing.

The 'Responsible Serving of Alcohol' course includes training for staff on dealing with intoxicated persons. If necessary, management or another appropriately trained staff member would escort the intoxicated person/s from the premises

7. Lighting**(a) within the boundaries of the premises.**

The external areas on the site are adequately lit and fitted with movement sensors. The area around the Spiegeltent will be floodlit during hours of operation with ambient lighting inside the tent including special performance lighting during performances.

(b) security lighting outside the premises.

The external areas on the site are adequately lit with movement sensor lights, and exit/entry points are monitored by CCTV cameras.

8. Details of the provision of music including the frequency and hours of entertainment provided by live bands and DJs.

Live music is an integral part of the Circus Oz show and the ensemble includes a three-piece band. The new build on site includes a properly equipped and sound proofed band rehearsal room and smaller practice rooms. Final rehearsals will be held in rehearsal room 1 which is acoustically treated and centrally located within the site. Noise emissions from the rehearsal rooms will not impact on neighbouring properties.

Live and/or recorded music will accompany most performances or functions held in the Spiegeltent which are permitted to be held between the hours of 10am to 9pm Monday to Wednesday and 1pm to Midnight Thursday to Sunday and for 25 weeks of the year. Circus

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Oz has undertaken to restrict noise level emissions from the Spiegeltent to reduce the impact on amenity for neighbours in accordance with the Planning Permit conditions. Noise level emissions will be regularly monitored and adjusted if necessary to ensure compliance.

Live music and/or recorded music may also feature as part of occasional festivals or community events held on site. These events can cater to audience numbers of up to 250 people unless with prior written consent of the Responsible Authority.

9. Details of waste management plan including storage and hours of collection for general rubbish and bottles associated with the licensed premises.

The waste storage area on the new site is located at the Eastern border in the area just south of the car parking area. Circus Oz will employ a reputable Waste and Recycling Contractor for regular waste collection as outlined in the following table

■ Waste Bin Schedule (typical Circus Oz operations)

Waste stream	Generation rate (L/mth)	Bin size / type	Bin Quantity	Collection frequency	Bin area* (m ²)
General waste	1,500	240L wheelie bin	2	Weekly	0.86
Commingle recyclingables	3,000	240L wheelie bin	3	Weekly	1.29
Paper and cardboard	300	240L wheelie bin	2	Monthly	0.86
Workshop waste	1,500	1,500L skip	1	As required	2.63
Bin Storage area					5.7

Additional waste and recycling bins will be sourced from the Waste and Recycling Contractor for events. At the time of planning the event, the Waste and Recycling Contractor can provide advice as to the number and type of bins best suited to the event. However, to provide an indication of bin storage area requirements, it is estimated that Circus Oz will need to source up to an additional six (6) waste bins (240 litre wheelie bins) and six (6) commingled recycling bins (240 litre wheelie bins) to cater for up to 250 people (pers. comm Lisa Coffa, City of Yarra).

All waste and recycling collections are to be undertaken between the hours of 7:00am and 7:00pm, Monday to Friday and 9:00am to 8:00pm Saturday and Sunday.

Please see Circus Oz Waste Management Plan for more detail, also submitted to Council as a Permit Condition.

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10. Table identifying all noise sources associated with the premises likely to impact on nearby residential property.

AREA	NOISE TYPE	NOISE SOURCE	TIMES	MEASURES TO BE UNDERTAKEN TO ADDRESS ALL NOISE SOURCES IDENTIFIED, INCLUDING ON AND OFFSITE NOISE ATTENUATION MEASURES.
Spiegeltent	Amplified music noise	Noise survey performed before performances	Weekdays from 4pm except every second Thursday (or when VCAT evening sittings are scheduled at the NJC)	Noise Level measurements surveys / sound system adjusted to comply with noise emissions levels specified by Planning permit. Written documentation presenting results to be kept on file
Spiegeltent	Amplified music noise, crowd noise	performances, festivals, sponsor events and functions	Confined to hours as per planning permit. 10am – 9pm Monday to Wednesday 1pm – midnight Thursday to Sunday and may only be scheduled for a maximum of 25 weeks per calendar year.	Noise Level measurements surveys / sound system adjusted to comply with noise emissions levels specified by Planning permit. Results to be documented and kept on file.
Rehearsal rooms	Music noise	Rehearsals	Business hours 10 – 6 weekdays	Acoustic treatment of rooms should contain noise. Noise survey checks.
Perry St Entrance	Excited children	Participants arriving/leaving public classes	After school program 4 – 6.30pm weekdays, kids classes 10am - 4.30pm weekends	Monitor. Encourage consideration of neighbours via announcements at end of class
Perry St entrance	Smaller truck movements	Delivery vehicles loading and unloading	During business hours	Monitor
Johnston St entrance	Crowd noise	Audience arriving/leaving attended rehearsals	2 – 4 times per year. Between 1.30pm and 9pm Monday – Wednesday. Between 1.30pm and 11pm Thursday - Sunday	Monitor. Encourage consideration of neighbours via announcement at end of rehearsal
Johnston St entrance/ loading dock	Small truck movement	Spiegeltent functions, deliveries	Business hours	regular noise surveys to be performed
Johnston St entrance	Rowdy behaviour	Audience members leaving the Spiegeltent after night time performances	9pm Monday – Wednesday 11pm Thursday – Sunday maximum 25 weeks per year	Visible signage at tent egress points. Announcements from FOH staff asking for consideration of neighbours.
Johnston St entrance/ loading dock	Semi-trailer movement	Loading and unloading events are restricted to 8 per calendar year	Semi trailer accesses and leaves site between midnight and 7am (to avoid traffic on Johnston st)	Monitor
Workshop	Machinery noise	Air compressor driven tools	During business hours	Monitor

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11. Any other measures to be undertaken to ensure minimal amenity impacts from the licensed use.

PRIOR TO OCCUPANCY	ASSESS CURRENT NOISE LEVELS AT SITE (PRE CIRCUS OZ TENANCY)		Emissions readings at various times of day and week at specific points on site with close proximity to the NJC, the Tote, the nearest neighbour.	Reputable Noise Consultant such as Marshall Day or Audiometric & Acoustic Services
	ASSESS ACOUSTIC PERFORMANCE OF SPIEGELTENT		Confirm construction materials. Engage acoustic Engineer to calculate sound leakage levels.	- as per Planning Permit conditions Construction has been reviewed
	COMMUNICATE WITH LOCAL COMMUNITY		Inform community about Circus Oz 's intended activities on site in particular Spiegeltent programming	- host information sessions inviting local community (City of Yarra, NJC, The Tote, local schools, NMIT, local traders) - Attend Smith St Traders Association meetings.
	NEGOTIATE WITH NEIGHBOURS		Agreements made with the Tote and with NJC regarding scheduling of noisy events, and emission levels.	- Draft agreement with The Tote (complete) - Agreement with NJC (avoid court session times as per schedule above)
TRANSITION	IDENTIFY BIG NOISE EMISSION EVENTS			<ul style="list-style-type: none"> - Spiegeltent performances - Spiegeltent rehearsals - Special festival events - Large truck movements - Waste collection - use of air compressor (workshop)
	IDENTIFY NOISE EMISSION POINTS AT NEW PREMISES		<ul style="list-style-type: none"> - Identify noisy areas - measure proximity from neighbours 	<ul style="list-style-type: none"> - waste management plan - architectural
	IDENTIFY TYPICAL NOISE EMISSION TIMES		Typical scheduling of noisy events	<ul style="list-style-type: none"> - waste management plan - Circus Oz scheduling – classes, rehearsals, rehearsals with audiences - Spiegeltent programming - Liquor licence
TENANCY	MONITOR NOISE EMISSIONS		Emissions readings during Spiegeltent operations at specific points on site with close proximity to the NJC, the Tote, the nearest neighbour.	- Sound Operator on duty using calibrated sound level instrumentation
			Emissions readings during workshop operations at specific points on site with close proximity to the NJC, the Tote, the nearest neighbour.	- Sound engineer using calibrated sound level instrumentation
			Emissions readings during workshop operations at specific points on site with close proximity to the NJC, the Tote, the nearest	Sound engineer using calibrated sound level instrumentation

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			neighbour.	
	MANAGE NEIGHBOURS EXPECTATIONS		Regular communication and engagement with neighbours	
	REVIEW COMPLIANCE STRATEGIES		<ul style="list-style-type: none"> - effectiveness of noise barriers - effectiveness of restrictions on noise emissions levels - effectiveness of waste management strategy 	<ul style="list-style-type: none"> - Monitor waste management practice - keep records of noise emissions
	ADJUST COMPLIANCE STRATEGIES		If necessary	

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VICTORIA**



Acoustic Assessment for the Proposed Circus Oz Site

ACOUSTIC ASSESSMENT FOR THE PROPOSED
CIRCUS OZ SITE AT NO.35 JOHNSTON ST.
COLLINGWOOD.

- Final for Planning Submission (Rev B)
- 18th July 2011

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Acoustic Assessment for the Circus Oz Site - 35 Johnston St. Collingwood.



Acoustic Assessment for the Proposed Circus Oz Site

ACOUSTIC ASSESSMENT FOR THE PROPOSED CIRCUS OZ
SITE AT NO.35 JOHNSTON ST. COLLINGWOOD

- Final for Planning Submission (Rev B)
- 18th July 2011

Sinclair Knight Merz
ABN 37 001 024 095
Floor 11, 452 Flinders Street
Melbourne VIC 3000
PO Box 312, Flinders Lane
Melbourne VIC 8009 Australia
Tel: +61 3 8668 3000
Fax: +61 3 8668 3001
Web: www.skmconsulting.com

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Glossary

Term	Description
dB	Decibel – Sound Pressure Level expressed in decibels is 20 log of the ratio between the measured sound pressure level and the reference pressure. The reference pressure is 0.000002 Pascal (Newtons per square meter), the threshold of hearing.
dB(A)	A Sound Pressure Level where the sound is filtered in accordance with the A-weighting scale. The A weighting scale is a weighting scale which generally corresponds to the inverse of the 40 dB (at 1 kHz) equal-loudness curve. The A weighting parallels the sensitivity of the human ear when it is exposed to normal levels.
L_{A10}	The A weighted sound pressure level that is exceeded for 10% of the measurement period (approximately the average maximum noise level)
L_{A10oct}	Means the Linear Sound Pressure Level for a specified octave band that is exceeded for 10% of the time
L_{A90}	The A weighted sound pressure level that is exceeded for 90% of the measurement period (represents the background noise level)
L_{A90oct}	Means the Linear Sound Pressure Level for a specified octave band that is exceeded for 90% of the time
L_{Aeq}	The equivalent continuous sound level. The steady dB(A) level which would produce the same A weighted sound energy over a stated period of time as the specified time – varying sound.
Day Period	The time between 0700 and 1800 hours
Evening Period	The time between 1800 and 2200 hours
Night Period	The time between 2200 and 0700 hours

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Acoustic Assessment for the Circus Oz Site - 35 Johnston St. Collingwood.



1. Introduction

The SKM acoustics team was requested to perform an acoustic assessment on behalf of Arts Victoria to determine the acoustic suitability of the proposed Circus Oz Site located at No. 35 Johnston St. Collingwood.

It is proposed to demolish two sections of the existing buildings on the site and replace them with new work areas, offices and training facilities.

It is also proposed to construct a Spiegel tent in a section of vacant land on the eastern side the property.

It is proposed to operate shows at this site which may continue through the night up to 1:00 am in the morning.

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Acoustic Assessment for the Circus Oz Site - 35 Johnston St. Collingwood.

2. Background

2.1. Proposed Circus Oz Building Layout Site

Arts Victoria are intending to move the Circus Oz from the Port Melbourne site to a new site located at No. 35 Johnston St. Collingwood.

SKM was commissioned to perform an environmental acoustic assessment to define the likely impact of the Circus on the local community noise environment.

Buildings at the existing site are to be demolished (buildings titled Block D) and replaced with new buildings housing new training facilities, general offices areas and work areas to construct new props and to upgrade circus equipment. The construction works will also include a Spiegel tent on the proposed site.

The Circus Oz site is proposed operate to 1:00 AM.

Figure 1 presents a plan view showing building s proposed for demolition at the proposed site while Figure 2 shows the proposed layout of new buildings on site.

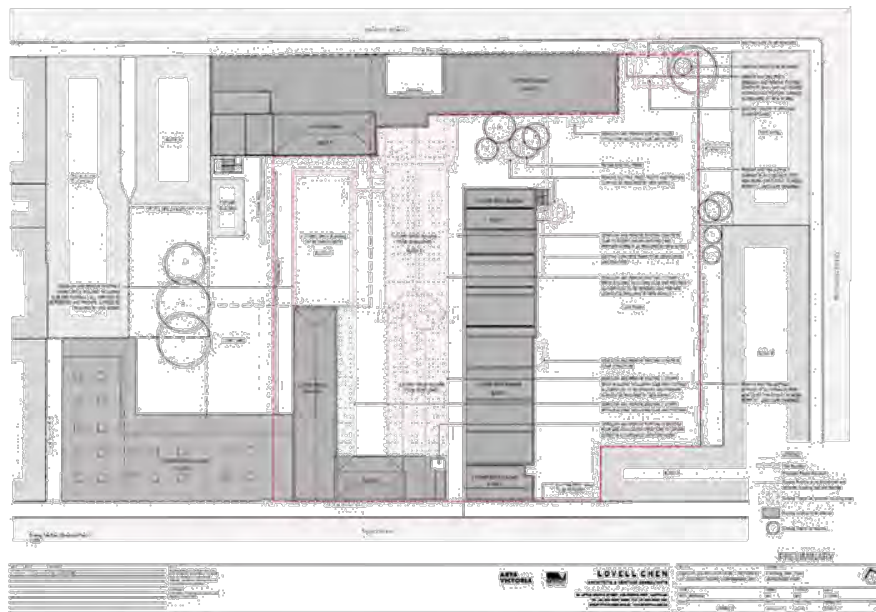


Figure 1 Buildings Proposed for demolition on the Existing Johnston St. Site

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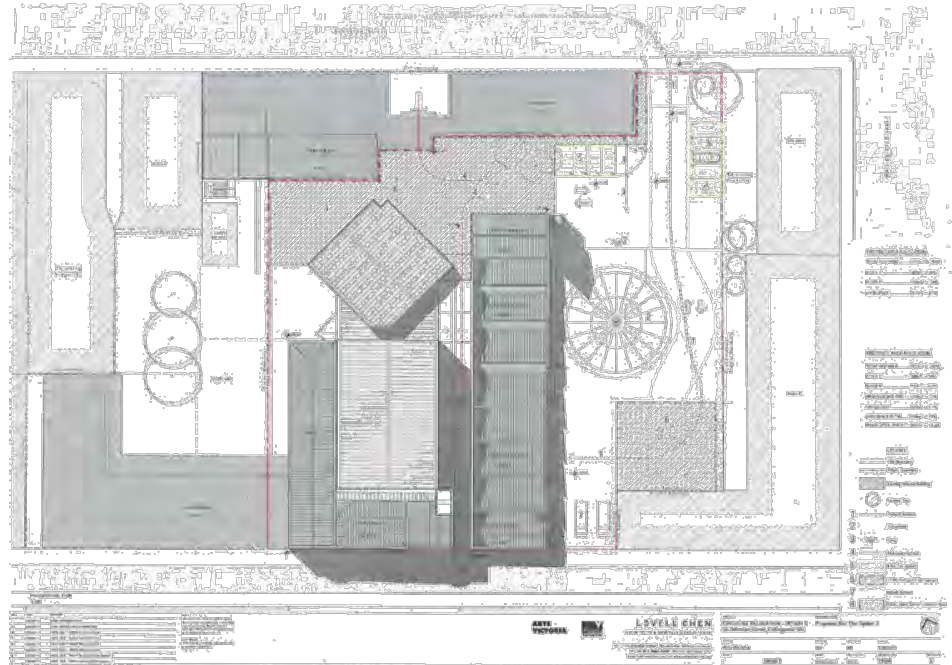


Figure 2 Proposed layout of the Circus Oz Site

2.2. Site Description

The immediate land use in the area surrounding the proposed site consists mainly of a mixture of commercial / industrial/ school, with some residential spread within the area.

Directly adjoining the proposed Circus Oz site are the:

- Tote Hotel located on the north – east corner boundary.
The Tote Hotel has bands playing regularly at this site from Wednesday to Sunday with operating hours 4:00 pm – late (3:00 am Thursday – Saturday).
Bands start playing from between 4 – 9 pm until late.
It was noted that a band was clearly audible at the proposed site as early as 3:00 pm during an initial review of the site on Tuesday 22/6.
- Department of Justice Building on the eastern Boundary of the proposed site.
The Department of Justice Building consists of a 3 storey brick building located on the adjoining eastern boundary. The building is located on Wellington St, Collingwood.

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Residential and commercial (terrace style) buildings are located on the eastern side of Wellington St.

These properties are acoustically shielded by the Department of Justice building from the proposed Circus Oz site. It is believed that the noise emissions from the Circus Oz will have negligible impact at these sites due to the noise from road traffic noise and from the Tote Hotel.

- The southern boundary of the proposed Circus Oz site faces onto Perry Street.
On the southern side of Perry facing the Circus Oz site is St Josephs Primary School and the Northern Melbourne Institute of TAFE.

The NMIT building is a multi story building while the St Josephs primary school building is a single storey building.

These buildings are partially acoustically shielded by a 2.5 – 3 metre high brick wall currently constructed on the southern boundary of the proposed Circus Oz site.

- Johnston Street forms the northern boundary of the proposed site.
Commercial buildings and 1st floor commercial with possible 2nd storey residential living areas are located on the northern side of Johnston St.
It must be noted that Johnston St. has considerable road traffic movement which has a significant impact on the ambient noise levels.

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3. Noise Criteria

3.1. Music Noise

To determine the noise limit criteria for noise emissions from the Circus Oz site during a performance, the State Environment Protection Policy [SEPP] (Control of Music Noise From Public Premises) No.2 was used.

It is assumed that the main noise source during a Circus Oz performance at night will be from the 'backing band'.

Most of the acts of the Circus Oz would be of a relatively quiet nature.

The SEPP No. N-2 Noise Policy goal is to protect residents from noise levels of music noise that may affect the beneficial uses made of noise sensitive areas while recognising the community demand for a wide range of musical entertainment.

Beneficial uses of noise sensitive areas protected by this policy are:

For noise from indoor venues, the normal domestic and recreational activities and, in particular, sleep in the night period

and

for noise from outdoor venues, the normal conversation, and sleep after 11 p.m.

This policy applies to all public premises and protects beneficial uses of noise sensitive areas from music noise emitted from those premises.

In this policy, the music noise assessed may include, in addition to noise from music sources, noise from human voices and activities within the premises that are associated with the music sources.

The policy does not prescribe noise limits for noise associated with the arrival and departure of people attending the premises. Nor does it prescribe noise limits for hearing conservation purposes within indoor and outdoor venues.

3.2. Industrial Noise (maintenance workshop)

This section relates to the construction noise levels generated due to the manufacturing of 'props' and maintenance work for the 'on going' operation of Circus Oz.

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To determine the noise limit criteria applicable to this type of noise generating operation, the State Environment Protection Policy (Control of Noise from Commerce, Industry and Trade) No.N-1 has been used.

This policy prescribes noise limits for commercial, industrial or trade premises. The following types of noise emitted from commercial, industrial or trade premises are not assessed by this Policy:

- Music
- Voices
- Noise From crowds
- Noise from fire arms
- Noise from lawn mowing
- Noise from construction or demolition activities on building sites
- Noise from audible intruder, emergency or safety alarms
- Noise from aircraft except for ground maintenance activities
- Noise from mobile farm machinery
- Noise from scar guns and anti hail guns
- Noise from Livestock on a farm or in a saleyard
- Noise from a from a fire pump used in an emergency
- Noise from non commercial vehicles except for maintenance

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4. Measurement Instrumentation

Noise level measurements were conducted using a Bruel and Kjaer 2250 Hand Held Analyser. This is a type 1 sound level meter as defined in IEC 61672:2003 and has been calibrated in a NATA accredited laboratory.

Calibration was checked before and after the noise level measurements using a Bruel and Kjaer Acoustic Calibrator Type 4231. The Acoustic Calibrator has been calibrated in an NATA accredited laboratory.

Noise data was analysed using BZ5503 and Noise Explorer software.

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5. Existing Background Noise Environment

To provide an indication of the relevant EPA Noise Limits, a noise survey was performed on Monday 27th June (day time measurements) and on Wednesday 29th June 2011 (evening & night time measurements).

The attended background noise level measurements were performed in accordance with the methodology presented in the SEPP No N-1 Noise Policy Schedule C3.2.

5.1. Night Time Background Noise levels

To assess the potential acoustic impact due to rehearsals and shows presented by the Circus Oz, an attended night time noise survey was performed to determine the typical ambient noise levels around the proposed Circus Oz site. This attended noise survey was performed Wednesday 29th & 30th June, 2011.

A background noise level measurement was performed at a derived position away from the Tote Hotel to obtain a background noise level without the influence of band music noise.

A second noise measurement was performed at the front facade of the nearest residential building located at 54 Johnston Street. This location would be most likely to be acoustically impacted by noise from the Tote and from Circus Oz.

For the purpose of establishing the background noise level for the music assessment, an analysis of the ambient noise levels was performed to determine the background noise level Octave Band spectrum.

5.2. Day Time Noise levels

An attended day time measurement was also performed to obtain an indicative background noise level to allow for the determination of a Noise Limit for noise levels generated by maintenance operations being performed at the Circus Oz site.

The Noise Limits were determined in general accordance with the methodology presented in the SEPP No N-1 Noise policy.

The determination of the Noise Limits takes into account not only the background noise levels within the area of proposed site but also land usage using the Town Planning Zonings of the local area.

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6. Measurement Results

6.1. Environmental Noise Measurements

6.1.1. Day Time Ambient Noise levels

Table 1 below presents the background noise level (LA90) results at the nearest residential property that may be directly impacted by noise generated by Circus Oz.

Two ten minute measurements were taken between 1300 and 1400 hours on the 26th June, 2011.

The weather condition at the time of the noise survey was calm with clear skies.

The dominant noise source was road traffic noise which was consistent throughout the noise survey. It was noted there was heavy traffic usage on both Johnston and Wellington Streets.

It was also noted no band noise was audible from the Tote Hotel.

Table 1 Environmental noise levels at 54 Johnston Street During the Daytime

Measurement Position & Time	L _{A90} Sound Pressure Level (dBA : re 2 x 10 ⁻⁵ Pa)									
	Octave Band Centre Frequency (Hz)									Overall SPL
	31.5	63	125	250	500	1K	2K	4K	8K	
Measurement 1	27	39	44	47.5	50.5	54.5	52.5	44.5	32	58.5
Measurement 2	25.5	37	43	46.5	49.5	53	51.5	43	30	58
Average	26	38	43	47	50	54	52	44	31	58

6.1.2. Evening Time Ambient Noise levels

Table 2 below presents the background noise level (LA90) results at 54 Johnston Street.

Two ten minute measurements were taken between 2100 and 2200 hours on the 29th June, 2011.

The weather condition at the time of the noise survey was calm with clear skies.

The dominant noise source was road traffic noise which was consistent throughout the noise survey. It was noted there was heavy traffic usage on both Johnston and Wellington Streets. Band noise from the Tote Hotel was not audible at the time of the noise survey.

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Table 2 Environmental noise level at 54 Johnston Street During the Evening

Measurement	L _{A90} Sound Pressure Level (dBA : re 2 x 10 ⁻⁵ Pa)									
	Octave Band Centre Frequency (Hz)									Overall A_ weighted SPL
	31.5	63	125	250	500	1K	2K	4K	8K	
Measurement 1	23	35	42	45	50	53.5	51	41	29	57.5
Measurement 2	25	36	41	43.5	50.5	55.5	52	40.5	32	58.5
Average	24	35	41	44	50	54.5	51	41	31	58

6.1.3. Night Time Ambient Noise Levels – Music Noise Limit Criteria

Noise measurements were performed at two measurement locations.

- At a Derived Point without the influence of Band Music from the Tote Hotel
- At 54 Johnston St. with band music from the Tote Hotel audible.

6.1.3.1. Derived Measurement Point

Background noise levels were taken at a derived point to measure the typical background noise levels without the influence of band noise from the Tote Hotel. The selected position was near the intersection of Johnston St. and Smith St. The traffic noise levels measured at this location would be similar to that at 54 Johnston Street.

Noise level measurements were performed between 10:50 p.m. & 12:30 a.m. on the 29th & 30th June, 2011 over 2 periods of 10 minutes each. Table 3 shows the result obtained.

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Table 3 Background noise level (LA90) measurements performed at the Derived Point

Measurement	L ₉₀ Sound Pressure Level (dB : re 2 x 10 ⁻⁵ Pa)									
	Octave Band Centre Frequency (Hz)									Overall A weighted SPL
	31.5	63	125	250	500	1K	2K	4K	8K	
Measurement 1	56	57.5	54.5	51	49	49.5	46	37.5	28	54
Measurement 2	56	59	54	57	48	50	45.5	35	25	53
Average	56	58	54	54	48	50	46	36	37	54

Table 4 below presents the noise level results at No. 54 Johnston St. with band music from the Tote Hotel and road traffic noise. These noise level measurements were performed between 11:05 p.m. & 12:00 a.m. on the 29th & 30th June, 2011 over 2 periods of 10 minutes each.

Table 4 Background noise level (LA90) measurements at No.54 Johnston Street with band music from the Tote Hotel

Measurement	L ₉₀ Sound Pressure Level (dB : re 2 x 10 ⁻⁵ Pa)									
	Octave Band Centre Frequency (Hz)									Overall A weighted SPL
	31.5	63	125	250	500	1K	2K	4K	8K	
Measurement 1	64	71	69	70.5	67	63	60	54	46	69
Measurement 2	69.5	76	65.5	61	62	62	60	51	40.5	66.5
Average	67	74	67	66	65	63	60	53	43	68

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6.2. Circus Operations Noise Levels

Arts Victoria has supplied noise data for various Circus OZ noise sources as measured at the 'Sausage Factory' site. The noise levels are representative of the typical noise levels that may be generated by various operations in the different work areas and operating areas at the proposed Circus Oz site. Table 5 presents these noise levels. It has been assumed that these noise level measurements were performed at a distance of 1 metre from the noise sources.

Table 5 Measured Sound pressure Levels taken During Various Operations at the 'Sausage Factory' Site

Measurement Location / Operation	A Weighted Sound Pressure Level (dBA re: 2×10^{-5} Pa)	
Workshop	Free Running (Non contact Operation)	Work Operation (contact Operation)
Drop saw	84	97
Compressor	83	102
Angle Grinder	86	103
Drill Hall	Ambient	Rehearsal
Internal Sound Pressure Level	65	83
Music Room (Band Practice)		Rehearsal
Vibro phones & 2 Saxophones	-	87
Drums, Keyboard & Guitar – Low Level	-	83
Drums, Keyboard & Guitar – High Level	-	88

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7. Calculated Noise Limits

7.1. SEPP Noise Limit Criteria

7.1.1. Day / Night Time Noise Levels – Music Noise (No. N-2 Noise Policy)

The indicative Noise Limit criteria have been calculated in accordance with the methodology presented in Clause 14(a) of the SEPP No N-2 Noise Policy. These are only indicative because they are based on the short sample noise level measurements only.

■ Day Time

The day / evening noise limit is determined using **Noise Limit = L_{A90} (Background Noise level) +5dBA.**

Table 6 below presents the calculated noise limits for the operational activities performed by the Circus Oz.

Table 6 Calculated Noise Limits for Music by Circus Oz

Nearest Noise Sensitive Receiver	A Weighted Sound Pressure Level (dBA re 2×10^{-5} Pa)	
	Day Time Noise Criterion (0700 – 1800 hrs)	Evening Time Noise Criterion (1800 -2200 hrs)
Johnston St. / Wellington	63 dBA	63 dBA

■ Night Time

The night time noise limit is determined using **Noise Limit = $L_{OCT 90}$ +8 dB.**

Table 7 below presents the calculated noise limit for the music performed by the Circus Oz.

Table 7 Calculated Night time Noise Limit for Music by Circus Oz

Measurement	L_{90} Sound Pressure Level (dB : re 2×10^{-5} Pa)									
	Octave Band Centre Frequency (Hz)									Overall SPL
	31.5	63	125	250	500	1K	2K	4K	8K	
Night Time Noise	64	66	63	59	57	58	54	46	36	70

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Limit										
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7.1.2. Noise Limit Criteria Levels – Maintenance Noise (No. N1 Noise Policy)

The Noise Limit criteria have been determined for the day time, evening and night time periods.

Table 7 below presents the calculated noise limits for the maintenance type work being performed on the Circus Oz site. The Noise Limits has been based on the attended noise survey and the relevant land zoning.

Table 8 Calculated Noise Limits for Maintenance Operations for the Circus Oz site

Nearest Noise Sensitive Receiver	A Weighted Sound Pressure Level (dBA re 2×10^{-5} Pa)		
	Day Time Noise Criterion (0700 – 1800 hrs)	Evening Time Noise Criterion (1800 -2200 hrs)	Night Time Noise Criterion (22200 – 0700 hrs)
Johnston St. / Wellington St.	64	61	57

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8. Noise Level Predictions

The noise levels generated by the activities occurring at various locations at the proposed Circus Oz site have been calculated based on the noise levels presented by Arts Victoria.. The noise level calculations have been based on noise emissions from the proposed Circus Oz site only, and do not take into account band noise from the Tote Hotel.

8.1. Noise Emission Due to Activities in the Spiegel tent

The predicted noise levels at each of the noise sensitive receiver locations were determined based on a Sound Pressure Level of 92 dBA as the noise emission of the band.

Table 9 presents the predicted noise levels at three noise sensitive receiver locations due to noise from the Spiegel tent.

