



Agenda

Council Meeting

7.00pm, Tuesday 25 October 2022

Richmond Town Hall

Council Meetings

Council Meetings are public forums where Councillors come together to meet as a Council and make decisions about important, strategic and other matters. The Mayor presides over all Council Meetings, and they are conducted in accordance with the City of Yarra Governance Rules.

Council meetings are decision-making forums and only Councillors have a formal role. However, Council is committed to transparent governance and to ensuring that any person whose rights will be directly affected by a decision of Council is entitled to communicate their views and have their interests considered before the decision is made.

Question Time

Yarra City Council welcomes questions from members of the community.

Registration

To ask a question, you will need to register and provide your question by 7.00pm on the day before the meeting. Late registrations cannot be accepted, and you will be unable to address the meeting without registration.

Asking your question

During Question Time, the Mayor will invite everyone who has registered to ask their question. When your turn comes, come forward to the microphone and:

- state your name;
- direct your question to the Mayor;
- don't raise operational matters that have not been previously raised with the organisation;
- don't ask questions about matter listed on tonight's agenda
- don't engage in debate;
- if speaking on behalf of a group, explain the nature of the group and how you are able to speak on their behalf.

You will be provided a maximum of three minutes to ask your question, but do not need to use all of this time.

Comments not allowed

When you are addressing the meeting, don't ask a question or make comments which:

- relate to a matter that is being considered by Council at this meeting;
- relate to something outside the powers of the Council;
- are defamatory, indecent, abusive, offensive, irrelevant, trivial or objectionable;
- deal with a subject matter already answered;
- are aimed at embarrassing a Councillor or a member of Council staff;
- include or relate to confidential information; or
- relate to something that is subject to legal proceedings.

Addressing the Council

An opportunity exists to make your views known about a matter that is listed on the agenda for this meeting by addressing the Council directly before a decision is made.

Registration

To ask address Council, you will need to register by 7.00pm on the day before the meeting. Late registrations cannot be accepted, and you will be unable to address the meeting without registration.

Addressing the Council

Before each item is considered by the Council, the Mayor will invite everyone who has registered in relation to that item to address the Council. When your turn comes, come forward to the microphone and:

- state your name;
- direct your statement to the Mayor;
- confine your submission to the subject being considered;
- avoid repeating previous submitters;
- don't ask questions or seek comments from Councillors or others; and
- if speaking on behalf of a group, explain the nature of the group and how you are able to speak on their behalf.

You will be provided a maximum of three minutes to speak, but do not need to use all of this time.

Comments not allowed

When you are addressing the meeting, don't make any comments which:

- relate to something other than the matter being considered by the Council;
- are defamatory, indecent, abusive, offensive, irrelevant, trivial or objectionable;
- are aimed at embarrassing a Councillor or a member of Council staff;
- include or relate to confidential information; or
- relate to something that is subject to legal proceedings.

Arrangements to ensure our meetings are accessible to the public

Council meetings are held on the first floor at Richmond Town Hall. Access to the building is available either by the stairs, or via a ramp and lift. Seating is provided to watch the meeting, and the room is wheelchair accessible. Accessible toilet facilities are available. Speakers at the meeting are invited to stand at a lectern to address the Council, and all participants are amplified via an audio system. Meetings are conducted in English.

If you are unable to participate in this environment, we can make arrangements to accommodate you if sufficient notice is given. Some examples of adjustments are:

- a translator in your language
- the presence of an Auslan interpreter
- loan of a portable hearing loop
- reconfiguring the room to facilitate access
- modification of meeting rules to allow you to participate more easily

Recording and Publication of Meetings

A recording is made of all public Council Meetings and then published on Council's website. By participating in proceedings (including during Question Time or in making a submission regarding an item before Council), you agree to this publication. You should be aware that any private information volunteered by you during your participation in a meeting is subject to recording and publication.

Order of business

1. **Acknowledgement of Country**
2. **Attendance, apologies and requests for leave of absence**
3. **Announcements**
4. **Declarations of conflict of interest**
5. **Confidential business reports**
6. **Confirmation of minutes**
7. **Question time**
8. **Council business reports**
9. **Notices of motion**
10. **Petitions and joint letters**
11. **Questions without notice**
12. **Delegates' reports**
13. **General business**
14. **Urgent business**

1. Acknowledgment of Country

“Yarra City Council acknowledges the Wurundjeri Woi Wurrung people as the Traditional Owners and true sovereigns of the land now known as Yarra.

We acknowledge their creator spirit Bunjil, their ancestors and their Elders.

We acknowledge the strength and resilience of the Wurundjeri Woi Wurrung, who have never ceded sovereignty and retain their strong connections to family, clan and country despite the impacts of European invasion.

We also acknowledge the significant contributions made by other Aboriginal and Torres Strait Islander people to life in Yarra.

We pay our respects to Elders from all nations here today—and to their Elders past, present and future.”

2. Attendance, apologies and requests for leave of absence

Attendance

Councillors

- | | |
|-----------------------|--------------|
| • Cr Sophie Wade | Mayor |
| • Cr Edward Crossland | Deputy Mayor |
| • Cr Stephen Jolly | Councillor |
| • Cr Herschel Landes | Councillor |
| • Cr Anab Mohamud | Councillor |
| • Cr Claudia Nguyen | Councillor |
| • Cr Bridgid O’Brien | Councillor |
| • Cr Amanda Stone | Councillor |

Council officers

- | | |
|--------------------|--|
| • Sue Wilkinson | Chief Executive Officer |
| • Brooke Colbert | Group Manager Advocacy and Engagement |
| • Malcolm Foard | Director Community Wellbeing |
| • Ivan Gilbert | Group Manager Chief Executive’s Office |
| • Gracie Karabinis | Group Manager People and Culture |
| • Chris Leivers | Director City Works and Assets |
| • Wei Chen | Director Corporate, Business and Finance |
| • Mary Osman | Director Planning and Place Making |
| • Rhys Thomas | Senior Governance Advisor |
| • Mel Nikou | Governance Officer |

Leave of absence

- | | |
|--------------------------|------------|
| • Cr Gabrielle de Vietri | Councillor |
|--------------------------|------------|

3. Announcements

An opportunity is provided for the Mayor to make any necessary announcements.

4. Declarations of conflict of interest

Any Councillor who has a conflict of interest in a matter being considered at this meeting is required to disclose that interest either by explaining the nature of the conflict of interest to those present or advising that they have disclosed the nature of the interest in writing to the Chief Executive Officer before the meeting commenced.

5. Confidential business reports

Nil

6. Confirmation of minutes

RECOMMENDATION

That the minutes of the Council Meeting held on Tuesday 13 September 2022 be confirmed.

7. Question time

An opportunity is provided for questions from members of the public.

8. Council business reports

Item		Page	Rec. Page	Report Presenter
8.1	Proposal to Declare Land Abutting Sandeman Place Fitzroy as Public Highway	9	11	Ivan Gilbert - Group Manager Chief Executive's Office
8.2	Yarra City Council Annual Report 2021/2022	12	14	Jessica Matrakis - Communications and Engagement Lead
8.3	Mayors Park Tennis and Netball Centre Management	15	26	Sally Jones - Manager Recreation and Leisure Services
8.4	Heidelberg Road corridor - Permanent Built Form Provisions	27	48	Leonie Kirkwood - Practice Leader Strategic Planning
8.5	Draft Cremorne Urban Design Framework	50	67	Leonie Kirkwood - Practice Leader Strategic Planning
8.6	Victorian Government Service Standard Framework for Waste and Recycling	68	71	Oliver McNulty - Manager City Works
8.7	PPE22/0190 - Fitzroy Gasworks - 111 Queens Parade, North Fitzroy	72	124	Mary Osman - Director Planning and Place Making
8.8	Climate Emergency Plan mid-term review	147	153	Michael Oke – Sustainability Unit Manager
8.9	Audit and Risk Committee Biannual Activity Report	154	156	Rhys Thomas - Senior Governance Advisor

9. Notices of motion

Nil

10. Petitions and joint letters

An opportunity exists for any Councillor to table a petition or joint letter for Council's consideration.

11. Questions without notice

An opportunity is provided for Councillors to ask questions of the Mayor or Chief Executive Officer.

12. Delegate's reports

An opportunity is provided for Councillors to table or present a Delegate's Report.

13. General business

An opportunity is provided for Councillors to raise items of General Business for Council's consideration.

14. Urgent business

An opportunity is provided for the Chief Executive Officer to introduce items of Urgent Business.

8.4 Heidelberg Road corridor - Permanent Built Form Provisions

Executive Summary

Purpose

The purpose of this report is for Council to consider whether to:

- (a) Replace the current interim Design and Development Overlay – Schedule 18 (interim DDO18) with a permanent DDO18, amend Local Area Policy at Clause 11.03-1L Activity Centres, apply two new Heritage Overlays, delete the Heritage Overlay from a property and make subsequent changes to Incorporated and Background Documents;
- (b) Request consent from the Minister for Planning to prepare and exhibit draft Amendment C273 in accordance with the Terms of Reference for the Yarra Activity Centre Standing Advisory Committee, appointed under Section 151 of the *Planning and Environment Act 1987*; and
- (c) Request the Minister for Planning in accordance with section 8(1)(b) and section 20(4) of the *Planning and Environment Act 1987* extend the expiry date for interim DDO18.

Key Issues

Interim DDO18 applies to Commercial zoned land along Heidelberg Road and was approved by the Minister for Planning in September 2021. As it is due to expire after 22 April 2023, Council needs to progress permanent provisions to manage change and provide as much certainty as possible about future development outcomes.

Financial Implications

The costs associated with preparing strategic work to underpin permanent provisions; exhibition of the amendment and Advisory Committee fees have been considered within the Strategic Planning budget.

PROPOSAL

In summary, that Council:

- (a) adopt the proposed permanent built form provisions for the Heidelberg Road precincts, changes to the Heritage Overlay and the supporting documents which informed the provisions as the basis of a proposed amendment to the Yarra Planning Scheme;
- (b) request the Minister for Planning give consent to prepare and exhibit Draft Amendment C273yara in accordance with the Terms of Reference for the Yarra Activity Centre Standing Advisory Committee, appointed under Section 151 of the *Planning and Environment Act 1987*;
- (c) note that should the Minister for Planning determine not to proceed with an *Advisory Committee*, Council would proceed with the 'usual' full amendment process, seeking 'authorisation' from the Minister for Planning to exhibit an amendment; and
- (d) request the Minister for Planning extend the expiry date for interim DDO18 which applies to the Heidelberg Road Precincts for an extra 12 months while the permanent provisions are considered.

8.4 Heidelberg Road corridor - Permanent Built Form Provisions

Reference	D22/263303
Author	Casey Lord - Senior Strategic Planner
Authoriser	Director Planning and Place Making

Purpose

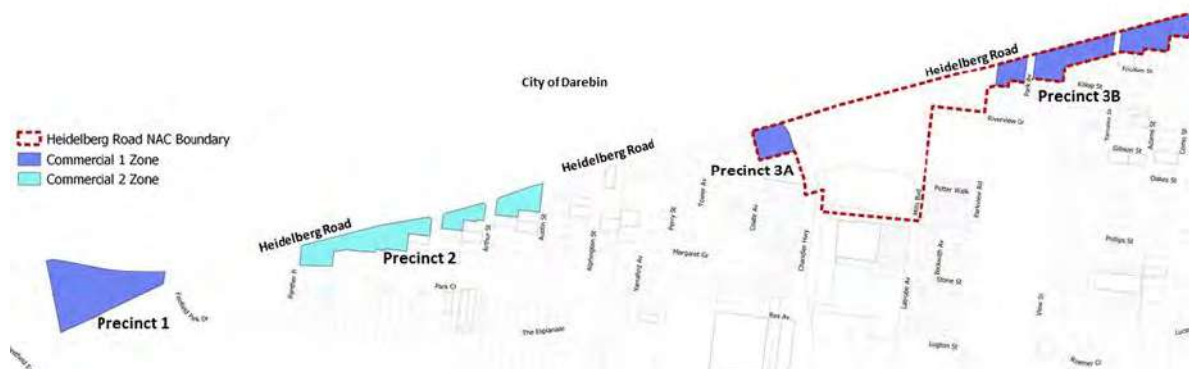
1. The purpose of the report is for Council to consider whether to:
 - (a) replace the current interim Design and Development Overlay – Schedule 18 (interim DDO18) with a permanent DDO18, amend Local Area Policy at Clause 11.03-1L Activity Centres, apply two new Heritage Overlays, delete the Heritage Overlay from a property and make subsequent changes to Incorporated and Background Documents;
 - (b) request consent from the Minister for Planning to prepare and exhibit draft Amendment C273 in accordance with the Terms of Reference for the Yarra Activity Centre Standing Advisory Committee, appointed under Section 151 of the Planning and Environment Act 1987; and
 - (c) request the Minister for Planning in accordance with section 8(1)(b) and section 20(4) of the Planning and Environment Act 1987 extend the expiry dates for interim DDO18.

Critical analysis

History and background

Context

2. Draft Amendment C273 applies to Commercial 1 and Commercial 2 zoned land along Heidelberg Road, Fairfield/Alphington. Part of the area is included in the Heidelberg Road Neighbourhood Activity Centre (NAC). See Location Map below.



Note: Proposed permanent Design and Development Overlay – Schedule 18 does not apply to the former Alphington Paper Mill (APM) site.

3. Plan Melbourne, the Yarra Housing Strategy (2018) and the recently adopted planning policy framework in Amendment C269 direct the majority of employment and housing growth in Yarra to activity centres.
4. The Yarra Housing Strategy (2018) notes a large amount of development is already occurring within the former Alphington Paper Mill (APM) site in the Heidelberg Road NAC.
5. The Yarra Spatial Economic and Employment Strategy (2018) supports growth in retail and other employment uses in the NAC and commercial areas along Heidelberg Road.
6. Council's adopted Clause 11.03-1L Activity Centres policy includes the objective '*to manage a sustainable network of activity centres that facilitate appropriate economic and housing growth and provide attractive places for social and community interaction*'.

7. Planning policy reinforces the importance of the NAC and surrounding commercial areas in accommodating growth while limiting any adverse impacts on established residential areas.
8. The former APM site is identified in Plan Melbourne as an 'urban renewal precinct' and is identified as a 'high change area' in Yarra's Housing Strategy and in adopted Clause 16.01-1L 'Location of residential development' of Amendment C269. Redevelopment of this site is guided by a site-specific Development Plan Overlay (DPO) and an approved Development Plan.
9. Land along Heidelberg Road in Fairfield and Alphington is experiencing development pressure due to its good access to Melbourne CBD, proximity to public transport and other employment and activity centres.
10. Recent planning permit applications and VCAT decisions show that development pressure is continuing to occur, within commercial areas outside the former APM site.
11. The communities in Fairfield and Alphington have expressed concerns about the scale and amount of development and have sought stronger planning provisions to achieve greater certainty and a balance between allowing some development, retaining a sense of place and protecting adjacent residential areas.

Strategic work – Draft LAP and Background technical studies

12. In 2019/20, officers prepared a draft Local Area Plan (draft LAP) in conjunction with officers from Darebin Council and draft interim planning provisions for the south side of Heidelberg Road (Yarra only).
 13. The approach was to prepare a common strategic framework, the Local Area Plan, for the Heidelberg Road Corridor and for Yarra to commence detailed work on built form and heritage provisions for those areas within the Yarra municipality.
 14. The draft LAP includes a strategic framework for the corridor and proposes the introduction of an interim Design and Development Overlay (DDO) for the Commercial 1 and Commercial 2 Zone areas and some Heritage Overlays along Heidelberg Road. The draft LAP provides key directions for future land use, transport, built form, public realm and housing changes in the commercial and industrial areas on both sides of Heidelberg Road.
 15. Detailed built form, heritage and traffic assessments were undertaken to inform the draft LAP and interim DDO18. This work included the:
 - (a) Heidelberg Road Heritage Review (Stage 1), RBA Architects Conservation Consultants 2019;
 - (b) Heidelberg Road Heritage Review (Stage 2), RBA Architects Conservation Consultants 2019;
 - (c) Part 1: Heidelberg Road Built Form Framework – Urban Context Analysis, Hodyl & Co, July 2019;
 - (d) Part 2: Heidelberg Road Built Form Framework – Design Strategy and Recommendations, Hodyl & Co, November 2019; and
 - (e) Traffic and Vehicle Access Assessment, Heidelberg Road, Fairfield/Alphington, Traffix Group, November 2019.
 16. Yarra's detailed built form, heritage and transport work is focussed on the commercial zones in the three precincts identified in the draft LAP along the south side of Heidelberg Road (noting that Precinct 3 is split into two sub-precincts):
 - (a) Precinct 1: Commercial 1 Zone land between Yarra Bend Road and Fairfield Park Drive;
 - (b) Precinct 2: Commercial 2 Zone land between Panther Place and Austin Street, Fairfield;
 - (c) Precinct 3A: Commercial 1 Zone land between Chandler Highway and Coate Avenue, part of the Heidelberg Road Neighbourhood Activity Centre (NAC); and
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- (d) Precinct 3B: Commercial 1 Zone land east of the former APM site between Parkview Road and Como Street, part of the Heidelberg Road NAC.
17. Council considered the draft LAP and interim DDO18 on 4 February 2020 and resolved to undertake future consultation on the LAP and request the Minister for Planning approve interim built form provisions.
18. Consultation was undertaken on the draft LAP and interim DDO in 2021 (see paragraph 67).
19. The draft LAP will be finalised once Darebin City Council has completed its detailed background work. Yarra City Council adopted the draft LAP as an interim position on 4 February 2020.

Interim Design and Development Overlay

20. At its meeting on 4 February 2020, Council resolved to request the Minister for Planning approve proposed interim DDO18 for commercial areas along Heidelberg Road, Fairfield and Alphington (via Planning Scheme Amendment C272).
21. It also requested the Minister consider the application of two interim Heritage Overlays (HO451 and HO455) at 730-734 Heidelberg Road, Alphington and at 760-764 Heidelberg Road, Alphington (both in Precinct 3B).
22. Amendment C272 was approved with changes. All requirements were approved as discretionary provisions (whereas Council had requested a mix of mandatory and discretionary requirements - see Tables 1-4 for further information). The requested interim Heritage Overlays were not supported.
23. Interim DDO18 came into effect on 22 October 2021.
24. As the interim DDO will expire on 22 April 2023, Council needs to progress permanent provisions to ensure interim DDO18 remains in place while permanent provisions are progressed.

Discussion

Review of interim DDO18

25. Officers have undertaken a review of the interim DDO to test its suitability for translation into permanent provisions and recommend necessary refinements. The review included:
- (a) Consideration of consultation feedback from 2021 (see Community and Stakeholder Engagement section of this report below);
 - (b) Feedback on interim DDO18 from relevant Council Units, including Statutory Planning and Strategic Transport;
 - (c) Consideration of Victorian Civil and Administrative Tribunal (VCAT) cases within the area to enhance the clarity and workability of the provision;
 - (d) Learnings from other recently adopted amendments (such as C291 – Bridge Road and Victoria Street, and C293 - Collingwood South Precinct); and
 - (e) Ensuring it will achieve the development outcomes sought for the precincts.

Proposed Planning Scheme Amendment C273

26. The proposed draft amendment consists of the following elements. It:
- (a) Amends Local Area Policy at Clause 11.03-1L Activity Centres and inserts a new section on the Heidelberg Road Neighbourhood Activity Centre to guide built form within the activity centre (see Attachment 1).

Note: This aspect of the amendment relies on the approval of Amendment C269 (planning policy framework). If Amendment C269 is not approved before exhibition on C273 commences, the updated policy would be included in existing Clause 21.08 Neighbourhoods - Fairfield - Alphington;

- (b) Replaces interim Design and Development Overlay Schedule 18 (interim DDO18) with permanent Design and Development Overlay Schedule 18 (DDO18) which applies to the land zoned Commercial (C1Z and C2Z) along Heidelberg Road (see Attachment 2);
- (c) Applies a Heritage Overlay - HO451 to 730-734 Heidelberg Road, Alphington (see Attachment 3 for the Statement of Significance);
- (d) Applies a Heritage Overlay - HO455 to 760-764 Heidelberg Road, Alphington (see Attachment 3 for the Statement of Significance);
- (e) Deletes Heritage Overlay HO362 from 2 Killop Street, Alphington (see Attachment 1 for the Deletion Map);
- (f) Amends the Schedule to Clause 43.01 Heritage Overlay to include HO451 and HO455 (see Attachment 1);
- (g) Amends the Schedule to Clause 72.04 Incorporated Documents to include “The Post Office and the group of shops on Heidelberg Road Statement of Significance” and “Cooper Knitting Factory (former) Statement of Significance” (see Attachment 1) and updates the “City of Yarra Database of Heritage Significant Areas” (see Attachment 1);
- (h) Amends the Schedule to Clause 72.08 Background Documents (see Attachment 1) to include:
 - (i) Heidelberg Road Heritage Review (Stage 1), RBA Architects Conservation Consultants 2019 (see Attachment 4);
 - (ii) Heidelberg Road Heritage Review (Stage 2), RBA Architects Conservation Consultants 2019 (see Attachment 5);
 - (iii) Part 2: Heidelberg Road Built Form Framework – Design Strategy and Recommendations, Hodyl & Co, November 2019 (see Attachment 6);
 - (iv) Traffic and Vehicle Access Assessment, Heidelberg Road, Fairfield/Alphington, Traffix Group, November 2019 (see Attachment 7);
- (i) Amends Planning Scheme Map Nos. 3DDO and 4DDO by replacing interim DDO18 with a permanent DDO18 (see Attachment 1); and
- (j) Amends Planning Scheme Map No. 4HO by applying HO451 and HO455 and deleting HO362 from the property at 2 Killop Street, Alphington (see Attachment 1).

Update to Clause 11.03-1L Activity Centres

27. Clause 11.03-1L Activity Centres (under the Heidelberg Road, Alphington subheading) would include strategies to reinforce expectations around use and development in the NAC (see Attachment 1). The updated policy is informed by the built form framework, development plan for the former APM site and draft LAP.

Update to Schedule to Clause 72.04 Incorporated Documents

28. Statements of Significance for new heritage overlays must be included in the Planning Scheme as incorporated documents (as outlined in *Planning Practice Note 1 – Applying the Heritage Overlay*). The following Statements of Significance will be added to the list of Incorporated Documents in the Schedule to Clause 72.04 (see Attachment 3 for the Statements of Significance and Attachment 1 for the Schedule to Clause 72.04):
- (a) The Post Office and the group of shops on Heidelberg Road Statement of Significance; and
 - (b) Cooper Knitting Factory (former) Statement of Significance.
29. Updates to the Incorporated Document - “City of Yarra Database of Heritage Significant Areas” are also required to reflect the proposed changes to the Heritage Overlay. The document date will be updated and the new heritage overlays and their gradings will be added (See Attachment 1).

Update to Schedule to Clause 72.08 Background Documents

30. The Schedule to Clause 72.08 Background Documents will be updated to reference the following documents (See Attachment 1):
- (a) Heidelberg Road Heritage Review (Stage 1), RBA Architects Conservation Consultants 2019;
 - (b) Heidelberg Road Heritage Review (Stage 2), RBA Architects Conservation Consultants 2019;
 - (c) Part 2: Heidelberg Road Built Form Framework – Design Strategy and Recommendations, Hodyl & Co, November 2019; and
 - (d) Traffic and Vehicle Access Assessment, Heidelberg Road, Fairfield/Alphington, Traffix Group, November 2019.
31. This change is required to ensure relevant background work that informed the amendment is referenced in the planning scheme and can be used to inform planning decisions, when needed. (see Attachments 4 to 7).

Proposed Heritage Overlays

730-734 Heidelberg Road, Alphington (group of 3 shops)

32. The group of three shops at 730-734 Heidelberg Road, Alphington (Figures 1 and 2) are of historical and aesthetic significance to the City of Yarra as one of the few remnant intact buildings from the Interwar period phase of development in this part of Heidelberg Road (Alphington Village). Whilst modest in scale, they retain their original parapets and unusually their original shopfronts.



Figure 1: 730 Heidelberg Road



Figure 2: 732-734 Heidelberg Road

760-764 Heidelberg Road, Alphington

33. Initially constructed as three premises in 1922, the single storey building at 760-764 Heidelberg Road, Alphington (Figure 3), was expanded and later consolidated by A. Cooper Knitting Manufacturer. It is representative of the commercial and industrial development that occurred during the Interwar period along Heidelberg Road, and was one of several knitting enterprises that were established along Heidelberg Road at this time. The expansion of the building during the late 1930s and early 1940s is indicative of the important role of the local knitting industry during WWII. The single storey building is aesthetically significant as an intact example of an Interwar period building constructed on a prominent corner site. It is distinguished by its parapet and projecting piers. The original pattern of openings, shopfront division, and canopy remains mostly intact.



Figure 3: 760-764 Heidelberg Road



Figure 4: 2 Killop Street

2 Killop Street, Alphington

34. 2 Killop Street (Figure 4) is located in one of the small peripheral sections of HO362. The late 20th century house at 2 Killop Street was identified as an anomaly at the edge of the Alphington East Precinct which is largely characterised by Victorian, Edwardian and Interwar houses with pitched, gabled or hipped roofs. It varies greatly from the graded building stock in the precinct and it is recommended to be removed from the precinct on this basis in the heritage advice.

Proposed Permanent Design and Development Overlay Schedule (DDO18)

35. The key tool within the Victorian Planning Provisions that Councils can use to locally guide built form and design of new development is a Design and Development Overlay (DDO).
36. A DDO is a complementary planning tool to the zone and is one of a number of planning provisions in addition to policy which must be taken into account when determining a planning permit application.
37. It cannot act as a defacto heritage control, manage land use or address other issues such as shop vacancies or housing affordability. It can only focus on design and development outcomes in the context of the overall policy framework and other relevant provisions.
38. The proposed DDO18 seeks to provide a balance between allowing a certain level of new development in the commercial areas along Heidelberg Road and protecting existing heritage buildings (including those proposed in C273), sensitive residential and parkland interfaces.
39. The DDO schedule is informed by the Built Form Framework and heritage built form recommendations to address the unique built form character of each precinct.
40. The proposed permanent DDO18 is based on sound strategic background work and learnings from recent amendments, such as C191, C291 and C293.

General changes to interim DDO18

41. Updates and changes to interim DDO18 include:
- (a) refining the wording to increase the clarity of requirements and alignment with recent amendment work;
 - (b) introducing a balance of mandatory controls and discretionary requirements to guide built form elements to ensure good outcomes towards sensitive residential interfaces, parkland and towards heritage buildings and the public realm (see Tables 1-4);
 - (c) taking recent planning context, such as recent amendments and VCAT decisions, and approach to heritage provisions into account;
 - (d) refining the design requirements to ensure good pedestrian oriented and façade outcomes; and

- (e) updating maps to communicate the built form provisions more clearly.

42. See Attachment 2 for the proposed permanent DDO18.

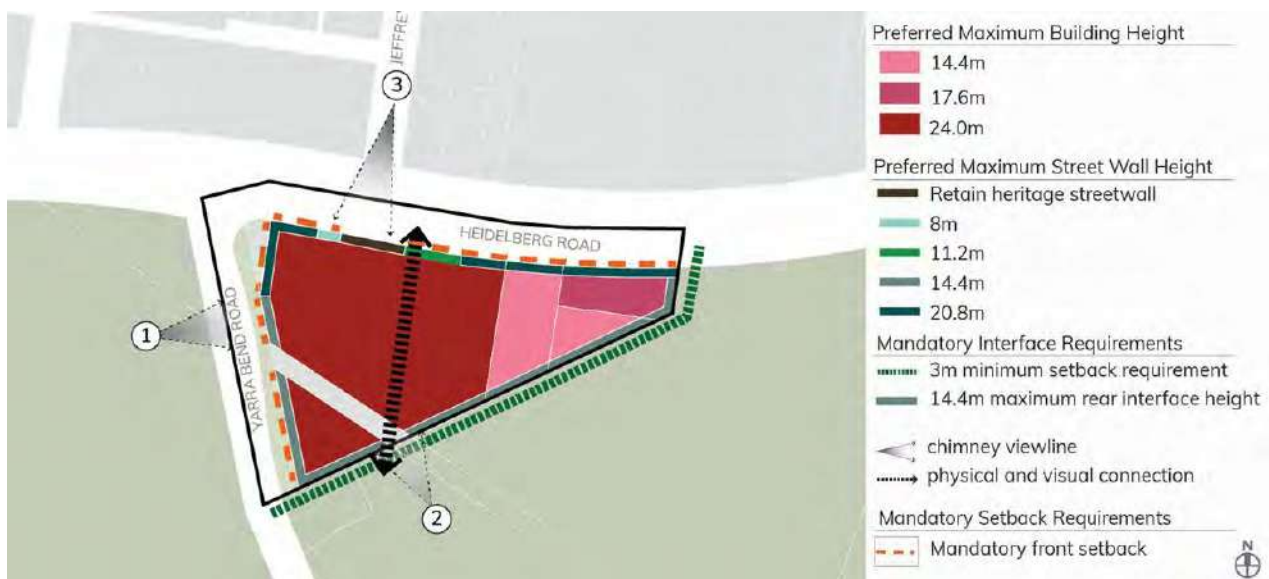
Recommended built form requirements and controls

43. The proposed permanent DDO18 would guide new development by:

- (a) requiring a 3 metre front setback to significantly improve pedestrian amenity along Precincts 1, 2, 3A (noting that a 4.5 metre front setback is required to Coate Avenue) and 3B (between Parkview Road and Park Avenue);
- (b) framing the Heidelberg Road corridor with a street wall edge of generally 2-4 storeys to create distinction between lower and upper levels and maintain pedestrian scale;
- (c) allowing for a more robust street wall where the corridor significantly widens in Precincts 1 and 3A;
- (d) retaining a sense of openness through upper-level setbacks of generally 6 metres towards Heidelberg Road;
- (e) allowing for building heights, generally between 4-5 storeys in Precincts 2 and 3B;
- (f) allowing for building heights (on selected larger sites) of up to 7 storeys in Precinct 1 (within the Porta site) and 8 storeys in Precinct 3A (for the section of the site at the corner of Heidelberg Road and Chandler Highway) (ensuring building heights remain subordinate to development on the former APM site);
- (g) setting bulk away from residential and parkland interfaces to minimise amenity impacts and visibility, mainly by:
 - (i) ground floor setback requirements where existing dwellings are within 15m or less of a rear boundary;
 - (ii) an 8 metre maximum boundary/rear wall; and
 - (iii) development to be contained within a 45-degree angle above; and
- (h) including numerous design requirements to ensure good pedestrian outcomes, well-designed facades and sensitive responses towards heritage buildings.

44. Tables 1-4 below compare the main built form parameters from the approved interim DDO18 with the proposed permanent DDO18. The main differences are in relation to which parameters are recommended as mandatory, given the approved interim DDO18 has no mandatory controls. In the tables: 'M' – identifies a mandatory control and 'D' – identifies a discretionary provision.

Table 1: Precinct 1



Design Strategy – Source: Heidelberg Road Built Form Framework				
Creation of a mid-rise precinct that frames Heidelberg Road and steps down towards the adjacent parks to maintain the prominence of the landscape setting. The Porta heritage building is retained, views to the brick chimney are enhanced through sensitive redevelopment and a new north-south pedestrian connection links Heidelberg Road to the park.				
Require-ments	Council adopted interim DDO18 4 Feb 2020	Gazetted Interim DDO18	Proposed Permanent DDO18	Justification
Building height	Porta: 24m – M Remaining: 14.4m-17.6m – D Council resolution on 4 Feb 2020: reduce max height of Porta to 7 storeys and 5 storeys for remaining parts.	Porta: 24m – D Remaining: 14.4m-17.6m - D	Porta: 24m – D Remaining: 14.4m-17.6m - D	<p>The heights were reduced by Council on 4 February 2020 and approved in the interim DDO18.</p> <p>It is recommended the lower heights are retained as discretionary and additional criteria for proposals that exceed the preferred maximum height are applied to assess additional offsite impacts, such as overshadowing and visual bulk.</p> <p>Proposals exceeding the preferred height would need to achieve:</p> <ul style="list-style-type: none"> • increased separation distances; • higher ESD standards; • no additional overshadowing of sensitive interfaces; and • increased private and communal open space (where the proposal includes dwellings). <p>This approach is consistent with other recent amendments. It allows for the assessment of a proposal and its alignment with built form outcomes sought by the DDO.</p> <p>The VCAT case for the site at 224-256 Heidelberg Road, Fairfield (VCAT Reference P760/2021), gave considerable statutory weight to the heights in the interim DDO18 and DDO1 (Yarra (Birraring) River Corridor) in upholding Council’s refusal of the application.</p>
Street wall height	8m-20.8m – D	8m-20.8m - D	8m-20.8m - D	<p>Recommended in the built form framework prepared by Hodyl & Co. and approved in the interim DDO18.</p> <p>Creates a well-defined street edge which does not visually dominate in relation to the width of the corridor.</p> <p>In combination with the front setback, the six-storey street wall towards the north-western corner provides a more balanced street wall height against the width of the Heidelberg Road corridor and Yarra Bend Road intersection. The street wall steps down towards the heritage building to retain the sense of openness around it.</p>
Front Setback to street(s)	3m to Heidelberg Rd (except for heritage buildings) and Yarra Bend Rd - M	3m to Heidelberg Rd (except for heritage buildings) and Yarra Bend Rd - D	3m to Heidelberg Rd (except for heritage buildings) and Yarra Bend Rd - M	<p>Recommended in the built form framework prepared by Hodyl & Co. and approved in the interim DDO18 (as discretionary).</p> <p>Officers recommend the setback requirement is applied as a mandatory minimum to ensure a uniform street edge along with the robust street wall height.</p> <p>The setback also supports opportunities for street activation and significantly improved pedestrian amenity and access through landscaping, inclusion of outdoor seating and trading space.</p> <p>The VCAT case for the site at 224-256 Heidelberg Road, Fairfield (VCAT Reference P760/2021) gave considerable statutory weight to the setback requirements towards Yarra Bend Road in the interim DDO18 in upholding Council’s refusal of the application.</p>
Minimum upper-level setback	Upper-level setback above Heidelberg Rd, Yarra Bend Rd	Upper-level setback above Heidelberg Rd, Yarra	Upper-level setback above Heidelberg Rd, Yarra	<p>Recommended in the built form framework prepared by Hodyl & Co. and approved in the interim DDO18.</p> <p>The balance between street wall height and a 6m upper-level setback would achieve well-proportioned buildings where the upper levels form a recessed, lighter element above a solid base</p>

	street walls: 6m – M	Bend Rd street walls: 6m – D	Bend Rd street walls: 6m – D	building form. Officers recommend this is applied as a discretionary requirement as the reduced building height means the upper levels are a comparatively small element compared to the lower levels.
Maximum rear interface height	14.4m or 4 storeys height at park interface - M	14.4m or 4 storeys height at park interface – D	14.4m or 4 storeys height at park interface - M	Recommended in the built form framework prepared by Hodyl & Co. as mandatory and approved in the interim DDO18 as discretionary. Officers recommend a mandatory control to protect parkland from unreasonable overshadowing and reduce visual bulk when viewed from the parkland.
Upper-level setbacks (above rear interface)	Envelope to be contained within 45-degree angle above - M	Envelope to be contained within 45-degree angle above – D	Envelope to be contained within 45-degree angle above - M	The mandatory controls provide an appropriate balance between creating an urban edge and activation (e.g. by providing surveillance of the car park area) and ensuring that the buildings are set within the landscape and are not the dominant feature. This is demonstrated by the existing four-storey apartment development which sits within the scale of the large canopy trees.
Minimum rear boundary setback	3m ground floor rear boundary setback from parklands - M	3m ground floor rear boundary setback from parklands – D	3m ground floor rear boundary setback from parklands - M	The VCAT case for the site at 224-256 Heidelberg Road, Fairfield (VCAT Reference P760/2021) gave considerable statutory weight to the requirements in the interim DDO18 and DDO1 in upholding Council's refusal of the application. The view lines identified in the built form framework and Map 1 of interim DDO18 were also of importance in VCAT's decision.

Table 2: Precinct 2



Design Strategy - Source: Heidelberg Road Built Form Framework

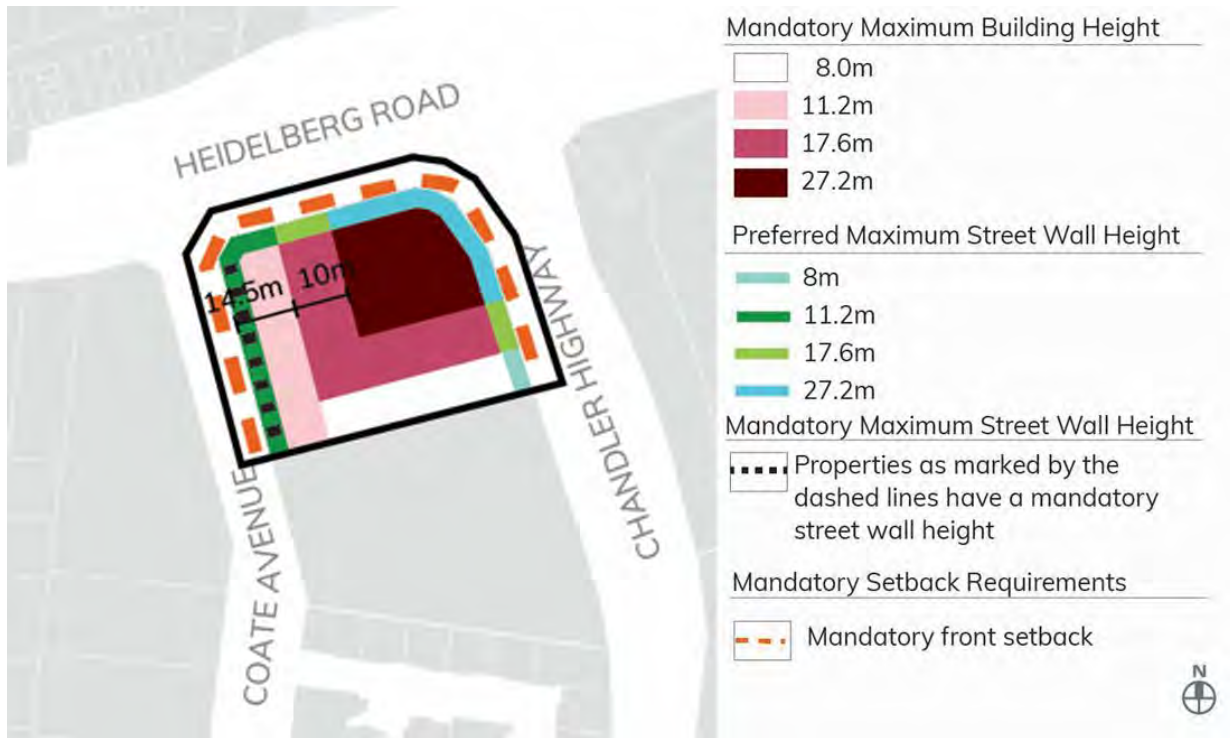
Creation of a mid-rise, commercial precinct that frames Heidelberg Road with active uses and additional greening opportunities.

Requirements	Council adopted interim DDO18 4 Feb 2020	Gazetted Interim DDO18	Proposed Permanent DDO18	Justification
Height of the building/s	16m - 20m (4 to 5 storeys) – M Council: reduce max height from 5 and 6 storeys (20 to 24m) to 4 and 5 storeys (16 to 20m) (except for sites	16m - 20m (4 to 5 storeys) - D	16m - 20m (4 to 5 storeys) - D	The heights were reduced by Council on 4 February 2020 and approved in the interim DDO18. It is recommended the lower heights are retained as discretionary and additional criteria for proposals that exceed the preferred maximum height are applied to assess additional offsite impacts, such as overshadowing and visual bulk. Proposals exceeding the preferred height would need to achieve:

	proposed to be affected by interim HO).			<ul style="list-style-type: none"> increased separation distances; higher ESD standards; and no additional overshadowing of sensitive interfaces. <p>This approach is consistent with other recent amendments. It allows for the assessment of a proposal and its alignment with built form outcomes sought by the DDO.</p>
Street wall height	16m to Heidelberg Rd – M 8m to Park Cres – M 16m to side streets - M	16m to Heidelberg Rd D 8m to Park Cres – D 16m to side streets - D	16m to Heidelberg Rd – M 8m to Park Cres - M 16m to side streets - D	<p>The built form framework prepared by Hodyl & Co. recommended the application of this requirement as a mandatory requirement.</p> <p>In combination with the ground floor setback, the four-storey street wall to Heidelberg Road would provide for a balanced street wall edge against the width of Heidelberg Road corridor. A two storey street wall would apply to Park Crescent.</p>
Front Setback to street(s)	3m to Heidelberg Rd and Park Cres - D	3m to Heidelberg Rd and Park Cres - D	3m to Heidelberg Rd and Park Cres - M	<p>Recommended in the built form framework prepared by Hodyl & Co. and approved in the interim DDO18 (as discretionary).</p> <p>Officers recommend the setback requirement is applied as a mandatory minimum to ensure a uniform street edge along with the street wall height.</p> <p>The setback also supports opportunities for street activation and a significantly improved pedestrian amenity and access through landscaping, inclusion of outdoor seating and trading space.</p>
Minimum upper-level setback	6m from frontage streetwall (Heidelberg Rd and Park Cres – M 3m from other side street - D	6m from frontage streetwall (Heidelberg Rd and Park Cres) and 3m from other side street - D	6m from frontage streetwall (Heidelberg Rd and Park Cres) – D 3m from other side street - D	<p>Recommended in the built form framework prepared by Hodyl & Co. and approved in the interim DDO18.</p> <p>The balance between street wall height and a 6m upper-level setback would achieve well-proportioned buildings where the upper levels form a recessed, lighter element above a solid base building form.</p> <p>Officers recommend this is applied as a discretionary requirement as the reduced building height means the upper levels are a comparatively small element compared to the lower levels.</p> <p>The 3m discretionary setback towards side streets is appropriate as it minimises overshadowing of opposite footpaths and retains a sense of openness for the side streets.</p>
Maximum rear interface height	Development adjoining a residential property outside the overlay should not exceed a max boundary wall height of 8m - D	Development adjoining a residential property outside the overlay should not exceed a max boundary wall height of 8m – D	Development adjoining a residential property outside the overlay should not exceed a max boundary wall height of 8m – M	<p>Recommended in the built form framework prepared by Hodyl & Co. as mandatory and approved in the interim DDO18 as discretionary.</p> <p>Officers recommend the rear interface requirements are applied as mandatory requirements to ensure residential properties are protected from overshadowing and overlooking, and visual bulk is minimised when viewed from the residential properties to the south.</p> <p>The built form framework demonstrated these requirements are necessary to ensure appropriate outcomes are achieved and sensitive spaces to the</p>

Upper-level setbacks (above rear interface)	Envelope to be contained within 45-degree angle above - M	Envelope to be contained within 45-degree angle above - D	Envelope to be contained within 45-degree angle above - M	south are not unreasonably impacted by new development.
Minimum rear boundary setback	A minimum of 3 metres if the dwelling on the adjacent residential property is located less than 15 metres from the property boundary - M	A minimum of 3 metres if the dwelling on the adjacent residential property is located less than 15 metres from the property boundary - D	A minimum of 3 metres if the dwelling on the adjacent residential property is located less than 15 metres from the property boundary - M	

Table 3: Precinct 3A



Design Strategy - Source: Heidelberg Road Built Form Framework

Provide a well-designed mid-rise, mixed-use building that marks the prominent corner location and respects the character of the neighbourhoods to the south and west. Incorporate a landscape setback to all boundaries to provide an attractive, garden setting to Coate Avenue and the southern boundary and to significantly improve the pedestrian experience to Heidelberg Road and Chandler Highway.

Requirements	Council adopted interim DDO18 4 Feb 2020	Gazetted Interim DDO18	Proposed Permanent DDO18	Justification
Height of the building/s	27.2m – M Note: at corner of	27.2m – D Note: at corner of	27.2m – M Note: at corner of	Recommended in the built form framework prepared by Hodyl & Co. as discretionary and approved in the interim DDO18 as discretionary.

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	<p>Heidelberg Rd & Chandler Hwy</p> <p>17.6m – M (centre of the site)</p> <p>11.2m – M (Coate Avenue)</p> <p>Council: Ensure that recent Council and VCAT decisions regarding overall heights and transitioning to rear properties 582 H'Berg Rd are reflected in future documents.</p>	<p>Heidelberg Rd & Chandler Hwy</p> <p>17.6m –D (centre of the site)</p> <p>11.2m – D (Coate Avenue)</p>	<p>Heidelberg Rd & Chandler Hwy</p> <p>17.6m – M (centre of the site)</p> <p>11.2m – M (Coate Avenue)</p>	<p>Unlike the former APM site, this site is not identified as a 'high change area' in the Strategic Housing Framework Plan (adopted Clause 16.01-1L) in C269. There is a need to ensure that any development strikes a balance between providing a response to the very wide road corridors, the site's location in the NAC next to the former APM site and the need to transition down towards the low-rise residential areas to the south and west.</p> <p>The VCAT decision in relation to the site has been considered, however as the decision in 2018 provided no specific recommendation around height, the officers have relied on the Built Form Framework.</p> <p>Officers recommend a mandatory height so that the form steps down from the former APM site towards the low-rise residential area.</p>
Street wall height	<p>8m-27.2m to Chandler Hwy & Heidelberg Rd – M</p> <p>11.2m to Coate Avenue – M</p>	<p>8m-27.2m to Chandler Hwy & Heidelberg Rd – D</p> <p>11.2m to Coate Avenue - D</p>	<p>8m-27.2m to Chandler Hwy & Heidelberg Rd – D</p> <p>11.2m to Coate Avenue – M</p>	<p>Recommended in the built form framework prepared by Hodyl & Co. as mandatory and approved in the interim DDO18 as discretionary.</p> <p>Officers recommend a mandatory requirement is applied along Coate Avenue to ensure the street wall respects the low-rise residential character. This would operate alongside the precinct-specific upper-level setbacks.</p> <p>Officers recommend a discretionary requirement applies along Heidelberg Road and Chandler Highway. These frontages on the wider section of the corridor can accommodate a more robust street wall.</p> <p>Along with the front setbacks, this would achieve a sense of enclosure and definition to the street edge while not visually dominating the street.</p> <p>The proposed requirements generally align with Council's position during the VCAT case for 582 Heidelberg Road (VCAT Reference P1558/2018).</p>
Front Setback to street(s)	<p>3m to Heidelberg Rd and Chandler Highway - M</p> <p>4.5m to Coate Ave – M</p>	<p>3 metres to Heidelberg Road and Chandler Highway- D</p> <p>4.5m to Coate Ave – D</p>	<p>3m to Heidelberg Rd and Chandler Highway - M</p> <p>4.5m to Coate Ave – M</p>	<p>Recommended in the built form framework prepared by Hodyl & Co. and approved in the interim DDO18 (as discretionary).</p> <p>Officers recommend the setback requirement as a mandatory minimum to ensure an improved street edge, along with the street wall height.</p> <p>The setback also supports opportunities for street activation and a significantly improved pedestrian amenity and access through landscaping, inclusion of outdoor seating and trading space.</p> <p>The measurements generally align with Council's position during the VCAT case for 582 Heidelberg Road (VCAT Reference P1558/2018).</p>
Minimum upper-level setback	<p>6m from Heidelberg Rd and Chandler</p>	<p>6m from Heidelberg Rd and</p>	<p>6m from Heidelberg Rd and Chandler</p>	<p>Recommended in the built form framework prepared by Hodyl & Co. and approved in the interim DDO18.</p> <p>The balance between street wall height and a 6m upper-</p>

	Hwy Street wall – D 10 metres from Coate Ave streetwall, further 10 metres above secondary street wall – M	Chandler Hwy Street wall – D 10 metres from Coate Ave streetwall, further 10 metres above secondary street wall – D	Hwy Street wall – D 10 metres from Coate Ave streetwall, further 10 metres above secondary street wall – M	level setback would achieve well-proportioned buildings where the upper levels are a recessed, lighter element above a solid base building form. Officers recommend this is applied as a discretionary requirement towards Heidelberg Road/Chandler Highway as the reduced building height means the upper levels form a comparatively small element compared to the lower levels. Mandatory upper-level setbacks towards Coate Avenue are recommended to ensure a development appropriately transitions down towards the residential character. The measurements generally align with Council's position during the VCAT case for 582 Heidelberg Road (VCAT Reference P1558/2018).
Maximum rear interface height	Development adjoining a residential property outside the overlay should not exceed a max boundary wall height of 8m – D	Development adjoining a residential property outside the overlay should not exceed a max boundary wall height of 8m – D	Development adjoining a residential property outside the overlay should not exceed a max boundary wall height of 8m – M	Recommended in the built form framework prepared by Hodyl & Co. as mandatory and approved in the interim DDO18 as discretionary. Officers recommend the rear interface requirements are applied as mandatory requirements to ensure residential properties are protected from overshadowing and overlooking, and visual bulk is minimised when viewed from the residential properties to the south. The built form framework demonstrated that these requirements are necessary to ensure appropriate outcomes are achieved and sensitive spaces to the south are not unreasonably impacted by new development.
Upper-level setbacks (above rear interface)	Envelope to be contained within 45-degree angle above - M	Envelope to be contained within 45-degree angle above - D	Envelope to be contained within 45-degree angle above - M	The measurements generally align with Council's position during the VCAT case for 582 Heidelberg Road (VCAT Reference P1558/2018).
Minimum rear boundary setback	4.5m setback – M	4.5m setback – D	4.5m setback – M	

Table 4: Precinct 3B



Design Strategy - Source: Heidelberg Road Built Form Framework

Develop a new mid-rise character for the existing neighbourhood centre which complements the scale and facilities in the former Alphington Paper Mills site. Enhance the setting of heritage buildings and the fine-grain development patterns through a low-street wall height.

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Requirements	Council adopted interim DDO18 4 Feb 2020	Gazetted Interim DDO18	Proposed Permanent DDO18	Justification
Height of the building/s	<p>17.6m East of Parkview Rd – M</p> <p>11.2m – to rear land between Park Ave and View St – M</p> <p>14.4m – on Heidelberg Rd at eastern end of precinct – M</p> <p>Council: Reduce max height to 5 storeys (17.6m). Ensure that recent Council and VCAT decisions regarding overall heights and transitioning to rear properties 700-718 H'Berg Rd are reflected in future documents.</p>	<p>17.6m East of Parkview Rd – D</p> <p>11.2m – to rear land between Park Ave and View St – D</p> <p>14.4m – on Heidelberg Rd at eastern end of precinct – D</p>	<p>17.6m East of Parkview Rd – D</p> <p>11.2m – to rear land between Park Ave and View St – D</p> <p>14.4m – on Heidelberg Rd at eastern end of precinct – D</p>	<p>The heights were reduced by Council on 4 February 2020 and approved in the interim DDO18.</p> <p>It is recommended the lower heights are retained as discretionary and additional criteria for proposals that exceed the preferred maximum height are applied to assess additional offsite impacts, such as overshadowing and visual bulk.</p> <p>Proposals exceeding the preferred height would need to achieve:</p> <ul style="list-style-type: none"> • increased separation distances; • higher ESD standards; • no additional overshadowing of sensitive interfaces; and • increased private and communal open space (where the proposal includes dwellings). <p>This approach is consistent with other recent amendments. It allows for the assessment of a proposal and its alignment with built form outcomes sought by the DDO.</p>
Street wall height	<p>700-718 Heidelberg Road: 4 storeys – M</p> <p>Between Park Avenue and Yarralea Street: 2 storeys – M</p> <p>Between Yarralea and Como Street: 4 storeys – M</p> <p>Side streets: Varies – M</p>	<p>700-718 Heidelberg Road: 4 storeys – D</p> <p>Between Park Avenue and Yarralea Street: 2 storeys – D</p> <p>Between Yarralea and Como Street: 4 storeys – D</p> <p>Side streets: Varies - D</p>	<p>700-718 Heidelberg Road: 4 storeys – M</p> <p>Between Park Avenue and Yarralea Street: 2 storeys – M</p> <p>Between Yarralea and Como Street: 4 storeys – M</p> <p>Side streets: Varies - D</p>	<p>The mix of recommended street wall heights would reflect the existing valued character of the NAC which includes some heritage buildings.</p> <p>The street wall heights provide for a lower street wall edge fronting the narrower section of Heidelberg Road and a taller street wall where the road widens.</p>
Front Setback to street(s)	<p>3m to Heidelberg Road between Parkview Rd to Park Ave – D</p> <p>0m to Heidelberg Road between Park Ave to Yarralea St – D</p>	<p>3m to Heidelberg Road between Parkview Rd to Park Ave – D</p> <p>0m to Heidelberg Road between Park Ave to Yarralea St - D</p>	<p>3m to Heidelberg Road between Parkview Rd to Park Ave – M</p> <p>0m to Heidelberg Road between Park Ave to Yarralea St – D</p>	<p>Recommended in the built form framework prepared by Hodyl & Co. and approved in the interim DDO18.</p> <p>Officers recommend no front setback applies on Heidelberg Road between Park Avenue and Yarralea Street due to the presence of heritage buildings and the importance of a consistent street edge in this section.</p> <p>Officers recommend a mandatory setback applies on Heidelberg Road between Parkview Road and Park Avenue to support opportunities for street activation and a significantly</p>

				improved pedestrian amenity and access through landscaping, inclusion of outdoor seating and trading space.
Minimum upper-level setback	Upper-level setback above Heidelberg Rd street walls: 6m - M 3m – upper-level setback behind a side street wall – D	Upper-level setback above Heidelberg Rd street walls: 6m – D 3m – upper-level setback behind a side street wall – D	Above Heidelberg Road between Parkview Rd and Park Ave: 6m – M Above Heidelberg Road between Park Ave and Yarralea St: 6m – M Above Heidelberg Road between Yarralea St and Como St 6m – D 3m – upper-level setback behind a side street wall - D	Recommended in the built form framework prepared by Hodyl & Co. to apply mandatory requirements on Heidelberg Road. The balance between street wall height and a 6m upper-level setback would achieve well-proportioned buildings where the upper levels form a recessed, lighter element above a solid base building form. Officers recommend a discretionary requirement applies to the wider section of Heidelberg Road (where a Public Acquisition Overlay will require an approximately. 10m front setback). A mandatory 6m requirement for other areas is recommended to provide a clear distinction between podium and upper levels where Heidelberg Road is narrower. The 3m discretionary setback on side streets is appropriate as it minimises overshadowing of opposite footpaths and retains a sense of openness for the side streets.
Maximum rear interface height	Development adjoining a residential property outside the overlay should not exceed a max boundary wall height of 8m – D	Development adjoining a residential property outside the overlay should not exceed a max boundary wall height of 8m – D	Development adjoining a residential property outside the overlay should not exceed a max boundary wall height of 8m – M	Recommended in the built form framework prepared by Hodyl & Co. as mandatory and approved in the interim DDO18 as discretionary. Officers recommend the rear interface requirements are applied as mandatory requirements to ensure residential properties are protected from overshadowing and overlooking, and visual bulk is minimised when viewed from the residential properties to the south.
Upper-level setbacks (above rear interface)	Envelope to be contained within 45-degree angle above - M	Envelope to be contained within 45-degree angle above – D	Envelope to be contained within 45-degree angle above - M	The built form framework demonstrated that these requirements are necessary to ensure appropriate outcomes are achieved and sensitive spaces to the south are not unreasonably impacted by new development.
Minimum rear boundary setback	A minimum of 3 metres if the dwelling on the adjacent residential property is located less than 15 metres from the property boundary – M	A minimum of 3 metres if the dwelling on the adjacent residential property is located less than 15 metres from the property boundary – D	A minimum of 3 metres if the dwelling on the adjacent residential property is located less than 15 metres from the property boundary – M	

Mandatory Controls

45. Proposed permanent DDO18 includes some proposed *mandatory controls* which address elements most important to the respective location, such as street wall heights, some building heights, upper-level setbacks, front or rear setbacks, including at ground level.

46. Guidance of the application of *mandatory controls* is provided in the State Government *Planning Practice Note 59* and *60*. *Planning Practice Note 60* which details that mandatory height and setback controls would only be considered in ‘exceptional circumstances’, where they are absolutely necessary to achieve the built form objectives or outcome identified within a comprehensive built form analysis.
47. Specifically, the Practice Notes require that *mandatory controls* must:
 - (a) be underpinned by comprehensive strategic work;
 - (b) consistent with state policy;
 - (c) take into consideration recent development activity; and
 - (d) provide capacity to accommodate growth.
48. The application of *mandatory controls* has been carefully considered and applied selectively and are not proposed to apply across all precincts and/or to all requirements within the proposed DDO18.
49. *Mandatory controls* are applied in the proposed DDO where it is considered ‘absolutely necessary’ (in accordance with PPN59). See Tables 1-4 for further details.
50. The extensive strategic work undertaken provides sufficient justification for the use of *mandatory controls* in the DDO schedule.

Consideration of VCAT decisions

51. In its resolution to adopt the interim DDO18 on 4 February 2020, Council requested officers consider details of key VCAT decisions when drafting the permanent DDO provisions.
52. This was mainly related to the site at 700-718 Heidelberg Road, Alphington. The VCAT decision required the development proposal to be reduced from 7 to 5 storeys and to provide a rear ground level setback for deep soil planting.
53. The decision found that a taller building may be possible on the site, but the proposal in front of the tribunal at the time required the changes described above for an approval.
54. Since the 2020 Council resolution, and prior to the gazettal of interim DDO18, a new planning permit was applied for and approved by VCAT. It allows for a 7-storey building with a revised rear interface response and façade design.
55. With this approval, the context has changed and some VCAT recommendations are no longer relevant and would lead to an inappropriate design response.
56. Officers have considered the recent VCAT cases and recommend the use of some elements of the decisions (such as rear interface requirements).
57. The built form recommendations for Precinct 3A generally reflect Council’s position during the VCAT case for 582 Heidelberg Road. Some refinements have been made as the height recommended in the proposed permanent DDO18 is lower than the proposal that was the subject of the VCAT case.

Extension of interim DDO18

58. Interim DDO18 will expire after 22 April 2023. An extension of this expiry date for 12 months is required to ensure appropriate and orderly planning while permanent provisions are being considered for inclusion in the Yarra Planning Scheme (via due process).
59. It is proposed that Council request the Minister for Planning extend the interim provisions through a Ministerial amendment under sections 8(1) (b) and 20(4) of the *Planning and Environment Act 1987*. Officers note that exhibition and notification requirements of sections 17, 18 and 19 of the Act do not apply in respect of this form of amendment.

Next steps – Consideration of the proposed amendment.

60. In 2021, the Minister for Planning set up the Yarra Activity Centres Standing Advisory Committee (SAC). The objective of the Committee is to provide consistent advice in a transparent, simpler, more timely and cost-efficient process on any proposed new planning provisions referred to it by the Minister.
61. To date, the Committee has considered two planning scheme amendments – Amendment C291yara – Bridge Road and Victoria Street Built Form Provisions and Amendment C293yara – Collingwood South Mixed Use Precinct Permanent Built Form Provisions.
62. Both amendments were exhibited, submissions received and were considered by the SAC. The amendments are awaiting approval by the Minister for Planning.
63. Under the SAC amendment process, the draft amendment is notified and publicly exhibited. Council would then consider the submissions and form a preferred position from which to advocate to the Advisory Committee.
64. Council then requests the Minister refer the submissions and amendment to the Advisory Committee and a hearing follows. Council then receives and considers the Committee's report and provides its final position to the Minister for Planning.

Options

65. In pursuing permanent built form provisions, there are two options for Council:
 - (a) request consent from the Minister for Planning to prepare and exhibit draft Amendment C273 as per Terms of Reference of the Yarra Activity Centre Standing Advisory Committee (SAC); or
 - (b) request 'authorisation' from the Minister for Planning so that Council could commence the 'usual' amendment process for Amendment C273. This pathway would include the amendment being assessed by a Panel (if required), similar to the SAC amendment process.
66. Based on previous experience with Amendments C291 (Bridge Road and Victoria Street) and C293 (Collingwood South Precinct), it is recommended Council continue to utilise the SAC and pursue the amendment via the SAC process.