Table 9 Calculated Noise Levels Due To Band Noise From the Spiegel tent

Noise Sensitive Receiver Position	Predicted Sound Pressure Level (dBA)	Comment
Residential Building – Johnston St.	57	Noise level prediction at the front facade of No. 70 Johnston St. – clear line of sight (60 metres from Spiegel tent)
Department of Justice Building	69	Noise level prediction at rear facade of building -clear line of sight (15 metres from Spiegel tent)
St. Josephs Primary School	48	Noise level prediction at the front facade of the St. Josephs Primary School. – barrier in line of sight (40 metres from Spiegel tent)

8.2. Noise Emission from Rehearsal Rooms 1 & 2

Table 10 presents the predicted noise levels at the three noise sensitive receiver locations due to noise from the Rehearsal Rooms 1 & 2.

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For this prediction, we have used the Transmission Loss of 15 dB which takes into account the possible low frequency character (bass / drums) of the band music.

Table 10 Calculated Noise Levels Due To Band Noise From the Rehearsal Rooms 1 & 2

Noise Sensitive Receiver Position	Predicted Sound Pressure Level (dBA)	Comment
Residential Building – Johnston St.	43	Noise level prediction at the front facade of No. 54 Johnston St. – clear line of sight (50 metres from Rehearsal Room 2)
Department of Justice Building	42	Noise level prediction at rear facade of building -clear line of sight (52 metres from Rehearsal Room 1))
St. Josephs Primary School	46	Noise level prediction at the front facade of the St. Josephs Primary School. – Assume clear line of sight (25 metres from Rehearsal Room 1)

8.3. Noise Emissions from Set Workshop

In predicting the acoustic impact of equipment used for set making, the worst case scenario of the Drop Saw, Compressor and Angle Grinder all being in operation simultaneously within the Set Workshop was used. The overall Sound Pressure Level emission was determined to be 106 dBA.

It was assumed that the outer facade of the Set Workshop is a brick construction.

It was also assumed that the transmission loss of the brick wall would achieve a 30 dB noise reduction at the dominant frequencies generated by the equipment.

Table 11 presents the predicted noise levels at three noise sensitive receiver locations due to noise from the Set Workshop.

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Table 11 Calculated Noise Levels Due To Equipment Noise from the Set Workshop

Noise Sensitive Receiver Position	Predicted Sound Pressure Level (dBA)	Comment
Residential Building – Johnston St.	37	Noise level prediction at the front facade of No. 70 Johnston St. – clear line of sight (75 metres from Set Workshop)
Department of Justice Building	44	Noise level prediction at rear facade of building -clear line of sight (38 metres from Set Workshop)
St. Josephs Primary School	29	Noise level prediction at the front facade of the St. Josephs Primary School. – barrier in line of sight (50 metres from Set Workshop)

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9. Discussion

9.1. Circus Oz Operations

The acoustic impact due to the operation of Circus Oz at the proposed site has been determined and the influence of band noise from the Tote Hotel has not been considered.

9.1.1. Spiegel tent Activities

The Spiegel tent is to be located in the eastern section of the court yard of the proposed site opposite the entrance from Johnston Street.

It is proposed to construct the Spiegel tent from canvas and light weight timber to be used as an entertainment area where people will be able to socialize and watch the Circus Oz acts.

These construction materials will afford little, if at all, any noise attenuation due to transmission loss across the tent structure, especially at the lower acoustic frequencies.

Noise due to general people activities has not been taken into account into the acoustic calculations.

Table 12 below presents in tabulated form whether the noise emissions from activities in the Spiegel tent meet the SEPP No. N-2 Noise Policy.

Table 12 Matrix of the compliance with the SEPP Noise Policy No N-2 at the three noise sensitive locations

Measurement Location	Compliance with EPA Noise Policy No N-2		
	Day Time Noise Criterion (0700 – 1800 hrs)	Evening Time Noise Criterion (1800 -2200 hrs)	Night Time Noise Criterion (22200 – 0700 hrs)
No. 70 Johnston St.(nearest residence)	Complies	Complies	Complies
Department of Justice Building	May Comply*	May Comply*	May Comply*
St. Josephs Primary School	Complies	Complies	Complies

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* If Circus Oz is to have a performance in the Spiegel tent during the day time period, the predicted noise levels at the building facade due to noise emission from the Spiegel tent will be approximately 69 dBA.

This is in the order of 6 dBA over the nominal noise limit of 63 dBA.

The calculations have been based on the assumption that the Spiegel tent is located approximately 15 metres from the Department of Justice Building facade.

Taking into account a 15 – 20 dBA transmission loss across the Department of Justice building facade, the predicted internal noise level within the Department of Justice building would be in the order of 50 – 55 dBA.

Australian Standard / New Zealand AS/NZS 2107 titled 'Acoustics – Recommended Design Sound Levels and Reverberation Times for Building Interiors' recommends internal noise levels for various occupancies and for noise that is reasonably steady in level. Music noise does not quite fit into this category but if assessed against the maximum recommended noise level of 45 dBA for a general office area, then the predicted internal music noise level would be in excess.

To reduce the acoustic impact of music noise on the internal noise levels of the Department of Justice building would likely require an upgrade of the glazing on the western face of the building.

9.1.2. Music in the Rehearsal Rooms 1 & 2

The external walls of the proposed rehearsal rooms (D2 and D4) are to be constructed using Bondor 'Equitilt' composite panel. This panel consists of a 0.6 mm thick sheet metal facing / Expanded Polystyrene / 0.6 mm thick sheet metal facing. The total thickness of the panel is to be 150 mm thick.

The acoustic performance of such a panel is rated at only Rw 25.

Table 13 below presents in tabulated form whether the noise emissions from activities in the Rehearsal Rooms would meet the SEPP Noise Policy No. N-2.

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Table 13 Matrix of the compliance with the SEPP Noise Policy No. N-2 at the three noise sensitive locations

Measurement Location	Compliance with SEPP Noise Policy No. N-2		
	Day Time Noise Criterion (0700 – 1800 hrs)	Evening Time Noise Criterion (1800 -2200 hrs)	Night Time Noise Criterion (22200 – 0700 hrs)
No. 54 Johnston St.	Complies	Complies	Complies
Department of Justice Building	Complies	Complies	Complies
St. Josephs Primary School	Complies	Complies	Complies

9.1.3. Set Workshop

Table 14 below presents in tabulated form whether the noise emissions from activities in the Set Workshop meet the SEPP Noise Policy No. N-1.

Table 14 Matrix of the compliance with the SEPP Noise Policy No. N-1 at the three noise sensitive locations

Measurement Location	Compliance with Noise Policy No.N-1		
	Day Time Noise Criterion (0700 – 1800 hrs)	Evening Time Noise Criterion (1800 -2200 hrs)	Night Time Noise Criterion (22200 – 0700 hrs)
No. 54 Johnston St.	Complies	Complies	Complies
Department of Justice Building	Complies	Complies	Complies
St. Josephs Primary School	Complies	Complies	Complies

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9.2. Noise Emissions from the Tote Hotel

Night time noise level measurements were taken in accordance with the SEPP Noise Policy No. N-2 at No. 54 Johnston St. when a band was playing in the Tote Hotel.

The Effective Noise Level for this sample was determined by measuring the $L_{10\text{ OCT}}$ Sound Pressure Levels in accordance with the SEPP Noise Policy No. N-2.

Table 15 below presents the measured $L_{10\text{ OCT}}$ Sound Pressure Levels.

Table 15 L_{10A} Noise Level measurements at No.54 Johnston Street with band music from the Tote Hotel

Measurement	L_{10} Sound Pressure Level (dB : re 2×10^{-5} Pa)									
	Octave Band Centre Frequency (Hz)									Overall A – weighted SPL
	31.5	63	125	250	500	1K	2K	4K	8K	
Measurement 1	76.5	78	71.5	73.5	65	67.5	66	56	46	69
Measurement 2	76	80	70	72	66	67	64	57	47	66.5
Average	76	79	71	73	66	67	65	57	47	68

A comparison of the Effective Noise Level ($L_{10\text{ OCT}}$) with the calculated Noise Limit as presented in Table 7 above shows that the music noise levels emitted from the Tote Hotel when the band was performing would have exceeded the music Noise Limit criterion for the Night time period. It can also be seen that the noise limit could also be exceeded for the evening time period.

The SEPP N-2 Noise Policy state in Schedule B2 paragraph 3(c):

‘Where two or more premises contribute to the Effective Noise Level in a noise sensitive area, derived noise limits may be set so that the contribution from each of the premises when combined together will meet the noise limit at the noise sensitive area’.

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However, the predicted the noise emissions from the proposed Circus Oz site meets the calculated Noise Limits at all noise sensitive receiver locations except possibly at the Department of Justice building due to activities in the Spiegel tent.

All other noise emissions due to Circus Oz are predicted to comply with the SEPP Nos. N-1 and N-2 at all noise sensitive receiver locations.

9.3. Vehicle and Pedestrian Movement on the Proposed Site.

From the initial layout plans of the proposed Circus Oz site, there will be in the order of 22 vehicles located within the boundary of the site

The noise levels generated by the vehicle movements will be insignificant when compared with the noise levels generated by general road traffic noise along Johnston and Wellington Streets. This noise will be masked by the general road traffic noise at the noise sensitive receiver locations.

Pedestrian noise due to people talking and moving will also have a minimal acoustic impact on the overall noise levels within the site due the dominant noise from the ambient road traffic noise and music noise from the Tote Hotel.

9.4. Noise Intrusion into the Proposed Circus Oz Site

It was noticed that the band noise from the Tote Hotel could have an adverse impact on the acoustic environment within the Spiegel tent.

At the time of the night time noise survey, access to the Circus Oz site was not available. An indicative noise level measurement was performed at the main gate entrance to the Circus Oz site and the music noise level was determined to be approximately 70 – 73 dBA with a band playing at the Tote Hotel.

As the lounge bar in the Tote Hotel (where the bands perform) is almost directly opposite to the proposed location of the Spiegel tent, it can be expected that the noise level due to the bands could be in the order of 75 - 80 dBA at the Spiegel tent location. This is likely to have a significant impact on the ambient environment inside the Spiegel tent and should be noted.

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10. Conclusion

Noise surveys have been performed at the proposed Circus Oz site to determine the existing acoustic environment and the likely acoustic impact of noise emissions from activities due to Circus Oz.

Attended noise level measurements were conducted to determine indicative Noise Limits in accordance with SEPP Noise Policies Nos. N-1 and N-2. Based on music noise levels and noise level data provided by Arts Victoria, it was determined that the noise emissions due to Circus Oz would meet the noise limits. Music noise emission from the Spiegel tent might be considered non compliant at the Department of Justice Building.

The predicted noise emission into the local community due to Circus Oz are of the order of 10 dBA below the existing background noise levels. The existing background noise levels are dominated by road traffic noise along Johnston Street and band noise from the Tote Hotel.

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Collingwood Arts Precinct
Traffic Engineering Report
Planning Scheme Amendment

Client // Creative Victoria
Office // VIC
Reference // V116280
Date // 16/12/16

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Collingwood Arts Precinct

Traffic Engineering Report

Planning Scheme Amendment

Issue: Final 16/12/16

Client: Creative Victoria

Reference: V116280

GTA Consultants Office: VIC

Quality Record

Issue	Date	Description	Prepared By	Checked By	Approved By	Signed
A	16/12/16	Final	Justin Gale	Chris Coath	Simon Davies	

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Attachment 3 - Noise and Traffic Reports CAP

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Attachment 3 - Noise and Traffic Reports CAP

1. Introduction

1

1.1 Background & Proposal

A Planning Scheme Amendment is being sought to the Collingwood Arts Precinct (CAP) site located at 35 Johnston Street and 241 Wellington Street in Collingwood. Specifically, the site is proposed to be rezoned from a Mixed Use Zone (MUZ) to a Special Use Zone (SUZ).

The SUZ is proposed as a mechanism to guide the future development and revitalisation of the CAP. Further to the proposed rezoning, a Parking Overlay is being sought to exempt the need for a permit to reduce (including reduce to zero) the parking requirement under Clause 52.06 of the planning scheme for those uses listed in Section 1 (permit not required) in the Schedule to the Special Uses Zone.

GTA Consultants was commissioned by Creative Victoria (Department of Economic Development, Jobs, Transport and Resources) to undertake a traffic engineering assessment of the proposed Planning Scheme Amendment.

1.2 Purpose of this Report

The report outlines the development of the proposed car parking overlay, including particular consideration of:

- i the intended use of the CAP site, guided by the concept plan
- ii the adequacy of the proposed pedestrian, bicycle and public transport access arrangements to the site
- iii the adequacy of the sustainable travel facilities and services provided for the site
- iv the inability to provide car parking on the site without significant detriment to existing buildings
- v the appropriateness of the proposed Schedule to the Parking Overlay for the Precinct.

1.3 References

In preparing this report, reference has been made to the following:

- o Concept plans for the proposed development prepared by Fieldwork
- o Yarra Planning Scheme
- o City of Yarra Bicycle Strategy 2010 – 2015
- o City of Yarra Bike Strategy – 2016 Refresh
- o Australian Standard / New Zealand Standard, Parking Facilities (AS2890)
- o an inspection of the site and its surrounds
- o other documents as nominated.

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2. Existing Conditions

2

2.1 Subject Site

The site located at 35 Johnston Street and 241 Wellington Street in Collingwood is bound by Johnston Street to the north and Perry Street to the south. The site was formerly the home of the Collingwood Technical College and is currently managed by Creative Victoria which is part of the Department of Economic Development, Jobs, Transport, and Resources (DEDJTR).

The eastern half of the site is currently occupied by Circus Oz while the remainder of the site with an existing floor area of approximately 4,800sqm is currently unoccupied.

Primary vehicle access to the site is provided via Johnston Street. Two vehicle crossovers to Perry Street also provide vehicle access to two roller doors, one located immediately west of Building E and one located at the eastern end of Building F.

Approximately seventeen (17) marked car parking and loading areas are located within the Circus Oz section of the site with vehicle access provided via the Johnston Street access. A total of 4 car parking spaces are located at the eastern end of Building F are associated with the remainder of the CAP site.

The subject site is shown in Figure 2.1, with the land zoning provided in Figure 2.2.

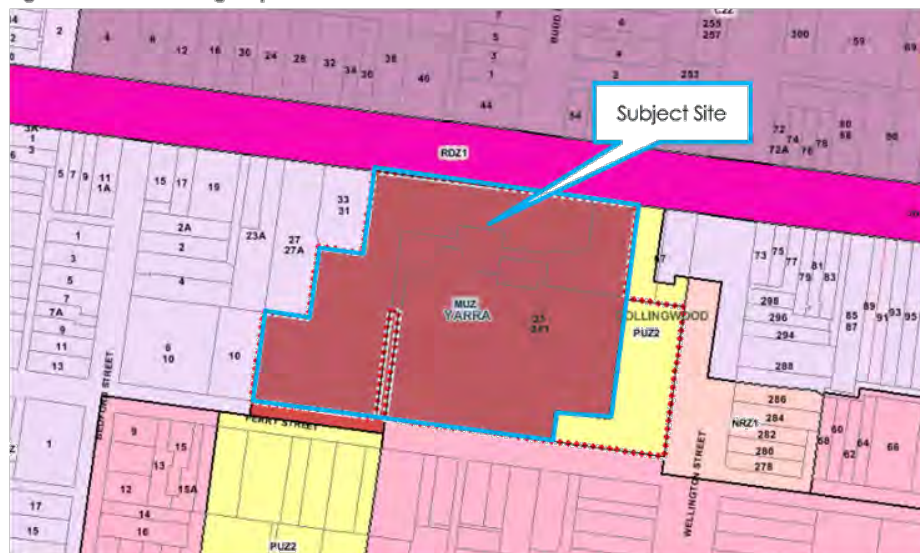
Figure 2.1: Subject Site and its Environs



(Reproduced with Permission from Nearmap)

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Figure 2.2: Land Zoning Map



(Reproduced from Land Channel web site)

2.2 Road Network

2.2.1 Johnston Street

Johnston Street is a secondary arterial road (VicRoads controlled within a Road Zone Category 1) aligned in an east-west direction and configured with a five lane carriageway. The middle lane operates as a contraflow lane westbound in the AM peak period, eastbound in the PM peak period and also serves as a right turn lane at intersections along its length. The westbound and eastbound kerbside lanes operate as bus lanes during the AM and PM peak periods, respectively. Outside of Clearways, parking on Johnston Street is generally subject to two hour parking restrictions during business hours.

2.2.2 Perry Street

Perry Street is a local road which operates one-way eastbound between Smith Street and Wellington Street. Parallel parking is permitted on the north side of the carriageway and is generally subject to a four hour time restriction during business hours. An existing on-street loading zone is also located on the north side of Perry Street immediately west of the subject site while a 1/4P parking space is located adjacent to Building D.

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2.3 Sustainable Transport

2.3.1 Public Transport

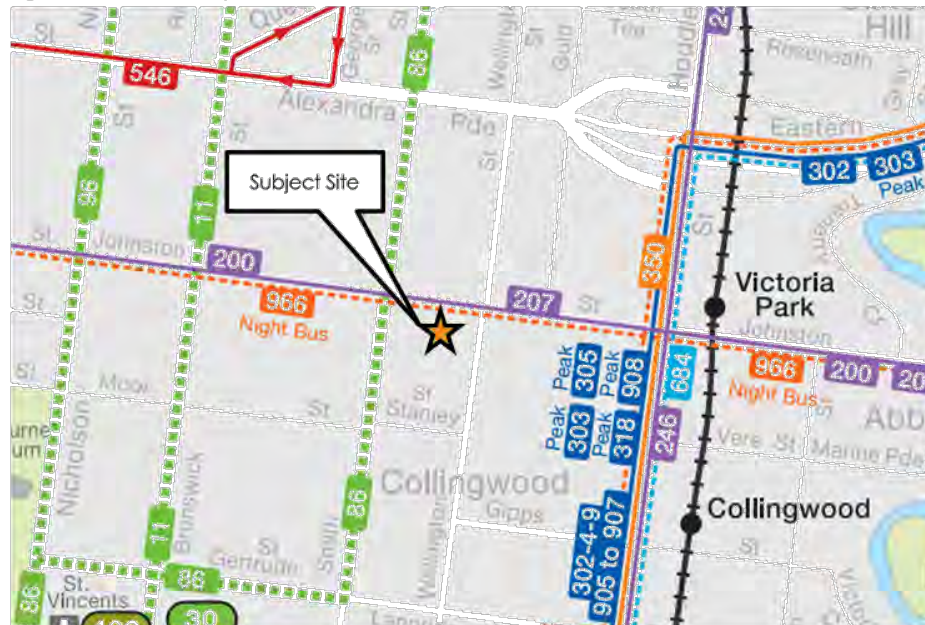
The site is well serviced by public transport with tram route 86 operating along Smith Street to the west and bus routes 200 and 207 operating along Johnston Street to the immediate north.

A significant number of bus routes also operate along Hoddle Street, within approximately 600m walking distance east of the site. Victoria Park railway station is also located within 700m walking distance of the site.

Tram route 86 (Bundoora RMIT – Waterfront City) operates along Smith Street within 200m walk of the site while tram route 11 (West Preston to Victoria Harbour) operates along Brunswick Street within 700m walking distance of the site.

Figure 2.3 summarises the existing public transport provision surrounding the subject site.

Figure 2.3: Public Transport Provision



2.3.2 Pedestrian Infrastructure

Pedestrian paths are located on both sides of the majority of roads in the surrounds whilst signalised pedestrian crossings are located at the Johnston Street/Wellington Street and Johnston Street/Smith Street intersections to the east and west respectively.

2.3.3 Cycle Infrastructure

The Principal Bicycle Network (PBN) is a network of on and off-road cycling corridors that have been identified to support cycling for transport and access major destinations in metropolitan Melbourne. The PBN was reviewed and updated in 2012 by VicRoads and all local Councils.

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The PBN is also a 'bicycle infrastructure planning tool' to guide State investment in the planning and development of the future metropolitan Melbourne bicycle network. In this regard, a subset of the PBN has been identified and elevated to a higher level of priority, mainly on the basis of potential for separation from motorised traffic, making these routes more attractive to less experienced bike riders. These cycling corridors are referred to as Bicycle Priority Routes (BPRs) and form part of the modal priorities for the road network set out in the VicRoads SmartRoads framework. Strategic Cycle Corridors (SCC) form another subset of the PBN, and represent an initiative outlined in Plan Melbourne to support walking and cycling in Central Melbourne. SCCs are intended to be corridors designed to provide high quality bicycle infrastructure to, and around, major activity areas in metropolitan Melbourne. Plan Melbourne outlines a subset of the SCCs for the proposed expanded central city area.

It is noted that the type of bicycle facility (i.e. on or off-road and separated or shared) has not been indicated as part of the PBN and BPRs. Rather, the PBN and BPRs show the proposed cycling network. The associated facilities should be delivered in accordance with the relevant standards and guidelines, such as the Australian Standards, Austroads Guides and VicRoads' Cycle Notes.

Dedicated on-road bicycle lanes currently exist on Wellington Street to the immediate east of the site, providing north-south connections to nearby east-west dedicated bicycle priority routes.

The PBN and BPRs in the vicinity of the study area are shown in Figure 2.4.

Figure 2.4: VicRoads Principal Bicycle Network and Bicycle Priority Routes



Source (www.maps.vic.gov.au/TransMaps/)

The City of Yarra 'Bicycle Strategy 2010 – 2015' and '2016 Refresh' outline further initiatives that are proposed within the municipality. Strategy 1.1a of the 2016 Refresh is the installation of a 'Copenhagen style' bike lane on Wellington Street between Gipps Street and Johnston Street by removing one side of parking in the street.

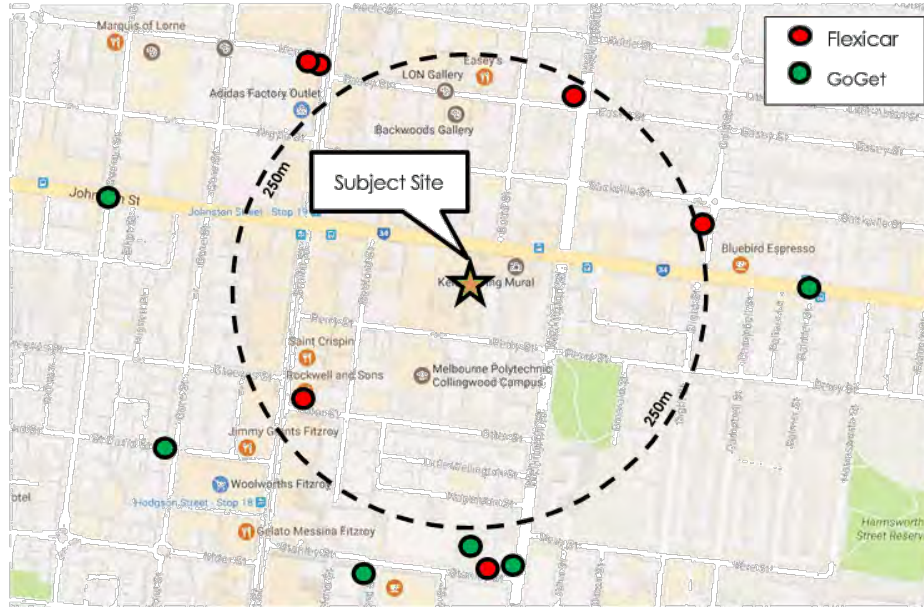
2.4 Car Share

The site enjoys access to a number of nearby car sharing services including 3 pods located within 250m walking distance of the site. The share cars located in close proximity of the site are shown in Figure 2.5.

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Car share usage is continuing to grow throughout the inner municipalities of Melbourne.

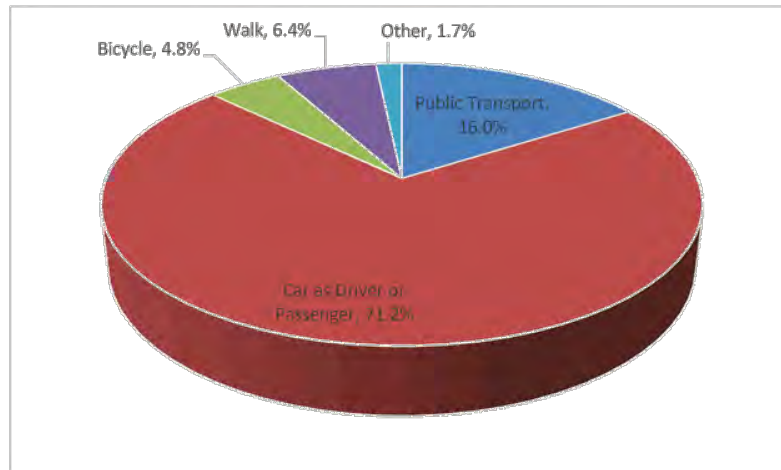
Figure 2.5: Existing Car Share Locations



2.5 Mode Share in City of Yarra

Figure 2.6 summarises the journey to work statistics gathered from The Australian Bureau of Statistics (ABS) 2011 Census 'Travel to Work' data for the City of Yarra.

Figure 2.6: Existing Modes of Travel to Work in the City of Yarra

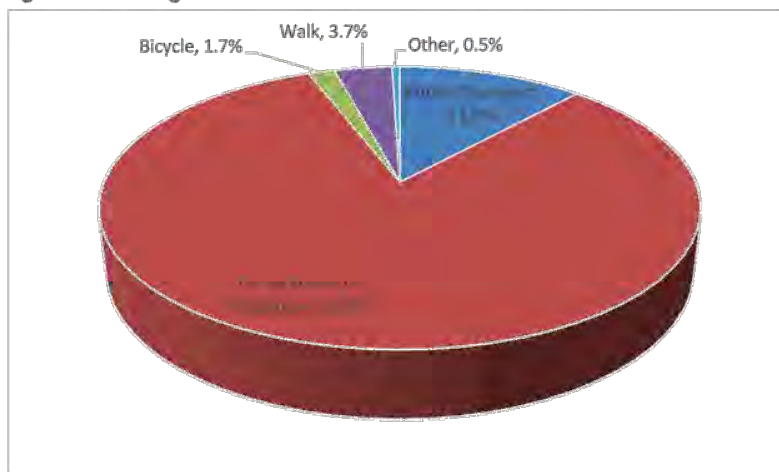


Source: Australian Bureau of Statistics 2011 Census

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For the purpose of comparison, Figure 2.7 shows the existing method of travel utilised for journeys to work from the Greater Melbourne (SA4) statistical area as defined by the ABS (i.e. all residents of the 'Greater Melbourne' statistical area).

Figure 2.7: Existing Modes of Travel to Work from Greater Melbourne



Source: Australian Bureau of Statistics 2011 Census

As illustrated above, those working in the municipality of Yarra typically have a higher level of use of public transport and active travel modes compared to the 'Greater Melbourne' average. Specifically, use of private vehicles is approximately 82% for Greater Melbourne compared to 71% for the municipality of Yarra.

Further to the above, the City of Yarra Bicycle Strategy 2010 – 2015 aimed to increase cycling of the journey to work of the City of Yarra residents from 6.4% in 2006 to 15% by 2015.

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3. Proposed Amendment

3

3.1 Special Use Zone

The purpose of the proposed Special Use Zone is to recognise the Collingwood Arts Precinct as a significant arts and cultural precinct within greater Melbourne. The Special Use Zone includes a number of Section 1 (permit not required) uses which are directly related to arts and cultural purposes or other complimentary uses such as food and drink premises.

The rezoning of the land to Special Use Zone in and of itself is not expected to have any significant traffic and parking implications. Indeed, the rezoning will result in the major uses currently permitted by the existing Mixed Use Zone such as residential, office and shop to require a permit for their use within the new zone unless they are provided in conjunction with the use of the site for arts and creative industries.

To facilitate the reuse of the site including the existing buildings for arts and creative industries a Schedule to the Parking Overlay is proposed that will remove the need for a permit to reduce (including reduce to zero) the parking requirement for those uses that do not require a permit as specified in the Schedule to the Special Uses Zone.

3.2 Parking Overlay

The proposed parking overlay is provided at Appendix A.

The objectives of the parking overlay are set out below:

- To encourage the re-use of buildings within the Collingwood Arts Precinct for arts and creative industries as identified in Schedule [NUMBER] to the Special Use Zone.
- To acknowledge the existing built form and context of the site and the constrained ability to provide on-site car parking.
- To recognise the site's proximity to the Principal Public Transport Network and to reduce car parking demand by encouraging use of sustainable and active transport modes to and from the site.
- To improve amenity for pedestrians around and through the site by minimising vehicle access to and through the site.

To facilitate the objectives, the key component of the schedule is to not require a permit to reduce (including reduce to zero) the number of parking spaces required for any use listed in Section 1 (permit not required) of the Table of Uses to the Special Use Zone.

Any other use not listed in Section 1 of the Table of Uses to the Special Use Zone would be required to provide parking in accordance with Column B of Table 1 of Clause 52.06-5 of the Yarra Planning Scheme or seek a permit to reduce this requirement as per Clause 52.06-3.

The following considers the appropriateness of the proposed parking overlay.

Attachment 3 - Noise and Traffic Reports CAP

4. Strategic Justification for the Parking Overlay

4

4.1 General

The introduction of the proposed Parking Overlay is considered appropriate to encourage and facilitate the use of an otherwise underutilised site comprising an existing floor area of approximately 4,800 sqm for arts and cultural purposes. The following sets out some of the key considerations based on the decision guidelines in Clause 52.06-6 of the Yarra Planning Scheme.

4.1.1 The practicality of providing car parking on the site

As previously discussed, in addition to Circus Oz, the remainder of the site is occupied by a number of three to four storey buildings with an overall floor area of approximately 4,800 sqm as well as a central courtyard area.

In this regard, it is impractical to provide additional car parking on the site without significantly changing the existing character of the site by the removal of buildings and/or the central courtyard area which is intended for public use and pedestrian access.

It is noted that the Collingwood Arts Precinct is covered by a heritage overlay and that the intention is to reuse the existing buildings and outdoor spaces for arts and creative uses, rather than undertake a substantial redevelopment of the site.

4.1.2 Any car parking deficiency associated with the existing use of the land

While the land is currently unoccupied, the site was previously occupied by the Collingwood Technical College and has historically been used for educational purposes. As such, it is considered highly likely that a parking deficiency would have been associated with the previous use of the land.

4.1.3 The impact of fewer car parking spaces on local amenity, including pedestrian amenity

The site is located within the heart of Collingwood on the corner of Johnston Street and Wellington Street. The reduction in car parking demand in this area including the reduction in vehicle movements crossing footpaths will assist in improving local amenity including pedestrian amenity. Furthermore, the concept plan includes a new pedestrian connections through the existing courtyard on the site between Johnston Street and Perry Street.

4.1.4 Access to or provision of alternative transport modes to and from the land

As set out in Section 2.3 of this report, the site is well served by public transport services including train, bus and tram services. In addition, both Johnston Street and Wellington Street are part of the Principal Bicycle Network with designated on-road bicycle lanes provided on Wellington Street.

Attachment 3 - Noise and Traffic Reports CAP

A number of existing car share pods are located within the vicinity of the site and the supply and demand for this type of transport continues to grow.

In addition to the above, it is anticipated that the development of the land will include a provision of bicycle parking in excess of the statutory requirements. It is also recommended that a proportion of any on-site parking provided be allocated to Car Share providers to further encourage the use of Car Share rather than private vehicles.

4.1.5 The character of the surrounding area and whether reducing the car parking provision would result in a quality/positive urban design outcome.

The ability to retain and reuse the existing buildings, including heritage buildings, and courtyard on the site and provide potential new pedestrian connections through the site between Perry Street and Johnston Street will be enabled by the removal of the need to provide car parking for the proposed arts and cultural uses.

4.1.6 The availability of alternate car parking in the locality of the land

The on-street parking supply in the vicinity of the site is generally time restricted during business hours with some areas of Permit Parking also evident. Given the above, the use of the land as envisaged with limited on-site car parking is unlikely to result in significant changes to long-term parking demands in the area as there is limited unrestricted on-street parking available.

4.1.7 Any Relevant Local Planning Policy or Incorporated Plan

Clause 18

Clause 18 of the Planning Scheme is designed to reflect the intent of State Government guidance and contains objectives and strategies in relation to Transport which are relevant to this development, including, but not limited to:

- Create a safe and sustainable transport system by integrating land-use and transport.
- Plan or regulate new uses or development of land near an existing or proposed transport route to avoid detriment to, and where possible enhance the service, safety and amenity desirable for that transport route in the short and long terms.
- Encourage higher land use densities and mixed use developments near railway stations, major bus terminals, transport interchanges, tramways and principal bus routes.
- Pedestrian and cyclists access to public transport should be facilitated and safeguarded.
- Promote the use of sustainable personal transport.
- Integrate planning for cycling with land use and development planning and encourage as alternative modes of travel.
- Achieve greater use of public transport by increasing densities, maximising the use of existing infrastructure and improving the viability of the public transport operation.

Clause 21

Local Planning Policy Clause 21.06 contains objectives and strategies to assist in the reduction of car dependencies by promoting the use of active and sustainable transport alternatives. The clause states the following with respect to car parking:

Attachment 3 - Noise and Traffic Reports CAP

"Parking availability is important for many people, however in Yarra unrestricted car use and parking is neither practical nor achievable. Car parking will be managed to optimise its use and to encourage sustainable transport options."

The Clause also includes the following specific objectives:

"To provide safe and convenient pedestrian and bicycle environments".

"To facilitate public transport usage".

"To reduce the reliance on the private motor car".

"To reduce the impact of traffic".

Encouraging the use of public transport and walking and cycling as modes of transport is central to achieving the above objectives.

The proposed redevelopment of this site is a prime opportunity to achieve this objective by encouraging the use of public transport, cycling, and walking and not encouraging the use of motor vehicles by the provision of additional car parking in the area.

Relevant Local Government Policy

In November 2013, the City of Yarra adopted the 'Parking Management Strategy 2013-2015'. It is noted that the purpose of the policy is as follows:

"The purpose of this document is to provide a policy framework to guide officers in the management of the parking resource."

The strategy has three overall goals, one of those being:

"Plan and manage transport and urban development to minimise the need for people to have to drive cars so that the demand for parking is contained and managed effectively."

Central to achieving the goals of the strategy are 16 principles, those considered most relevant to the development proposal are reproduced as follows:

- *"Ensure that new development are self-sufficient in meeting their parking needs – with the exception of encouraging reduced parking or no car parking developments for sites very close to public transport stops.*
- *Encourage walking, cycling and public transport usage for mobility and movement across the city."*

The above therefore indicates, amongst other things, that Council will consider, where appropriate, reduced parking developments for sites with very good public transport access. It is considered that the subject site is considered to have very good access to several public transport options.

Policy Summary

Encouraging the use of all modes of transport other than private motor vehicles is central to achieving the strategies, goals and objectives outlined within several Council policies. As noted previously, the site is located within an activity centre, has very good access to public transport and is expected to provide bicycle parking and facilities in excess of statutory requirements.

In this regard, the reuse of the site as an arts and cultural precinct represents a prime opportunity to promote the visions of the above policies by encouraging the use of more sustainable forms of transport and by adopting a strategy to reduce car parking demand within this area.

Attachment 3 - Noise and Traffic Reports CAP

5. Conclusion

5

Based on the analysis and discussions presented within this report, the following conclusions are made:

- i The site is proposed to be rezoned to a Special Use Zone to facilitate the reuse and development of the site as an arts and cultural precinct known as the Collingwood Arts Precinct.
- ii The proposed schedule to the parking overlay will assist in the efficient re-use of the site for the intended purposes by removing the need to:
 - o provide on-site car parking; and
 - o seek a planning permit to reduce the statutory parking requirement for the uses listed in Section 1 (permit not required) of the Table of Uses within the Schedule to the Special Use Zone.
- iii The Parking Overlay will enable on-going use and re-use of the buildings with a variety of arts and cultural uses that may change over time, therefore avoiding on-going planning permit applications for parking dispensation when a new Section 1 use commences.
- iv All other uses not listed in Section 1 (permit not required) of the Table of Uses within the Schedule to the Special Use Zone would be required to provide parking as per Column B of the Table to Clause 52.06-5 or seek a permit to reduce this requirement as per Clause 52.06-3.
- v It is considered that appropriate strategic justification exists for the implementation of the Parking Overlay for the Collingwood Arts Precinct based on:
 - o The in-practicality of providing car parking on the site due to the desire to retain the existing buildings
 - o The historical parking deficiency associated with the previous use of the land for Educational purposes
 - o The positive impact of fewer car parking spaces will have on local amenity including pedestrian amenity
 - o The availability of alternate transport modes including trains, trams, buses, bicycles and share cars and Council's aim to increase utilisation of alternate transport modes, and in particular bicycles
 - o The character of the surrounding area and the reduction in car parking provision resulting in a positive urban design outcome
 - o On-street parking in the vicinity of the site is generally time restricted which will limit the ability for long-term parking to occur on-street
 - o A range of state and local government policies.

Attachment 3 - Noise and Traffic Reports CAP

Appendix A

Appendix A



Draft Parking Overlay

V116280 // 16/12/16

Planning Scheme Amendment // Issue: Final
Collingwood Arts Precinct, Traffic Engineering Report



Attachment 3 - Noise and Traffic Reports CAP

YARRA PLANNING SCHEME

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SCHEDULE [NUMBER] 1 TO THE PARKING OVERLAY

Shown on the planning scheme map as P0[NUMBER]

COLLINGWOOD ARTS PRECINCT

1.0 Parking objectives to be achieved

- To encourage the re-use of buildings within the Collingwood Arts Precinct for arts and creative industries as identified in Schedule [NUMBER] to the Special Use Zone.
- To acknowledge the existing built form and context of the site and the constrained ability to provide on-site car parking.
- To recognise the site's proximity to the Principal Public Transport Network and to reduce car parking demand by encouraging use of sustainable and active transport modes to and from the site.
- To improve amenity for pedestrians around and through the site by minimising vehicle access to and through the site.

2.0 Permit requirement

A permit is not required under Clause 52.06-3 to reduce (including reduce to zero) the number of car parking spaces required under Clause 52.06-5 for any use listed in Section 1 of the Table of Uses in Schedule [Number] to the Special Use Zone.

3.0 Number of car parking spaces to be provided

For the purpose of Clause 52.06-5A, the car parking requirement for a use that:

- is listed in Section 1 of the Table of Uses in Schedule [Number] to the Special Use Zone; and
- is not listed in Table 1 of clause 52.06;

is zero.

For a use that:

- is not listed in Section 1 of the Table of Uses in Schedule [Number] to the Special Use Zone; and
- is listed in Table 1 of Clause 52.06-5;

the car parking rate in Column B of Table 1 in Clause 52.06 applies.

4.0 Decision guidelines for permit applications

Before deciding on an application vary the number of car parking spaces required, the Responsible Authority must consider as appropriate the decision guidelines set out in Clause 52.06-6.

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<p>Melbourne</p> <p>A Level 25, 55 Collins Street PO Box 24055 MELBOURNE VIC 3000 P +613 9851 9600 E melbourne@gta.com.au</p>	<p>Brisbane</p> <p>A Level 4, 283 Elizabeth Street BRISBANE QLD 4000 GPO Box 115 BRISBANE QLD 4001 P +617 3113 5000 E brisbane@gta.com.au</p>	<p>Adelaide</p> <p>A Suite 4, Level 1, 136 The Parade PO Box 3421 NORWOOD SA 5067 P +618 8334 3600 E adelaide@gta.com.au</p>	<p>Townsville</p> <p>A Level 1, 25 Sturt Street PO Box 1064 TOWNSVILLE QLD 4810 P +617 4722 2765 E townsville@gta.com.au</p>
<p>Sydney</p> <p>A Level 6, 15 Help Street CHATSWOOD NSW 2067 PO Box 5254 WEST CHATSWOOD NSW 1515 P +612 8448 1800 E sydney@gta.com.au</p>	<p>Canberra</p> <p>A Tower A, Level 5, 7 London Circuit Canberra ACT 2600 P +612 6243 4826 E canberra@gta.com.au</p>	<p>Gold Coast</p> <p>A Level 9, Corporate Centre 2 Box 37, 1 Corporate Court BUNDALL QLD 4217 P +617 5510 4800 F +617 5510 4814 E goldcoast@gta.com.au</p>	<p>Perth</p> <p>A Level 2, 5 Mill Street PERTH WA 6000 PO Box 7025, Cloisters Square PERTH WA 6850 P +618 6169 1000 E perth@gta.com.au</p>

www.gta.com.au

Attachment 4 - GSLAC Submission - 35 Johnston Street SUZ track changed

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-C--

SCHEDULE [NUMBER] TO THE SPECIAL USE ZONE

Shown on the planning scheme map as **SUZ[number]**.

COLLINGWOOD ARTS PRECINCT**Purpose**

To recognise the Collingwood Arts Precinct as a significant arts and cultural precinct of State and Metropolitan significance.

To provide for the use and development of the site as an integrated hub for the arts and creative industries, including studios, workshops, galleries, rehearsal, performance, live music and event spaces for training, production, entertainment and community engagement activities.

~~To provide for a broad range of arts and cultural activities including studios, workshops, galleries, rehearsal, performances and events spaces, and spaces for training, production and community engagement activities and associated accommodation for artists and workers in the creative industries.~~

To facilitate the activation of the site and provide for public access and the use of outdoor spaces, ~~including the western courtyard, for informal outdoor recreation, events and performances including live music entertainment events managed in accordance with a Noise Management Plan approved under this schedule.~~

To encourage the adaptive re-use and re-purposing of existing vacant buildings including provision for contemporary design responses.

To ensure that the future use and development of the Collingwood Arts Precinct does not unduly impact on the amenity of surrounding areas.

~~To recognise the site's proximity to the Principal Public Transport Network and to reduce car parking demand by encouraging use of sustainable and active transport modes to and from the site.~~

1.0
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Table of uses**Section 1 - Permit not required**

Use	Condition
Accommodation	Must be used in conjunction with the use of the site for arts and creative industries, or must be accommodation for artists or other workers in the creative industries.
Art and craft centre	
Cabaret	
Caretaker's house	
Cabaret	<u>Must be in accordance with an approved Masterplan</u>
Cinema based entertainment facility	
Cinema based entertainment facility	
Dancing school	
Education centre (other than Primary School or Secondary School)	<u>Must be used in conjunction with the use of the site for arts and creative industries</u>
Food and drink premises	<u>The leasable floor area must not exceed 150 square metres</u>
Home occupation	<u>Must be used in conjunction with the use of the site for arts and creative industries</u>
Home occupation	<u>Must be in accordance with an approved Masterplan</u>
Leisure and recreation (other than Major sports and recreation facility and Motor racing track)	
Market	
Office	<u>Must be used in conjunction with the use of the site for arts and creative industries</u>

Attachment 4 - GSLAC Submission - 35 Johnston Street SUZ track changed

Place of assembly (other than Amusement Parlour, and Nightclub)	Must be in accordance with an approved Masterplan
Rehearsal studio	
Rehearsal studio	
Art studio	
Shop	Must be used in conjunction with the use of the site for arts and creative industries
Store	
Store	
Utility installation (other than reservoir)	
Any use listed in Clause 62.01	Must meet the requirements of Clause 62.01.

Section 2 - Permit required

Use	Condition
Accommodation (if the condition in Section 1 is not met)	Must be used in conjunction with the use of the site for arts and creative industries, or must be accommodation for artists or other workers in the creative industries.
Child care centre	
Night club (other than Cabaret)	
Industry (other than Refuse disposal and Transfer station)	Must not be a purpose listed in the table to Clause 52.10.
Manufacturing sales	
Office (if the condition in Section 1 is not met)	
Shop (if the condition in Section 1 is not met)	
Warehouse (other than Store)	Must not be a purpose listed in the table to Clause 52.10.
Any other use not in Section 1 or 3	

Section 3 - Prohibited

Use
Animal husbandry
Aquaculture
Brothel
Service station
Corrective institution
Transport terminal
Veterinary centre
Corrective institution
Intensive animal husbandry

2.0 Use of land

2.1 Application Requirements

An application to use land must be accompanied by the following information, as appropriate:

- Any Master Plan approved for the site.
- The purpose of the use and the types of activities which will be carried out.
- The likely effects, if any, on nearby uses including noise levels, traffic, parking, the hours of delivery and dispatch of goods and materials, hours of operation.
- The compatibility of the proposed use with the purpose of the zone and the use of the site as an arts and creative industries precinct.

Attachment 4 - GSLAC Submission - 35 Johnston Street SUZ track changed

- Maintenance of areas associated with the use.

2.2 Decision Guidelines

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Before deciding on an application for a planning permit or a request to approve a Master Plan, the responsible authority must consider as appropriate how the proposal would contribute to:

- Any Masterplan approved for the site.
- The delivery of an arts and creative industries precinct of State and Metropolitan importance.
- Facilitating an evolving physical and cultural development of the site which reflects the changing needs of the arts and creative community.
- Integrating the precinct into its context and maximising opportunities for community access and use.
- Recognising the importance of accommodating a variety of events in integrating the precinct with the surrounding community.
- ~~Maintaining the heritage values of the site and providing protection to and appreciation of the Keith Haring mural, while providing for contemporary design responses and adaptive re-use of the site.~~
- ~~Organising access and circulation to maximise accessibility by pedestrians and cyclists and minimising conflicts with vehicles.~~
- ~~Encouraging sustainable transport choices by maximising opportunities for access by means other than private cars and minimising on site car parking.~~
- The integrated planning of the precinct.

2.3 Noise Management

The use of the land must be managed so as to limit noise in accordance with the following:

- State Environment Protection Policy (Control of Noise from Industry, Commerce, and Trade) No. N-1 ("SEPP N-1").
- State Environment Protection Policy (Control of Music Noise from Public Premises) No. N-2 ("SEPP N-2").
- The following Patron Noise Criteria:

For L_{Aeq} noise levels, the less stringent of the following:

- if noise is assessed external to a Noise Sensitive Building, noise emissions must comply with noise limits determined according to SEPP N-1 (as though patron noise were a noise source regulated by this Policy, but with no other modification to the application of the Policy).
- if noise is assessed internal to a Noise Sensitive Building, noise emissions must achieve the lower of the design sound level range for (whichever is relevant) houses and apartments in inner city areas, entertainment districts, or near major roads, as provided in Australian Standard AS2107:2016 'Acoustics—Recommended design sound levels and reverberation times for building interiors'.

For L_{Amax} noise levels, during the hours of 10pm-7am:

- 'Sleep Disturbance Criteria', being a noise level of 55dB L_{Amax} , assessed inside a habitable room of a Noise Sensitive Building normally used for the purpose of sleeping.

Attachment 4 - GSLAC Submission - 35 Johnston Street SUZ track changed

- For the purpose of the Patron Noise Criteria, 'Noise Sensitive Building' means a building referred to in the definition of 'Noise sensitive area' in SEPP N-1, being a Dwelling, Residential Building, Caretaker's House, Hospital, Hotel, Institutional Home, Motel, Reformatory Institution, Tourist Establishment or Work Release Hostel.

When deciding on an application (including an application for approval of a Master Plan under this schedule) the responsible authority must have regard to, as appropriate:

- The Noise Management Plan prepared by Noise Consulting & Management Pty Ltd dated 21 December 2016. This Noise Management Plan may be amended to the satisfaction of the Responsible Authority.
- For noise that is not subject to SEPP N-1, SEPP N-2 or patron noise criteria (as specified in this clause), EPA publication no. 1254, 'Noise Control Guidelines' 1254 and any other relevant noise guidelines published by EPA from time to time.

2.4 Management of waste

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Before any use commences (other than uses for which a planning permit has already been granted) a Waste Management Plan must be prepared to the satisfaction of the Responsible Authority. This requirement does not apply if waste management arrangements are provided for under a Masterplan approved by the Responsible Authority under clause 4.3.

The use of the land must be generally in accordance with an approved Waste Management Plan under this clause, or generally in accordance with an approved Master Plan under clause 4.3, as appropriate.

If in the opinion of the responsible authority a Waste Management Plan is not relevant to the evaluation of an application, or if the responsible authority considers that an existing approved Waste Management Plan adequately manages the waste anticipated to be generated by a new use, the responsible authority may waive the requirements of this clause.

3.0 Subdivision

3.1 Application requirements

An application to subdivide land must be accompanied by the following information, as appropriate:

- Any Masterplan approved for the site.
- The purpose of the subdivision.
- The likely effects the subdivision will have on adjacent land.
- The effects of the subdivision on the use of the site as an integrated creative arts precinct.

3.2 Decision guidelines

Before deciding on an application, the responsible authority must consider, as appropriate:

- The interface with adjoining land and any existing residential properties adjoining the site.
- The effect the subdivision will have on the potential of the land to accommodate existing and potential future uses in accordance with the purpose of this zone.

3.3 Exemption from notice and review

An application to subdivide land is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

Attachment 4 - GSLAC Submission - 35 Johnston Street SUZ track changed

4.0 Buildings and Works

4.1 Application Requirements

An application to construct a building or construct or carry out works must be accompanied by the following information, as appropriate:

- Any Masterplan approved for the site
- A plan, drawn to scale, that shows:
 - The boundaries and dimensions of the site.
 - Relevant ground levels.
 - The location, height and purpose of buildings and works on adjoining land.
 - The layout and use of existing and proposed buildings and works, accessways, and car parking and loading areas.
 - Elevation drawings to scale indicating the colour and materials of all proposed ~~buildings and works.~~ buildings and works.
 - Details of proposed landscaping.
 - Details of any heritage fabric to be altered or removed.
 - Vehicle and pedestrian entry and exit points for the site.

4.2 Decision Guidelines

Before deciding on an application, the responsible authority must consider, as appropriate:

- Any Masterplan approved for the site.
- The development of the site as an integrated arts precinct and the purposes of the zone.
- The interface with adjoining land, especially the relationship with existing residential properties.
- The location and type of access to the site.
- The provision and location of car parking.
- The appearance and bulk of buildings having regard to the adjoining land, especially the relationship with residential areas.
- The provision for landscaping.
- The movement of pedestrians and cyclists and vehicles providing for supplies, waste removal, emergency services and public transport.
- The effect of the proposed buildings and works on the amenity of the neighbourhood, including the effects of noise, lighting, overshadowing, building bulk and privacy.
- Maintaining the heritage values of the site and providing protection to and appreciation of the Keith Haring mural, while providing for contemporary design responses and adaptive re-use of the site.
- Organising access and circulation to maximise accessibility by pedestrians and cyclists and minimising conflicts with vehicles.
- Encouraging sustainable transport choices by maximising opportunities for access by means other than private cars and minimising on site car parking

4.3 ~~Masterplan~~

~~A Masterplan may be prepared for the precinct (or part of the precinct) and approved by the Responsible Authority.~~

~~An approved Masterplan may include, but is not limited to, the following:~~

- ~~The arrangement of land uses and activities across the precinct.~~
- ~~Access and circulation arrangements.~~
- ~~Measures to manage traffic, car parking and loading.~~
- ~~Proposed buildings and works including the location, extent and design of buildings.~~
- ~~Landscaping and the treatment of external areas.~~
- ~~Measures to manage noise.~~
- ~~Measures to protect the heritage features of the site while providing for contemporary design responses.~~
- ~~Waste management arrangements.~~

~~Once approved, the Masterplan may be amended to the satisfaction of the responsible authority.~~

Attachment 4 - GSLAC Submission - 35 Johnston Street SUZ track changed

4.4 Exemption from notice and review

An application to construct a building or construct or carry out works ~~for a use in Section 1 of this schedule~~ that is generally in accordance with an approved Masterplan is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

5.0 Advertising signs

Advertising sign requirements are at Clause 52.05. This zone is in Category 1.

4.36.0 Masterplan

A Masterplan may be prepared for the precinct (or part of the precinct) and approved by the Responsible Authority.

An approved Masterplan ~~may~~ must include, but is not limited to, the following:

- The arrangement of land uses and activities across the precinct.
- Maximum patron numbers across the site
- Access and circulation arrangements.
- Measures to manage traffic, car parking and loading.
- Proposed buildings and works including the location, extent and design of buildings.
- Landscaping and the treatment of external areas.
- Measures to manage noise.
- Measures to protect the heritage features of the site while providing for contemporary design responses.
- Waste management arrangements.

Once approved, the Masterplan may be amended to the satisfaction of the responsible authority.

Attachment 5 - Parking Overlay Schedule Track Changes

YARRA PLANNING SCHEME

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SCHEDULE [NUMBER] 1 TO THE PARKING OVERLAY

Shown on the planning scheme map as P0[NUMBER]

COLLINGWOOD ARTS PRECINCT

1.0 Parking objectives to be achieved

- To encourage the re-use of buildings within the Collingwood Arts Precinct for arts and creative industries as identified in Schedule [NUMBER] to the Special Use Zone.
- To acknowledge the existing built form and context of the site and the constrained ability to provide on-site car parking.
- To recognise the site's proximity to the Principal Public Transport Network and to reduce car parking demand by encouraging use of sustainable and active transport modes to and from the site.
- To improve amenity for pedestrians around and through the site by minimising vehicle access to and through the site.

2.0 Permit requirement

A permit is not required under Clause 52.06-3 to reduce (including reduce to zero) the number of car parking spaces required under Clause 52.06-5 for any use listed in Section 1 of the Table of Uses in Schedule [Number] to the Special Use Zone.

3.0 Number of car parking spaces to be provided

For the purpose of Clause 52.06-5A, the car parking requirement ~~for a use that:~~

- ~~for a use that~~ is listed in Section 1 of the Table of Uses in Schedule [Number] to the Special Use Zone; ~~and is zero.~~
- ~~is not listed in Table 1 of clause 52.06;~~
~~is zero.~~

~~For a use that:~~

- ~~for a use that:~~
 - is not listed in Section 1 of the Table of Uses in Schedule [Number] to the Special Use Zone; and
 - is listed in Table 1 of Clause 52.06-5;
- the car parking rate in Column B of Table 1 in Clause 52.06 applies.

Any other uses not within the above categories must be provided with car parking to the satisfaction of the Responsible Authority.

Attachment 5 - Parking Overlay Schedule Track Changes

4.0 Decision guidelines for permit applications

Before deciding on an application vary the number of car parking spaces required, the Responsible Authority must consider as appropriate the decision guidelines set out in Clause 52.06-6.

Attachment 6 - Council submission to GSLAC amendment for 35 Johnston Street and 50 Perry Street

The Chair
Government Land Standing Advisory Committee
Planning Panels Victoria
1 Spring Street,
Melbourne VIC 3000

Dear Sir/Madam,

35 JOHNSTON STREET AND 50 PERRY STREET, COLLINGWOOD

Council wishes to lodge a submission to the Government Land Standing Advisory Committee on the proposed planning scheme changes for 35 Johnston Street and 50 Perry Street, Collingwood.

Council at its meeting on 21 March, 2017 considered the State Government proposal to rezone the subject land from Mixed Use Zone (MUZ) to a Special Use Zone (SUZ), together with a new Schedule to the SUZ, a new Schedule to Clause 52.43 (Live Music and Entertainment Noise), and applying the Parking Overlay with a new Schedule to the Parking Overlay at Clause 45.09.

Importantly, Council values the importance of the Collingwood Arts Precinct and as such supports the amendment, subject to the changes suggested in the attachment to this letter. The attachment will form part of Council's submission at an Advisory Committee hearing.

The changes in the attachment address the following key issues identified by Council officers:

- The Schedule to the Special Use Zone provides an unacceptable level of flexibility and lack of accountability in terms of future use on the site.
- The requirements for a Masterplan can be strengthened so that more of the activity that occurs on the site in the future is related a Masterplan that is approved by Council as the Responsible Authority.
- Some drafting issues identified in the Schedule to the Parking Overlay.

Attachment 6 - Council submission to GSLAC amendment for 35 Johnston Street and 50 Perry Street

Attached to this covering letter are track changed versions of the Schedule to the Special Use Zone and the Schedule to the Parking Overlay.

If you have any questions regarding the submission, please contact Evan Burman, Strategic Planner at Yarra City Council on 9205 5075.

Yours sincerely,

Bruce Phillips

Director Planning and Place Making

11.3 Potential for Council to offer Environmental Upgrade Agreements (EUA's) - 2017 update

Trim Record Number: D17/25380

Responsible Officer: Assistant Director Planning and Place Making

Purpose

1. To update Councillors on the progress of EUA's in Victoria and whether there has been any change in the value proposition for City of Yarra.
2. This responds to the Council Resolution from 19/04/2016 (Attachment 1).

Background

3. In late 2015 the Victorian State Government amended the local government act to allow all Victoria Local governments to enable EUA legislation locally.
4. Following this, Councillors endorsed a Motion that: *"Council receives a report from officers, before or at the first Council meeting of December 2015, detailing the costs and benefits of Environmental Upgrade Agreements in the City of Yarra once the legislative and regulatory circumstances support their use beyond the City of Melbourne boundaries, and how Council or its entities (e.g. Yarra Energy Foundation) could promote the use of Environmental Upgrade Agreements in the City of Yarra"*.
5. Based on an extensive Officer review at the time (refer Attachment 1), the recommendation was:
 - (a) *at this stage, not to pursue the commencement of Environmental Upgrade Agreements in the City Of Yarra; and*
 - (b) *request officers present an update to Councillors in early 2017, reviewing progress of EUA's in Victoria and whether there has been any change in the value proposition.*
6. The main reasons for this recommendation in 2016 were:
 - (a) *"The uptake, based on Melbourne and NSW experience, is expected to be very low.*
 - (b) *"This is very new legislation and exact implications for Council are as yet unclear, however would have long term consequences.*
 - (c) *"Other Victorian Local Governments appear to be in the same position of uncertainty, creating a situation where there could be considerable change or consensus in the next 12 months that could change the value of the proposal.*
 - (d) *"Whilst the State Government has amended the Local Government Act to allow EUA's, it is not currently supporting Councils with implementation of agreements (as occurs in NSW).*
 - (e) *"There are no known businesses waiting on EUA's being enacted in Yarra, and there have been no questions asked from the business community.*
 - (f) *"The program is currently unbudgeted, with no allocated resources. There could also be longer term negative financial implications which are unclear at this point in time."*
7. A year on, a brief review suggests that the value proposition for Yarra has not changed, and the recommendation would again be to not pursue the commencement of Environmental Upgrade Agreements in the City of Yarra.
8. The main reason for this are:
 - (a) there has not been any significant changes to the program offering improving the value proposition for Yarra;
 - (b) there has not been any local business contact Yarra requesting Yarra enact this legislation;

- (c) there is no evidence of significant uptake from Victorian Councils who have enacted this legislation; and
- (d) the program is unbudgeted, with no allocated resources.

External Consultation

- 9. Officers have consulted with the Yarra Energy Foundation (YEF) and Sustainable Melbourne Fund (SMF).
- 10. SMF indicated that the value proposition had not changed since officers last sought advice. SMF advised that there are now 11 Councils (including City of Melbourne) who are offering EUA's, and there have been approximately 17 new projects instigated in Victoria in the last 12 months.
- 11. YEF has advised that it has not had any approaches from businesses interested in this program.

Internal Consultation (One Yarra)

- 12. Conversations were held with the Economic Development Unit which also confirmed the lack of request for Yarra to enable EUA Legislation.