Community and stakeholder engagement

Community consultation on the draft LAP, Interim DDO18, and Interim Heritage Overlays

67. Prior to the Minister approving interim DDO18, community consultation was undertaken from June – July 2021 in conjunction with Darebin City Council. Feedback was sought on the draft LAP (for both Darebin and Yarra) and on the strategic background work and interim DDO18 (for Yarra only).
68. Consultation efforts included:
 - (a) website: fact sheets, links to Darebin webpage, contact details and feedback form;
 - (b) posts on social media;
 - (c) post card drop within 200 metres of Heidelberg Road and to key institutions;
 - (d) feature in Yarra Life e-newsletter; and
 - (e) direct emails to Yarra's advisory committees and resident groups (including invitation to meet).
69. Council received 24 responses in relation to areas within Yarra. Officers analysed the feedback and considered it to inform the permanent provisions and Amendment C273. Some feedback was supportive of the interim DDO18, whilst other feedback expressed concerns about overdevelopment.
70. Some feedback related to the draft LAP and to broader matters that draft Amendment C273 cannot address.

71. The main topics raised and response are shown in Table 5:

Table 5: Feedback from June/July 2021 consultation

Topic	Response
Draft LAP (transport and movement, scale of development)	<p>The draft LAP would be finalised once Darebin City Council has completed its background work to inform their detailed position for their side of Heidelberg Road.</p> <p>The timing of finalising the draft LAP is not yet known.</p>
Increasing heights / overdevelopment	<p>Proposed DDO18 has been informed by independent urban design and heritage advice and includes learnings from recent amendments and VCAT cases. Additional criteria (relating to building separation, ESD, overshadowing and additional open space (where the proposal includes dwellings) have been added where discretionary heights are recommended.</p>
Overshadowing concerns	<p>The built form framework analysed overshadowing impacts from potential new development in detail. The proposed requirements will ensure that impacts are minimised.</p>
Need to improve public realm / open space	<p>The proposed DDO18 includes front setback requirements to street(s), where possible, to improve the pedestrian amenity.</p>
<p>Traffic congestion and parking concerns.,</p> <p>Mixed views on the bicycle lane trial on Heidelberg Road – some support and some opposition</p>	<p>Traffic changes have occurred over the recent years, including the continuing development on the former APM site, the construction of the new Chandler Highway bridge and the bicycle lane trial along Heidelberg Road.</p> <p>The Yarra Transport Strategy and future action plan address transport matters more broadly.</p> <p>The detailed traffic engineering report that informs Amendment C273 advises that the network can accommodate the potential changes that DDO18 would guide.</p>
Concerns for heritage buildings and requests for additional overlays in residential areas	<p>A detailed heritage review for land proposed to be included in DDO18 has been undertaken. Consequently, two additional heritage overlay areas are recommended.</p> <p>Residential land is not subject to this amendment.</p>
Concerns of wind effects due to taller new development	<p>The proposed DDO includes application requirements to ensure potential wind impacts are mitigated.</p>
Support for more mandatory controls	<p>The proposed DDO includes a balanced approach to applying mandatory controls and discretionary requirements. Mandatory controls are recommended towards the rear sensitive interfaces, to improve pedestrian amenity towards parts of Heidelberg Road and ensure a pedestrian scale towards Heidelberg Road is maintained.</p>

Reinstating historic laneways	Public laneways have been considered as part of the built form framework where they exist. The matter of laneways is addressed under adopted Clause 15.01-1L Urban Design: Support development that re-establishes laneways through the development site where such links were part of the historic street pattern.
Controlling land uses / requesting certain commercial uses	The use tables in a zone, is set by the State Government and guides land uses. A DDO cannot control land uses. Proposed DDO18 encourages lower levels of development be designed to accommodate commercial activity at the ground floor by specifying a commercial floor height and incorporating adaptable commercial and residential floor layouts so as to allow for a variety of uses over time.
Request for more community services	A DDO cannot require the provision of community facilities and services.

Policy analysis

Alignment to Community Vision and Council Plan

72. The Amendment supports the following themes in the *Yarra 2036 Community Vision*:
- (a) Priority 8.1 – Growing Sustainably – Advocate for development and planning design that shaped by and meets our community’s future needs; and
 - (b) Priority 8.2 – Growing Sustainably – Keep our heritage visible while we encourage innovative and sustainable growth.
73. The Amendment supports the following strategies in the Council Plan 2021-2025:
- (a) Strategic Objective 3 – Local economy – ‘Manage access, safety and amenity to enhance people’s experience when visiting Yarra’; and
 - (b) Strategic Objective 4 – Place and nature – ‘Protect, promote and maintain our unique heritage and ensure development is sustainable’.

Climate emergency and sustainability implications

74. The Amendment will help facilitate sustainable development in locations with good access to employment, public transport and other amenities.
75. Policy and provisions elsewhere in the Yarra Planning Scheme respond to the climate emergency, namely the Planning Policy Framework and Council’s Environmentally Sustainable Design Policy at Clause 22.17 and Clause 15.02-1L Environmentally Sustainable Development (ESD) in the adopted Local Policy in Amendment C269.

Community and social implications

76. There are no adverse community or social implications in preparing strategic work to underpin permanent built form provisions for the Neighbourhood Activity Centre and commercial areas along Heidelberg Road.
77. Improved built form provisions would help provide clarity around the anticipated future development of the centre and commercial areas.

Economic development implications

78. There are no economic development implications for preparing strategic work to underpin permanent built form provisions for the centre and surrounding area.

79. An amendment may aid in providing further stimulus to the retail precincts.

Human rights and gender equality implications

80. There are no known human rights implications for requesting the Minister for Planning to progress a permanent DDO and HO controls.

Operational analysis

Financial and resource impacts

81. The costs associated with preparing strategic work to underpin permanent provisions has been considered within the strategic planning budget, mainly during the financial years of 19/20 and 20/21.
82. The costs associated with the exhibition of the amendment would be within the strategic planning budget.
83. Legal costs with regard to the preparation and representation before, during and after the panel or committee hearing would be subject to the Governance budget and span across this and the coming financial year.

Legal Implications

84. The amendment would be progressed in accordance with the provisions of the *Planning and Environment Act 1987* and Yarra Activity Centres Standing Advisory Committee Terms of Reference issued by the Minister for Planning on 10 June 2021.
85. Council must ensure natural justice to all parties and to maintain the integrity of the Amendment process per Section 32 of the Terms of Reference.

Conclusion

86. Draft Amendment C273 is supported by state and local policy directions of Plan Melbourne, Yarra's current and adopted local policy framework and relevant Yarra strategies.
87. Proposed permanent DDO18 is based on sound strategic background work comprising of independent urban design, heritage and transport advice. It is also informed by a detailed review, including consideration of the June/July 2021 feedback, recent VCAT decisions and planning scheme amendments.
88. The draft amendment seeks to replace the interim DDO18 with a permanent provision, introduce 2 new Heritage Overlays, delete the Heritage Overlay from a single property, update related local policy, and include new/updated incorporated documents and background documents.
89. The proposed permanent DDO18 and permanent HOs balances the need to accommodate new development with heritage buildings and sensitive residential and parkland interfaces in the commercial areas along Heidelberg Road.
90. The permanent provisions are essential to ensure that new development in the commercial areas is an appropriate scale and development on larger sites remains subordinate to the 'high change area' within the former Alphington Paper Mill site.
91. The proposed DDO18 include built form and design requirements that minimise amenity impacts and ensure new development achieves better design outcomes, tailored to each precinct.
92. Officers recommend that Council requests consent from the Minister for Planning to prepare and exhibit draft Amendment C273 as per the Terms of Reference for the Yarra Activity Centre Standing Advisory Committee (SAC).
93. Interim DDO18 is due to expire after 22 April 2023. It is recommended that Council request the Minister for Planning extend the expiry provision within DDO18 for a further 12 months while the permanent DDO is progressed.

RECOMMENDATION

1. That Council:
 - (a) notes the officer report and Attachments 1-7 introducing proposed permanent provisions for commercial areas along Heidelberg Road;
 - (b) Adopts: Heidelberg Road Heritage Review (Stage 1), RBA Architects Conservation Consultants 2019; Heidelberg Road Heritage Review (Stage 2), RBA Architects Conservation Consultants 2019; Part 2: Heidelberg Road Built Form Framework – Design Strategy and Recommendations, Hodyl & Co, November 2019; Traffic and Vehicle Access Assessment, Heidelberg Road, Fairfield/Alphington, Traffix Group, November 2019; in Attachments 4, 5, 6, and 7 as the general basis for Amendment C273 to the Yarra Planning Scheme;
 - (c) Adopts the amendment documentation for proposed Amendment C273, including proposed local planning policy in Clause 11.03-1L Activity Centres (or Clause 21.08 Neighbourhoods should Amendment C269yara not be approved at the time of the exhibition), Design and Development Overlay Schedule 18, Heritage Overlays 451 and 455 and their Statements of Significance, updates to the Schedule to Clause 43.01 Heritage Overlay, and updates to the Schedule to Clause 72.04 Incorporated Documents and 72.08 Background Documents at Attachments 1, 2 and 3 as the basis for proposed Amendment C273;
 - (d) Requests consent from the Minister for Planning to prepare and exhibit draft Amendment C273 as per the Terms of Reference for the Yarra Activity Centre Standing Advisory Committee, appointed under Section 151 of the Planning and Environment Act 1987;
 - (e) Determines that should the Minister for Planning decide not to provide consent under the Terms of Reference for the Yarra Activity Centre Standing Advisory Committee, Council as the Planning Authority, apply to the Minister for Planning (Minister) under section 8A of the Planning and Environment Act 1987, for 'authorisation to prepare and exhibit the Amendment';
 - (f) Requests the Minister for Planning to prepare, adopt and approve Amendment C312yara to the Yarra Planning Scheme, in accordance with the Minister's powers under sections 8(1)(b) and section 20(4) of the Planning and Environment Act 1987 to extend the expiry dates for the Design and Development Overlay Schedule 18 which applies to commercial areas along Heidelberg Road, on an interim basis for 12 months while the permanent provisions are formally considered;
 - (g) Authorises officers to consult with the Minister, in accordance with sections 8(1)(b) and 20(4) of the Act, to assist the Minister to prepare, adopt and approve the Amendment to extend the interim provisions; and
 - (h) Authorises the CEO to make any minor adjustments required to meet the intent of the resolution.

Attachments

- 1 Attachment 1 - Amendment C273 Documents
- 2 Attachment 2 - Proposed Permanent DDO18
- 3 Attachment 3 - C273 Heritage Statement of Significance
- 4 Attachment 4 - Heidelberg Road Heritage Review Stage 1
- 5 Attachment 5 - Heidelberg Road Heritage Review Stage 2
- 6 Attachment 6 - Heidelberg Road Built Form Framework
- 7 Attachment 7 - Heidelberg Road Transport Engineering Advice

**YARRA PLANNING SCHEME
DRAFT AMENDMENT C273YARA
EXPLANATORY REPORT**

Who is the planning authority?

This draft amendment has been prepared by the Yarra City Council, for the Minister for Planning who is the planning authority for this draft amendment.

The draft amendment has been made at the request of Yarra City Council.

Land affected by the Amendment

The draft amendment applies to land currently within interim Design and Development Overlay – DDO18 along the south side of the Heidelberg Road corridor in Yarra, specifically:

- Commercial 1 Zone and Commercial 2 Zone land along Heidelberg Road as shown in figure 1 & 2
- 730-734 Heidelberg Road, Alphington (Lot 1 & 2 PS643181, Lot 1 & 2 LP38884) and 760-764 Heidelberg Road, Alphington (Lot 1 TP596569) as shown on figure 3.

The draft amendment also applies to 2 Killop Street, Alphington (Lot 2 PS626210) as shown in figure 4.

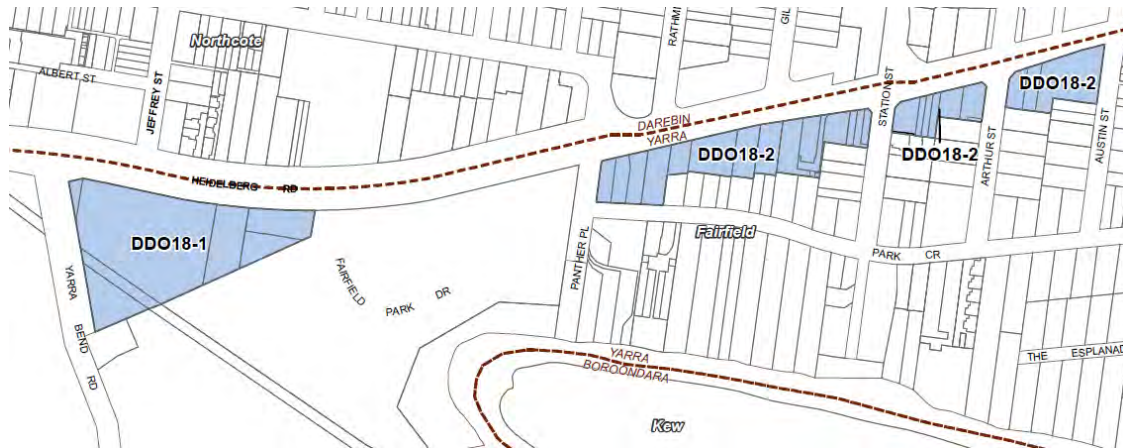


Figure 1: Proposed Design and Development Overlay along Heidelberg Road.

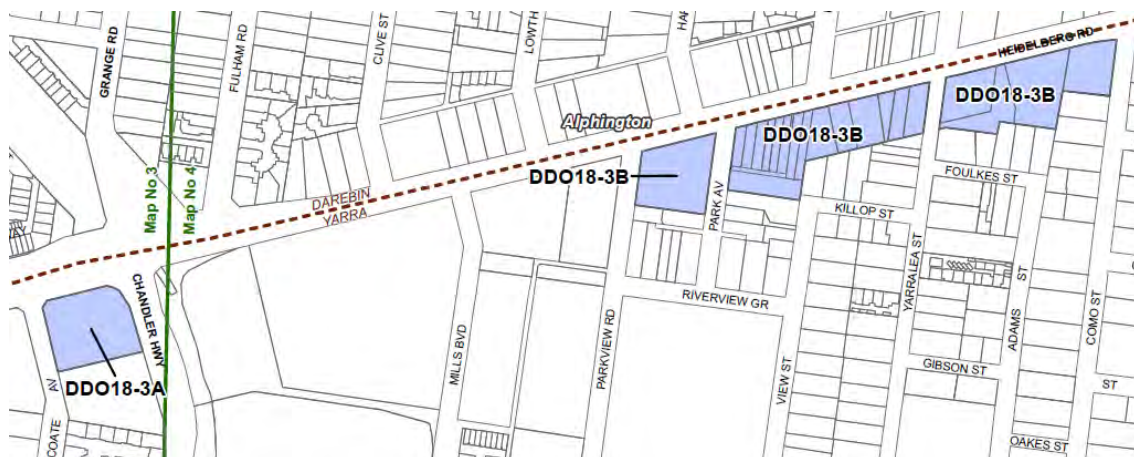


Figure 2: Proposed Design and Development Overlay along Heidelberg Road.

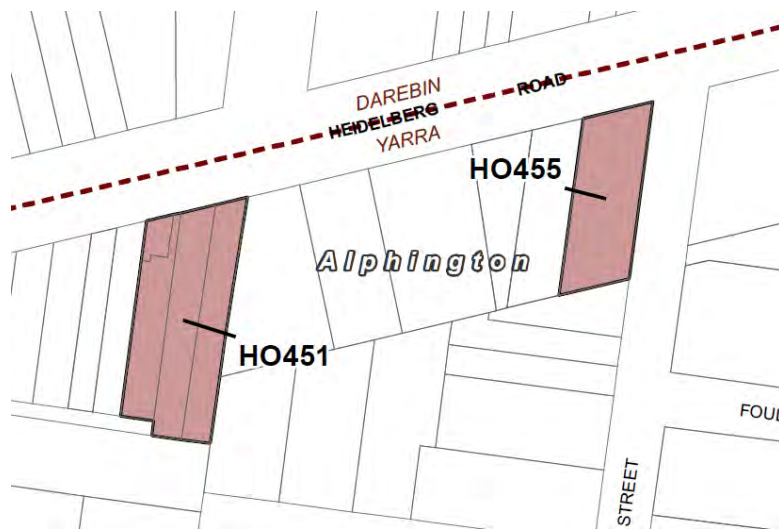


Figure 3: Apply Heritage Overlay at 730-734 & 760-764 Heidelberg Road, Alphington.



Figure 4: Delete Heritage Overlay (part of HO 362) at 2 Killop Street, Alphington.

What the amendment does

Interim Design and Development Overlay – DDO18 currently applies along the south side of the Heidelberg Road corridor in Yarra.

Council has undertaken a review of the interim DDO to test its suitability for translation into a permanent provision and recommend necessary refinements.

The draft amendment implements the recommendations of the following strategic planning work:

- *Heidelberg Road Heritage Review (Stage 1), RBA Architects Conservation Consultants 2019;*
- *Heidelberg Road Heritage Review (Stage 2), RBA Architects Conservation Consultants 2019;*
- *Part 2: Heidelberg Road Built Form Framework – Design Strategy and Recommendations, Hodyl & Co, November 2019; and*
- *Traffic and Vehicle Access Assessment, Heidelberg Road, Fairfield/Alphington, Traffix Group, November 2019.*

The draft amendment seeks to:

- Amend Local Area Policy at Clause 11.03-1L Activity Centres under the Heidelberg Road, Alphington subheading to include updated policy to guide built form within the Heidelberg Road Neighbourhood Activity Centre. Note: this update is to the adopted Amendment C269

planning policy framework which is not yet approved. This update could also be applied to existing Clause 21.08 Neighbourhoods, subheading Fairfield – Alphington.

- Replace interim Design and Development Overlay Schedule 18 (interim DDO18) with permanent Design and Development Overlay Schedule 18 (DDO18) which applies to the land zoned Commercial (C1Z and C2Z) along Heidelberg Road.
- Apply Heritage Overlay HO451 to the properties at 730-734 Heidelberg Road, Alphington.
- Apply Heritage Overlay HO455 to the property at 760-764 Heidelberg Road, Alphington.
- Delete Heritage Overlay HO362 from the property at 2 Killop Street, Alphington.
- Amend the Schedule to Clause 43.01 Heritage Overlay to include HO451 and HO455.
- Amend the Schedule to Clause 72.04 Incorporated Documents to include “The Post Office and the group of shops on Heidelberg Road Statement of Significance” and “Cooper Knitting Factory (former) Statement of Significance” and update the “City of Yarra Database of Heritage Significant Areas”.
- Amend the Schedule to Clause 72.08 Background Documents to include:
 - *Heidelberg Road Heritage Review (Stage 1), RBA Architects Conservation Consultants 2019;*
 - *Heidelberg Road Heritage Review (Stage 2), RBA Architects Conservation Consultants 2019;*
 - *Part 2: Heidelberg Road Built Form Framework – Design Strategy and Recommendations, Hodyl & Co, November 2019; and*
 - *Traffic and Vehicle Access Assessment, Heidelberg Road, Fairfield/Alphington, Traffix Group, November 2019.*
- Amend Planning Scheme Map Nos. 3DDO and 4DDO by replacing interim DDO18 with a permanent DDO18.
- Amend Planning Scheme Map No. 4HO by applying HO451 and HO455 and deleting HO362 from the property at 2 Killop Street, Alphington.

Strategic assessment of the Amendment

Why is the Amendment required?

The draft amendment has been requested by Yarra City Council to provide strategic guidance and greater certainty on the future land use planning and development outcomes of the commercial areas along the Heidelberg Road corridor, between the Darebin and Merri Creeks. The draft amendment responds to the development pressures being experienced along the corridor and the continuing need to provide for Melbourne’s housing and economic growth.

The scale and density of development approved and currently being proposed along Heidelberg Road has increased substantially in recent years. The most influential development being the ‘redevelopment’ of the former Alphington Paper Mill (APM) site on the south-east corner of the Chandler Highway and Heidelberg Road. The redevelopment is already driving new development proposals in the Heidelberg Road Neighbourhood Activity Centre and is likely to continue to be a catalyst for further development and land use changes along Heidelberg Road.

The Minister for Planning gazetted Amendment C272yara to the Yarra Planning Scheme to apply an interim built form control (DDO18) to the area. The interim control came into effect on 22 October 2021 and has been used to manage development while a permanent control was progressed.

To ensure appropriate and orderly planning, Council has undertaken a review of the interim built form control to test its suitability for translation into a permanent provision and recommend refinements.

Design and Development Overlay Schedule 18

Draft Amendment C273yara implements the built form recommendations of the *Part 2: Heidelberg Road Built Form Framework – Design Strategy and Recommendations, 2019* by introducing Schedule 18 to the Design and Development Overlay (DDO18) on a permanent basis. The built form work was prepared by Hodyl & Co (urban design) with input from RBA Architects Conservation Consultants (heritage), and Traffix Group (access and movement). These reports provide a strong strategic basis for the future planning of the area.

Permanent DDO18 aims to provide a balanced approach by guiding different levels of potential development across the Heidelberg Road Precincts. It aims to ensure that new buildings will respond to heritage fabric and minimise impacts on sensitive residential interfaces such as backyards, and public spaces like footpaths, kerb outstands and parklands.

The built form and design requirements are split into 'General Requirements' that apply across the Precincts and 'Precinct Design Requirements' that are specific to each Precinct. The requirements cover elements such as street wall height, upper level front and side street setbacks, building height, interface and rear setbacks, overshadowing and daylight access, front setbacks to street(s), building separation, building layout and access, parking and loading bay requirements.

Permanent DDO18 includes a mix of mandatory maximum requirements and preferred maximum requirements.

The permanent provision is required to ensure that new development within the commercial areas along Heidelberg Road is appropriately guided and that certainty on planning outcomes is increased. The provision is based on sound strategic background work and learnings from recent amendments.

It is proposed that the background work is listed in the Schedule to Clause 72.08 Background Documents.

Heritage Overlays (HO451 and HO455)

Draft Amendment C273yara implements the heritage recommendations of *the Heidelberg Road Heritage Review (Stage 1 & 2)*, *RBA Architects Conservation Consultants 2019* by applying the Heritage Overlay to the properties 730-734 Heidelberg Road, Alphington (group of 3 shops) and 760-764 Heidelberg Road, Alphington.

The Statement of Significance for these sites are proposed to be listed as incorporated documents in the Schedule to Clause 72.04 Incorporated Documents. It is also proposed to update the "City of Yarra Database of Heritage Significant Areas" (listed as an incorporated document in the Schedule to Clause 72.04) to show these changes. The Heritage Reviews (Stage 1 and 2) are proposed to be listed as background documents in the Schedule to Clause 72.08.

The amendment also deletes the Heritage Overlay (HO362) from 2 Killop Street, Alphington. The late 20th century house was determined to be an anomaly at the edge of the Alphington East Precinct largely characterised by Victorian, Edwardian and Interwar houses with pitched, gabled or hipped roofs. It varies greatly to the graded building stock in the precinct and it was recommended by the heritage advisor to be removed from the precinct on this basis.

Clause 11.03-1L Activity Centres

Updating Clause 11.03-1L Activity Centres under the Heidelberg Road, Alphington subheading would include strategies to reinforce expectations around use and development in the Neighbourhood Activity Centre. The updates are informed by the built form framework, development plan for the former Alphington Paper Mill site and draft Local Area Plan. Note: this update is to the adopted Amendment C269 planning policy framework which is not yet approved. This update could also be applied to existing Clause 21.08 Neighbourhoods, subheading Fairfield – Alphington.

How does the Amendment implement the objectives of planning in Victoria?

The draft amendment implements the objectives in Section 4 of the *Planning and Environment Act 1987* (the Act), in particular:

- a) to provide for the fair, orderly, economic and sustainable use, and development of land;
- b) to secure a pleasant, efficient and safe working, living and recreational environment;
- c) to conserve and enhance those buildings, areas or other places which are of scientific, aesthetic, architectural or historical interest, or otherwise of special cultural value; and
- d) to balance the present and future interests of all Victorians.

The draft amendment facilitates housing growth as well as economic growth and creates more economically viable mixed-use precincts in the commercial zoned land along Heidelberg Road.

How does the Amendment address any environmental, social and economic effects?

The draft amendment is consistent with the overarching goal in the planning scheme to:

Integrate relevant environmental, social and economic factors in the interests of net community benefit and sustainable development.

The draft amendment is expected to generate positive social and economic benefits as it will facilitate development within the area, providing opportunities for economic development, housing and employment growth.

Does the Amendment address relevant bushfire risk?

The land affected by the draft amendment is not located within an area of identified bushfire risk.

Does the Amendment comply with the requirements of any Minister's Direction applicable to the amendment?

The draft amendment complies with Ministerial Direction No. 9 in addressing and responding to the Metropolitan Planning Strategy, *Plan Melbourne 2017-2050*.

The draft amendment complies with the Ministerial Direction on the *Form and Content of Planning Schemes*.

The draft amendment C273yara is consistent with the following Directions contained in *Plan Melbourne 2017-2050*:

Direction 1.1 - Create a city structure that strengthens Melbourne's competitiveness for jobs and investment, which seeks to strengthen the competitiveness of Melbourne's employment land. The draft amendment provides appropriate policy direction for the planning and development of the Commercial 1 and Commercial 2 Zone land along Heidelberg Road including the Heidelberg Road Neighbourhood Activity Centre.

Direction 5.1 - Create a city of 20-minute neighbourhoods which aims to cluster new housing in activity centres and other places that offer good access to jobs, services and public transport and includes policy for local governments to prepare structure plans for activity centres to accommodate growth. The draft amendment will facilitate renewal of the Commercial 1 and Commercial 2 Zone land along Heidelberg Road including the Heidelberg Road Neighbourhood Activity Centre, which will improve local employment, housing and commercial opportunities.

How does the Amendment support or implement the Planning Policy Framework and any adopted State policy?

The draft Amendment supports and implements the Planning Policy Framework in responding to the following clauses:

Clause 11 – Settlement provides context and implements the key principles of *Plan Melbourne 2017-2050*, which includes providing for housing choice and affordability by planning for expected housing needs and providing for reduced ongoing living costs by increasing housing supply near public transport and services. It also encourages the consolidation of residential activities within existing urban areas and development in existing residential areas. The amendment provides a framework for the orderly planning and high-quality development of commercially zoned land along Heidelberg Road in a manner consistent with the directions of *Plan Melbourne 2017-2050*.

Clause 15 – Built Environment and Heritage seeks to create urban environments that are safe, functional and provide good quality environments with a sense of place and cultural identity. This Clause also sets out the importance of ensuring the conservation of places, which have identified heritage significance. The amendment supports this clause by providing appropriate built form guidance to ensure that development is site responsive and appropriate. The amendment also protects heritage significant buildings by applying the Heritage Overlay.

Clause 16 – Housing emphasises the importance of providing enough quality and diverse housing that meets the growing diverse needs of Victorians in locations in or close to activity centres and sites that offer good access to jobs, services and transport. It requires councils to identify areas that offer opportunities for more medium and high-density housing near employment and transport in Metropolitan Melbourne. The amendment provides strategic guidance on the appropriate scale of development.

Clause 17 – Economic Development seeks to encourage development which meets the community's needs for retail, entertainment, office and other commercial services and provides a net community benefit in relation to accessibility, efficient infrastructure use and the aggregation and sustainability of commercial facilities. The amendment supports this clause by facilitating opportunities for a mix of office, retail, and residential uses in the commercially zoned land along Heidelberg Road.

Clause 18 – Transport promotes the creation of a safe and sustainable transport system and promotes the use of sustainable personal transport. The amendment implements the objectives of this clause by facilitating development, which is well serviced by public transport.

VPP11: Settlement

- Clause 11.02-1S Supply of Urban Land – To ensure a sufficient supply of land is available for residential, commercial, retail, industrial, recreational, institutional and other community uses.
- Clause 11.02-2S Structure Planning - To facilitate the orderly development of urban areas.
- Clause 11.03-1S Activity Centres - To encourage the concentration of major retail, residential, commercial, administrative, entertainment and cultural developments into activity centres that are highly accessible to the community.
- Clause 11.02-3S Sequencing of development - To manage the sequence of development in areas of growth so that services are available from early in the life of new communities

VPP15: Built Environment and Heritage

- Clause 15.01-1S Urban Design - To create urban environments that are safe, healthy, functional and enjoyable and that contribute to a sense of place and cultural identity
- Clause 15.01-2S Building design - To achieve building design outcomes that contribute positively to the local context and enhance the public realm.
- Clause 15.01-3S Subdivision design - To ensure the design of subdivisions achieves attractive, safe, accessible, diverse and sustainable neighbourhoods.
- Clause 15.01-4S Healthy neighbourhoods - To achieve neighbourhoods that foster healthy and active living and community wellbeing.
- Clause 15.01-5S Neighbourhood character - To recognise, support and protect neighbourhood character, cultural identity, and sense of place.
- Clause 15.03-1S Heritage conservation - To ensure the conservation of places of heritage significance.
- Clause 15.02-1S Energy and resource efficiency -To encourage land use and development that is energy efficient.

VPP 16: Housing

- Clause 16.01-1S Housing supply -To facilitate well-located, integrated and diverse housing that meets community needs.

VPP17: Economic Development

- Clause 17.01-1S Diversified economy - To strengthen and diversify the economy.
- Clause 17.02-1S Business - To encourage development that meets the community's needs for retail, entertainment, office and other commercial services.

VPP 18: Transport

- Clause 18.01-2S Transport system - To coordinate development of all transport modes to provide a comprehensive transport system.
- Clause 18.02-2S Public Transport - To facilitate greater use of public transport and promote increased development close to high-quality public transport routes.
- Clause 18.02-4S Car parking -To ensure an adequate supply of car parking that is appropriately designed and located.

How does the Amendment support or implement the Local Planning Policy Framework, and specifically the Municipal Strategic Statement?

The draft amendment includes a new policy about the neighbourhood activity centre within the Municipal Strategic Statement and supports and implements the LPPF specifically the Municipal Strategic Statement through:

Clause 21.04 Land use:

Clause 21.04-1– Accommodation and housing:

Objective 1 - To accommodate forecast increases in population.

- *Strategy 1.2 - Direct higher density residential development to Strategic Redevelopment Sites identified at clause 21.08 and other sites identified through any structure plans or urban design frameworks.*

Clause 21.04-2 - Activity centres:

Objective 4 - To maintain a balance between local convenience and regional retail roles in Yarra's activity centres.

- *Strategy 4.1- Increase the range of retail, personal and business services, community facilities, and recreation activities, within individual centres.*
- *Strategy 4.3 - Support the role of all activity centres, including Neighbourhood Activity Centres, in providing local day-to-day needs of residents of all abilities.*

Clause 21.04-3 – Industry, Office and Commercial:

Objective 8 -To increase the number and diversity of local employment opportunities.

21.04-5- Parks, gardens and public open space:

- *Strategy 13.3 - Ensure new development does not have a negative impact on adjoining open space.*

Clause 21.05 - Built form:

21.05-1– Heritage

Objective 14 - To protect and enhance Yarra's heritage places.

21.05-2 – Urban design:

Objective 16 - To reinforce the existing urban framework of Yarra.

Objective 19 - To create an inner city environment with landscaped beauty.

Objective 20 - To ensure that new development contributes positively to Yarra's urban fabric.

- *Strategy 20.3 - Reflect the fine grain of the subdivision pattern in building design where this is part of the original character of the area.*

21.05-3 – Built form character:

Objective 23 - To maintain and strengthen the identified character of each type of identified built form within Yarra.

21.05-4 – Public environment:

Objective 28 - To provide a public environment that encourages community interaction and activity.

- *Strategy 28.1- Encourage universal access to all new public spaces and buildings.*
- *Strategy 28.2- Ensure that buildings have a human scale at street level.*

21.06 - Transport:

Clause 21.06-1 – Walking and cycling

Objective 30 - To provide safe and convenient pedestrian and bicycle environments.

Clause 21.06-3 – The road system and parking

Objective 32 - To reduce the reliance on the private motor car.

Objective 33 - To reduce the impact of traffic.

21.07 Environmental sustainability:

21.07-1- Environmentally sustainable development

Objective 34 - To promote environmentally sustainable development.

Clause 22.02 - Development Guidelines for Sites Subject to the Heritage Overlay

22.02-4 - Objectives:

To conserve Yarra's natural and cultural heritage.

To conserve the historic fabric and maintain the integrity of places of cultural heritage significance.

Clause 22.05- Interface Uses Policy

22.05-2 – Objectives:

To enable the development of new residential uses within and close to activity centres, near industrial areas and in mixed use areas while not impeding the growth and operation of these areas as service, economic and employment nodes.

To ensure that residential uses located within or near commercial centres or near industrial uses enjoy a reasonable level of amenity.

22.05-4 - Dwelling Design:

Minimise the potential for views from existing business or industrial premises to habitable room windows and private open space areas, through the use of appropriate siting, setbacks, articulation and screens.

Clause 22.10 - Built Form and Design Policy

22.10-2 – Objectives:

Ensure that new development makes a positive contribution to the streetscape through high standards in architecture and urban design.

Limit the impact of new development on the amenity of surrounding land, particularly residential land.

Clause 22.13 - Residential Built Form Policy

22.13-2 – Objectives:

To limit the impact of new development on surrounding land, particularly on low rise residential areas.

Clause 22.17 - Environmentally Sustainable Development

22.17-2 – Objectives:

The overarching objective is that development should achieve best practice in environmentally sustainable development from the design stage through to construction and operation.

Amendment C269yara seeks to introduce a new planning policy framework into the Yarra Planning Scheme. It is currently being considered by the Minister for Planning for approval. Draft amendment C273yara is consistent with the new planning policy framework as in C269yara.

How does the amendment support or implement the Municipal Planning Strategy?

Amendment C269yara to introduce the new municipal planning strategy (MPS) is currently being considered by the Minister for Planning for approval. The draft amendment is consistent and builds on the new MPS.

Does the Amendment make proper use of the Victoria Planning Provisions?

The draft amendment uses the most appropriate VPP tools to achieve its objectives. A Design and Development Overlay (DDO) is the best tool to control future built form and the Heritage Overlay is used to protect places of heritage significance.

How does the Amendment address the views of any relevant agency?

Council sought the views of the Department of Environment, Land, Water and Planning (DELWP) and the Department of Transport in drafting the Amendment while preparing the interim controls.

Further views of relevant agencies will be sought during exhibition of the draft amendment C273yara.

Does the Amendment address relevant requirements of the Transport Integration Act 2010?

The draft amendment is consistent with the requirements of the *Transport Integration Act 2010* and will facilitate development outcomes that promote the principles of transit oriented development.

Particular consideration has been given to ensuring that vehicular movements do not impact on the Principal Public Transport Network.

Resource and administrative costs

- **What impact will the new planning provisions have on the resource and administrative costs of the responsible authority?**

The draft amendment will have some impact on the general operation of Council's statutory planning department as it will facilitate some new forms of development and land use.

The application of planning controls is considered to provide a more consistent assessment of planning permit applications. This is considered to ultimately reduce costs by providing more certainty to the community.

Where you may inspect this Amendment

The draft amendment is available for public inspection, free of charge, during office hours at the following places:

Planning Counter Richmond Town Hall 333 Bridge Road Richmond VIC 3121	Information Counter Collingwood Town Hall 140 Hoddle Street Abbotsford VIC 3067
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Yarra City Council web-site – draft amendment C273yara: www.yarracity.vic.gov.au/amendment

Submissions

Any person who may be affected by the draft amendment may make a submission to the Yarra City Council. Submissions about the draft amendment must be received by 5pm **on TBC**

A submission must be sent to either:

- By post to “Strategic Planning Unit – Amendment C273, PO Box 168 Richmond, VIC 3181” or
- by email at StrategicPlanning@yarracity.vic.gov.au (please use “Amendment C273” in the subject header)

Standing Advisory Committee hearing dates

In accordance with the Terms of Reference for the Yarra Activity Centre Standing Advisory Committee and if the Minister resolves to refer the matter to the Committee, the following dates for the Standing Advisory Committee are reserved for this amendment:

- **Directions hearing: TBC.**
- **Standing Advisory Committee hearing: TBC.**

For more information on the Committee, please visit planning.vic.gov.au/panels-and-committees/browse-panels-and-committees/projects/yarra-activity-centres-standing-advisory-committee

Planning and Environment Act 1987

YARRA PLANNING SCHEME

AMENDMENT C273YARA

INSTRUCTION SHEET

The planning authority for this amendment is the Minister for Planning.

The Yarra Planning Scheme is amended as follows:

Planning Scheme Maps

The Planning Scheme Maps are amended by a total of 4 attached map sheets.

Overlay Maps

Amend Planning Scheme Map Nos. 3DDO, 4DDO, and 4HO in the manner shown on the 4 attached maps marked "Yarra Planning Scheme, Amendment C273yara".

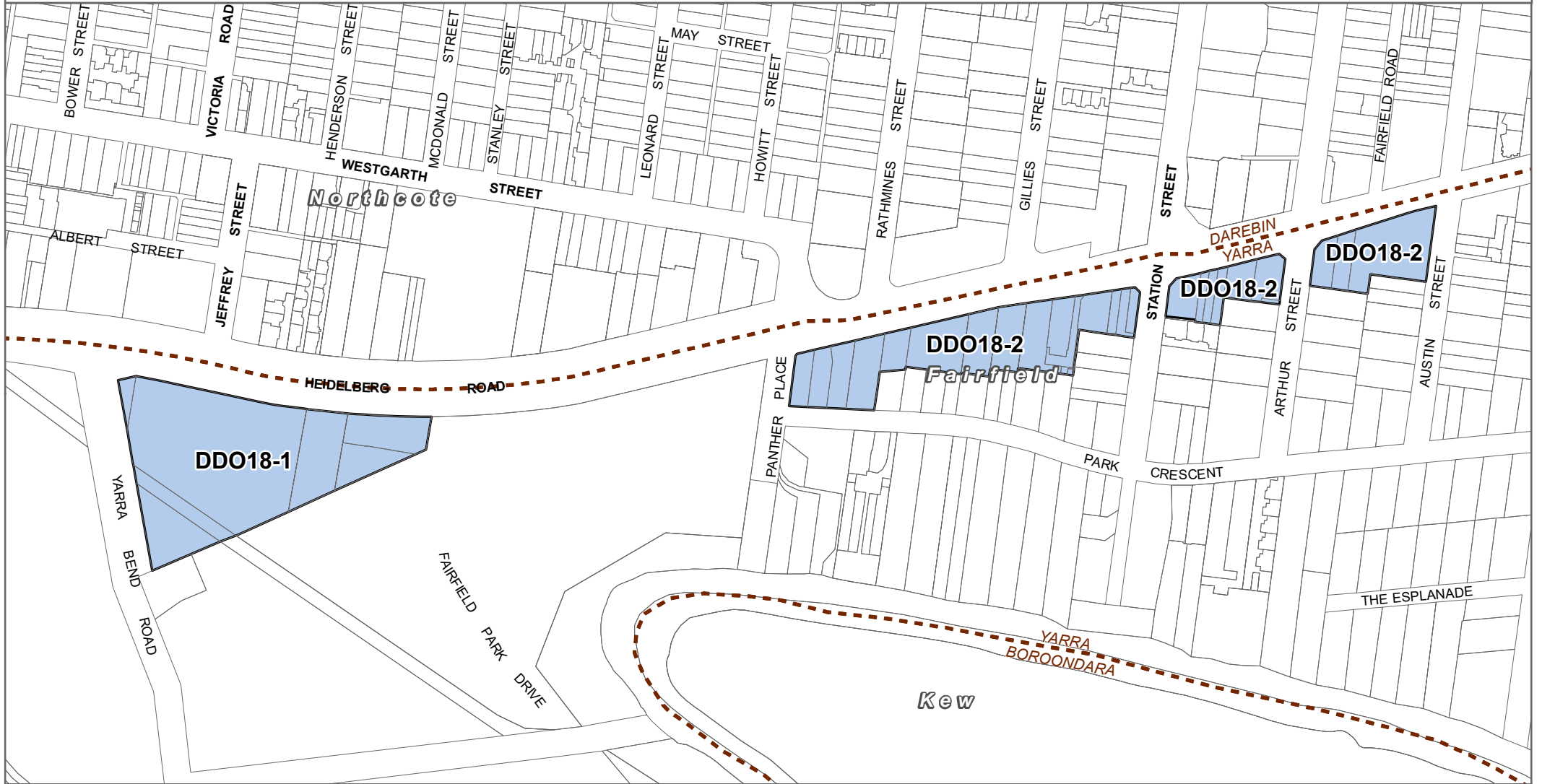
Planning Scheme Ordinance

The Planning Scheme Ordinance is amended as follows:



1. In **Planning Policy Framework** – insert new Clause 11.03-1L Activity Centres in the form of the attached document.
2. In **Overlays** – Clause 43.01, replace Schedule with a new Schedule in the form of the attached document.
3. In **Overlays** – Clause 43.02, replace Schedule 18 with a new Schedule 18 in the form of the attached document.
4. In **Operational Provisions** – Clause 72.04, replace the Schedule with a new Schedule in the form of the attached document.
5. In **Operational Provisions** – Clause 72.08, replace the Schedule with a new Schedule in the form of the attached document.

End of document

YARRA PLANNING SCHEME - LOCAL PROVISION AMENDMENT C273yara



LEGEND

-  DDO18 - Design and Development Overlay - Schedule 18
-  Local Government Area

Disclaimer

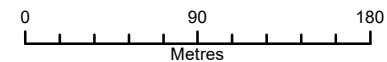
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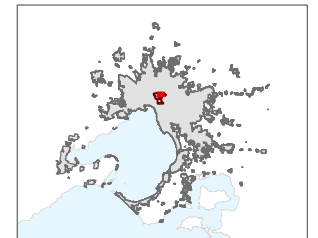
Print Date: 8/30/2022

Amendment Version: 3

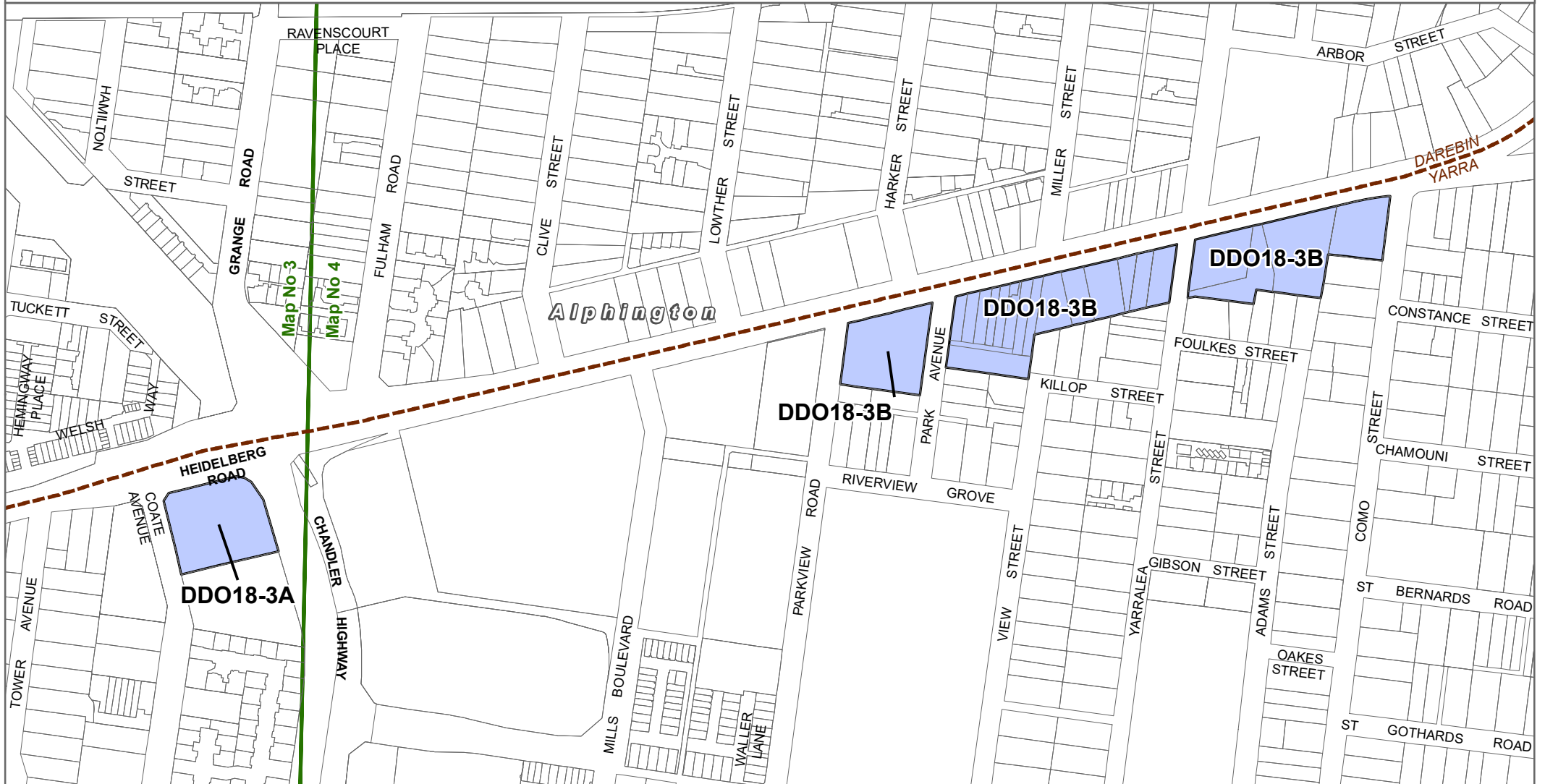


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Part of Planning Scheme Map 3DDO



YARRA PLANNING SCHEME - LOCAL PROVISION AMENDMENT C273yara



LEGEND

-  DDO - Design and Development Overlay
-  Local Government Area

Disclaimer

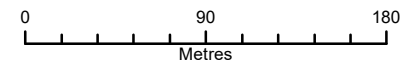
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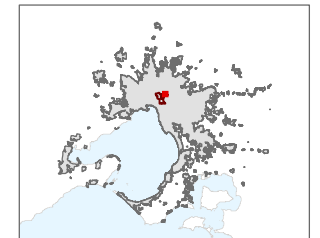
Print Date: 8/30/2022

Amendment Version: 3

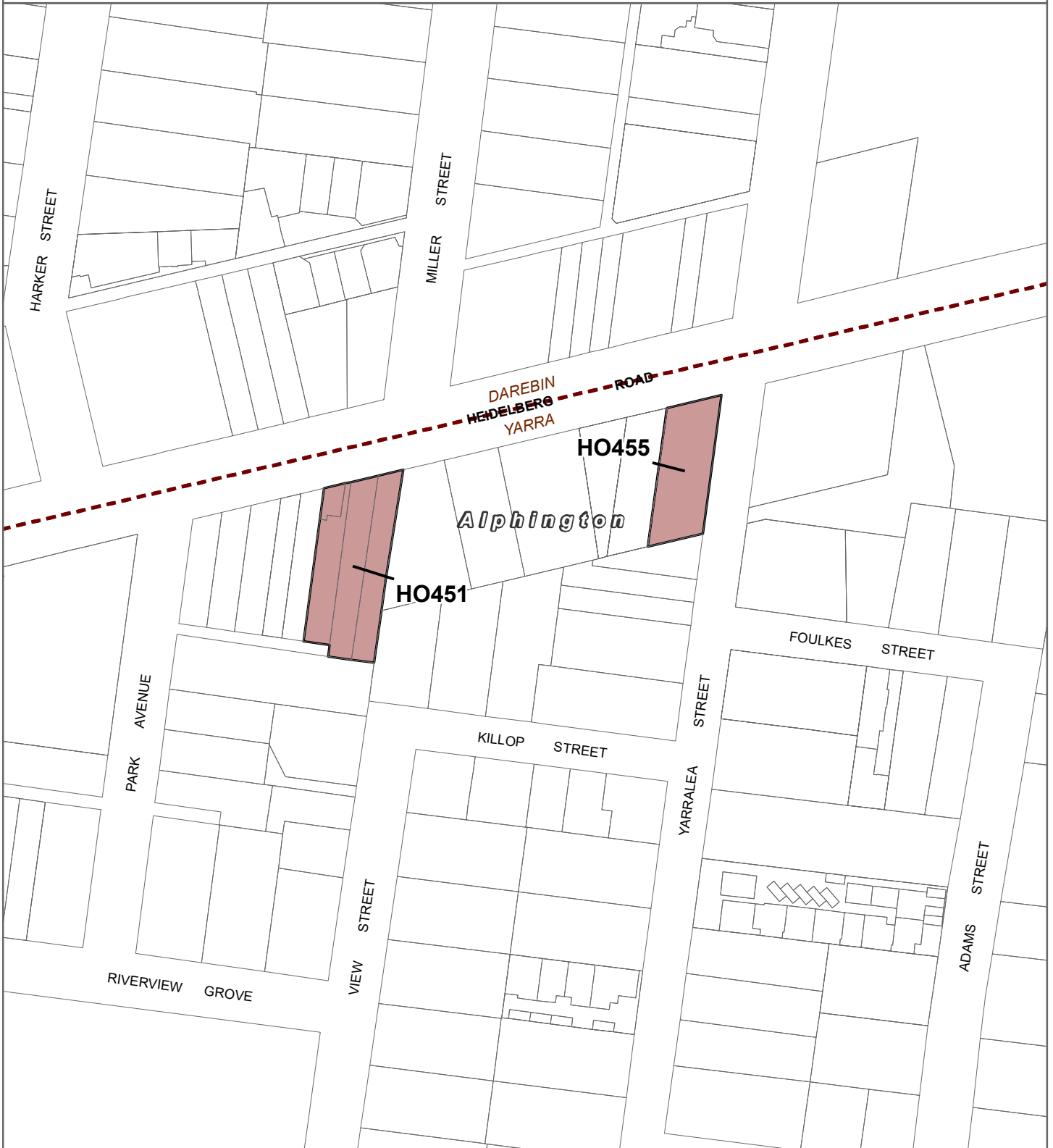


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Part of Planning Scheme Maps 3DDO & 4DDO

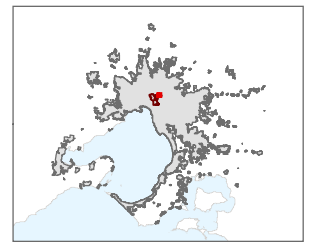


YARRA PLANNING SCHEME - LOCAL PROVISION AMENDMENT C273yara



LEGEND

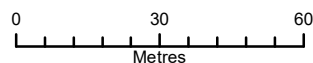
- HO - Heritage Overlay
- Local Government Area



Part of Planning Scheme Map 4HO

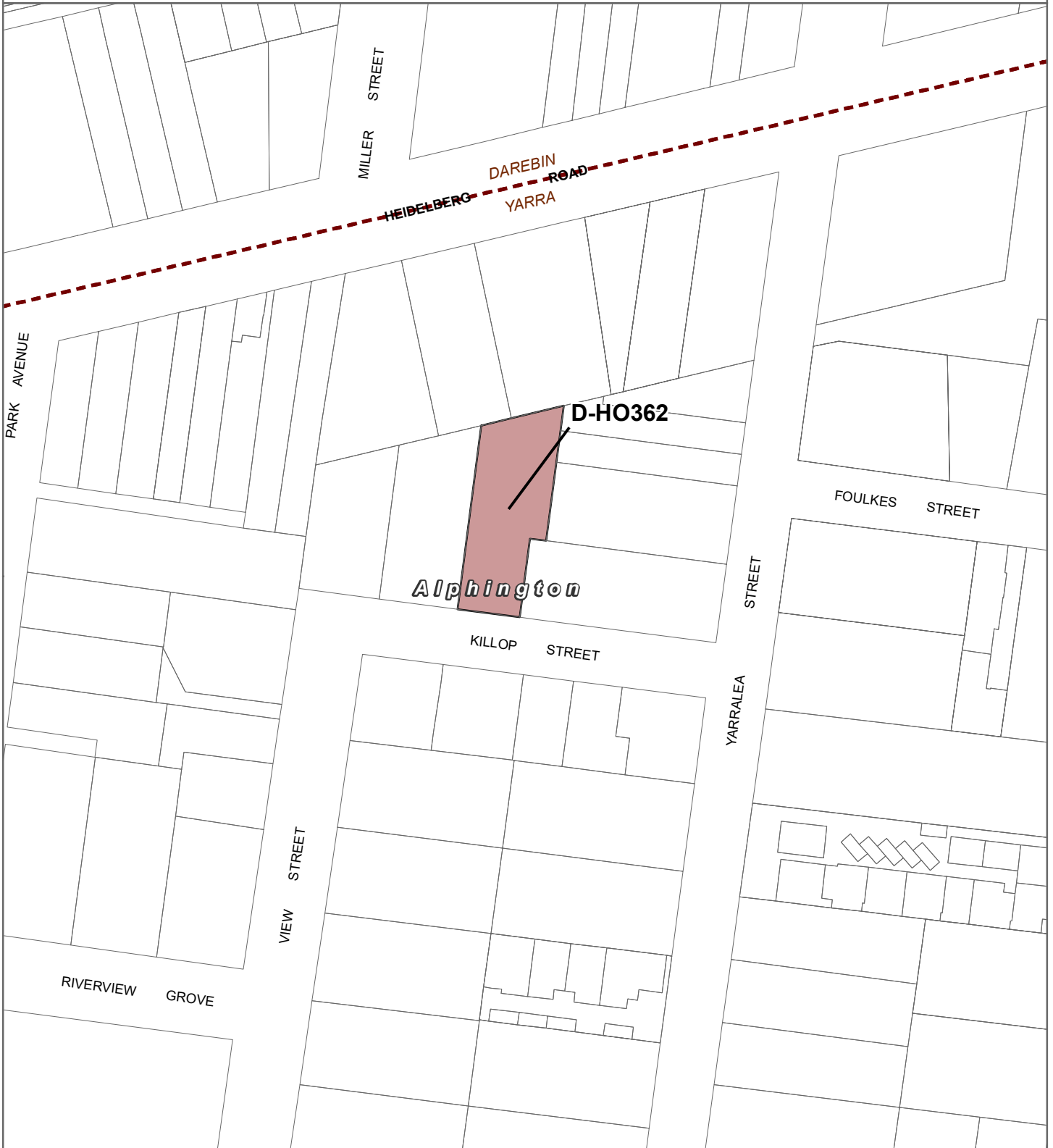
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 Print Date: 9/2/2022
 Amendment Version: 3



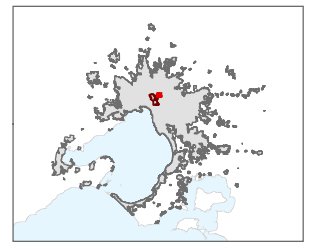
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YARRA PLANNING SCHEME - LOCAL PROVISION AMENDMENT C273yara



LEGEND

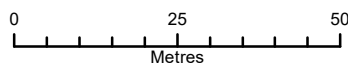
- D-HO - Area to be deleted from a Heritage Overlay
- Local Government Area



Part of Planning Scheme Map 4HO

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Environment,
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Updates to local policy as adopted in C269yara

Clause 11.03-1L Activity Centres

[...]

Neighbourhood Activity Centres

[...]

Heidelberg Road, Alphington

Promote the retail and community roles of the activity centre.

Support the expansion of the activity centre by directing taller development to its western end within the Alphington Paper Mill major regeneration area (Yarra Bend Developments).

Support ~~more~~ moderate built form on land outside of the Alphington Paper Mill major regeneration area.

Enhance the quality of the public realm and pedestrian experience at the intersection of ~~along~~ Heidelberg Road, side streets and ~~the~~ Chandler Highway.

Support the increase of sustainable modes of transport.

Ensure development does not visually dominate or unreasonably overshadow private open space in adjacent residential areas.

Minimise direct vehicle access onto Heidelberg Road.

~~Support development along Heidelberg Road, east of Parkview Avenue that achieves fine grain building frontages and contributes to a positive pedestrian street environment, through appropriate street wall height, upper level setbacks and landscaping.~~

582 Heidelberg Road

Promote employment focussed or mixed-use development.

Support a well-designed, mid-rise development that does not compete with development east of Chandler Highway and respects the surrounding low-rise residential character by transitioning down in height towards the west and south.

Ensure development respects the existing low-rise residential character of Coate Avenue through low-rise street walls and significant upper-level setbacks.

Incorporate a landscape setback to all boundaries to provide an attractive, garden setting to Coate Avenue and the southern boundary and to significantly improve the pedestrian experience to Heidelberg Road and Chandler Highway.

Provide vehicular access from Coate Avenue.

Yarra Bend Development (Alphington Paper Mill regeneration area – between Chandler Highway and Parkview Avenue)

Create a mixed-use precinct located towards Heidelberg Road that complements the existing shopping strip in the neighbourhood activity centre.

Support convenience retailing services and community facilities within walking distance of residential uses, with commercial employment opportunities along main road frontages.

Encourage walking and cycling to/from and around the site in all directions that provides an easy and pleasant experience and facilitate links with public transport.

Allow for a prominent development at the intersection of Chandler Highway and Heidelberg Road as a sole marker to the area, stepping down in height towards residential interface to the east and towards the Yarra River Corridor to maintain the prominence of the landscape setting to the south and low-rise residential setting to the east.

Retain links to the site's industrial past and encourage the adaptive reuse of existing significant heritage buildings for shared community and/or cultural uses.

The shopping strip (area between Parkview Avenue and Como Street)

Support a range of retail, commercial and residential uses within the existing neighbourhood centre which complements the facilities in the former Alphington Paper Mills site.

Support mid-rise development (up to 5 storeys) that achieves fine grain building frontages and contributes to a positive pedestrian street environment, through low-rise street walls, legible upper-level setbacks, pedestrian-oriented design and landscaping.

Ensure development sensitively responds to the existing valued heritage and fine-grain shopfront character.

Enhance the amenity and walkability of the precinct for pedestrians through introducing a generous landscape front setback in the block bounded by Como Street and Yarralea Street.

Provide vehicular access from the side street or from the rear of the property.

Heidelberg Road Neighbourhood Activity Centre Plan



LEGEND

- Yarra LGA boundary
- Neighbourhood Activity Centre
- Open space
- Waterway



AMENDMENT C273YARA

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
	<p>Incorporated plan: Incorporated Plan under the provisions of clause 43.01 Heritage Overlay, Planning permit exemptions, July 2014</p>							
HO449	<p>70 BAKER STREET RICHMOND House and fence</p> <p>Incorporated plan: Incorporated Plan under the provisions of clause 43.01 Heritage Overlay, Planning permit exemptions, July 2014</p>	Yes	No	No	No	No	No	No
HO450	<p>38-50 REGENT STREET & 35-49 LITTLE HODDLE STREET RICHMOND Henry Walters' Boot Factories (Former)</p> <p>Incorporated plan: Incorporated Plan under the provisions of clause 43.01 Heritage Overlay, Planning permit exemptions, July 2014</p>	No	No	No	No	No	No	No
HO451	<p>730-734 (including 730A), Heidelberg Road, Alphington</p> <p>Statement of significance: The Post Office and the group of shops on Heidelberg Road</p>	No	No	No	No	No	No	No
HO452	<p>HIGHETT, EGAN & MUIR STREETS AND JIKA PLACE RICHMOND West Richmond Railway Station complex</p> <p>Incorporated plan: Incorporated Plan under the provisions of clause 43.01 Heritage Overlay, Planning permit exemptions, July 2014</p>	Yes	No	No	No	No	No	No
HO453	<p>Building Society Cottages Precinct, Richmond</p>	No	No	No	No	No	No	No

AMENDMENT C273YARA

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
	<p>Incorporated plan: Incorporated Plan under the provisions of clause 43.01 Heritage Overlay, Planning permit exemptions, July 2014</p>							
HO454	<p>Church Street North Precinct, Richmond</p> <p>Incorporated plan: Incorporated Plan under the provisions of clause 43.01 Heritage Overlay, Planning permit exemptions, July 2014</p>	Yes	No	No	No	No	No	No
HO455	<p>760-764 Heidelberg Road, Alphington</p> <p>Statement of significance: Cooper Knitting Factory (former)</p>	No	No	No	No	No	No	No
HO456	<p>Gardner Street Precinct, Richmond</p> <p>Incorporated plan: Incorporated Plan under the provisions of clause 43.01 Heritage Overlay, Planning permit exemptions, July 2014</p>	No	No	No	No	No	No	No
HO457	<p>Highett Street Precinct, Richmond</p> <p>Incorporated plan: Incorporated Plan under the provisions of clause 43.01 Heritage Overlay, Planning permit exemptions, July 2014</p>	No	No	No	No	No	No	No
HO458	<p>Lincoln Street Precinct, Richmond</p> <p>Incorporated plan: Incorporated Plan under the provisions of clause 43.01 Heritage Overlay, Planning permit exemptions, July 2014</p>	No	No	No	No	No	No	No
HO459	<p>Wells Street Precinct, Richmond</p> <p>Incorporated plan:</p>	No	No	No	No	No	No	No

SCHEDULE TO CLAUSE 72.04 DOCUMENTS INCORPORATED IN THIS PLANNING SCHEME

System Note: The following ordinance will be modified in Sub-Clause:72.04 INCORPORATED DOCUMENTS, Schedule:SCHEDULE TO CLAUSE 72.04 DOCUMENTS INCORPORATED IN THIS PLANNING SCHEME

1.0**Incorporated documents**

Name of document	Introduced by:
5-15 Mayfield Street, Abbotsford, Incorporated Document, October, 2018	C188
10 Bromham Place, Richmond Incorporated Document, February 2013	C171
18-62 Trenerry Crescent, Abbotsford (Incorporated Plan, May 2018)	C218
32-68 Mollison Street and 61-69 William Street, Abbotsford July 2013	C170
36-52 Wellington Street, Collingwood - Incorporated Document, September 2020	C285yara
351-353 Church Street, Richmond – Incorporated Document, February 2019	C225
462-482 Swan Street, Richmond Incorporated Document, September 2020	C282yara
520 Victoria Street, 2A Burnley Street, and 2 – 30 Burnley Street, Richmond, Burnley Street West Precinct - Incorporated Plan, 2012	C150
Atherton Gardens – Fitzroy, September 2010	C136
Caulfield Dandenong Rail Upgrade Project, Incorporated Document, April 2016	GC37
Chandler Highway Upgrade Incorporated Document, March 2016 (Amended December 2017)	GC80
City of Yarra Database of Heritage Significant Areas, September 2022	C273yara
Cremorne Balmain Dover Street Project	NPS1
Crown Land Car Park Works, Burnley, August 2005	C92
Fitzroy Former Gasworks Site, Incorporated Document, February 2018	C242
Flying Fox Campsite, Yarra Bend Park, December 2004	C90
Hurstbridge Rail Line Upgrade 2017 Incorporated Document, January 2017	GC60
Incorporated Plan under the provisions of clause 43.01 Heritage Overlay, Planning permit exemptions, July 2014	C178
Local Policy “Protection of Biodiversity” Sites of Remnant Vegetation (Biosis 2001)	C49
Melbourne City Link Project – Advertising Sign Locations, November 2003	VC20
Melbourne Metro Rail Project: Upgrades to the Rail Network Incorporated Document, May 2018	GC96
North East Link Project Incorporated Document, December 2019	GC98
Planning and Design Principles for the Richmond Maltings Site, Cremorne – November 2007	C101
Richmond Walk Up Estate Redevelopment, September 2010	C136
Social housing redevelopment; Atherton Gardens Estate, Fitzroy, and Richmond Public Housing Estate, Richmond, for which the Minister for Planning is the Responsible Authority, May 2010	C135
Specific Site and Exclusion – Lot 2 on PS433628L (452 Johnston Street, Abbotsford	C56
Swan Street Works, Burnley, June 2005	C91

AMENDMENT C273YARA

Name of document	Introduced by:
Tramway Infrastructure Upgrades Incorporated Document, May 2017	GC68
Victoria Gardens Building Envelope and Precinct Plan and Precinct 3 Plan – Warehouse Area	C7
Victoria Gardens Urban Design Guidelines	NPS1
Victorian Institute of Forensic Mental Health Thomas Embling Hospital Incorporated Document, August 2020	C279yara
Walk Up Village, 81-89 Rupert Street, Collingwood – August 2020	C283yara
Yarra Gardens Precinct Plan, December 2009	C128
Yarra Development Contributions Plan 2017 (HillIPDA, April 2019)	C238yara
Yarra High Streets (Victoria Street and Bridge Road) Statements of Significance: Incorporated Document (May 2020)	C245yara
Alphington Link Incorporated Document (May 2022)	C300yara
The Post Office and the group of shops on Heidelberg Road Statement of Significance	C273yara
Cooper Knitting Factory (former) Statement of Significance	C273yara

**System Note: The following ordinance will be modified in Clause:72
ADMINISTRATION AND ENFORCEMENT OF THIS PLANNING SCHEME,
Sub-Clause:72.08 BACKGROUND DOCUMENTS**

C273yara

SCHEDULE TO CLAUSE 72.08 BACKGROUND DOCUMENTS

System Note: The following ordinance will be modified in Sub-Clause:72.08 BACKGROUND DOCUMENTS, Schedule:SCHEDULE TO CLAUSE 72.08 BACKGROUND DOCUMENTS

1.0

C273yara

Background documents

Name of background document	Amendment number - clause reference
<i>Lower Yarra River Study - Recommendations Report</i> (Department of Environment, Land Water and Planning, 2016)	VC197 - Schedule 1 to Clause 42.03 (SLO) and Schedule 1 to Clause 43.02 (DDO)
<i>Yarra Bend Park Strategy Plan</i> (Parks Victoria, 1999)	VC197 - Schedule 1 to Clause 42.03 (SLO)
<i>The Middle Yarra Concept Plan – Dights Falls to Burke Road</i> (Melbourne Parks and Waterways, 1990)	VC197 - Schedule 1 to Clause 42.03 (SLO)
<i>The Lower Yarra Concept Plan – Dights Falls to Punt Road</i> (Melbourne Metropolitan Board of Works, 1986)	VC197 - Schedule 1 to Clause 42.03 (SLO)
Heidelberg Road Heritage Review (Stage 1), RBA Architects Conservation Consultants 2019	C273yara - Schedule 18 to Clause 43.02 (DDO)
Heidelberg Road Heritage Review (Stage 2), RBA Architects Conservation Consultants 2019	C273yara - Schedule 18 to Clause 43.02 (DDO)
Part 2: Heidelberg Road Built Form Framework – Design Strategy and Recommendations, Hodyl & Co, November 2019	C273yara - Schedule 18 to Clause 43.02 (DDO)
Traffic and Vehicle Access Assessment, Heidelberg Road, Fairfield/Alphington, Traffix Group, November 2019	C273yara - Schedule 18 to Clause 43.02 (DDO)

HO450	Regent	Street	46-50 (Unit 9)	Richmond	Henry Walters' Boot Factory (Paragon Shoes)	281620	Individually Significant	1900-1915
HO450	Regent	Street	46-50 (Unit 10)	Richmond	Henry Walters' Boot Factory (Paragon Shoes)	281625	Individually Significant	1900-1915
HO450	Regent	Street	46-50 (Unit 11)	Richmond	Henry Walters' Boot Factory (Paragon Shoes)	281630	Individually Significant	1900-1915
HO450	Regent	Street	46-50 (Unit 12)	Richmond	Henry Walters' Boot Factory (Paragon Shoes)	281630	Individually Significant	1900-1915
HO450	Regent	Street	46-50 (Unit 13)	Richmond	Henry Walters' Boot Factory (Paragon Shoes)	281635	Individually Significant	1900-1915
HO450	Regent	Street	46-50 (Unit 14)	Richmond	Henry Walters' Boot Factory (Paragon Shoes)	281635	Individually Significant	1900-1915
HO450	Regent	Street	46-50 (Unit 15)	Richmond	Henry Walters' Boot Factory (Paragon Shoes)	281640	Individually Significant	1900-1915

HO451

HO451	Heidelberg	Road	730	Alphington	The Post Office and the group of shops on Heidelberg Road	359780	Individually Significant	1922
HO451	Heidelberg	Road	730A	Alphington	The Post Office and the group of shops on Heidelberg Road	359790	Individually Significant	1922
HO451	Heidelberg	Road	732	Alphington	The Post Office and the group of shops on Heidelberg Road	270910	Individually Significant	1922
HO451	Heidelberg	Road	734	Alphington	The Post Office and the group of shops on Heidelberg Road	270915	Individually Significant	1922

HO452

HO452	Egan	Street	(No Land Parcel Number In Landvic)	Richmond	West Richmond Station - Rail Bridge And Former Station Yard	0	Individually Significant	1900-1910
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HO453

HO453	Lincoln	Street	48	Richmond		154290	Contributory	1850-1890
HO453	Lincoln	Street	50	Richmond		154285	Contributory	1850-1890
HO453	Lincoln	Street	52	Richmond		154280	Not contributory	1950-1970
HO453	Lincoln	Street	54	Richmond		154275	Contributory	1850-1890
HO453	Lincoln	Street	56	Richmond		154270	Contributory	1850-1890
HO453	Lincoln	Street	58	Richmond		154265	Contributory	1850-1890
HO453	Lincoln	Street	60	Richmond		154260	Contributory	1850-1890
HO453	Lincoln	Street	62	Richmond		154255	Contributory	1850-1890

HO454

HO454	Church	Street	97	Richmond		150965	Contributory	1850-1890
HO454	Church	Street	99	Richmond		150970	Contributory	1850-1890
HO454	Church	Street	101	Richmond		150975	Contributory	1850-1890
HO454	Church	Street	103	Richmond	Shop & Residence	150980	Contributory	1850-1890
HO454	Church	Street	105	Richmond	Shop & Residence	150985	Contributory	1850-1890
HO454	Church	Street	107	Richmond	Shop & Residence	150990	Contributory	1850-1890
HO454	Church	Street	109-111	Richmond	Prince Of Wales Hotel	150995	Contributory	1850-1890
HO454	Church	Street	123-125	Richmond		151005	Contributory	1900-1915
HO454	Church	Street	127	Richmond	Shop & Residence	151010	Contributory	1850-1890
HO454	Church	Street	129	Richmond	Shop & Residence	151015	Contributory	1850-1890
HO454	Church	Street	133	Richmond	Queens Arms Hotel, Later Shop & Residence	151020	Contributory	1850-1890
HO454	Church	Street	135	Richmond	Bristol Hotel, Former	151025	Individually Significant	1860-1880
HO454	Church	Street	137	Richmond	Bakers Shop & Residence	151030	Contributory	1850-1890
HO454	Church	Street	139	Richmond	Vacant Site	151035	Not contributory	Unknown
HO454	Church	Street	141	Richmond	Shop & Residence	151040	Contributory	1850-1890
HO454	Church	Street	143	Richmond	Shop & Residence	151045	Contributory	1850-1890
HO454	Church	Street	145	Richmond	Shop & Residence	151050	Contributory	1850-1890
HO454	Church	Street	147-149	Richmond	Motor Garage	151055	Not contributory	1930-1940
HO454	Church	Street	156-162	Richmond	James Jones General Store, Shop & Residence, Former	150520	Contributory	1850-1890

HO454	Church	Street	164	Richmond	Naughten's Hotel, Later Citizens Park Hotel, And Dt's	189695	Individually Significant	1870-1890
HO454	Church	Street	166-170 (Unit 1)	Richmond	Weatherill's Butcher's Shop & Residence, Former?	282715	Not contributory	1860-2000
HO454	Church	Street	166-170 (Unit 2)	Richmond		282720	Not contributory	1990-2000
HO454	Church	Street	166-170 (Unit 3)	Richmond		282725	Not contributory	1990-2000
HO454	Church	Street	166-170 (Unit 6)	Richmond		282740	Not contributory	1990-2000
HO454	Church	Street	166-170 (Unit 7)	Richmond		282745	Not contributory	1990-2000
HO454	Church	Street	166-170 (Unit 8)	Richmond		282750	Not contributory	1990-2000
HO454	Church	Street	176	Richmond	House	356760	Contributory	1880-1890
HO454	Church	Street	176A	Richmond		360580	Not contributory	2000-2010
HO454	Church	Street	176A (First Floor)	Richmond		360590	Not contributory	2000-2010
HO454	Church	Street	178 (Ground Floor)	Richmond	James Lentell Building, Later Confectioner And Bakery	189710	Individually Significant	1870-1880
HO454	Church	Street	178 (First Floor)	Richmond	James Lentell Building, Later Confectioner And Bakery	189715	Individually Significant	1870-1880

HO455

HO455	Heidelberg	Road	760-764	Alphington	Cooper Knitting Factory (former)	352660	Individually Significant	1922
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HO456

HO456	Allans	Place	1	Richmond		292575	Not contributory	2000-2010
HO456	Allans	Place	2	Richmond		313470	Not contributory	2000-2010
HO456	Allans	Place	3	Richmond		292580	Not contributory	2000-2010
HO456	Allans	Place	4	Richmond		313480	Not contributory	2000-2010
HO456	Allans	Place	6	Richmond		192965	Not contributory	2000-2010
HO456	Allans	Place	8-10	Richmond		192960	Not contributory	2000-2010
HO456	Buckingham	Street	95	Richmond		154780	Contributory	1870-1890
HO456	Buckingham	Street	97	Richmond		154785	Contributory	1870-1890
HO456	Buckingham	Street	99	Richmond		154790	Contributory	1870-1890
HO456	Buckingham	Street	100	Richmond		155090	Contributory	1870-1890
HO456	Buckingham	Street	101	Richmond		154795	Contributory	1870-1890
HO456	Buckingham	Street	102	Richmond		155085	Contributory	1870-1890
HO456	Buckingham	Street	103	Richmond		154800	Contributory	1870-1890
HO456	Buckingham	Street	104	Richmond		155080	Contributory	1870-1890
HO456	Buckingham	Street	105	Richmond		154805	Contributory	1870-1890
HO456	Buckingham	Street	106	Richmond		155075	Contributory	1870-1890
HO456	Buckingham	Street	107	Richmond		154810	Not contributory	1870-1890
HO456	Buckingham	Street	108	Richmond		155070	Contributory	1870-1890
HO456	Buckingham	Street	110	Richmond		155065	Contributory	1870-1890
HO456	Buckingham	Street	111	Richmond		154815	Not contributory	Unknown
HO456	Buckingham	Street	112A	Richmond		155055	Not contributory	1990-2000
HO456	Buckingham	Street	112B	Richmond		155060	Not contributory	1990-2000
HO456	Buckingham	Street	113	Richmond		154820	Contributory	1870-1890
HO456	Buckingham	Street	114	Richmond		155050	Contributory	1870-1890
HO456	Buckingham	Street	115	Richmond		154825	Contributory	1925-1930
HO456	Buckingham	Street	116	Richmond		155045	Contributory	1870-1890
HO456	Buckingham	Street	117	Richmond	Vacant Site	154830	Not contributory	Unknown
HO456	Buckingham	Street	118	Richmond		155040	Contributory	1870-1890
HO456	Buckingham	Street	119	Richmond		154835	Contributory	1850-1890
HO456	Buckingham	Street	120	Richmond		155035	Contributory	1870-1890
HO456	Buckingham	Street	121	Richmond		154840	Contributory	1850-1890
HO456	Buckingham	Street	122	Richmond		155030	Contributory	1870-1890

HO361	Nicholson	Street	52	Fitzroy	Royal Terrace	265775	Victorian Heritage Register	1854-1856
HO361	Nicholson	Street	54	Fitzroy	Royal Terrace	265780	Victorian Heritage Register	1854-1856
HO361	Nicholson	Street	56	Fitzroy	Royal Terrace	265785	Victorian Heritage Register	1854-1856
HO361	Nicholson	Street	58	Fitzroy	Royal Terrace	265790	Victorian Heritage Register	1854-1856
HO361	Nicholson	Street	60	Fitzroy	Royal Terrace	265795	Victorian Heritage Register	1854-1856
HO361	Nicholson	Street	62	Fitzroy	Royal Terrace	265800	Victorian Heritage Register	1854-1856
HO361	Nicholson	Street	64	Fitzroy	Royal Terrace	265805	Victorian Heritage Register	1854-1856
HO361	Nicholson	Street	66	Fitzroy	Royal Terrace	265810	Victorian Heritage Register	1854-1856
HO361	Nicholson	Street	68	Fitzroy	Royal Terrace, part	265815	Victorian Heritage Register	1854-1856
HO361	Nicholson	Street	70-74	Fitzroy	Staffa House	265820	Individually Significant	1850s-
HO361	Nicholson	Street	76	Fitzroy		107500	Individually Significant	1860-1880
HO361	Nicholson	Street	78	Fitzroy		232200	Individually Significant	1860-1880
HO361	Nicholson	Street	80	Fitzroy		107505	Individually Significant	1860-1880
HO361	Nicholson	Street	80-82 (rear)	Fitzroy	Factory/warehouse/stable?	107505	Individually Significant	1900-1915?
HO361	Nicholson	Street	82	Fitzroy	Grantown House	265840	Individually Significant	1850-1870
HO361	Nicholson	Street	84	Fitzroy		265845	Individually Significant	1888
HO361	Nicholson	Street	86	Fitzroy		265850	Individually Significant	1888
HO361	Nicholson	Street	88	Fitzroy	Convent of Mercy Complex	265855	Victorian Heritage Register	1887-1890
HO361	Nicholson	Street	98	Fitzroy	Cairo Bachelor Flats, also Uxbridge fence	265860	Victorian Heritage Register	1936
HO361	Nicholson	Street	106	Fitzroy		266040	Individually Significant	1865
HO361	Nicholson	Street	108	Fitzroy	Waratah	266045	Individually Significant	1865
HO361	Nicholson	Street	110	Fitzroy		266050	Individually Significant	1863
HO361	Nicholson	Street	112	Fitzroy		266055	Individually Significant	1863
HO361	Nicholson	Street	114	Fitzroy		266060	Individually Significant	1858-
HO361	Nicholson	Street	116-118	Fitzroy	Dr Hewlett's house & surgery, later The Nunnery	266065	Individually Significant	1889
HO361	Nicholson	Street	120	Fitzroy		266070	Individually Significant	1888
HO361	Nicholson	Street	122	Fitzroy	Denny house	266075	Victorian Heritage Register	1862-
HO361	Nicholson	Street	124	Fitzroy	St Andrews Hotel, later Pumphouse Hotel	266080	Individually Significant	1926
HO361	Nicholson	Street	130	Fitzroy		266085	Individually Significant	1900-1915
HO361	Nicholson	Street	132-134	Fitzroy	Langridge House or Best house	266090	Individually Significant	1881, 1888
HO361	Nicholson	Street	136	Fitzroy	Broomfield	266095	Individually Significant	1884
HO361	Nicholson	Street	138	Fitzroy	Molesworth House	266100	Individually Significant	1870-1890
HO361	Nicholson	Street	140	Fitzroy		266105	Individually Significant	1860-1880
HO361	Nicholson	Street	144	Fitzroy		266110	Individually Significant	1871
HO361	Palmer	Street	8	Fitzroy		266340	Contributory	1870-1890
HO361	Palmer	Street	10	Fitzroy		266345	Contributory	1870-1890
HO361	Royal	Lane	3	Fitzroy	Stables	1	Individually significant	1860-1890

HO362

HO362	Adams	Street	7	Alphington	First Alphington Boy Scout Hall Number 20	272735	Individually Significant	1930-1940
HO362	Adams	Street	38	Alphington		271295	Not contributory	Unknown
HO362	Chamouni	Street	1	Alphington		273755	Contributory	1915-1925
HO362	Chamouni	Street	2	Alphington		273740	Contributory	1915-1925
HO362	Chamouni	Street	3	Alphington		273760	Contributory	1920-1930
HO362	Chamouni	Street	4	Alphington		273745	Contributory	1900-1915
HO362	Chamouni	Street	5	Alphington		273765	Not contributory	1930-1940
HO362	Chamouni	Street	6	Alphington		273750	Contributory	1915-1925
HO362	Chamouni	Street	7	Alphington		280830	Not contributory	1980-1900
HO362	Como	Street	1	Alphington		274340	Contributory	1915-1925
HO362	Como	Street	2	Alphington		273870	Contributory	1880-1890

HO362	Como	Street	3	Alphington		274345	Contributory	1900-1915
HO362	Como	Street	1/5	Alphington	L'espoir	335330	Contributory	1850-1890
HO362	Como	Street	2/5	Alphington	L'espoir	335340	Contributory	1850-1890
HO362	Como	Street	6	Alphington		273880	Not contributory	1930-1940
HO362	Como	Street	7	Alphington	Gladore	274355	Contributory	1880-1890
HO362	Como	Street	8	Alphington		273885	Not contributory	1950-1960
HO362	Como	Street	9	Alphington	Berkeley	274360	Contributory	1890-1900
HO362	Como	Street	10	Alphington		273890	Not contributory	1940-1950
HO362	Como	Street	11	Alphington		274365	Not contributory	1950-1960
HO362	Como	Street	12-14	Alphington		273895	Not contributory	1960-1970
HO362	Como	Street	13	Alphington	Lewis House	274370	Individually Significant	1980-1995
HO362	Como	Street	15	Alphington	Verona	274375	Contributory	1890-1900
HO362	Como	Street	16	Alphington		273905	Contributory	1900-1915
HO362	Como	Street	18	Alphington	Vacant	273915	Not contributory	Unknown
HO362	Como	Street	21	Alphington	Darrabyn	283430	Individually Significant	1900-1915
HO362	Como	Street	22	Alphington		273945	Contributory	1880-1890
HO362	Como	Street	23	Alphington		274390	Not contributory	1990-2000
HO362	Como	Street	1/24	Alphington		273965	Not contributory	1960-1970
HO362	Como	Street	2/24	Alphington		273975	Not contributory	1960-1970
HO362	Como	Street	3/24	Alphington		273985	Not contributory	1960-1970
HO362	Como	Street	4/24	Alphington		273995	Not contributory	1960-1970
HO362	Como	Street	5/24	Alphington		274005	Not contributory	1960-1970
HO362	Como	Street	6/24	Alphington		274015	Not contributory	1960-1970
HO362	Como	Street	7/24	Alphington		274025	Not contributory	1960-1970
HO362	Como	Street	8/24	Alphington		274035	Not contributory	1960-1970
HO362	Como	Street	9/24	Alphington		274045	Not contributory	1960-1970
HO362	Como	Street	10/24	Alphington		274055	Not contributory	1960-1970
HO362	Como	Street	11/24	Alphington		274065	Not contributory	1960-1970
HO362	Como	Street	12/24	Alphington		274075	Not contributory	1960-1970
HO362	Como	Street	25	Alphington	Willisau	274395	Contributory	1880-1890
HO362	Como	Street	26	Alphington		274085	Contributory	1900-1915
HO362	Como	Street	27	Alphington		274400	Contributory	1915-1925
HO362	Como	Street	28	Alphington		274095	Contributory	1890-1900
HO362	Como	Street	29	Alphington		274405	Contributory	1915-1925
HO362	Como	Street	30	Alphington		274105	Contributory	1900-1915
HO362	Como	Street	30a	Alphington		274115	Not contributory	1980-1990
HO362	Como	Street	30 (rear)	Alphington	Glasshouse	274105	Individually Significant	1880-1910
HO362	Como	Street	31	Alphington		274410	Not contributory	1970-1980
HO362	Como	Street	34	Alphington		274140	Contributory	1890-1900
HO362	Como	Street	35	Alphington		274420	Not contributory	1915-1925
HO362	Como	Street	36	Alphington		274145	Contributory	1890-1900
HO362	Como	Street	37	Alphington		301330	Not contributory	1990-2000
HO362	Como	Street	38	Alphington		274155	Contributory	1880-1890
HO362	Como	Street	39	Alphington		274430	Not contributory	1940-1950
HO362	Como	Street	40	Alphington		274165	Contributory	1880-1890
HO362	Como	Street	41	Alphington		274435	Individually Significant	1915-1925
HO362	Como	Street	42	Alphington		274175	Contributory	1880-1890
HO362	Como	Street	43	Alphington		274440	Contributory	1915-1925
HO362	Como	Street	44	Alphington		274185	Contributory	1880-1890
HO362	Como	Street	45	Alphington		274445	Contributory	1925-1935

HO362	Como	Street	46	Alphington		274195	Contributory	1915-1925
HO362	Como	Street	47	Alphington		274450	Contributory	1915-1925
HO362	Como	Street	1/48	Alphington		274205	Not contributory	1960-1970
HO362	Como	Street	2/48	Alphington		274220	Not contributory	1960-1970
HO362	Como	Street	3/48	Alphington		274230	Not contributory	1960-1970
HO362	Como	Street	4/48	Alphington		274240	Not contributory	1960-1970
HO362	Como	Street	5/48	Alphington		274250	Not contributory	1960-1970
HO362	Como	Street	6/48	Alphington		274260	Not contributory	1960-1970
HO362	Como	Street	7/48	Alphington		274270	Not contributory	1960-1970
HO362	Como	Street	8/48	Alphington		274280	Not contributory	1960-1970
HO362	Como	Street	9/48	Alphington		274290	Not contributory	1960-1970
HO362	Como	Street	10/48	Alphington		274295	Not contributory	1960-1970
HO362	Como	Street	11/48	Alphington		274305	Not contributory	1960-1970
HO362	Como	Street	12/48	Alphington		274315	Not contributory	1960-1970
HO362	Como	Street	49	Alphington		274455	Contributory	1900-1915
HO362	Como	Street	52	Alphington		274320	Not contributory	1960-1970
HO362	Como	Street	54	Alphington		274325	Contributory	1915-1925
HO362	Como	Street	56	Alphington		274330	Contributory	1900-1915
HO362	Como	Street	58	Alphington		274335	Contributory	1925-1930
HO362	Killop-	Street	2	Alphington		271895	Contributory	1915-1925
HO362	Lucerne	Crescent	2	Alphington		291115	Individually Significant	1900-1915
HO362	Lucerne	Crescent	8	Alphington		272260	Contributory	1915-1925
HO362	Lucerne	Crescent	10	Alphington		272265	Contributory	1930-1940
HO362	Lucerne	Crescent	30	Alphington		272280	Contributory	1880-1890
HO362	Lucerne	Crescent	32	Alphington		272285	Not contributory	1960-1970
HO362	Lucerne	Crescent	34	Alphington		272290	Contributory	1915-1925
HO362	Lucerne	Crescent	36	Alphington		272295	Contributory	1915-1925
HO362	Lucerne	Crescent	37	Alphington	Loddon Lea	272490	Individually Significant	1900-1915
HO362	Lucerne	Crescent	48	Alphington		272315	Contributory	1900-1915
HO362	Lucerne	Crescent	50	Alphington		272320	Not contributory	1960-1970
HO362	Lucerne	Crescent	50a	Alphington		272325	Not contributory	Unknown
HO362	Lucerne	Crescent	52	Alphington		272330	Contributory	1915-1925
HO362	Lucerne	Crescent	56	Alphington	Frater House And Studio	272340	Individually Significant	1915-1925
HO362	Lucerne	Crescent	60	Alphington		272345	Contributory	1915-1925
HO362	Lucerne	Crescent	67	Alphington	Loddon Lee	272545	Contributory	1900-1915
HO362	Lucerne	Crescent	69	Alphington		272550	Not contributory	1960-1970
HO362	Lucerne	Crescent	73	Alphington		272555	Contributory	1915-1925
HO362	Lucerne	Crescent	85	Alphington		272575	Contributory	1915-1925
HO362	Lucerne	Crescent	87	Alphington		272580	Contributory	1900-1915
HO362	Lucerne	Crescent	91	Alphington	Mona	272585	Contributory	1900-1915
HO362	Lucerne	Crescent	95	Alphington		272595	Contributory	1915-1925
HO362	Lucerne	Crescent	97	Alphington		272600	Contributory	1915-1925
HO362	Park	Avenue	9	Alphington		274100	Contributory	1925-1935
HO362	Parkview	Road	28	Alphington		273610	Contributory	1900-1915
HO362	Phillips	Street	2	Alphington		273725	Contributory	1915-1925
HO362	Phillips	Street	4	Alphington		273730	Contributory	1900-1915
HO362	Riverview	Grove	1	Alphington		274060	Contributory	1915-1925
HO362	Riverview	Grove	3	Alphington		274070	Contributory	1915-1925
HO362	Riverview	Grove	1/5	Alphington		286480	Not contributory	1970-1980
HO362	Riverview	Grove	2/5	Alphington		286485	Not contributory	1970-1980

HO362	Riverview	Grove	7	Alphington		274090	Contributory	1915-1925
HO362	Riverview	Grove	11	Alphington		274110	Contributory	1915-1925
HO362	Riverview	Grove	13	Alphington		274120	Not contributory	1930-1940
HO362	Roemer	Crescent	2	Alphington		274130	Contributory	1915-1925
HO362	Roemer	Crescent	4	Alphington		274135	Contributory	1915-1925
HO362	Roemer	Crescent	5	Alphington		274225	Contributory	1880-1890
HO362	Roemer	Crescent	6	Alphington		274150	Contributory	1915-1925
HO362	Roemer	Crescent	8	Alphington		274160	Contributory	1915-1925
HO362	Roemer	Crescent	10	Alphington		274170	Contributory	1915-1925
HO362	Roemer	Crescent	12	Alphington		274180	Contributory	1915-1925
HO362	Roemer	Crescent	14	Alphington		274190	Not contributory	1915-1925
HO362	Roemer	Crescent	16	Alphington		274200	Contributory	1915-1925
HO362	St Bernards	Road	5	Alphington		270040	Contributory	1890-1900
HO362	St Bernards	Road	7	Alphington		270045	Contributory	1890-1900
HO362	St Bernards	Road	9	Alphington		270050	Contributory	1890-1900
HO362	St Bernards	Road	11	Alphington		270055	Contributory	1890-1900
HO362	St Bernards	Road	13	Alphington	Elimatta	270060	Contributory	1890-1910
HO362	St Gothards	Road	1	Alphington		270145	Contributory	1900-1915
HO362	St Gothards	Road	1a	Alphington		270150	Not contributory	Unknown
HO362	St Gothards	Road	2	Alphington		270110	Contributory	1915-1925
HO362	St Gothards	Road	3	Alphington		270160	Individually Significant	1900-1915
HO362	St Gothards	Road	3a	Alphington		270155	Contributory	1930-1940
HO362	St Gothards	Road	4	Alphington		270115	Contributory	1915-1925
HO362	St Gothards	Road	5	Alphington		270165	Contributory	1915-1925
HO362	St Gothards	Road	6	Alphington		270120	Contributory	1915-1925
HO362	St Gothards	Road	7	Alphington		270170	Not contributory	1990-2000
HO362	St Gothards	Road	8	Alphington		270125	Contributory	1915-1925
HO362	St Gothards	Road	9	Alphington		270175	Individually Significant	1900-1915
HO362	St Gothards	Road	10	Alphington		270130	Not contributory	Unknown
HO362	St Gothards	Road	12	Alphington		270135	Contributory	1890-1900
HO362	St Gothards	Road	14	Alphington	Inglegarra	270140	Contributory	1890-1910
HO362	View	Street	2	Alphington		271040	Contributory	1915-1925
HO362	View	Street	4	Alphington		271045	Contributory	1920-1935
HO362	View	Street	6	Alphington		271050	Contributory	1930-1940
HO362	View	Street	8	Alphington		271055	Contributory	1930-1940
HO362	View	Street	10	Alphington		271060	Contributory	1915-1925
HO362	View	Street	12	Alphington		271065	Contributory	1915-1925
HO362	View	Street	14	Alphington		271070	Contributory	1930-1940
HO362	View	Street	16	Alphington		271075	Contributory	1930-1940
HO362	View	Street	18	Alphington		271080	Contributory	1930-1940
HO362	View	Street	20	Alphington		271085	Contributory	1930-1940
HO362	View	Street	22	Alphington		271090	Contributory	1930-1940
HO362	View	Street	24	Alphington		271095	Contributory	1930-1940
HO362	View	Street	26	Alphington		271100	Contributory	1930-1940
HO362	View	Street	28	Alphington		271115	Contributory	1925-1930
HO362	View	Street	30	Alphington		271120	Contributory	1925-1930
HO362	View	Street	32	Alphington		271130	Contributory	1915-1925
HO362	View	Street	34	Alphington		271140	Contributory	1925-1930
HO362	View	Street	36	Alphington		271150	Contributory	1930-1940
HO362	View	Street	38	Alphington		271160	Contributory	1930-1940

HO362	View	Street	40	Alphington		271170	Contributory	1930-1940
HO362	View	Street	42	Alphington		271180	Contributory	1925-1930
HO362	View	Street	44	Alphington		271195	Contributory	1930-1940
HO362	View	Street	48	Alphington		271200	Contributory	1915-1925
HO362	Yarralea	Street	2	Alphington		271730	Contributory	1925-1930
HO362	Yarralea	Street	3	Alphington		272105	Contributory	1915-1925
HO362	Yarralea	Street	4	Alphington		271735	Contributory	1915-1925
HO362	Yarralea	Street	5	Alphington		272110	Contributory	1915-1925
HO362	Yarralea	Street	6	Alphington		271740	Not contributory	1930-1940
HO362	Yarralea	Street	7	Alphington		272115	Contributory	1915-1925
HO362	Yarralea	Street	8	Alphington		271745	Contributory	1915-1925
HO362	Yarralea	Street	9	Alphington		272120	Contributory	1915-1925
HO362	Yarralea	Street	11	Alphington		272125	Contributory	1915-1925
HO362	Yarralea	Street	15	Alphington		272130	Contributory	1915-1925
HO362	Yarralea	Street	17	Alphington		272135	Not contributory	1970-1980
HO362	Yarralea	Street	19	Alphington		272140	Contributory	1915-1925
HO362	Yarralea	Street	21	Alphington		272145	Contributory	1915-1925
HO362	Yarralea	Street	23	Alphington		272150	Contributory	1915-1925
HO362	Yarralea	Street	25	Alphington		272155	Not contributory	1925-1930
HO362	Yarralea	Street	26	Alphington	Alphington State School	271910	Contributory	1915-1925
HO362	Yarralea	Street	27	Alphington		272160	Contributory	1915-1925
HO362	Yarralea	Street	29	Alphington		272165	Contributory	1915-1925
HO362	Yarralea	Street	33	Alphington		272170	Not contributory	1970-1980
HO362	Yarralea	Street	35	Alphington		272175	Contributory	1915-1925
HO362	Yarralea	Street	37	Alphington		272180	Not contributory	1915-1925
HO362	Yarralea	Street	39	Alphington		272185	Not contributory	1915-1925
HO362	Yarralea	Street	42	Alphington		271970	Contributory	1900-1915
HO362	Yarralea	Street	43	Alphington		272190	Contributory	1900-1915
HO362	Yarralea	Street	44	Alphington		271975	Contributory	1900-1915
HO362	Yarralea	Street	45	Alphington		272195	Contributory	1915-1925
HO362	Yarralea	Street	46	Alphington		271980	Contributory	1900-1915
HO362	Yarralea	Street	47	Alphington		272200	Contributory	1900-1915
HO362	Yarralea	Street	48	Alphington		271985	Contributory	1900-1915
HO362	Yarralea	Street	1/49	Alphington		272205	Not contributory	1970-1980
HO362	Yarralea	Street	2/49	Alphington		272210	Not contributory	1970-1980
HO362	Yarralea	Street	3/49	Alphington		272215	Not contributory	1970-1980
HO362	Yarralea	Street	4/49	Alphington		272220	Not contributory	1970-1980
HO362	Yarralea	Street	53	Alphington		272225	Contributory	1915-1925
HO362	Yarralea	Street	55	Alphington		272230	Contributory	1915-1925
HO362	Yarralea	Street	57	Alphington		299305	Contributory	1970-1980
HO362	Yarralea	Street	59	Alphington		272240	Individually Significant	1915-1925

HO363

HO363	Bennett	Street	1	Richmond		152025	Not contributory	1915-1925
HO363	Bennett	Street	1a	Richmond		152030	Contributory	1920-1930
HO363	Bennett	Street	2	Richmond		151720	Contributory	1900-1915
HO363	Bennett	Street	2a	Richmond		151725	Contributory	1915-1925
HO363	Bennett	Street	2aa	Richmond		151715	Contributory	1915-1925
HO363	Bennett	Street	3	Richmond		152020	Contributory	1900-1915
HO363	Bennett	Street	4	Richmond		151730	Contributory	1915-1925

SCHEDULE 18

SCHEDULE 18 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY

Shown on the planning scheme map as **DDO18**.

HEIDELBERG ROAD PRECINCTS

1.0 Design objectives

- To ensure development supports:
 - a new mid-rise contemporary character with buildings providing a street wall to frame Heidelberg Road and a transition in building scale down towards each of the adjacent parklands in Precinct 1;
 - an emerging low-rise commercial character which comprises development set behind a landscape strip, with a consistent street wall, and recessive upper levels along Heidelberg Road in Precinct 2;
 - a new contemporary mid-rise development in Precinct 3A that addresses the prominent corner location, without competing with the adjacent high-rise development on the east side of Chandler Highway, and which provides perimeter landscape setbacks as well as streetwall and building heights that transition down in scale to the adjacent low rise forms in Coate Avenue;
 - a new low-rise character with a mix of retained heritage and complementary street wall heights along Heidelberg Road, and a landscape setback between Yarralea Street and Como Street in Precinct 3B.
- To ensure development delivers a high quality landscaped interface that incorporates canopy trees (where appropriate), openness and a significantly improved pedestrian amenity along Heidelberg Road providing passive surveillance and activated, pedestrian-oriented façades.
- To ensure development responds to heritage fabric through recessive upper level development, a legible transition in scale from taller building forms towards the interface with heritage buildings, and retains the prominence of and key view lines to the former 'Porta' chimney and heritage factory at 224-256 Heidelberg Road, Fairfield.
- To ensure development responds to sensitive interfaces by ensuring the overall scale and form of new development provides a legible transition to low-rise residential areas and protects these properties from an unreasonable loss of amenity through visual bulk, overlooking and overshadowing.

2.0 Buildings and works

A permit is not required to:

- extend a ground floor at the rear provided:
 - the maximum building height is not more than 4 metres above ground level.
- alter an existing building façade provided:
 - the alteration does not include the installation of an external roller shutter;
 - in a C1Z, at least 80 per cent of the building facade at ground floor level is maintained as an entry or window with clear glazing.
- construct an awning to an existing building that projects over a road, if it is authorised by the relevant public land manager.

2.1 Definitions

Heritage building means any building subject to a Heritage Overlay, graded as either Contributory or Individually Significant (including properties on the Victorian Heritage Register).

Laneway means a road reserve of a public road 9 metres or less wide.

Parapet height does not include features such as brackets, pediments, urns, finials or other decorative elements.

Public realm means all streets and spaces open to the public but does not include laneways.

Street wall means the facade of a building at the street boundary or if a front setback is required in this DDO, the front of the building.

Street wall height means the height of the street wall measured at the vertical distance between the footpath at the centre of the frontage and the highest point of the building at the street wall, with the exception of architectural features and building services.

Upper level means development above the height of the street wall.

Upper level setback means the minimum distance between the development above the height of the street wall (including projections such as balconies, building services and architectural features) and the street wall.

Street boundary means the boundary between the public street and the private property.

Rear interface is the rear wall of any proposed building or structure whether on the property boundary or set back from the property boundary.

2.2 General Requirements

The requirements below (including both the General Requirements and Precinct Design Requirements) apply to an application to construct a building or construct or carry out works.

A permit cannot be granted to vary a requirement expressed with the term 'must'.

2.2.1 Street wall height requirements

Development must not exceed the mandatory maximum street wall heights as shown on Maps 2, 3A, 3B.

Development should not exceed the preferred maximum street wall heights as shown on Maps 1, 2, 3A and 3B unless all the following requirements are met, to the satisfaction of the Responsible Authority:

- the built form outcome as a result of the proposed variation satisfies the Design Objectives at Clause 1.0 of this Schedule;
- the proposed street wall height provides an appropriate transition, scaling down to the interface with a heritage building;
- the proposed street wall height does not visually overwhelm the adjoining heritage building; and
- the proposed street wall height provides an appropriate transition, scaling down to the interface with low rise residential areas.

Infill development adjoining a heritage building should match the parapet height of the adjoining building for a minimum of 6 metres in length.

The street wall on corner buildings should continue the main frontage street wall height for a minimum of 8 metres to the side street, but then transition down in height to match the rear or side interface as required.

Development of non-heritage buildings on street corners should provide a corner splay at minimum of 1 x 1 metre at the site's corner boundaries.

Development should retain the visual prominence of:

- the heritage street wall in the vistas along the street;
- heritage fabric of the return facades of heritage buildings on corner sites.

2.2.2 Upper level front and side street setback requirements

Upper levels above the Heidelberg Road, Yarra Bend Road, Park Crescent, Chandler Highway and Coate Avenue street walls:

- must be setback by a minimum of 6 metres in Precinct 3B from Heidelberg Road between Parkview Road and Yarralea Street;
- must be setback by a minimum of 10 metres from Coate Avenue in Precinct 3A and must be set back an additional minimum of 10 metres above the secondary step;

- should be set back by a minimum of 6 metres in:
 - Precinct 1
 - Precinct 2
 - Precinct 3A from Heidelberg Road and Chandler Highway
 - Precinct 3B from Heidelberg Road between Yarralea Street and Como Street

Development should be setback at upper levels a minimum of 3 metres above a side street wall.

Upper levels should:

- be visually recessive when viewed from the public realm to ensure development does not overwhelm the streetscape and minimises upper level bulk;
- contain upper level setbacks above the street wall within a maximum of two steps (including the setback above the street wall below as one step) to avoid repetitive steps in the built form.

Heritage Buildings:

Should be setback in excess of the minimum upper level setback requirements where:

- it would facilitate the retention of a roof form and/or chimneys that are visible from the public realm, or a roof or any feature that the relevant statement of significance identifies as contributing to the significance of the heritage building or streetscape;
- it would maintain the perception of the three-dimensional form and depth of the building;
- a lesser setback would detract from the character of the streetscape when viewed directly or obliquely along the street.

2.2.3 Building height requirements

Development must not exceed the mandatory maximum building heights shown on Map 3A.

Development should not exceed the preferred maximum building heights shown on Maps 1, 2, and 3B.

A permit should only be granted to construct a building or construct or carry out works which exceeds the preferred maximum building height shown on Maps 1, 2 and 3B where, in addition to other requirements of this DDO, all the following requirements are met to the satisfaction of the responsible authority:

- the built form outcome satisfies:
 - the relevant Design Objectives in Clause 1.0;
 - the Overshadowing and Daylight Access Requirements in Clause 2.2.5;
- the proposal will achieve each of the following:
 - greater building separation than the minimum requirements in this schedule;

- excellence for environmental sustainable design measured as a minimum BESS project score of 70%; and
 - no additional overshadowing impacts on secluded private open space to residentially zoned properties or adjacent parkland or reserves, beyond that which would be generated by a proposal that complies with the preferred building height.
- where the proposal includes dwellings, it also achieves:
 - communal and/or private open space provision that exceeds the minimum standards in Clause 55.07 and/or 58, as relevant.

Architectural features (except service equipment or structures) may exceed the mandatory or preferred maximum building height.

Service equipment and/or structures including balustrades, unenclosed pergolas for communal areas, roof terraces, shading devices, plant rooms, lifts, stair wells, structures associated with pedestrian access, green roof areas and other such equipment may exceed the mandatory or preferred maximum height provided that:

- the equipment/structures do not cause additional overshadowing of secluded private open space to residential land, opposite footpaths, kerb outstands or planting areas in the public realm; and
- the equipment/structures are no higher than 2.6 metres above the mandatory or preferred maximum height; and
- the equipment/structures (other than solar panels, green roofs and roof terraces) occupy less than 50 per cent of the roof area.

2.2.4 Interface and rear setback requirements

Development adjoining a residential property outside this overlay must not exceed a maximum boundary wall height of 8 metres.

Development along the parkland interface in Precinct 1 must not exceed a maximum building height of 14.4 metres (as shown on Map 1).

Upper levels above a rear boundary wall must be set back from the rear boundary and be contained within a 45 degree setback envelope. The envelope's angle is to be measured perpendicular to the development site's boundary, taken from the centre of the boundary.

Upper level setbacks above the rear boundary wall should be contained within a maximum of two steps (including the setback above the boundary wall below as one step) or be contained within a sloped façade to avoid repetitive stepping of individual levels.

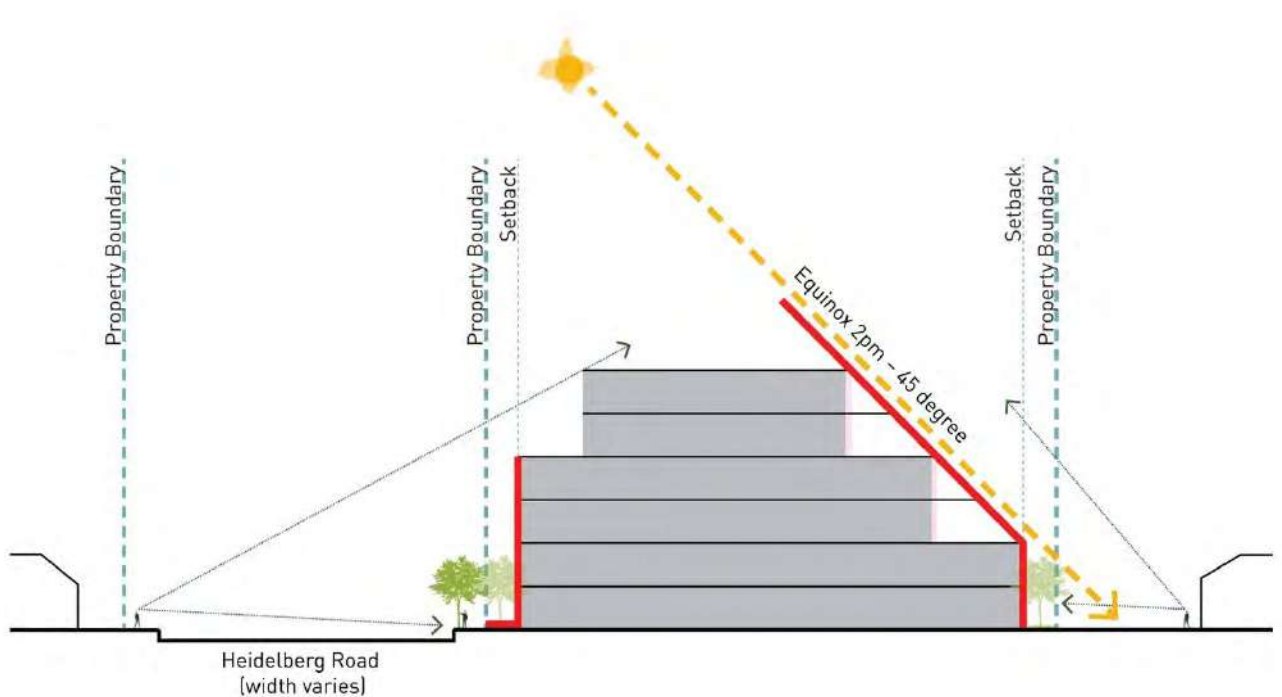
Development should respond to existing secluded private open spaces by setting back at upper levels to create a sense of separation, minimise overshadowing and reduce building bulk.

Development should not visually dominate adjoining residential sites outside this overlay.

Development must provide minimum ground floor setbacks to the rear boundary as follows:

- Precinct 1: a minimum of 3 metres (as shown on Map 1);
- Precinct 3A: a minimum of 4.5 metres;
- All other areas: a minimum of 3 metres if the dwelling on the adjacent residential property is located less than 15 metres from the property boundary.

Figure 1 – Indicative Cross Section and Measurements



2.2.5 Overshadowing and daylight access requirements

Development should meet the objective of Clause 55.04-5 Overshadowing for adjoining land within a residential zone, including where separated by a laneway.

Development should not overshadow:

- the opposite footpath of a side street, from property boundary to kerb between 10 am and 2 pm on 22nd September; and
- any opposite kerb outstands, seating and/or planting areas (as applicable), between 10am and 2pm on 22 September.

Development should not increase the amount of overshadowing to Yarra Bend Park, TH Westfield Reserve and surrounding open space, as caused by existing conditions, measured between 10am and 2pm on 22 September.

2.2.6 Building separation, amenity and equitable development requirements

An application for development should provide a design response that considers the future development opportunities of adjacent properties in terms of outlook, daylight and solar access to windows, as well as managing visual bulk.

Where development shares a common boundary within the overlay, upper level development should:

- for buildings up to 14.4 metres (or 16 metres in the Commercial 2 Zone), be setback 4.5m from the common boundary, where a habitable window or balcony facing the common boundary is proposed on the subject site and/or exists on the adjoining property; and
- for buildings up to 14.4 metres (or 16 metres in the Commercial 2 Zone), be setback 3.0m from the common side boundary where a commercial or non-habitable window facing the common boundary is proposed on the subject site and/or exists on the adjoining property.

Where the common boundary is a laneway, the setback is measured from the centre of the laneway.

Where development consists of multiple buildings and/or separate upper levels, upper level development should:

- be setback a minimum of 9m from each other, where a habitable window or balcony is proposed; and
- be setback a minimum of 6m from each other where a commercial or non-habitable window is proposed.

2.2.7 Building layout requirements

Lower levels of development should:

- Be designed to accommodate commercial activity at the ground floor, incorporating a commercial floor height of approximately 4 metres floor to floor height;
- Incorporate adaptable commercial and residential floor layouts, demonstrating how each could be combined or divided so as to allow for a variety of uses over time.

2.2.8 Front setback design requirements

Front setbacks (as identified for each precinct in Clause 2.3 Precinct Design Requirements under the heading ‘front setback requirements’) should be designed to create a sense of openness and clear line of sight at pedestrian level between the public footpath and street wall.

Front setback design should provide a high quality landscaped interface that significantly enhances the pedestrian experience along Heidelberg Road.

2.2.9 Other design requirements

Development should achieve good urban design outcomes and architectural excellence by including, but not being limited to:

- achieving active frontage design at ground level to create a pedestrian-oriented environment and passive surveillance towards the public realm;
- achieving fine-grain commercial façade design at ground floor for development in the Commercial 1 Zone;
- creating an appropriate ratio of solid and void elements;
- creating visual interest through the arrangement of fenestration, balconies and the application of architectural features including external shading devices, windowsills;
- maintaining an appropriate level of design simplicity by avoiding overly busy facades that rely on a multitude of materials and colours;
- maintaining existing openings and the inter-floor height of a heritage building and avoid new floor plates and walls cutting through historic openings;
- avoiding highly reflective glazing in openings of heritage buildings;
- encouraging the retention of solid built form behind retained heritage facades and avoid balconies behind openings so as to avoid facadism;
- not competing with the more elaborate detailing of the heritage building(s) on the subject site or adjoining land;
- avoiding large expanses of glazing with a horizontal emphasis; and
- ensuring projections such as balconies, building services, architectural features (other than shading devices, mouldings etc.) do not intrude into a setback and do not visually dominate the façade.

Lower levels of development should:

- avoid large expanses of facades with floor to ceiling glazing and limited entries at the ground floor;
- allow unobstructed views through openings into the ground floor of buildings;
- on sites where no street setback requirement is identified and where abutting narrow footpaths of less than 1.8 metres, provide for front setbacks and/or generous, recessed building entrances to provide space for pedestrian circulation and include space for landscaping, outdoor trading, seating and/or visitor bicycle parking;
- locate building service entries/access doors and cabinets away from the primary street frontage, or where not practically possible, they should be sensitively designed to integrate into the façade of the building and complement the street frontage and character.

The design of upper levels of development should:

- distinguish between the lower and upper levels through materials and articulation, with visually lightweight materials and colours applied above the street wall;
- be designed so that side walls are articulated and read as part of the overall building design and not detract from the streetscape when viewed from direct and oblique views along the streetscape.

Development should avoid blank walls visible to the public realm, including from side streets.

Side walls in a mid-block location which are visible permanently or temporarily from adjoining residential sites and/or the public realm should be designed to provide visual interest to passing pedestrians through colour, texture, materials and/or finishes.

Projections such as building services and architectural features (other than shading devices, mouldings etc.), balconies and balustrades should not protrude into a street wall and an upper level setback, except for terraces directly above a podium, as applicable.

Development interfacing with areas of public open space should:

- provide a suitable transition in scale to the interface with the public open space;
- ensure that development does not visually dominate the public open space;
- provide passive surveillance from lower and upper levels of the building.

2.2.10 Access, parking and loading bay requirements

Pedestrian access to buildings should be achieved via streets and avoid primary access from laneways. Where pedestrian access from a laneway is appropriate, the building setback should include a pedestrian refuge or landing.

Ensure pedestrian entrances are clearly visible, secure, well lit and have an identifiable sense of address.

Residential and commercial pedestrian entrances should be distinguishable from each other.

The common pedestrian areas of new buildings should be designed with legible and convenient access, with hallway and lobby areas of a size that reflects the quantity of apartments serviced and which can be naturally lit and naturally ventilated.

Car parking should be located within a basement or concealed from the public realm.

Bicycle parking should be provided to the rates and design guidelines recommended in the Built Environment Sustainability Scorecard (BESS) tool.

Bicycle ingress/egress should be provided directly from adjacent bicycle lanes and paths.

Resident and staff bicycle parking should be located and designed to be secure and conveniently accessible from the street and associated uses.

Vehicle ingress and egress into development, including loading facilities and building servicing, should be designed to ensure a high standard of pedestrian amenity and limit potential conflict between vehicle movements and pedestrian activity and avoid adversely impacting the continuity of the public realm.

Vehicle ingress/egress points should be spaced apart from other existing and/or proposed ingress/egress points to avoid wide crossover points.

Development should not provide new vehicular access from Heidelberg Road and avoid disruptions to bicycle lanes.

Development should consolidate multiple vehicle access points along Heidelberg Road, where applicable.

Development with redundant vehicle access points should reinstate the kerb, line-marked parking bays, and relocate any parking signs.

Where a ground level setback is provided to achieve practicable vehicle access to a laneway, a minimum headroom clearance of 3.6 metres should be provided to any overhang of the first floor and careful consideration given to create a safe pedestrian environment.

Properties on the inside corner of bends in laneways or at intersections between two laneways should provide a minimum 3m x 3m splay to facilitate vehicle access.

2.3 Precinct Design Requirements

2.3.1 Precinct 1

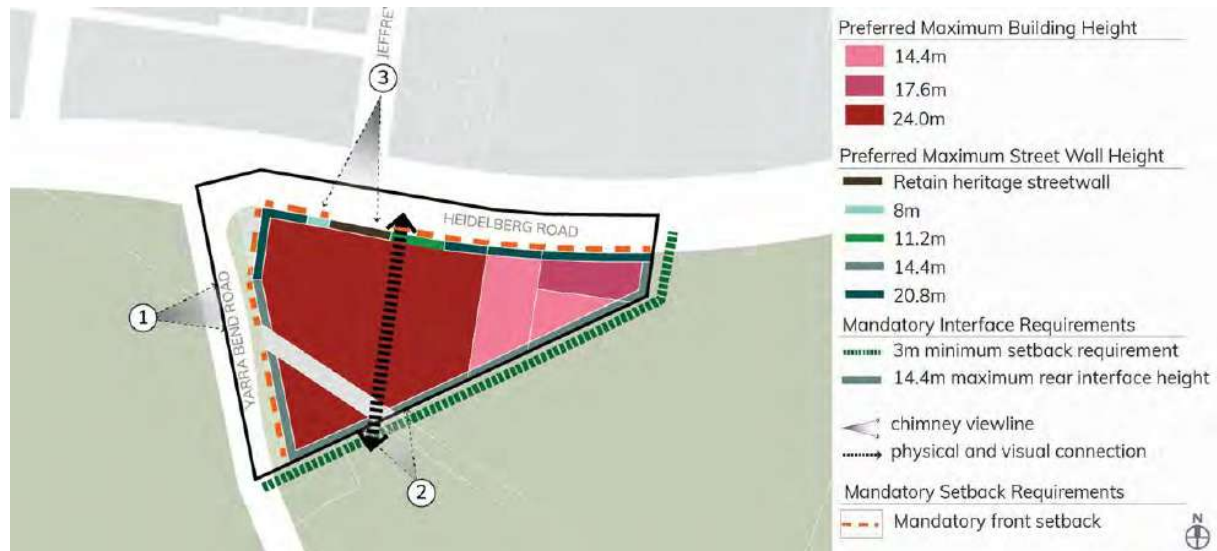
Design requirements

Development should:

- be separated into multiple buildings and provide a fine grain subdivision pattern.
- achieve a character that resembles a mix of old industrial and new commercial towards Heidelberg Road and fine-grain residential development that is sensitive to the surrounding parklands including Yarra Bend Park, TH Westfield Reserve and surrounding open space.
- create a sense of openness around the heritage factory building and chimney by gradually stepping down towards the factory and creating a strong separation from the chimney.
- achieve open view lines to the chimney from the TH Westfield Reserve to the south, opposite footpath on Yarra Bend Road and the corner of Jeffrey Street and Heidelberg Road, as shown on Map 1.
- consider the adaptive re-use of the heritage buildings and/or integrate them with any new buildings on the site.
- provide a physical and visual connection from Heidelberg Road through to TH Westfield Reserve to the south, on land to the east of the building affected by Heritage Overlay 421.
- provide for a communal space next to the building affected by Heritage Overlay 421.
- provide a positive interface (visual interest and passive overlooking) to Yarra Bend Park, TH Westfield Reserve and surrounding open space that does not visually dominate the landscape setting.
- provide for canopy trees throughout the site to create a stronger connection with the surrounding parklands, provide for shade and to help reduce the heat island effect.
- utilise natural materials and colours to minimise the dominance of its bulk and blend in with the surrounding parklands, including vertical greening.

Front setback requirements

Development in Precinct 1 must be setback by a minimum of 3 metres to Heidelberg Road (except for heritage buildings) and must be setback by a minimum of 3 metres to Yarra Bend Road to provide better separation with Heidelberg Road and space for circulation and canopy landscaping.



Map 1: Building and Street Wall Heights for Precinct 1

2.3.2 Precinct 2

Design requirements

Development along Heidelberg Road should:

- achieve an active commercial façade.
- create a pedestrian-oriented environment at lower levels to improve pedestrian amenity, safety and the vibrancy of the area.

Development with dual frontages to Heidelberg Road and Park Crescent should create a street wall behind a front setback towards Park Crescent to not dominate the low, residential character of Park Crescent.

Front setback requirements

Development in Precinct 2 must be setback by a minimum of 3 metres to Heidelberg Road and must be setback by a minimum of 3 metres to Park Crescent to provide better separation with Heidelberg Road and space for circulation and landscaping.



Map 2: Building and Street Wall Heights for Precinct 2

2.3.3 Precinct 3A

Design requirements

Development along Heidelberg Road and Chandler Highway should:

- achieve a fine grain, activated commercial building façade at the street wall levels.
- create a pedestrian-oriented environment at lower levels to improve pedestrian amenity, safety and the vibrancy of the area.

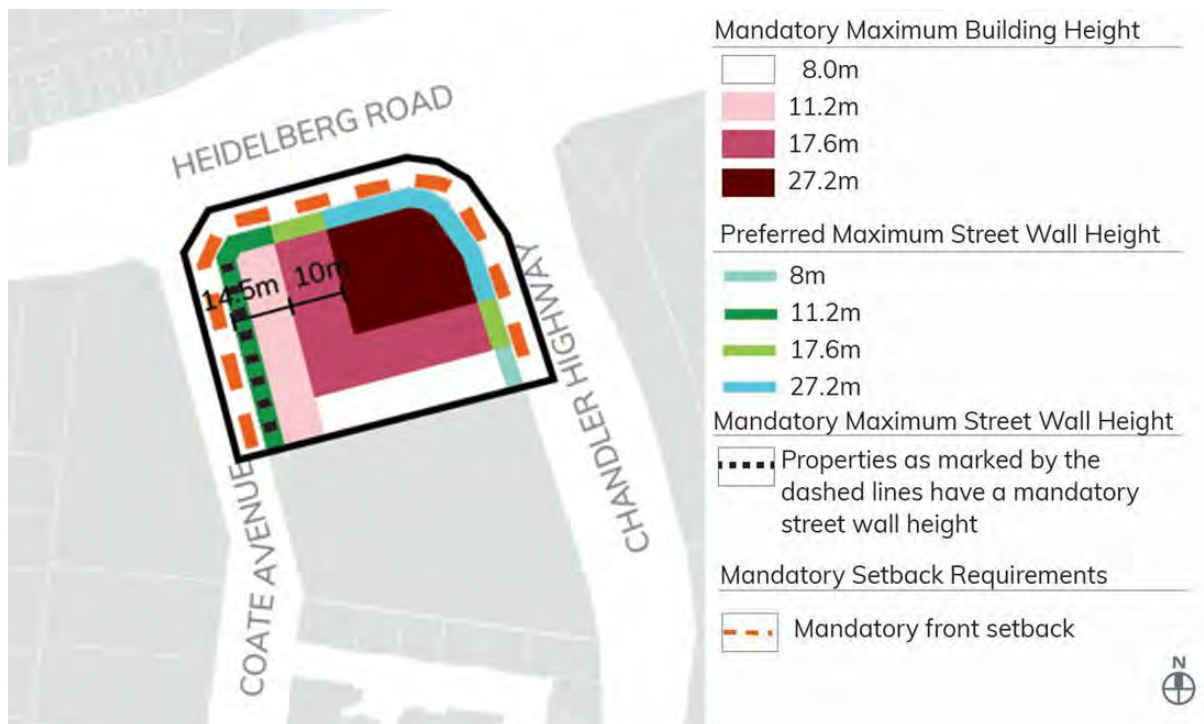
Along Coate Avenue, development should achieve a fine-grain, lower residential character to blend in with the character of the street.