Financial Implications

- 13. There are no financial implications to not pursue the commencement of Environmental Upgrade Agreements in the City of Yarra.
- 14. If Council did want to pursue the commencement of Environmental Upgrade Agreements in the City of Yarra, updated financial and resource implications would need to be sought.

Economic Implications

- 15. There are potential negative Economic implications of not offering Environmental Upgrade Agreements to Yarra business, but given the low expected uptake and lack of current interest this is deemed insignificant.
- 16. If at any stage there was a strong push or offering of EUA's by Yarra businesses, the potential to establish and offer local agreements could be considered by Council at that time.

Sustainability Implications

- 17. There are potential negative Sustainability implications of not offering Environmental Upgrade Agreements to Yarra business, but given the low expected uptake and lack of current interest this is deemed insignificant.
- 18. If at any stage there was a strong push or offering of EUA's by Yarra businesses, the potential to establish and offer local agreements could be considered by Council at that time.

Social Implications

- 19. There are no known social implications of this proposal.

Human Rights Implications

- 20. There are no known human rights implications of this proposal.

Communications with CALD Communities Implications

- 21. There are no communications with CALD communities implications of this proposal.

Council Plan, Strategy and Policy Implications

- 22. This report responds to an Action in the Yarra Environment Strategy 2013-17 under Objective 3.4.1 – *Provide direct support to residents and businesses to reduce and offset emissions towards carbon neutrality by 2020*. Action 3.4.1.2 states that *Council will continue to develop partnerships and joint activities, & support projects including: Investigation of the potential benefits from establishing Environmental Upgrade Agreements (such as in the City of Melbourne; this would require changes to the Local Government Act and would be best undertaken on a state level through MAV or NAGA).*

Legal Implications

23. There are no legal implications of this proposal.

Other Issues

24. Nil

Options

25. Option 1 – continue to not enact Environmental Upgrade Agreement legislation at this time.
26. Option 2 – indicate a preference to enact Environmental Upgrade Agreement legislation, and request an updated report on the financial and resource implication, and process for doing so.

Conclusion

27. It is recommended that Yarra continue to not pursue the commencement of Environmental Upgrade Agreements in the City of Yarra at this time, as there have not been any significant changes to the program offering improving the value proposition for Yarra.

RECOMMENDATION

1. That Council note the officer report in relation to Environmental Upgrade Agreements.
2. That Council do not pursue the commencement of Environmental Upgrade Agreements in the City of Yarra at this time, but reconsider if any Yarra businesses indicate an interest in the process in the future.

CONTACT OFFICER: Michael Oke
TITLE: Environment Coordinator
TEL: 9205 5723

Attachments

- 1 Council Resolution from April 2016

Attachment 1 - Council Resolution from April 2016

COUNCIL

RESOLUTION ITEM 4781

TO: ENVIRONMENT COORDINATOR

FOR ACTION

ENVIRONMENTAL UPGRADE AGREEMENTS

Meeting Date: 19/04/2016

Target Date: 03/05/2016

Notes:

File Number: D16/37884

COUNCIL RESOLUTION

Moved: Councillor Gaylard

Seconded: Councillor Coleman

1. That Council:

- (a) note the officer report providing further information on Environmental Upgrade Agreements;
- (b) note the benefits and implications of the opportunities presented by Environmental Upgrade Agreements;
- (c) resolve that, at this stage, not to pursue the commencement of Environmental Upgrade Agreements in the City Of Yarra; and
- (d) request officers present an update to Councillors in early 2017, reviewing progress of EUA's in Victoria and whether there has been any change in the value proposition.

CARRIED UNANIMOUSLY

ACTION TAKEN BY OFFICER

Please provide information regarding your work on this action in the notes section of TRIM. Do not update this Word document.

11.4 Business Advisory Group - appointment of business representatives

Trim Record Number: D17/22567

Responsible Officer: Manager City Strategy

Purpose

1. For Council to appoint four representatives from the business community to the Business Advisory Group (BAG).

Background

2. The BAG Terms of Reference (TOR) require that one-third of the Advisory Group rotate each year.
3. There are currently four vacant positions on the BAG.
4. Council's policy on the appointment of community and/or other members to Council Committees states that the *Council will by resolution make the final determination on the selection and appointment of members to any Council Committee.*
5. Members appointed to the BAG will be appointed for a two year term and will be eligible to serve no more than two consecutive terms (four years).

External Consultation

6. The nomination period for the four positions on the BAG was held for a period of four weeks. Advertisements calling for nominations for membership of the BAG appeared on Councils website and Facebook and in two separate editions of Councils Business e-bulletin.

Internal Consultation (One Yarra)

7. Information on the nomination process was featured as a Hot Topic.

Financial Implications

8. The ongoing costs associated with the BAG are covered by the Economic Development Unit operational budget. There are no significant financial implications that would arise out of appointing the additional members.

Economic Implications

9. The objectives of the BAG include:
 - (a) capitalising on the knowledge, experience and skill available in Yarra's large and dynamic local business community to assist Council with the implementation of the objectives in the Yarra Economic Development Strategy;
 - (b) raising the profile of the business community in Council activities and planning to support the significant contribution made to our local economy;
 - (c) ensuring that a key sector of the community is provided with an appropriate communication mechanism with Council; and
 - (d) providing strategic advice to Council on issues that affect the business community.

Sustainability Implications

10. There are no apparent sustainability implications.

Social Implications

11. There are no apparent social implications.

Human Rights Implications

12. There are no apparent human rights implications.

Communications with CALD Communities Implications

13. There are no apparent implications relating to CALD communities.

Council Plan, Strategy and Policy Implications

14. The BAG will continue to play a key role in overseeing the implementation and review of Councils Economic Development Strategy.

Legal Implications

15. There are no apparent legal implications.

Nomination to the BAG

16. A total of 10 nominations were received. These have been assessed by the two BAG Council delegates Cr Fristacky and Cr Searle and the Coordinator, Economic Development. An assessment of all nominations has been circulated as a confidential addendum to this report. Nominations considered the following:
- (a) representation across the municipality;
 - (b) representation of different size businesses;
 - (c) representation of different business sectors;
 - (d) local business proprietor, property owner and/or employee based in the City of Yarra;
 - (e) not a member of another Yarra City Council Community Advisory Committee;
 - (f) business experience and skills, relevant network connections;
 - (g) ability to work collaboratively with other businesses and/or between business and government;
 - (h) a strong interest in the ongoing development of a prosperous and unique economy in the City of Yarra;
 - (i) willing to commit 2 hours per quarter (8 hours per annum) in attending Business Advisory Group meetings; and
 - (j) an ability to complement the skills and attributes of other members of the Group.

Conclusion

17. Four business representatives are required to be appointed to the BAG. Council is requested to now make these formal appointments.

RECOMMENDATION

1. That Council note the report of officers in relation to the Business Advisory Group.
2. The Council, having considered the nominations received for the four business representative positions on the BAG, appoint the following applicants to the BAG:
- (a) _____
 - (b) _____
 - (c) _____
 - (d) _____
3. That the successful and non-successful nominees be advised in writing of the outcome of their nomination.

CONTACT OFFICER: Kim Swinson
TITLE: Coordinator Economic Development

TEL: **9205 5303**

Attachments

There are no attachments for this report.

11.5 Yarra Libraries Local History Policy 2017

Trim Record Number: D16/183215

Responsible Officer: Director Community Wellbeing

Purpose

1. To seek Council's adoption of the attached Yarra Libraries Local History Policy 2017.

Background

2. The Yarra Libraries Local History Collection Development Policy (LHCDP) in Attachment One is a revision of the LHCDP 2009.
3. The purpose of a LHCDP is to define the principles and processes guiding the acquisition, conservation and organisation of information and resources pertaining to the history of the local area within the context of Yarra Libraries overall mission. It provides an objective framework for building a balanced Local History collection in accordance with community needs and diverse formats that support access to information.
4. The LHCDP is a key document in the strategic framework of a public library. It is vital to the development of Local History collections, to enrich the community's understanding of its local identity as well as the past which has shaped the present. It is an essential management tool enabling responsible asset management.
5. The Local History collection and its use must be continually developed and monitored to ensure that it meets evolving community needs. The collection requires ongoing assessment to take advantage of potential developments and enhancements that respond to the needs of the community. It aims to make local history resources available in the most appropriate and cost-effective way.
6. The LHCDP includes guidance on providing specialist advice to guide and inform local history collection requirements and access to national and global electronic resources, including a growing number of online heritage resources.

External Consultation

7. The LHCDP 2017 was made available at libraries and on the website for community comment from mid-January 2017 to end of February 2017 to allow sufficient time to engage all relevant stakeholders.
8. Key stakeholder groups such as the Local History groups were engaged. Feedback and comments were received from both the Collingwood and Fitzroy Local Historical Societies on the revised policy. These comments were considered and resulted in minor changes to the Policy as outlined below:
 - (a) Section 2.3: Reference to "Post-1836" has been removed, as requested by the Collingwood Historical Society;
 - (b) Section 6.1: The reference to "purchasing digital formats in preference to physical formats", has been revised to read "purchasing digital formats or physical formats where available."; and
 - (c) Section 10: The words, "We aim to consult with the relevant Historical Societies if additional expertise regarding relevance of content is required" have been replaced with "We will consult with the relevant Historical Societies regarding relevance of content where required."

Internal Consultation (One Yarra)

9. Not applicable.

Financial Implications

10. There are no immediate financial implications resulting from this report or the Policy. Yarra Libraries operates within the operating budget adopted by Council 2016/2017 and will implement the Policy within existing resources.

Economic Implications

11. There are no economic implications inherent in this report.

Sustainability Implications

12. There are no sustainability implications inherent in this report.

Social Implications

13. The LHCDP ensures Yarra Libraries continues to serve the community through the acquisition, cataloguing, classification, preservation, digitisation and provision of access to a well-organised and balanced collection reflecting Yarra's diverse cultural heritage, and meeting the needs of its diverse demographic profile.

Human Rights Implications

14. Yarra Libraries actively supports the Charter of Human Rights and Responsibilities Act 2006. The Charter of Human Rights identifies 20 basic responsibilities for agencies delivering services on behalf of local communities. Yarra Libraries, through a range of policies, supports freedom of thought, conscience, religion and belief, freedom of expression, taking part in public life and cultural life.

Communications with CALD Communities Implications

15. Yarra Libraries meets the needs of the culturally and linguistically diverse members of the Yarra community through the provision of information and resources in community languages where appropriate. The library service engages the community through the delivery of programs and services that engage, value and promote cultural diversity within the community.

Council Plan, Strategy and Policy Implications

16. Development of the Local History Collection Development Policy with public feedback is a key action of the Yarra Libraries Strategic Plan which impacts significantly on all collection related actions through the acquisition, cataloguing, classification, preservation and provision of access to a well-organised and balanced collection reflecting Yarra's diverse cultural heritage, and meeting the needs of its diverse demographic profile.

Legal Implications

17. There are no legal implications inherent with this policy.

Other Issues

18. There are no other issues to report.

Options

19. No other options have been considered.

Conclusion

20. The revision of the Local History Collection Development Policy is an essential step to ensure Yarra Libraries continues to provide an appropriate framework for building a balanced Local History collection in accordance with community needs and diverse formats that support access to information.

RECOMMENDATION

1. That Council adopt the attached Yarra Libraries Local History Policy 2017.

CONTACT OFFICER: Felicity Macchion

TITLE: Manager Yarra Libraries
TEL: 9205 5389

Attachments

1 Local History Collection Development Policy (DRAFT for Endorsement) March 2017

**Attachment 1 - Local History Collection Development Policy (DRAFT for Endorsement)
March 2017**



Yarra Libraries

Local History Collection Development POLICY

1. Introduction

The Yarra Libraries Local History Collection is of central importance in supporting and enriching the community's understanding of its local identity as well as the past which has shaped the present. The acquisition, conservation and organization of these resources, prior to making them appropriately accessible for different levels of community need, is a primary responsibility of Yarra, one which cannot be appropriately shared with other Library services. While other library collections are regularly culled, Local History resources are of lasting and cumulative value for developing local knowledge and understanding.

The Yarra Libraries Collection Development Policy (YLCDP) guides and informs development of the general public library collections. The Yarra Libraries Local History Collection Development Policy is a companion document to the YLCDP, providing specialist advice to guide and inform local history collection requirements.

Yarra Libraries provides library and information services to residents, workers and visitors to the City of Yarra at its five branch libraries. Our service is enhanced by the Yarra Libraries virtual library, providing access to national and global electronic resources, including a growing number of online heritage resources.

2. Background

Our local communities have a strong sense of identity. Yarra's libraries are one of the main sources for people seeking information pertaining to the history of the area. There is a high level of interest in local studies and the libraries have close links with community based groups such as local historical societies. Numerous rural, interstate and overseas inquiries are also received.

The LHC is recognised as an important element of the library collection, particularly given that the municipality includes Melbourne's earliest municipalities. Australian Library and Information Association (ALIA) states in its Policy and Planning Guidelines for Public Libraries that: "The public library has a responsibility to ensure that information about the history of the local community is collected, retained and made available."

Attachment 1 - Local History Collection Development Policy (DRAFT for Endorsement) March 2017

The history of the local community shall encompass:

- 2.1 The area within the municipal boundary of the City of Yarra
- 2.2 As municipal boundaries change Yarra's heritage collections will draw on the resources of neighbouring libraries and other institutions to cover new areas.
- 2.3 The collection period encompasses the pre-settlement era (pre-1835) to the present.
- 2.4 The social, political, economic and physical history of the above areas, including all cultural and ethnic groups.
- 2.5 Where appropriate, materials with a wider area coverage than the City of Yarra will be included, (e.g. adjacent areas in other municipalities).

Yarra Libraries provide services and collections from five public libraries located at Carlton, Collingwood, Fitzroy, North Fitzroy and Richmond. All branches have a Local History collection, predating the formation of the City of Yarra, reflecting the diverse municipalities that amalgamated in 1994. These collections vary in size and content, according to their collection development precedent.

Yarra Libraries will develop the five Local History Collections as an integrated resource for the whole city. While each collection is housed in a separate library and will continue to be focussed on its former municipal area, the future development of Yarra's Local History Collection is based on meeting the educational and research needs of their different local communities in a comprehensive, consistent, coherent and equitable way.

Yarra Libraries is committed to an ongoing digitising programme to improve access to our collection. We have a digital asset management tool (Portfolio) which enables PDFs, images, videos, and audio files to be accessed and available via our online catalogue.

3. Local History Collection Development Policy Rationale

The Local History Collection Development Policy is an essential component of effective public library collection management. Informed by professional standards, driven by community need and the strategic planning process, the Local History Collection Development Policy communicates the parameters of the collection and a framework for its development.

The Collection Development Policy document has been developed by the Yarra Libraries specialist staff according to professional industry guidelines. Responsibility for collection development and delivery sits with the Co-ordinator Community Learning and Partnerships. For Local History resources, responsibility sits with the Team Leader Digital and Community Learning. These roles report to the Manager Library Services, who is responsible for policy delivery.

The purpose of Yarra's Local History Collection (LHC) is to serve the community through the acquisition, cataloguing, classification, preservation and provision of access to a well-organised and balanced collection reflecting Yarra's diverse cultural heritage, and meeting the needs of its diverse demographic profile.

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It should conduct this in the following ways:

- 3.1 By acting as a source of information on the history of the City of Yarra. Recent materials held or related to collections in neighbouring municipalities will not be actively collected.
- 3.2 By continuing to acquire material relevant to the community in these designated areas.
- 3.3 By providing a safe and secure repository for material of past, present or future interest that might otherwise be lost to the community.
- 3.4 By acting as a central point of knowledge and contact for all interested parties, both users and collectors.
- 3.5 By ensuring that we protect our digital content aligning with best practice to ensure long term access.
- 3.6 Through the availability of contemporary LHC location tools.
- 3.7 By working cooperatively with Local Historical Societies.

The lasting historical and information value of the items in the LHC is more important than just visual appeal. The materials in this collection are intended for both general education purposes and specialised research. To maximise their longevity, they are not available for loan. Duplicate copies of some published resources may be available in the lending collection. Preservation and/or conservation work will be undertaken according to professional standards and advice. Material in poor condition may be withdrawn from public access. Where possible, access to document copies will be provided.

Yarra's branch libraries have differing storage and preservation capacities. Yarra Libraries therefore has the responsibility of ensuring that when unique material is acquired it is provided with appropriate archival storage in a suitable location for the integrated collection.

4. Policy Context

4.1 Council Plan

The direction of the *Yarra Libraries Strategic Plan* is notably characterised by strategic objectives of the City of Yarra. These are:

- Serving Yarra's Community
- Supporting a diverse and dynamic Yarra

4.2 Yarra Libraries Strategic Plan

The Yarra Libraries Strategic Plan is developed to accord with the priorities of the Council Plan framework. The Yarra Library Strategy 2017 – 2020 will inform the implementation of this CDP.

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4.3 Yarra Heritage Strategy 2015 – 18

Refers to the objects, collections, documents and records of the City which encompass both Aboriginal and non-Aboriginal heritage.

5. Collection Development Framework

5.1 Collection Principles

The Yarra Libraries Collection Development Policy:

- Supports the development and delivery of identified strategic objectives and actions
- Supports the identification and allocation of collection funds for current and future priorities
- Ensures that objectivity and professional rigor inform the selection and weeding processes
- Provides guidelines for the collection, including subject profiles, depth of coverage and resource formats
- Provides an essential tool for induction, reference and ongoing staff training

5.2 Funding and Management

Sustainable funding for the purchase of library materials is critical to the development of the Yarra Libraries Local History collections.

Budgets for specific collections are reviewed annually by the Library Leadership Team, taking into account usage levels, community trends, special collection needs, developing trends and emerging formats. Local History specific funding to support cumulative use and storage needs will be submitted to the budget cycle for consideration in accordance with Council's priorities and strategic objectives.

5.3 Community and Stakeholder Engagement

Local History Societies in Yarra have a strong commitment to ensuring that the library acquires and preserves relevant Local history material. Yarra Libraries has a commitment to engaging with the community and uses a range of community consultation mechanisms. Local History Societies are welcome to provide suggestions and feedback by advising staff, attending local library branch meetings, using feedback forms at each branch and via the website. We also welcome any suggestions for items to purchase that fit our selection guidelines.

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6. Access

6.1 Collection Access

Yarra Libraries ensures access by:

- Providing knowledgeable, friendly and helpful staff.
- Providing an online catalogue accessible via the library website offering 24/7 service.
- Providing prompt, ongoing, regular cataloguing of Local History new acquisitions which are added to the online catalogue, minimising backlog.
- Answering Local History enquiries that are received either via the library website, in person or by phone in a timely manner.
- Contributing to State or National co-operative ventures which will enhance access to Yarra's photograph collection.
- Investigating and contributing to any future digitization projects.
- Providing scanned copies of copyright-free digital images to individuals as requested. This may incur a small charge to cover the cost of providing images on a DVD.
- Working with Local Historical Societies to increase access via digitization of material
- Endeavouring to purchase duplicate copies to ensure lending copies of materials wherever possible and as the budget allows.
- Ensuring that fragile and unique material is handled appropriately.
- Ensuring that the library stores items in conditions that will assist long term preservation, with periodic review to ensure appropriate conditions for different formats.
- Purchasing digital formats or physical formats where available.

The local history collection is a non-lending collection and as such resources are not made available to other libraries through inter library loan (SWIFT, LibraryLink or Trove). In instances where Yarra has the last remaining copy of significant historical material, Yarra Libraries may provide duplicate copies for other organizations. On request, items may be made available for viewing at other branch libraries subject to condition of items and branch storage conditions

6.2 Policy Statement

- All materials purchased or donated for the LHC become the property of the City of Yarra.
- A donation form must be filled out by any person or institution wishing to donate items to the Local History collection. Donations are to be without restriction regarding use or future disposition.

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- Any proposed acquisitions will be subject to the selection guidelines before inclusion in the collection. Worthwhile materials that do not fit the above criteria will be offered to other appropriate organisations/institutions.
- The LHC is a reference collection for use in the library only.

6.3 Policy Review

A review of the Local History Collection Development Policy, including a public comment process, will be conducted every four years to ensure that the collections are managed in accordance with contemporary professional standards in line with community expectations.

7. Scope

This is a non-lending collection, although lending copies for the general collection are purchased where possible. The aim of the Local History collection is to provide a range of materials reflecting the rich diverse heritage of the municipality. The library aims to acquire, describe and preserve resources relating to the City of Yarra in co-operation with Yarra's local history groups.

These individual collections consist of books, theses, photographs, newspapers, maps, periodicals, journal articles, pamphlets, ephemera, manuscripts, portraits, prints, microform, rare books, audio and videotapes, DVDs and electronic databases. The collection features small realia items.

Collections may also include artworks that best illustrate the physical, artistic, cultural, social and political history and development of the City of Yarra. Artworks will be acquired and managed in accordance with the City of Yarra Collection Management Policy. Art works may be referred to the Yarra Art and Heritage Collection according to the policy guidelines of that companion document.

8. Selection Criteria and guidelines

8.1 Responsibility

Staff responsible for management of the LHC will interpret and apply the selection guidelines for both purchases and gifts under guidance from library specialist staff.

8.2 Selection

The acquisition of material for the LHC must focus on the community and its needs. Collection of materials should be pro-active and retrospective. The following list, while not exhaustive, provides guidelines for the type of material that may be sought for inclusion:

8.2.1 Published histories or other works relevant to the area.

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- 8.2.2 Oral history recordings, videotapes, films etc. made about the Cities of Yarra and its residents.
- 8.2.3 Records of local businesses and associations, schools, churches, sporting bodies, welfare organisations etc.
- 8.2.4 Papers of prominent local families/individuals, genealogies and manuscripts.
- 8.2.5 Family history research papers and publications relevant to the City of Yarra.
- 8.2.6 Reference material such as bibliographies, directories and guides. This may include guides to holdings of relevant material in other manuscript or archival collections.
- 8.2.7 Community based publications such as newspapers, magazines, gazettes, newsletters, etc.
- 8.2.8 Documents/research papers/photographs relating to buildings (private, commercial and public) and property.
- 8.2.9 Research papers and theses pertaining to local areas produced at a tertiary education level.
- 8.2.10 Studies undertaken by private or government bodies referring to, or having impact on, the local communities.
- 8.2.11 Photographic works of local historical interest including prints, postcard, slides and negatives.
- 8.2.12 Artworks by local artists or depicting local scenes or personalities or as fitting the criteria for acquisition within the City of Yarra Collection Management Policy.
- 8.2.13 Materials that provide instruction relating to genealogy, local history, preservation, oral history and similar topics.
- 8.2.14 Maps and plans including Survey plans, Land subdivision plans, Architectural plans and Aerial photographs.
- 8.2.15 Community archives of selected local organisations.
- 8.2.16 Ephemera, including leaflets, flyers, pamphlets, original documents such as land deeds, certificates and military papers.
- 8.2.17 Local authors writing about the City of Yarra.
- 8.2.18 Periodicals about local history and genealogy of Melbourne and Victoria.
- 8.2.19 Digital material which is relevant to the local area or Melbourne
- 8.2.20 All other relevant local history materials that meet selection guidelines.
- 8.2.21 Materials that will support education on City of Yarra local history.

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- 8.3 Whenever possible, material should be acquired in duplicate for security and preservation reasons. The need for copies for lending should also be considered.

Any materials deemed by the Team Leader Digital and Community Learning to enhance the collections of the LHCs or to enlarge its scope in a coherent way may be added to the collection, regardless of whether or not it falls into a category listed above or below.
- 8.4 Acceptance of donations to the permanent collections shall be made on the following conditions:
 - 8.4.1. Consistency with Yarra Libraries' mission and the purpose of the LHC.
 - 8.4.2. Authority, authenticity and reliability.
 - 8.4.3. Educational, interpretive, research and/or exhibit value.
 - 8.4.4. Adequacy of documentation.
 - 8.4.5. Clear title available from the source of acquisition.
 - 8.4.6. Ability of the library to care properly for and use the acquisition.
 - 8.4.7 In selecting materials for purchase or accepting donations, the library shall be cognizant of the space and preservation constraints inherent in the local library, the research potential of the item, and the suitability of the material in Yarra Libraries comparative to other repositories.
 - 8.4.8 Gifts in the form of material for the LHCs are evaluated under the same criteria as purchased materials. The Team Leader Digital and Community Learning may decline to accept for the LHC any gift of material that seems inappropriate to the collection or that would be more appropriate in another repository. In such cases, the would-be donor should be informed of the scope of the LHC and, if possible, put in touch with an institution that might wish to accept the offered gift.
- 8.5 Yarra Libraries' justifications for not accepting gifts may include, but not necessarily be limited to the following:
 - 8.5.1 Outside the library's scope.
 - 8.5.2 Deteriorated or lacking in physical integrity unless unique.
 - 8.5.3 Cannot be preserved.
 - 8.5.4 an inauthentic or duplicate document
 - 8.5.5 Limited relevance and research potential

The limited shelving space in the LHC is a consideration in adding material to the collection. The Team Leader Digital and Community Learning may find it necessary to decline a large gift of material of a kind already included in the collection should there be insufficient shelving or space to accommodate it.
- 8.6 Donors of material to the LHC will be routinely asked to provide written documentation/information accompanying the gift to the library and giving the Library the right to dispose of the gift as they see fit. Gifts of collections

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accepted by library specialist staff need not be retained as a whole, but may be divided, with some items put into the LHC, and remaining items donated-on or sold.

All primary source materials donated should be accompanied by identifying documentation together with any other relevant descriptive documentation including provenance and the donor's right to dispose of the material.

Artworks donated to the collection will be required to meet criteria outlined in The City of Yarra Collection Management Policy.

- 8.7 In instances where artefacts are offered, but not acquired due to inability to store or maintain, efforts should be made to record items as much as possible through photographs, measurement and description.

Selection Tools

These will include:

- Professional journals
- Online databases
- External professional advice
- Bibliographies

9. Format

While Yarra Libraries aims to cover a range of formats, exceptionally large items may be considered suitable for storage in the Council's Art and Heritage collection. All materials considered must be directly relevant to the collection parameters and we would need to always consider if storage space is sufficient.

It is preferable to include material in digital format subject to copyright reproduction and budget constraints. This includes the digitization of materials already held by the Yarra Libraries in order to maximise access to the collection. Opportunities to apply for grants to enable this will be pursued.

10. Weeding

Weeding is the term used to describe the process by which an item is withdrawn from the collection. Duplicate, irrelevant or damaged materials will be withdrawn by the Team Leader Digital and Community Learning with input from the specialist staff. We will consult with the relevant Historical Societies regarding relevance of content where required.

Items will remain in the collections as long as they retain their physical integrity, identity, and authenticity, and as long as they remain relevant and useful to the purposes and activities of Yarra Libraries. Withdrawals of materials will be considered when these conditions no longer prevail or if the Library should receive similar items in better condition. In all instances we will offer donations of any weeded material to other organizations which have an

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interest and role in preserving historical information. Space is limited in our libraries. While not the sole determining factor, it may be a consideration in deciding to keep or withdraw material. The Yarra Libraries Local History collection is exempt from standard weeding processes.

11. References

- Yarra Libraries Collection Development Policy. Yarra City Council 2015.
<http://www.yarracity.vic.gov.au/ignitionsuite/uploads/docs/Yarra%20Libraries%20-%20Collection%20Development%20Policy%202015.pdf>
- Information Literacy for all Australians
http://www.alia.org.au/policies/information_literacy.html
Adopted 2001. Amended 2003, 2006.
- ALIA Statement on free access to information
<http://alia.org.au/policies/free.access.html>
Adopted 2001. Amended 2007, 2015.
- Lunn Veronica 2006, Content and Access in Victorian Public Libraries: A Strategic Framework 2006–2010. Library Board of Victoria, Melbourne
- IFLA/UNESCO Public Library Manifesto.1994.
<http://www.ifla.org/VII/s8/unesco/eng.htm>
- Yarra Council Arts and Cultural Strategy 2016–20
<http://yarracityarts.com.au/arts-and-cultural-strategy-2011-2015/>

11.6 National Disability Insurance Scheme - Public Forum Options

Trim Record Number: D17/28936

Responsible Officer: Director Community Wellbeing

Purpose

1. To provide advice to Council on the Disability Advisory Committee (DAC) Motion for Council to convene a public forum on the NDIS implementation in the North East Melbourne Area (NEMA).

Background

2. Throughout the NDIS transition phase so far, Council has consulted frequently with its Disability Advisory Committee (DAC) on all aspects of the roll-out. At the DAC meeting on 29 November 2016, the Committee passed a Motion recommending that Council considers convening a public forum with the five NEMA councils, their DACs and the Brotherhood of St Laurence (BSL), to discuss the implementation of the NDIS and potential gaps in services, and in access to advocacy, in the NEMA region.
3. As a result of a Delegate's report presented on 20 December 2016 by the DAC Chair, Council requested an Officer's report to provide advice on DAC's recommendation, outlining:
 - (a) whether there is interest from other DACs and NEMA councils in attending or collaborating on such a forum; and,
 - (b) whether the forum could be delivered within the budgeted resources for FY16/17.
4. This report provides advice on the feasibility of convening a public forum with DACs and councils in the NEMA region.

External Consultation

5. Yarra DAC's initiative and Council Resolution were communicated informally to NEMA councils with a request:
 - (a) to identify whether their DACs would be interested in collaborative work towards a public forum, as a form of peer-empowerment and enablement of local communities to progress social change influenced by NDIS;
 - (b) whether they would provide resources in support of their DACs, contribute funds to engage an independent project manager to coordinate and facilitate DACs' works, and provide in-kind assistance as required (venue, promotion, administrative tasks, etc.); and
 - (c) to indicate a preferred option out of four possible options (see Options section of this report), for the ownership of the project and an approach to the development of the forum.
6. In addition, contact was made with the Victorian Disability Advisory Council (VDAC). VDAC provides advice to the Minister for Housing, Disability and Ageing, Martin Foley MP on strategies to increase the participation of people with disability in the Victorian community. Members are made up of people with disability and carers of people with disability. VDAC works with other community and government advisory groups to create opportunities for all Victorians.
7. Responses are included in the Options section of this report.