Development should achieve a sense of openness by providing strong separation of upper levels from Coate Avenue and residential properties to the rear.

Front setbacks should be designed and landscaped to include canopy trees and blend in with the residential character of the street.

Front setback requirements

Development in Precinct 3A must be setback by a minimum of 3 metres to Heidelberg Road and Chandler Highway and must be setback by a minimum of 4.5 metres to Coate Avenue to provide better separation with Heidelberg Road and space for circulation and canopy landscaping.



Map 3A: Building and Street Wall Heights for Precinct 3A

2.3.4 Precinct 3B

Design requirements

Buildings should achieve a fine-grain, retail character along Heidelberg Road that includes a stall riser, pilasters, a verandah or canopy (where applicable) and clerestory window above the verandah or canopy.

Development along Heidelberg Road should provide active frontages to improve pedestrian amenity, safety and the vibrancy of the area.

Front setbacks east of Yarralea Street should be designed to include canopy trees and soft landscaping to create a more pedestrian-friendly environment and avoid the dominance of car parking areas.

Front setback requirements

Development in Precinct 3B must be setback by a minimum of 3 metres to Heidelberg Road between Parkview Road and Park Avenue to provide better separation with Heidelberg Road and space for circulation and canopy landscaping.

No front setback to Heidelberg Road should be provided between Park Avenue and Yarralea Street.



Map 3B: Building and Street Wall Heights for Precinct 3B

3.0 Subdivision

None specified.

4.0 Signs

None specified.

5.0 Application requirements

The following application requirements apply to an application for a permit under Clause 43.02, in addition to those specified elsewhere in the scheme and should accompany an application, as appropriate, to the satisfaction of the responsible authority:

- A site analysis and urban design context report which demonstrates how the proposal achieves the Design Objectives and requirements of this schedule;
- A desktop wind effects assessment for proposed development over 16 metres in height to assess the impact of wind on:
 - the safety and comfort of the pedestrian environment on footpaths and other public spaces while walking, sitting and standing; and
 - the safety and effects on cyclists travelling along bicycle routes that are adjacent to the development.
- A Traffic Engineering Report prepared by a suitably qualified traffic engineer that demonstrates how the development:
 - minimises impacts on the level of service, safety and amenity of the arterial road network (including bicycle lanes);
 - reduces car dependence and promotes sustainable transport modes, and
 - assesses the impacts of traffic and parking in the Precincts including an assessment of the ongoing functionality of laneway/s, where applicable.
- A landscaping plan.

6.0 Decision guidelines

The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specified in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

- whether the requirements in Clauses 2.2 and 2.3 are met;
- whether the proposal provides a high-quality public realm interface that either activates the street edge or provides an engaging and well-designed street interface, and contributes positively to the pedestrian environment and other areas of the public realm;
- whether the development retains the prominence of the heritage street wall in the vistas along the primary street frontage;
- whether heritage buildings on street corners retain their prominence, including their three-dimensional form, when viewed from the opposite side of the primary and secondary street;
- whether upper level development above the heritage street wall is visually recessive and does not visually overwhelm the heritage buildings;
- whether a strong sense of separation between upper levels and street walls is achieved when viewed from the opposite side of the street;
- whether the proposal responds to the presence of heritage buildings either on, or in close proximity to the site through a suitable transition in scale of street-wall, upper level setbacks and building height;
- whether the development delivers design excellence, including but not limited to building siting, scale, massing, articulation and materials;
- whether upper side and rear setbacks are sufficient to limit the impact on the amenity of existing dwellings, limit bulk of new development and increase a sense of openness;
- does the design respond to the interface with existing low-scale residential properties, including avoiding additional overshadowing of secluded private open space;
- whether proposed buildings and works will avoid overshadowing of footpaths and public spaces;
- whether the proposal has considered the equitable development opportunities of neighbouring properties in terms of achieving good internal amenity for future proposals through building separation and design;
- whether the proposed built form mitigates negative wind effects created by the development;
- the impact of development on traffic and parking in the nearby area, including on the functionality of laneways and bicycle lanes;
- whether the layout and appearance of areas set aside for vehicular access, loading, unloading and the location of any proposed car parking is practicable, safe and supports a pedestrian-oriented design outcome.

The Post Office and the group of shops on Heidelberg Road Statement of Significance

Heritage Place:	730-734 (including 730A) Heidelberg Road, Alphington	PS ref no:	HO451
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Figure 1: The Post Office, 730 Heidelberg Road, Alphington



Figure 2: 730A (right), 732 (Centre) and 734 (left) Heidelberg Road, Alphington

What is significant?

The group of three Interwar period shops at 730-734 Heidelberg Road, Alphington, constructed circa 1922.

Significant aspects of the shops include the brick walls and parapets (now painted), hipped roof forms (largely concealed), street canopies including original soffit linings, the configuration of the shopfronts, shopfront joinery and finishes, tiled mouldings to the west end of no. 732, the recessed entrance of no. 730 including floor tiles, mouldings, pressed metal ceiling and toplights with textured glass. In addition, the original side access to no. 730 (now 730A) as it extends to the depth of the front hipped roof of no. 732, where the intact (unpainted) return walls remain visible.

How is it significant?

The group of three shops at 730-734 Heidelberg Road, Alphington are of local historical and aesthetic significance to the City of Yarra.

Why is it significant?

The group of three shops at 730-734 Heidelberg Road, Alphington are of historical significance to the City of Yarra as one of the few remnant intact buildings that denote the Interwar period phase of development in this part of Heidelberg Road area (Alphington Village) during the early 1920s, when building activity increased, including much redevelopment, and the area's commercial function was consolidated. Whilst no. 734 was not purpose built as a post office, it has functioned as such for about half a century. (Criterion A)

The group of three shops are aesthetically significant as an intact group of single-storey, brick Interwar period commercial buildings. Whilst modest in scale, they retain their original parapets and unusually their original shopfronts, two of which were manufactured by Duff (nos 732-734), with the other (no. 730) being notable for intact canopy, the copper finish to its framing and green tiles to the stallboard. (Criterion E)

Primary source

Heidelberg Road Heritage Review (stage 2) 2019 – recommendations & citation for proposed heritage overlays

Cooper Knitting Factory (former) Statement of Significance

Heritage Place:	760-764 Heidelberg Road, Alphington	PS ref no:	HO455
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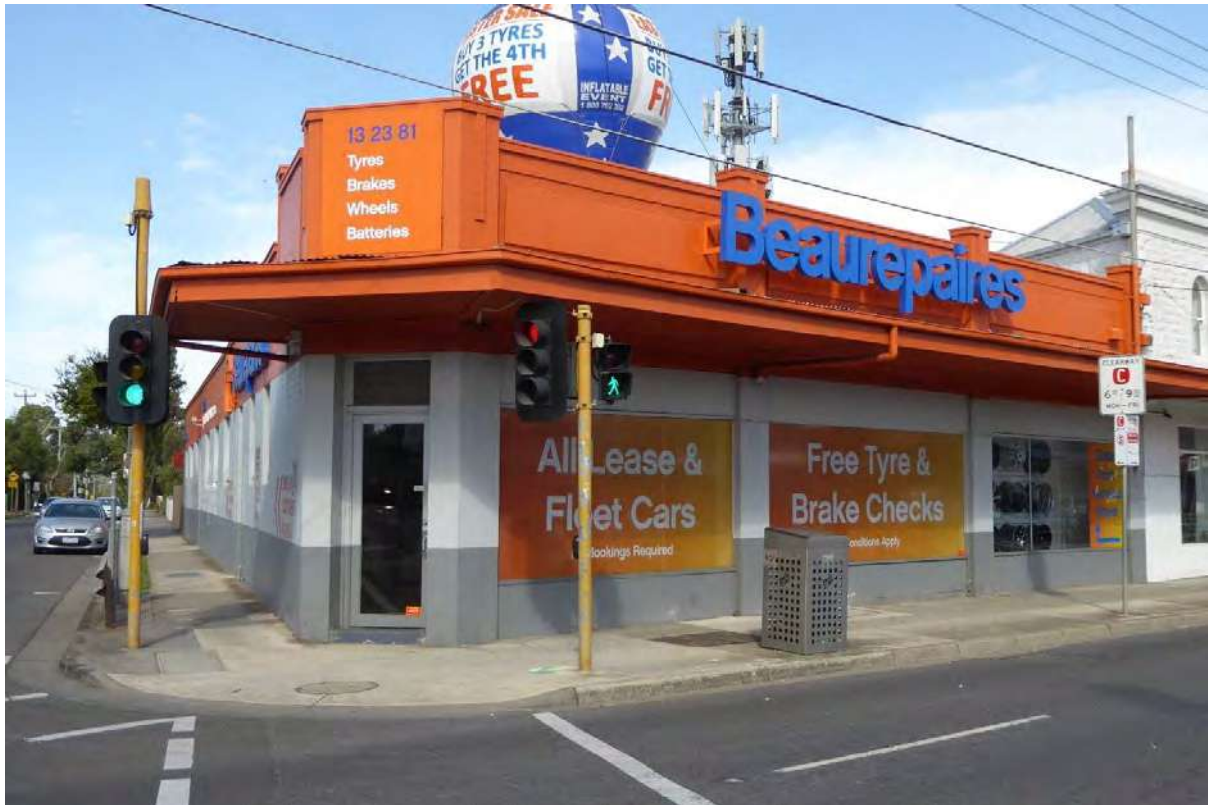


Figure 1: 760-764 Heidelberg Road, Alphington

What is significant?

The single storey brick building at 760-764 Heidelberg Road, Alphington, constructed 1922, with additions made during the late 1930s and early 1940s.

Significant aspects include the Heidelberg Road and Yarralea Street facades including brick parapet, visible gable roof sections (primarily to the east side), chamfered corner entrance, concrete lintels, pattern of openings and shopfronts, canopy, and remnant wall moulding (west end of north elevation).

How is it significant?

The single storey building at 760-764 Heidelberg Road, Alphington – the former A. Cooper Knitting factory - is of local historical and aesthetic significance to the City of Yarra.

Why is it significant?

Initially constructed as three premises in 1922, the single storey building at 760-764 Heidelberg Road, Alphington was expanded and later consolidated by A. Cooper Knitting Manufacturer. It is representative of the commercial and industrial development that occurred during the Interwar period along Heidelberg Road, and in particular was one of a few knitting enterprises that were established along Heidelberg Road at this time. The rapid expansion of the building during the late 1930s and early 1940s is indicative of the important role of the local knitting industry during WWII. (Criterion A)

The single storey building is aesthetically significant as an intact example of an Interwar period building constructed on a prominent corner site. The brick building is distinguished by its parapet and projecting piers articulated with a combination of smooth and roughcast render contrasting with variations in the brickwork. The original pattern of openings, shopfront division, and canopy remains mostly intact. (Criterion E)

Primary source

Heidelberg Road Heritage Review (stage 2) 2019 – recommendations & citation for proposed heritage overlays



No.756-758



No. 358

Heidelberg Road Heritage Review (Stage 1)
2019

Stage 1 Report

City of Yarra

RBA ARCHITECTS +
CONSERVATION CONSULTANTS PTY LTD
FITZROY STREET 4C/171
ST KILDA VIC AUSTRALIA 3182

+613 9525 5666 TEL
+613 9525 4906 FAX

rba@rbaarchitects.com.au EMAIL
www.rbaarchitects.com.au WEB



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Personnel:

Anthony Hemingway	Architectural Historian/Senior Associate
Erin Williams	Heritage Consultant
Ashleigh Ngan	Architectural Graduate

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1 INTRODUCTION

1.1 Brief

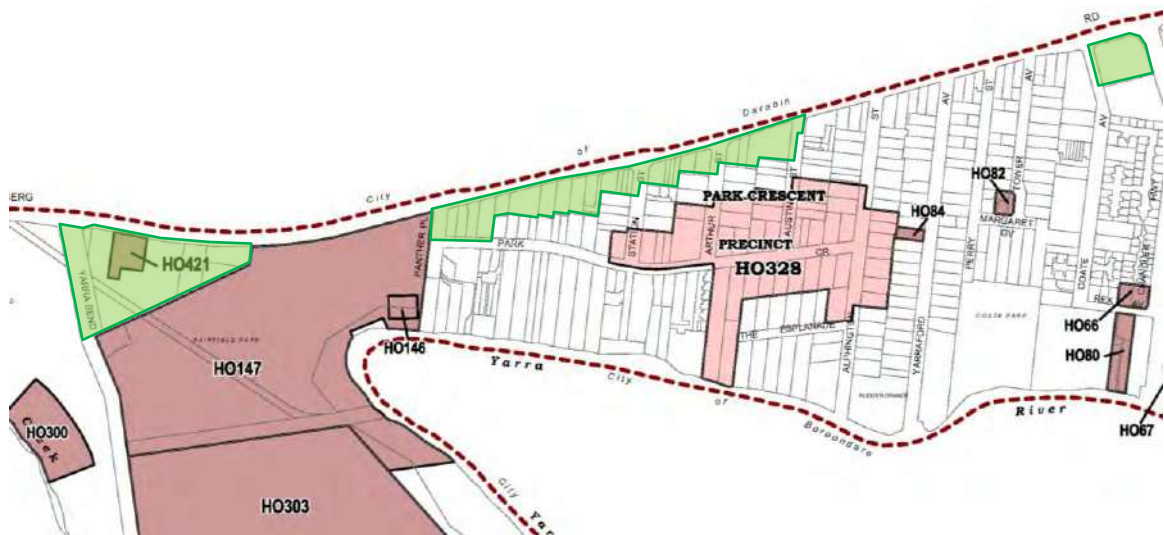
This report, the *Heidelberg Road Heritage Review 2019*, has been prepared by RBA Architects + Conservation Consultants for the City of Yarra. It relates to the building stock within the Commercial 1 and Commercial 2 Zones along (the south side of) Heidelberg Road between the Merri and Darebin creeks.

The brief was to undertake the following:

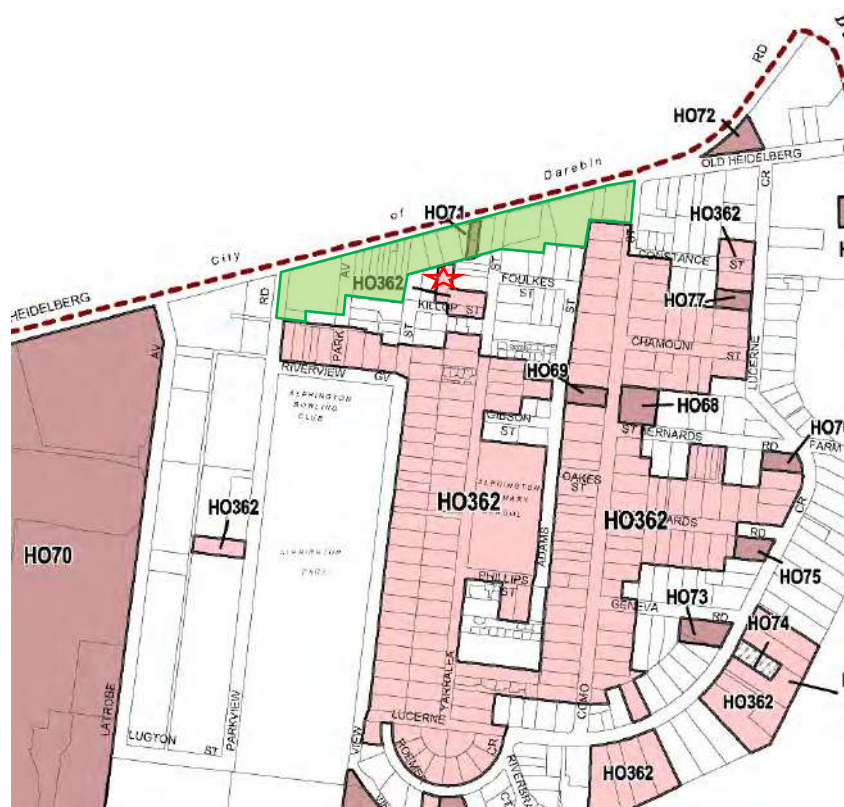
- Review the heritage values of HO 71 (756-758 Heidelberg Road, Alphington) and the currency of its citation,
- Determine whether 2 Killop Street, Alphington should be included in HO362 (Alphington East Precinct),
- Identify any sites of potential heritage value in the study area,
- Provide recommendations for built form controls for any existing and recommended heritage overlays.

1.2 Location

The study area primarily relates to the commercial zones along the south side of Heidelberg Road and extends across parts of the suburbs of Fairfield (western part) and Alphington (eastern part).



Map No. 3HO
Existing HO421 (Porta)
Western part of study area (Commercial 1 and 2 Zones) shown in green



Map No. 4HO showing existing HO71 and 2 Killop Street, Alphington (identified with a star, part of HO362). Eastern part of study area (Commercial 1 and 2 Zones) is shown in green

1.3 Background

Previous Heritage Studies

- City of Yarra, Review of Heritage Overlay Areas 2007, Appendix 8, prepared by Graeme Butler and Associated, revised May 2017
- City of Yarra Heritage Review, Thematic History Volume 1, prepared by Allom Lovell & Associates, July 1998
- City of Northcote Urban Conservation Study prepared by Graeme Butler, revised February 1983

Existing Listings

There are three sites in the study area listed in the Schedule to the Heritage Overlay:

- HO421 – Porta factory, Heidelberg Road, 224 Heidelberg Road. Existing citation not to be reviewed.
- HO70 – Australian Paper Mills Boiler House, 626 Heidelberg Road. This building has been the subject of a previous amendment and will not be considered as part of this project.
- HO71 – Shops, 756-758 Heidelberg Road – basalt building constructed circa 1860. Existing citation to be reviewed.

1.4 Acknowledgements

The authors are grateful for the assistance provided by the City of Yarra officers.

2 METHODOLOGY

2.1 Introduction

The methodology adopted in undertaking this phase of Heidelberg Road Heritage Review (stage 1) was in accordance with the processes and criteria outlined in the *Australia ICOMOS Charter for the Conservation of Places of Cultural Significance*, known as the *Burra Charter* (Australia ICOMOS, 2013). The key tasks included:

- Site inspections - largely limited to a visual assessment of each property from the perimeter and were undertaken during April 2019.
- Preliminary historical research and analysis of the extant fabric in relation to documentary evidence where applicable.
- Preparing data sheets.
- A preliminary assessment of the significance of each place based on the research and the extant fabric, and with reference to the relevant HERCON criteria.

2.2 Research

Primary sources were mainly employed and have included:

- Photographs, including aerial photographs, held by:
 - Landata (aerial),
 - Darebin Archives,
 - National Archives of Australia (NAA),
 - State Library of Victoria (SLV),
 - University of Melbourne (aerial).
- Drainage plans (Yarra Valley Water),
- Melbourne Metropolitan Board of Works (MMBW) Plans, held by the State Library Victoria,
- Sands & McDougall's street directories,
- Plans (SLV, etc),
- Various newspapers (*Age*, *Argus*, *Herald*, etc).

2.3 Data Sheets + Citation

A data sheet was prepared for individual buildings, or groups of buildings, of potential significance within the study area. A citation was prepared for HO71.

Within the data sheets/citation the following are provided:

- Name (usually relating to the original owners or purpose),
- Address,
- Summary history – including date of construction and period (Federation, Interwar, Post WWII),
- Summary description – including assessment of condition and intactness (poor, fair, good),
- Comparative – examples of similar places,
- Relevant HERCON Criteria,
- Recommendation.

2.4 Preliminary Assessment of Significance

For heritage professionals generally in Australia dealing with post-contact cultural heritage, the process outlined in the *Burra Charter* underpins the approach to heritage assessment and conservation adopted by the authors of this report.

Burra Charter

The methodology adopted in the assessment of the significance (or heritage values) has been in accordance with the process outlined in the *Burra Charter* (or The Australia ICOMOS Charter for Places of Cultural Significance). As outlined in the *Burra Charter*, the criteria considered include aesthetic (including architectural), historical, scientific (or technical), social and spiritual values. These values have been translated into the HERCON Criteria, discussed below.

2.5 Applying the Heritage Overlay

'Applying the Heritage Overlay' August 2018 (Planning Practice Note 1) provides guidance about the use of the Heritage Overlay, including the following:

- What places should be included in the Heritage Overlay?
- What are recognised heritage criteria?
- Writing statements of significance.

The practice note indicates that the HERCON criteria are to be employed when assessing heritage significance.

HERCON Criteria

These widely used criteria were adopted at the 1998 Conference on Heritage (HERCON) and are based on the earlier, and much used, Australian Heritage Commission (now Australian Heritage Council, AHC) criteria for the Register of the National Estate (RNE).

The HERCON criteria are essentially a rationalised (more user-friendly) version of the AHC Criteria (which included different sub-criteria for cultural or natural heritage). **It is also noted in the aforementioned practice note that 'The adoption of the above criteria does not diminish heritage assessment work undertaken before 2012 using older versions of criteria.'** Reference to the relevant HERCON criteria is outlined at the end of the data sheets. The definition of these criteria are outlined in the following table.

Criterion	Definition
A	Importance to the course, or pattern, of our cultural or natural history (historical significance).
B	Possession of uncommon, rare or endangered aspects of our cultural or natural history (rarity).
C	Potential to yield information that will contribute to an understanding of our cultural or natural history (research potential)
D	Importance in demonstrating the principal characteristics of a class of cultural or natural places or environments (representativeness)
E	Importance in exhibiting particular aesthetic characteristics (aesthetic significance).
F	Importance in demonstrating a high degree of creative or technical achievement at a particular period (technical significance).
G	Strong or special association with a particular community or cultural group for social, cultural or spiritual reasons. This includes the significance of a place to Indigenous peoples as part of their continuing and developing cultural traditions (social significance)
H	Special association with the life or works of a person, or group of persons, of importance in our history (associative significance).

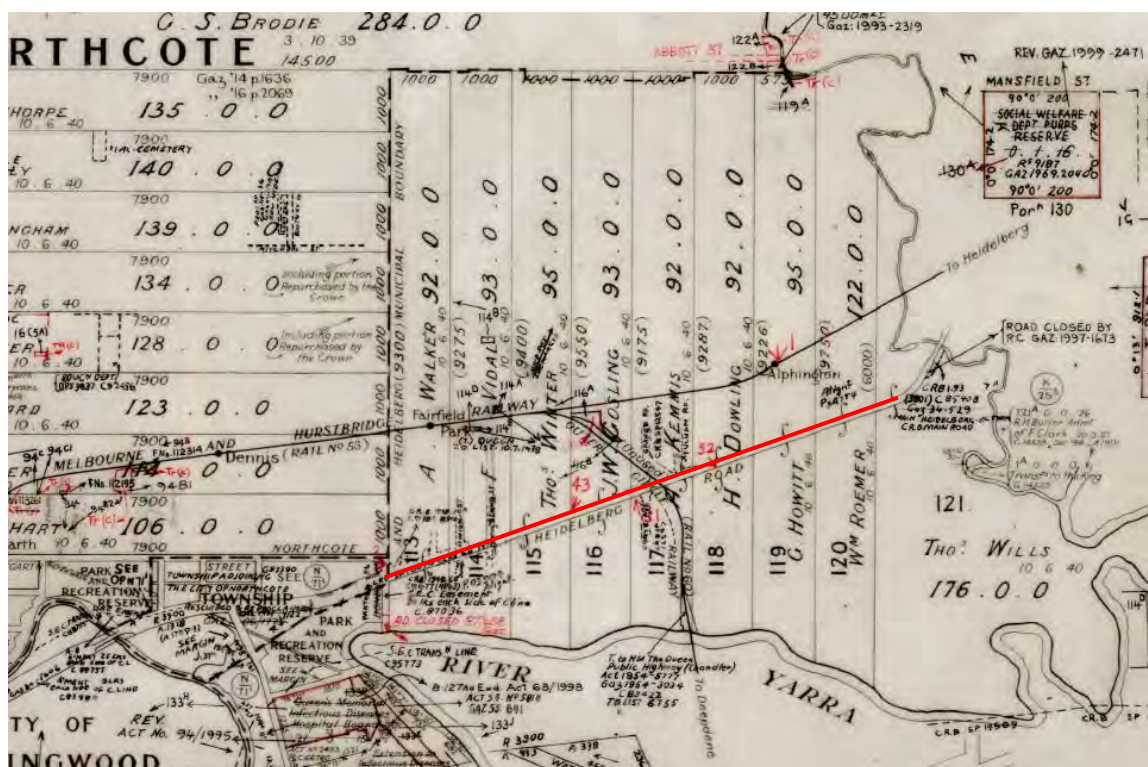
3 STUDY AREA – Overview of Development

3.1 Introduction

The existing heritage studies provide scant information regarding the residential and commercial development within the Heidelberg Road study area, particularly during the 20th century. Due to the lack of background information a brief historical overview has been produced, outlining what is evident from historical aerials and MMBW plans.

3.2 Historical Overview

The study area along Heidelberg Road is located in what was the Parish of Jika Jika. The area includes sections of what is now Fairfield, Alphington and Northcote. During the 1840s the land was divided into a series of narrow allotments with frontages to the Yarra River or Darebin Creek.¹



Part of Parish of Jika Jika J16(5)
 Section of Heidelberg Road within study area (red line)
 (Source: Landata)

From the mid to late 1800s Heidelberg Road functioned as the main thoroughfare for visitors en route to the town of Heidelberg and the shops served those travelling as well as local residents.² This included Fulham Grange, an early farm and vineyard **owned by the Perry Brother's** and **"Alphington Village,"** a resting place for travellers west of the Darebin Creek crossing, established by William Manning in 1854.

In 1900 several shops and retailers occupied the south side of Heidelberg Road. At that time the section of road between Grange Road and Como Street included a butcher, general store, post office and telephone office.³

¹ City of Darebin
² City of Yarra Heritage Review, Allom Lovell & Associates, 1998
³ Sands & McDougall directory, 1900

By 1914, as evident in the following MMBW, some parts of Heidelberg Road had been fairly developed, though other sections were sparsely developed. Early houses and buildings along Heidelberg Road were either timber or masonry (brick or stone), often with smaller outbuildings to the rear.



1914 MMBW Detail Plan 1318
Showing east end of Heidelberg Road - about Yarralea Street, Alphington
(Source: SLV)

By 1931, there was considerable development along Heidelberg Road. For instance, the land between Panther Place and Grange Road consisted of typically narrow, rectangular allotments with some larger, vacant lots.



1931 Aerial, Heidelberg Road - west end of study area (between Panther Place and Grange Road)
(Source: Landata, 1931 Nov - Maldon Prison, Proj. No. 1931, Run 15, Frame 2741)

By 1945, according to the following aerials, a number of light commercial buildings (a combination of shops and warehouses) had been constructed on the south side in this section of the Heidelberg Road. Some smaller parcels had been consolidated into larger sites.



1945 Aerial, Heidelberg Road – west end of study area (between Panther Place and Austin Street)
(Source: Landata, Melbourne and Metropolitan Area Project, Proj. No. 5, Run 27, Frame 59,529)



1945 Aerial, Heidelberg Road – east end of study area (between Parkview Road and Lucerne Crescent)
The northern half of Alphington East Precinct (HO362) is evident
(Source: Landata)

By 1956, the concentration of commercial/light industrial sites along parts of Heidelberg Road, similar to the current circumstance, had been established. A number of previously vacant sites had been developed with small warehouses, etc.



1956 Aerial, Heidelberg Road – study area
(Source: Landata, Melbourne Outer Suburbs Project, Pro. No. 250, Run 13, Frame 146)

A 1978 aerial highlights the large number of light commercial buildings that had been constructed along the south side of Heidelberg Road by that time, including at the Paper Mills site.



1978 Aerial, Heidelberg Road – study area
(Source: Landata)

4 FINDINGS

4.1 Introduction

This section outlines the findings regarding the following:

- HO71,
- Places of Potential Heritage Value,
- Other Sites Considered,
- 2 Killop Street, Alphington.

4.2 HO71

It was confirmed that the basalt building at 756-758 Heidelberg Road (HO71) more than adequately meets threshold for local significance.

An updated citation was prepared for HO71 that meets the contemporary standard, refer to Appendix A.

4.3 Places of Potential Heritage Value

Datasheets (refer to Appendix B) have been prepared for five individual places which were considered to be of potential heritage significance. Details of these five sites are outlined in the following table. Of these, three were considered to have strong potential to meet the threshold for local heritage significance (those at nos 358, 730-734, and 760-764), with the other two (nos 460, 774) less so.



On further discussion with Council, only three have been recommended for further work in Stage 2 and were reviewed in regards to built form analysis and recommended controls.




Address – No. (Heidelberg Rd)	Name	Date/Period	Details
358	House & Tyre Outlet	Late 1950s/Postwar	House – Modernist influence Unusual combination of house and adjoining tyre outlet
460	Duncan Rubber Co. Showroom & Warehouse	1956/Postwar	Sawtooth roof forms Intact facades
730-734	Group of 3 shops	1933/Interwar	Single storey buildings with intact shopfronts
760-764	A Cooper Knitting Manufacturer	Mid-1930s/Interwar	Single storey building with intact parapet and most original openings
774	Former Ampol Service station	By 1965/Late 20 th century	Stages of construction not certain Original frame (sign) survives


4.4 Other Sites Considered

The following table outlines the other buildings in the study area which were noted for their potential heritage value. A data sheet was not prepared for this group as they were generally assessed as not having sufficient potential to reach the threshold for local heritage significance.

Of this group, the earlier buildings had been altered to varying degrees, whereas the two late 20th century buildings were largely intact. The latter two, whilst good examples, are difficult to assess in regards to other similar building stock in the municipality as there are few, if any, from this time that have been assessed as having heritage value. This is in part because many such buildings are located within precincts whose significance primarily relates to late 19th century (Victorian), and possibly early 20th century (Federation and/or Interwar) phases of development and so have tended to be graded non-contributory. These two buildings might warrant consideration as part of a latter 20th century review of similar buildings across the municipality.

No.	Details	Image
276	<p>Office building (Wellways)</p> <p>Brutalist style – constructed in 1975 (Yarra Valley sewerage plan). Architect unknown.</p> <p>From 1965 until at least 1971 - occupied by Collins Motors (The Age).</p> <p>Large two storey Brutalist building with a bold rectangular form, bands of textured concrete, and metal-framed windows with brise-soleil (sunshade) to upper level with some intervening textured, cream brick piers to side and rear.</p> <p>Some yellow/gold tinted glass – possibly Stopray range, manufactured by the Glaverbel (Industries) or similar.</p> <p>Intact, good example but only 45 years old and limited appreciation for this type of architecture</p>	
388	<p>Shop/Warehouse (Beds for Backs)</p> <p>Interwar/Art Deco brick garage – ca late 1930s</p> <p>From at least 1933 (Sands) 388-394 is occupied by Elite Knitting Co Pty Ltd</p> <p>1935 MMBW – extant building not evident. Subject site consists of four lots (brick building to two west sites and timber buildings to east side)</p> <p>1945 aerial – extant building evident</p> <p>Curved entrance with original brick steps, JD emblem, rendered parapet, steel hopper windows to recessed west section.</p> <p>Roof truss/sawtooth roof evident through vehicular opening.</p> <p>Façade is considerably altered</p>	

No.	Details	Image
402	<p>Shop (Platform Commercial Furniture)</p> <p>Interwar/brick garage – ca 1940</p> <p>1931 Aerial – structure with a similar building footprint and gabled roofs as extant building</p> <p>From at least 1933 (Sands) – 402-406 is occupied by F W Smith, motor garage</p> <p>1935 MMBW – one brick and one timber building on the front of the site</p> <p>1945 aerial – extant buildings evident</p> <p>Brick parapet with brick capping</p> <p>Large openings introduced to façade, much altered</p> <p>Short return at east end intact</p>	
728	<p>Shop (Take shape)</p> <p>Interwar period</p> <p>Prior to 1945 – no. 728 not listed</p> <p>1945 aerial – site appears vacant</p> <p>1955 – no. 728 is listed and included in nos 726-730</p> <p>1968 image (NAA) – originally had a stepped parapet with steel framed hopper windows. Rosmear Engineering Co.</p> <p>P/L, plumber's requisites, occupy the building (Sands)</p> <p>Much altered facade</p>	
750-754	<p>Pair of shops</p> <p>Federation period – by 1914, possibly earlier</p> <p>1898 – site is possibly occupied by Mrs H Schmidt bakery and occupied by Wm Johnson, baker in 1904 (Sands)</p> <p>1914 MMBW – extant building footprint evident (timber building)</p> <p>1916 – site (then no. 373) is listed as vacant</p> <p>Single storey pair with pyramidal/hip roof. Pair of curved parapets and capped piers, altered openings.</p> <p>Relatively early, distinctive form, but partly altered facades</p>	
782	<p>Factory/Head Office (Machinery Forum)</p> <p>Contemporary two storey brick building constructed in 1981 (Yarra Valley sewerage plan). Architect unknown.</p> <p>1954 – previous factory on subject (782-788) with bluestone buildings</p> <p>1962 – previous factory on subject site. Auction for 'modern factory, store, offices</p> <p>1978 aerial - vacant but site configuration suggested</p> <p>Unusual L-shaped plan and form dictated by site configuration which wraps around no. 774.</p> <p>Distinctive undulating façade to the north west and south west corner. Arched entry door and 'lozenge'-shaped window on the east facade</p>	

No.	Details	Image
	<p>Machinery Forum, since 1956 (website)</p> <p>Intact Late 20th century building with distinctive form, and broadly reflective of Brutalist style</p> <p>Good example but less than 40 years old and limited appreciation for this type of architecture</p>	

4.5 2 Killop Street, Alphington

2 Killop Street is located in the north end of the Alphington East Precinct Heritage Overlay (HO362). This residential precinct is broadly bound by Lucerne Crescent (to the east), Romer Crescent (to the south) and View Street (to the west), however there are some smaller outlier sections.

2 Killop is located in one of the small peripheral sections of HO362 that also includes 59 Yarralea Street. The latter is a weatherboard-clad bungalow with a distinctive oblong opening to the porch.

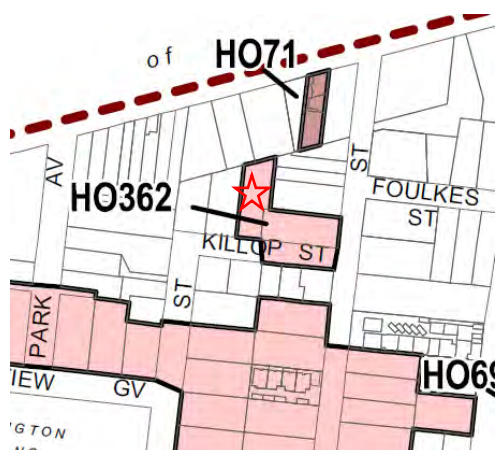
The statement of significance for Alphington East Precinct Heritage Overlay (HO362) is as follows:

Why is it significant?

The precinct is historically and aesthetically significant as a residential enclave, physically contained by the early transport route of Heidelberg Road and the natural barrier of the Yarra River, which was built up in the late Victorian-era and early 20th century as consistent and well preserved house groups in the Italianate, Federation and Californian Bungalow suburban styles, differing to most other parts of the City by their garden setting and deep garden setback. (Criteria A & E)

The precinct is aesthetically significant for the distinctive street layout arising from Manning's early Alphington village subdivision, with its curving base in the form of Roemer Crescent at the Yarra River. (Criterion E)

The precinct is historically significant for the artistic associations of the area as the chosen domain (along with the adjoining Ivanhoe and Heidelberg areas) of many prominent artists and businessmen linked with art and artisan pursuits from the late 19th and early 20th centuries. (Criterion A)



HO362 and 2 Killop Street (star)



2 Killop Street (Nearmap 2019)

2 Killop Street is a single storey house constructed during the late 20th century and consists of a series of rendered cubic forms with a parapet concealing the roof. The colour palette is relatively plain and with teal accents to the trims

and mouldings. The street façade features a curved bay window and a small entrance canopy but is dominated by a double width garage.

To the west at 4 Killop Street is a much altered timber building, probably dating to the Interwar period, which is appropriately not included in the heritage overlay.

The house at 2 Killop Street is an anomaly at the edge of the Alphington East Precinct largely characterised by Victorian, Edwardian and Interwar houses with pitched, gabled or hipped roofs. Prevailing wall types include weatherboard, face pressed brick and stucco wall cladding. Street facing timber verandahs supported by timber posts are also common.⁴ 2 Killop Street was constructed during a latter period and varies greatly in style, form and materials to the graded building stock in the precinct. The building is not related to the significance of the precinct (HO362) and should be omitted.



2 Killop Street – façade/south elevation, included in HO362



4 Killop Street – not included in HO362



59 Yarralea Street – east elevation, included in HO362



59 Yarralea Street – south elevation, included in HO362

⁴ Statement of Significance, Alphington East Precinct (VHD)

5 BUILT FORM REVIEW

5.1 Introduction

Built form guidelines have been developed for the following five sites, two of which are existing heritage overlays (HO71 + HO421) and three proposed heritage places.

- Porta Factory - 224 Heidelberg Road, Fairfield (HO421)
- House (Valeema) & Tyre Outlet - 358 Heidelberg Road, Fairfield
- Group of 3 Shops - 730-734 Heidelberg Road, Alphington
- Former Butcher Shop - 756-758 Heidelberg Road, Alphington (HO71)
- A Cooper Knitting Manufacturer - 760-764 Heidelberg Road, Alphington

5.2 Porta Factory - 224 Heidelberg Road, Fairfield (HO421)

Existing Conditions

The Porta Factory was constructed during the 1920s. The original roof consists of three narrow, gabled roofs and a tall corniced chimney stack in exposed face brick to the south of the building. On the west face **is the word 'PORTA'**, vertically displayed in cream bricks. The Heidelberg Road façade retains the original stepped parapet and consists of a moulded brick capping and paired corbels. The street level façade has four original window openings with multi-paned timber-framed windows, concrete lintels and moulded brick sills and a single course brick plinth. The east (side) elevation retains an original window and moulded brick sill. Nearby is a small, original dormer roof section.

A sympathetic, narrow brick addition (relating to the vehicular entry) constructed after 1945, has been built to the west of the original building extending past and enclosing the area around the brick chimney. The detailing on the street facing parapet matches that of the original section and also includes a corbelled brick eave to the west end. The façade has a single timber-framed window with matching moulded brick sill and a double timber door with vertical boards and concrete lintel. To the west side is a small skillion roofed addition.

The entire street façade has been painted.

The Porta Factory is included within DDO1-j (Map No. 3DDO) of the Yarra Planning Scheme and part of the site constitutes heritage overlay HO421. The heritage overlay includes a small section of the more recently constructed metal clad building to the west. This section is not considered to have heritage significance.

Recommendations

- Retain original, three narrow gable roofs including dormer roof in the north-west corner,
- Retain brick chimney stack including iron bands,
- **Maintain a view/sightline of the original cream brick 'PORTA' sign integrated into the west face of the chimney.** The sightline would ideally be from the north-west corner of the site and from Heidelberg Road.
- Generally, retain an open space to the rear of the building to allow interpretation of the original roof forms,
- Retain street facades including original stepped parapet and detailing, openings, concrete lintels, multi-paned timber-framed windows and moulded brick sills. Encourage removal of paint to reveal original face brick finish.
- Maintain visibility of the single window on the east (side) elevation. Retain a 5-metre setback from Heidelberg Road and about a 3-metre separation from the east (side) elevation.
- Retain post-1945 narrow, gable roofed brick addition to the west including parapet, openings, windows and doors, and parapet on the street façade. Allow removal of small skillion-roofed structure on the west side.
- Maintain a 3-metre setback from Heidelberg Road and a 3-metre separation from the west (side) elevation of the post-1945 brick addition.

- Allow the removal of the recently constructed metal clad building to the west



Retain single window, lintel and sill on east elevation



Retain view/sightline of integrated 'PORTA' sign on chimney from the street

5.3 House (Valeema) & Tyre Outlet, 358 Heidelberg Road, Fairfield

Existing Conditions

The two storey L-shaped house was built in two stages and consists of the rectangular east half of the building constructed circa late 1950s, and the subsequent west wing and entry porch. The parts are highly intact and integrated. The strong form of the building is most visible from Panther Place from the south and west, where the gable roofs and blank west wall are most evident. Openings and steel-framed windows on the south and west elevations are original elements.



Panther Place (west elevation) – building form and original openings



Park Crescent (south elevation) – original openings

The single storey tyre outlet building is visible from the intersection of Heidelberg Road and Panther Place. The original parapet roof of the tyre outlet and prominent gable roof of the house are visible. On the north-east corner of the tyre outlet building is a chamfered brick corner with original raised street numbers. The upper section of the brick façade features brick courses in a header configuration and a horizontal, shallow recessed panel on the north façade.

Below this is an original garage opening and rendered concrete lintel. On the west façade of the tyre outlet is an original timber door and two large original steel windows. To the front of the site are the location of the petrol bowers is evident.



Heidelberg Road – original parapet roof, openings, doors and windows of tyre outlet. Original bowser pipes to front of site (indicated).



Heidelberg Road – original roof form of house is visible

Recommendations

House (Valeema)

- Limited opportunity for alterations and additions,
- Retain existing building form including gable roofs and entry portico,
- Retain original glazed roof tiles,
- Retain original openings and steel-framed windows,
- Retain existing pattern of openings and minimal window to wall/ratio, to maintain the bold yet restrained aesthetic of the house.

Tyre outlet

- Limited opportunity for alterations and additions,
- Retain original brick parapet,
- Additions should maintain a 2-metre setback from the edge of the parapet,
- Additions should maintain a 2-metre setback from the north face of the house, ensuring the original roof form remains legible,
- Retain original brick detailing including header and rowlock course configurations, shallow recessed panel and small chamfered brick corner with raised signage to the north-west corner,
- Retain original openings, concrete lintel and timber door and steel framed windows on the west façade,
- Retain indications of bowsers to the front of the site,
- Allow removal of lightweight canopy to the west side,
- **Retain original street numbers reading '358' and possibly 'Bridgestone tires' sign,**
- Opportunity to extensively glaze-in open parts of the site.

5.4 Group of 3 Shops - 730-734 Heidelberg Road, Alphington

Existing Conditions

The group of three shops maintain intact parapets that are visible from Heidelberg Road. The single storey post office building at no. 730 has an original square pediment and capped brick piers, which have been painted. The original hipped roof to the rear of the parapet is visible from Heidelberg Road. Nos 732 and 734 have original plain parapets

that have also been painted. A narrow walkway between no. 730 and no. 732 enables visibility of the east parapet return of no. 730 and the west parapet return of no. 732. The original red brick walls are also evident.



No. 730 – the original parapet, east return and hipped roof are visible from Heidelberg Road



No. 732 – the original parapet and west return are visible from Heidelberg Road

The shopfront of no. 730 is highly intact and retains the original shopfront and symmetrical configuration. This consists of the original recessed entry floor tiles with pressed metal ceiling above, dark green wall tiles and mouldings to the lower section, copper-finish shopfronts and toplight windows with textured/patterned glass.

Nos 732 and 734 retain original shopfronts bearing the name of the manufacturer (Duff) however these have been overpainted. A section of the original horizontal moulding to the west end of no. 732 also survives.



No. 730 – original shopfront



No. 730 – original section of roof visible from Heidelberg Road indicated by the dotted line. Visible parapet returns of nos 730 and 732 indicated by the red arrows (Source: Nearmap)

Recommendations

General

- Retain brick parapets. Encourage paint removal to expose original red brick,
- Retain walkway/separation to front between no. 730 and no. 732 to maintain visibility of the east parapet return of no. 730 and the west parapet return of no. 732. Allow removal/replacement of the brick wall and door leading to the walkway.
- Retain original canopies.

No. 730

- Maintain original gable roof form and visibility from Heidelberg Road. Additions to the first floor and above should retain a 6-metre setback from the street parapet,
- Retain original shopfront configuration and finishes including recessed entry, original floor tiles, pressed metal ceiling, wall tiles and mouldings, copper finish shopfronts and toplight windows with translucent glass.

Nos 732 and 734

- Additions to the first floor and above should retain a 6-metre setback from the street parapet,
- Retain original shopfront joinery. Encourage removal of paint to expose original finishes.

5.5 Former Butcher Shop, 756-758 Heidelberg Road, Alphington (HO71)

Existing Conditions

The two storey building was constructed circa 1860 and is the oldest surviving commercial building of the Alphington Village. The former Butcher Shop has original basalt walls (now overpainted), hipped roof form, rendered moulded parapet, three semi-circular arched windows with brick dressings and keystones to the first level street façade, and a single window with brick dressings to the west (side) elevation.

The building is located at the west end of Heidelberg Road and has a prominent street presence. This is partly due to the low scale massing of the adjacent buildings. The street façade, original parapet and hipped roof are widely visible from the street and the west (side) elevation remains exposed.



The entire west (side) elevation of the former Butcher Shop (HO71) remains exposed



The prominence of the former Butcher Shop (HO71) is evident from the intersection of Heidelberg Road and Yarralea Street
(Source: googlemaps)

Recommendations

- Retain form and visibility of the original hipped roof.
- Retain original basalt walls and plinth. Encourage removal of paint to reveal basalt and brick.
- Retain two storey street façade including three arched openings and timber-framed windows to the first floor with brick dressings and keystones, and moulded parapet and cornice.
- Retain single arched opening and timber-framed window to the first floor of the western (side) elevation.
- A maximum of three storeys to the rear to retain the prominence of the subject building.
- Maintain a minimum 3-metre separation from the west (side) elevation to enable visibility of the original basalt wall and window opening, and legibility of the original building form.
- Retain shopfront wall tiles (contributory element) and encourage removal of paint to expose original finishes.

5.6 A Cooper Knitting Manufacturer, 760-764 Heidelberg Road, Alphington

Existing Conditions

The former factory building is located adjacent to the former Butcher Shop (HO71), in the south-west corner of the intersection of Heidelberg Road and Yarralea Street. The original brick parapet of the former factory building has been painted and consists of capped brick piers and small sections of roughcast render. Sections of original soldier course detailing have been retained below the parapet edge.

A remnant section of an early brick wall to the front of the site is visible from Yarralea Street. The original eastern plane of the narrow, gable roof beyond is also visible from street level.



The original form is visible from the intersection of Heidelberg Road and Yarralea Street



Original roof form and openings along Yarralea Street

The Heidelberg Road façade retains the original chamfered entrance in the north-east corner, three original window openings and timber frames to the east and west window. An original moulding at the west end also survives. The Yarralea Street façade retains original concrete lintels and door and window openings, although three windows at the south end have been truncated and glass bricks added. The timber windows and doors are either original or early additions.



Retain eastern plane of narrow, gable roof
(Source: Nearmap)



Encourage removal of paint and fixed rendered panels on parapet

Recommendations

- Retain original form of parapet and brick detailing,
- Encourage removal of paint on brick walls and parapet to reveal earlier finishes. Encourage removal of non-original panels on parapet,
- Maintain a 6-metre setback from Heidelberg Road,

- Retain front gable roof section,
- Retain the eastern plane of the long, gable roof along Yarralea Street,
- A maximum of three storeys to the rear of the building to retain the prominence of the former Butcher Shop, adjacent to the subject site,
- Maintain street facades including the chamfered entrance, original concrete lintels, openings, timber windows, timber shopfront frames and mouldings, and concrete lintels on the street facades,
- Retain shop canopy,
- Allow the removal/modification of buildings to the west half of the site that are not visible from the road.

APPENDIX A - HO71 Citation

FORMER BUTCHER SHOP (HO71)

Address	756-758 Heidelberg Road, Alphington
Significance	Local
Construction Dates	c. 1860
Period	Early Victorian
Date Inspected	2019



Statement of Significance

What is Significant?

The two storey Former Butcher Shop at 756-758 Heidelberg Road, Alphington, constructed circa 1860. Significant aspects include the basalt walls, both front and side elevations (now overpainted), hipped roof, rendered moulded parapet, three semi-circular arched windows to the upper level façade with basalt sills, brick quoining and keystones, and a single window with basalt sill and brick quoining to the west side.

The tiling to the shop front (now overpainted) dates to the early 20th century and is a contributory element. The canopy dates to the mid-20th century and is also a contributory element.

How is it Significant?

The Former Butcher Shop at 756-758 Heidelberg Road, Alphington, is of local historical and aesthetic significance to the City of Yarra.

Why is it Significant?

The Former Butcher Shop is of historical significance as the oldest surviving commercial building of the original Alphington Village, established in the 1850s and 1860s. The village was a small enclave along the main road and served as a stopping point

en-route to Heidelberg. The building was occupied by a succession of butchers from the 1860s to the early 1930s. (Criterion A)

The Former Butcher Shop is of aesthetic significance as a largely intact (apart from the shop front) early-Victorian period commercial building of basalt construction. The building is designed in the Regency style and features a well-composed, symmetrical façade of elegant proportions with pronounced classical detailing such as quoining and keystones (in light of much contemporary architecture). The building demonstrates restrained classical detailing largely in basalt - walls of rock-faced ashlar blocks contrasting with plain-faced (smooth) elements (prominent keystones, sills and the moulded parapet), in combination with brick (possibly cream) quoining to the windows. (Criterion E)

Description

The building at 756-758 Heidelberg Road, Alphington, is a two-storey building with predominantly basalt walls designed in the Regency style. The façade is symmetrically composed and consists of a basalt plinth, three arched windows at first floor and a prominent moulded parapet and cornice that conceals a metal-clad hipped roof.

The Regency style refers to the era in the early 19th century when George IV was Prince Regent and is stylistically characterised by a simplified and restrained elegance. Facades were symmetrically composed and typically consisted of a parapet concealing the roof beyond. This is evident in the balanced arrangement of arched openings and prominent moulded cornice. While stucco was the main material, external masonry walls were often finished in face brickwork with refined and subtle brick detailing. This is reflected in the subtle projections of brick quoining to the windows and outer edges of the facade and visual contrast of brick (possibly cream) and basalt.



Symmetrical first floor features three original arched windows



Shop front

The first floor windows are intact and feature brick quoining, keystones and projecting basalt sills with a smooth finish that contrasts with the rough basalt wall. Projecting keystones and expressed basalt quoining to the edges of the façade, demonstrate a restrained elegance indicative of the Regency style.

The west (side) elevation is exposed and features a single segmental arched window with brick quoining and smooth basalt sill. The timber sash windows are not original however are consistent with the aesthetic of the building. The façade, including the basalt parapet and side elevations have been painted, however it is likely the brick quoining to the windows was originally cream coloured.

The shop front of the subject building has undergone substantial alterations, probably multiple times. The entrance is off centre and there are large timber-framed windows to either side. The lower part of the wall consists of overpainted tiles dating from the early 20th century and the upper section has been rendered. A cantilevered canopy dating from the mid-20th century has replaced the earlier verandah.



View from east



West elevation

Possible Architect

Given the refinement of the façade, it is likely the subject building was architect designed. The possible application of cream brick quoining to windows and doors is a stylistic device evident in the work of prominent Melbourne architects, Crouch and Wilson. The practice was responsible for the design of a number of noted buildings during the Victorian period, including 12 Jolimont Terrace in East Melbourne constructed in 1868. The two storey basalt residence is similar to that of the former Butcher Shop and consists of cream brick quoining and lintels to windows and doors.

The partnership of Crouch & Wilson was formed in 1858 by Thomas James Crouch (1832-89) and Ralph Wilson. Crouch was born in Hobart and was trained by Alexander Dawson. In 1852 he moved to Melbourne and in 1859 formed a partnership with **Ralph Wilson. Due to Crouch's Wesleyan background, the practice was engaged to design some 40 churches throughout Victoria, Tasmania and New Zealand.** They are particularly noted for their use of brickwork, both polychrome and in combination with basalt.⁵

History

The subject site formed part of Crown Portion 120, Parish of Jika Jika, comprised of 122 acres purchased by Sydney based merchant Charles William Roemer in 1840.⁶ In 1852, the property was acquired by Charles Montagu Manning (Solicitor General of Sydney) who saw potential for establishing a resting place on the way to Heidelberg. Manning subdivided the land into 130 lots of varying sizes, including provision for shops on both sides of Heidelberg Road.⁷ From 1853, lots from the 'Alphington Estate' were being offered for sale.⁸

This fine estate is about three miles from Melbourne, on the high road to Heidelberg. It contains 122 acres of land, subdivided into lots to suit purchasers; and is particularly well adapted for market gardens, villas, etc.⁹

The land was slow to sell however, with only 16 sales by the end of 1856.¹⁰ The map below shows that by 1855 a few buildings had been constructed in Alphington either side of Heidelberg Road. Amongst the earliest buildings were a general store with post office, the Wesleyan Chapel (extant) in Darebin, a bakery, a butcher (subject site) and at least two hotels.¹¹

⁵ Elva Errey, 'Victorian Architectural Ornament 1880-1920', Melbourne University 1972, p332

⁶ Roemer also purchased other land in the Port Phillip District, totalling 847 acres (343 hectares) - A F Pike, 'Charles William Roemer 1799-1874', Australian Dictionary of Biography

⁷ Andrew Lemon, *The Northcote Side of the River*, 1983, p48

⁸ The Alphington Estate was named after Alphington (near Exeter) in Devonshire, England, the birthplace of Charles Montagu Manning. *The Argus*, 5 April 1854, p9.

⁹ *The Argus*, 8 August 1853, p8

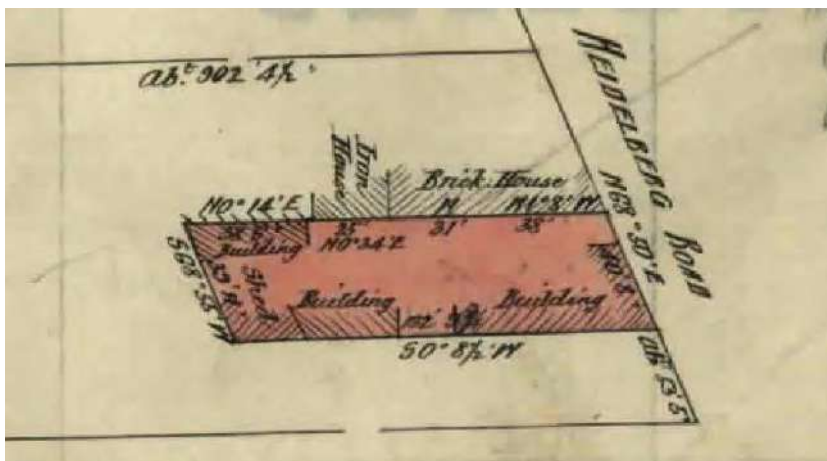
¹⁰ A Lemon, *The Northcote Side of the River*, p48

¹¹ A Lemon, *The Northcote Side of the River*, p48



'Melbourne and its suburbs' (compiled by James Kearney, 1855).
 Map dated 1855 showing a section of Heidelberg Road in Fairfield and Alphington. The map shows Heidelberg Road largely undeveloped, apart from three buildings (indicated) in the vicinity of the subject site.
 (Source: State Library of Victoria)

Land containing the subject site was purchased by John Mason in 1855. The extant two storey basalt building was constructed in the late 1850s or early 1860s. By 1863 it was owned and occupied by the butcher Thomas King(s), and there was a slaughter house and yards to the east.¹² It appears that King also worked as a baker for a period during the 1860s.¹³ During the 1860s, King placed various advertisements for staff in the newspaper, including for 'a steady man as butcher' (February 1863), 'a young lad that can kill and dress sheep' (March 1865), and 'a steady baker' (April 1865).¹⁴ The building is said to have undergone improvements in 1865 and in 1868.¹⁵



Plan of the subject site in 1887 (Heidelberg Road is at right). Hatching indicates the location of buildings.
 (Source: Certificate of Title, Vol. 1886 Fol. 194)

¹² Graeme Butler, *City of Northcote Urban Conservation Study*, 1982 (Building Reference Number: 49)

¹³ *The Argus*, 25 April 1865, p1; Sands and McDougall's Street Directories 1866+1867

¹⁴ *The Argus*, 3 February 1863, p1 (the reference to 'J' C King in the advertisement appears to be in error); *The Argus*, 8 March 1865, p1; *The Argus*, 25 April 1865, p1.

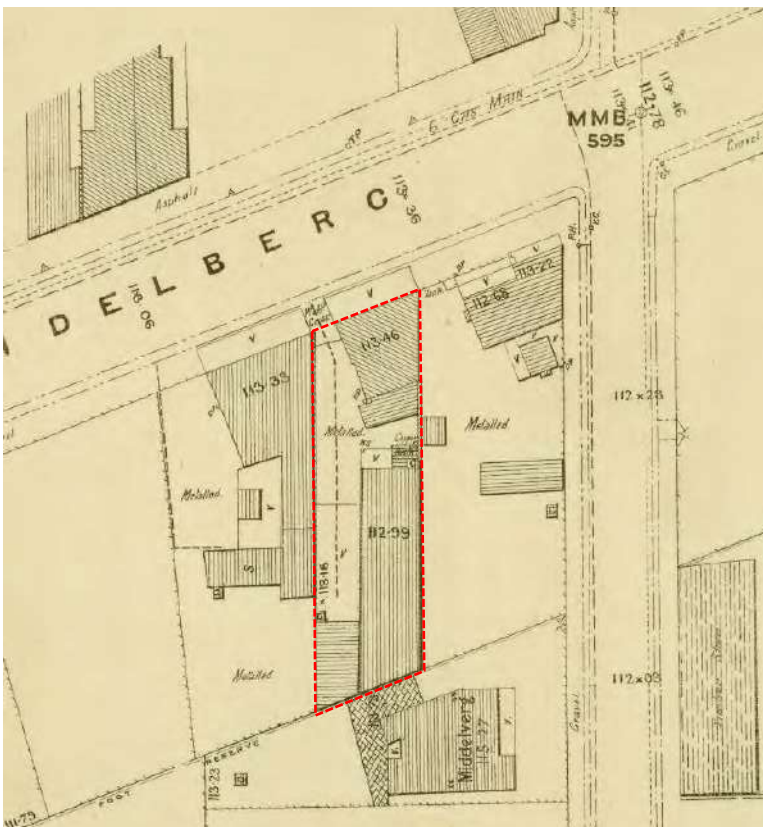
¹⁵ Graeme Butler, *City of Northcote Urban Conservation Study*, 1982 (Building Reference Number: 49)

Around 1879, the subject site was purchased by John Woolcock, butcher of Johnston Street Collingwood¹⁶ - the Torrens title was obtained by Woolcock in 1887.¹⁷ The 1887 title plan shows a building at the front of the site in the location of the subject building, as well as buildings on the east boundary and a building at the rear on the west boundary.

It appears that John Woolcock occupied the building for several years following the purchase. John Woolcock died in 1912, and the property was retained by members of the Woolcock family until 1959, after which time there have been various owners.¹⁸

From about 1890 to the early 1930s, the shop was leased to various butchers, including Ferrar Phillips (c.1890-1904), Charles W Phillips (1905-1912), Frederick R Vizard (1913-1915), Alfred Helm (1917-1919), and A E Wortley (1920-c.1930). After this time, the use of the site changed – in the 1930s and 1940s it was occupied by fruiterers, first William Eldridge and later L Buffington, and from the 1950s to the 1970s by confectioners including S C Gailey, S Brown, B Gaynor, I S Martin and K Papas.¹⁹

The MMBW plan of 1914 shows the footprints of the buildings at the subject site at that time. The extant basalt building is shown with a front verandah and additional masonry and timber sections attached behind (probably single storey, now removed). Large timber buildings and a covered area are shown in the rear yard, with access from Heidelberg Road provided on the west side of the site. The building footprints are consistent with those in the 1945 aerial photograph.

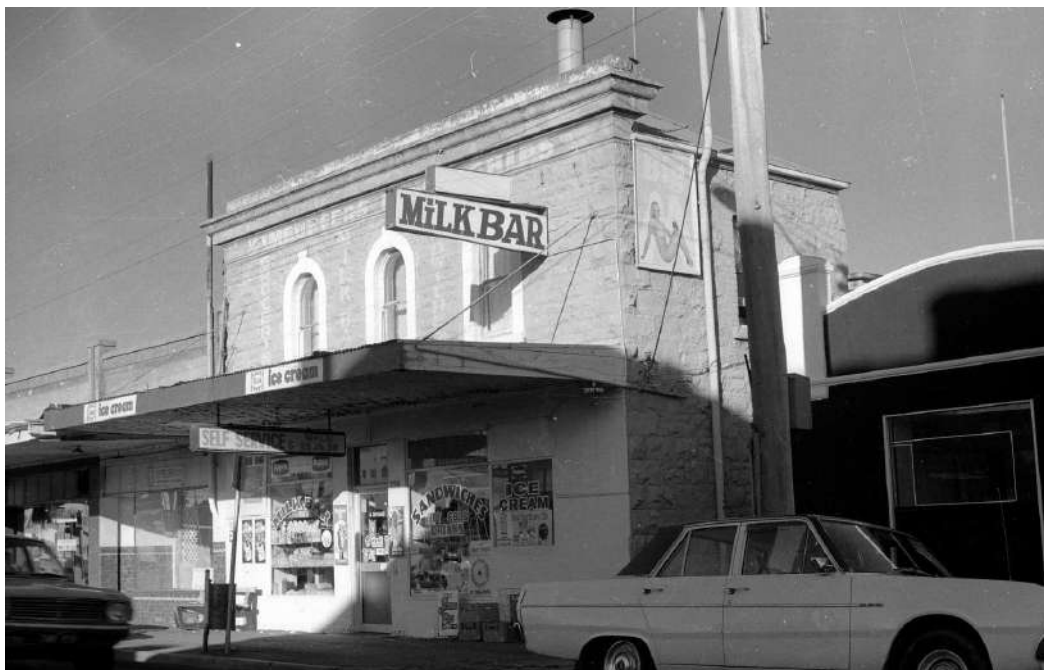


MMBW Detail Plan No. 1318, dated 1914
(Source: State Library of Victoria)

¹⁶ Graeme Butler, *City of Northcote Urban Conservation Study*, 1982 (Building Reference Number: 49)
¹⁷ Certificate of Title, Vol. 1886 Fol. 194
¹⁸ Certificate of Title Vol. 5698 Fol. 563
¹⁹ Sands and McDougall's Street Directories



Aerial photograph dated December 1945
(Source: Landata, Melbourne and Metropolitan Area Project, Proj. No. 5, Run 27, Frame 59523)



Photograph dated 1982
(Source: Darebin Archives LHRN3652)

By the 1980s, the front verandah had been replaced with a cantilevered canopy and the basalt walls had been painted. Between 2013 and 2016, the rear part of the site behind the basalt building was redeveloped.

Comparative Analysis

Details of most of the buildings dating to the 1850s and 1860s with an individual heritage overlay in the City of Yarra are summarised in the following table. Several are constructed in basalt with others being of brick and timber. Only a few display the influence of the Regency style, primarily HO183 + HO121 (both houses), or have some classicising detailing. The main shop buildings on Victoria Street, Abbotsford (Nos 295+511) are less articulated examples. In light of this, it can be said that the subject building is a rare and distinguished example of its type in the municipality.

HO	Name & Address	Date	Details
HO3 (VHR H654)	Former Grosvenor Common School 2 Bond Street, Abbotsford	1863	Two storey brick with some basalt to ground floor
HO227	House 6 Bosisto Street, Richmond	1858	Single storey basalt, coursed and quarry faced, cornice and parapet. Some Regency style influence
HO102	House 10-16 Derby Street, Collingwood	1868-69	Two storey bi-chrome brick, rendered cornice/parapet
HO13	House 13 Grosvenor Street, Abbotsford	1867	Single storey brick cottage, gable roof
HO14	House 19 Grosvenor Street, Abbotsford	Pre-1864	Single storey timber cottage, gable roof
HO268	Roeberry House 3 Hull Street, Richmond	1861	Two storey rendered stone with some classicising detailing
HO270	House 72 Kent Street, Richmond	1865	Single storey basalt cottage, gable roof Later verandah detailing
HO27	House 233 Langridge Street, Richmond	Pre-1870	Single storey brick, hipped roof, symmetrical Has some Italianate detailing including cornice
HO275	Warehouse 41 Madden Grove, Richmond	Circa 1860s (?)	Single storey basalt with red and cream brick detailing
HO115	Houses x2 12 Napoleon Street, Collingwood	Pre-1858 (?)	Single storey basalt, rendered façade, hipped roof
HO38	Houses x2 39 Nicholson Street, Abbotsford	1868-69	Part single, part two storey basalt with rendered façade, gable roofs
HO183 (VHR H172)	Royal Terrace 50-68 Nicholson street, Fitzroy	Circa 1860	Three storey, Regency style – basalt and render
HO121	Houses 37 Oxford Street, Collingwood	1869	Two storey brick, brown brick with cream brick quoining + dressings, concealed roof. Regency style
HO122	Houses 39-41 Oxford Street, Collingwood	Pre-1858	Pair of single, storey timber cottages with gable roofs
HO56	Shop 295 Victoria Street, Abbotsford	1868	Two storey, rendered masonry corner building with parapet concealing roof, limited detailing
HO61	Shop 511 Victoria Street, Abbotsford	1860	Two storey, red-brown brick corner building, with rendered parapet concealing roof, limited detailing

Condition

Good

Integrity

Largely intact

Previous Assessment

City of Northcote Urban Conservation Study (Graeme Butler, 1982) – Recommended for state and local listing.

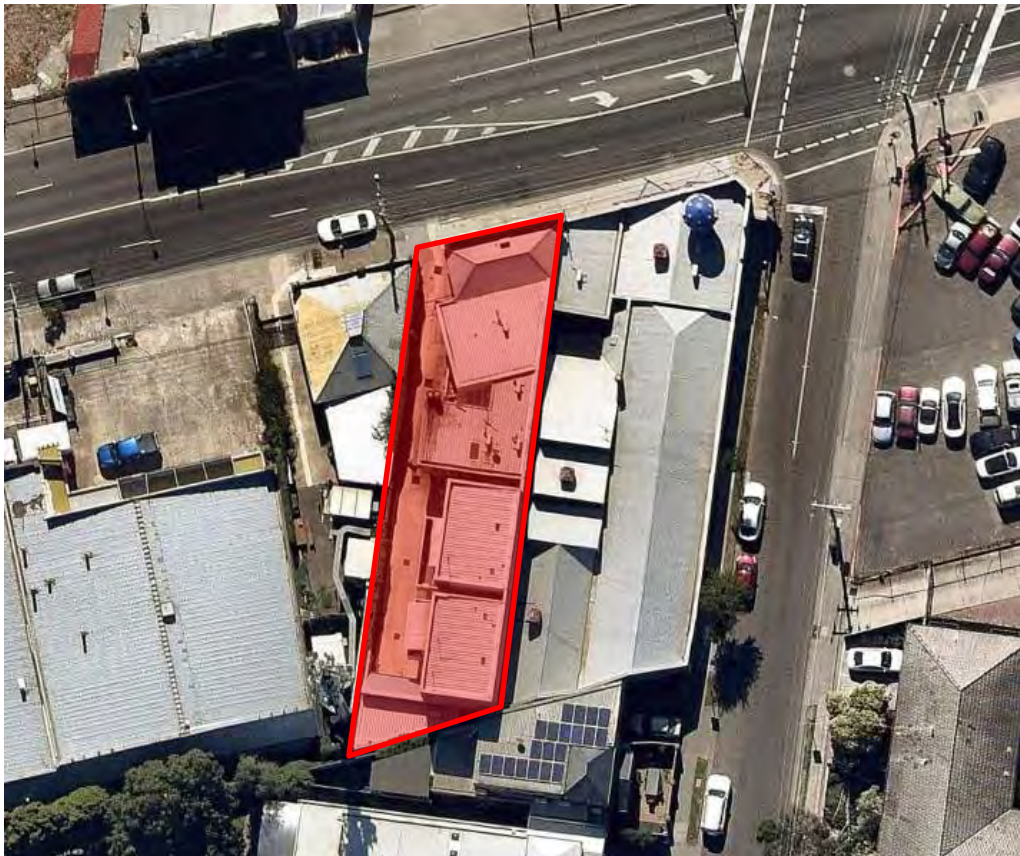
City of Yarra Heritage Review (Allom Lovell and Associates, 1998) – Graded 'B'²⁰ and recommended for heritage overlay controls

Heritage Overlay Schedule Controls

External Paint Controls	No
Internal Alteration Controls	No
Tree Controls	No
Outbuildings and/or Fences	No

Extent of Heritage Overlay

The proposed extent of the heritage overlay would be to maintain the current heritage overlay (HO71) as indicated.



Recommended extent of Heritage Overlay
(Source: Nearmap, April 2019)

²⁰ Three gradings were used in the study: 'A' (primary significance – state level), 'B' (primary significance – local level) and 'C' (contributory significance – local level)

APPENDIX B – Data Sheets

No.	Address	Name
1	358 Heidelberg Road, Fairfield	House (<i>Valeema</i>) & Tyre Outlet
2	460 Heidelberg Road, Fairfield	Duncan Rubber Co. Showroom & Warehouse
3	730-734 Heidelberg Road, Alphington	Group of 3 shops
4	760-764 Heidelberg Road, Alphington	A Cooper Knitting Manufacturer
5	774 Heidelberg Road, Alphington	Former Ampol Service station

HOUSE (VALEEMA) & TYRE OUTLET

Address 358 Heidelberg Road, Fairfield

Previous Assessment N/A



1945 aerial showing earlier timber buildings on the subject site (Source: Landata)



Nearmap 2019

History

- In 1935 several timber framed buildings (probably a house and outbuildings) were located on the subject site. At that stage Panther Place was known as Bond Street. In 1938 no. 358 was occupied by Miss A R Williams, confectioner.¹
- In 1945 several structures are apparent on the site however the extant buildings are not evident.²
- In 1945 no. 358 was occupied Mrs E F Chappel and in 1950 was occupied by Chas H Chappell.³ The garage is not listed in 1950.
- By 1955 no. 358 had two listings - Lincoln Tyre Service, motor tyres and Mrs E Z Chappell.⁴
- The extant house and tyre outlet were constructed in stages. It is likely the first part of the house and tyre service were constructed during this period.
- A 1956 aerial⁵ shows part of the extant house to the south of the site and part of the extant tyre outlet to the north of the site.
- In 1960 no. 358 is listed as Fairfield Tyre Service. R A Cutts is also listed as occupying the site.⁶
- Alterations: A western wing and entrance gable have been added to the house and the tyre outlet may have been extended to the south. Painted signage has been added to the street facades of the tyre outlet.

¹ Sands & McDougall directory, 1938

² 1945 Dec - Landata, Melbourne and Metropolitan Area Project, Proj. No. 5, Run 27, Frame 59529

³ Sands & McDougall directory, 1945-1950

⁴ Sands & McDougall directory, 1955

⁵ 1956 Feb - Landata, Melbourne Outer Suburbs Project, Pro. No. 250, Run 13, Frame 146

⁶ Sands & McDougall directory, 1960



Panther Street (west) elevation



House – Park Crescent (primary) façade
Junction between original (right) and additions (left)



Tyre outlet



Tyre outlet

Description

- No. 358 is located at the intersection of Heidelberg Road and Panther Place. A two storey Post-WWII period dwelling with an L-shaped footprint is located to the south of the site with frontages to Panther Place and Park Crescent. The painted brick building is indicative of the period with its uncomplicated form and consists of two low-pitched intersecting gable roofs and a single storey entrance gable on the Park Crescent façade **with a small sign 'Valeema.'**⁷ Original elements such as the steel windows and glazed roof tiles remain.
- It is likely that the house was built in two stages as discerned by the slightly different colour tiling to the roof sections (west wing and entry porch) and a break in the brickwork.
- A single storey garage/tyre outlet adjoins the dwelling to the north. The brick building is largely rectangular in footprint, with a chamfered north-west corner. The brick walls and parapet roof have been painted. Large steel framed windows and roller door openings are located on the north and west facades. A brick flange at the north-east corner accommodates a street number (probably original). awning/carport extends from the south end of the west façade.
- **A frame mounted 'Bridgestone tires' sign has been installed on the roof** (possibly at an early stage). Original bowser pipes remain to the front of the site.
- Condition: poor fair good
- Intactness: poor fair good

Comparative

- No ready comparison for this combination.

⁷ The derivation of the name is uncertain, possibly an adaptation of Walima – banquet part of traditional Islamic wedding.

- 28-30 Johnston Street, Collingwood (part of HO324, Johnston Street Precinct). Not significant. Basic brick garage constructed in 1960s-70s.

HERCON Criteria

Criterion	Detail	Applicability
A	Historical	The two storey brick house and tyre outlet are representative of the Post-WWII period activity along the Heidelberg Road, which has long been a major thoroughfare with pockets of commercial/industrial development.
E	Aesthetic	Intact and unusual example of a combined house and adjoining tyre outlet on a prominent corner site, probably built in stages during the 1950s.

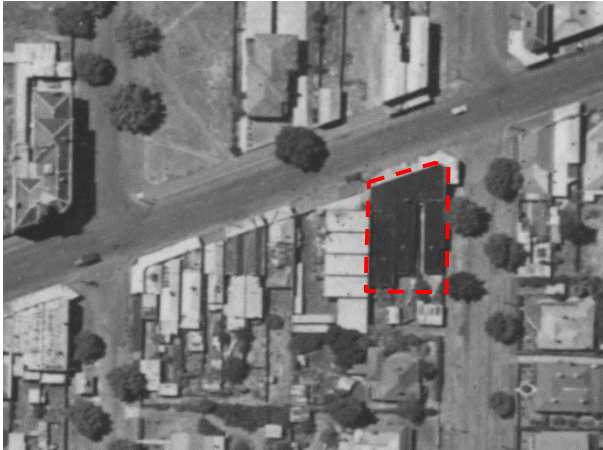
Recommendations

It is considered that the place has strong potential to meet the threshold for local significance and is recommended for further assessment during Stage 2.

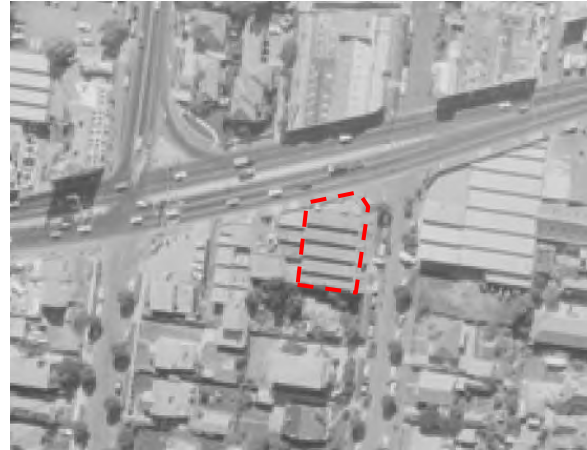
DUNCAN RUBBER CO. SHOWROOM & WAREHOUSE

Address 460 Heidelberg Road, Fairfield

Previous Assessment N/A



1945 aerial showing earlier buildings on the subject site
(Source: Landata)



1978 aerial showing the extant building
(Source: Landata)

History

- In 1945 the site was occupied by three small buildings with verandahs along Heidelberg Road, and larger rectangular sections with chimneys to the rear.¹
- From 1945 to 1950, nos 460-464 (the subject site) was occupied by various individuals and retail shops. In 1945 Mrs L A Crocker, fruiterer is listed at no. 460 and Mrs S Connor, a grocer, is listed at no. 464. In 1950 T Robbins occupied a factory situated at no. 464 and rubber goods manufacturer, Duncan Rubber Co. occupied no. 462.
- From 1955 to 1965 nos. 460-464 (the subject site) was occupied solely by rubber goods manufacturers, Duncan Rubber Co Py Ltd.² The site appears vacant in a 1956 aerial of the site,³ and it is likely the extant building was constructed soon after for the company.
- The extant building is evident in a 1978 aerial.⁴
- Alterations: The original materials have been painted and tile cladding has been added to the chamfered corner entry at street level.

¹ 1945 Dec - Landata, Melbourne and Metropolitan Area Project, Proj. No. 5, Run 27, Frame 59523

² Sands & McDougall directory, 1955-1965

³ 1956 Feb - Landata, Melbourne Outer Suburbs Project, Pro. No. 250, Run 13, Frame 146

⁴ 1978 Apr - Landata, Western Port Foreshores, Proj. No. 1716, Run 3, Frame 105



Heidelberg Road (north) elevation



Arthur Street (east) elevation



Sawtooth roof forms from Heidelberg Road



Steel framed windows on the Arthur Street (east) facade

Description

- The two storey brick building is located at the intersection of Heidelberg Road and Arthur Street and is currently occupied by a tile company. The entrance to the building is located on a chamfered corner to the north-east edge of the site. The Heidelberg Road façade has a brick sections at either end with a partial return to Arthur Street.
- The Heidelberg Road façade is divided into four bays by a series of steel girders. The ground floor features four large rectangular windows while the first floor consists of four large tripartite timber windows with wide mullions. Unusually horizontal timber board cladding spans between the steel girders. The chamfered corner is articulated in a similar manner.
- The original cream brick colour employed to the street elevations has been recently painted (evident on Google streetview). Original openings with multi-paned steel framed windows and brick sills remain at ground and first floor level and prominent rainwater heads are recessed at street level. The rear (south) façade retains the original red brick facade and cream brick quoining to the south west corner of the building.
- The sawtooth roofs are highly visible and highlight the function of the building, and is unusual in this largely suburban context.
- Condition: poor fair good
- Intactness: poor fair good

Comparative

- Few buildings of the Post WWII period are probably noted as contributory but several are located in precincts whose significance relates to earlier periods (Victorian, Federation and Interwar).
- An example at 409-429 Gore Street, Fitzroy (part of HO334, South Fitzroy Precinct). Not contributory. Two storey, brick factory, now painted also, constructed during 1960s. It has long banks of metal framed windows and recessed glazed entry.

HERCON Criteria

Criterion	Detail	Applicability
A	Historical	Representative of the Post-WW II period and the expansion of commercial development in the Heidelberg Road area during the 1950s.
E	Aesthetic	Intact example of a two-storey Postwar period factory built on a prominent corner site with sawtooth roof forms. The brick building is distinguished by the expression of steel girders, large expanses of glazing and combination of materials including timber boards. Original openings, timber and steel windows remain intact.

Recommendations

It is considered that the place has potential to meet the threshold for local significance and could be further assessed during Stage 2.

GROUP OF 3 SHOPS

Address 730-734 Heidelberg Road, Alphington

Previous Assessment N/A



1945 Aerial
(Source Landata)



Nearmap 2019

History

- In 1933 the subject site at nos 730-734 was vacant.¹
- In 1933, nos 730-734 were listed as occupied.² This includes R Nickless, fruiterer at no. 730 and E J March, bootmaker at no. 734.
- The extant buildings at nos 730-734 appear in a 1945 aerial.³ A hipped gable roof is situated to the front of the three sites with narrower buildings and minor structures to the south of no. 732 and 734.
- By 1968, the Alphington Post Office was located no. 730.⁴ The building continues to operate as a Post Office.



Heidelberg Road elevation – no. 734 (left) and no. 732 (right)



Heidelberg Road elevation – no. 730 (left) and no. 728 (right)

¹ MMBW Plan 116, dated ca 1933

² Sands & McDougall directory, 1933

³ 1945 Dec - Landata, Melbourne and Metropolitan Area Project, Proj. No. 5, Run 27, Frame 59523

⁴ National Archives of Australia, 1968



1968 Heidelberg Road elevation – no. 730 (Alphington Post Office)
(Source: NAA)



Shopfront – no. 730



Description

- The group of three Interwar period shops at nos 730-734 Heidelberg Road are each located on a narrow allotment.
- Nos 732-734 are a single storey brick pair of shops with a plain parapet that conceals two metal clad hipped gable roofs. To the rear of no. 732 is a narrow outbuilding and a timber shed. The two structures appear in a 1945 aerial and are possibly original.

The street facades are identical and for the most part retain original shopfronts. The badge of the manufacturer (Duff) survives on each shopfront however these have been overpainted. The configuration of the shopfronts and detailing of the parapet is typical of the Interwar period and despite having been painted, are intact.

- No. 730 is single storey brick building and is more elaborate than nos. 732-734. The stepped parapet features a square pediment and capped brick piers at either end. The original configuration of the shopfront remains and features a central recessed tiled entry with pressed metal ceiling, copper-finish shopfronts and dark green tiles and mouldings to the lower section.
- Condition: poor fair good
- Intactness: poor fair good

Comparative

- Interwar period shops have probably not been well assessed in some precincts if the period of significance relates primarily to earlier periods, Victorian and Federation.
- 207 Bridge Road, Richmond (part of HO310, Bridge Road Precinct). Not contributory. Single storey brick shop constructed in 1930. Altered shopfront though Roman brown brick pier intact.
- 160 Johnston Street, Collingwood (part of HO324, Johnston Street Precinct). Not contributory. Single storey brick, paired shops with stepped parapet and recessed shopfronts. Largely intact.

HERCON Criteria

Criterion	Detail	Applicability
A	Historical	Representative of the Interwar period and the expansion of commercial development in the Heidelberg Road area.
E	Aesthetic	Intact group of single-storey Interwar period buildings. The brick buildings feature original parapets and intact shopfronts.

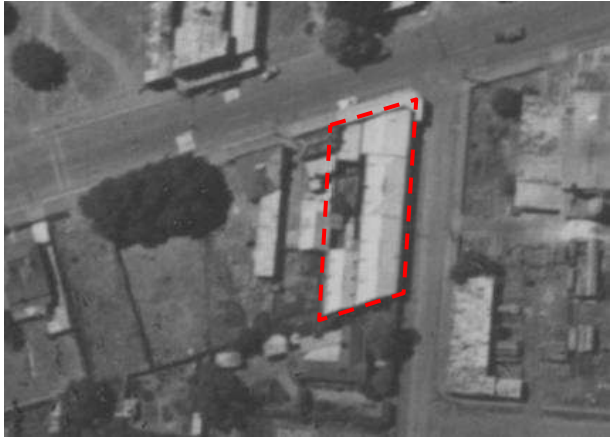
Recommendations

It is considered that the place has strong potential to meet the threshold for local significance and is recommended for further assessment during Stage 2.

A COOPER KNITTING MANUFACTURER

Address 760-764 Heidelberg Road, Alphington

Previous Assessment N/A



1945 Aerial



Nearthmap, 2019

History

- In 1914 the subject site at the intersection of Heidelberg Road and Yarra Street (now Yarralea Street) was occupied by timber-framed structures with a frontage to Heidelberg Road.¹ Smaller timber structures were situated to the middle of the site.
- Mrs Mary Kennedy, grocer, occupied no. 383 (the subject site) from at least 1916 to 1921.²
- Various occupants are listed as occupying no. 383 during the 1920s – early 1930s. In 1922, no. 383 is occupied by Ryan James, hairdresser and tobacconist. The building later functions as a billiard saloon from 1925-1931.³
- The timber structures remained on the site until at least 1933.⁴ (At this stage Yarra Street had been renamed Sutton Street however Street Directories maintain Yarra Street until 1942 at which stage it is renamed Yarralea Street).
- In 1933, no. 383 was renumbered as nos 760-764. No. 760 was listed as vacant while no. 762 and no. 764 were occupied by D Nicholson, hairdresser and J Harris, billiard saloon.⁵ In 1935 no. 760 was occupied by F Fletcher, boot repairer, no. 762 was listed as vacant and no. 764 was occupied by D Nicholson, hairdresser.⁶
- A Cooper, knitted goods manufacturer occupied no. 764 in 1938 and 1945⁷ for who the extant building was probably constructed.
- The extant building, with the exception of a few minor structures to the west of the site is evident in a 1945 aerial of the site.⁸ The site consisted of small gabled roof forms to the north of the site with narrower gabled roof forms to the rear.
- Alterations: The central window on the Heidelberg Road façade has been modified and the original façade has been repainted. Rendered panels have been added to the parapet. The southern windows on the east elevation have been truncated in height.

¹ MMBW Detail Plan 1318, dated 1914

² Sands & McDougall directory, 1919-1921

³ Sands & McDougall directory, 1925-1931

⁴ MMBW Plan 116, dated ca 1933

⁵ Sands & McDougall directory, 1933

⁶ Sands & McDougall directory, 1935

⁷ Sands & McDougall directory, 1938, 1945

⁸ 1945 Dec - Landata, Melbourne and Metropolitan Area Project, Proj. No. 5, Run 27, Frame 59523



Heidelberg Road (north) elevation



1982 image – showing original parapet detailing



Yarralea Street (east) elevation



Yarralea Street (east) elevation

Description

- The single storey brick building is located on the corner of Heidelberg Road and Yarralea Street. The entrance is located on a chamfered north-east corner.
- The building consists of diverse roof sections indicating some staged construction on the site and typically clad with metal sheeting.
- A brick parapet features on the Heidelberg Road facade with a partial return to Yarralea Street. The parapet consists of capped brick piers that extend through the parapet and small sections of roughcast render. Original contrasting brick elements such as soldier course detailing below the parapet edge has been painted. The south and east facades are relatively plain.
- Concrete lintels and the original pattern of openings remain on the street facades and the outermost windows on the Heidelberg façade retain their original timber mouldings. The north end of the Yarralea Street façade features original timber windows with brick sills. Further south glass bricks have been introduced into the truncated openings.
- Condition: poor fair good
- Intactness: poor fair good

Comparative

- Interwar period factories/warehouses have probably not been well assessed in some precincts if the period of significance relates primarily to earlier periods, Victorian and Federation.
- 33 Spensley Street, Clifton Hill (part of HO316, Clifton Hill East Heritage Overlay). Not significant. Brick factory/warehouse, now units constructed in 1925. Façade largely intact.

HERCON Criteria

Criterion	Detail	Applicability
A	Historical	Representative of the Interwar period and commercial development in the Heidelberg Road area during the late 1930s.
E	Aesthetic	Intact example of a single-storey Interwar period building built on a prominent corner site. The brick building is distinguished by its parapet and projecting piers, and articulated with a combination of smooth and rough cast render. Original pattern of openings remains intact with some change to the windows.

Recommendations

It is considered that the place has strong potential to meet the threshold for local significance and is recommended for further assessment during Stage 2.

FORMER AMPOL SERVICE STATION

Address 774-780 Heidelberg Road, Alphington

Previous Assessment N/A



1978 Aerial



Nearmap, 2019

History

- In 1933¹ several timber-framed structures were located on the subject site and by 1945² multiple structures were evident on the site. Some of the structures are visible in 1954.³
- In 1955, no. 774 is not listed.
- In 1956 several structures were situated on the east half of the site while the remainder of the site consisted of a large grassed area. The extant building is not evident.⁴
- In 1960 Richardson & Kirwan Pty Ltd operated a 'used trucks' business from the site⁵
- In 1965 no. 778 is listed as Ampol Service Station.
- The subject building is evident in a 1978 aerial of the site.⁶ The roof form is slightly different from the existing roof however the building footprint is the same. The entire site appeared to be paved.

¹ MMBW Plan 116, dated ca 1933

² 1945 Dec - Landata, Melbourne and Metropolitan Area Project, Proj. No. 5, Run 27, Frame 59523

³ 1954 Mar - Landata, Melbourne and Metropolitan Project No. 3, Proj. No. 174, Run 19, Frame 42

⁴ 1956 Feb - Landata, Melbourne Outer Suburbs Project, Pro. No. 250, Run 13, Frame 146

⁵ Sands & McDougall directory, 1960

⁶ 1978 Apr - Landata, Western Port Foreshores, Proj. No. 1716, Run 3, Frame 105



Yarralea Street (west) elevation



Heidelberg Road - west wing



Heidelberg Road – east end



Signage in north-west corner

Description

- The single storey Postwar service station is located on the corner of Heidelberg Road and Yarralea Street. The building is located to the rear south-east corner of the site and comprises an east and a west wing. A sign, whose frame is probably original, is located in the north-west corner of the site while the remainder of the site is concreted.
- The skillion roof forms are a distinctive feature of the building. The west wing consists of a metal-clad roof and external wall that **'wraps'** around the building while the east wing consists of a cantilevered roof supported on thin columns, forming a carport. The north facades have a combination of infill panelling and glazing.
- The original brick wall on the south facade remains however some openings have been modified.
- Condition: poor fair good
- Intactness: poor fair good

Comparative

- It seems no service station has been identified as contributory to a precinct, or individually significant, in the municipality. This may be the most intact, relatively early example.
- 206-208 Johnston Street, Fitzroy (part of HO334, South Fitzroy Precinct). Not contributory. Had been a similar type but altered/rebuilt.
- 786-794 Nicholson Street, Fitzroy North (part of HO327, North Fitzroy Precinct). Not contributory. Similar scale/type which may be (partly) intact.

HERCON Criteria

Criterion	Detail	Applicability
A	Historical	Representative of the Postwar period and the expansion of commercial development in the Heidelberg area during the early 1960s.
E	Aesthetic	Intact example of a Postwar period service station built on a prominent corner site. The building is distinguished by its skillion roof forms which are evocative of the period.

Recommendations

It is considered that the place has potential to meet the threshold for local significance and could be assessed during Stage 2.



No.760-764



No. 730-734

Heidelberg Road Heritage Review (Stage 2) 2019

Stage 2 Report (recommendations & citations for proposed heritage overlays)

City of Yarra

RBA ARCHITECTS +
CONSERVATION CONSULTANTS PTY LTD
FITZROY STREET 4C/171
ST KILDA VIC AUSTRALIA 3182

+613 9525 5666 TEL
+613 9525 4906 FAX

rba@rbaarchitects.com.au EMAIL
www.rbaarchitects.com.au WEB



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Personnel:

Anthony Hemingway Architectural Historian/Senior Associate
Erin Williams Heritage Consultant
Ashleigh Ngan Architectural Graduate

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1 INTRODUCTION

1.1 Overview

This report, the *Heidelberg Road Heritage Review 2019*, has been prepared by RBA Architects + Conservation Consultants for the City of Yarra.

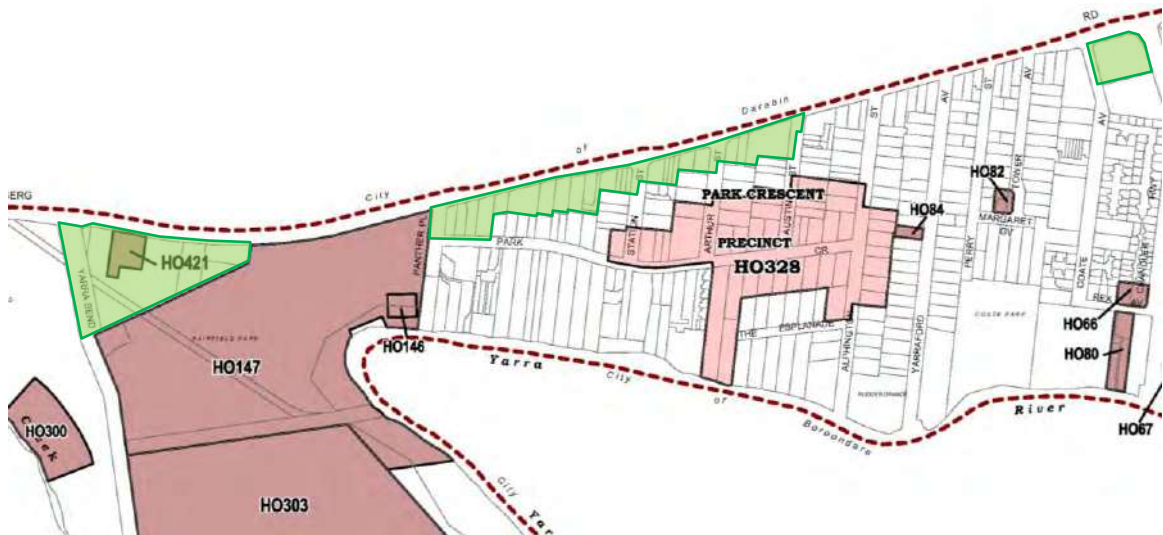
It provides recommendations and citations for the following three proposed heritage overlays within the Commercial 1 and Commercial 2 Zones along (the south side of) Heidelberg Road between the Merri and Darebin creeks.

- No. 358 – House (*Valeema*) & Service Centre
- No. 730-734 – Group of 3 shops
- No. 760-764 – A. Cooper Knitting Factory (Former)

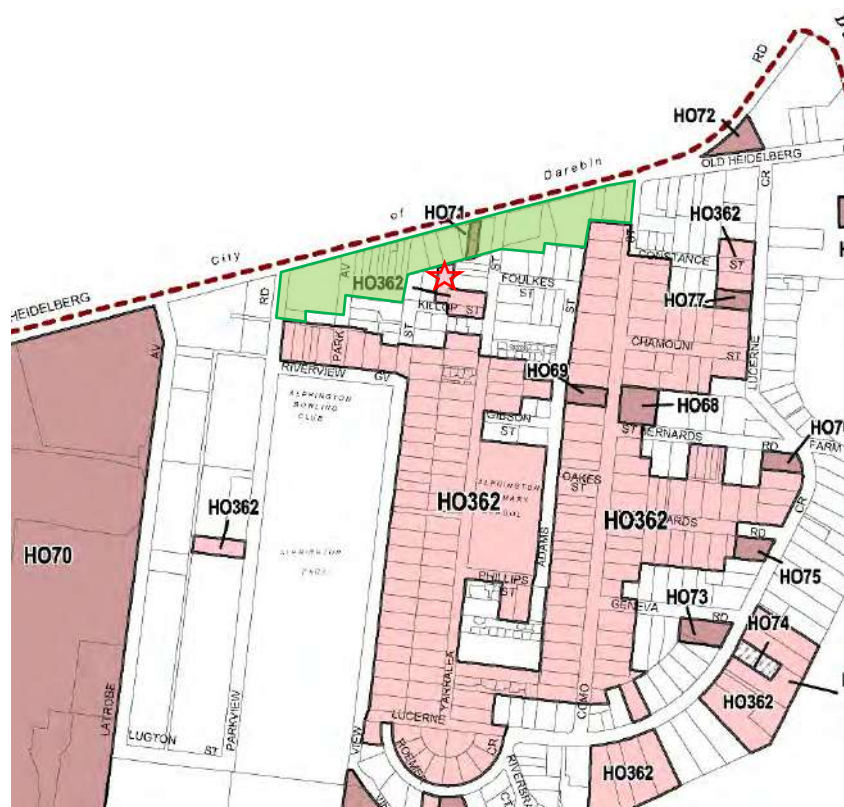
In addition, an overview of the development along Heidelberg Road has been prepared to assist with the preparation of the citations and the attribution of significance.

1.2 Location

The study area primarily relates to the commercial zones along the south side of Heidelberg Road and extends across parts of the suburbs of Fairfield (western part) and Alphington (eastern part).



Map No. 3HO
Existing HO421 (Porta)
Western part of study area (Commercial 1 and 2 Zones) shown in green



Map No. 4HO showing existing HO71 and 2 Killop Street, Alphington (identified with a star, part of HO362). Eastern part of study area (Commercial 1 and 2 Zones) is shown in green

1.3 Background

Previous Heritage Studies

- City of Yarra, Review of Heritage Overlay Areas 2007, Appendix 8, prepared by Graeme Butler and Associated, revised May 2017
- City of Yarra Heritage Review, Thematic History Volume 1, prepared by Allom Lovell & Associates, July 1998
- City of Northcote Urban Conservation Study prepared by Graeme Butler, revised February 1983

Existing Listings

There are three sites in the study area listed in the Schedule to the Heritage Overlay:

- HO421 – Porta factory, Heidelberg Road, 224 Heidelberg Road.
- HO70 – Australian Paper Mills Boiler House, 626 Heidelberg Road.
- HO71 – Shops, 756-758 Heidelberg Road – basalt building constructed circa 1860. Existing citation was reviewed in Stage 1.

1.4 Acknowledgements

The authors are grateful for the assistance provided by the City of Yarra officers.

2 METHODOLOGY

2.1 Introduction

The methodology adopted in undertaking this phase of Heidelberg Road Heritage Review (stage 2) was in accordance with the processes and criteria outlined in the *Australia ICOMOS Charter for the Conservation of Places of Cultural Significance*, known as the *Burra Charter* (Australia ICOMOS, 2013). The key tasks included:

- Site inspections - largely limited to a visual assessment of each property from the perimeter.
- Historical research and analysis of the extant fabric in relation to documentary evidence.
- Preparation of a physical description.
- Assessment of the significance of each site based on the research and the extant fabric.
- Preparation of citations (statement of significance, history and description) for those places warranting heritage protection, with reference to the relevant HERCON criteria.

2.2 Research

A combination of primary and secondary sources were consulted as follows.

Primary sources were mainly employed and have included:

- Photographs, including aerial photographs, held by:
 - Landata (aerial),
 - Darebin Archives,
 - National Archives of Australia (NAA),
 - State Library of Victoria (SLV),
 - University of Melbourne (aerial).
- Drainage plans (Yarra Valley Water),
- Melbourne Metropolitan Board of Works (MMBW) Plans, held by the State Library Victoria,
- Sands & McDougall's street directories,
- Plans (SLV, etc),
- Various newspapers (*Age*, *Argus*, *Herald*, etc.).

2.3 Citations

A citation was prepared for 3 individual buildings, or groups of buildings, of potential significance within the study area.

Within the citations the following are provided:

- Name (usually relating to the original owners or purpose),
- Address,
- History – including date of construction and period (Federation, Interwar, Post WWII),
- Description – including assessment of condition and intactness (poor, fair, good),
- Comparative – examples of similar places,
- Relevant HERCON Criteria,
- Recommendation.

2.4 Assessment of Significance

Each statement of significance is provided in the recognised, three part format of:

- What is significant?
- How is it significant?
- Why is it significant?

Burra Charter

For heritage professionals generally in Australia dealing with post-contact cultural heritage, the process outlined in the *Burra Charter* underpins the approach to heritage assessment and conservation adopted by the authors of this report.

The methodology adopted in the assessment of the significance (or heritage values) has been in accordance with the process outlined in the *Burra Charter* (or The Australia ICOMOS Charter for Places of Cultural Significance). As outlined in the *Burra Charter*, the criteria considered include aesthetic (including architectural), historical, scientific (or technical), social and spiritual values. These values have been translated into the HERCON Criteria, discussed below.

2.5 Applying the Heritage Overlay

'Applying the Heritage Overlay' August 2018 (Planning Practice Note 1) provides guidance about the use of the Heritage Overlay, including the following:

- What places should be included in the Heritage Overlay?
- What are recognised heritage criteria?
- Writing statements of significance.

The practice note indicates that the HERCON criteria are to be employed when assessing heritage significance.

HERCON Criteria

These widely used criteria were adopted at the 1998 Conference on Heritage (HERCON) and are based on the earlier, and much used, Australian Heritage Commission (now Australian Heritage Council, AHC) criteria for the Register of the National Estate (RNE).

The HERCON criteria are essentially a rationalised (more user-friendly) version of the AHC Criteria (which included different sub-criteria for cultural or natural heritage). **It is also noted in the aforementioned practice note that 'The adoption of the above criteria does not diminish heritage assessment work undertaken before 2012 using older versions of criteria.'** Reference to the relevant HERCON criteria is outlined at the end of the data sheets. The definition of these criteria are outlined in the following table.

Criterion	Definition
A	Importance to the course, or pattern, of our cultural or natural history (historical significance).
B	Possession of uncommon, rare or endangered aspects of our cultural or natural history (rarity).
C	Potential to yield information that will contribute to an understanding of our cultural or natural history (research potential)
D	Importance in demonstrating the principal characteristics of a class of cultural or natural places or environments (representativeness)
E	Importance in exhibiting particular aesthetic characteristics (aesthetic significance).
F	Importance in demonstrating a high degree of creative or technical achievement at a particular period (technical significance).
G	Strong or special association with a particular community or cultural group for social, cultural or

Criterion	Definition
	spiritual reasons. This includes the significance of a place to Indigenous peoples as part of their continuing and developing cultural traditions (social significance)
H	Special association with the life or works of a person, or group of persons, of importance in our history (associative significance).

3 STUDY AREA – Overview of Development

3.1 Introduction

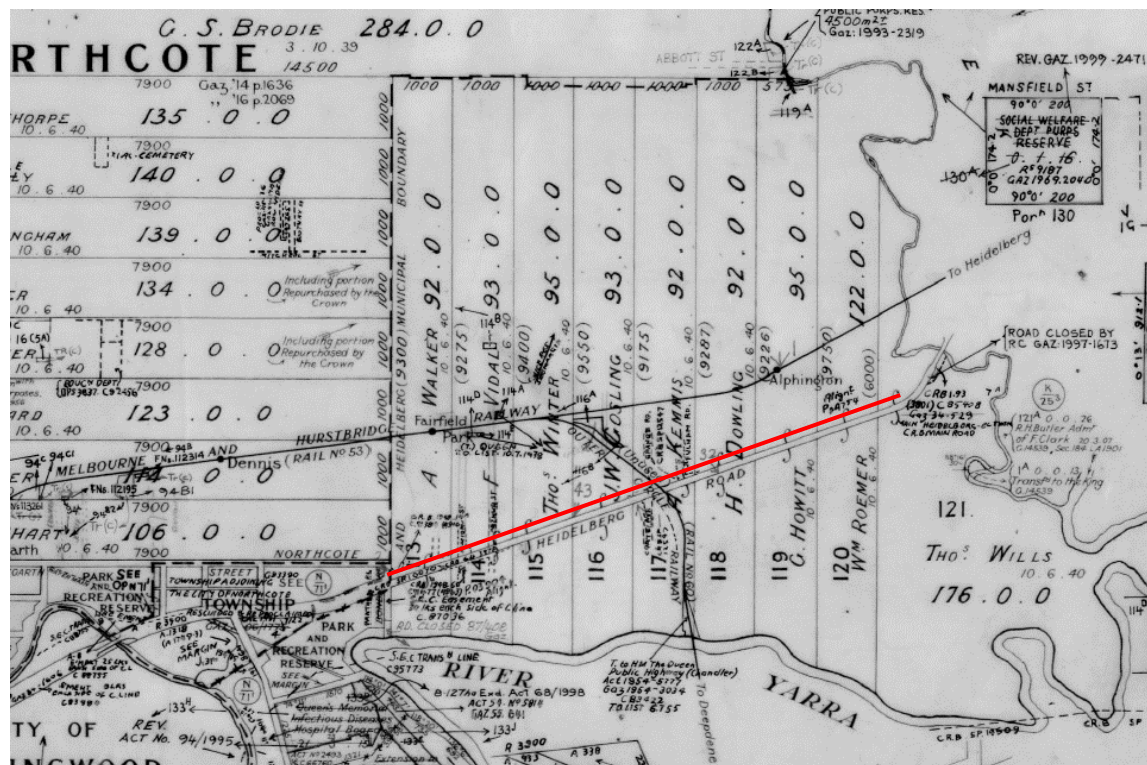
Heidelberg Road has a distinct history as it was established as a major thoroughfare early in Melbourne's history and has accommodated some industrial activity in an otherwise largely residential/suburban part of Melbourne.

This preliminary overview of development of the relevant part of Heidelberg Road has been prepared given the lack of specific detail regarding it in the Thematic History (Allom Lovell & Associates, *City of Yarra Heritage Review*, vol. 1, 1998). This is partly due to the study area being located on the periphery of the municipality, and over the last 150 years of European development, Heidelberg Road has typically formed the barrier between municipalities, such that its history has been traditionally fragmented by this artificial division.

3.2 Historical Overview

The study area along Heidelberg Road is located in what was the Parish of Jika Jika. During the 1840s the land was initially divided into a series of large but generally narrow (about 90-100 acres) Crown allotments with frontages to the Yarra River or Darebin Creek. The rich soil, topography of the land, and close proximity to the Yarra River was considered desirable.¹ The area includes sections of what is now Fairfield, Alphington and Northcote.

Heidelberg Road was developed along the line of an earlier track leading to Heidelberg via a crossing point on the Darebin Creek (now Alphington). The Heidelberg area attracted wealthy settlers from the late 1830s. Despite the road being well used by those travelling to and from Heidelberg, development along the road (in today's Alphington and Fairfield region) was initially slow.²



Part of Parish of Jika Jika J16(5)
 Section of Heidelberg Road within study area (red line) (Source: Landata)

1 City of Darebin
 2 Andrew Lemon, *The Northcote Side of the River*, 1983, p35-36

Alphington Village was established by William Montagu Manning (Solicitor General of Sydney) who acquired Charles William Roemer's original Crown Allotment 120 in 1852 and, seeing potential for a resting place on the way to Heidelberg, subdivided the land into 130 lots of varying sizes, including provision for shops on both sides of Heidelberg Road.³ The allotments were distributed around a north-south access road called Yarra Street (now Yarralea Street) which extended towards the Darebin Creek.⁴

From 1853, lots from the 'Alphington Estate' were offered for sale.⁵ The land was slow to sell however, with only 16 sales by the end of 1856.⁶ Amongst the earliest buildings in Alphington were a general store with post office, a bakery, and at least two hotels.⁷ The former butcher shop at 756-8 Heidelberg Road was built circa 1860 and is the oldest surviving commercial building of the original Alphington Village. The Wesleyan Chapel constructed circa 1859 on the north side of Heidelberg Road also remains. (City of Darebin)



'Melbourne and its suburbs' (compiled by James Kearney, 1855).

Map dated 1855 showing a section of Heidelberg Road in Fairfield and Alphington with only a few buildings recorded (indicated). (Source: State Library of Victoria)

While the Fairfield-Alphington area remained sparsely settled it was not unproductive and local farms were able to provide local produce to travellers passing through the area.⁸ **'Fulham Grange' was an early farm** and nursery located halfway between Fairfield and Alphington and along the Yarra River. Established in 1849 by Richard Perry, the portion of land consisted of 75 acres on the north side of Heidelberg Road and 25 acres of land on the south side of the Heidelberg Road, which was bound by the Yarra River.⁹ Fulham Grange was captured by painter Eugene von Guerard in 1855 and depicted the untamed bush **and 'real Australian trees... with a degree of care and accuracy of scientific value.'**¹⁰ A farmhouse, vegetable garden, vineyard, carriage and birdhouse are evident in von Guerard's painting **'...a pocket of cultivation nestled cosily in the bush.'**¹¹

³ A Lemon, *The Northcote Side of the River*, p48

⁴ Graeme Butler, *City of Northcote Urban Conservation Study*, revised February 1983, p5

⁵ The Alphington Estate was named after Alphington (near Exeter) in Devonshire, England, the birthplace of William Montagu Manning. *The Argus*, 5 April 1854, p9.

⁶ A Lemon, *The Northcote Side of the River*, p48

⁷ A Lemon, *The Northcote Side of the River*, p48

⁸ A Lemon, *The Northcote Side of the River*, p48

⁹ *Leader*, 24 December 1875, p9

¹⁰ A Lemon, *The Northcote Side of the River*, p48

¹¹ A Lemon, *The Northcote Side of the River*, p48

The nursery expanded in 1869 to include a jam factory with fruit cultivated in the extensive gardens, supplying various fruit preserves, bottled fruits and the like.¹²



1855 The farm of Mr Perry on the Yarra. Painted by Eugene von Guerard
(Source: www.bonhams.com/auctions/21362/lot/17/)

The land boom of the 1880s saw prominent land speculators Charles Henry James and Percy Dobson acquire large sections of land along Heidelberg Road, and large housing estates were established in the Fairfield-Alphington area. Many of the estates were created from early farms lining the Yarra River such as Fulham Grange.

James and Dobson were responsible for housing estates such as Fulham Grange Estate, Fairfield Park and St James Park. 95 allotments in the Fulham Grange Estate were auctioned as early as March 1883 at a **'most successful sale'** held by Gemmell, Tuckett and Co.¹³

In April of that year, further allotments in the Fulham Grange Estate with frontages to the Yarra River were advertised and catered to affluent buyers.

'To Gentlemen Seeking Suitable Land for the Erection of Villa Residences, Speculators, Builders, and Others.
Fulham Grange Estate. Heidelberg Road. **Perry's Nurseries.**
... **All that valuable block of land, situated on the Heidelberg-road**, and known as the
Fulham Grange Estate,
being that portion of the estate having frontages to the
River Yarra'

'Subdivided into 75 full sized building allotments... being only about four miles from the City of Melbourne, commanding most charming and lovely views as far as the eye can reach.'

'**The whole of this land had been in a high state of cultivation for a great many years.** Sections of land sold at £100 per acre were planted with ornamental trees and shrubs as well as fruit trees of every description... **within a very short distance of the Clifton-hill omnibuses and equal distance from the Alphington and Fairfield park railway stations.'**

A further 47 allotments in the Fulham Grange Estate Extension No. 3 were auctioned in September 1884.¹⁴ 30 of those allotments were located along the newly named Grange Road and Fulham Road. In September 1885 **'valuable village sites'** fronting Heidelberg Road were advertised.¹⁵

¹² www.bonhams.com/auctions/21362/lot/17/ accessed 5 September 2019; **'Messrs. Perry Brothers' Jam Factory,'** *Cornwall Chronicle*, 15 May 1869, p2

¹³ Real Estate, *Herald*, 5 March 1883, p3. It is likely the 1883 auction included allotments with Heidelberg frontages, however due to poor image quality this could not be verified.

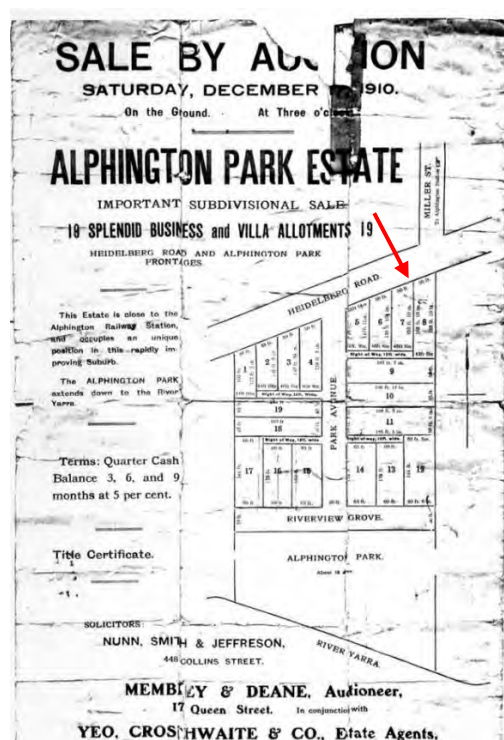
¹⁴ Land Sale at Fulham Grange, *Age*, 1 September 1884, p6

¹⁵ Sales by Auction, *Argus*, 2 May 1883, p6

The *Alphington East Residential Precinct* (HO362) captures this early phase of residential development and is described as ‘a residential enclave physically contained by the early transport route of Heidelberg Road and the natural barrier of the Yarra River that includes Victorian, Federation and Interwar houses, with garden allotments.’²⁰ A number of notable artists resided in the area during the early 20th century, such as William Frater (56 Lucerne Crescent - now demolished) and William McInnes (54 Lucerne Crescent).

Several institutional buildings were constructed at the west end of Heidelberg Road (Fairfield) during the late 19th century such as the Yarra Bend Asylum Lodge and the Infectious Diseases Hospital. The Grand View Hotel (HO36) at 429 Heidelberg Road was constructed in 1888 and by the turn of the century a modest strip of shops had been erected along Heidelberg Road, between the Merri Creek and Austin Street.²¹ This included several grocers, a laundry, dairy, news agent, bootmaker, fruiterer and wood merchants and saw mills.

In 1910 ‘19 splendid business and villa allotments’ in the Alphington Park Estate on the south side of Heidelberg Road were advertised.²² The subdivision centred around Park Avenue and was bound by Riverview Grove to the south. The subdivision consisted of eight business allotments, each with an approximately 50-foot frontage to Heidelberg Road.



Auction notice for Alphington Park Estate in Alphington dated 1910, showing eight rectangular allotments with frontage to Heidelberg Road.

(Source: State Library of Victoria)

By 1914 several buildings in the original Alphington Village had been demolished and new buildings erected.²³ Some buildings such as the former butcher shop remained although additional structures had been constructed to the rear. Small groups of timber houses had been constructed south of the street facing allotments while some lots remained vacant.

²⁰ Statement of Significance, Alphington East Precinct, VHD.

²¹ Sands & McDougall directory, 1900, p286

²² Auction Notice dated 1910, State Library of Victoria

²³ 1914 MMBW Detail Plan 1318, SLV



1914 MMBW Detail Plan 1318
Showing Alphington Village at the east end of Heidelberg Road - about Yarralea Street, Alphington
(Source: SLV)

Some sections along Heidelberg Road remained largely undeveloped as evident in the following MMBW dated 1914. Allotments directly to the west of the Alphington Village between St Elmo Road (now Latrobe Avenue) and Park Avenue were vacant while some development (four masonry public buildings) had occurred on the north side of Heidelberg Road.²⁴



1914 MMBW Detail Plan 1317
Showing the east end of Heidelberg Road - about Parkview Road, Alphington
(Source: SLV)

24

Trevor Westmore, MMBW Plans – Terms and abbreviations, September 2018

By 1919 the Fairfield-Alphington region had experienced a steady increase in the number of commercial and residential buildings. The north side of Heidelberg Road consisted of primarily residential buildings while the south side consisted of a combination of primarily small commercial premises, including grocers, cycle builders, a confectioner, and a bootmaker, with some residential buildings.²⁵

During the 1920s a number of businesses related to the motor vehicle industry were being established along Heidelberg Road, e.g. garages, engineers, motor body parts were being built.²⁶ In 1921, there was one motor garage, along Heidelberg Road, located between Parkview Road and Park Avenue.²⁷ By 1923 this had been renamed the G H Underwood Alphington Central Motor Garage.²⁸ By 1926 three motor garages were listed on the north side of Heidelberg Road and two were listed on the south side.²⁹

The following MMBW dated 1931, shows the development along the west end of Heidelberg Road, Fairfield between Panther Place and Grange Road. The south side of the road consisted of typically narrow, rectangular allotments with some larger, vacant lots. The west end by Panther Place was considerably developed and consisted of narrow commercial buildings while the east end by Grange Road was sparsely developed and consisted of residential buildings.



1931 Aerial, Heidelberg Road - west end of study area (between Panther Place and Grange Road)
(Source: Landata, 1931 Nov - Maldon Prison, Proj. No. 1931, Run 15, Frame 2741)

During the mid-1920s/early 1930s a small number of light commercial/manufacturing businesses were constructed along Heidelberg Road. In 1924 knitting manufacturer, F G Stirling was first listed on the south side of Heidelberg Road, close to Bond Street and in 1933 the Elite Knitting Co Pty Ltd was listed in a similar location.³⁰ During the late 1930s there was an increased demand for wool textile manufacturing in a bid **to aid in Australia's war effort**. In 1935 the Alpha Spinning Mills are listed at 714-716 Heidelberg Road and in 1938 A Cooper, knitting manufacturer is listed at no. 760.

The following aerials dated 1945, show a number of light commercial buildings (a combination of shops and warehouses) had been constructed along parts of Heidelberg Road. Some smaller parcels had been consolidated into larger sites and warehouse buildings constructed.

²⁵ Sands & McDougall directory, 1919, p104
²⁶ Sands & McDougall directory, 1930, p312
²⁷ Sands & McDougall directory, 1921, p92
²⁸ Sands & McDougall directory, 1923, p97
²⁹ Sands & McDougall directory, 1926, various
³⁰ Sands & McDougall directory, 1924, p375, 1933, p313



1945 Aerial, Heidelberg Road, Fairfield - west end of study area (between Panther Place and Austin Street)
(Source: Landata, Melbourne and Metropolitan Area Project, No. 5, Run 27, Frame 59,529)



1945 Aerial, Heidelberg Road, Alphington – east end of study area (between Parkview Road and Lucerne Crescent)
The northern half of Alphington East Precinct (HO362) is evident
(Source: Landata, Melbourne and Metropolitan Area Project, No. 5, Run 27, Frame 59,523)

Some allotments in the former Alphington Village had also been subdivided and narrow buildings constructed while some adjacent sites remained vacant.

By 1956, the concentration of commercial/light industrial sites to the far east and west ends of Heidelberg Road, similar to the current circumstance, had been established. A number of previously vacant sites had been developed with small warehouses, etc.



1956 Aerial, Heidelberg Road – study area
(Source: Landata, Melbourne Outer Suburbs Project, No. 250, Run 13, Frame 146)

A 1978 aerial highlights the large number of light commercial buildings that had been constructed along the south side of Heidelberg Road by that time, including at the Paper Mills site.



1978 Aerial, Heidelberg Road – study area
(Source: Landata, Western Port Foreshores, Project No. 1716, Run 3, Frame 105)

4 STAGE ONE – PRELIMINARY REVIEW AND BUILT FORM GUIDELINES

4.1 Components

The stage one report related to the following:

- HO71 – review of heritage value and citation,
- Preliminary assessment of places of potential heritage value,
- 2 Killop Street, Alphington.
- Built form review

4.2 HO71

It was confirmed that the basalt building at 756-758 Heidelberg Road (HO71) more than adequately meets threshold for local significance. An updated citation was prepared for HO71 that meets the contemporary standard.

4.3 Places of Potential Heritage Value

Datasheets were prepared for five individual places which were considered to be of potential heritage significance. Details of these five sites are outlined in the following table. Of these, three were considered to have strong potential to meet the threshold for local heritage significance (those at nos 358, 730-734, and 760-764), with the other two (nos 460, 774) less so.

On further discussion with Council, only three have been recommended for further work in Stage 2 and were reviewed in regards to built form analysis and recommended controls.

Address – No. (Heidelberg Rd)	Name	Date/Period	Details
358	House (<i>Valeema</i>) & Tyre Outlet	Late 1950s/Postwar	House – Modernist influence Unusual combination of house and adjoining service centre
460	Duncan Rubber Co. Showroom & Warehouse	1956/Postwar	Sawtooth roof forms Intact facades
730-734	Group of 3 shops	1933/Interwar	Single storey buildings with intact shopfronts
760-764	A. Cooper Knitting Manufacturer	Mid-1930s/Interwar	Single storey building with intact parapet and most original openings
774	Former Ampol Service station	By 1965/Late 20 th century	Stages of construction not certain Original frame (sign) survives

Other Sites Considered

Seven other buildings in the study area which were noted for their potential heritage value and were assessed as not having sufficient potential to reach the threshold for local heritage significance at this time: nos 276, 388, 402, 728, 750-754, and 782 Heidelberg Road.

Of this group, the earlier buildings had been altered to varying degrees, whereas the two late 20th century buildings were largely intact but difficult to assess in regards to other similar building stock in the municipality as there are few, if any, from this time that have been assessed as having heritage value.

4.4 2 Killop Street, Alphington

A review of 2 Killop Street, located in the north end of the Alphington East Precinct Heritage Overlay (HO362), was undertaken.

2 Killop Street is located in one of the small peripheral sections of HO362 that also includes 59 Yarralea Street. The latter is a weatherboard-clad bungalow with a distinctive oblong opening to the porch. The late 20th century house at 2 Killop Street was determined to be an anomaly at the edge of the Alphington East Precinct largely characterised by Victorian, Edwardian and Interwar houses with pitched, gabled or hipped roofs. It varies greatly to the graded building stock in the precinct and it was recommended to be removed from the precinct on this basis.

4.5 Built Form Review

Built form guidelines have been developed for the following five sites, two of which are existing heritage overlays (HO71 + HO421) and three proposed heritage places.

- Porta Factory, 224 Heidelberg Road, Fairfield (HO421).
- House (*Valeema*) & Service Centre, 358 Heidelberg Road, Fairfield.
- Group of 3 Shops, 730-734 Heidelberg Road, Alphington.
- Former Butcher Shop, 756-758 Heidelberg Road, Alphington (HO71).
- A. Cooper Knitting Factory (Former), 760-764 Heidelberg Road, Alphington.

5 STAGE TWO – CITATIONS FOR PROPOSED HERITAGE OVERLAYS

5.1 Recommendations

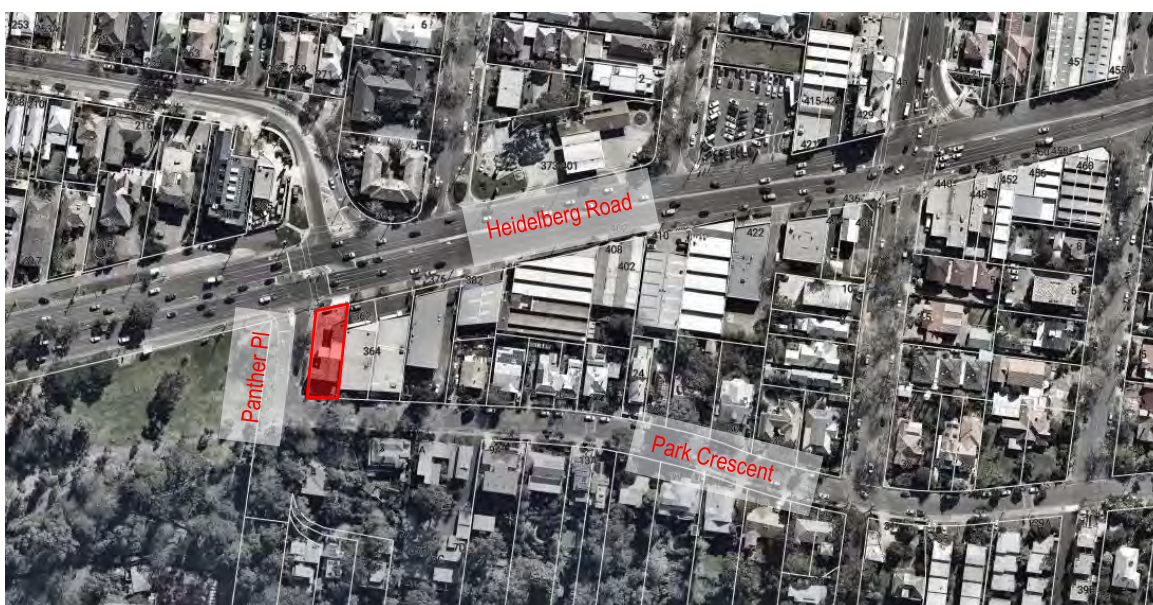
Further research and analysis undertaken during 2019 has confirmed that the three proposed heritage overlays warranted recommendation for inclusion in the Schedule to the Heritage Overlay in the Yarra Planning Scheme. The proposed citations for these places are included in the Appendix A.