Internal Consultation (One Yarra)

8. Consultation has occurred within the Aged and Disability Access & Inclusion team at this point. Further consultation would occur, subject to the outcome of this report.

Financial Implications

9. With regard to Council's request for advice on whether the forum could be delivered within the budgeted resources for 2016-2017 financial year, it is noted that the current program resources are committed to development of Council's next Access & Inclusion Plan.
10. Assuming the forum included limited catering, transport subsidies, use of a council venue during office hours, and with an estimated attendance of 200 to 300 people, these costs would be around \$5,000. The catchment for a forum extends from Whittlesea to Yarra and so consideration of issues such as the venue, timing and transport needs to be thoroughly considered.
11. The more significant resourcing issue is the Officer time to coordinate and plan the forum. This would include extensive liaison with the other councils and DACs, pre-planning to clarify and develop shared objectives and the focus of the forum, marketing and promotion, organising appropriate facilitation, bookings, organising the program and post event follow up. To ensure such an event was professional, effective and had a constructive focus, it would require an independent project manager / facilitator to focus on planning and organisation across a 3 to 4 month period. It is estimated an Officer working part-time over this period would cost around \$8,000 to \$11,000.
12. Council staff would also need to support the process and this would mean that other planned work would need to be placed on hold. While the resourcing may vary across the Options, Yarra's contribution would still remain high if it were the lead organisation.
13. From a resourcing perspective, the preferred option is to obtain resources from another lead agency and / or alternatively gain the commitment of the NEMA councils to contribute funds and engage a project manager.

Economic Implications

14. There are no economic implications associated with this report.

Sustainability Implications

15. There are no sustainability implications associated with this report.

Social Implications

16. A well planned public forum has the potential to provide a vehicle for people with disability and their local advisory groups (DACs) to reflect on the first 6 to 12 months of the NDIS in NEMA and identify what is working well, what may need improvement and what gaps exist in the new environment. This would empower people to have their say and be an important source of information for DACs and councils as they continue to advocate on behalf of the community. The timing of the forum may require consideration as 2016-2017 is a 'phasing in' year, meaning that people eligible for the NDIS will transition across the whole year.

Human Rights Implications

17. The NDIS represents the application of human rights as enshrined in the United Nations Convention on the Human Rights of Persons with Disabilities (2006) such as Article 10 – the right to live independently and be included in the community.
18. Council has a strong commitment to supporting older people and people with disability, and therefore to observe and promote their human rights. This occurs through roles of advocacy, engaging residents in decision-making, through service provision and building an inclusive community.
19. Supporting and encouraging people with disability and carers to participate in forums is consistent with human rights and is further underpinned by Council's Access & Inclusion Plan.

Communications with CALD Communities Implications

20. Communications with people from CALD background and Aboriginal background would need to be factored into the planning of the forum to ensure it is accessible and reflects the diversity of the NEMA region.

Council Plan, Strategy and Policy Implications

21. The Council Plan 2013 – 2017 notes that it operates under the Local Government Act with a primary objective to “endeavour to achieve the best outcomes for the local community having regard to the long term and cumulative effects of decisions”. The relevant strategic objectives are: Strategic Objective 1 (Celebrating Yarra’s uniqueness) and Strategic Objective 2 (Supporting Yarra’s community).
22. Through the MetroAccess program and the Access & Inclusion program, Council has two dedicated positions to support advocacy, resource the Disability Advisory Committee, oversee implementation of the Access and Inclusion Plan and undertake community capacity building.

Legal Implications

23. There are no immediate legal implications associated with this report.

Other Issues

24. Nil

Options

25. The following Options have been canvassed with NEMA councils at Officer level:-

(a) **Option 1** – DACs’ ownership; councils’ funding.

This option proposes DACs to take ownership and run the forum. Substantial level of coordination would be expected from Yarra City Council and this would require an internal resource / role allocated for this project.

All councils involved would be expected to contribute some resources and funding, including in-kind administrative assistance and a financial contribution towards resourcing an independent project manager.

DACs would be able to affect local influence, detect potential gaps, and draw on councils’ strengths in responding to needs of the community.

(b) **Option 2** – DACs and VDAC collaboration.

NEMA DACs could collaborate with VDAC in leading the development of the forum. VDAC shares common goals with councils’ DACs, and has historically facilitated such initiatives with DACs.

A meeting was held with the VDAC Chair in early March 2017. Paragraph 29 of this report outlines VDAC’s position. In summary, under this option, VDAC would collaborate with councils to support a forum, however it does not have dedicated or discretionary funding available. The Chair of VDAC expressed a strong interest in exploring funding options through other mean. Under this option, Councils would most likely be required to provide cash and in-kind support.

(c) **Option 3** – Yarra Council to facilitate and resource

Yarra City Council would take responsibility for convening a forum with guidance from Yarra’s DAC, and facilitate collaboration of Yarra’s DAC with all NEMA DACs in consultation with their councils.

- (d) **Option 4** – Council advocates with other agencies to facilitate.
The key organisations involved in the roll-out of NDIS, including National Disability Insurance Authority (NDIA), Department of Health & Human Services (DHHS) and BSL, could be requested to conduct a public forum with NEMA DACs, in collaboration with NEMA councils. These leading organisations would benefit by gathering evidence directly from people with disability, in an enabling and empowering manner, as per priorities listed in the ILC (Information, Linkages and Community Building) framework. Part of the advocacy would include requesting that DAC's play a pivotal role in shaping the forum.

Summary of NEMA councils' responses – Officer level feedback

DAC Participation

26. Only one of the NEMA councils was able to confirm that their DAC is very interested in collaborating with other NEMA DACs and working towards a public forum, at this point. The other three councils are either in the midst of reforming their DAC or were not in a position to respond until the proposal was discussed at DAC meetings. One council operates a broad based disability network forum and does not have a DAC.

Resourcing

27. Most NEMA councils are willing to contribute some resources, either in kind or direct funding. The point was made that the proposal requires a preliminary planning session to clarify the type of forum NEMA DACs want, before committing to resource allocation and to involve other potential partners (i.e. VDAC) in the process.

Options

28. There was a mixed response to the question of auspicing and leadership of the forum. Three councils favoured Option 4, which proposes other agencies (i.e. DHHS, NDIA, BSL) to facilitate the project. One council nominated also Option 1 which requires NEMA councils to facilitate. There was support from one council for Option 3 - where Yarra City Council takes responsibility for convening the project under Yarra DAC's guidance.

Response from VDAC

29. Council Officers met with the Manager of the Office for Disability and Chair VDAC in early March to discuss their interest and potential support of the forum. Key points noted from this discussion was that:-
- (a) formal support would need to be endorsed by the full Committee which meets on 20 April 2017;
 - (b) the VDAC representatives offered in-principle support to collaborating with NEMA councils however indicated VDAC's participation needs to be seen in context of having a state-wide role. Councils have a local advocacy role; VDAC's role is to advise the Minister;
 - (c) VDAC does not hold a budget for such events, however are open to work with councils to explore funding options through other channels; and
 - (d) DHHS fund agencies under the Victorian Disability Advocacy Program for the purpose of empowering and enabling active civic participation and it would be useful to engage with this program also.
30. The Chair of VDAC offered to discuss the proposal at the next VDAC meeting and the Office for Disability undertook to discuss options with representatives from the DHHS – NDIS Unit.

Conclusion

31. In response to Council's request for advice as to whether there is interest from other DACs and NEMA councils in attending or collaborating on a public forum, preliminary communication with the relevant councils indicates that:

- (a) it is not possible at this stage to determine interest of all NEMA DACs in collaborating on a public forum. One council has expressed strong support and interest in this initiative. Three other councils could not confirm a position until it is discussed at their DACs and within the organisation;
 - (b) there was general support for the notion of a forum, however, it was suggested that further pre-planning was required to be clear on the agreed purpose and objectives of a forum;
 - (c) all NEMA councils expressed willingness to support their DACs when members' interest is confirmed; and collaborate on the project. Two councils were positive about contributing adequate resources. One council felt limited to in-kind support, and another council, although supportive of the initiative, could not commit resources at this point due to other priority commitments, such as DAP (Disability Action Plan) development, and the uncertainty around the future of the MetroAccess program; and
 - (d) based on the responses from councils, officers expressed in principle support for the project, as they were not in a position to make decisions, until a formal project proposal is delivered and approved within their organisation.
32. With regard to Council's request for advice as to whether the forum could be delivered within the budgeted resources for 16/17 financial year, it has been assessed that there are no resources available in the current budget to deliver on Options 1, 2 and 3, where Yarra would be expected to lead the project and commit resources.
 33. VDAC, through its Chair, has confirmed in-principle interest in Option 2, however not as a lead agency and advised VDAC is not in a position to commit funds. As to Option 4, the key stakeholders (NDIA, DHHS and BSL) have not been approached at this stage. Regardless which Option is undertaken, NEMA councils would be able to contribute in-kind support such as promotion via websites and digital media, provision of venue, etc.
 34. The majority of councils suggested Option 4, which is similar to Option 2 in terms of the expectation that other organisations take the lead role in collaborating with NEMA DACs, and resource the project.
 35. In summary, the preliminary research has demonstrated that conducting a regional forum is a significant undertaking and that further work is required to establish a clear project brief, inclusive of agreed objectives and to then obtain formal endorsement from other agencies, including the availability of funding and in-kind support. It is not recommended that Council take the lead role in facilitating a regional forum as, in addition to the resourcing issue, the success of such a forum, requires the commitment, engagement and participation of key stakeholders and DAC's.
 36. The potential role for Yarra Council in facilitating or collaborating to deliver a forum must be considered in the context of all the other work being required of the Aged and Disability Services Branch in managing the transition to NDIS and Aged Care reforms more generally.
 37. As a way forward, Council may wish to write to the NEMA DAC's and councils and lead agencies (NDIA, DHHS, BSL) to propose a forum, to seek their interest in partnering with council (including provision of resources), and subject to responses, offer to host a project initiation meeting. This process will extend the timing and planning period however is necessary to establish any meaningful forum. A forum in the second half of 2017 or early 2018 also has the advantage of following the completion of the NDIS phasing-in period, and would see participants in a better position to provide informed feedback and potentially allow for a more informed conversation.

RECOMMENDATION

1. That Council provides in-principle support to the Yarra Disability Advisory Committee's proposal to hold a public forum to discuss the implementation of the National Disability Insurance Scheme, with a focus on the experience of people with disability, carers and families.
2. That Council notes that Officers recommend Option 4 (that Council advocates for other funded agencies such as DHHS, BSL, NDIA and VDAC to facilitate the forum), in collaboration with North East Metropolitan Area (NEMA) councils, as the preferred way forward.
3. That Officers write to DAC's and councils in the NEMA, and key stakeholders including DHHS, BSL, NDIA and VDAC to seek their formal response to Councils request for them to assist in facilitating a regional public forum, including their willingness to perform the facilitation role, provide resources, and their interest in participating in the planning and delivery of a forum.
4. That, subject to the response received from NEMA councils and key stakeholders outlined above, the Chief Executive Officer is authorised to determine Council's level of participation, if any, in planning for and holding a regional public forum.

CONTACT OFFICER: Adrian Murphy
TITLE: Manager Aged and Disability Services
TEL: 9205 5450

Attachments

There are no attachments for this report.

11.7 Edinburgh Gardens WWI Memorial Future Options

Trim Record Number: D17/25220

Responsible Officer: Director Community Wellbeing

Purpose

1. To provide Council with a report on future options to improve the prominence of the WW1 Sportsman's Memorial following a stakeholder consultation about relocation options within Edinburgh Gardens and further investigation into the financial implications of any such works.

Background

2. The Sportsman's War Memorial (Memorial) was erected in memory of members of the local sporting clubs who died in the First World War. The Memorial was unveiled on 9 December 1919 and appears to have been funded solely through public subscription amongst members of the various sporting clubs.
3. The Memorial is managed as a part of the Yarra Art and Heritage Collection.
4. Council has investigated the conservation and functionality of the Memorial over the last 17 years; the current management of the Memorial is guided by documents such as the Best Practice Forum (2000); Edinburgh Gardens Master Plan (2004); and Edinburgh Gardens Conservation Management Plan (2004).
5. Council received a petition in July 2015, lead petitioner Robert Soutter, requesting Council to consider a relocation and a full restoration of the WW1 Sportsman's Memorial in Edinburgh Gardens. Council considered a report in September 2015 outlining options for further investigation; these have now been concluded.
6. The Memorial is fabricated in concrete and consists of a series of Tuscan columns with an open canopy created by beams and rafters. There are a number of ornamentations and decorative features.
7. According to the Fitzroy Cricket Club in 1919 "...a combined committee decided, in order to memorialize and honour the heroism of those Members who fell at the front, to erect a cenotaph in the shape of a Pergola on the pathway leading to the entrance of the Member's Reserve. The work was designed by Mr Edward Twentyman, and carried out in cement concrete, under his supervision."
8. The Memorial is architecturally significant as being the only example of this type of memorial in Victoria. Structurally the Memorial is unique with the concrete cast in one section.
9. The Memorial was designed as a canopied passage or arbour, and as a grand entrance way to the sporting grounds. In general, the purpose of war memorials required them to be inspiring, conspicuous, enduring and able to serve as a focal point for Anzac and Remembrance Day ceremonies.
10. There have been a number of changes since the erection of the Memorial that has had an impact of its original intent and prominence.
11. When the Memorial was constructed the VFL football ground was fully enclosed by fencing. In the decades since the oval was last used for this purpose, the environment and context has been altered as the land has been transitioned into a publicly accessible park.
12. The Fitzroy Bowling Club was granted permission to expand in the 1960s and 1990s and its buildings now immediately abuts the Memorial to the north, significantly encroaching on the monument. Water tanks have been added in recent years and the passage operates as delivery area for the Bowling Club and often delivery vehicles are parked and empty crates are stored.
13. The former City of Fitzroy gave permission for an electrical substation to be built on the site in the 1980s. It blocks the Memorial at one end and therefore its functionality as a gateway into the grounds has been unarguably compromised.

14. The substation is owned by Citipower and serves power to the adjacent sporting facilities in the park.
15. Both of the surrounding buildings have a detrimental impact on the structural and physical appearance of the Memorial.
16. In 2002, as determined by the Best Practice Forum and the 2004 *Conservation Management Plan* for the site, the Memorial was cleared of trees, bushes and vines which had grown around and over the structure and reduced its visibility and eroded its condition.
17. Council has received two grants (in 2005 and 2010 respectively) from the Federal Government through the Restoring Community War Memorials Grants Program towards improving this Memorial. Works have been undertaken to clean the monument, restore the decorative urns and stabilize the concrete through acrylic render.
18. Council commissioned a new structural assessment of the Memorial in August 2015. The primary purpose of the assessment was to re-evaluate the condition of the Memorial structure and to assess whether the structure might survive a move.
19. The report indicates the Memorial to be generally in a good condition except for the western beam; this is the one that directly abuts the substation.
20. The western beam has significant carbonation and as a result has significant corrosion of its bottom reinforcement. Its location against the substation structure means it is currently stable. However, it is likely that the reconstruction of the entire beam will be required as part of any significant works to relocate the Memorial or remove the electrical substation.
21. Over the last 20 years public concern has been raised on a number of occasions (in 2000, 2005, and 2014-15) with Council directly and in popular press about the condition and lack of respect given to the Memorial.
22. Council received a petition of in excess of 1100 signatures in July 2015 (lead petitioner Robert Soutter), requesting Council to consider a relocation and a full restoration of the WW1 Sportsman's Memorial in Edinburgh Gardens.
23. A site in front of the tennis courts with a small pedestrian path has been identified as a possible new location for the Memorial, which is around 200 metres away from the existing (original) site.
24. Officers have been unable to identify an ideal location that would enable the Memorial to be used as per its original intent, as a canopied passageway and formal entrance into the main gardens and sporting grounds while being visible from the main streetscape.

External Consultation

25. In November 2015, Arts & Cultural Services convened a meeting facilitated by heritage consultant Ray Tonkin with identified stakeholders including RSL and sporting club representatives, Ward councillors, academics with expertise in WWI history and architecture, local history members and relevant Council staff from Open Space and Strategic Planning teams.
26. The intention of the meeting was to discuss future options for the Memorial, specifically related to its location.
27. A briefing document was prepared by Council officers and made available to all participants. This document was designed to brief stakeholders on relevant factors to consider in their deliberations, in particular the logistical, financial and heritage implications of options discussed.
28. There was a general consensus across this group of stakeholders that the most practical approach would be to pursue the removal and relocation of the Citipower substation on the assumption that it would cost considerably less than relocating the Memorial; additionally this approach would have less heritage implications. This was prior to understanding the cost of this approach.

29. Members of the Combined Tramways/East Melbourne RSL, Robert Soutter and Paul McCrohan, however, have expressed a view that in its current location the Memorial will always be compromised (even if the substation is removed) due to its proximity to the bowling club building.
30. The investigation of further interpretive and re-landscaping opportunities to improve the access and appearance of the Memorial also broadly supported by the attendees; particularly from the RSL and Fitzroy Football Club.
31. The group involved in the consultation also agreed that further historic research and restoration opportunities should be explored. The Fitzroy History Society have been undertaking research into the soldiers' names that were inscribed on the original bronze plaques (five of six which are lost).
32. That is now complete and the members have now a list of all the names of the soldiers, along with a photograph and a short biography of each. The plaques can now be replicated and the material be used for creating a new document for public dissemination.
33. In the light of the new information regarding the condition of the memorial and the cost implications for the relocation, feedback has been sought again from the members who participated in the facilitated meeting in November 2015. Some have expressed deep disappointment of the prospect that the Memorial or the substation cannot be relocated. Others understand that it is a significant impost on ratepayers' funds.

Internal Consultation (One Yarra)

34. Relevant stakeholders have been consulted throughout the process including staff from Open Space, Strategic Planning, Building Projects and Engineering Services.

Financial Implications

35. Council allocated \$50,000 in this current financial year to enable condition reports and any identified urgent works to stabilise the Memorial. \$5,118 has been spent so far on the surveyors report. This leaves \$44,882 to be directed towards the options identified.
36. Council has already spent an additional \$2,800 last financial year for a feasibility assessment by an electrical engineer of options for relocating the substation, which included negotiation and scoping with Citipower in preparation for a surveyors' detailed report.
37. It is possible that Citipower may instigate replacement of the substation in the future as technology improves or service requirements change; however there is no certainty and this is only speculation.
38. Council allocates \$37,000 to the maintenance of the entire Art & Heritage Collection (valued at over \$4m) per year and given the budget limitations, only urgent works are usually accommodated, with programmed maintenance conducted on a staged basis.
39. The Collection maintenance budget is unable to cover the major works required, however it is currently contributing to the replication of the porcelain wreath to be reinstated on the monument and the custom made display box at a cost of approximately \$9,700.
40. A quote solely for remedial work to the western beam of the monument has not been sought given it seems sufficiently stable as it is currently supported by the adjacent substation.
41. The Victorian Government offers grants of up to \$20,000 available through the Restoring Community War Memorials and Avenues of Honour Grants Program; this is a rolling deadline program.
42. The Federal Government offers grants through the Department of Veteran's Affairs called Major Commemorative Grants, however the latest round closed on 3 January 2016 and it is uncertain if further rounds of this grants program will be offered. Through this program, grants are available in excess of \$4,000. However, it is unclear whether this project would meet the Major Commemorative Grants guidelines, as it supports "the restoration of existing national/state/territory or otherwise significant war memorials".

43. It is anticipated that any available grants will be sufficient to fund the replacements of the Memorial's ornamentations (replicas of the shields and the ceramic wreath), but not for major works such as a relocation of the Memorial or the substation.
44. The relocation of the substation or Memorial would require an allocation from Council's capital works budget. There is no forward allocation for this project in the budget at this stage.

Economic Implications

45. The economic implications are not considered in this report.

Sustainability Implications

46. There are no significant sustainability implications related to this report.

Social Implications

47. The Memorial is of great social importance to many people. It was built in the memory of those who lost their lives in WW1 and it was designed so that the memory of this remains living in the community.
48. Since being built in 1919, Anzac Day ceremonies have been documented as having taken place at the site of the Memorial and Officers have been advised local groups continue to use the Memorial for this purpose.

Human Rights Implications

49. There are no human rights implications related to this report.

Communications with CALD Communities Implications

50. There has been no specific targeted approach for CALD communities in relation to the consultation conducted in November 2015.

Council Plan, Strategy and Policy Implications

Edinburgh Gardens Conservation Management Plan

51. In October 2002, Council commissioned Allom Lovell and Associates to prepare a Master Plan and Conservation Management Plan for Edinburgh Gardens. Extensive community consultation was conducted in the formulation of both these documents which still serves as an important reference for Council in the ongoing management of the Gardens.
52. The Conservation Management Plan did not recommend a relocation of the Memorial. Rather it suggests the conservation of the Memorial and reinstatement of the original decorative elements - or reconstruction if they cannot be found. It suggested the dignity of the Memorial should be restored by upgrading the landscape setting, including removal of the adjacent substation to a less visually intrusive location at the rear of the grandstand, reinstatement of the path entrance under the structure and replanting.
53. The Master Plan was finalised in 2004 but was not formally adopted by Council.

City of Yarra Collection Management Policy

54. The Memorial structure is managed by Arts and Cultural Services, as one of the items in Council's Art and Heritage Collection. The Collection consists of over 800 items valued at \$4.5m.

Legal Implications

55. There are no legal implications related to this report.

Other Issues

56. The Memorial is not singularly recognised in the Victorian Heritage Register. The grandstand is the only part of the Edinburgh Gardens on the Victorian Heritage Register.

Options

57. Upon review of the public concern, broader situation and heritage advice, some options for council to consider are as follows:
- (a) reinstatement of the functionality of the walkway through the Memorial by undertaking a relocation of the substation (thus retaining the Memorial in its original setting); or
 - (b) to relocate the Memorial to the pedestrian only path west of the tennis club, creating a new walkway under the Memorial and ensuring the Memorial is adjacent to the relevant sporting clubs near the tennis courts, the oval and the bowling club; or
 - (c) fund enhancement works and explore other options for future funding. The enhancement works would attempt to address access and prominence of the Memorial such as re-landscaping including investigating links to the main pathway for pedestrians and more suitable plantings; an interpretive project to provide detailed background information and context about the Memorial to park users and the replacement of the missing shields with the names of the fallen sporting club members/soldiers.

Option A

- 58. To relocate the existing electrical substation so the Memorial can be retained in its original position and its intended function is restored (as much as possible), as an entry pathway into the park.
- 59. In the Conservation Management Plan for the Edinburgh Gardens by Allom Lovell (2004), conservation architects produced a report on the conservation aspects of the built environment in the park including the Sportsmen's War Memorial.
- 60. Noting the location of the substation and the bowling club, Allom Lovell commented
- 61. "Today it is hard to understand what dictated these locations (of the bowling club and substation). Now unfortunately, it is neither feasible, nor likely, that the Bowling Club would be reconfigured in the foreseeable future. Therefore, it is recommended that the substation be relocated to an alternative, less intrusive location such as the rear of the grandstand or elsewhere in proximity".
- 62. This option was further supported by a number of stakeholders during the 2014-15 forums including RSL representatives, historians and representatives from the Fitzroy Historical Society.
- 63. A quantity surveyor, experienced in heritage works was engaged to prepare a summary cost for this option which includes the quote from Citipower for the relocation of the substation to an area some 10 -15 meters from the current location.
- 64. The total cost for the relocation of the substation, the repairs to the Memorial (treatment or replacement of its degraded western beam) and the additional electrical upgrades and limited landscaping works is estimated to cost at least \$510,000 ex GST.
- 65. Council should further note that Citipower would also require a new easement created for a new substation and the relocated services. Council would be required to undertake and pay for these legal processes. The budget has been included within the estimate. The timeframes for this process to be completed would be approximately 12 months and require the consent of the State Government as this is Crown Land.
- 66. The positive considerations for Option A are the Memorial remains in its original location, the ability to use the Memorial as a walkway is reinstated, and a partial solution to the architectural interference presented by the substation and bowling club is achieved.
- 67. The negative considerations for Option A are that once the substation has been relocated the prominence of the structure would still be affected by the close proximity of the bowling club building, and the poor aspect of the pedestrian/access path from Brunswick Street. This area often has vehicles parked for the bowling club and water tanks have been installed which contribute to the sense of the Memorial being undervalued for its cultural and heritage links.

68. At \$510,000 ex GST, this option is estimated to be the most expensive option.

Option B

69. To relocate the Memorial to the pedestrian-only path adjacent to the tennis courts and to the pavilion and grandstand.
70. As part of an earlier best practice forum convened by Council regarding the Memorial, the National Trust submitted a statement in August 2000. In part it noted:
- “The structure should not only be repaired to a stable condition but its original setting/function should also be restored. The easiest solution is to demolish the small structure and re-align the path to run through the pergola again. However, being butted up against a two story wall is not an attractive location for the pergola or the path. The Burra Charter and heritage practice do not normally support the relocation of heritage places, since this removes them from their original context and often therefore diminishes our understanding of them, and their historical significance. In this case however, relocation to a nearby point that would make it again a feature of a main entrance to the grounds, reinstating its prominence, and preserving its close relationship with the sporting facilities in the park may in fact enhance its original purpose and significance. Simply relocating the structure a few meters to the south would achieve these aims. Relocation within the park or another place altogether would be more problematic, since the relationship to the sporting facilities may be lost.”*
71. With any consideration of an appropriate new position for the Memorial, it is necessary to seek to maintain the relationship of the Memorial to the sporting facilities, its original location and its function as an entranceway.
72. One possible option has been to consider moving the Memorial over the recently installed pedestrian pathway from Brunswick Street. This was discounted as this provides a service road for large maintenance vehicles across the park (providing principally landscaping and lighting maintenance).
73. After discussions with the Open Space team, it was determined that the path to the west of the tennis courts which runs towards the pavilion and grandstand would be the optimal site for the Memorial given the relationship requirements and site restrictions.
74. The positive considerations for this option include the proximity to the sporting venues being maintained; the prominence of the structure within the gardens will be re-established; it allows the Memorial to have a pathway through it as per the original design; the openness of the area allows for the revised landscaping of the trees and associate shrubs.
75. The quantity surveyor has estimated the cost for the relocation of the Memorial, the associated repairs and the further landscaping works and new lighting have been estimated to cost in the order of \$400,000.
76. This sum does not include the engagement of a conservator or heritage consultant to oversee the work which would be advisable. This would add additional cost.
77. This option will have a significant impact on the material fabric of the Memorial as it would require the saw-cutting of the monument into multiple sections for transport and reassembly at a new site; the weight of the monument prohibits movement as a total structure given the sizeable crane required could not be accommodated in the gardens.
78. The impact of a relocation on the Memorial monument would be substantial and would have significant implications on an important heritage asset managed by Council. While the National Trust above has indicated its relocation could be argued in the current situation, the proposal would need further planning and community consultation.
79. It should also be noted that the stakeholder forum held in 2015 also did not advocate for this option, but for the relocation of the electrical substation.

Option C

80. As a more cost effective, and yet still respectful way of honouring the intent of the Edinburgh Gardens Memorial, Council could undertake well-planned enhancement works to improve appreciation, access and prominence of the Memorial.
81. The Open Space team have investigated some preliminary options to improve the landscaping around the Memorial including new plantings, surface upgrades and realignment of path leading to the monument from the main pathway.
82. While the existing rosemary hedges frame and define the Memorial from the background of the bowling club and seem to strengthen its presence, some further planting could be achieved with a modest budget.
83. Further garden beds could be created at the entry to the Memorial which may include roses, a small tree and some ground cover planting to improve public perception and its appearance. It is also proposed that some further small hoop fencing could improve the appearance and ongoing growth of the rosemary bushes/hedges.
84. While Officers considered the ability to install a path to the east of the Memorial, this is considered problematic due to the existing trees in the ground and the height of the concrete edge where it would meet the asphalt paving. On this basis, realigning the pathway through the Memorial is not recommended due to the risks to the health of the elm trees.
85. In addition to reworking the plantings, the grey asphalt surface under the Memorial could also be replaced with exposed coloured aggregate concrete to again redefine the space and create more contrast with the various structures.
86. It is estimated this would cost around \$10,000 for the landscaping works outlined above.
87. An interpretive project could also be part of the minor enhancement works, which could be in the form of additional signage and/or panels installed on adjacent walls or as freestanding elements to inform visitors and park patrons of the significance and history of the Memorial.
88. A project like this would require historical research, compiling of imagery and primary sources into interpretive content and also design development and fabrication/installation.
89. A comparable project would be the Fitzroy Town Hall Reading Room interpretive project which comprised of seven light boxes installed in an internal corridor near the venue, led by a cultural consultant. This cost of such an installation is estimated to be in the order of \$30,000 to \$35,000 noting the potential for variations depending on scale and materials selected.
90. Another restoration option would be the reproduction of the missing bronze shields which were originally located on the columns of the Memorial and which listed the names of the fallen soldiers from the sporting clubs.
91. This is dependent on research being conducted by the Fitzroy Historical Society who have identified 33 of 38 or 39 names believed to have been on the shields (with one shield in storage with six names listed). The Fitzroy Historical Society have been undertaking research on a pro bono basis since the end of 2015.
92. The shields can be reproduced as all of the names have now been identified, however unfortunately there are no known photographs of the five missing shields for reference. Nevertheless, the shields could be reproduced in the style of the existing shield in storage and installed on the six columns.
93. An exact cost to complete these reproduction of the shields is not known, but it is expected it could be around \$5,000 for the production and engraving of replica bronze shields.
94. Some restoration enhancements are already underway and this includes the production of a replica of the painted porcelain wreath by the Centre for the Conservation of Cultural Materials. This is expected to be completed in March 2017 and will cost around \$9,000 once it is installed.