Address – No. (Heidelberg Rd)	Name	Date/Period	Details
358	House (<i>Valeema</i>) & Service Centre	House – 1955 (east wing), c.1980 (west wing) Service centre – 1955 Postwar	House – Modernist influence Unusual combination of house and adjoining service centre
730-734	Group of 3 shops	c.1922 Interwar	Single storey buildings with intact shopfronts
760-764	A. Cooper Knitting Factory (Former)	1922 Interwar	Single storey buiding with intact parapet and mostly original openings

5.2 Locations

The three proposed heritage overlays are indicated in the maps below-

- 358 Heidelberg Road – House (*Valeema*) & Service Centre



358 Heidelberg Road is located to the west end of the subject area at the intersection of Heidelberg Road and Panther Place
 (Source: Nearmap 2019)

- 730-734 Heidelberg Road – Group of 3 shops
- 760 Heidelberg Road – A. Cooper Knitting Factory (Former)



730-734 Heidelberg Road and 760 Heidelberg Road (at the intersection of Yarralea Street) are located to the east end of the subject area.
(Source: Nearmap 2019)

APPENDIX – Citations

	Address – No. (Heidelberg Rd)	Name
1	358	House (<i>Valeema</i>) & Service Centre
2	730-734	Group of 3 shops
3	760-764	A. Cooper Knitting Factory (Former)

HOUSE (*VALEEMA*) & SERVICE CENTRE

Address	358 Heidelberg Road, Fairfield
Significance	Local
Construction Dates	House (<i>Valeema</i>) 1955 (east wing) and c.1980 (west wing); Service centre 1955
Period	Postwar
Date Inspected	Early 2019



Statement of Significance

What is Significant?

The two storey house (*Valeema*) constructed in 1955 (main part/east wing) and circa 1980 (addition/west wing), and the single storey service centre constructed also in 1955, at 358 Heidelberg Road, Fairfield.

Significant aspects of the two storey house include the form of the building, gable roofs (including original glazed tiles), brick walls, steel-framed windows and minimal window to wall ratio. The '**Valeema**' name plate and light fittings on the entry porch are contributory elements.

As the brick walls of the building have a bagged/painted finish, it is appropriate to have paint controls so that future colour schemes and finishes are complementary.

Significant aspects of the single storey service centre building include the original parapet, brick walls including the small projection at the north-east corner (**with the original street number '358'**), original openings, including concrete lintel to the north

elevation, glazed timber door and steel-framed windows on the west elevation, and roof-mounted 'Bridgestone' and 'tires' signage. The bowser platforms to the front of the site are contributory elements. The lightweight canopy to the west side is not significant.

How is it Significant?

The House (*Valeema*) and Service Centre at 358 Heidelberg Road, Fairfield are of local historical and aesthetic significance to the City of Yarra.

Why is it Significant?

The House (*Valeema*) and Service Centre at 358 Heidelberg Road, Fairfield are of historical significance to the City of Yarra as a long-standing combined commercial and residential site from the late 19th century onwards, that represents a key phase of development during the Post-WWII period. The combined function of house and tyre service centre was established on the site in 1953, operating from two Victorian period buildings on the northern part of the site. During 1955, the whole site was extensively redeveloped whilst retaining the dual function of house and service centre though in two distinct buildings. (Criterion A)

The House (*Valeema*) and Service Centre are aesthetically significant as an unusual and intact combination of house and service centre that are expressed as two distinct entities and designed in the Postwar period. The design of the house and service centre reflect the Modernist aesthetic and consist of bold yet restrained forms that are enhanced by the prominent corner site. The low-pitched gable roof of the house is indicative of the Melbourne Regional Modernist style, which is uncommon in the municipality. (Criterion E)

Description

The site at 358 Heidelberg Road has three street frontages – Heidelberg Road (north), Panther Place (west) and Park Crescent (south). The house is located at the southern end of the site and the service centre at the north. A high brick fence, painted and with a timber gate, separates the two parts of the site. The prominent corner site overlooks Yarra Bend Park.

The two storey L-shaped house was designed in the Postwar period. It is a substantial house that was built in two stages and consists of the likely architect designed main rectangular, eastern section constructed in 1955, and the subsequent west wing and entry porch constructed circa 1980. The two parts are intact and well integrated as they have been designed in a similar mode but the junction between the two parts is evident to the south elevation.

The entrance to the house fronts Park Crescent, though the building is most visible from Panther Place to the west. A high timber fence has been erected along the south boundary and part way along the west boundary of the site. The south (front) yard and west (side) yard are grassed.

The main part of the house has a bold rectangular form and the west wing is a smaller version of it, both with prominent gabled ends that address the street. The low-pitched gable roofs of the extant house are clad in glazed tiles with minimal eaves overhang. The brick walls have been painted a neutral white colour with a bagged finish, enhancing the bold yet restrained aesthetic of the building.



House - façade, south elevation



House – south elevation, window openings and portico. The junction between the original eastern section and later west wing is evident.

The strong volumetric expression of the building is evident from Park Crescent and Panther Place. The south facing elevation is articulated by a series of rectangular openings (original as well as later additions) that maintain a minimal window to wall ratio pattern across the facades. The windows are typically steel-framed casement windows with a horizontal mullion to the upper section and brick sills. At ground floor is a brick entry porch addition constructed circa 1980 that mirrors the low-pitched slope of the main gable and the bagged finish of the brick walls. The porch features a steel entry grille and light fittings to either side. Above the door is a name plate **that reads 'Valeema.'**

The west wing presents a blank wall to the street boundary, which heightens the bold form of the building. The addition has been sympathetically designed and maintains the aesthetic evident in the original part of the house.

The upper section of the north facing gable end is visible above the garage from Heidelberg Road and Panther Place. There are vents to the roof space, visible in the upper part of the wall. At each end of the original wing is a narrow slit with a wider vent to the west wing.

Awning blinds have been added to various windows at the north-west corner of the wings.



House – west elevation



Service centre and house beyond – north-west corner from Heidelberg Road. Note 'Bridgestone' signage mounted to parapet.

The single storey service centre building at the front of the site at the intersection of Heidelberg Road and Panther Place was constructed at the end of the Postwar period and its cuboid form also reflects the Modernist aesthetic.

The building has an elongated footprint that is angled to Heidelberg Road but more so than that of the street itself. The front part of the site is paved and there is vehicular access from both roads. There are two concrete platforms, indicating the location of former petrol bowsers, to the front of the site. A brick wall defines the east boundary in front of the garage.

The brick building and parapet have been painted however the original openings and brick detailing is evident beneath. The north elevation has a large garage opening with (rendered) concrete lintel. The upper section of the brick façade features brick courses in a header configuration and a horizontal, shallow recessed panel on the north façade. There are original raised street numbers to the pier at the north-west corner and the small projection at the upper end of the of the service centre building in the north-east corner.

On the west façade of the service centre is an original glazed, timber-framed door and two large multi-paned steel windows. Awning blinds have been added above the openings and a lightweight canopy has been added to the garage opening at the south end.

A large 'Bridgestone' tyres sign has been mounted on the roof of the west side of the building and 'tires' to the east side, the latter being the preferred spelling in the North America. Their date has not been confirmed but the signage may date to the early 1960s as imported Bridgestone tyres were available in Australia from 1961.¹

¹ Age, 20 December 1961, p26. Bridgestone is a leading Japanese tyre manufacturer but an Australian division existed from 1980 to 2007 (<https://en.wikipedia.org/wiki/Bridgestone>, accessed 04.09.19)

Three flagpoles have been mounted on the building and are located on the front section of the roof, the north elevation and west elevation.



Service centre – north-east corner elevation
Note 'tires' signage mounted to parapet and small projection with street number (highlighted)



Service centre – west elevation

The house and service centre are indicative of the Modernist aesthetic which evolved from the late-1920s in Europe. The style was adopted internationally, in Australia primarily after WWII with several regional versions of Modernism evolving in various parts of the country.² Elements of the Modernist style include the use of geometric and bold forms and restrained facades with minimal, if any, ornamentation. Roofs concealed by parapets, contrasting angular forms, an emphasis on the horizontal, and the use of large steel-framed windows are typical of the Modernist style.

The design of the house is indicative of what has been identified as the Melbourne Regional version of Modernism which characteristically integrated a low-pitched roof – a broad gable or skillion - a familiar traditional roof form rather than the flat roofs of the International Modernist style.

History

The subject site formed part of Crown portion 113, Parish of Jika Jika, which was comprised of 92 acres purchased by A Walker in 1840.³ It was one of several similarly narrow allotments with frontages to the nearby creeks and the Yarra River.⁴ The rich soil, topography of the land, and close proximity to the Yarra River was considered desirable.⁵ Heidelberg Road was located to the south of the allotments and was developed along the line of an earlier track leading to Heidelberg. The road served as an early transport route for those travelling to and from Heidelberg and includes sections of what is now Fairfield and Alphington.

The land boom of the 1880s saw prominent land speculators Charles Henry James and Percy Dobson acquire large sections of land along Heidelberg Road. In 1883 a large section of Crown portion 113 that included the subject site, was transferred to James and Dobson and the land to the north of Park Crescent was subdivided.⁶ The allotments facing Heidelberg Road were of varying widths and depths. The subject site was located in the north-west corner of the subdivision.

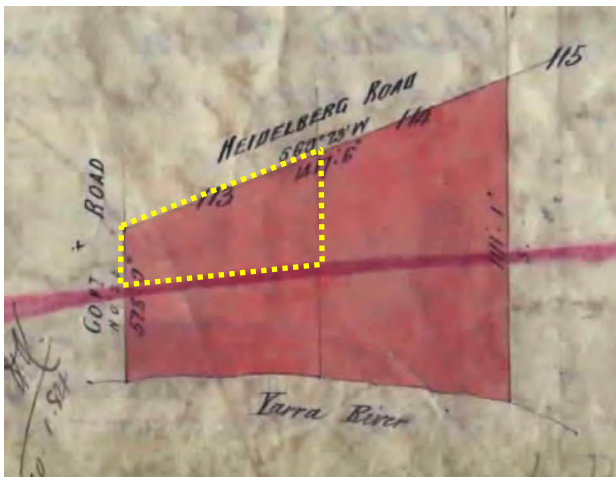
² Apperly, Richard & Reynolds, Peter L & Irving, Robert, 1926- & Mitchell, Solomon (1989). A pictorial guide to identifying Australian architecture : styles and terms from 1788 to the present (3rd ed). Angus & Robertson, Sydney p218

³ Landata, Parish of Jika Jika J16(5)

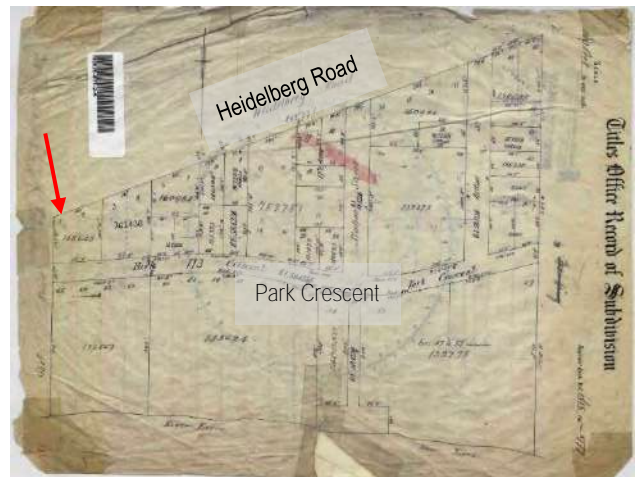
⁴ A Lemon, *The Northcote Side of the River*, p7

⁵ City of Darebin, www.darebin.vic.au

⁶ Certificate of Title, vol.1515/folio 977

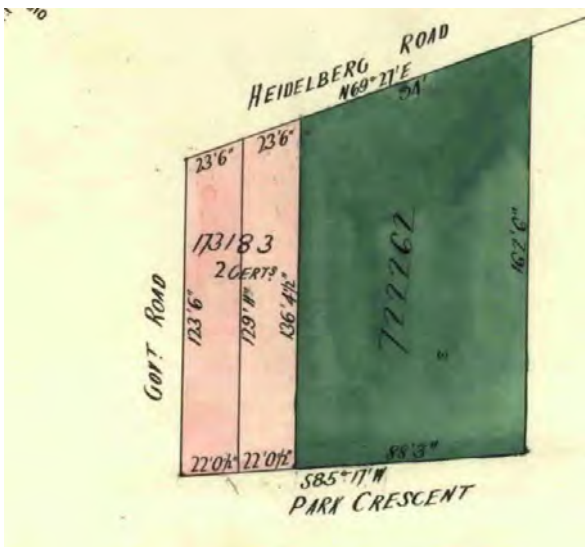


Plan of part of Crown portion 113 and 114, Park Crescent is roughly indicated. The approximate area of the 1883 subdivision is shown dashed.
(Source: Certificate of Title, vol.1515/folio 977)



Plan of subdivision in 1883. The location of the subject site is indicated by the red arrow.
(Source: Certificate of Title, vol.1515/folio 977)

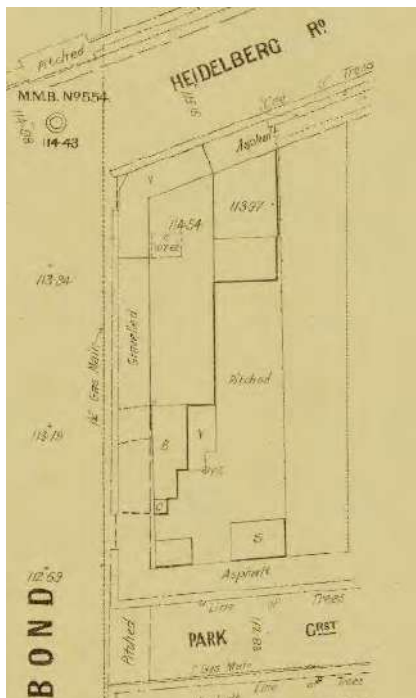
In 1885 land containing the subject site was transferred to salesman, Frederick William Oehr.⁷ Oehr further subdivided the land to form two narrow allotments and in 1886 the western allotment was transferred to grocer, John Dickson.⁸ At that stage the south side of Heidelberg Road remained sparsely developed with only five commercial premises between Bond Street (what is now Panther Place) and Austin Street.⁹ The western allotment was occupied by several grocers during the late 19th and early 20th century including Freeman Bros. (1897-1900), Charles Ponsford (1904 - grocery and hay and corn store), George H Osborne (1912), Edward G Ball (1913) and Walter L Newnham & Co. (1914).



In 1885 two narrow allotments with northern frontage to Heidelberg Road and southern frontage to Park Crescent were formed.
(Certificate of Title, vol.1767/folio 376)

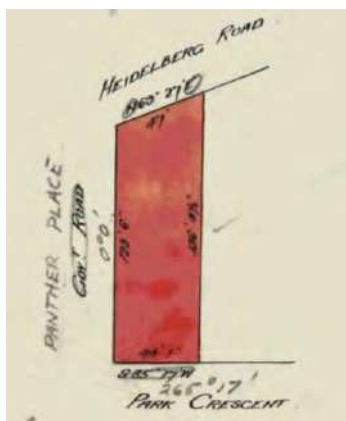
⁷ Certificate of Title, vol.1767/folio 376
⁸ Certificate of Title, vol.1840/folio 367801
⁹ Sands & McDougall directory, 1897, p221

In 1909 buildings with a narrow footprint and a north-west corner verandah are evident on the subject site.¹⁰



In 1909 there were narrow buildings on the subject site. (MMBW Detail Plan 1270, SLV)

In 1919 the two narrow allotments were consolidated to form the extant subject site, which had a 47 foot frontage to Heidelberg Road, 123.5 foot frontage to Panther Place and about a 44 foot frontage to Park Crescent. That year the site was transferred to grocers, Walter Lachlan Newnham and Charles Herbert Fullard, who continued to occupy the site until at least 1923.¹¹ At that stage there were 15 commercial premises on the south side of the road between Panther Place and Arthur Street. This included a blacksmith, saddler, cycle builder and bootshop.¹² During the 1920s the subject site was primarily occupied by grocers and produce dealers.¹³



In 1919 the two narrow allotments were consolidated to form the extant subject site. (Certificate of Title, vol.4181/folio 836140)

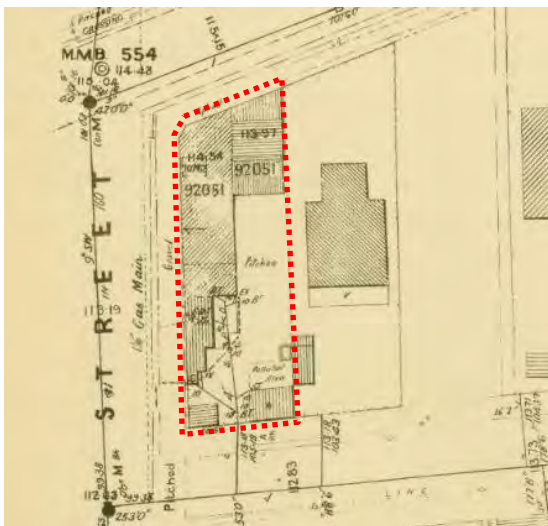
¹⁰ MMBW Detail Plan 1270, dated 1909
¹¹ Certificate of Title, vol.4181/folio 836140
¹² Sands & McDougall directory, 1919, p104, 326. The subject site is listed as 121-123 Heidelberg Road. Newnham & Son are listed as coachbuilders and occupy 221-223 Heidelberg Road at the intersection of Arthur Street.
¹³ Sands & McDougall directory, 1925, p390

From the 1920s, several motor vehicle related businesses such as garages, engineers and motor body parts, were being established along Heidelberg Road, being a major thoroughfare.¹⁴ By 1931, the allotments on the south side of Heidelberg Road consisted of light commercial buildings with some vacant lots and Panther Place had been renamed Bond Street. The subject site was occupied by H Stewart, grocer, and thereafter occupied by various produce merchants throughout the 1930s and early 1940s.¹⁵



Aerial photograph dated 1931, with subject site indicated
(Source: Landata, Maldon Prison Proj. No. 1931, Run 15, Frame 2741)

The MMBW plan dated 1935 shows that the front building was constructed of brick or stone with a hipped roof (and possible chimney) and the structures to the east side and south (rear) were constructed of timber.¹⁶



MMBW Detail Plan No. 1314, dated 1935
(Source: State Library of Victoria)

¹⁴ Sands & McDougall directory, 1930, p312

¹⁵ Sands & McDougall directory, 1931, p313

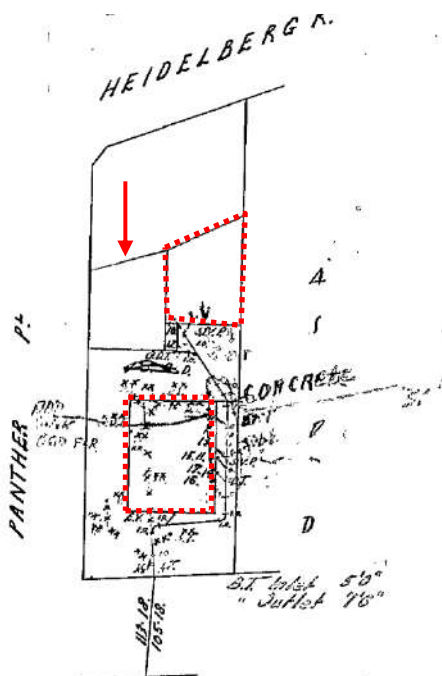
¹⁶ MMBW Detail Plan No. 1314, dated 1935

In 1945 the function of the site shifted from a commercial to residential purpose when it was occupied solely by Mrs E F Chappel, and then Charles H Chappell in 1950.¹⁷

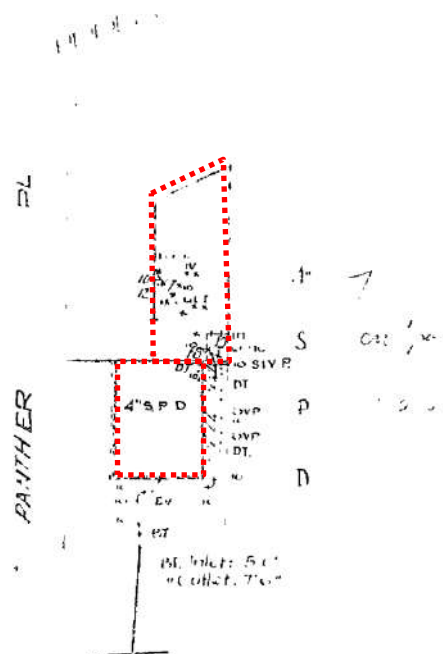
The combination of house and service centre was first established on the site in 1953 when it was purchased by George Bertram Stringer and Harry John King, who had recently become owners of the Lincoln Tyre Service.¹⁸ By 1955 the two independent functions were re-instated at the site when the front part was operating as the Lincoln Tyre Service and the rear house was being occupied by Mrs E Z Chappell.¹⁹

In 1955 the site was however completely redeveloped though henceforth continued to serve the dual function of a house and service centre. In June and July of that year, the Lincoln Tyre Service advertised for a few staff members.²⁰ By August 1955 the earlier structures had been demolished and replaced by the main part of the house and the front section of the service centre, as evident in the Property Sewerage Plan.²¹ Initially there was probably a canopy to the west side of the service centre building.²²

In 1959 the subject site was purchased by tyre dealer, Retford Alexander Cutts who occupied the residence and further developed the site.²³ The site operated as the Lincoln Tyre Service until 1960 at which stage it was listed as Fairfield Tyre Service.²⁴ By 1966 the service centre had been extended south to coincide with the existing footprint.²⁵ The canopy/structure to the west side had been removed and the extant courtyard wall between the service centre and house had been erected.



Property Sewerage Plan of subject site dated 1955. The main part of the house and the front section of the extant service centre are indicated and dashed red. To the west side of the service centre was probably a lightweight structure or canopy. (Source: Yarra Valley Water, Plan No. 92051-1)



Property Sewerage Plan of subject site dated 1966. The main part of the house and the extant service centre are indicated and dashed red. (Source: Yarra Valley Water, Plan No. 92051-2)

¹⁷ Sands & McDougall directories, various

¹⁸ Certificate of Title, vol.7842/folio 155

¹⁹ Sands & McDougall directory, 1955, p359

²⁰ Argus, 1955, various

²¹ Yarra Valley Water, Property Sewage Plan dated 1955, Plan No. 92051-1

²² The buildings are likely to have been designed by an architect however this has not been confirmed, in part because the building file for the subject site could not be located.

²³ Certificate of Title, vol.8218/folio 609

²⁴ Sands & McDougall directory, 1960, p376

²⁵ Yarra Valley Water, Property Sewage Plan dated 1966, Plan No. 92051-2

The main part of the house and service centre (as it stands today) are evident in 1969.²⁶ The south face of the house is blank with the exception of a single window at first floor level. The building has a light and monochromatic colour palette. The front of the site is paved and includes the extant bowser platforms. Cuts occupied the subject site until at least 1974.²⁷



Aerial photograph dated 1969, showing the main rectangular form of the extant house and extant service centre. The extant bowser platforms are evident to the front of the site.

(Source: Landata, 1969 Eastern Freeway Project, Proj. No.754, Run 4, Frame 143)

The existing lightweight canopy on the west side of the service centre had been constructed by 1979 while the extant west wing addition and entry porch of the house were constructed after this period.²⁸ The western face of the addition was designed in an aesthetic similar to the original house and remained blank to the street. Windows were likely added to the south face of the main part of the house at this stage.

The service centre currently operates as the Fairfield Alignment & Tyre Service.

Lincoln Tyre Service

Lincoln Tyre Service was established as a Lincoln Retreads P/L in 1933 with £5000 capital and £1 shares being offered. The original directors were John Herbert Rudge and Ian Wischer.²⁹ Their operations relocated over the years, commencing at 430 Riversdale Road Hawthorn, and later moving to 490 Toorak Road, Burwood.³⁰ By 1952, Stringer and King owned the company and had changed the trading name to Lincoln Tyre Service.³¹ The following advertisement, issued soon after the renaming of the company, indicates that they also supplied other garages.

²⁶ 1969 - Eastern Freeway Project, Proj. No.754, Run 4, Frame 143

²⁷ Sands & McDougall directory, 1974, p382

²⁸ Aerial photograph dated 1979, Landata, Heytesbury North Project, Proj. No. 793, Run 3, Frame 157 – image not reproduced due to poor quality

²⁹ 'New Companies', *Herald*, 20 July 1933, p38

³⁰ Sands & McDougall directory, 1950, p2756

³¹ *Age*, 31 July 1952, p9



(Source: *Mountain District Free Press*, 26 November 1953, p4)

Thematic Context/Comparative Analysis

- City of Yarra, Review of Heritage Overlay Areas 2007, Appendix 8 (revised May 2017)
- City of Yarra, Review of Heritage Overlay Areas 2007, Appendix 7: Individually significant places not from the main development era (revised May 2008)
- City of Yarra Heritage Review (Allom Lovell and Associates, 1998)
- City of Northcote Urban Conservation Study (Graeme Butler, revised February 1983)

Known comparable places in the City of Yarra

There are no ready comparisons for this combination across the municipality. Postwar buildings with individual overlays built after WWII are primarily ecclesiastical buildings and do not make for ready comparison.

There are no examples of Modernist houses or garages with individual overlays built after WWII as many such buildings are located within precincts whose significance primarily relates to the late 19th century (Victorian), and possibly early 20th century (Federation and/or Interwar) phases of development and so have tended to be graded non-contributory.

- 28-30 Johnston Street, Collingwood (part of HO324, Johnston Street Precinct). Not significant. Constructed in the 1960s-70s, the basic single storey brick building has a central, garage opening.
- 2 St Georges Road, Fitzroy North (part of HO327, North Fitzroy Precinct). Not contributory. Constructed between 1950-1965 the single storey brick motor garage has a rectangular form with a chamfered entrance and large garage openings. The garage has a parapet roof and large over painted, multi-paned steel windows.
- 310 St Georges Road, Fitzroy North (part of HO327, North Fitzroy Precinct). Not contributory. Group of four interlocking, two-storeyed brick units. Each unit has a rectangular form and a hip roof and the north and south walls of each unit remain blank. While the function of the building differs from the subject building, a similar Modernist aesthetic is evident.
- 50 Westbank Terrace, Burnley (part of HO331 Racecourse Precinct, Richmond). Not contributory. Constructed between 1950-1960 the single storey '**neighbourhood house**' consists of **two gable roofed structures** with a blank north wall and minimal openings. While the function of the building differs from the subject building, a similar Modernist aesthetic has been utilised.

Condition

Good

Integrity

Mostly intact

Previous Assessment

N/A

Heritage Overlay Schedule Controls

External Paint Controls	Yes
Internal Alteration Controls	No
Tree Controls	No
Outbuildings and/or Fences	No

Extent of Heritage Overlay

The proposed extent of the heritage overlay would be the parcel of land associated with 358 Heidelberg Road, Fairfield.



Recommended extent of heritage overlay
(Source: Nearmap, August 2019)

GROUP OF 3 SHOPS

Address	730-734 (including 730A) Heidelberg Road, Alphington
Significance	Local
Construction Dates	circa 1922
Period	Interwar
Date Inspected	Early 2019



No. 730



Side access no. 730A (right), no. 732 (centre) and no. 734 (left)

Statement of Significance

What is Significant?

The group of three Interwar period shops at 730-734 Heidelberg Road, Alphington, constructed circa 1922.

Significant aspects of the shops include the brick walls and parapets (now painted), hipped roof forms (largely concealed), street canopies including original soffit linings, the configuration of the shopfronts, shopfront joinery and finishes, tiled mouldings to the west end of no. 732, the recessed entrance of no. 730 including floor tiles, mouldings, pressed metal ceiling and toplights with textured glass. In addition, the original side access to no. 730 (now 730A) as it extends to the depth of the front hipped roof of no. 732, where the intact (unpainted) return walls remain visible.

How is it Significant?

The group of three shops at 730-734 Heidelberg Road, Alphington are of local historical and aesthetic significance to the City of Yarra.

Why is it Significant?

The group of three shops at 730-734 Heidelberg Road, Alphington are of historical significance to the City of Yarra as one of the few remnant intact buildings that denote the Interwar period phase of development in this part of Heidelberg Road area (Alphington Village) during the early 1920s, when building activity increased, including much redevelopment, and the area's commercial function was consolidated. Whilst no. 734 was not purpose built as a post office, it has functioned as such for about half a century. (Criterion A)

The group of three shops are aesthetically significant as an intact group of single-storey, brick Interwar period commercial buildings. Whilst modest in scale, they retain their original parapets and unusually their original shopfronts, two of which were manufactured by Duff (nos 732-734), with the other (no. 730) being notable for intact canopy, the copper finish to its framing and green tiles to the stallboard. (Criterion E)

Description

The group of three shops at nos 730-734 Heidelberg Road were constructed during the Interwar period. The shops are located in a commercial strip on the south side of Heidelberg Road between Park Avenue and Yarralea Street, and are each situated on a narrow allotment. No. 730 is separated from nos 732-734 by a narrow side access (part of no. 730A). The shops are single-storied and feature original parapets and shopfronts.

The subject shops are typical of the Interwar period and feature relatively plain parapets with little ornamentation and street canopies. The brick shop at no. 730 has a symmetrical façade and is the most elaborate of the three shops. The shop has a stepped brick parapet (overpainted) that conceals a metal clad hipped roof and features a square pediment and capped brick piers at either end. The east parapet return has been retained and the original red brick wall is evident. The combination of face brick to the parapet section and render to the lower section was common during the Interwar period.

The shopfront is highly intact and retains the original configuration and finishes which are indicative of the Interwar period. This consists of the recessed entry, floor tiles with pressed metal ceiling above, dark green wall tiles and mouldings to the stallboard, copper-finish shopfronts, and framing with highlights (often overpainted). The toplight windows have textured/patterned glass which is also indicative of the Interwar period. The canopy is clad in corrugated metal sheeting and has a plaster board lined soffit with thin metal battens **and a small 'Post Office' sign mounted on the lower edge of the fascia.**



Nos 730-734 Heidelberg Road



730 Heidelberg Road – parapet and shopfront



No. 730 Heidelberg Road – typical Interwar shopfront configuration



No. 730 Heidelberg Road – recessed entry including original tiles and pressed metal ceiling

Nos 732-734 are a pair of brick shops with a plain brick parapet (overpainted) that conceals two hipped roofs clad with corrugated sheeting. The west parapet return of no. 732 has been retained and the original red brick wall is evident. The east (side) brick wall of no. 734 has been painted.

The street facades are identical and for the most part retain original shopfronts. The badge of the manufacturer (Duff) survives on each shopfront however the framing has been painted on no. 732. Tiled mouldings to the west end of no. 732 also survive. The configuration of the shopfronts and detailing of the parapet is typical of the Interwar period and despite having been painted, are intact. The canopies have been slightly modified however the original metal corrugated lined soffit no. 732 remains.



Nos 732-734 Heidelberg Road - shopfronts



No. 734 Heidelberg Road – original badge of manufacturer (Duff)

The original side access (no.730A) extends the depth of the hipped roof of no. 732. This separation provides visibility of the intact side parapets of nos. 730 and 732. The front brick wall and door have more recently been added to the laneway and are not significant. To the rear of no.730A is a narrow outbuilding and a small shed (not inspected, evident from aerial). To the rear of no. 732 is a narrow outbuilding which appears in a 1922 sewerage plan of the site and is possibly original. To the rear of this is a detached timber shed which appears in a 1945 aerial.

History

The subject site formed part of Crown Portion 119, Parish of Jika Jika, which comprised of 95 acres purchased by G Howitt in 1840.¹ It was one of several similarly narrow allotments with frontages to the nearby creeks and the Yarra River.² The rich soil, topography of the land, and close proximity to the Yarra River was considered desirable.³ Heidelberg Road was located to the south of the allotments and was developed along the line of an earlier track leading to Heidelberg. The road served as an early transport route for those travelling to and from Heidelberg and included sections of what is now Fairfield and Alphington.

In 1852 Alphington Village was established by William Montagu Manning (Solicitor General of Sydney) serving as a resting place for travellers. The village, located east of the subject site, included the extant former butcher shop at no. 760 constructed circa 1860.

The land boom of the 1880s saw prominent land speculators acquire large sections of land along Heidelberg Road. In 1885 land containing the subject sites was purchased by Albert Miller.⁴ Land to the south side of Heidelberg Road was subsequently divided into narrow allotments extending to the Yarra River.⁵ In 1896 land containing the subject site was transferred to Edith Maud Bancroft.⁶

1 Landata, Parish of Jika Jika J16(5)
 2 A Lemon, *The Northcote Side of the River*, p7
 3 City of Darebin, www.darebin.vic.au
 4 Certificate of Title, vol.1424/folio 284790
 5 Record of Subdivision vol.1424/folio 284790
 6 Certificate of Title, vol 2608/folio 521464



Plan of subdivision in 1885. Land including the subject site was subdivided into narrow allotments
(Source: Certificate of Title, vol.1424/folio 284790)

In 1910 the section of land between Riverview Grove and Heidelberg Road was acquired by George Robert Bennet, James Eldridge Rowe and George Washington Whitcher, and subdivided to form the 'Alphington Park Estate.'⁷

The Estate was subdivided into '19 splendid business and villa allotments.'⁸ The allotments were roughly rectangular and included eight business allotments, each with an approximately 50-foot frontage to Heidelberg Road. Part of the subject site (no. 730) formed part of allotment 7, while the remainder of the site (nos 732 and 734) comprised allotment 8, which was slightly longer.

⁷ Certificate of Title, vol.3475/folio 694831

⁸ Auction notice for Alphington Park Estate in Alphington dated 1910



Auction notice for Alphington Park Estate in Alphington dated 1910, showing eight rectangular allotments with frontage to Heidelberg Road. The approximate location of subject sites 730-734 Heidelberg Road is indicated. Alphington Village is located directly east of the estate. (Source: State Library of Victoria)

The following MMBW plan dated 1914 shows the buildings that had been constructed nearby in Alphington Village to the east of the subject site and prior to its development. Early houses and buildings were either timber or masonry (brick or stone), often with smaller outbuildings to the rear. Amongst the earliest buildings in Alphington were a general store with post office, butcher, Wesleyan Chapel, a bakery, and at least two hotels.⁹ A police station was located adjacent to the subject site. Residential development had occurred to the south of Alphington Village while a large majority of the street allotments along Heidelberg Road (including the subject sites) remained vacant.¹⁰

⁹ A Lemon, *The Northcote Side of the River*, p48

¹⁰ 1914 MMBW Detail Plan 1317

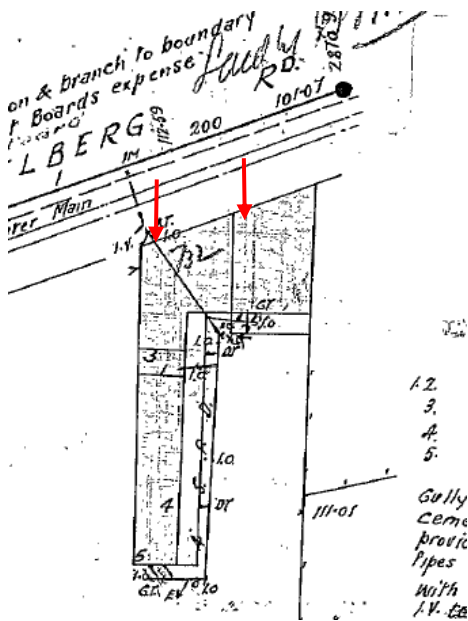


1914 MMBW Detail Plan 1318

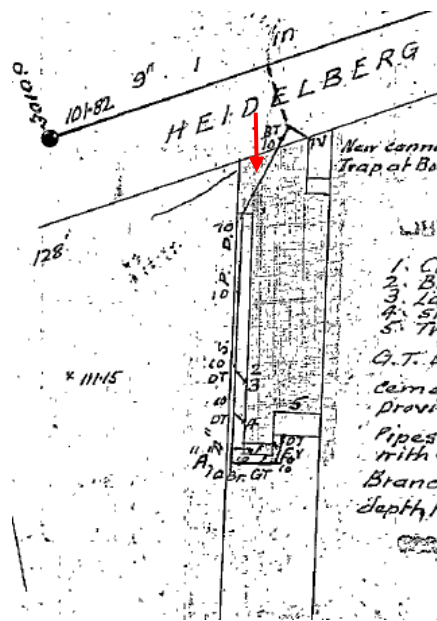
Showing the Alphington Village at the east end of Heidelberg Road - about Yarralea Street, Alphington. The approximate boundaries of the subject site is indicated and the sites are vacant. The police station is indicated by an arrow. (Source: SLV)

In 1916 allotment 8 (nos 732 and 734) was acquired by Thomas Trevena, tailor, which included the right of carriage way to the south of the site.¹¹ Nos 732 and 734 had been constructed by 1923.¹² Both buildings were roughly square in footprint with a chamfered corner however no. 732 included a narrow section that extended to the rear of the site.

The subject building at no. 730 had been constructed by 1924 and had a long narrow rectangular footprint with a stepped section, providing side access.¹³ All three building footprints are similar to the extant footprints on the site.



Plan of nos 732 & 734 dated 1923. The building footprint is almost identical to the extant buildings (Source: Yarra Valley Water, Plan No. 131291-2)



Plan of no. 730 dated 1924. The building footprint is almost identical to the extant buildings (Source: Yarra Valley Water, Plan No. 136627-6)

¹¹ Certificate of Title, vol.3966/folio 793102
¹² Yarra Valley Water, Plan No. 131291-2 dated 1923
¹³ Yarra Valley Water, Plan No. 136627-6 dated 1924

In 1924 the subject shops were occupied by various commercial businesses. Bootmaker, E J March is listed as occupying no. 730 from 1924-31 and no. 734 from 1933 until at least 1960.¹⁴ Ironmongers and estate agents appear to have occupied no. 732 in 1924 and 1925 and news agency G E A Richardson, appear to have occupied no. 734 in 1926 and 1927.¹⁵ Small commercial businesses continued to operate from the subject sites throughout the 1930s. A Nicholes, a tailor, occupied no. 732 from 1931 to 1933 and the adjacent site at no. 730 from 1935 to 1945.¹⁶

The subject buildings are evident in a 1945 aerial. A hipped gable roof as well as the canopies are evident to the front of the three buildings with narrower sections and minor structures to the rear of nos 730 and 732.



Aerial photograph, December 1945. 730-734 Heidelberg Road is indicated.
(Source: Landata, Melbourne and Metropolitan Area Project, Project No. 5, Run 27, Frame 59523)

In 1948 no. 730 was transferred to Robert Dunstan McMullen, engineer and Norma Eileen Cattermole, married woman.¹⁷

In 1948 drawings for a proposed brick factory to the rear of the no. 734 were submitted to the City of the Heidelberg.¹⁸ At this time, the occupier of the building was Mr G H Bryant and the owner of the property was J P Monro.¹⁹ The factory was constructed for Goodwear Plating, an electro plating and metal polishing business, however the building no longer survives.

Circa 1968 the Alphington Post Office was relocated to no. 730.²⁰ A historic photograph dated 1968 shows the unpainted brick parapet, canopy, shopfront configuration and wall tiles.²¹ In 2010 no. 730 was subdivided into two lots, no. 730 (front shop) and no. 730A (rear lot including side access). No. 730 continues to function as a Post Office.

¹⁴ Sands & McDougall directory, various. E J March is listed as occupying no. 359 from 1924-1931. In 1933 the street numbers are modified and March occupies the subject site at no. 730.

¹⁵ In 1924, ironmonger R S Witcher occupies no. 357 and Land & estate agents occupy no. 357a. G E A Richardson occupies no. 355.

¹⁶ Sands & McDougall directory, various. In 1931 A Nicholes occupies no. 357 which is listed as no. 732 in 1933.

¹⁷ Certificate of Title, vol.7554/folio 047

¹⁸ VPRS 010150, P 0000, Unit 000114

¹⁹ VPRS 010150, P 0000, Unit 000114

²⁰ Sands & McDougall directory, 1970, p5. The Alphington Post Office occupied 724 Heidelberg Road from at least 1942 to circa 1968.

²¹ B5919, 20, NAA



1968 - No. 730 (Alphington Post Office) and adjacent side access (front of 730A).
(Source: B5919, 20, NAA)

Thomas Duff & Bros. Pty Ltd.

Thomas Duff & Bros., was a family enterprise and one of Melbourne's well-known shopfitters, operating from circa 1910 until at least 1942.²² Duff operated from West Melbourne in the 1920s, relocating to Port Melbourne in the 1930s and North Melbourne in the early 1940s. They manufactured a variety of shop fittings including 'partitions, desks, chairs, tables, screens, showcases, shopfronts, etc.' Duff are responsible for the remodelling of the London Stores at the corner of Elizabeth and Bourke Streets, renovated at the end of the 1930s. The substantial shopfronts featured large display windows that integrated recent lighting methods.²³ Thomas Duff died in May 1921 at his residence in Carlisle Street, Balaclava.²⁴

Thematic Context/Comparative Analysis

- City of Yarra, Review of Heritage Overlay Areas 2007, Appendix 8 (revised May 2017)
- City of Yarra, Review of Heritage Overlay Areas 2007, Appendix 7: Individually significant places not from the main development era (revised May 2008)
- City of Yarra Heritage Review (Allom Lovell and Associates, 1998)
- City of Northcote Urban Conservation Study (Graeme Butler, revised February 1983)

Known comparable places in the City of Yarra

Interwar period shops have not been well assessed across the municipality as many such buildings are located within precincts whose significance primarily relates to the late 19th century (Victorian) and possibly early 20 century (Federation) phases of development and so have tended to be graded non-contributory.

- 207 Bridge Road, Richmond (part of HO310, Bridge Road Precinct). Not contributory. Constructed in 1930, the single storey brick shop has an altered shopfront though the Roman brown brick pier is intact.
- 160 Johnston Street, Collingwood (part of HO324, Johnston Street Precinct). Not contributory. The single storey brick, paired shops have a stepped parapet and recessed shopfronts. The shops are largely intact.

²² Sands & McDougall directory, various; Sands & McDougall directory, various; Sands & McDougall directory, 1942, p2173

²³ 'Building and Architecture: Modern Shopfronts,' *Age*, 26 December 1929, p4

²⁴ *Herald*, 13 January 1922, p10

Other municipalities

- 684-690 High Street, Reservoir (part of High Street (Reservoir) Commercial Precinct, City of Darebin). Contributory. Constructed in 1928, the Interwar period group of shops have original parapets. No. 682 has an original recessed shopfront.

Condition

Good

Integrity

Mostly intact

Previous Assessment

N/A

Heritage Overlay Schedule Controls

External Paint Controls Yes

Internal Alteration Controls No

Tree Controls No

Outbuildings and/or Fences No

Extent of Heritage Overlay

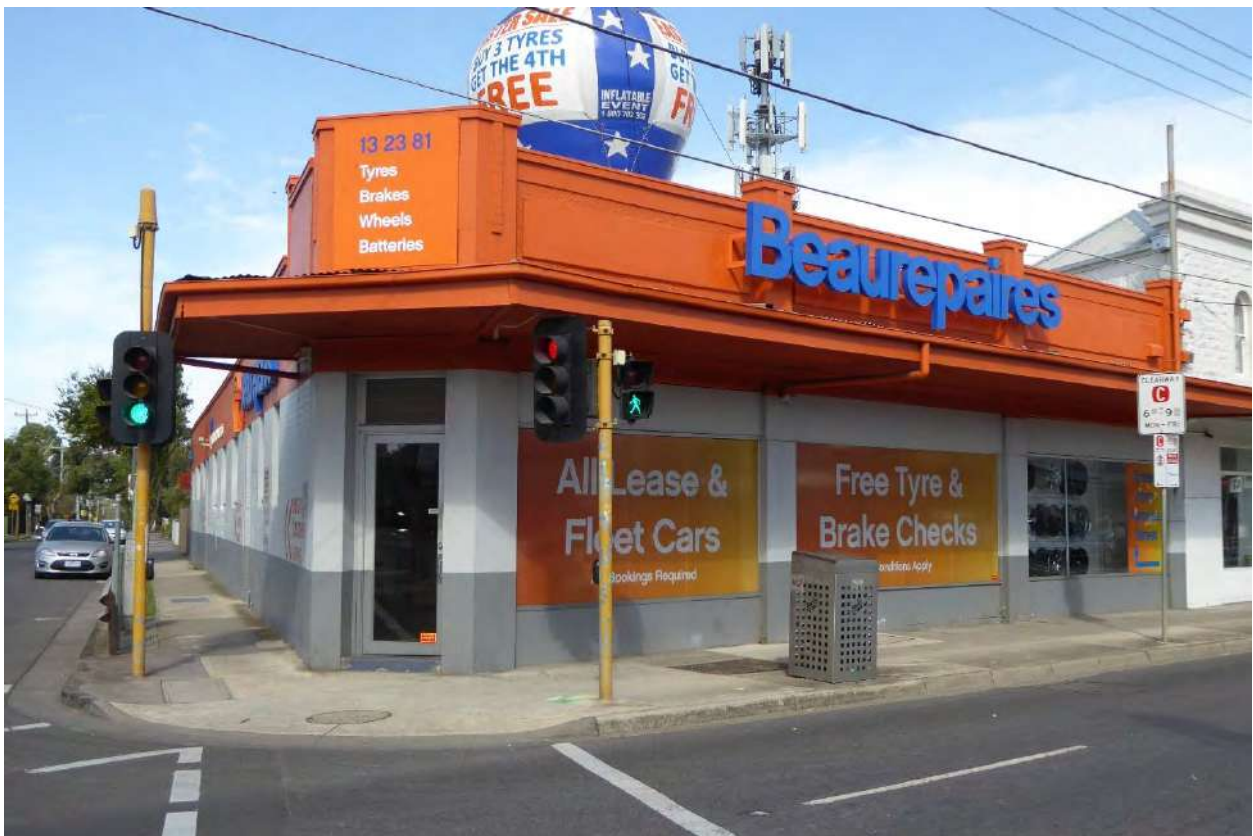
The proposed extent of the heritage overlay would be the parcels of land associated with 730, 730A, 732 and 734 Heidelberg Road, Alphington.



Recommended extent of heritage overlay
(Source: Nearmap 2019)

A. COOPER KNITTING FACTORY (FORMER)

Address	760-764 Heidelberg Road, Alphington
Significance	Local
Construction Dates	1922 (front part), 1930s-1940s additions
Period	Interwar
Date Inspected	Early 2019



Statement of Significance

What is Significant?

The single storey brick building at 760-764 Heidelberg Road, Alphington, constructed 1922, with additions made during the late 1930s and early 1940s.

Significant aspects include the Heidelberg Road and Yarralea Street facades including brick parapet, visible gable roof sections (primarily to the east side), chamfered corner entrance, concrete lintels, pattern of openings and shopfronts, canopy, and remnant wall moulding (west end of north elevation).

How is it Significant?

The single storey building at 760-764 Heidelberg Road, Alphington – the former A. Cooper Knitting factory - is of local historical and aesthetic significance to the City of Yarra.

Why is it Significant?

Initially constructed as three premises in 1922, the single storey building at 760-764 Heidelberg Road, Alphington was expanded and later consolidated by A. Cooper Knitting Manufacturer. It is representative of the commercial and industrial development that occurred during the Interwar period along Heidelberg Road, and in particular was one of a few knitting enterprises that were established along Heidelberg Road at this time. The rapid expansion of the building during the late 1930s and early 1940s is indicative of the important role of the local knitting industry during WWII. (Criterion A)

The single storey building is aesthetically significant as an intact example of an Interwar period building constructed on a prominent corner site. The brick building is distinguished by its parapet and projecting piers articulated with a combination of smooth and roughcast render contrasting with variations in the brickwork. The original pattern of openings, shopfront division, and canopy remains mostly intact. (Criterion E)

Description

The single storey brick building is located on the corner of Heidelberg Road and Yarralea Street. The entrance is located at the chamfered north-east corner. The building consists of various roof sections indicating some staged construction on the site and typically clad with metal sheeting.

A brick parapet extends the length of the Heidelberg Road facade with a partial return to Yarralea Street and its detailing is indicative of Interwar period design. It is divided into three sections (relating to the original three premises) defined by capped brick piers. The parapet has been overpainted but the original format of contrasting materials and textures remains evident (refer historic photograph below) – red brick against grey smooth and roughcast render. The piers mostly have a smooth rendered finish with a central brick strap and the intervening areas are mostly roughcast render (the panels are fixed over) defined by a soldier brick course above (but below the rendered parapet edge) and stretcher courses below (the upper one of which is projecting). Similar detailing is evident to the lower short return on Yarralea Street however the parapet to the chamfered corner is differentiated by having a smooth rendered finish.



Heidelberg Road (north) elevation



Yarralea Street (east) elevation

The shopfronts are typical of the Interwar period and consist of a recessed entry, large shopfront windows with lower masonry stallboard, framing with highlights (often overpainted) and a cantilevered awning, whose soffit is lined with a narrow corrugated sheet metal. The outermost windows on the Heidelberg façade retain curved sections of timber mouldings (possibly original) and there is an original section of moulded tiling to the pier/wall at the west end of the façade.



Yarralea Street (east) elevation – parapet detailing



Yarralea Street (east) elevation

The Yarralea Street elevation has a much lower parapet so that the long gable roof section, clad in corrugated sheet metal, is widely visible. The brick walls in stretcher bond are painted and a concrete lintel extends above all the openings except for the vehicular entry at the southern end.

The regular pattern of openings remains intact consisting of two doorways and several windows. To the north end, there are timber windows with brick sills though at the southern end the windows have been truncated and glass bricks have been introduced. The doorways have a toplight and largely glazed timber-framed door. The extant joinery to the windows and doors are likely not original but possibly indicative of the original types.

History

The subject site formed part of Crown Portion 120, Parish of Jika Jika, comprised of 122 acres purchased by Sydney based merchant Charles William Roemer in 1840.¹ It was one of several similarly narrow allotments with frontages to the nearby creeks and the Yarra River.² The rich soil, topography of the land, and close proximity to the Yarra River was considered desirable.³ Heidelberg Road was located to the south of the allotments and was developed along the line of an earlier track leading to Heidelberg. The road served as an early transport route for those travelling to and from Heidelberg and includes sections of what is now Fairfield and Alphington.

Development along Heidelberg Road remained sparsely settled.⁴ Alphington Village was established by William Montagu Manning (Solicitor General of Sydney) who acquired Roemer's original Crown Allotment 120 in 1852 and, seeing potential for a resting place on the way to Heidelberg, subdivided the land into 130 lots of varying sizes, including provision for shops on both sides of Heidelberg Road.

From 1853, lots from the 'Alphington Estate' were offered for sale. The land was slow to sell however, with only 16 sales by the end of 1856. Amongst the earliest buildings in Alphington were a general store with post office, a bakery, and at least two hotels. The former butcher shop at 756-8 Heidelberg Road was built circa 1860 and is the oldest surviving commercial building of the original Alphington Village. The Wesleyan Chapel constructed circa 1859 on the north side of Heidelberg Road also remains.

A building had been erected on the subject site by 1887 and was acquired by storekeeper, John McKillop who continued to occupy the building until at least 1900.⁵ That same year, land to the south of Alphington Village was subdivided and advertised as **Knockando Estate which comprised of '58 splendid villa sites.'**⁶ The following auction notice shows the subdivision and buildings in Alphington Village about Yarralea Street. The village catered to travellers as well as local residents and consisted of

¹ Landata, Parish of Jika Jika J16(5)

² A Lemon, The Northcote Side of the River, p7

³ City of Darebin, www.darebin.vic.gov.au

⁴ A Lemon, The Northcote Side of the River, p50

⁵ Auction Notice dated 1887, State Library of Victoria; Certificate of Title, vol.1886/folio 096, 1st edition – McKillop died in 1902: Sands & McDougall directory, 1900, p80

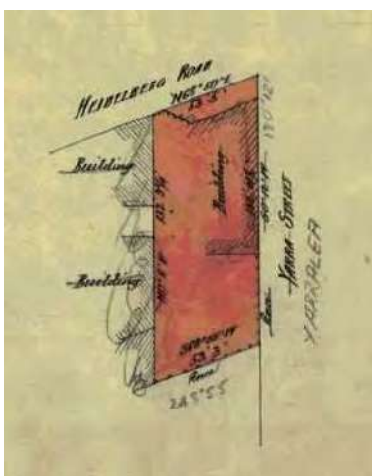
⁶ Auction Notice dated 1887, State Library of Victoria

several small shops including a baker, butcher, store, small cottages and residences. There was also a police station, post office, Alphington Hotel and the Wesleyan Church.



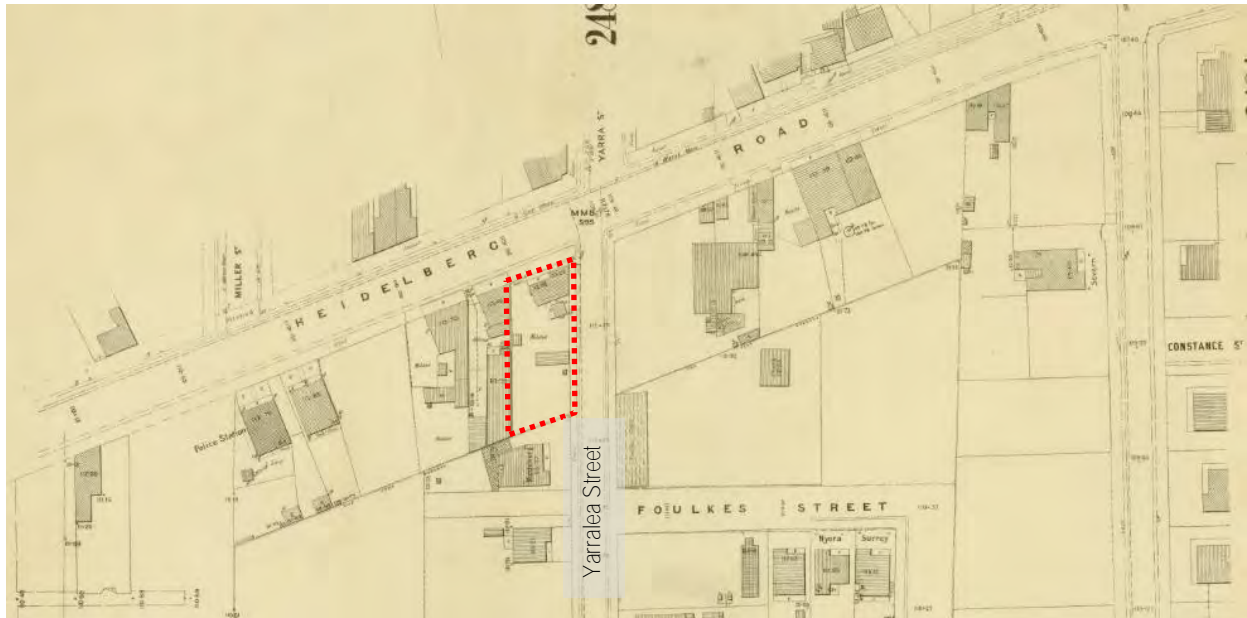
Extract from an auction notice for Knockando Estate in Alphington dated 1887, showing the development existing along Heidelberg Rd at that time. Yarra Street is to the centre of the image and Lucerne Estate is to the bottom of the image. (Source: State Library of Victoria)

An 1887 plan provides further detail about the subject site. The Heidelberg Road frontage measured about 53.5 feet while the Yarra Street (now Yarralea Street) frontage measured 160 feet. McKillop's store is positioned at the front of the site and has an angled street façade. The extant former butcher shop is also evident.



Plan of the subject site in 1887 (Source: Certificate of Title, Vol.1886 Fol. 096)

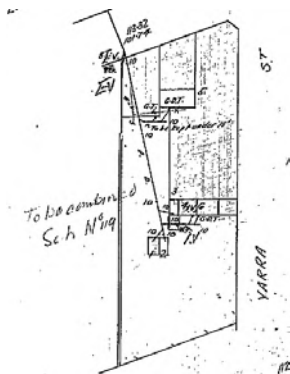
By 1914 some of the buildings on the south side of Alphington Village had been demolished. Some buildings such as the former butcher shop remained with additional structures constructed to the rear. The earlier **McKillop's store at the subject site had been demolished and replaced with several timber buildings with verandahs, fronting Heidelberg Road.**⁷ Smaller timber structures were situated in the middle of the site.



1914 MMBW Detail Plan 1318 showing Alphington Village at the east end of Heidelberg Road - about Yarralea Street, Alphington. The subject site is indicated and consists of several timber structures. (Source: SLV)

Grocer, Mrs Mary Kennedy, occupied the subject site from at least 1916 to 1921 at which stage it was transferred to James Ryan, a hotelkeeper of Heidelberg Road.⁸ James Ryan, hairdresser and tobacconist, was listed at the site in 1922 (then no. 383).⁹

In September 1922, Henry Thomas Rust – a farmer residing nearby at 19 Yarra Street, Alphington – acquired the site.¹⁰ It is not certain whether Ryan or Rust redeveloped the site, but at this time the original timber structures were replaced by the front part of the extant building, which was then comprised of three sections – two small premises and a larger corner premises (refer following).



Property Sewerage Plan dated September 1922. The footprints of the three original shops are outlined. (Source: Yarra Valley Water, Plan No. 101207-3)

⁷ MMBW Detail Plan No. 1318, dated 1914.

⁸ Certificate of Title, vol.1886/folio 096: Sands & McDougall directory, 1916-1921. The subject site is listed as no. 383.

⁹ Sands & McDougall directory, 1922-1923

¹⁰ Certificate of Title, vol.1886/folio 096, 1st edition

In 1923, H T Rust was identified at the site as a hairdresser and tobacconist (possibly an error) and in 1924, H T Rust was listed again as such, in addition to an adjoining unnumbered premises with a draper, John Foley. In 1925, H T Rust was identified as an estate agent at no. 381 as well as A Leithoff, hairdresser and tobacconist, and Phillip V McGavin, billiard saloon, both at no. 383.¹¹ A year prior, a billiard license had been transferred from Rust to McGavin.¹² The billiard saloon continued to operate at the site until 1931.¹³

By the early 1920s the Fairfield-Alphington region had experienced a steady increase in the number of residential and commercial buildings. The south side consisted of a combination of primarily small commercial premises, including grocers, cycle builders, a confectioner, and a bootmaker, with some residential buildings.¹⁴ During the 1920s to early 1930, other occupants at the site included a bootmaker and fruiterer.¹⁵ From about 1935 to 1965 the shop at no. 760 was leased to several boot repairers including F Fletcher from circa 1935 to 1950. The central shop at no. 762 was leased to a confectioner, Mrs D Stevens in 1938 before operating as a cake shop from circa 1942 to 1955.

In June 1938, Hilda Mary Cooper of Cedric Street, Ivanhoe acquired the site.¹⁶ From that time until circa 1970, the corner premises at no. 764 was occupied by knitted goods manufacturer, A. Cooper. For over a decade though (from 1938 to 1950) it seemed to also accommodate a hairdresser.¹⁷ In April 1939, the A T Cooper and Co P/L – 'knitting mill proprietor and manufacturers of woollen goods and warehouseman' – was established with £3000 capital. The two subscribers were Alan T Cooper, manufacturer of 16 Station Street, Aspendale and Robert N Vreland, solicitor of 430 Little Collins Street, Melbourne.¹⁸ In the 1937, Alan Theophilus Cooper had been identified as a mechanic living in Cedric Street, Ivanhoe, in the same street/at the same address as Hilda Mary.¹⁹

The late 1930s saw an increase in wool textile manufacturing **in a bid to aid in Australia's war effort**.²⁰ Local woollen mills intensified production to meet the demand for woollen goods such as blankets, rugs, hosiery and other knitted goods. It is not known what products the newly created company of A. Cooper sold however his business would have been impacted by the wartime demands that were placed on the textile industry. By 1940 Cooper had expanded their knitting manufacturing business and a narrow building was constructed on the south half of the site.²¹

By 1945 Cooper had further expanded, as evident in a historic aerial.²² The rear building had been extended north to form the extant gable roof on the east side of the site and a smaller building had been constructed in the south west corner of the site. The gable roof, skillion roof and street canopies of the three shops in the north part of the site are also evident.