Potential for external funding

95. Council could continue to explore other avenues of external funding to assist with either Option A and Option B. This could include nominating the Memorial to the Victorian Heritage Register which would open up other avenues for external funding. Officers understand this may be a difficult and lengthy process and it may not be successful.
96. There are some war memorials that are individually recognized such as: the Boer War Monument in Melbourne; the Kew War Memorial in Kew; and the South African War Memorial in St Kilda. Being an individually recognized memorial enables Council to apply for funding through the Living Heritage Program with grants of up to \$200,000 available.
97. There are a number of other State and Federal Government grants available, but none of the others appear to offer significant sums required for the relocation of the Memorial structure or the substation.

Conclusion

98. Since 2000 Council has made a concerted effort to restore the Memorial but the buildings that have been built around the Memorial limits its original intent as an entrance to the gardens.
99. The discussion about the relocation of the Memorial has been canvassed at the Best Practice Forum was held on 5 September 2000 with experts and the relocation of the Memorial was not supported.
100. In the subsequent years, a number of restoration actions have been undertaken to improve the Memorial; \$50,000 in the current budget represents the largest allocation.
101. Council is now in the position to plan a strategy for the future management of the Memorial, mindful of the obligation to prudently manage our cultural heritage and of the ongoing public awareness and concern for this important heritage structure.
102. Whilst the aspirations of the original location were to create a principal entrance of note and align the Memorial with the sporting facilities, a series of poor decisions has undermined the integrity of the monument.
103. The relocation options are not straightforward. Relocation of the substation does not revert the site to its original condition when the Memorial was built – the bowling club buildings will still abut the Memorial. Relocation of the Memorial would include the loss of the original conditions in terms of location and materiality.
104. Furthermore, the costs identified to achieve relocation of the substation or the Memorial are significant and there are limited funding options to enable the works to proceed.
105. Restoring the Memorial to the best possible condition, along with improving the profile of the Memorial via landscaping as well as the proposed interpretive project, has the capacity to raise greater community awareness of the sacrifices made by those who fought in WW1 and of the tribute paid to them by the sport clubs of the area in the erection of this Memorial.

RECOMMENDATION

1. That:
 - (a) Council allocate the remaining 2016/17 budget of \$44,882 towards landscaping works and ornamentation restorations related to the Edinburgh Gardens WWI Memorial and to work closely with local history groups to commission an interpretive project to improve public awareness of the Memorial;
 - (b) Officers commence work into nominating the Edinburgh Gardens WWI Memorial for inclusion into the Victorian Heritage Register; and
 - (c) Officers continue to investigate funding options to enable future works.

CONTACT OFFICER: **Siu Chan**
TITLE: **Unit Manager Arts, Culture and Venues**
TEL: **9205 5045**

Attachments

- 1** War Memorial Ed Gardens QS report 2016
- 2** Updated and final Briefing document - Memorial Stakeholder Meeting 19 November 2015
- 3** WWI Sportsmans War Memorial Edinburgh Gardens Structural Report - BHS Consultants - August 2015
- 4** Ray Tonkin Report on Memorial Consultation - Nov 2015 FINAL

Attachment 1 - War Memorial Ed Gardens QS report 2016



16071-CP1

17 November, 2016

City of Yarra
Level 2
31 Gleadell St
RICHMOND VIC 3121

Via email Ann.Limbrey@yarracity.vic.gov.au

Attention Ms. Ann Limbrey

Dear Ann,

Edinburgh Gardens War Memorial Improvements Cost Plan (2 Options)

As requested, we have prepared a Cost Plan for the two options as reported by the structural consultants on the above project.

Cost Summary

Options		
Option	Memorial Arbour Relocation	\$395,750 plus GST
Option	Memorial Arbour Restoration and Substation Relocation	\$510,000 plus GST

Please refer to the attached cost plan options for further information and the scope of works included.

All costs are reported exclusive of GST.

The cost plan is based on costs current at November 2016. No allowance has been made for cost escalation beyond this date.

The cost plan is based on the Structural Assessment Report as prepared by Beauchamp Hogg Spano Consultants Pty Ltd (August 2015) and is indicative of the possible order of cost for the options as per the consultants conclusions in the report.

Slattery Australia Pty Ltd Level 14, 160 Queen Street, Melbourne, Victoria 3000
Phone +61 3 9602 1313 **Email** melbourne@slattery.com.au **Website** www.slattery.com.au
Ref 16071-CP1 **Date** 17 November, 2016 **Page** 1 of 2

Attachment 1 - War Memorial Ed Gardens QS report 2016



Costs are based on the assumption that the project will be competitively tendered to a select list of at least four appropriate and interested builders. No allowances have been made for negotiated, staged or construction management forms of procurement.

The cost plan assumes that a structured cost planning / value engineering process will be followed throughout the design process and that the project will be appropriately documented prior to tender.

The cost plan options includes contingencies of 10 % for resolution of latent conditions that may occur on the project.

Please note that the cost plan specifically excludes any allowances for the following:

- Abnormal ground conditions (i.e. rock, ground water, filling, etc.)
- Site decontamination and remediation
- Cost escalation beyond November 2016
- Client overhead costs
- Client management / direct costs
- Goods and Services Tax

We trust the above and the enclosed meets with your requirements. However, should you require any clarifications or further information please do not hesitate to contact Des Lynch at our office.

Yours faithfully,
Slattery Australia Pty Ltd

A handwritten signature in blue ink, appearing to read "Sarah Slattery".

Sarah Slattery
Director and State Lead, VIC
dl.sl
encl.

Attachment 1 - War Memorial Ed Gardens QS report 2016

Memorial Arbour Proposed Works
Edinburgh Gardens, North Fitzroy
Memorial Arbour Relocation

10 November, 2016

Element/ Trade			Total (\$)
Preliminaries			75,000
Substructure			41,000
Columns			18,000
Arbour Beams and Pergola			38,750
Render and Painting			26,500
Fitments			9,000
Electrical Services			12,500
Demolition - Dismantling Memorial Components			85,500
Landscaping & Improvements			12,500
Total Building Cost (at November, 2016)			318,750
Contract Contingency	Item	10.0%	32,000
Cost Escalation (beyond November 2016)	Item		Excluded
Total Construction Cost (at November, 2016)			350,750
Consultants Fees (approx. 12%)	Item		45,000
Goods & Services Tax	Item		Excluded
Total End Cost (at November, 2016)			395,750

This estimate is based on the Structural Assessment Report of the World War 1 Memorial as prepared by Beauchamp Hogg Spang Consultants Pty Ltd -August 2015



Ref 16071 - Memorial Arbour Relocation Date 17/11/2016 Page 1 of 1

Attachment 1 - War Memorial Ed Gardens QS report 2016

Elemental Summary - Memorial Arbour Relocation

WW1 Memorial Arbour Proposed Works - Edinburgh Gardens. North Fitzroy
Cost Plan No. A

slattery

10 November, 2016

Description	Cost/m2	Total
Preliminaries		75,000
Substructure		41,000
Columns		18,000
Arbour Beams and Pergola		38,750
Render and Painting		26,500
Fitments		9,000
Electrical Services		12,500
Demolition - Dismantling Memorial Components		85,500
Landscaping & Improvements		12,500
Total for Memorial Arbour Relocation	0	318,750

Attachment 1 - War Memorial Ed Gardens QS report 2016

Detailed Cost Plan - Memorial Arbour Relocation

WW1 Memorial Arbour Proposed Works - Edinburgh Gardens. North Fitzroy
Cost Plan No. A



10 November, 2016

No.	Description	Unit	Quantity	Rate	Total
Preliminaries					
<u>Preliminaries/ Margins and Overheads</u>					
1	Builder's general preliminaries	Item			45,000
2	Margins/ Overheads	Item			30,000
Total for Preliminaries					75,000
Substructure					
<u>Site Preparation</u>					
3	Sawcut and take up sections of asphalt pavements and clear away from site	m2	60	15	900
4	Grade, trim and level existing ground surfaces to required levels	m2	90	15	1,350
<u>Bored Piers and Ground Slab</u>					
5	750 dia. reinforced concrete bored piers including drilling, concrete, reinforcement and associated items (No.6)	m	24	625	15,000
6	Reinforced concrete pier caps (No.6)	m3	3	850	2,550
7	Clear away excavation	m3	10	50	500
8	Sand base/ waterproof barrier under ground slab	m2	60	15	900
9	Coloured concrete 200 ground slab and edge thickenings	m2	60	180	10,800
10	Exposed aggregate finish to ground slab and seal	m2	60	150	9,000
Total for Substructure					41,000
Columns					
<u>Columns</u>					
11	Erect concrete columns as taken (approx. 3.0m high) including pedestal, column and capital with dowell fixing pedestal base to pile caps and sealing around base of pedestal with watertight joint and sealant	No	6	1,500	9,000
12	S.steel dowells up to 600 long to column/ base joint	No	12	25	300
13	Surface repair and preparation to columns prior to render and finish (No. 6)	m2	48	50	2,400
14	Temporary propping to columns prior to beam installation	No	6	300	1,800
15	Craneage for installation of columns	Item			4,500
Total for Columns					18,000
Arbour Beams and Pergola					
<u>Primary Beams, Pergola and associated Items</u>					
16	Erect east beam B1 as taken (5m long) including dowell and weather tight joint to column capital	Item			2,500

16071-CPA

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Attachment 1 - War Memorial Ed Gardens QS report 2016

Detailed Cost Plan - Memorial Arbour Relocation

WW1 Memorial Arbour Proposed Works - Edinburgh Gardens. North Fitzroy
Cost Plan No. A

slattery

10 November, 2016

No.	Description	Unit	Quantity	Rate	Total
17	S. steel dowells up to 450 long	No	8	20	160
18	Surface repair and preparation to beam prior to render and finish (No. 1)	m2	16	75	1,200
19	Replacement west beam B1 (5m long) cast to match existing B1 including mould, concrete, reinforcement, mouldings, cast in fixing inserts, associated items and erect to match east beam B1 with dowell fixing to column capitals	Item			7,320
20	S. steel dowells up to 450 long	No	8	20	160
21	Surface preparation for render and finish	m2	16	20	320
22	Primary beams B2 as taken (7.2m long nominal) including dowell and weather tight joints to column capital at mid span and ends dowell jointed to main beams B1	No	2	1,750	3,500
23	S steel dowells up to 450 long	No	16	20	320
24	Surface repair and preparation to beam prior to render and finish (No. 2)	m2	24	75	1,800
25	Replacement end sections to pergola beams up to 600 long including matching process to existing pergola beams to restore to original length (6.9m nominal), manufacture of matching section and dowell fixing to end of existing pergola beam	No	7	750	5,250
26	Erect pergola beams (6.9m long nominal) including dowell and weather tight joint to primary beams B2	No	7	600	4,200
27	S. steel dowells up to 300 long	No	28	15	420
28	Surface repair and preparation to pergola beam prior to render and finish (No. 7)	m2	45	80	3,600
29	Craneage for installation of beams	Item			8,000
Total for Arbour Beams and Pergola					38,750
Render and Painting					
<u>Render (Cement based render with approved waterproofing additives)</u>					
30	Two coat render to 300 dia. columns including pedestals and capitals (No.6)	m2	48	150	7,200
31	Two coat render to primary beams (No.4)	m2	56	150	8,400
32	Two coat render to pergola beams (No. 7)	m2	45	100	4,500
<u>Paint</u>					
33	Approved paint system to columns	m2	48	50	2,400
34	Approved paint system to beams	m2	100	40	4,000
Total for Render and Painting					26,500
Fitments					
<u>Fitments</u>					

Attachment 1 - War Memorial Ed Gardens QS report 2016

Detailed Cost Plan - Memorial Arbour Relocation

WW1 Memorial Arbour Proposed Works - Edinburgh Gardens. North fitzroy
Cost Plan No. A

slattery

10 November, 2016

No.	Description	Unit	Quantity	Rate	Total
35	Reinstate orb urns to match original	No	4	350	1,400
36	Refit marble plaque as per Conservation Management Plan	Item			350
37	Historical context sign - details of Memoriam, its original location and relocation	Item			5,000
38	Signage panel to replacement primary beam west B1 to restore to original state	Item			2,250
Total for Fitments					9,000
Electrical Services					
<u>Electrical Services / Light fittings</u>					
39	Feature light fittings and installation (No 5)	Item			4,000
40	Electrical service reticulation, connection and fees	Item			8,500
Total for Electrical Services					12,500
Demolition - Dismantling Memorial Components					
<u>Dismantling Memorial Components, Protection and Preparation for Relocation</u>					
41	Clean down structures, prepare for dismantling, carefully site measure and document status of beams, columns, mouldings for relocation	Item			6,000
42	Provide for replica moulds for replacement of any damaged components including works to west beam B1 and replacement ends to pergola beams which will be reinstated to original	Item		0	5,750
43	Remove light fittings and associated electrical reticulation	Item			750
44	Remove marble dedication sign from south beam B2	Item			500
45	Strip paint and render from 300 dia. (nominal) columns including pedestals and capitals	No	6	600	3,600
46	Strip paint and render from east main beam B1, 5m long (nominal) - note west beam B1 is to be replaced	No	1	1,250	1,250
47	Strip paint and render from main beams B2 - 7.2m long (nominal)	No	2	1,050	2,100
48	Strip paint and render from pergola beams. 6.2m long (nominal)	No	7	500	3,500
49	Sawcut around column pedestals prior to slab demolition	No	6	350	2,100
50	Sawcut through pergola beam fixings	No	14	200	2,800
51	Sawcut including mitre cuts as required to B1. B2 connections	No	4	350	1,400
52	Sawcut main beams B1/ B2 to disconnect from columns	No	6	350	2,100
53	Break up slab and excavate around column pedestals to expose footing and column connection/ prepare column pedestal for removal	No	6	900	5,400

Attachment 1 - War Memorial Ed Gardens QS report 2016

Detailed Cost Plan - Memorial Arbour Relocation

WW1 Memorial Arbour Proposed Works - Edinburgh Gardens. North Fitzroy
Cost Plan No. A



10 November, 2016

No.	Description	Unit	Quantity	Rate	Total
54	Timber protection casts for column component with hessian wrapping	No	6	800	4,800
55	Timber protection casts for east beam B1 with hessian wrapping	No	1	1,500	1,500
56	Timber protection casts for east beam B2 with hessian wrapping	No	2	1,200	2,400
57	Timber protection casts for pergola beams with hessian wrapping	No	7	500	3,500
58	Remove and pack for relocation pergola beams	No	7	500	3,500
59	Remove and pack for relocation beams B1/ B2	No	3	2,000	6,000
60	Remove and set aside damaged beam B1 (west)	No	1	850	850
61	Remove and pack for relocation columns	No	6	1,000	6,000
62	Cranage/ hoisting to components	Item			12,500
63	Relocation to prepared site	Item			3,000
64	Demolish ground slab up to line of Sub Station	m2	40	30	1,200
65	Clear site, make good, backfill and landscape area to suit with required protection works	Item			3,000
Total for Demolition - Dismantling Memorial Components					85,500
Landscaping & Improvements					
<u>Landscaping</u>					
66	Prepare, 200 topsoil, mulch to garden area	m2	30	30	900
67	Provide "Rosemary" hedge plants to form hedge including planting, initial maintenance	No	50	120	6,000
68	Make good adjoining lawn areas	Item			2,600
69	Provide for irrigation and connection to water supply	Item			3,000
Total for Landscaping & Improvements					12,500

Attachment 1 - War Memorial Ed Gardens QS report 2016

Memorial Arbour Proposed Works
Edinburgh Gardens, North Fitzroy
Memorial Arbour Restoration

12.11.2016

Element/ Trade			Total (\$)
Preliminaries			70,500
Render and Painting			26,500
Fitments			1,100
Electrical Services including Sub Station Relocation			227,500
Demolition			34,200
Reinstatement Works			33,550
Landscaping & Improvements			16,650
Total Building Cost (at November, 2016)			410,000
Contract Contingency	Item	10.0%	45,000
Cost Escalation (beyond November 2016)	Item		Excluded
Total Construction Cost (at November, 2016)			455,000
Consultants Fees (approx. 12%)	Item		55,000
Goods & Services Tax	Item		Excluded
Total End Cost (at November, 2016)			510,000

This estimate is based on the Structural Assessment Report of the World War 1 Memorial as prepared by Beauchamp Hogg Spang Consultants Pty Ltd -August 2015



Ref 16071 - Memorial Arbour Restoration Date 17/11/2016 Page 1 of 1

Attachment 1 - War Memorial Ed Gardens QS report 2016

Elemental Summary - Memorial Arbour Restoration/ Relocate Sub Station

WW1 Memorial Arbour Proposed Works - Edinburgh Gardens. North Fitzroy
Cost Plan No. B

slattery

16 November, 2016

Description	Cost/m2	Total
Preliminaries		70,500
Render and Paint		26,500
Fitments		1,100
Electrical Services		227,500
Demolition, Reinstatement Works		67,750
Landscaping & Improvements		16,650
Total for Memorial Arbour Restoration/ Relocate Sub Station	0	410,000

Attachment 1 - War Memorial Ed Gardens QS report 2016

Detailed Cost Plan - Memorial Arbour Restoration/ Relocate Sub Station

WW1 Memorial Arbour Proposed Works - Edinburgh Gardens. North fitzroy
Cost Plan No. B

slattery

16 November, 2016

No.	Description	Unit	Quantity	Rate	Total
Preliminaries					
<u>Preliminaries/ Margins and Overheads</u>					
1	Builder's general preliminaries	Item			35,500
2	Margins/ Overheads	Item			35,000
Total for Preliminaries					70,500
Render and Paint					
<u>Render and Painting</u>					
<u>Render (Cement based render with approved waterproofing additives)</u>					
3	Two coat render to 300 dia. columns including pedestals and capitals (No. 6)	m2	48	150	7,200
4	Two coat render to primary beams (No.4)	m2	56	150	8,400
5	Two coat render to pergola beams (No.7)	m2	45	100	4,500
<u>Paint</u>					
6	Approved paint system to columns	m2	48	50	2,400
7	Approved paint system to beams	m2	100	40	4,000
Total for Render and Paint					26,500
Fitments					
<u>Fitments</u>					
8	Reinstate orb urns and fit to top of west beam B1	No	2	350	700
9	Check, repair and refit orb urns to top of east beam B1	No	2	200	400
10	Refit marble plaque to face of south beam B2	Item		350	
11	Signage panel to replacement primary beam west B1 as per original	Item		2,250	
Total for Fitments					1,100
Electrical Services					
<u>Electrical Services</u>					
12	Feature light fittings and fittings (No.5)	Item			4,000
13	Electrical services reticulation, connection and fees	Item			8,500
14	Substation relocated Supply/ Mains relocation and associated items and equipment as advised	Item			185,000
15	Concrete works to sub station incl. apron and pit/ trench for HV cables, equipment and associated works	Item			30,000
Total for Electrical Services					227,500

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Attachment 1 - War Memorial Ed Gardens QS report 2016

Detailed Cost Plan - Memorial Arbour Restoration/ Relocate Sub Station

WW1 Memorial Arbour Proposed Works - Edinburgh Gardens. North fitzroy
Cost Plan No. B

slattery

16 November, 2016

No.	Description	Unit	Quantity	Rate	Total
Demolition, Reinstatement Works					
	<u>Demolition</u>	<u>Item</u>			
16	Demolish sub station structure prior to restoration of memorial - Electrical Services disconnection by "City "power	Item			7,200
17	Provide for replica moulds for replacement beam - west B1	Item			4,800
18	Remove light fittings and redundant cables etc..	Item			700
19	Remove marble dedication sign for upgrade to finishes	Item			500
20	Strip paint and render from columns including pedateals and capitals	No	6	600	3,600
21	Strip paint and render from main beams B1 east (5m long)	No	1	1,250	1,250
22	Strip paint and render from main beams B2 approx 7.2m long	No	2	1,050	2,100
23	Strip paint and render from pergola beams - 6.2m long	No	7	500	3,500
24	Sawcut main beam west B1 at connection to beam B2 for removal of beam	No	2	350	700
25	Sawcut main beam west B1 at connection to column and prepare for removal of west beam B1	No	2	350	700
26	Demolish west beam B1 - 5,0m long (nominal)	No	1	1,250	1,250
27	Temporary propping to columns prior to removal of beam	Item			800
28	Scabble and break top of existing slab for upgrade to finish	m2	40	75	3,000
29	Craneage for removal of west beam B1	Item			2,500
30	Clean structure for repair works and clear away rubbish	Item			1,600
	<u>Reinstatement Works</u>				
31	Replacement west beam B1 (5m long) cast to match existing B1 east including mould, concrete, reinforcement, mouldings, cast in fixing inserts, associated items and erect to match east beam with dowell fixings to column capital	Item			8,500
32	S.steel dowells up to 450 long	No	8	20	160
33	Surface preparation for render and finish	m2	16	20	320
34	Repair column surfaces - No 6	m2	48	90	4,320
35	Repair beam surfaces - B1 east	m2	16	90	1,440
36	Repair beam surfaces - B2 north and south	m2	24	90	2,160
37	Repair pergola beam surfaces - No.7	m2	45	70	3,150
38	Craneage for installation of replacement beam B1 west	Item			2,500
39	Concrete screed on existing slab - note to include aggregate and Fabric mesh reinforcement	m2	40	125	5,000
40	Exposed aggregate finish incl. polishing and seal	m2	40	150	6,000
Total for Demolition, Reinstatement Works					67,750

Attachment 1 - War Memorial Ed Gardens QS report 2016

Detailed Cost Plan - Memorial Arbour Restoration/ Relocate Sub Station

WW1 Memorial Arbour Proposed Works - Edinburgh Gardens. North fitzroy
Cost Plan No. B

slattery

16 November, 2016

No.	Description	Unit	Quantity	Rate	Total
	Landscaping & Improvements				
	<u>Landscaping</u>				
41	Restore lawn, landscaping after removal of Sub Station	m2	40	60	2,400
42	Extend road to service relocated Sub station	m2	50	125	6,250
43	Extend path around relocated Substation	m2	30	100	3,000
44	Relocate existing water tanks - south of Bowls Club	Item			5,000
	Total for Landscaping & Improvements				16,650

Attachment 2 - Updated and final Briefing document - Memorial Stakeholder Meeting 19 November 2015

**Key Stakeholder Consultation
Sportsman's War Memorial, Edinburgh Gardens North Fitzroy
Thursday 19 November 2015, 10am – 12pm**

Consultation objectives:

To engage key stakeholders of the Sportsman's War Memorial in Edinburgh Gardens to discuss future options for the Memorial, specifically related to its location.

The key stakeholders are identified as those representing organisations and individuals who have been involved in the establishment of the Memorial, those with relevant expertise (heritage, conservation, war memorials, open space or other relevant areas) and those who have a direct interest in the Memorial.

Council has engaged Ray Tonkin to facilitate the discussion with the intention of developing some options for Council in the future management of the Memorial. This document is to brief stakeholders on relevant factors in their deliberations, in particular the logistical, financial and heritage implications of options discussed.

The outcomes of the Consultation will be presented for Council consideration.

BACKGROUND:

1. Location



The memorial is located in its original position in Edinburgh Gardens, North Fitzroy.

Attachment 2 - Updated and final Briefing document - Memorial Stakeholder Meeting 19 November 2015

The design of Edinburgh Gardens was established by the early 1900s. When the Memorial was initially constructed the football ground and stand to the south side of the monument was fully enclosed by fencing (refer to photo below), but a path ran directly through the arbour.



Fitzroy sporting clubs' war memorial at Edinburgh Gardens



Changes to the gardens after this time involved the addition and expansion of buildings. The (current) Bowling Club building to the north of the Memorial was built around 1967 and an extension of this building was constructed in the 1990s on approval from the former City of Fitzroy, further encroaching on the arbour/monument. It is unclear when precisely the electrical substation/transformer abutting the west end of the Memorial was constructed, but it probably dates from the 1960s or 1970s. It blocks the Memorial at one end and therefore its functionality, as an entrance gateway into the grounds, is unarguably compromised. This substation is owned and operated by Citipower. Both of these buildings have a negative impact on the structural and physical appearance of the war memorial.

2. Architect and design

The Sportsman's War Memorial is architecturally significant as being the only example of this type of memorial in Victoria. It was designed by architect Edward Twentyman Junior (1869-1960), who also designed the 1925 Memorial Band Rotunda located in the gardens.ⁱ Twentyman Jnr's most recognised building is the Northcote Carnegie Library (High Street) designed in 1911. His father was a more notable architect and the founder of the well-known architectural firm Twentyman and Askew who did work during the Boom years of the 1880's. Edward Jnr served his articles in his father's firm in the 1880's as an 'apprentice' designer.ⁱⁱ After his father's death he continued the Twentyman and Askew firm, but it did not reach the levels of excellence and standing previously attained under Twentyman Snr's tenure.

Structurally the memorial is unique with the concrete cast in one section. The Memorial is unique in its design. Consultation of published books on war memorials both in Australia and the United Kingdom have failed to uncover any similar designs. In relation to other war memorials, the pergola relates to the memorial gateway which was a common form of remembrance across Australia. In general the purpose of war memorials required them to be

Attachment 2 - Updated and final Briefing document - Memorial Stakeholder Meeting 19 November 2015

inspiring, conspicuous, enduring and able to serve as a focal point for Anzac and Remembrance Day ceremonies.

Below is an image from 1932, which documents an official ceremony at the Memorial (source The Argus, 26 Apr 1932):



To Council's knowledge, there were 13 pieces of ornamentation related to the original Memorial of which only seven (four are reproductions) now remain. These include:

- (a) ceramic wreath (restored in 2002 and currently being assessed by the Centre for Cultural Materials Conservation for options for a replica wreath to be re-mounted on the Memorial);
- (b) six bronze shields with the names of 38 sporting club members who fell in the First World War (only one remains and is located in secure storage to protect it from theft or vandalism). The Fitzroy History Society is currently researching the names on the shields and has identified 24 of the 38 names. It is hoped the shields can be reproduced and reinstated on the Memorial's columns like the originals;
- (c) four funerary urns (originals destroyed or stolen). Four reproductions were installed on the upper corners of the archway in 2011 following a restoration grant;
- (d) the marble plaque fixed to the concrete structure with the dedication inscription: "This memorial has been erected by / The Fitzroy Cricket, Football, Bowling, Baseball and Tennis Clubs / To perpetuate the memory of members who fell in the Great War 1914-1918.";
- (e) honour board (naming the community members who enlisted) which is not believed to have ever been attached to the memorial but located nearby in the Fitzroy Cricket Club. According to an unconfirmed report made by a community member a number of years ago, the Honour Board was moved to the gatehouse building near the existing grandstand around 20 years ago. The gatehouse was later burnt down through vandalism; it is presumed that the board was also destroyed by the fire.

Attachment 2 - Updated and final Briefing document - Memorial Stakeholder Meeting 19 November 2015

3. History

The Sportsman's War Memorial was erected in memory of members of the local sporting clubs who died in the First World War. It was unveiled on 9 December 1919. The unveiling was performed by Brigadier-General Lloyd, with a guard of honour that included returned members of the sporting clubs and members of the local branch of the Sailors and Soldiers League.

The Memorial appears to have been funded solely through public subscription amongst members of the various sporting clubs. A letter from D.J. Chandler, a Councillor affiliated with the cricket club, requesting *'permission to erect a suitable Memorial to members of that and other affiliated Sports Clubs...'* was received by the City of Fitzroy at the time.

During the time of construction and the following ten years after completion, the Memorial played an important role for local residents and members of the sporting groups as a way of remembering and honoring former friends and relatives. The memorial was probably used for services up until the completion of the Shrine of Remembrance when most small local remembrance services declined and the focus was on much larger ceremonies at the state war memorial.

4. Previous restoration 2000-2011

A Best Practice Forum was held on 5 September 2000 at Collingwood Town Hall investigating future options for the Memorial. The decisions unanimously reached in the forum were documented, but unfortunately not the process or discussions which led to the conclusions. The recommendations were to leave the Memorial in its present location for the time being; to clear the surrounding vegetation to improve access and visibility; for Council to investigate the relocation of the substation built immediately next to the Memorial. Council undertook works to restore the Memorial following this, including a conditions assessment and removal of overgrown plants which had covered a good part of the upper monument.

In 2005-2006, Council successfully applied for and received \$7000 from the Federal Government through the Restoring Community War Memorials Grants Program. Conservation consultancy ARTCARE was engaged to undertake restoration works and this included concrete stabilization treatment, chemical cleaning and the cleaning/restoration of the last bronze shield and the marble epitaph on the top of the memorial.

In 2010-2011, Council applied for and received another grant of \$7200 from the same Restoring Community War Memorials Grants Program to undertake further restoration on the monument. The funds were used to replace the missing funerary urns from the top of the memorial as well as engaging a conservator to clean and apply acrylic render to the structure. Council's Open Space team then undertook some sensitive landscaping around the monument to add to a sense of place and emphasise the Memorial's significance. The structure became more visible and, though still boxed in on two sides by other buildings, now more clearly defined.

In October 2002, Council commissioned John Patrick with Allom Lovell and Associates to prepare a *Master Plan* and *Conservation Management Plan* for Edinburgh Gardens.

Attachment 2 - Updated and final Briefing document - Memorial Stakeholder Meeting 19 November 2015

Extensive community consultation was conducted in the formulation of both these documents which still serves as an important reference for Council in the ongoing management of the Gardens.

The plan did not recommend a relocation of the Memorial. Rather it suggests the conservation of the Memorial and reinstatement of the original decorative elements, upgrading the landscape setting including removal of the adjacent substation to a less visually intrusive location at the rear of the grandstand, reinstatement of the path entrance under the structure and replanting. A number of these actions related to the ornaments have been completed, and Council is currently exploring options to replicate the original porcelain wreath and bronze shields.

CURRENT PROCESS:

5. Structural Assessment 2015

Council commissioned BHS Consultants to undertake a new structural assessment of the Memorial in August 2015. The primary purpose of the assessment was to re-evaluate the condition of the Memorial structure and to assess whether the structure might survive a move. The report indicates the Memorial to be in good condition generally except for the western beam (B1); this is the one that abuts the substation.

The western beam has significant carbonation and as a result has extensive corrosion of its bottom reinforcement. This beam will require the corroded reinforcement and the carbonated concrete to be replaced; probably the reconstruction of the entire beam will be required.

6: Logistical considerations

Memorial relocation:

The structural report indicates that the Memorial can be relocated, but given the limitations of the surrounding area (such as neighbouring trees and infrastructure) and the significant weight of the monument which is estimated at approximately 20,000kg or 20 tonnes, the relocation will require it to be cut into sections and reconstructed. There will significant damage to the original structure and works done to repair this in the new location.

Any new location for the Memorial will require testing of the ground/soil via a geotechnical assessment, analysis of tree clearance and impacts to paths, and would be subject to wider community consultation. One proposed location has been raised as an option, which is a site in front of the Fitzroy Tennis Club. This site appears to meet the criteria outlined above and also maintains the connection between the Memorial and resident sporting clubs.

A preliminary cost estimate for the relocation of the Memorial is **\$377,000** inc GST.

Substation relocation:

The substation/transformer next to the Memorial is owned by Citipower and is still in operation. A query has been lodged with the company and they have now assigned a project manager to investigate relocating the asset in greater detail, but following is their initial advice:

Attachment 2 - Updated and final Briefing document - Memorial Stakeholder Meeting 19 November 2015

- 1) The substation solely serves the local sporting clubs and facilities in the gardens and does not serve any other parts of the network in the surrounding area.
- 2) It is connected to a high voltage power line running from Brunswick Street and the substation could in principle be moved to another part of the grounds, however the further away it is placed from the source power line then the more the cost will increase.
- 3) Citipower has advised that Council will need to identify the preferred new location for the substation in order to cost it. Citipower will not be suggesting preferable locations as they cannot accommodate the extra work.
- 4) It is also possible that the substation could be decommissioned and demolished altogether, to be replaced with a direct connection to the street power from Brunswick Street, however this needs to be further investigated.
- 5) For further scoping on both options outlined above, Citipower has advised that Council may need to hire an electrical consultation for this work.
- 6) The electrical load needed to power the sites currently using the transformer would also need to be determined. This also needs to be provided by Council and cannot be investigated by Citipower.
- 7) Council would have to bear all the costs associated with the substation relocation or removal, given its decommission would be required by Council and not Citipower
- 8) An initial cost estimate of **\$150,000** plus has been quoted, however more in depth costing of all the options would be required.

The slip lane from Brunswick Street that leads to the substation and Memorial is also used as an unloading dock and parking for the Fitzroy Victoria Bowling & Sports Club's vehicles; this will also need to be considered as part of any future re-development or enhancement of the Memorial site.

While the removal of the substation could provide a pathway (and even a grander avenue) to be reinstated through the Memorial from Brunswick Street, the north side of the monument would still remain located right up against the rear of the Bowling Club building, and not freestanding as intended. The seven upper beams of the Memorial (on top of the perimeter entablature beams) were cut off on the north side to make way for the bowling club extension, therefore the original construction and symmetry of the monument will remain compromised in this position.

7. Financial implications

Costs:

Council allocates \$37,000 to the maintenance of its Art & Heritage Collection (encompassing over 800 items) per year and given the budget limitations, only urgent works are usually accommodated and programmed maintenance conducted on a staged basis. This would not meet the costs associated with relocating the substation or the Memorial.

Attachment 2 - Updated and final Briefing document - Memorial Stakeholder Meeting 19 November 2015

Grants available:

The State Government has grants of up to \$20,000 available through the Restoring Community War Memorials and Avenues of Honour Grants Program; this is a rolling deadline program. Additionally, the State Government also offers grants of up to \$20,000 available through the Victorian Veterans Council - Anzac Centenary Community Grants Program, however the deadline for this has passed.

The Federal Government has grants through the Department of Veteran's Affairs called Major Commemorative Grants; the next round closes 3 January 2016, with no published ceiling for the level of grants made. However, it is unclear whether this project would meet the Major Commemorative Grants guidelines as it supports "the restoration of existing national/state/territory or otherwise significant war memorials".

It is anticipated that these grants will be sufficient to fund the replacements of the ornamentations (replicas of the shields), but not for major works such as a relocation of the Memorial or the substation. If Council wish to proceed with obtaining more solid information and details about relocating the substation in particular, then engaging an electrical consultant would also be an additional cost consideration.

ⁱ Goad and Wills Ed, *Encyclopedia of Australian Architecture*, 2012.

ⁱⁱ *Ibid.*

Attachment 3 - WWI Sportsmans War Memorial Edinburgh Gardens Structural Report - BHS Consultants - August 2015

B H S
CONSULTANTS

**STRUCTURAL ASSESSMENT OF
WORLD WAR I
SPORTSMAN'S MEMORIAL
EDINBURGH GARDENS, NORTH FITZROY**



Consulting Civil &
Structural Engineers
Project Management
Building Evaluation
Expert Witness
Heritage Structures
Asset Management

DIRECTORS:

David Hogg
Joseph Spano

CONSULTANT:

David Beauchamp

**PREPARED FOR
CITY OF YARRA
AUGUST 2015**



BEAUCHAMP HOGG SPANO CONSULTANTS PTY LTD
ACN 088 069 710 11 Amsterdam Street Richmond VIC 3121
Phone: (03) 9421 2345 Fax: (03) 9421 2346 Email: bhs@bhsc.com.au

Attachment 3 - WWI Sportsmans War Memorial Edinburgh Gardens Structural Report - BHS Consultants - August 2015

**STRUCTURAL ASSESSMENT OF
WORLD WAR I SPORTSMAN'S MEMORIAL
EDINBURGH GARDENS, NORTH FITZROY**

1.0 INTRODUCTION

At the request of Ann Limbrey from the City of Yarra, a structural condition assessment of the World War One Sportman's Memorial in Edinburgh Gardens North Fitzroy, has been carried out. It is understood that the City of Yarra are investigating a proposal to relocate and refurbish the 'Memorial'. The purpose of the assessment is to assist the Council with their decision making process in relation to proposed works to the Memorial. Reference is made to the previous Structural Audit & Footing Investigation Report dated July 2002 by our office which included a footing and geotechnical investigation at the existing Memorial site.

The basic methodology adopted for this structural condition assessment included:

- A visual inspection of the Memorial – note the west face of the western primary beam is concealed by the existing adjacent substation
- Limited intrusive investigations by Protecon (Vic) Pty Ltd including two approximate 50mm diameter cores each approximately 100mm deep and three approximate 200mm by 200mm by 60mm deep break out exposures of primary beam bottom reinforcement
- Recording conditions and structure where exposed
- Study observations, measurements and photographs recorded
- Make assessment and prepare report

This report is released subject to the following qualifications and conditions:

- The report may only be used and reproduced in full by the named addressee for the purpose for which it was commissioned and in accordance with the corresponding Conditions of Engagement.
- The report is based on an inspection and assessment of the structure where exposed or access was able to be gained.
- The report contains an assessment based on the conditions observed at intrusive investigations site. These conditions may or may not be representative across the whole structure / site.
- A geotechnical investigation is yet to be carried out at a proposed location of the Memorial near the northern end of the west boundary to the Fitzroy Tennis Club.
- This report does not exhaustively address the building's compliance with current codes and regulations.
- This report does not address the presence of contaminate building materials.
- This report does not contain an exact specification for remedial works required to conserve or restore the building.

2.0 OBSERVATIONS & FINDINGS

2.1 TYPE OF CONSTRUCTION

The Memorial structure includes:

- Primary perimeter beams B1 and B2, and 7 number secondary pergola beams PB1 supported on six number columns – refer attached sketch SK01 in Appendix A

Attachment 3 - WWI Sportsmans War Memorial Edinburgh Gardens Structural Report - BHS Consultants - August 2015

3

- The construction of the primary framing beams was confirmed to be of reinforced concrete from the intrusive investigations (cores and exposures) carried out – note it is expected that the pergola beams and columns are of the same construction and that the concrete was all cast on site and there would be reinforcement bars connecting primary beams to the columns and pergola beams to the primary beams
- B1 beams on west and east sides are 920mm deep by 260mm wide except for the approximate central third which is 1500mm by 260mm wide beam – all dimensions inclusive of rendered finishes
- B2 beams on the north and south sides are 480mm deep by 260mm wide
- PB1 beams spanning between B2 beams are 280mm deep by 140mm wide
- The bottom reinforcement was exposed to the inside edge of both B1 beams and the northern B2 beam, and found to consist of 12.7mm diameter (or half inch bars) – the limited extent of exposures did not confirm the total number of bottom bars with exposures stopping after two bars were found in the western B1 and one bar (in significantly better condition) was found in both the eastern B1 and northern B2 beams
- No ligature reinforcement to any of the beams was found at the exposures
- Circular support columns with a square head and pedestal
- The footings to the columns in geotechnical report M5042 carried out by AGS-Schult dated 22 July 2002 prepared for, and included in, the BHS previous Structural Audit and Footing Investigation Report indicates that the footings are concrete and founded on natural silty clay soils.

2.2 SITING

The Memorial is currently unsatisfactorily sited to the east side of the Substation and south side of the Bowls Club Pavilion. It is understood that the Substation and the Pavilion were later structures which obscure the Memorial from the public unlike the original intent of such a Memorial.

2.3 STRUCTURAL CONDITION

The following structural condition has been assessed from the visual and intrusive investigations – refer Appendix B for photographs:

- The western B1 has a series of longitudinal cracks along the eastern side face and underside of the beam.
- The other beams are generally free of similar structural cracking.
- The exposure to the western B1 found a rendered coat of approximately 10mm over the structural concrete to the beam, and a concrete cover (or thickness of structural concrete over the first bottom bar) of 20mm from the side and 30mm from the bottom.
- The exposure to the eastern B1 found a rendered coat of approximately 12mm over the structural concrete to the beam, and a concrete cover of 40mm from the side and 40mm from the bottom. This amount of cover is significantly better than for the western B1.
- The exposure to the northern B2 found a rendered coat of approximately 20mm over the structural concrete to the beam, and a concrete cover of 37mm from the side and 25mm from the bottom. The bottom cover is minimal and likely to lead to problems if consistent along the beam.
- The bottom bar reinforcement to the western B1 beam was corroded and has a result had lost cross sectional area. The current diameter was 11.5mm instead of the original 12.7mm which is 9.5% reduction in diameter and 18% reduction in cross sectional area – the latter translating to a 20% reduction in the structural strength of the beam assuming all reinforcement in this beam is corroded to the same degree.
- The bottom bar reinforcement to the eastern B1 and northern B2 beams was

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found to have minor surface corrosion and no loss of cross sectional area – that is the diameter of the bars was measured as 12.7mm.

- The core sample taken from the western B1 just above the reinforcement exposure, was tested for extent of carbonation and found it to have 30mm penetration into the structural concrete.
- The core sample taken to the northern B1 was found to have zero carbonation into the structural concrete.
- The exposure to the structural concrete of the western B1 was significantly easier to create than it was to the eastern B1 and northern B2 beams. The aggregate density and the compaction of the concrete to the latter two was found to be higher than the former and of course the former had a series of longitudinal cracks which meant sections of the concrete were already delaminated.

3.0 DISCUSSION OF FINDINGS

- The Protecon (Vic) Pty Ltd report of the intrusive investigations is attached to Appendix C of this report.
- The Memorial is generally in good condition except for the western B1 beam.
- The structural concrete to the western B1 beam is carbonated from the external surface of the concrete beam to 30mm below the surface. The reinforcement bars exist within this carbonated zone. The depth of the carbonated zone will increase with time. Carbonation of concrete refers to the reduction in the alkalinity of the concrete, the latter which is required to maintain a passive environment for the reinforcement to exist in without corroding. Concrete with a high porosity is susceptible to carbonation as carbon dioxide from the air can more easily penetrate into the concrete which results in the reaction which reduces its alkalinity. Once the alkaline environment reduces, the reinforcement will corrode which will result in the reinforcement expanding in size, which in turn creates bursting stresses in the concrete, and the concrete to spall away. This spalling defect was found to the western B1 and it was also observed that this beam has been previously repaired and is spalling again. Once the symptoms of spalling are observed in reinforced concrete structures, this usually means the carbonation and reinforcement corrosion is well advanced.
- The concrete to the western beam B1 appears to be of lesser quality than concrete found at exposure sites to the eastern B1 beam and the northern B2 beam. The reason for this may have been variability in the concrete batched / delivered for this beam, its original higher exposure to the prevailing westerly weather direction prior to Substation construction, and / or related to water being trapped at the beam since the construction of the Substation (as the other three beams which could be argued to be as exposed to the weather as much as the west beam are also able to dry out).
- The Protecon report states the western beam being in danger of collapse if it did not have the substation east wall built partially under it. It is my opinion that pieces of spalled concrete would have likely fallen had the substation not have been there, however the beam itself would have likely remained self supporting as there are still gaps between the underside of the beam and the top of the substation wall.
- The western B1 beam requires significant repair including the removal and replacement of all the corroded reinforcement and carbonated concrete. Given the degree of carbonation, this is likely to mean a rebuild of the western B1 beam regardless of whether the Memorial remain in its existing position or be relocated to another place in the Edinburgh Gardens.

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4.0 FEASIBILITY OF RELOCATION OF MEMORIAL

Given the above findings and observations on site the following comments are made in relation to the feasibility of relocating the Memorial:

- The Memorial can be relocated however it would need to be done in sections. The total weight of the structure is estimated to be approximately 20,000kgs (20 tonnes) broken down as follows:
 - Pergola beams PB1 500kgs each
 - Beams B1 4000kgs each
 - Beams B2 2500kgs each
 - Columns 600kgs each
- Given the location of trees and structures within the Gardens it is expected that the sections would need to be broken down to individual elements, that is beam by beam, column by column etc, as the maximum weight of an element would be 4000kgs for Beam B1.
- The thought of relocating all the beams in one operation has been considered however the total aggregate weight of all the beams is approximately 16,500kgs which would therefore require a significant mobile crane to lift and travel it to a new location, which is unlikely to be able to be done given the location of trees and existing structures in the Gardens as noted above. Also there are complications with the stability of the beams B1 in particular if such an operation was to be considered.
- The western B1 beam will not need to be relocated as it is recommended it be rebuilt in the new location once columns, B2 beams and eastern B1 beam are in place.
- The relocation of the Memorial element by element would mean saw cutting the interface of the various elements to disconnect them from each other and allow them to be lifted and transported individually. This work would create some damage to the existing structure which would require repair at the new location.
- The relocated elements would need to be reconnected to each other to reinstate the integrity of the structure. Again this would cause some damage and would require local repairs.
- The entire Memorial would finally need to be coated with a suitable product to provide protection to the elements against the ingress of water, chloride and carbon dioxide to prevent or minimise the risk of carbonation and corrosion reinforcement in the future. Such coatings are equivalent to adding concrete thickness to the cover over reinforcement except they will require more maintenance than if the structure had been constructed with more concrete cover over reinforcement. There are a number of products on the market and most of them are clear coatings.
- From the geotechnical investigation report associated with our previous Structural Audit and Footing Investigation, and the authors work for the reconstruction of the northern courts to the Fitzroy Tennis Club, the soil geology near the west boundary of the Fitzroy Tennis Club is expected to include fill and silty soils overlying natural silty clays. These clays are highly reactive basaltic clays which mean they shrink when they are dry and swell when they are wet. The shrink / swell characteristic of this soil results in volume changes in the soil, which leads to movements to light buildings / structures depending on the stiffness and founding depth of the footing system. New isolated pad footings to a minimum of 1.5 metre depth are expected to be adequate for the Memorial, however this will need to be confirmed with the geotechnical investigation for the proposed new location of the Memorial which has been commissioned but not yet completed.

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5.0 CONCLUSIONS

The Memorial is assessed to be in general good condition except for the western beam B1. The western beam B1 has significant carbonation and as a result has significant corrosion of its bottom reinforcement. This beam will require the corroded reinforcement and the carbonated concrete to be replaced. It is my opinion that this would involve the reconstruction of the entire beam.

It is understood that the Council is considering a proposal to relocate the Memorial to the path outside the west boundary of the Fitzroy Tennis Club which will give it greater exposure to the public and locate it closer to the sports clubs that the fallen soldiers the Memorial is for belonged to. It is my opinion that the Memorial is able to be relocated however this would need to be done element by element, that is beam by beam, column by column. This is partly due to the weight of the structural elements and aggregate weight of the entire structure, partly due to the required crane size and travel logistics involved with moving the elements; and partly due to trees, paths and structures within the Gardens to be negotiated in such a relocation.

The relocation works would result in some localised damage to the structure as elements were freed from one another to allow them to be relocated, and then discrete structural connectivity details constructed in order to reinstate the structural integrity of the Memorial. Once all structural works and repairs to render finishes were carried out, the structure would need a protective coating to minimise carbonation of the concrete in the future and its associated defects of corroding reinforcement and spalling concrete.

If you have any queries or require further assistance, please contact the undersigned at our office.



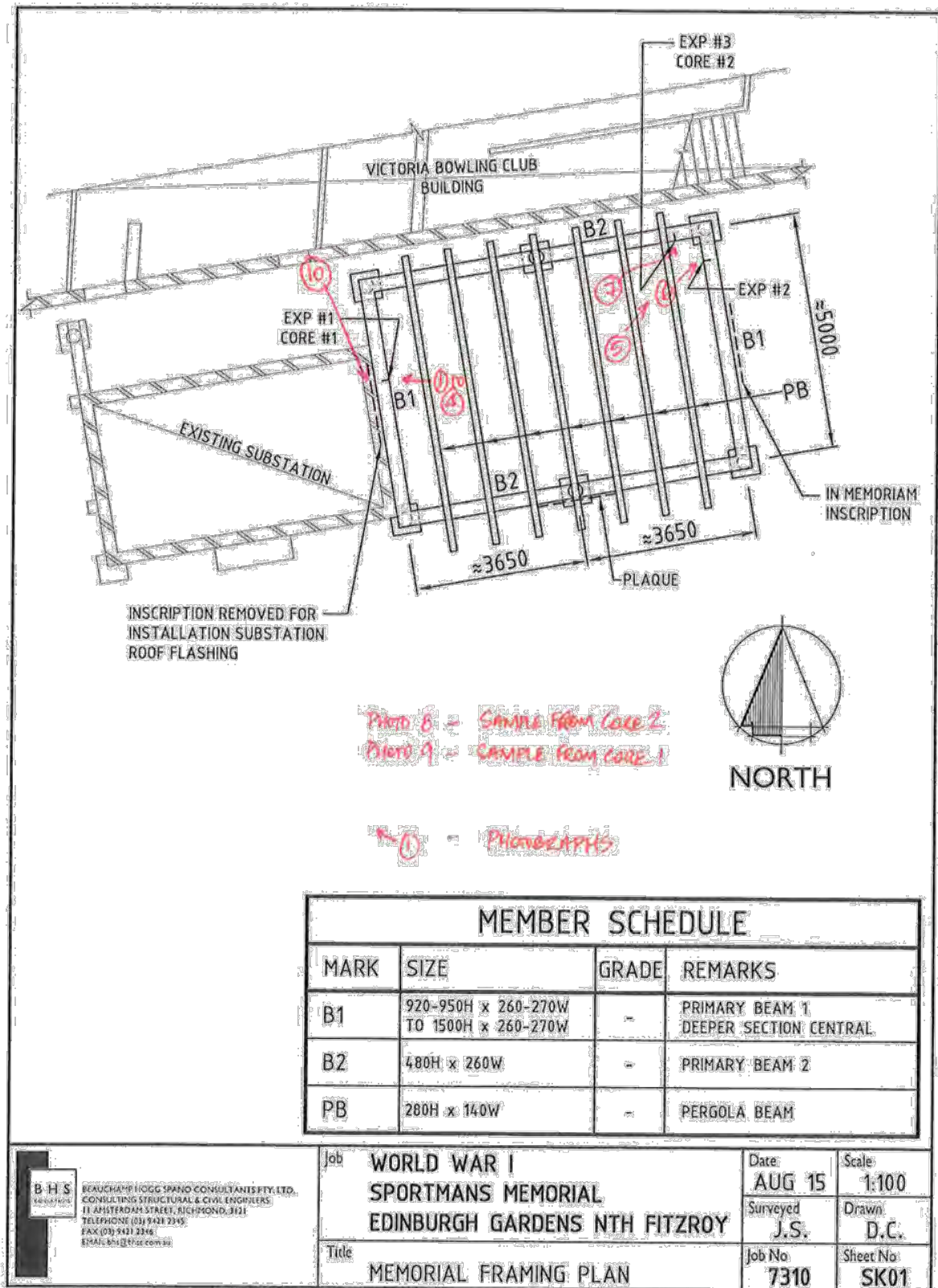
BEAUCHAMP HOGG SPANO CONSULTANTS
JOE SPANO
BECivil, MIEAust, CPEng, RBP EC#1433

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Appendix A

- **Building / Site photograph and observation location
Drawing SK01**

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Appendix B

- **Photographs of existing conditions and site observations**

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7310 rpt ph01.JPG



7310 rpt ph02.JPG

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7310 rpt ph03.JPG



7310 rpt ph04.JPG

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7310 rpt ph05.JPG



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7310 rpt ph07.JPG



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7310 rpt ph09.JPG



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Appendix C

- **Protecon Site Investigation Report**

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PROTECON

Concrete Preservation Technology

Mickja Pty Ltd as trustee of the Norton
Family Trust trading as Protecon (Vic)
ABN 81 230 982 785 ACN 076 954 599
Member Australian Concrete Repair Association
Member Australian Institute of Waterproofing
Builders Licences DB-L 21371 CB-L 30410

Unit 13 18-20 Powlett Street Moorabbin 3189

Phone 9555 0111
Fax 9555 0688
Mobile 0417 185 491
email norton@protecon.com.au

22 August 2015

Joe Spano
BHS Consultants
11 Amsterdam Street
Richmond 3121

Re: Site Investigation Report War Monument – Edinburgh Park North Fitzroy

Introduction:

Protecon (Vic) were asked to complete some destructive investigation work on the World War One monument located on the southern boundary of the bowling club grounds, in conjunction with Joe Spano of BHS Consultants.

The work included core holes and phenolphthaleine carbonation testing, and physical breakout of the western main beam which is in poor condition, and two other beams in relatively good condition. The work was completed on Friday 14 August 2015.

Works Log:

1100 Core in western main beam hit steel and damaged core. Broke out cracked concrete on the bottom eastern edge of the western main beam to expose 12 mm diameter moderately corroded reinforcing steel (photo)

1140 Moved to eastern main beam and saw cut breakout to steel. Same configuration but this steel (in good concrete) was in very good condition.

1210 Same excavation done on northern connecting beam and same configuration and in good condition.

1220 Cored northern beam adjacent excavation between grid to get carbonation test analysis. Test showed nil carbonation.

1240 Cored western main beam (in poor condition) and tested for carbonation. Test revealed carbonated to 30 mm

Repair:

The following day the demolition sites were repaired with polymer modified repair mortar and painting will be completed week of 24 August after adequate curing time has elapsed.

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2.

Conclusion:

In our opinion the western main beam is severely compromised by carbonation damage (concrete cancer) and if not supported by the newly constructed sub-station adjacent, it would be in danger of collapse.

The corresponding eastern main beam is in a far lesser state of degradation and is sound. The lateral beams are in very good condition and sounding of the traversing planks indicated they are in good condition.

Yours sincerely,
PROTECON (VIC)

Glenn Norton
Victorian Manager

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Attachment 3 - WWI Sportsmans War Memorial Edinburgh Gardens Structural Report - BHS Consultants - August 2015



Attachment 4 - Ray Tonkin Report on Memorial Consultation - Nov 2015 FINAL

**Report on Key Stakeholder Consultation
Sportsman's War Memorial, Edinburgh Gardens North Fitzroy
19.11.2015**

Ray Tonkin

179 Spensley St
Clifton Hill 3068
ph 03 9029 3687
mob 0408 313 721

tonka@bigblue.net.au

Attachment 4 - Ray Tonkin Report on Memorial Consultation - Nov 2015 FINAL

Attachment 4 - Ray Tonkin Report on Memorial Consultation - Nov 2015 FINAL

1. INTRODUCTION AND PURPOSE OF CONSULATION

Consultation objectives:

To engage key stakeholders of the Sportsman's War Memorial in Edinburgh Gardens to discuss future options for the Memorial, specifically related to its location.

The key stakeholders are identified as those representing organisations and individuals who have been involved in the establishment of the Memorial, those with relevant expertise (heritage, conservation, war memorials, open space or other relevant areas) and those who a direct interest in the Memorial.

The intention was to develop some options for Council in the future management of the Memorial.

Briefing Document

A briefing document was prepared by Council officers and made available to all participants. This document was designed to brief stakeholders on relevant factors in their deliberations, in particular the logistical, financial and heritage implications of options discussed.

2. ATTENDEES

Councillor Roberto Colanzi	Mayor, City of Yarra
Bob Soutter	Combined Tramways/East Melbourne RSL - Committee
Paul McCrohan	Combined Tramways/East Melbourne RSL - President
Dr Bart Ziino	History professor - Deakin University
Mike Moore	Fitzroy History Society
Peter Woods	Fitzroy History Society
David Shying	Secretary, Fitzroy Victoria Bowling & Sports Club
Joan Eddy	Fitzroy Football Club - President
Siu Chan	Senior Coordinator Arts, Culture & Venues - Council
Louisa Marks	Arts & Cultural Services - Council
David Cameron	Strategic Planner - Council
Carrie Lindsay	Coordinator Open Space Project Design - Council

3. AGENDA

The following agenda was used to direct the consultation

1. *Welcome and Introduction – Mayor Roberto Colanzi, Nicholls Ward Councillor*
2. *Briefing document and background - Siu Chan, Senior Coordinator Arts, Culture & Venues*
3. *Introductions:
Participants to introduce themselves and briefly explain their interest in the issue. This will provide an opportunity to identify the key issues.*

Attachment 4 - Ray Tonkin Report on Memorial Consultation - Nov 2015 FINAL

4. *Outline and agreement on issues*
5. *General discussion*
6. *Conclusions*

Towards the end of the consultation participants took an opportunity to view the memorial and fortuitously were able to inspect the interior of the substation and discuss its use with a representative of Citipower.

4. KEY ISSUES

The following issues were identified:

.1 Relocation of Memorial. This is the result supported by Bob Soutter and Paul McCrohan from the Combined Tramways/East Melbourne RSL. They believe that in its current location the memorial will always be compromised. Council received a preliminary estimate that a relocation could cost approximately \$370,000, but further detailed scoping is required. A site adjacent to the tennis courts has been identified as a possible new location for the memorial. However, this is well away from the existing site.

.2 Removal of the substation. The construction of the electric substation adjacent to western end of the memorial has blocked the capacity of the memorial to be used as a walk through arbor. The substation is used by Citipower to distribute power to the sporting facilities in the Edinburgh Gardens. The meeting with the Citipower officer on site revealed that the building is not required, but that a secure alternative facility would need to be provided for the distribution board located within the building. Citipower has provided an estimate that the removal of the substation and relocation of the facility could cost approximately \$150,000 and there would be expected additional costs on top of this to engage an electrical consultant to undertake the feasibility assessment.

.3 Future of the bowling club. There is no proposal to demolish the current bowling club buildings. Therefore, if the memorial is to stay in its current location it would be desirable for changes to be made to the wall and environs adjacent to the clubrooms to provide a more appropriate environment for the memorial. The issue of the club's water tanks and ongoing parking of the bowling club's van next to their building was raised and the fact that this also impacts on the visual and physical accessibility of the memorial.

.4 Further repairs and restoration. Around 2000 Council undertook significant repairs and restoration works which cleared away vegetation, revealed two memorial plaques, relocated the marble dedication plaque to the south side and undertook landscape works to the surrounds. At this time a ceramic wreath was rescued and is now permanently located at the Fitzroy Town Hall (although it is currently being assessed for replication). One of the two revealed bronze plaques was subsequently stolen soon after the restoration works and the remaining one was then taken away for safe keeping to collection storage.

Regardless of the relocation issue there are a number of works that should be undertaken. The beam on the western end (adjacent to the substation) is cracking with evidence of reinforcement corrosion. This should be repaired. The missing bronze plaques should be replaced, perhaps with a material that is not attractive to thieves. This will also depend on the history research to determine what names were engraved on the plaques.

The concrete purlins that run north south across the memorial to create the arbor feature an end detail to emulate timber purlins. However, this detail was removed for the northern end of all the purlins to enable the two storey extension of the bowling club to be built. If the memorial is to stay in its current location it will not be possible to reinstate this detail on that side.

Attachment 4 - Ray Tonkin Report on Memorial Consultation - Nov 2015 FINAL

The investigation of further interpretive and re-landscaping opportunities to improve the access and appearance of the memorial had widespread support, particularly from the RSL and Fitzroy Football Club. The proposal for a mural along the bowling club wall, in particular a battle scene, was proposed. There was broad support for a mural or aesthetic improvements along that wall to highlight the memorial in its current position and its long history and relationship with the community. This will be explored in the short term. Other options such as perforated steel sheets with imagery or text or alternatives, mounted along the bowling club wall to create an avenue leading to the memorial, was also raised and had general consensus.