¹¹ Sands & McDougall directory, note that the listings were typically delayed by a year

¹² 'Law Notices', *Age*, 14 March 1924, p6

¹³ Sands & McDougall directory, 1925-1933. In 1933 the building is renumbered as no. 764.

¹⁴ Sands & McDougall directory, 1919, p104

¹⁵ Sands & McDougall directory, 1922, p96

¹⁶ Certificate of Title, vol.1886/folio 096, 1st edition

¹⁷ Sands & McDougall directory, 1938, 1945

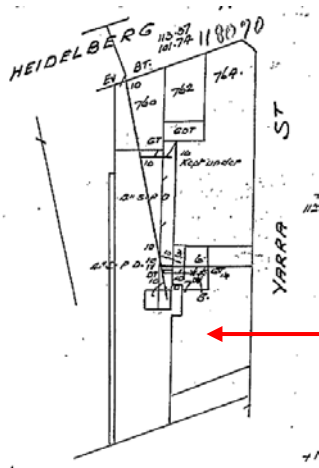
¹⁸ 'New Companies', *Herald*, 29 April 1939, p2

¹⁹ Electoral Role 1937, subdivision of Ivanhoe, p19

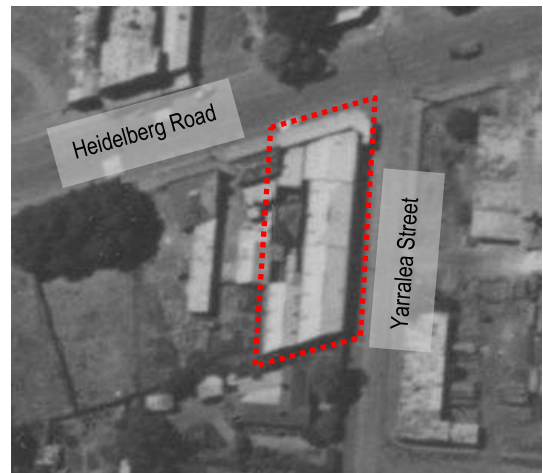
²⁰ **'Local woollen mills helping in war effort.'** *Herald*, 22 June 1940, p6

²¹ Yarra Valley Water, Property Sewerage Plan, Plan No. 101207-0

²² Landata, Melbourne and Metropolitan Area Project, Project No. 5, Run 27, Frame 59523, 1945



The Property Sewerage Plan dated 1940 shows a second building was constructed to the rear of the site indicating the expansion of knitted goods manufacturer, A T Cooper Pty Ltd. (Source: Yarra Valley Water, Plan No. 101207-0)



An aerial photograph dated December 1945 (Source: Landata, Melbourne and Metropolitan Area Project, Proj. No. 5, Run 27, Frame 59523)

In 1952, ownership of the site was transferred to A T Cooper & Company P/L, who retained it until 1969, when it was sold to Lawrence Valentine Rigby, used car proprietor and Doreen Muriel Rigby.²³ In 1970, A T Cooper, drapers were listed at no. 760 with TAB of Victoria at no. 762 and State Savings Bank (SSB) at no. 764. Both TAB and SSB were listed in 1974 but Cooper was not listed at no. 760.²⁴

Subsequently the three premises have been consolidated and the Heidelberg Road shopfront entrances modified. Despite changes to the original shopfronts, the division of the three premises and early timber mouldings remain. Toplights are likely concealed by extant sheeting and other early building fabric beyond this may survive.

A historic photograph dated 1982 shows the original presentation of the parapet. Render with contrasting red brick soldier course detail is evident below the parapet edge.²⁵



Photograph dated 1982 showing original presentation of parapets (Source: Darebin Archives LHRN3652)

²³ Certificate of Title, vol.1886/folio 096, 1st and 2nd editions

²⁴ Sands & McDougall directory, 1974, p11

²⁵ A soldier course is a set of bricks laid vertically with the narrow face exposed.

Thematic Context/Comparative Analysis

- City of Yarra, Review of Heritage Overlay Areas 2007, Appendix 8 (revised May 2017)
- City of Yarra, Review of Heritage Overlay Areas 2007, Appendix 7: Individually significant places not from the main development era (revised May 2008)
- City of Yarra Heritage Review (Allom Lovell and Associates, 1998)
- City of Northcote Urban Conservation Study (Graeme Butler, revised February 1983)

Known comparable places in the City of Glen Eira

To date, many Interwar period factories and warehouses that have been included as an individual overlay consist of iconic landmark factories or complexes that are often built on a much larger scale than the subject building:

- Dimmeys, 140-160 Swan Street, Cremorne (HO335). The grand two storey drapery store was built in stages between 1907 and 1918 and extended in 1986. The brick building was designed in the American Romanesque style by notable architects H W & F B Tompkins and consists of large plate glass windows (that have replaced original display cases at ground floor) and a four storey clock tower surmounted by a dome.²⁶ The ground floor functions as a showroom, similar to the subject building.
- Rosella Factory Complex Precinct, 57 & 64 Balmain Street, Cremorne (HO349). The Rosella complex was designed by architect J E Burke and established in 1905 with subsequent buildings constructed during the 1920s. Elements include parapets and saw-tooth roofs, large window openings at street level (previously with multi-paned glazing) rendered concrete lintels and a combination of timber and steel-framed doors and windows.²⁷

Interwar period factories/warehouses/showrooms have also been included as individually significant or contributory buildings within precincts:

- MacRobertson Pty Ltd, confectionary works offices, former, 214 Argyle Street, Fitzroy (part of HO334 South Fitzroy Precinct). The single storey factory building was constructed in 1937 for manufacturer MacRobertson Pty. Ltd. Designed in the Moderne style the rendered façade features a banded parapet, overpainted brick work detailing and large steel framed multi-paned windows.
- London Baby Carriage Manufacturers Pty Ltd, Factory and showroom, 151-161 Bridge Road, Richmond (part of HO310 Bridge Road Precinct). The factory and showroom was built in 1941 for Hilda and Ruby Wrixon and is located on a corner site. The single storey brick building was designed in the Moderne style and features a parapet with horizontal banding which has been painted. The street façade consists of large expansive window openings.
- 33 Spensley Street, Clifton Hill (part of HO316, Clifton Hill East Heritage Overlay). Not significant. Brick factory/warehouse, now units constructed in 1925. Façade largely intact.
- Pelaco Factory (& Sign), Former, 21-31 Goodwood Street, Richmond (part of HO332C Richmond Hill Precinct). Individually significant. Large industrial complex associated with Australian shirt manufacturer, Pelaco constructed circa 1922. The utilitarian four storey brick building consists of a rendered upper floor, large steel framed windows and an early neon sign\

Condition

Good

Integrity

Mostly intact

Previous Assessment

N/A

²⁶ Statement of Significance, Dimmeys, VHD.

²⁷ Statement of Significance, Rosella Factory Complex Precinct, VHD.

Heritage Overlay Schedule Controls

External Paint Controls	Yes
Internal Alteration Controls	No
Tree Controls	No
Outbuildings and/or Fences	No

Extent of Heritage Overlay

The proposed extent of the heritage overlay would be the parcel of land associated with 760-764 Heidelberg Road, Alphington



Recommended extent of heritage overlay
(Source: Nearthmap 2019)

Part 2: Heidelberg Road Built Form Framework

DESIGN STRATEGY & RECOMMENDATIONS
PREPARED FOR THE CITY OF YARRA



FINAL November 2019

This independent report has been prepared for the City of Yarra. All due care has been taken in the preparation of this report. Hodyl + Co, however, are not liable to any person or entity for any damage or loss that has occurred, or may occur, in relation to that person or entity taking or not taking action in respect of any representation, statement, opinion or advice referred within this report.

Prepared by Hodyl + Co
www.hodyl.co

Project team:
Leanne Hodyl
Huei-Han Yang
Bec Fitzgerald



November 2019

Version E

Contents

This is **Part 2** of the Built Form Framework prepared for the Heidelberg Road Corridor on behalf of the City of Yarra. It incorporates the development of an urban design strategy and specific built form recommendations for the commercial zoned land on the southern side of the road corridor in Fairfield and Alphington.

Part 1 incorporates the urban context analysis that informs the development of this strategy and provides further background to the recommendations included in this Part 2 Report.

Part 1 incorporates:

- The existing strategic planning context for the study area (Section 2)
- The existing local planning context (Section 3)
- The existing physical and character attributes of each precinct (Section 4).

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Overview

Delivering on strategic objectives

Heidelberg Road is a major arterial road that connects the suburbs of Fairfield and Alphington to the central city in the south-west and to the north-eastern suburbs. The road is dominated by heavy traffic and characterised by a generally poor public realm with indistinctive low-rise warehouses and commercial buildings lining the street.

The exception is found within the Heidelberg Road Neighbourhood Centre which includes intact shopfronts and a small number of heritage buildings, as well as the Porta site in the west, which includes a heritage warehouse and brick chimney which is an important landmark within the precinct.

The study area for this report includes three precincts:

- Precinct 1 - Yarra Bend
- Precinct 2 - Fairfield Commercial
- Precinct 3 - Heidelberg Road Neighbourhood Activity Centre

Strategic and design objectives have been established for the corridor. These have been applied and tested within this report to determine appropriate development controls within each precinct.

This study only considers land on the southern side of the street, within the City of Yarra.

The following strategic objectives have been established which guide the overall scale of development along the corridor.

1. Recognise that the development scale on the former Alphington Paper Mills site is strategically positioned as the highest scale of development intensification along the corridor.
2. Recognise the sites to the immediate east and west of the former paper mills site as strategic sites given the proximity to this urban renewal area, access to multiple street frontages and site size.
3. Recognise the Porta site as a strategic site due to its large size and capacity to support multiple buildings, housing diversity and new pedestrian connections to the park.
4. Support a 'moderate' scale of development intensification on all other sites within Precincts 1 and 3.
5. Support a 'moderate' scale of development intensification for commercial uses only in Precinct 2.
6. Deliver well-designed, durable and adaptable developments on all sites, including support for commercial uses in the lower floors of all buildings.

This is summarised in Figure 1.

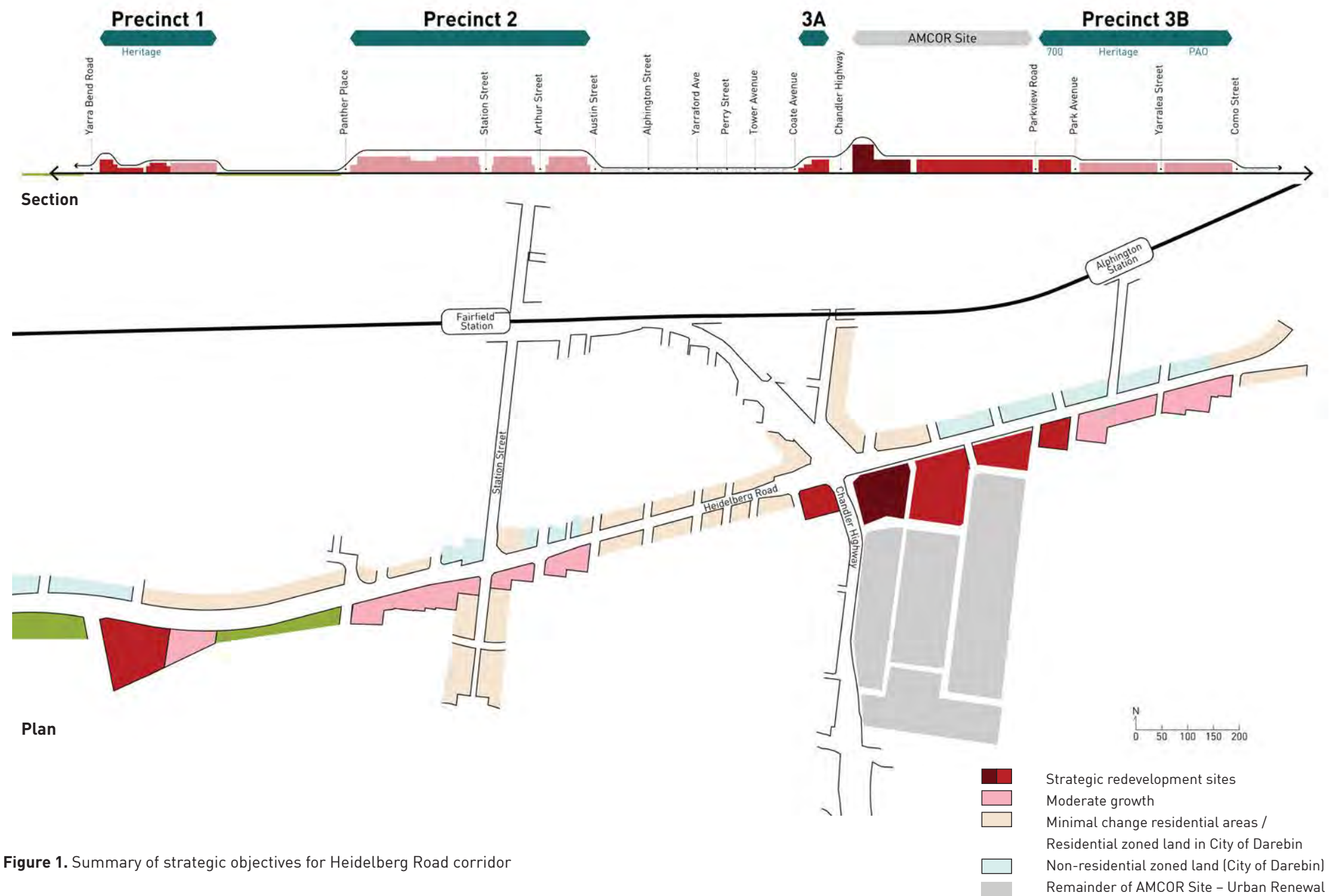


Figure 1. Summary of strategic objectives for Heidelberg Road corridor

Delivering good quality design outcomes

The following design objectives have been established which guide the form (heights and setbacks) and design quality of new buildings.

1. Improve the quality of the public realm through increased activation and enhancement of the pedestrian environment.
2. Establish a new preferred character for each precinct that responds to the existing context.
3. Carefully manage the impact of new development on sensitive land uses to the south.

These design objectives have been considered at two scales:

- Corridor-wide considerations where common attributes that occur along the whole corridor are assessed and proposed controls developed that can apply generally across all new development.
- Precinct-specific considerations where the locally specific context must be taken into account to determine appropriate development controls.

The range of considerations and the planning controls proposed to respond to them are articulated in Figure 2.

Precinct-specific considerations

Create a new **positive street character** by framing the street with high-quality buildings while maintaining a sense of openness, ensuring that buildings are not visually dominant when viewed from within the street.

This is achieved by applying **street wall height and upper level setback** controls that respond to specific conditions within each precinct, including the need to respond to the scale and design of existing heritage buildings and street widths.

Recommended street wall heights vary from **2 to 6 storeys**. Above this a **6 metre setback** is proposed (with additional upper levels setback at a 45 degree angle in Precinct 3B).

Corridor-wide considerations which generally apply to all development across the study area.

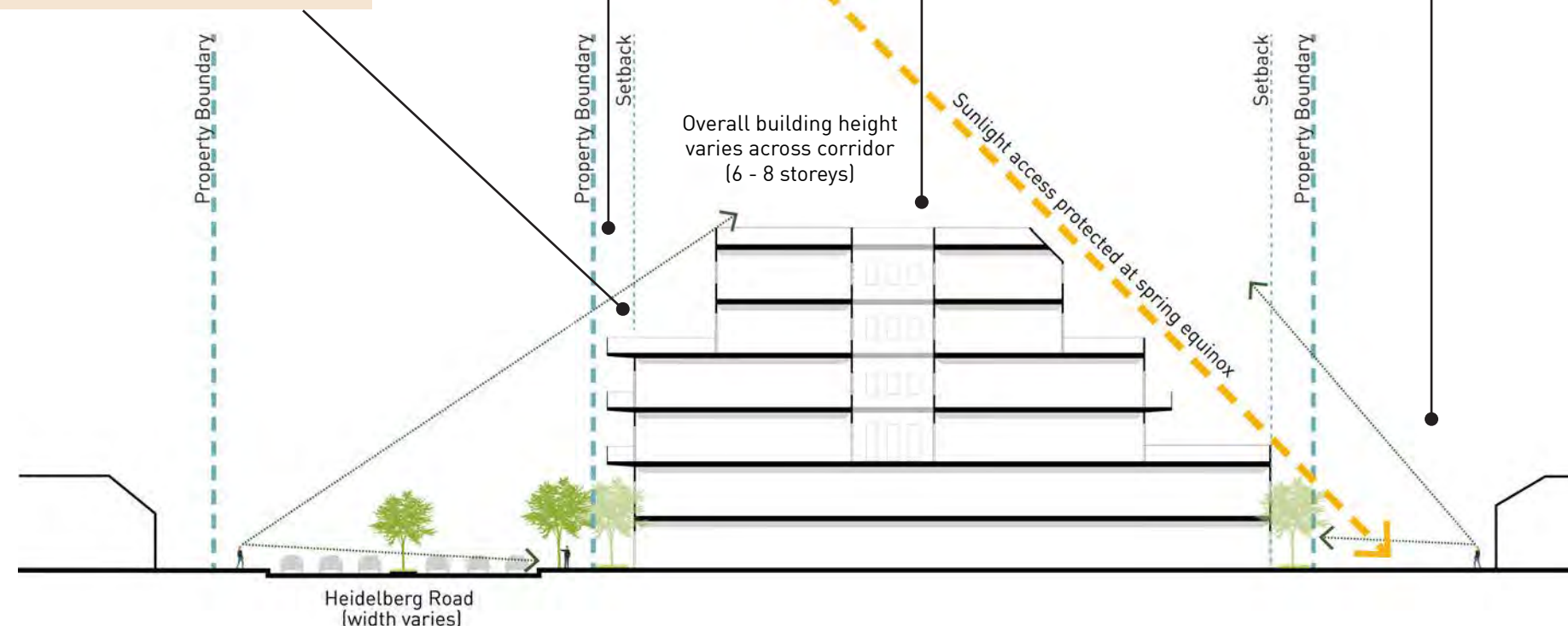
Create a more **welcoming and attractive street** through the inclusion of a **front setback control**. This requires developers to setback new buildings from the street to create more pedestrian space, opportunities for additional tree planting and more street-based activity.

A **3 metre setback** is recommended for the majority of the corridor.

Create **liveable apartments and office buildings** with good levels of natural light, outlook and privacy. **Building setback and separation controls** ensure that there is adequate distance between buildings on the same or adjacent properties. Setback distances are related to the building height and internal use, with primary living spaces and balconies requiring greater separation.

Protect the **amenity of adjacent residential areas**, addressing potential impacts from overshadowing, visual bulk and reductions in privacy, through the inclusion of a **rear interface control**. This designates the form of new buildings (heights and setbacks) along the property boundary that directly interfaces with existing residential sites to the south.

A maximum **2 storey building** height on the boundary is proposed. Where the existing house is less than 15 metres from the property boundary, a minimum **3 metre setback** is required within the new development to support the provision of landscaping.



What determines overall building heights?

Overall building heights are determined by the integration of the strategic objectives and design objectives (corridor-wide and precinct-specific design considerations) including:

- Supporting the preferred overall scale of development (based on strategic planning context).
- Establishing a preferred character within Heidelberg Road.
- Mitigating the visual impact of upper levels when viewed from adjacent residential sites.

A significant amount of built form testing has been included within this report. The following building heights are proposed for each development scale:

- Strategic redevelopment sites - 8 storeys.
- Moderate growth sites - 6 storeys.

Figure 2. Summary of design objectives and recommended planning controls.

1. Developing a built form framework

1.1 Establishing strategic objectives

The study area is a linear corridor of single-depth sites that front directly to Heidelberg Road¹. It includes three precincts:

Precinct 1 – Yarra Bend

Commercial 1 zoned (CZ1) land that is immediately adjacent to large parklands which front the Yarra River. The site includes the Porta construction site which includes a large heritage industrial warehouse and distinctive brick chimney.

Precinct 2 – Fairfield Commercial

Commercial 2 zoned (CZ2) land in relatively close proximity to the Fairfield Neighbourhood Activity Centre. The areas to the immediate south are zoned Neighbourhood Residential and include predominantly 1-2 storey, detached housing.

Precinct 3 – Encompasses the Heidelberg Road Neighbourhood Activity Centre and is in close proximity to the Alphington train station. The precinct has two distinct sub-areas:

- **Precinct 3A – Alphington West**

A single, large site to the west of the former Alphington Paper Mill site on the corner of Chandler Highway and Heidelberg Road. The site is zoned Commercial 1 and interfaces directly with Neighbourhood Residential zoned areas to the west and south. The Yarra Housing Strategy identifies this site as the western extension of the Neighbourhood Activity Centre.

- **Area 3B – Existing Heidelberg Road Neighbourhood Activity Centre**

This includes a number of heritage, narrow-fronted buildings. It is also affected by an existing Public Acquisition Overlay (PAO) that requires front setbacks from the road reserve in the order of 12 metres.

The Heidelberg Road corridor is located in relatively good proximity to public transport, community facilities and the Yarra River recreational corridor. The redevelopment of the former paper mill site will significantly transform the character of the area, bringing a significant number of new residents and expanding the extent of and overall activation within the Heidelberg Road Neighbourhood Activity Centre.

The Commercial 1 zoned precincts (Precincts 1 and 3) are therefore suitably zoned and located to support a greater level of development intensification, in particular for mixed-use developments that incorporate commercial or retail uses at the Heidelberg Road ground floor interface with apartments above.

The Commercial 2 zoned precinct (Precinct 2) supports a greater intensification of commercial uses.

All precincts are relatively undeveloped with 1-3 storey large format showrooms, offices and warehouses. There are two existing 4-storey residential apartment buildings (one in Precinct 1 and one in Precinct 3).

Planning context

There are a number of relevant planning policies and decisions that influence the context of this study.

Clause 21.05 – Built form in the Yarra Planning Scheme

Clause 21.05 provides guidance on the preferred urban design outcomes sought in the municipality, including building heights. Specifically it includes:

- Objective 17 – To retain Yarra’s identity as a low-rise urban form with pockets of higher development. Within this objective is included:
 - Strategy 17.1 – Ensure that development outside of activity centres and not on Strategic Redevelopment Sites reflects the prevailing low-rise urban form.
 - Strategy 17.2 – Development on strategic redevelopment sites or within activity centres should generally be no more than 5-6 storeys unless it can be demonstrated that the proposal can achieve specific benefits.

Commercial and residential zoning interfaces

It is an established position through VCAT decisions that residential properties next to commercial or industrial zones cannot expect the same level of residential amenity as properties which are located in the middle of a residential zone. Similarly, owners of commercial or industrial properties immediately adjacent to residential properties have to take into consideration amenity impacts on

residential properties.

The **City of Yarra’s Housing Strategy (2018)** supports the delivery of a ‘moderate’ scale of housing within the Commercial 1 Zoned precincts. This includes support for increased residential densities and housing diversity through a mix of infill and shop-top apartment developments.

Of specific relevance to the Heidelberg Road Corridor the study notes:

- Heidelberg Road is anticipated to take a significant amount of residential growth. The majority of this will be concentrated within the former Alphington Paper Mill site.
- The former paper mill site is identified as a ‘High change area’ on the Strategic Housing Framework Plan.
- The CZ1 land is identified as ‘Moderate change area’.
- The CZ2 area is designated a ‘Non-residential area’.
- The residential zoned areas surrounding the corridor are noted as ‘Minimal change area’.

The **City of Yarra’s Spatial Economic and Employment Strategy (SEES)**, 2018, identifies the changing nature of the local economy as it shifts from a manufacturing and industrial hub to a knowledge, services and creative industries driven economy.

The strategy includes a strategic direction to retain Commercial 2 zoned land to support a diversity of business and employment opportunities.

Recent planning applications

Planning applications have recently been made for two mixed-use developments at 582 and 718 Heidelberg Road (both in Precinct 3). Both applications were considered at VCAT following Council’s objections. A range of issues were identified including that both buildings were considered too tall for the specific local context.

- The application at 718 Heidelberg Road, a site of approximately 3,000m² immediately to the east of the former Alphington Paper Mill site, was approved with a condition that it be reduced from 8 to 5 storeys. The relationship of the 8 storey building to the existing residential context was considered unacceptable as it ‘presents as overwhelming in scale and approaches too close to its neighbours to the south’². The VCAT decision also notes that Council’s condition to reduce the building to 5 storeys was ‘possibly too conservative’.
- 582 Heidelberg Road, a site of approximately 3,700m² immediately to the west of the former paper mill site, was refused a permit for a 13-storey high building. A taller building was supported on the corner of Chandler Highway and Heidelberg Road, however, the visual bulk of the proposed development was considered to detrimentally affect the ‘character and ‘feel’ of that neighbourhood’³.

In both cases, however, support for mixed-use developments and a degree of development intensification was supported.

Strategic Objectives

The overarching built form response to the corridor is therefore driven by the following objectives:

1. Recognise that the development scale on the former Alphington Paper Mills site is strategically positioned as the highest scale of development intensification along the corridor.
2. Recognise the sites to the immediate east and west of the former paper mills site as strategic sites given the proximity to this urban renewal area, access to multiple street frontages and each site size.
3. Recognise the Porta site as a strategic site due to its large size, and capacity to support multiple buildings, housing diversity and new pedestrian connections to the park.
4. Support a ‘moderate’ scale of development intensification on all other sites within Precincts 1 and 3.
5. Support a ‘moderate’ scale of development intensification for commercial uses only in Precinct 2.
6. Deliver well-designed, durable and adaptable developments on all sites, including support for commercial uses in the lower floors of all buildings.

The application of these objectives within all precincts is the core subject of this report and will determine the scale and form of new development.

1 There is one property within the study area that fronts Park Avenue and not Heidelberg Road.

2 Tribunal decision: Aleks Nominees Pty Ltd v Yarra CC VCAT 1315 [22 October 2018 - PLN17/0040]

3 The Churches of Christ Vic Tas v Yarra CC [2019] VCAT 842 - PLN17/0858. Disclosure: Leanne Hodyl provided expert urban design evidence to Yarra CC for this case.

6 **Heidelberg Road Built Form Framework** | Hodyl + Co

1.2 Determining design objectives & principles

Design Objectives

The urban design approach is founded on 3 key objectives which respond to the analysis of the existing physical context which is included in the Part 1 report. These are:

1. Improve the quality of the public realm through increased activation and enhancement of the pedestrian environment.
2. Establish a new preferred character for each precinct that responds to the existing context.
3. Carefully manage the impact of new development on sensitive land uses to the south.

For each objective, the existing context is outlined and the design principles to deliver on the objective in response to these existing conditions are articulated.

These design principles guide the development of corridor-wide and precinct-specific design controls.

1. Improve the quality of the public realm through increased activation and enhancement of the pedestrian environment.

Existing context

The current quality of the public realm is poor across all three precincts due to the:

- Dominance of traffic along the corridor. At present, it is foremost a vehicular thoroughfare, with limited qualities that invite people to spend time in each precinct.
- Narrow footpaths in many locations that are unsuitable to support development intensification.
- Limited on-street parking which means pedestrians are often immediately adjacent to fast-moving vehicles.
- Limited street activation.
- Multiple vehicular crossovers which compromise pedestrian comfort and safety.
- Minimal street tree planting to mitigate the negative impacts of high traffic volumes and narrow footpaths.
- Limited pedestrian weather protection.

A number of sites have landscaped ground floor setbacks which do improve the pedestrian experience by providing some visual relief within the street, opportunities for planting and additional pedestrian circulation space.



Figure 3. Example of minimal street tree planting, limited weather protection and immediate proximity of footpaths to high traffic volumes.



Figure 4. Example of a landscape setback which provides some visual relief and opportunities for greening to improve the quality of the pedestrian experience.



Figure 5. Example of poor street activation - at grade car parks front directly onto street.

Design principles

Without a significant reduction in traffic volumes there are major constraints within the road corridor to improve the quality of the public realm. The following design approaches are therefore imperative to improve the quality of the public realm.

- Increase activation of Heidelberg Road by requiring active street edges in all precincts.
- Improve pedestrian comfort and accessibility through inclusion of ground floor setbacks to the street where the existing conditions for pedestrians are poor and heritage fabric is not compromised.
- Provide additional opportunities for greening of the street within the front setback.
- Incorporate weather protection at entrances within the front setback and continuous weather protection in the Heidelberg Road Neighbourhood Activity Centre.
- Locate all future carparking underground in basements.
- Locate vehicular crossovers from rear lanes or side streets where possible.
- Rationalise the number of existing crossovers to Heidelberg Road where multiple crossovers exist on single sites.
- No additional vehicular crossovers are supported on Heidelberg Road.

2. Establish a preferred character along Heidelberg Road for each precinct that responds to the existing context.

Existing context

Heidelberg Road is fronted by predominantly 1-3 storey commercial buildings, including large format retail, warehouses and offices. There are two, four-storey residential buildings. The lot sizes and shapes vary significantly along the corridor. They include rows of narrow, traditional 'shopfront' sites as well as wide, larger sites that accommodate large format commercial and industrial uses.

The Heidelberg Road Neighbourhood Activity Centre includes traditional fine-grain shopfronts which is distinct from the remainder of the study area.

There are a small number of heritage buildings located within Precincts 1 and 3.

The existing character of the corridor varies within each precinct, however common attributes include:

Positive attributes

- Leafy residential side-streets which provide attractive green street views at intersections.
- Some sites have ground floor setbacks which include landscape treatments such as paving, understorey planting and small trees.

Negative attributes

- Poorly defined street edges, with generally low-scale development and inconsistent street setbacks.
- Sites with at-grade car parking directly fronting the street.
- Generally low-medium quality building design and materials, including a lack of articulation and visual interest.



Figure 6. Example of leafy side street that interfaces with Heidelberg Road (Precinct 2).



Figure 7. Example of a positive attribute - landscape setbacks that create more room for pedestrian movement and landscape treatments, as well as negative attributes - low-medium quality building materials with low levels of street activation and visual interest.



Figure 8. Traditional shop-fronts and heritage detailing in Precinct 3 which contribute to the positive character of the street.

Design Principles

In each precinct:

- Protect existing heritage buildings and support sensitive redevelopment where appropriate.
- Identify & enhance the specific existing valued attributes in each precinct while supporting a moderate level of development.
- Identify the preferred building typologies that align with the preferred new character area and the preferred future uses.
- Frame Heidelberg Road with high-quality development.
- Balance a sense of enclosure and openness within the street through appropriately scaled street wall heights and sufficient upper level setbacks. This will vary in each context.
- Transition buildings heights at corner sites from the Heidelberg Road frontage down to the existing residential side-streets.
- Separate upper level buildings sufficiently to deliver good levels of internal amenity (outlook, privacy and access to daylight and sunlight).
- On deep, narrow lots, party wall construction and the inclusion of generous light-wells are encouraged.

The preferred Heidelberg Road character is further articulated within each precinct proposal to achieve these design principles.

3. Carefully manage the impact on sensitive residential uses and parkland to the south.

Existing context

Precinct 1 immediately interfaces to large parkland areas.

All sites within Precincts 2 and 3 directly interface with residential properties to the south. These properties are within Neighbourhood Residential Zones where limited change in character is anticipated and where a maximum building height of 9 metres applies.

Design Principles

In Precinct 1 ensure development does not visually dominate or unreasonably overshadow the parklands to the south of Precinct 1.

In Precincts 2 and 3:

- Upper levels to be visually recessive when viewed from the private open space of adjacent dwellings.
- Sky-views from within the private secluded open space of dwellings to the south are provided above recessed upper levels.
- Ensure development does not visually dominate or unreasonably overshadow private open space in adjacent residential areas.

1.3 Urban design strategy

The following plan illustrates the application of the strategic and design objectives as an urban design strategy for the study area.

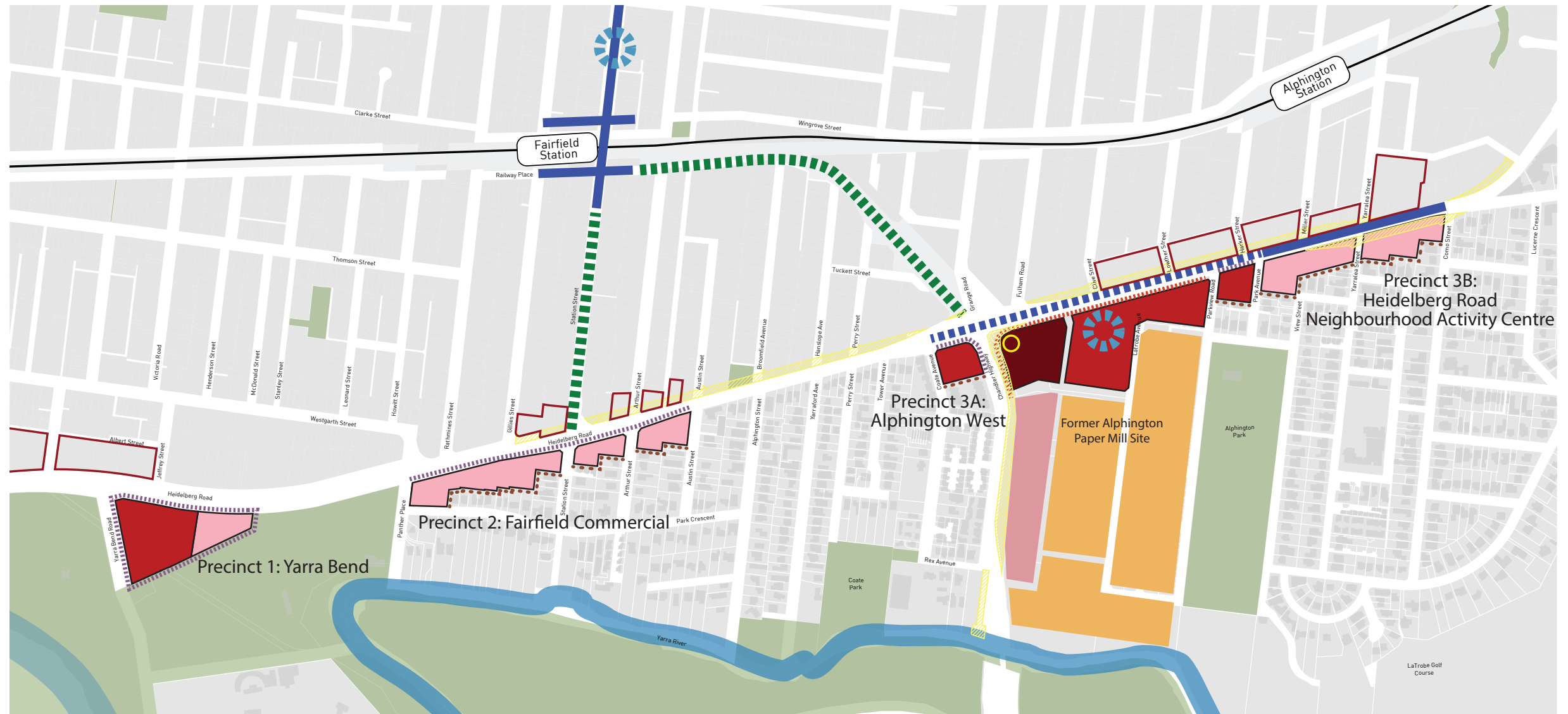
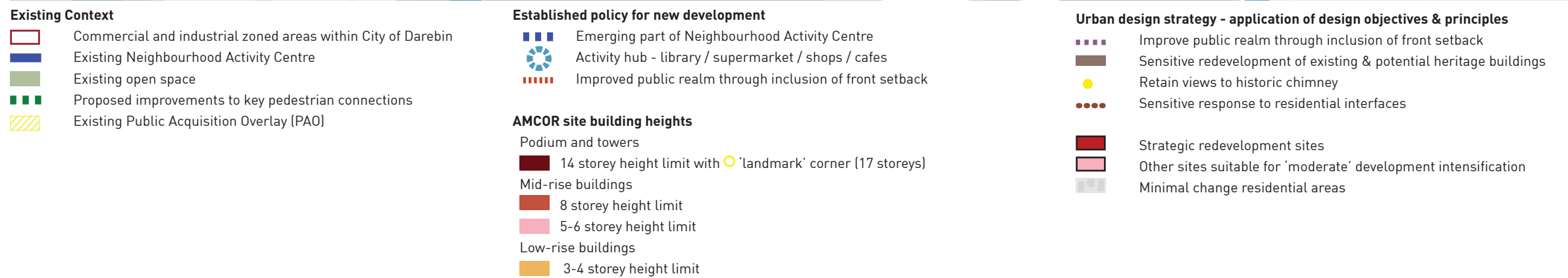


Figure 9. Urban design strategy



2. Corridor-wide considerations

The development of a Built Form Framework for Heidelberg Road can be considered in regards to:

- Corridor-wide considerations that are common along the corridor that occur within each precinct
- Precinct-specific considerations.

The corridor-wide considerations are considered in this chapter. They include:

1. Determining the appropriate standard rear-interface controls that are needed to protect the amenity of existing sensitive residential interfaces to the immediate south.
2. Determining front setback provisions that will improve the quality of the public realm in Precincts 1 and 2 and the western end of Precinct 3.
3. Determining appropriate building separation and upper side setback conditions that will ensure good levels of internal amenity for building occupants.

2.1. Consideration 1 - Rear-interface controls

The study area is defined by the single line of commercially zoned properties that front the southern side of Heidelberg Road and which interface directly with residential zoned properties to the south.¹

There is often tension created when planning policy objectives that support development intensification in commercial zoned areas seemingly conflict with other planning policies that support the protection of high levels of amenity within residentially zoned areas.

The east-west orientation of Heidelberg Road exacerbates this tension as overshadowing impacts will be more significant than in other orientations.

The key issues that must be addressed when determining appropriate design responses along this interface include:

- Mitigating the impacts of overshadowing
- Minimising the visual impact of bulky or tall buildings
- Ensuring reasonable levels of privacy are delivered.

This must be assessed for two types of interface arrangements:

- Rear to rear boundaries which is the typical condition for mid-block sites
- Rear to side boundaries which is the typical condition for corner sites.

The key building elements that impact the visual, overshadowing and privacy amenity impacts are:

- Height of walls on rear boundaries
- Requirement for ground level rear setbacks
- Setbacks of upper levels from the rear boundary
- Overall building heights.

These elements can be considered for the whole length of the study area as there is a generally consistent relationship between site orientation and interface conditions.

Mitigating the impacts of overshadowing

The Yarra Planning Scheme articulates the minimum sunlight access requirements for secluded private open space within a residential zone. These are defined in Clauses 54 and 55 which designate that 'at least 75 per cent, or 40 square metres with a minimum dimension of 3 metres, whichever is the lesser area, of the secluded private open space should receive a minimum of five hours of sunlight between 9am and 3pm on 22 September'.

To test appropriate interface responses to existing sensitive uses to the south (parkland in Precinct 1 and residential uses in Precincts 2, 3A and 3B) detailed overshadowing modelling was undertaken to measure and assess that these minimum requirements can be met.

Boundary wall heights of 4 metres, 7.2 metres (4m commercial + 3.2 residential floor heights), 8 metres (2 commercial floors) and 12 metres (3 commercial floors) were tested. This modelling is illustrated in Appendix A.

The modelling demonstrates that boundary wall heights of up to 8 metres in height can generally meet the overshadowing requirements as specified in the planning scheme.

A summary of the overshadowing impacts of an 8 metre boundary wall height is demonstrated in Figure 10. This illustrates that due to the orientation of rear property boundaries to the direction of sunlight that the depth of shadow into the southern residential properties is generally consistent across the day.

This figure illustrates that adjacent sites that have a deep backyard greater than 11 metres (8 metres, plus the minimum 3 metre depth for sunlight access) can easily meet the minimum Clause 54/55 requirements.

On sites with shallow backyards, the sunlight requirements are either met through sunlight access to a large secluded side yard, or through a ground floor setback within the development site. This is necessary to ensure that the minimum 3 metre depth of sunlight is provided within the secluded private open space immediately adjacent to the dwelling.

In order to understand how upper levels above the boundary wall height might affect overshadowing it is necessary to consider the angle of the sun at the September equinox. There will be an additional overshadowing impact if development is constructed that intrudes into the direct line of the sun coming over the boundary wall height.

Figure 11 demonstrates the angle of the sun above the horizon at the September equinox (called the altitude). Between 11 and 2pm (which meets 3 hour minimum requirement) the lowest angle of the sun is 45 degrees (at 2pm).

Before 11am and after 2pm the altitude angle drops below 45 degrees however the direction of sunlight is coming from a more easterly direction (before 11am) and more westerly direction (after 2pm) with the longer shadows therefore falling on adjacent properties that front Heidelberg Road rather than the residential properties to the south.

¹ There is one property within the study area that fronts Park Avenue and not Heidelberg Road.

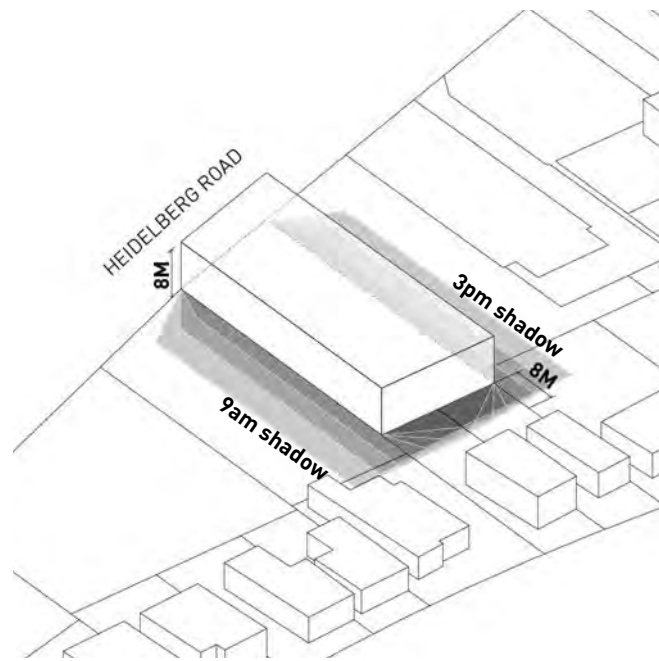
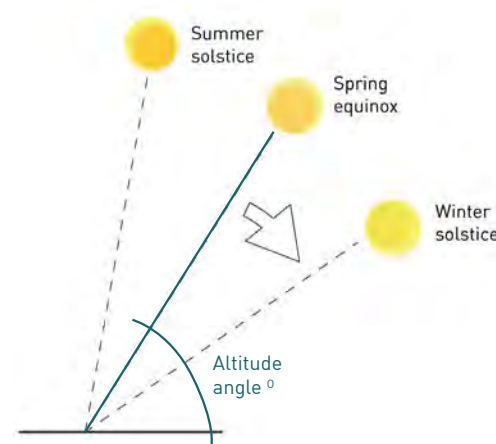


Figure 10. Extent of overshadowing of a 8 metre high wall on boundary at the September equinox. The cumulative overshadowing impacts between 9am and 3pm are demonstrated. The overshadowing impacts generally meet the minimum requirements of Clause 54 and 55 on all sites and interfaces.



Time	Azimuth [°]	Altitude [°]
9:00	61.8	32.2
10:00	47.4	41.9
11:00	28.6	49.2
12:00	5.3	52.7
13:00	34.1	51.2
14:00	320.1	45.4
15:00	304	36.6

Figure 11. Sunlight angles in Melbourne at the equinox. Between 11am and 2pm the sun altitude angle is above 45 degrees. This is when the direction of the shadow from the boundary wall height falls most directly on the adjacent residential properties (as shown in Figure 10)

Minimising the visual impact of bulky or tall buildings

There are two conditions that have been tested within the modelling:

- Condition 1 - where the adjacent dwelling is setback from the rear property boundary by 15 metres
- Condition 2 - where the adjacent dwelling is setback from the rear property boundary by 11 metres.

Condition 1 represents a small number of properties within the study area. Condition 2 is the more common condition.

Condition 1: Adjacent dwelling is setback 15 metres from the rear boundary.

The visual impact of boundary wall heights of 4 metres, 7.2 metres, 8 metres and 12 metres were assessed, together with three alternate setback provisions for upper levels:

- 6 metre setback
- 45 degree angle setbacks
- 12 metre setback.

Two overall height limits have been tested for each scenario - 5 storeys and 8 storeys. This modelling is included in Appendix B.

The modelling demonstrates and emphasises that the distance that the upper levels are setback and the overall height of the upper levels has a direct bearing on the visual amenity impact from within the private

secluded open space within the residential properties to the south.

Each scenario was considered against the proposed design principles (see section 1.2) that include:

- Upper levels are to be visually recessive when viewed from within the private secluded open space.
- Sky-views from within the private secluded open space of dwellings to the south are to be maintained above recessed upper levels.

The following conclusions can be drawn from the modelling. For **5 storey** high buildings:

- A 6 metre setback above the boundary wall height is not considered acceptable as upper levels are too visually dominant. (This would also compromise

the maximum overshadowing requirements).

- A boundary wall height of 8 metres is not too visually dominant at this distance and meets the design principles.
- A 45 degree setback above an 8 metre boundary wall height does meet the design principles.
- A 12 metre setback for a five storey building does meet the design principles. This is a very similar outcome to the 45 degree angle setback.

Collectively they demonstrate that an overall 5 storey height limit is acceptable with either a 45 degree or 12 metre setback when considering the visual impact on adjacent sites.

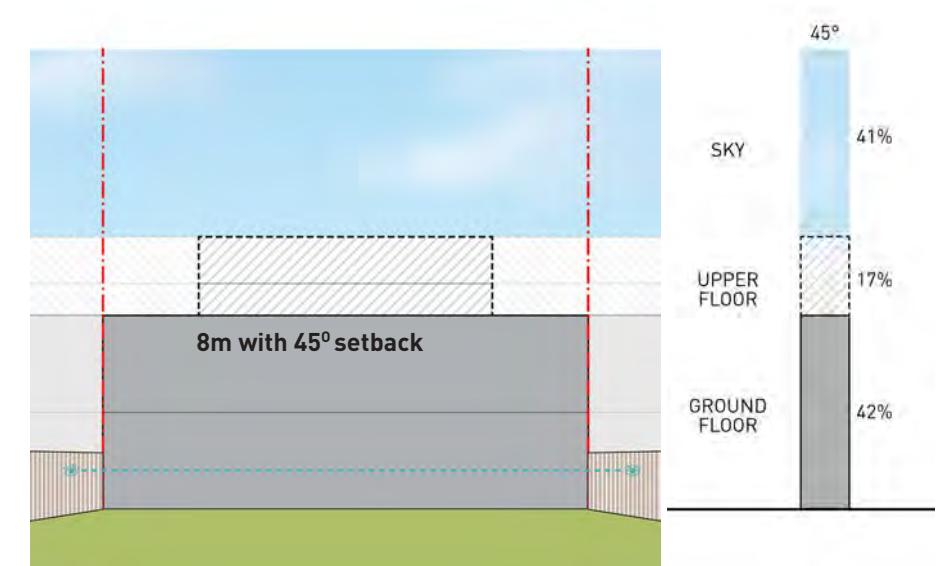
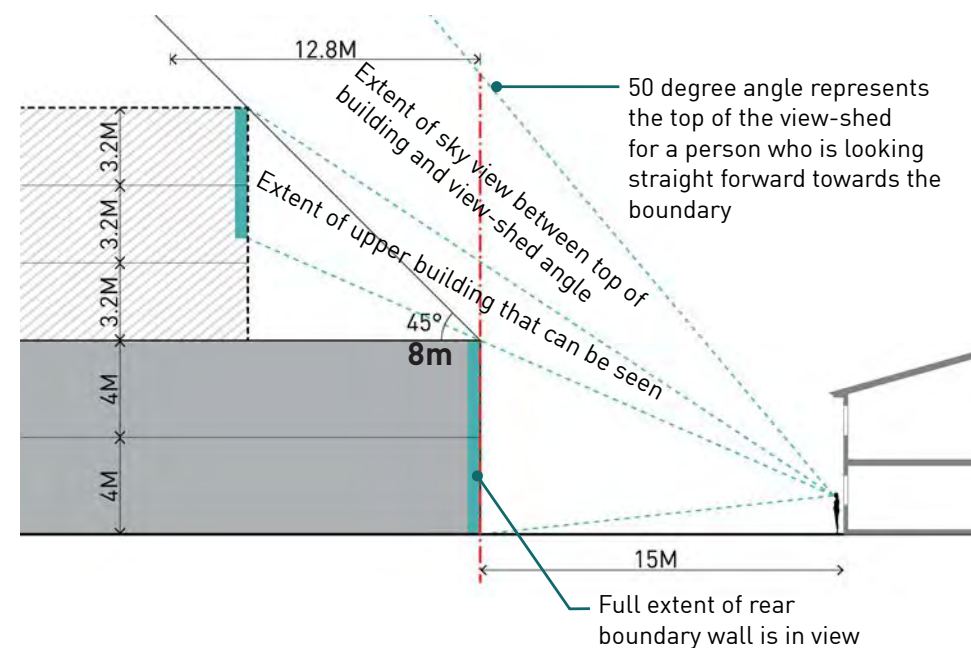


Figure 12. Acceptable degree of visual impact for 5 storey buildings where the adjacent dwelling is 15 metres from the boundary.

For **8 storey** high buildings only one condition met the design principles:

- An 8m boundary wall height with a 45 degree setback for all upper levels.

This is demonstrated in Figure 13.

Condition 2: Adjacent dwelling is setback 11 metres from the rear boundary.

The same scenarios were tested for condition 2. The following conclusions can be drawn from the modelling. For **5 storey** high buildings:

- A 6 metre setback above the boundary wall height is not considered acceptable and upper levels are too visually dominant. (This would also compromise the maximum overshadowing requirements).
- A boundary wall height of 8 metres is not acceptable as it is too visually dominant.
- A boundary wall height of 7.2

metres is only just acceptable. If the viewpoint was taken from any closer than 11 metres this would no longer be acceptable.

- A 45 degree setback above an 8 metre boundary wall height does meet the design principles.
- A 12 metre setback for a five storey building does meet the design principles.

The key difference between Condition 1 and 2 is the impact of the boundary wall height.

Recent VCAT cases highlight the effectiveness of mitigating the visual impact of the development at ground level through the inclusion of a landscape setback.

This approach was supported in the VCAT cases for 718 and 582 Heidelberg Road. In the case of 718 Heidelberg Road the following position was taken by the tribunal:

- Acceptance that a 3 metre setback could provide sufficient landscape buffer to adjacent dwellings as it

can accommodate canopy trees.

- Support for an increased buffer to improve the useability of the landscape space for occupants of the new development.

In the case of 582 Heidelberg Road a 4.5 metre landscape setback was proposed adjacent to a 2.5 storey building height and was generally supported by the applicant, Council, all expert witnesses and the VCAT panel.

Inclusion of a 3 metre setback has been tested in the modelling (see Figure 14) and illustrates the effectiveness of this in reducing the visual impact of the development.

Delivering good design

It is important that good architectural design is also achieved. Within the setback envelope, development should step back in a maximum of two steps to avoid 'wedding cake' outcomes.

Key recommendation

The following rear interface development controls are proposed for all developments sites with direct residential interface. These ensure that overshadowing, visual impact of the boundary wall and upper levels are taken into consideration.

Condition 1 - Rear to rear boundary condition where the adjacent dwelling is sited 15 metres from the boundary:

- Maximum boundary wall height of 8 metres.
- Above this, all upper levels to be setback at a 45 degree angle.

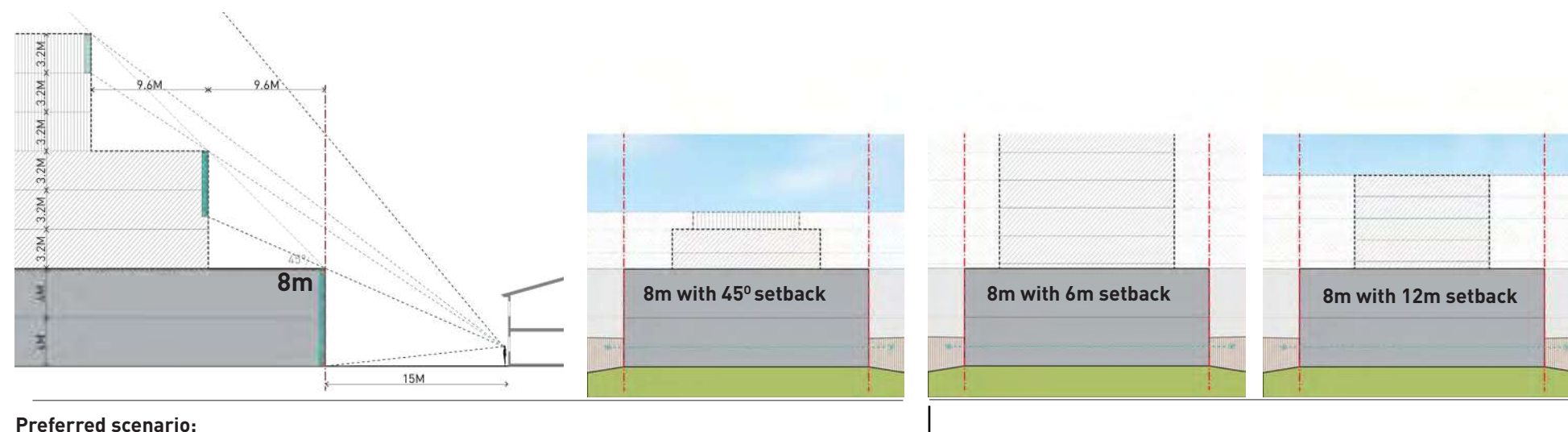
Condition 2 - Rear to rear or rear to side boundary conditions where the adjacent dwelling is sited less than 15 metres from the boundary:

- Minimum 3 metre ground floor setback from the boundary.
- Maximum building height located at the setback distance of 8 metres.
- Above this, all upper levels to be setback at a 45 degree angle.

On all sites, the minimum sunlight access requirements as stipulated in Clause 54 and 55 apply to adjacent secluded private open space and must be considered.

These controls are demonstrated in Figure 15.

For comparative purposes only, the setback requirements of Clause 54 and 55 are also illustrated.



Preferred scenario:
Maximum 8 metre high wall on boundary with upper levels setback at 45 degree angle

Unacceptable scenarios: Maximum 8 metre high wall on boundary with upper levels setback only 6 (left) or 12 (right) metres

Figure 13. Visual impact of 8 storey height limit with different upper level setbacks applied. Note: All views are drawn in one point perspective.

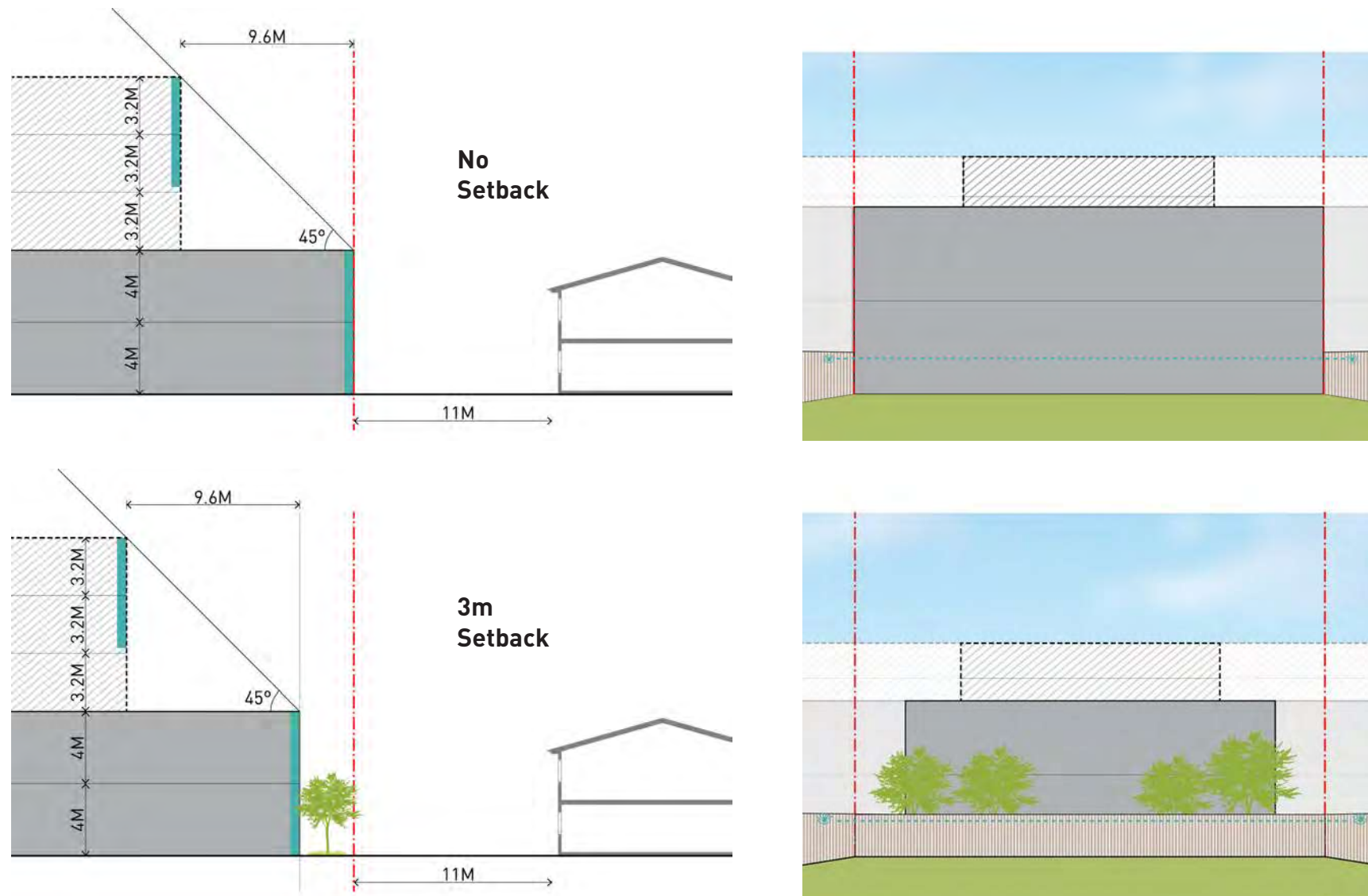


Figure 14. View from 11 metres - no ground level setback (above) and a 3 metre ground level setback (below)

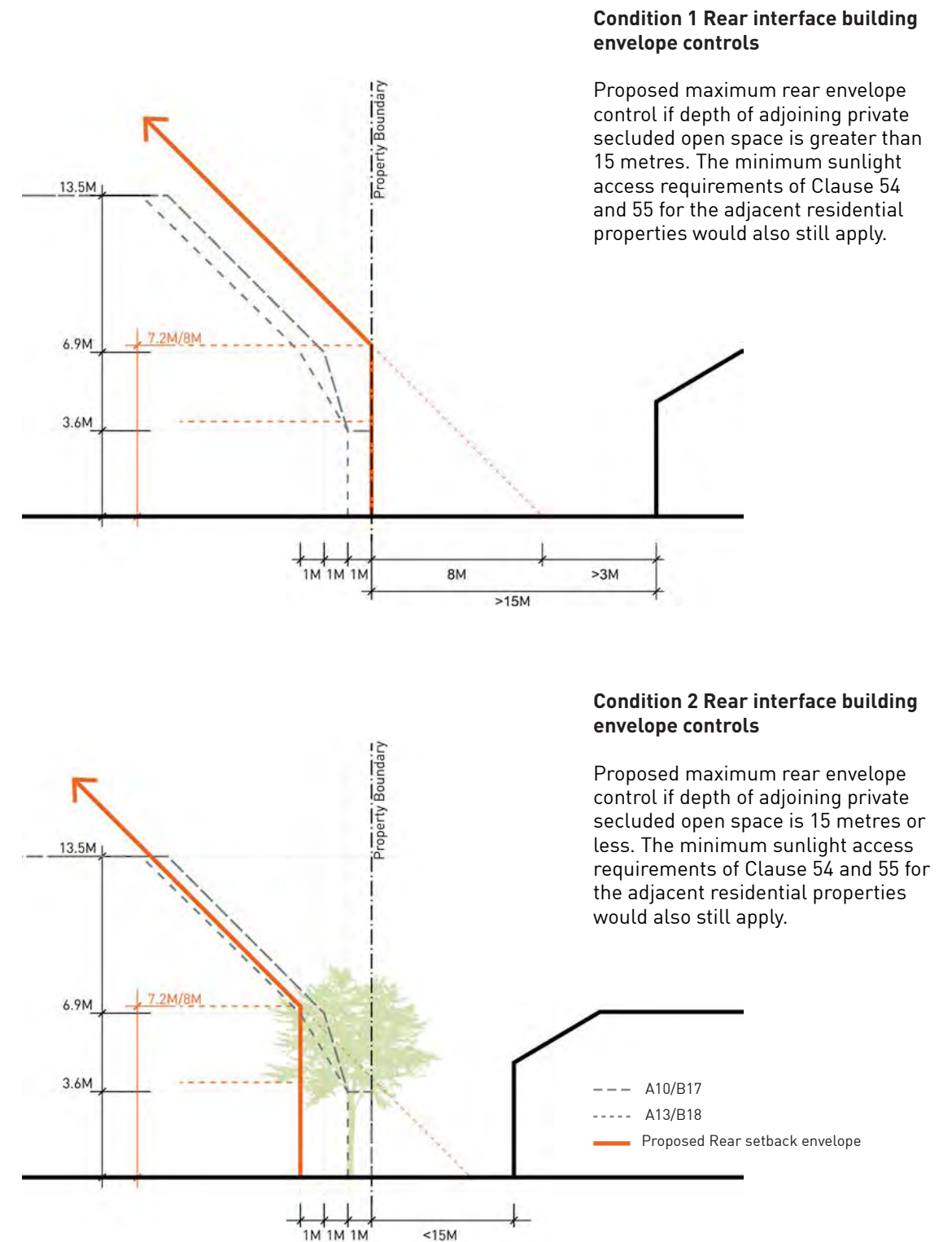


Figure 15. Proposed rear interface controls for Condition 1 and Condition 2. These are proposed as mandatory on all sites.