.5 Further historic research. Mike Moore from the Fitzroy History Society has undertaken to complete the research being undertaken to reveal the names that were inscribed on the bronze plaques.

5. CONCLUSIONS

.1 There was a general consensus that the most practical approach would be to pursue the removal of the substation. The relocation would be likely to produce a facsimile of the memorial rather than be a relocation.

.2 Further discussions should take place with the Bowling Club to establish a plan which would improve the environs of the memorial.

.3 A plan for further repairs and restoration of the memorial be prepared (this would include replicating the plaques using the names established from Mike Moore's research) and funds sought to undertake those works.

.4 Council consider commissioning a history of the memorial and its environs in order to provide a basis for future interpretation of the significance of the memorial.

11.8 Potential Processes for: (a) Community / Developer Engagement re Development Proposals and (b) Sourcing of Independent Heritage Advice

Trim Record Number: D17/4667

Responsible Officer: Group Manager Chief Executive's Office

Purpose

1. To consider potential:
 - (a) options to encourage genuine early engagement between developers and community concerning development proposals; and
 - (b) models and the costing thereof in providing in-house technical heritage advice that is independent of Council's planning desk, in order to assist residents, developers and others in planning applications.

Background

2. Council will recall its resolution of 20 December 2016:

'1. That Council note:

- (a) its resolution of 22 November referring the matter of appointing a heritage advisor to the budget process for consideration;*
- (b) this report re the questions raised at the 6 December Council Meeting and responses thereto; and*
- (c) that the responses to the questions raised will be conveyed to the questioner and also referred to the budget process as necessary.*

2. That Council receive a report in the March meeting cycle on:

- (a) how developers can be encouraged to undertake genuine engagement with the community at an early stage of a development proposal;*
- (b) the development of practice guidelines with examples, on the importance of heritage protection that could be made available at Council's planning desk to assist residents, developers and others in planning applications; and*
- (c) various models, options and costings for the provision of in-house, technical heritage advice, that is independent of the Planning Departments, and which is prepared following:*
 - (i) consultation with the City of Yarra's Heritage Advisory Committee; and*
 - (ii) which has regard to the Heritage Victoria Guidelines on the appointment of heritage advisors, which can be found at http://www.dtpli.vic.gov.au/_data/assets/pdf_file/0010/244864/HERITAGE_ADVISORY_SERVICE_BRIEF_2014_August.pdf.*

3. In addressing the specific questions arising from the above resolution, the following is noted:

2. (a) how developers can be encouraged to undertake genuine engagement with the community at an early stage of a development proposal:

- (a) For context it is appropriate to note the current processes which trigger engagement between a Developer and community representatives include:

- (i) in the instance of “large scale” developments (*e.g. 6 storeys plus or others of significant development impact*), at the start of the formal advertising period, Council Planners initiate a information session between the applicant developer and parties who are formally notified by Council, of the pending application (*an interesting note, Council is on occasions criticised by Developers for what they consider is the overly generous extent to which Council notifies and invites adjacent property owners/occupiers of the proposal to meet*);
- (ii) all objections received by Council from the formal advertising period are forwarded on to the applicant Developer to be taken into consideration;
- (iii) in the instance of most planning applications which attract 6 or more submissions, the Council Planners initiate a consultation meeting between the objectors and the applicant ahead of the matter being referred to IDAC
- (iv) it is also appropriate to note that in respect of current Planning Legislation and Regulations, it is not possible for the Council to enforce an early engagement between applicants and community members. As such, any proposed process would desirably need to be sufficiently persuasive to encourage all parties to accept that there is value in such a process, in order to encourage participation. Thus whilst Council planners encourage Developers:
 - a. to participate in a pre-application meeting, and
 - b. to liaise with abutting property owners/occupiers pre application or representatives of the wider Community (i.e. abutting/nearby property owners or occupiers) prior to lodging an application;

there is currently no statutory provision for Council to require the above to occur.
- (b) For context it is also appropriate to reflect on current and potential issues which impact on the processing of a planning application, including:
 - (i) some 2/3rds of Yarra City is currently subject to heritage overlays;
 - (ii) the statutory requirement is for a planning authority to determine applications within 60 business days;
 - (iii) the introduction of additional meetings between applicants and community will potentially add extra time to the processing of applications;
 - (iv) there is a potential for failure to determine rates to increase;
 - (v) State regulations provide that applicants may pursue Councils at VCAT for application fees if Councils do not determine in the statutory 60 business day period;
 - (vi) whilst Council may encourage applicants to participate in consultation, there is already a rate of objection by some developer applicants to do so;
 - (vii) planners advise that to date, consultation meetings outside of the formal process have not proven to be particularly productive, generally due to applicants not attending; and
 - (viii) indications tend to be interpreted, that the development sector already has issues with turnaround time for processing applications.

2. (b) the development of practice guidelines with examples, on the importance of heritage protection that could be made available at Council’s planning desk to assist residents, developers and others in planning application:

- (a) I approached this element, by reviewing current processes, which includes the provision in the Richmond Town Hall Reception area of a wide range of Information/Fact Sheets relating to the Planning process and the provision of information on Council’s web-site;

- (b) The material on heritage currently uploaded to the web-site, is generally quite comprehensive and includes documents such as: heritage database, heritage precincts, heritage walks, understanding heritage gradings, heritage studies, heritage links and resources, etc.;
- (c) printed material which currently addresses the following subjects and makes mention of “heritage” include:
 - (i) Understanding planning in Yarra;
 - (ii) Overview of the Planning Permit Process;
 - (iii) Pre-application meetings
 - (iv) Overlays;
 - (v) Minor Building Works (Commercial) and (Residential);
 - (vi) Building or Extending a Dwelling;
 - (vii) Painting (*Planning guide and checklist*);
 - (viii) Fences;
- (d) in this regard it is considered a relatively easy step to modify and re-write the Facts/Information Sheet material as provided in a manner to specifically enhance background information inclusive of additional key explanations to especially expand and highlight the important role of heritage in planning considerations; and
- (e) in addition, it is proposed that modified enhanced fact sheet information will also be placed on the web-site.

2. (c) various models, options and costings for the provision of in-house, technical heritage advice, that is independent of the Planning Departments, and which is prepared following:

- (i) **consultation with the City of Yarra’s Heritage Advisory Committee; and**
- (ii) **which has regard to the Heritage Victoria Guidelines on the appointment of heritage advisors, which can be found at [http://www.dtpli.vic.gov.au/data/assets/pdf_file/0010/244864/HERITAGE_ADVISORY SERVICE BRIEF 2014 August.pdf](http://www.dtpli.vic.gov.au/data/assets/pdf_file/0010/244864/HERITAGE_ADVISORY_SERVICE_BRIEF_2014_August.pdf)**

- (a) In this regard I had for comparative purposes obtained, Position Descriptions for the following positions:
 - (i) Heritage Co-Ordinator – City of Whittlesea;
 - (ii) Team Leader – Heritage – City of Melbourne;
 - (iii) Coordinator Heritage and Cultural Landscapes – City of Ballarat.
- (b) I attended the Heritage Advisory Committee meeting of 30 January specifically to discuss the matter in line with Council’s formal resolution and have also had three separate meetings with a representative group from that Committee. In this regard in consultation with the City of Yarra’s Heritage Advisory Committee, I have incorporated the information gathered into a draft position description (refer **Attachment 1**) which is constructed to address the principles from Council’s resolution above, i.e.:
 - (i) the development of practice guidelines, on the importance of heritage protection that could be made available at Council’s planning desk to assist residents, developers and others in planning applications; and
 - (ii) costings for the provision of in-house, technical heritage advice, that is independent of the Planning Department and which has regard to the Heritage Victoria Guidelines.

- (c) In respect of the Heritage Victoria Guidelines I note the following:
 - (i) the explanatory notes provided an outline of:
 - a. what is a heritage advisory service;
 - b. the role of heritage advisor and the provision of heritage advisory services,
 - c. the conduct, qualifications and duties of advisors; and
 - d. the appointment, remuneration, supervision and management of advisors.

External Consultation

- 4. Discussions have been pursued with representatives of other municipalities, Heritage Victoria and Council's Heritage Advisory Committee.

Internal Consultation (One Yarra)

- 5. Discussions have been undertaken with Officers from the Statutory and Strategic Planning Units, the Finance Unit, Communications Unit and the Executive Team.

Financial Implications

- 6. As reported to Council on 20 December last, there will be a financial implication arising from the proposal as requested.
- 7. The estimated costs of engaging an officer to fill a position suggested in the attached draft position description, which is considered to most closely provide for the roles envisaged by the Heritage Advisory Committee representatives would be around \$140,000. This figure would include salary and on-costs plus a working budget to undertake tasks envisaged.

Economic Implications

- 8. Not specifically relevant to this report.

Sustainability Implications

- 9. The reasoning which initiated this report is a high level of concern in sections of the Yarra community about the sustainability of the City's extensive heritage capital.
- 10. As part of the birthplace of development in early Melbourne, Yarra presents many rich and diverse (and in some instances, the only) examples of the evolution of building and architectural styles and thus, the retained history of Metropolitan Melbourne's and indeed Victoria's and Australia's built development.

Social Implications

- 11. It is considered critical to ensure that the many excellent examples of Melbourne's built history within the City of Yarra, are retained, protected and appropriately entwined into the evolutionary development of this diverse and iconic community.

Human Rights Implications

- 12. Not specifically relevant to this report.

Communications with CALD Communities Implications

- 13. Not specifically relevant to this report.

Council Plan, Strategy and Policy Implications

- 14. Council's policies and strategies include many references to heritage retention and Council has also incorporated further planning controls concerning heritage, into the planning scheme.

Legal Implications

- 15. It is necessary to note and reflect on, that the Council as a Responsible Authority under the Yarra Planning Scheme, is obligated to assess and approve planning applications in a prescribed manner, including:
 - (a) the matters set out in Section 60 of the Planning & Environment Act;

- (b) the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies;
 - (c) the purpose of the zone, overlay or other provision;
 - (d) any matter required to be considered in the zone, overlay or other provision;
 - (e) the orderly planning of the area;
 - (f) the effect on the amenity of the area;
 - (g) the proximity of the land to any public land;
 - (h) factors likely to cause or contribute to land degradation, salinity or reduce water quality;
 - (i) whether the proposed development is designed to maintain or improve the quality of storm-water within and exiting the site;
 - (j) the extent and character of native vegetation and the likelihood of its destruction;
 - (k) whether native vegetation is to be or can be protected, planted or allowed to regenerate; and
 - (l) the degree of flood, erosion or fire hazard associated with the location of the land and the use, development or management of the land so as to minimise any such hazard.
16. There may also be mandatory referrals required under the Planning Scheme (as relevant to the zones and overlays or other provision in the Scheme). For instance, an application for removal of vegetation and construction of a second dwelling on a lot (and extensions to an existing dwelling) may be on land that is zoned residential, which also has a:
- (a) Heritage Overlay;
 - (b) Land Subject to Inundation Overlay;
 - (c) Environmental Significance Overlay; and
 - (d) Design and Development Overlay.
17. Thus, apart from any mandatory external referrals under the Scheme (i.e. Melbourne Water must be referred applications in a Land Subject to Inundation Overlay), planning officers would obtain advice from our engineers (if there is a reduction of car parking or new car parking being provided), the heritage consultants, the open space unit, the ESD officer and/or urban design unit.
18. There may be advice from each internal referral which may directly compete with other advice (i.e. what is best for ESD may be in conflict with advice from Heritage; or what is best from engineers (to provide car parking on site) may be in conflict by requiring more vegetation removal or more destruction of heritage fabric).
19. Another example could be, a Mixed Use development (*tavern/restaurant, liquor licence after 1am, and dwellings*) for a 7 storey building within a Commercial 1 zone (*Heritage Overlay, Design and Development Overlay, Environmental Audit Overlay*), may include internal referrals from engineers, heritage, Social Policy unit, Community Amenity, acoustic experts, urban design. Notably, what is considered the best outcome for one area (*i.e. noise minimisation by closing up window openings*), may be contrary to heritage; or what is best for urban design would be contrary to engineering (*i.e. providing more car parking on site versus active frontages*).
20. In addition to formal Planning Scheme requirements, i.e. the purpose of the zone/overlays/particular provisions (*which includes matters such as liquor licences, car parking, advertising signage, etc.*), there may also be local policy implications (*i.e. Council's direction on certain applications*) which needs to be considered. A practical example would be: Yarra Council wants to promote less car parking – which is in reality, a contradiction of clause 52.06 which says you should provide ALL car parking on site – which on occasions, can be in contradiction of heritage concerns which is to retain heritage fabric.

21. Council as the Responsible Authority (or its delegated Officers) are obligated to seek, consider and process all of the above expert advice and referral information in a professional and timely manner as set by planning legislation, the Planning Scheme, VCAT guidelines and any Council policies in order to maintain the integrity of each of the planning process inputs and final determinations. Council and authorised Officers will be also endeavouring in their determinations, to protect the Council from costly legal challenge at VCAT.
22. Thus, overall the planning scheme language is frequently “subjective” which provides avenues for disputation and appeals.

Other Issues

23. In detailed discussions with representatives of the Heritage Advisory Committee it became very clear that in order to achieve the aspirations presented by the Committee and community representatives, there would be need for an additional staff resource. It was clearly agreed that having regard to the tasks sought to be undertaken by such a heritage specialist resource, it is not considered possible nor practical for the current budget and staff/contract resources to merely be re-directed to such as a Co-ordinator Heritage Services.

Options

24. In order to:
 - (a) encourage increased engagement between developers and community early in the planning process;
 - (b) re-write and expand the information display at the Town Hall Planning Branch in order to specifically highlight practice guidelines and the importance of respecting and retaining appropriate heritage infrastructure and buildings in any development processes in order to better assist developers and residents alike;
 - (c) develop and implement an ongoing training program to educate staff across the Organisation on the importance and value of heritage;
 - (d) provide a direct open and promotional “heritage advice” to community and developers; and
 - (e) address the options to have an in-house Heritage Coordinator reporting to the Group Manager – Chief Executive’s Office so as to be independent from the Planning Branch;it would be necessary to create a new position with a distinct position description.

Conclusion

25. That Council:
 - (a) note the report;
 - (b) note the proposed Position Description, prepared in consultation with representatives of the Heritage Advisory Committee specifically to incorporate the aspirations of the Committee and interested community members;
 - (c) note its previous resolution of 22 November 2016, referring the matter of appointing a heritage officer resource to the budget for consideration; andrefer the additional information now to hand for consideration in conjunction with its budget deliberations.

RECOMMENDATION

1. That Council:
 - (a) note the report;
 - (b) note the previous resolution (22 November 2016) to refer the matter of engaging an independent Heritage Advisor to budget for consideration;
 - (c) note that if the position is funded in the 2017/18 budget, the duties and responsibilities would align with the Position Description as per **Attachment 1** hereto and would:
 - (i) undertake a re-write and expansion of information material provided at the Council's Planning Offices in a manner to specifically highlight practice guidelines and the importance of respecting and retaining appropriate heritage infrastructure and buildings in any development processes in order to better assist developers and residents alike; and
 - (ii) undertake to implement the suggested process to encourage more consultation and engagement between developers and community at the earliest possible stage of the planning process; and
 - (d) note that if a Heritage Advisor (suggested title to be "Heritage Coordinator") was to be appointed, the position will be positioned in the Chief Executive Officer's Division and report to the Group Manager – Chief Executive's Office so as to be independent from the Planning Branch; and
 - (e) now refer the report and **Attachment 1** for consideration in conjunction with its budget deliberations.

CONTACT OFFICER: Ivan Gilbert
TITLE: Group Manager Chief Executive's Office
TEL: 9205 5110

Attachments

- 1 Coordinator City Heritage - Draft Position Description

Attachment 1 - Coordinator City Heritage - Draft Position Description

Yarra City Council Draft Position Description Coordinator, City Heritage

ATTACHMENT 1

Position Title	Coordinator, City Heritage	Position No	New position
Reports To	Group Manager – Chief Executive's Office	Classification	
Branch	Governance	Date	August 2017
Group	CEO's Office		

The City of Yarra is an inclusive, diverse, and flexible organisation.

Position Purpose

The Coordinator City Heritage will:

- be located in the Chief Executive's Office to distinguish that their role is distinct from current staff and consultants involved in heritage matters in the Organisation;
- champion the City's Heritage Strategy and its implementation, providing a platform for heritage in the City, including its value to communities and its role in urban renewal and city growth. Through assisting in the delivery of a significant heritage program to update and educate and consolidate the understanding of the city's history and development;
- be responsible for ensuring that best practice and innovative approaches are piloted and adopted, including consideration of pre-settlement and post settlement cultural heritage; and
- assist in the procurement of specialist heritage consultants as required, to enable independent expert advice to Council. They will work closely with the Strategic and Statutory Planning Units to ensure appropriate heritage protection of the City's built environment through appropriate policy development.

The role will provide leadership and high quality professional heritage advice to give effect to Council's goals for proactively managing the municipality's development in a manner which acknowledges, respects and protects Yarra's rich heritage.

The Coordinator City Heritage will:

- foster and develop knowledge and expertise in heritage, drawing from best practice in other cities nationally and internationally.
- Identify heritage issues and challenges, and develop, articulate and implement evidence-based heritage initiatives to achieve outcomes sought by the Council.
- Work collaboratively with the other staff Coordinators and Managers across the Organisation to provide leadership in the understanding and appreciation of heritage matters.
- Coordinate and ensure the delivery of heritage strategies and projects to the expected time, cost and quality outcomes and to provide direction for project managers and project team members in the implementation of the said strategies and projects.
- Respond to stakeholders with advice on conservation of buildings, areas and other places of cultural significance.
- Provide people management for any staff reporting to this position.

Position Context

The purpose of the position is to contribute to the design and planning of the city to respect and honour our heritage.

The role of the position is to effectively liaise with and contribute to all Branches which shape a vision for Yarra's future and to develop staff skill to guide the next generation of Yarra's growth towards a sustainable and smart city whilst respecting and honouring our heritage. The position will thus liaise and work collaboratively with staff in the following branches/units:

- Libraries
- Strategic Planning

Attachment 1 - Coordinator City Heritage - Draft Position Description

- Urban Design
- Statutory Planning
- Economic Development
- City Works and Assets (including Building Services)
- Advocacy & Engagement
- Planning & Place Making
- Arts and Culture

to deliver key Council Plan Actions referencing and protecting heritage.

The Coordinator will champion as directed from time to time, the City's Heritage Strategy and its implementation, providing a platform for heritage in the City of Yarra, including demonstrating its value to communities and its key role in urban renewal and city growth. Through the formulation and implementation of a significant heritage program to update and consolidate the understanding and appreciation of Yarra's history and development, the Coordinator will be responsible for ensuring best practice and innovative approaches to be piloted and adopted, including consideration of pre-settlement and post settlement cultural heritage. They will to afford improved understanding, appreciation and protection of the City's heritage.

Key Result Areas and Measures:

Strategic Heritage expertise

To have the understanding and skills where necessary, to:

- lead the development of advice to Council on all aspects of heritage strategy including in particular the development of the Yarra Planning Scheme;
- engage across all branches in Council on heritage matters, understand and educate others in regard to legislation and policy related to heritage;
- draw from understanding of best practice heritage reviews and policies, nationally and internationally and decisions emanating from VCAT decisions or Panel Reviews, including to propose potential approaches for Council's consideration;
- build on existing strong relationships and partnerships across the heritage sector to inform policy and approach;
- champion innovation in heritage review methodology, including integration of Indigenous land history;
- develop case studies to inform Council's approach;
- assist in the procurement of heritage consultants (where appropriate);
- assist in leading any Planning Scheme Amendment processes encompassing heritage reviews, and enable required protections for heritage places; and
- be the custodian of heritage data for the City, responsible for ensuring data accuracy and reporting on heritage status of individual properties and areas in line with Heritage Policies; maintaining and modifying the database to reflect current status.

Leadership and management

To have the understanding and skills to:

- ensure that staff administering the planning scheme are kept informed on heritage issues and practice, including VCAT and Panel decisions;
- lead, plan and manage heritage projects identified in the Council Plan as directed by the Group Manager to meet expected outcomes, expenditures and timeframes;

Attachment 1 - Coordinator City Heritage - Draft Position Description

<ul style="list-style-type: none"> • prepare high-level advice and recommendations to senior management and Councillors on heritage policy, and contribute expertise into municipality-wide and neighbourhood scale visions and plans; and • lead and manage collaborations, partnerships, joint initiatives and ventures as allocated from time to time, to implement heritage policies and programs to further Council's adopted objectives.
People leadership <ul style="list-style-type: none"> • Be accountable in respect of heritage matters for the operational performance of allocated projects and the delegation and resource allocation for heritage advice service delivery as directed from time to time. • Provide clear education and direction to staff and build high accountability concerning heritage matters, through strong performance management including the establishment of meaningful goals, open communication and the provision of fair and timely feedback for project and operational staff. • Ensure awareness of and implementation of the employee code of conduct. • Ensure standard work practices of high quality are in place across the project teams.
Business Planning, Budget Management and Reporting <ul style="list-style-type: none"> • Actively contribute to the relevant staff and teams to effectively resource relevant heritage projects across the organisation. • Manage and supervise the development of annual project portfolio plans and budgets as referred, scoping such projects effectively from the outset. • Collaborate with the Group Manager and other appropriate staff to develop and document the annual branch business plans in relation to heritage matters. • Coordinate and monitor the delivery of the annual branch business plans and delivery of Council Plan Actions in relation to heritage. • Track and report progress of key heritage projects to senior management and Council. • Liaise with the Group Manager regarding progress achievements against the Annual Plan throughout the year and assist in any adjustment to programming and/or resources with changing priorities. • Prepare, monitor and review project and Branch budgets in relation to heritage elements, and report in a timely and accurate manner.
Stakeholder Engagement <p>The position will develop and maintain relevant relationships within the City of Yarra and with a range of external stakeholders at all levels and gain their cooperation to achieve Council's objectives in heritage protection, using a high level of consultation, negotiation and facilitation skills.</p> <p>Internal</p> <ul style="list-style-type: none"> • Work closely with the Group Manager and the other senior staff across the organisation to deliver integrated and coordinated strategies for the city. • Prepare written reports to Council and Committees as required from time to time and make expert verbal and written advice and reports and briefings to Councillors and senior management as required for the delivery of heritage projects. • Develop and maintain close working relationships with key internal stakeholders to share heritage information and knowledge, and to ensure that the work of the Coordinator is relevant, responsive and supported by relevant parts of the organisation, to ensure the Council's heritage visions, strategies, plans and policies are deliverable.

Attachment 1 - Coordinator City Heritage - Draft Position Description

External	<ul style="list-style-type: none"> Proactively engage with a wide range of external organisations, consultants, urban developers, community groups (including the Heritage Advisory Committee) and the public, in the course of managing the delivery of strategic plans, , structure plans and projects in the Council Plan. Develop and maintain close working relationships with senior representatives of relevant Commonwealth, State and Local Government agencies, stakeholder groups, planning and design professionals to gain their input, support and assistance to develop and implement Council's heritage visions, strategies and plans. Make expert presentations to conferences, seminars and public forums as may be required. Build and maintain strong working relationships with people inside and outside the organisation as required by Management or Council. Provide advice to the Group Manager and management team on a range of internal and external stakeholder issues in relation to heritage matters. Ensure appropriate consultation frameworks and communication mechanisms are in place for effective delivery of the Council Plan program. Represent Council at information forums on relevant heritage projects and initiatives.
Environment – Communication / Judgement	<p>Communication</p> <p>The Coordinator, City Heritage will liaise and communicate effectively with a wide range of external and internal stakeholders and resolve complex problems using initiative and a proactive approach.</p> <p>With internal stakeholders, the Coordinator, Heritage is required to:</p> <ul style="list-style-type: none"> Communicate effectively verbally and in writing Engage with a broad range of disciplines to deliver integrated strategy and policy outcomes Consult and gain co-operation for projects across the City Prepare high quality written and illustrated reports as requested for internal and external purposes including Councillor briefings and Council and Committee meetings As directed by the Group Manager, represent the branch in projects and programs led by other branches of the City of Yarra. <p>With external stakeholders, the Coordinator, Heritage is required to:</p> <ul style="list-style-type: none"> consult and gain co-operation with external stakeholders relevant to key heritage projects Develop and maintain relationships with key external stakeholders. respond to stakeholders to advise on heritage policy, conservation of buildings, areas and other places of cultural significance.
Judgement	<ul style="list-style-type: none"> Exercise leadership and discretion in decision making in the effective delivery of outputs. Exercise a high level of autonomy as a member of the branch and be responsible and accountable for the majority of day to day work decisions on assigned tasks. The Coordinator works under the broad guidance of the Group Manager, and has substantial independence and accountability for the operational performance, delegation, resource allocation and planning for their area of responsibility. Be responsible for leading the delivery of allocated heritage projects and programs across the branch.

Attachment 1 - Coordinator City Heritage - Draft Position Description

- Ensure the Group Manager and senior executives are kept informed of activities, work progress and any unusual or emerging issues.

Desired knowledge/experience/qualifications:

- Relevant qualifications in cultural heritage, archaeology, strategic planning, urban design, architecture (or similar) and extensive experience in cultural heritage management and/ or advisory services.
- Excellent understanding of the planning system as it relates to heritage and its protection.
- Demonstrated experience in heritage reviews and the process involved in delivering a Planning Scheme Amendment.
- Demonstrated experience and ability in the development of planning schemes with preference for the Victorian system and legislation which govern heritage.
- Demonstrated ability to lead, develop and motivate a staff across the Organisation and as required - manage consultants effectively.
- High level of interpersonal skills in negotiation, persuasion, exercising discretion and judgement in a complex, administrative and political working environment.
- Demonstrated personal networks in the Melbourne heritage, planning or design community, and related public sector agencies, and demonstrated understanding of the development industry in Yarra.
- Demonstrated understanding of strategies to enable and manage future growth and urban renewal, and of the relationship between cultural heritage, transport policy, strategic urban planning, economic development and environmental sustainability.
- Demonstrated experience in working closely with communities (including the Heritage Advisory Committee) in the development of future plans in areas undergoing change via urban renewal or growth.
- Excellent understanding regarding the theory of cities, including interaction of the natural and built environment, land use, infrastructure and people, and ability to draw on national and international best practice to inform City of Yarra's work.
- Ability to demonstrate and display City of Yarra values – accountability, integrity, courage, respect for self and others, and striving for excellence in heritage management. This includes a personal commitment to ensuring the team embraces EO and diversity principles.
- Proven capability to contribute to skill development of team members by providing coaching, mentoring and support.
- High level of computer literacy including being proficient in the use and application of the Microsoft Office Suite and the ability to use online social, engagement and communication media.

Desirable:

- Knowledge of how contemporary architecture and uses can be harmonised with heritage places.

Draft Position Description Coordinator, City Heritage

12.1 Notice of Motion No 7 of 2017 Cr McEvoy re Bike Infrastructure Funding

Trim Record Number: D17/32263

Responsible Officer: Group Manager Chief Executive's Office

I, Councillor Mike McEvoy, hereby give notice that it is my intention to move the following motion at the Ordinary Meeting of Council to be held on 21 March 2017:

“That Council:

- (a) applaud the Victorian Government’s announcement on 13 October 2016 to support growing rates of cycling and walking in Victoria with the launch of Active Transport Victoria (ATV), and specifically, the prioritisation of infrastructure to keep cyclists and pedestrians safe through the \$100million Safe Cyclists and Pedestrians Fund;
- (b) note the commitment of the newly established ATV to “work with local Councils and communities to better coordinate the planning of infrastructure and ensure projects are built where they are needed most and delivered on time”;
- (c) note that the growing number of cyclists and pedestrians in Yarra contributes to population health improvements, a more efficient transport network with less congestion, and a reduction in carbon emissions, but that without an investment in infrastructure that separates cyclists and pedestrians from motor traffic there is an increased risk of injury and casualties to the most vulnerable road-users;
- (d) note the significant work in Yarra over many years to develop leading bike strategy and infrastructure, along with policies to encourage an increase in walking in Yarra and link with neighbouring councils;
- (e) highlight Yarra’s Bike Strategy adopted in 2009 and the City of Yarra Bike Strategy 2016 Refresh that list over 25 projects to the value of approximately \$20 million in priority order; and
- (f) request that the Mayor and Chief Executive Officer urgently seek a meeting with the Minister for Transport to ascertain what funding will be available from the promised Safe Cyclists and Pedestrian Fund to support projects ready to proceed in the 2017/18 Financial Year.”

RECOMMENDATION

1. That Council:

- (a) applaud the Victorian Government's announcement on 13 October 2016 to support growing rates of cycling and walking in Victoria with the launch of Active Transport Victoria (ATV), and specifically, the prioritisation of infrastructure to keep cyclists and pedestrians safe through the \$100million Safe Cyclists and Pedestrians Fund;
- (b) note the commitment of the newly established ATV to "work with local Councils and communities to better coordinate the planning of infrastructure and ensure projects are built where they are needed most and delivered on time";
- (c) note that the growing number of cyclists and pedestrians in Yarra contributes to population health improvements, a more efficient transport network with less congestion, and a reduction in carbon emissions, but that without an investment in infrastructure that separates cyclists and pedestrians from motor traffic there is an increased risk of injury and casualties to the most vulnerable road-users;
- (d) note the significant work in Yarra over many years to develop leading bike strategy and infrastructure, along with policies to encourage an increase in walking in Yarra and link with neighbouring councils;
- (e) highlight Yarra's Bike Strategy adopted in 2009 and the City of Yarra Bike Strategy 2016 Refresh that list over 25 projects to the value of approximately \$20 million in priority order; and
- (f) request that the Mayor and Chief Executive Officer urgently seek a meeting with the Minister for Transport to ascertain what funding will be available from the promised Safe Cyclists and Pedestrian Fund to support projects ready to proceed in the 2017/18 Financial Year.