2.2 Consideration 2 - Front setback requirements

Existing conditions

There are three different footpath and setback conditions across the study area:

- 1-3 metre wide footpaths within Precincts 1, 2 and 3A (with varied building setbacks). There is little opportunity to improve the quality of the public realm within the road reserve as traffic volumes (and therefore carriageways) are unlikely to be reduced.
- The existing fine-grain shopfront area with 1.5-4m wide footpaths and not setbacks in Precinct 3. A setback is not desirable as it will compromise existing valued character and the retention of heritage buildings.

- The areas within Precinct 3 that are affected by the existing Public Acquisition Overlay (PAO) which requires a building setback in the order of 12 metres (east of Yarralea Street) and which narrows west of Yarralea Street.

The existing setback conditions are illustrated in Figures 16 - 29.

Determining appropriate ground floor setbacks in Precincts 1 & 2.

The existing footpath widths in Precincts 1 and 2 vary from 1 to 3 metres. The continuous clear pathway is in the order of 1-2m (clear from tree planting and other street furniture). This is considered too narrow considering the scale of development intensification that is anticipated on the street, and the increased pedestrian volumes that this will introduce.

The poor pedestrian conditions and environment is exacerbated by the high traffic volumes and the lack of on-street parking which means pedestrians are walking immediately adjacent to fast-moving traffic (60km/hr).

Inclusion of a front ground floor setback provides the opportunity to significantly improve this interface as well as provide for better internal amenity, which will support greater development intensification.

The setback distance should be informed by providing sufficient depth to:

- Support further activation of the street through inclusion of outdoor seating space and trading & display space.
- Improve pedestrian access into and out of building entrances and along Heidelberg Road.
- Opportunities to introduce greening into the front setback to soften the streetscape environment.

A modest setback of 3 metres is considered appropriate to achieve these aims (refer Figure 30).

Examples of landscape setbacks are demonstrated through existing developments within the study area (refer Appendix C). While they vary significantly in design quality, they do illustrate an improvement to the pedestrian experience through the creation of more space at the ground floor interface.

Support for ground floor setbacks have also been considered in two of the recent VCAT case:

- Support for a ground floor setback was included in the VCAT decision for 582 Heidelberg Road.
- The VCAT decision for 718 Heidelberg Road noted that a setback could be considered and could add value however would need to be considered through more detailed re-design.

Precinct 1 Existing interface to street



Figure 16. Section location plan

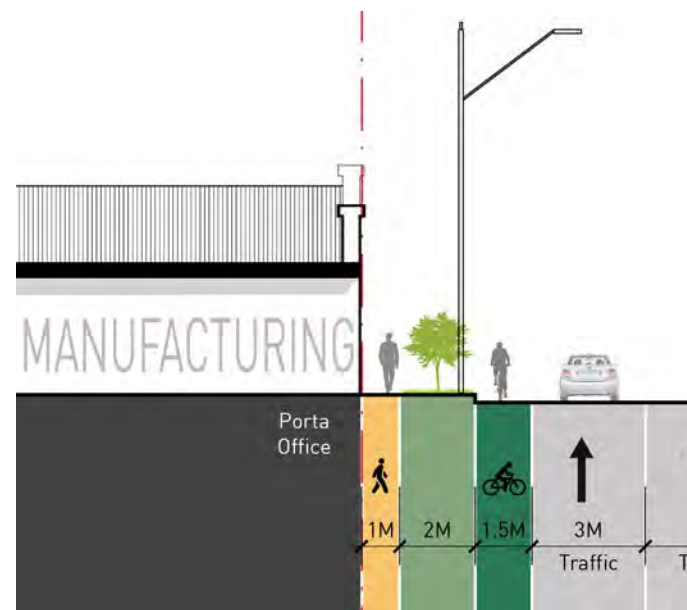


Figure 17. Existing street interface at location 1

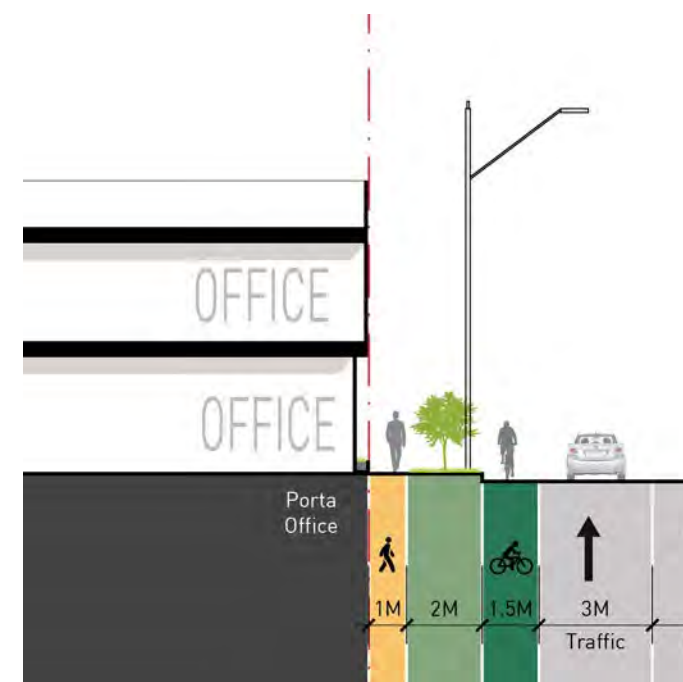


Figure 18. Existing street interface at location 2

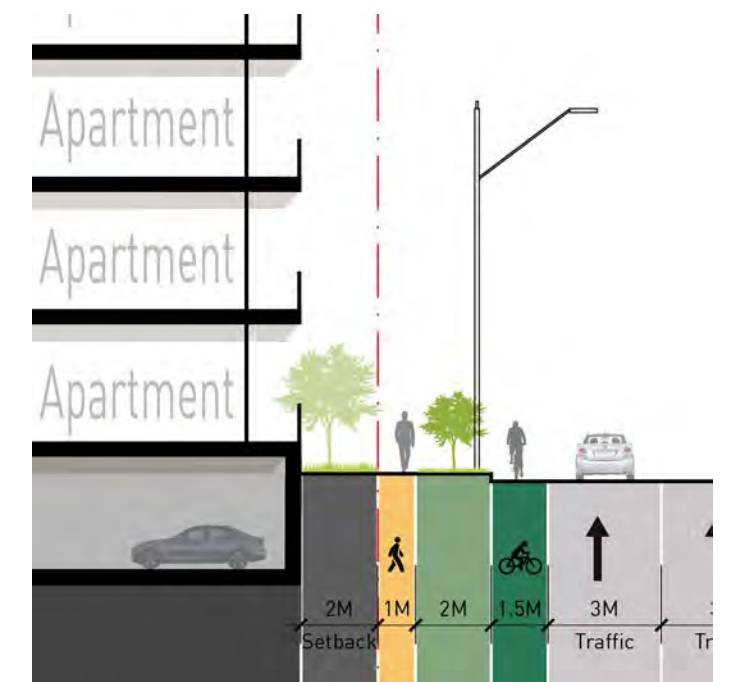


Figure 19. Existing street interface at location 3

Precinct 2 Existing interface to street



Figure 20. Section location plan

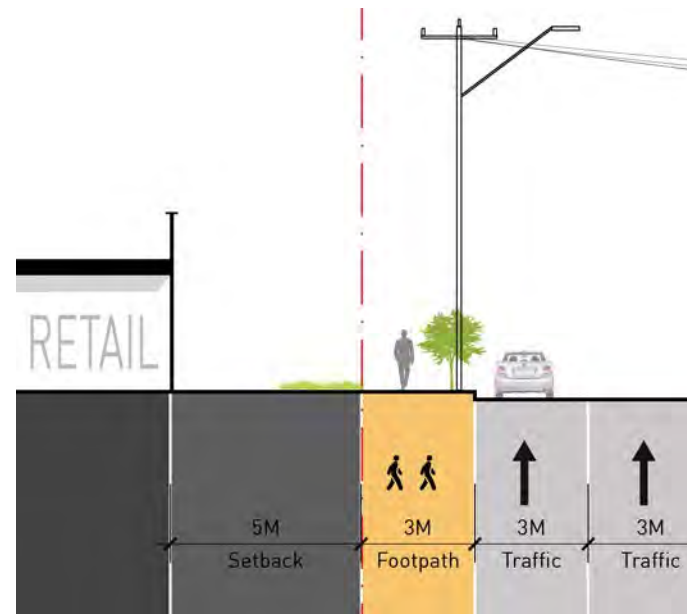


Figure 21. Existing street interface at location 1

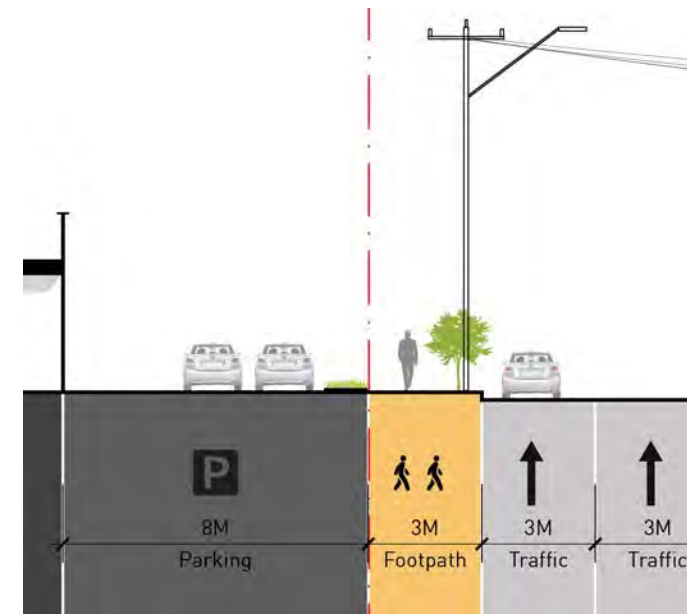


Figure 22. Existing street interface at location 2

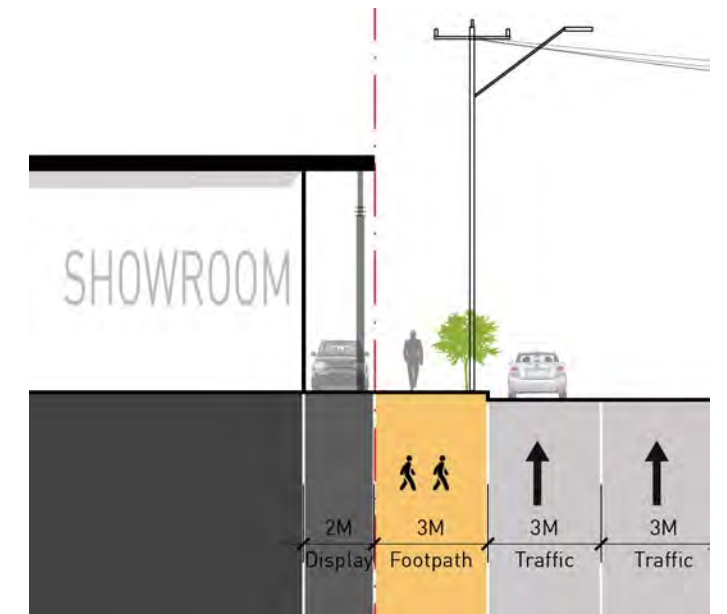


Figure 23. Existing street interface at location 3

Precinct 3A Existing interface to street

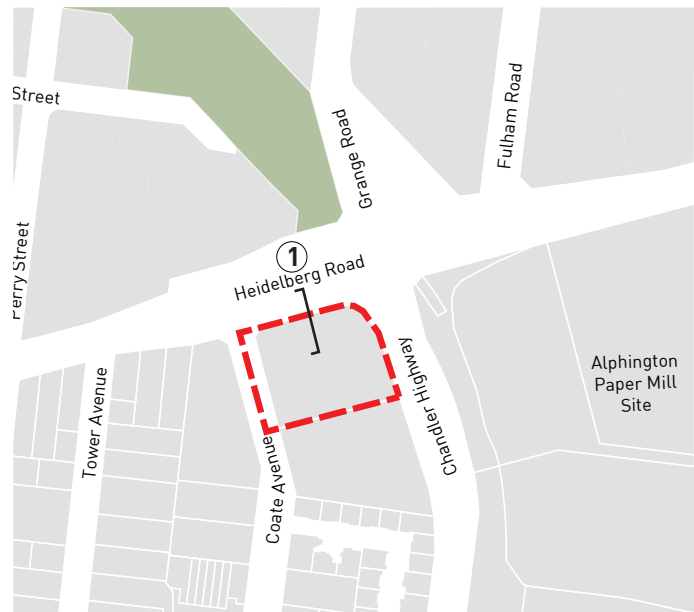


Figure 24. Section location plan

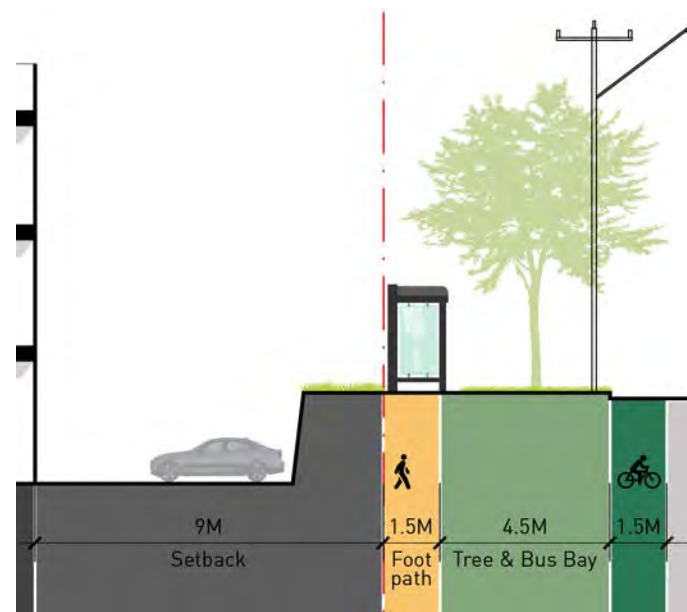
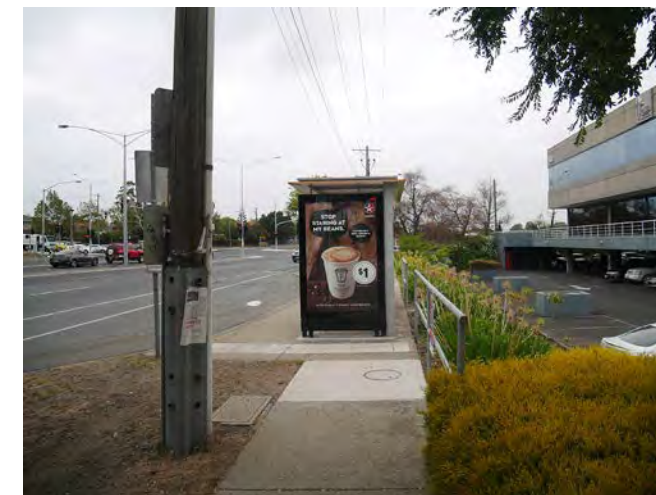


Figure 25. Existing street interface at location 1



Precinct 3B Existing interface to street



Figure 26. Section location plan

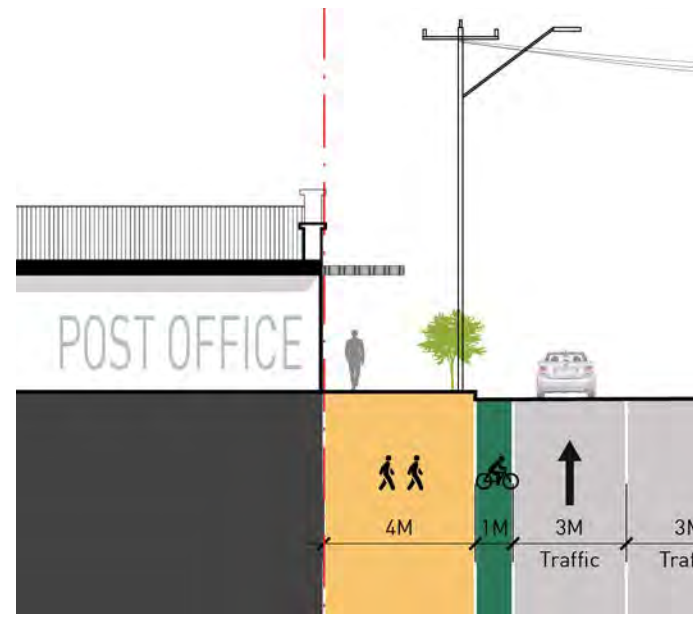


Figure 27. Existing street interface at location 1

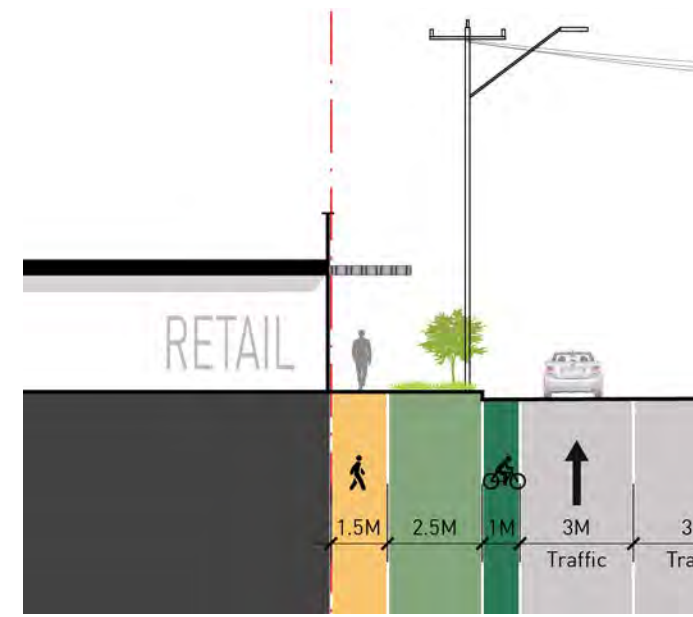


Figure 28. Existing street interface at location 2

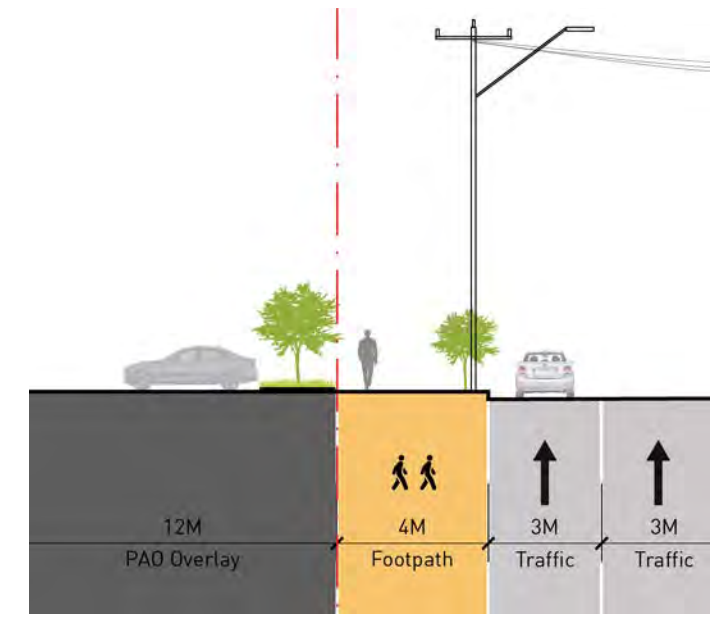


Figure 29. Existing street interface at location 3

Examples of opportunities to improve street quality and activation through the front setback.

Inclusion of a 3 metre setback will widen the amount of space for public and semi-public use to 4-4-5 metres on the road corridor in Precincts 1 and 3A and to 6 metres in Precinct 2.

This creates a more comfortable balance between the space dedicated to making the street more attractive, comfortable and safe and the space committed to the traffic requirements of the arterial corridor.

A 3 metre setback will only have a modest impact on development potential while the positive impacts to the public realm will be significant. At upper levels balconies and other building protections can protrude into this space and still achieve the design objective.

Importantly, the setback should be consistent to create a continuous street wall alignment that will support the creation of an active commercial street. This will also address the current poor character outcomes that are created by the existing diversity of setbacks and street interfaces.

Key recommendation

Adopt a consistent 3 metre building setback from the front boundary within Precincts 1, 2 and 3A where the existing public realm conditions are poor and there are limited heritage constraints.

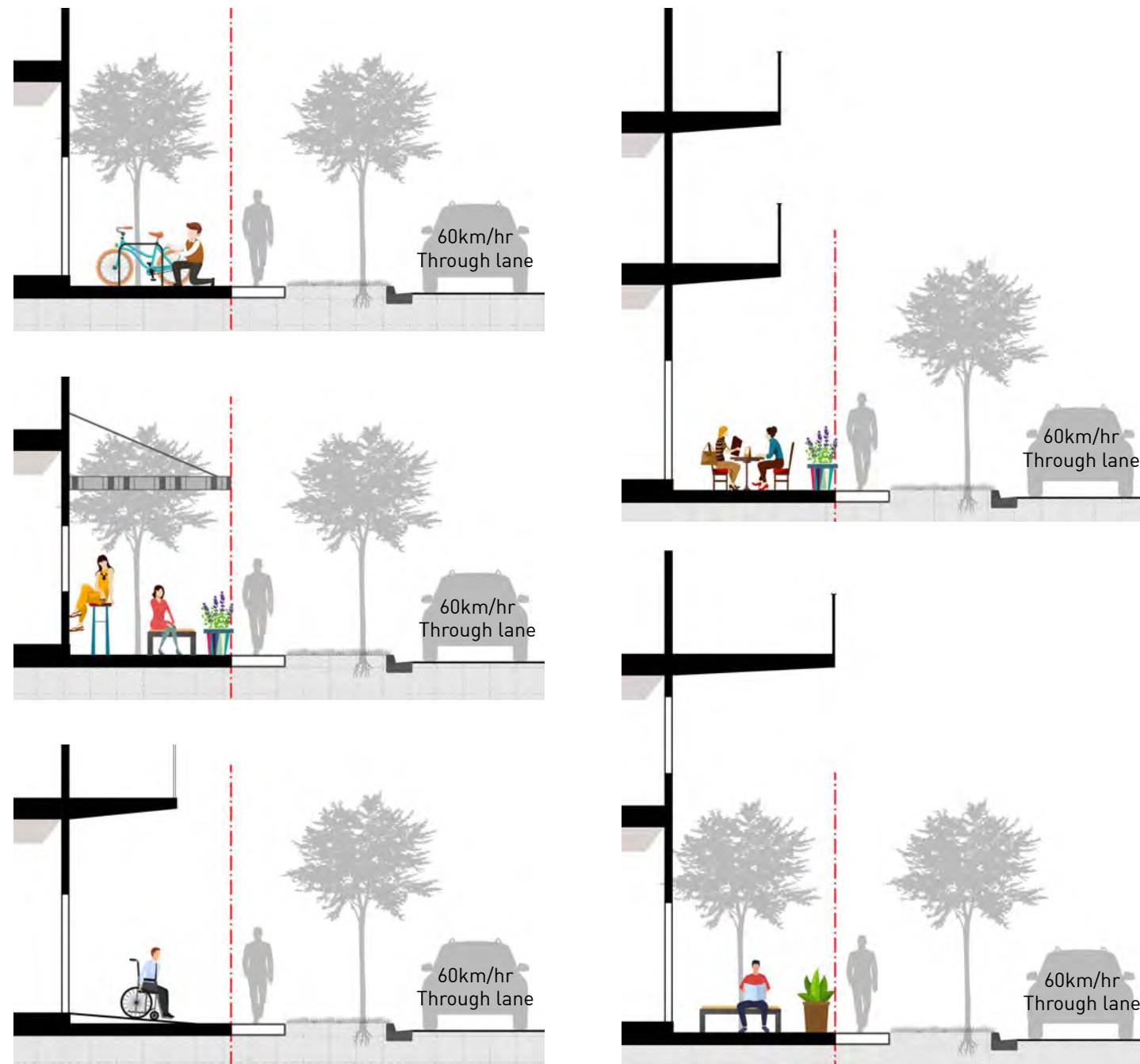


Figure 30. Illustration of 3 metre setback within the context of existing conditions on Heidelberg Road

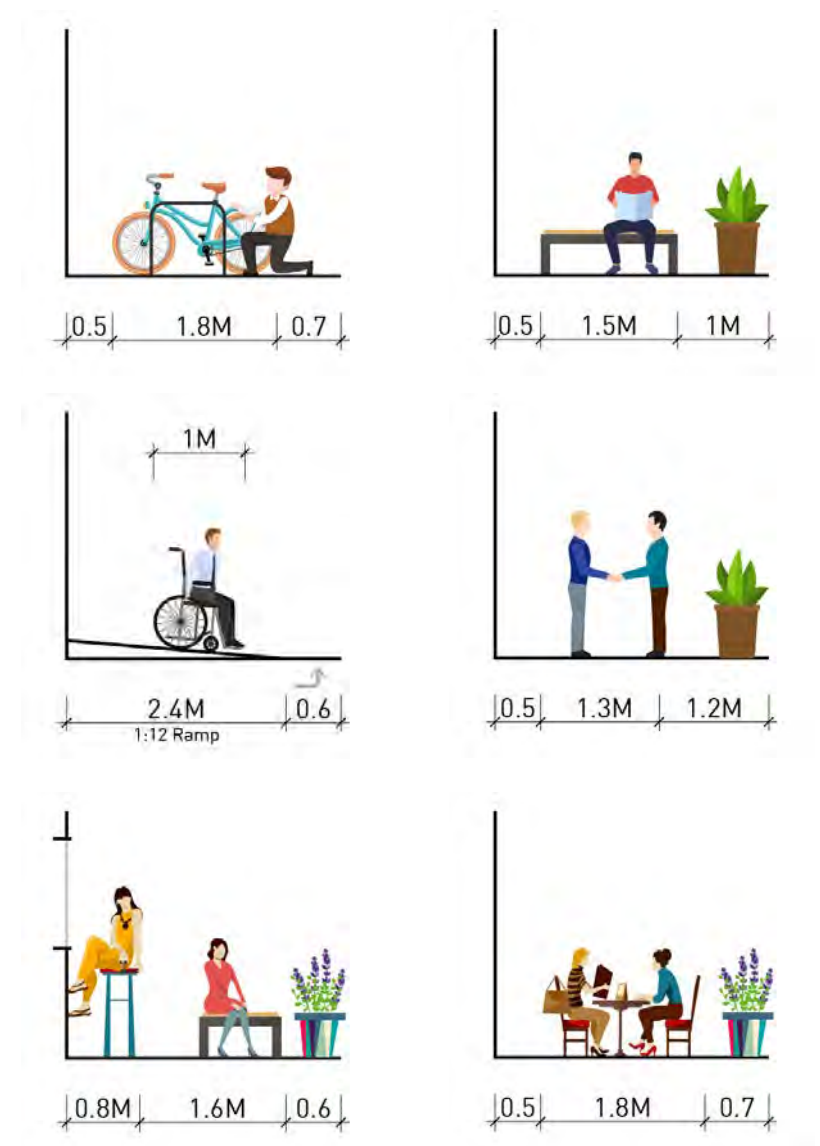


Figure 31. Dimensions for improving activation and access within 3 metre setback.



Figure 32. Precedent of front landscaped setback for street activation



Figure 33. Precedent of front setback for street activation

Design guidelines for landscape setback

- External spaces to be at the same grade as the footpath.
- External spaces to be predominantly hard-paved areas with some planting opportunities.
- Paving materials to be complementary to the existing streetscape design.
- Unobstructed access should be provided, avoiding the use of steps and narrow spaces between planting areas.
- The inclusion of small canopy trees is encouraged.

2.3 Consideration 3 - Building separation and side setback controls

Preferred building typologies

Generally party walling is encouraged across the study area. This prioritises the provision of internal amenity (access to daylight, sunlight, outlook and privacy) from the street and rear boundaries.

On narrow and small-medium sites, a party wall outcome is strongly preferred - side setbacks will not be possible without significantly diminishing the amount of development that can be achieved on each site or creating poor amenity outcomes for building occupants.

On larger sites, where a party wall outcome is not adopted, setbacks from side boundaries will need to be carefully considered to ensure that development equity and good levels of internal amenity are provided.

Side/rear setbacks and building separation

The Better Apartment Design Standards emphasise the importance of good building separation to deliver good quality apartment living. They do not specify metrics for setback requirements to achieve this outcome.

A number of planning scheme amendments for a range of high-density urban contexts, however, have recently considered the appropriate minimum distances that provide a minimal acceptable standard in medium-high density contexts.

A key attribute of many of these amendments is the importance of linking building separation with building height. That is, as buildings become taller, they should be set further apart.

The distinction between the amenity required from a residential primary living space/balcony to other internal uses, including commercial buildings as well as to bedrooms, kitchens and bathrooms in apartments is also considered.

The following proposed setbacks from side boundaries draw on these recent amendments and are proposed for the study area. These apply if buildings are not built on the side boundary.

Building height	Preferred separation (Suitable if there is a primary living space/balcony facing the boundary)	Minimum separation (Suitable when the use is not a primary living space or balcony facing the boundary)
Up top 4 storeys	4.5m	3m
5-8 storeys	6m	3m

Within sites, these setback distances are doubled to achieve sufficient building separation.

3. Precinct-specific considerations

3.1 Consideration 1 - Determining the preferred interface to Heidelberg Road

The preferred scale of development on Heidelberg Road is driven by the design principles to:

- Protect existing heritage buildings and support sensitive redevelopment where appropriate.
- Identify & enhance the specific existing valued attributes in each precinct while supporting a moderate level of development.
- Identify the preferred building typologies that align with the preferred new character area and the preferred future uses.
- Frame Heidelberg Road with high-quality development.
- Balance a sense of enclosure and openness within the street through appropriately scaled street wall heights and sufficient upper level setbacks. This will vary in each context.
- Support the design of well-proportioned buildings where the lower and upper levels form a well-balanced massing composition.
- Transition buildings heights at corner sites from the Heidelberg Road frontage down to the existing residential side-streets.

Street wall heights

This can be achieved through the introduction of a street wall height that:

- Steps down to existing single storey heritage buildings (Precinct 1)
- Street wall heights that align with existing valued heritage street character (relevant to Precinct 3B)
- Creates a well-defined street edge but which does not visually dominate. This is related to the overall street width.

Considering the poor quality of the street environment this balance is particularly important to achieve. Buildings that are visually overwhelming will exacerbate the impact of heavy traffic on the pedestrian experience.

To determine appropriate street wall heights, modelling of street views from the opposite side of the street was tested to consider various scenarios.

Overall building heights

The following criteria are to be met:

- Overall building heights do not visually dominate within the street.
- Upper levels above the street wall are setback to mitigate the visual impact of upper levels.
- Integration of overall heights with existing heritage buildings and streetscapes.

The design response to Heidelberg Road needs to be considered within each precinct as the conditions vary along the length of the corridor.

Additional assessment

This study does not consider the wind impacts from new developments. The scale and design of each development should ensure that negative wind impacts are not created that reduce the safety and comfort of pedestrians within the street.

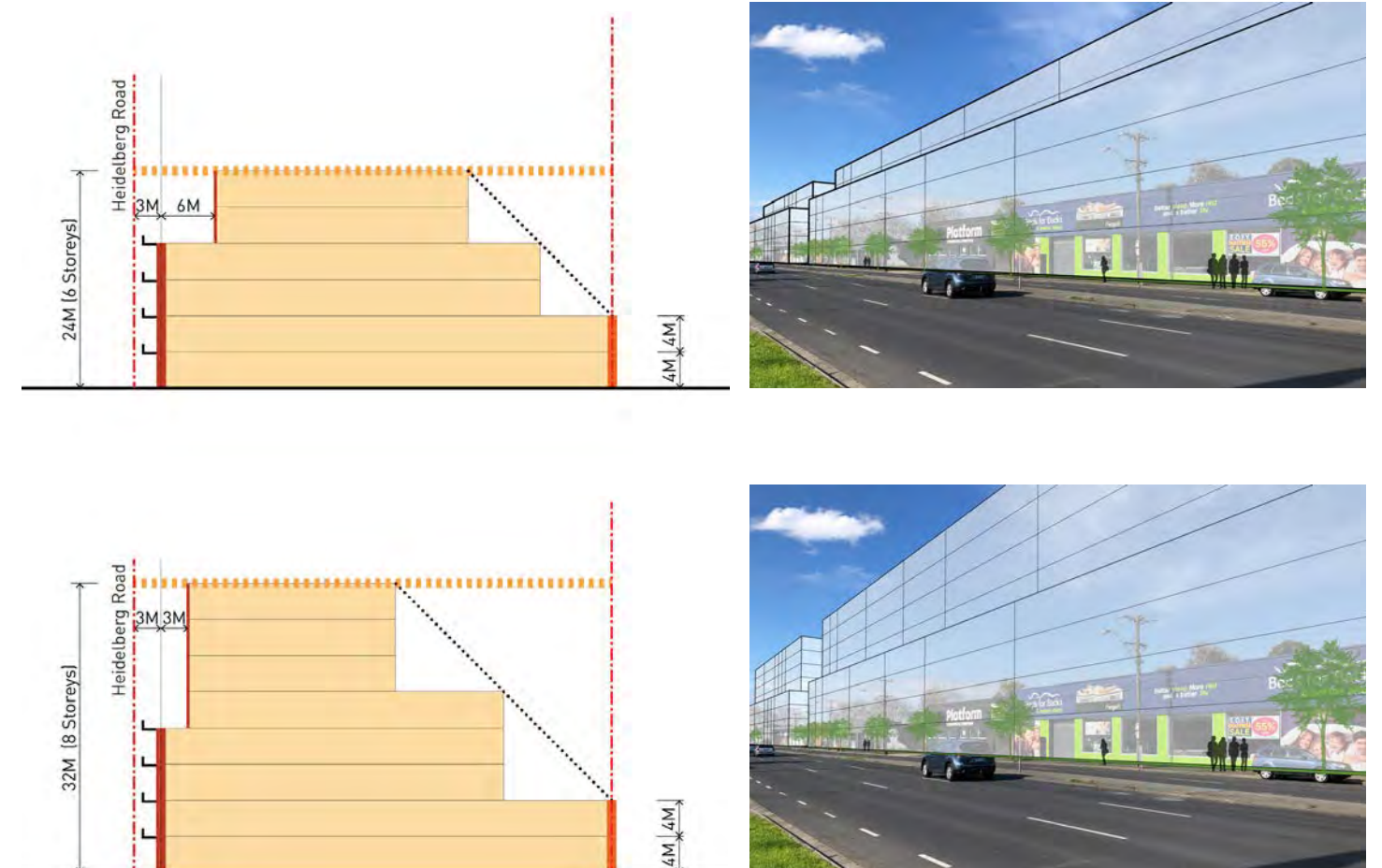


Figure 34. Examples of Heidelberg Road built form testing which considers alternate street and rear interface conditions

3.2 Summary of the key factors determining the preferred building envelope in each precinct

A summary of the key drivers determining the preferred built form controls is illustrated below.

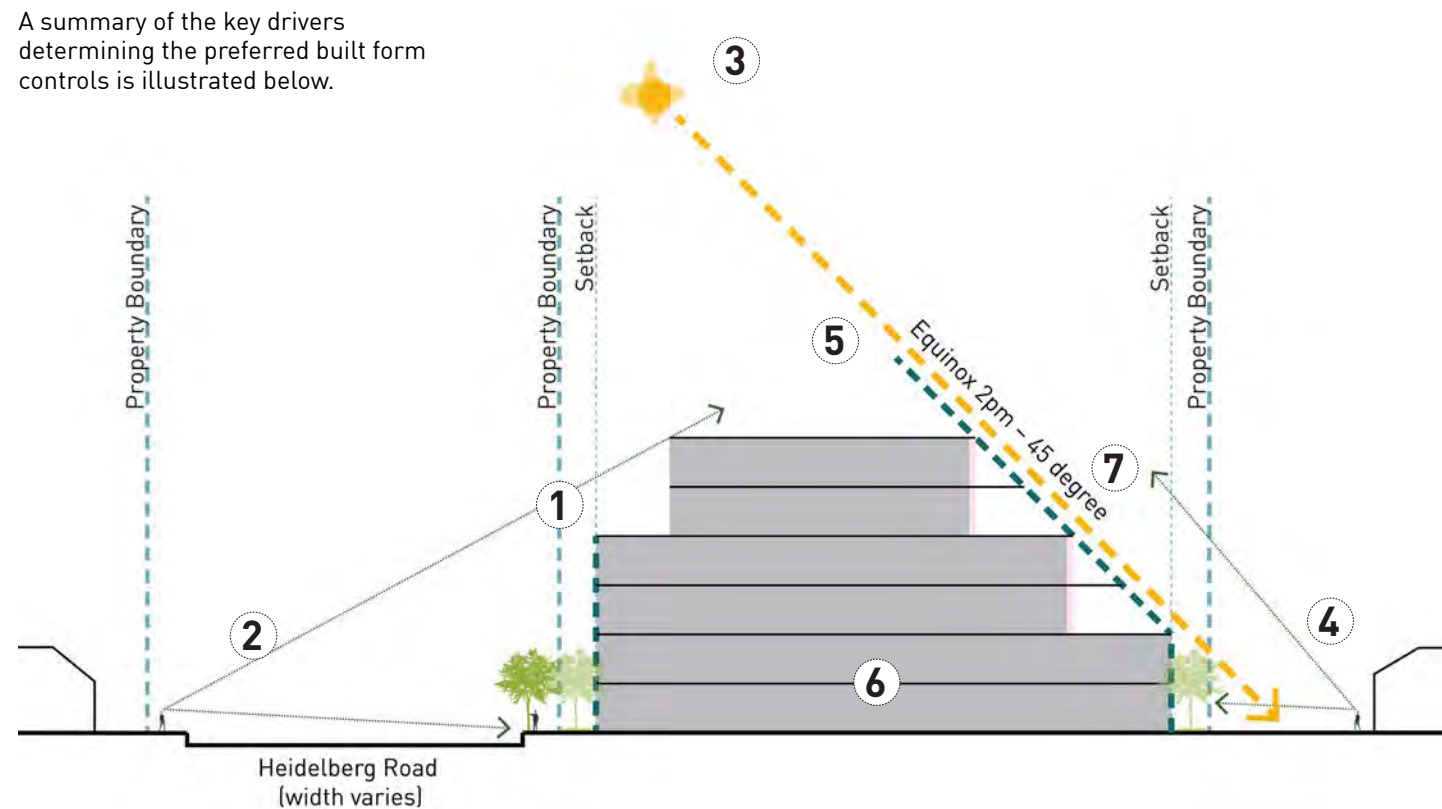


Figure 35. Summary of key drivers determining the development of the built form framework

1. Opportunities to improve the quality of Heidelberg Road for pedestrians identified with proposals to include ground level setbacks.
2. Street wall heights determined by consideration of the experience of the pedestrian in Heidelberg Road. Proposed controls balance the desire to improve street enclosure and definition without creating a 'canyon' effect. This is particularly critical considering the poor quality of the street environment created by the heavy traffic conditions.
3. Sunlight access to private open space protected at the equinox to meet Clause 54 and 55 of the Yarra Planning Scheme.
4. Visual bulk and privacy concerns addressed by two-storey boundary wall height, upper levels setbacks and ground level setbacks (where adjacent dwellings are within 15 metres of the site boundary).
5. Overall building height determined by:
 - Preferred overall scale of development (based on strategic planning context).
 - Preferred character within Heidelberg Road.
 - Mitigating impact of upper levels when viewed from residential sites.
6. Ensure commercial development is supported in the lower two floors of buildings in Commercial 1 Zone and all floors in Commercial 2 zone.
7. Within the rear setback envelope a maximum of 2 steps within the building massing to avoid a 'wedding cake' architectural response.

Built form testing was undertaken for representative sites within each precinct to test the effectiveness of the controls and to illustrate the potential design of new buildings.

Built form testing

The following assumptions have been used to develop and test the built form proposals in this project.

Residential building design

Building depth

Minimum building depth of 10m.

Preferred maximum of 18m. This aligns with the construction of a double-loaded corridor and good provision of natural light to apartments.

Maximum of 24m. This is based on meeting the Better Apartment Design Standards which allows a living room depth (including a kitchen) of 9m and allows for a central corridor (approx. 1.5-2.5 metres) and balconies (min. depth of 1.8m).

Building length

A maximum length of 50m has been adopted to avoid wide, visually dominant or bulky buildings.

Floor to floor heights
4m bottom two floors, 3.2m above.

Floorplates

Minimum of 600sqm to reflect development feasibility (unless site size is smaller, or the floor is the top floor which 'caps' a building)

Maximum floorplates are related to building height to ensure that large towers floorplates are not visually dominant or too bulky.

- Buildings up to 10 storeys - No maximum applied, building designs determined by building depth and length requirements. For example, a L-shaped building could be 50 x 50 metres with a 10-24m building depth.
- Buildings greater than 10 storeys - Not applicable

Floor to floor heights

4m ground floor
3.2m upper levels

Office building design

Building depth

Minimum depth of 10m.

Preferred maximum of 30m to enable good natural daylight to all floors.

Maximum of 50m to avoid wide, visually dominant or bulky buildings.

Building length

A maximum length of 50m has been adopted to avoid wide, visually dominant or bulky buildings.

Floor to floor heights

4m all floors

Development feasibility is considered through assumptions for minimum floorplates and building depths.

Precinct 1 - Yarra Bend

A. Key valued character attributes



Figure 36. Precinct 1 - Aerial image with precinct-specific character attributes identified

1. 262 Heidelberg Road (view from park) - 4 storey interface to the park creates a building scale that is diminutive to the large, existing canopy trees.
2. 262 Heidelberg Road (view from road) - 4 storey interface setback from street by a landscape buffer enhances sense of street definition without creating overly dominant built form. Additional upper floors set back above this height could be accommodated without compromising this outcome.
3. View along Heidelberg Road - existing landscape design is of varying quality, however provides visual relief and additional space for pedestrians within the heavily trafficked street.

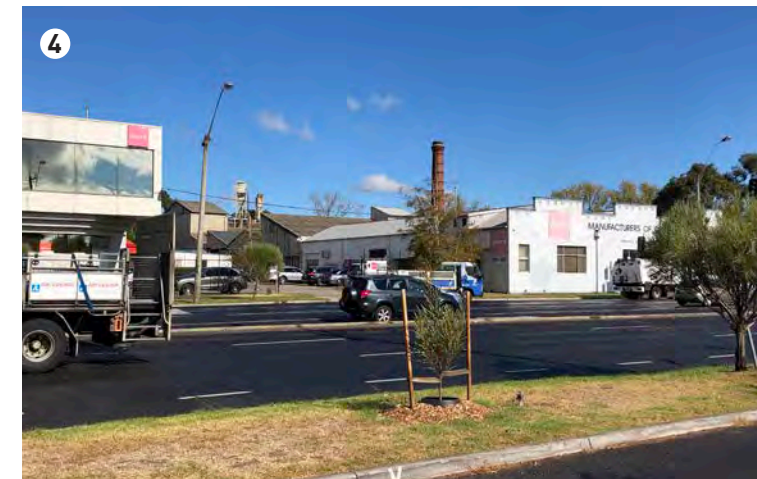


Figure 37. Key character attributes

4. Existing industrial heritage building (Porta), including single storey warehouses and brick chimney.
5. Precinct is surrounded by significant parkland setting, including existing landscape along Yarra Bend Road and expanses of open space.
6. Existing easement which precludes development above.
7. Existing 1970s office building.

B. Precinct specific design strategy

Creation of a mid-rise precinct that frames Heidelberg Road and steps down towards the adjacent parks to maintain the prominence of the landscape setting. The Porta heritage building is retained, views to the brick chimney are enhanced through sensitive redevelopment and a new north-south pedestrian connection links Heidelberg Road to the park.

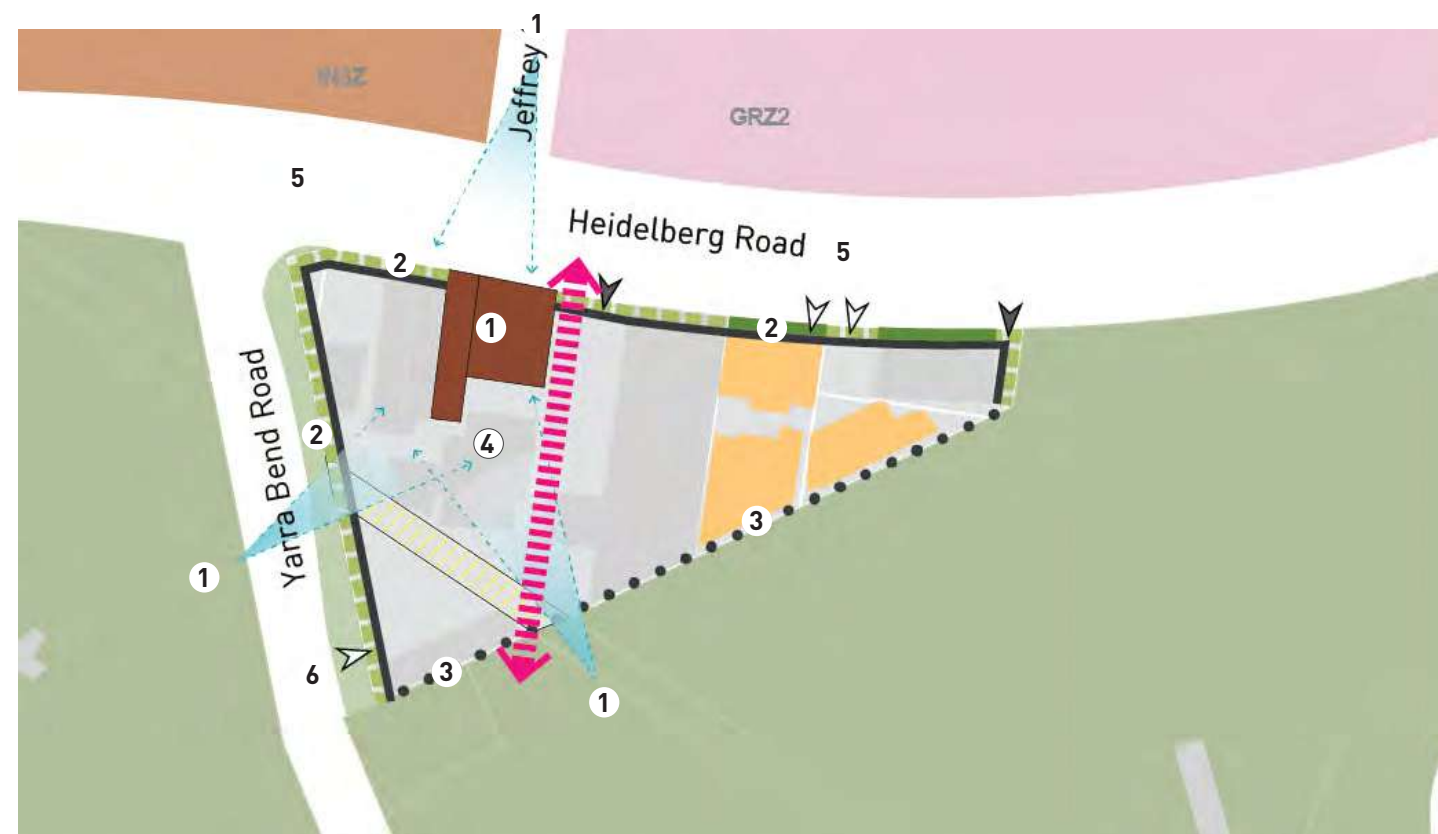
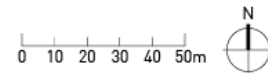


Figure 38. Design Strategy

- Existing heritage buildings
- Existing medium-density, mid-rise housing
- Existing vehicular access (retained/consolidated)
- Vehicular access (removal preferred)
- Existing landscape character and landscape setback (retained)
- Proposed 3m landscape setback
- Proposed 4 storey building height at interface to park
- Create urban street wall and activated edges along Heidelberg Road
- Proposed future public pedestrian link
- Yarra Valley Water easement
- View lines to chimney from Jeffrey Street & adjacent parks



Precinct-specific design objectives

Respect and enhance the setting of the Porta heritage building and brick chimney by framing the building with mid-rise development (4-8 storeys) - Location 1.

The Porta site includes a significant heritage warehouse building and a brick chimney which is an identifiable landmark in the precinct viewed from within the park and from Jeffrey Street. These are important attributes of the existing character, providing a connection to the social and economic history of the area and should be retained and adapted for re-use.

Views to the chimney from within Jeffrey Street and the park should be provided to maximise opportunities for the broader public to view and enjoy the heritage attributes of the site. Sufficient separation distances from the chimney to other new buildings should be provided to ensure that the chimney remains a prominent feature within the site. The overall scale of new development respects these existing heritage qualities and responds to the scale and features of the existing heritage building (refer to Figure 48).

Improve the pedestrian experience on Heidelberg Road and Yarra Bend Road through a 3 metre front setback - Location 2.

The existing landscape setback within the front of some properties improves the quality of the pedestrian experience by greening the otherwise largely asphalt landscape and by providing additional sense of openness/relief for pedestrian movement.

Provide a positive interface (visual interest and passive overlooking) to the park edges in a building scale that does not visually dominate or unreasonably overshadow TH Westfield Reserve and Yarra Bend Park Oval - Location 3.

Precinct 1 is located directly onto TH Westfield Reserve and new development must not unreasonably overshadow the park. The park area is significant in size and the area immediately to the south of the private land is currently an asphalt car park (i.e. a less sensitive use).

The existing 4 storey developments at 262 & 264 Heidelberg Road are successful demonstrations of an appropriately scaled building to the park edge.

A four-storey high building creates a positive interface to the park - it doesn't visually dominate the landscape setting - large canopy trees and open grassed areas retain their prominence - and the inclusion of balconies and doors to the park provides visual interest and improves safety.

Additional upper levels above 4 storeys should not increase overshadowing impacts onto the park.

Provide a diverse range of housing types on the strategic development site (Porta site) - Location 4.

The Porta site provides the opportunity to deliver a greater diversity of housing than is possible on other sites in the study area, many of which have significant site constraints. The inclusion of multiple buildings with internal communal courtyards to support high quality mid-rise developments is strongly encouraged.

Improve the character of Heidelberg Road by creating a comfortable sense of enclosure and definition to the street - Location 5.

This can be achieved through the introduction of a street wall height that provides a positive interface to the street but which does not visually dominate. This balance is particularly important to achieve considering the poor quality of the street environment. Buildings that are visually overwhelming will exacerbate the impact of heavy traffic on the pedestrian experience.

Minimise the impact of vehicular crossovers to Heidelberg Road and Yarra Bend Road - Location 6.

Vehicular access to most sites is provided from Heidelberg Road. This includes shared access for a number of sites. No additional vehicular crossovers are supported.

Precinct 1 - Yarra Bend

C. Determining Heidelberg Road development scale

Heidelberg Road varies in width along its length. In Precinct 1 it is in the order of 40 metres wide.

Precinct 1 includes a strategic site (the Porta site) and areas where a 'moderate scale' of development is supported through existing planning policy. The existing four-storey apartment building demonstrates the benefit of increasing the street wall height to provide greater definition to the street.

A range of scenarios for potential street wall and overall building heights have been tested, including:

8 storey developments with:

- 4 storey street wall and 3 metre upper level setback.
- 6 storey street wall with 3 metre setback.
- 6 storey street wall with 6 metre setback.
- 8 storey street wall height

6 storey developments with:

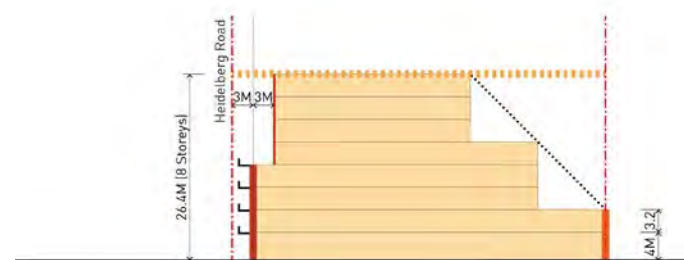
- 4 storey street wall and 3 metre upper level setback.
- 6 storey street wall with 3 metre setback.
- 6 storey street wall with 6 metre setback.

In each option, the 3 metre ground level front setback has been adopted.

An assessment of each option is provided against the design principles. The scenario that best delivers the design principles is the 8 Storey high building - Option 3. This includes a varied 4 - 6 storey street wall with upper 2 storeys set back by 6 metres.

8 Storeys - Option 1

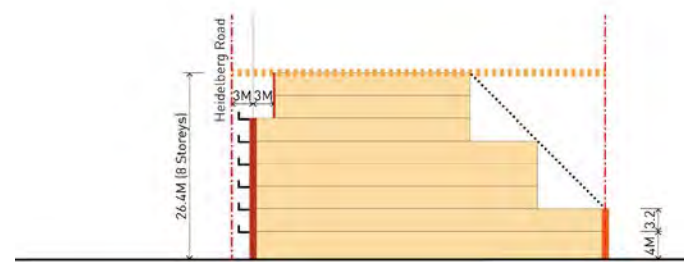
Street wall: 4 storeys
Upper level setback: 3 metres



- Effectively frames Heidelberg Road with the potential for high-quality development.
- Provides an improved sense of enclosure within the street due to upper levels. The four-storey street wall could be considered too low considering the wide road width.
- Creates an uncomfortably proportioned building where the lower and upper levels are of equal heights.

8 Storeys - Option 2

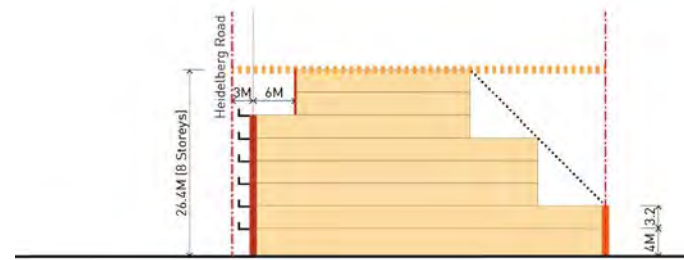
Street wall: 6 storeys
Upper level setback: 3 metres



- Effectively frames Heidelberg Road with the potential for high-quality development.
- The six-storey street wall provides a more balanced street wall height against the wide street.
- Supports the design of well-proportioned buildings where the upper levels are a recessed, lighter element above a stronger base building form.

8 Storeys - Option 3

Street wall: 4-6 storeys
Upper level setback: 6 metres



Preferred development outcome

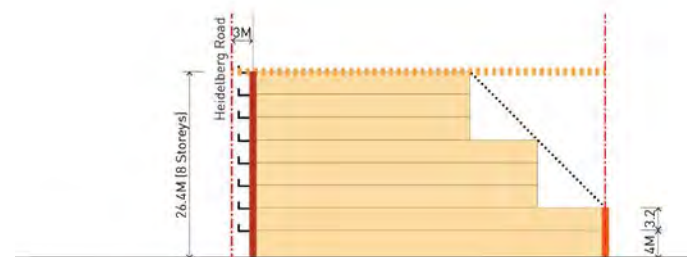
- Effectively frames Heidelberg Road with the potential for high-quality development.
- The six-storey street wall provides a more balanced street wall height against the wide street.
- Including 4 storey elements provides better integration with existing apartment building.
- Supports the design of well-proportioned buildings where the upper levels are a recessed, lighter element above a stronger base building form. The increased upper level setback to 6 metres provides a marginal improvement on the 3 m setback as the base building form becomes more prominent and the upper levels less visible.

Figure 39. Testing of 8 storey developments with varied street wall heights and upper level street setbacks.

Precinct 1

8 Storeys - Option 4

Street wall: 8 storeys
Upper level setback: N/A

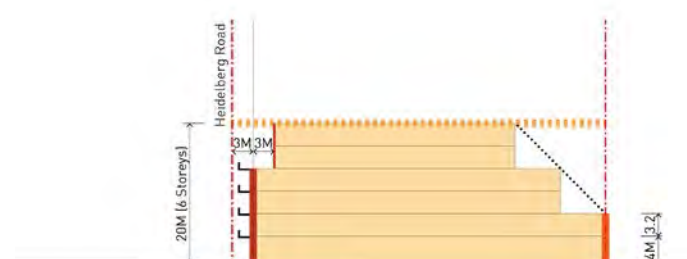


- The eight-storey street wall is too visually dominant. Together with the high levels of traffic this could create a poor quality public realm.

Figure 40. Testing of 8 storey developments with varied street wall heights and upper level street setbacks.

6 Storeys - Option 1

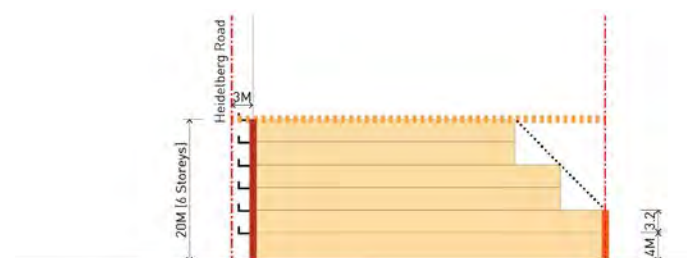
Street wall: 4 storeys
Upper level setback: 3 metres



- Effectively frames Heidelberg Road with the potential for high-quality development.
- Provides an improved sense of enclosure within the street due to upper levels. The four-storey wall and overall six storey building height could be further increased on the 40m wide road corridor.
- Supports the design of well-proportioned buildings where the upper levels are a recessed, lighter element above a stronger base building form.

6 Storeys - Option 2

Street wall: 6 storeys
Upper level setback: N/A



- Effectively frames Heidelberg Road with the potential for high-quality development.
- The six-storey street wall provides a more balanced street wall height against the 40m wide street.
- Additional upper levels could be included and support the design of well-proportioned buildings as long as the base building remained prominent to support the delivery of a mid-rise building character.

Figure 41. Testing of 6 storey developments with varied street wall heights and upper level street setbacks.

Key recommendation

Introduce an 8 storey building height control in Precinct 1 with a varied 4 - 6-storey street wall height and upper levels to be setback 6 metres.

The sites at 274 - 276 cannot achieve an 8 storey height as the sites are too shallow. The application of the rear interface control mean that a six storey height can be achieved and is therefore proposed for these two sites.

Precinct 1 - Yarra Bend

C. Determining Heidelberg Road development scale

The proposed relationship to Heidelberg Road of the proposed built form outcome is illustrated below.

This demonstrates a balanced degree of enclosure to the 40 metre wide street, without creating visually dominant buildings. It also illustrates the benefit of the 3 metre front setback in improving the composition of the street and the quality of the pedestrian environment at ground level.

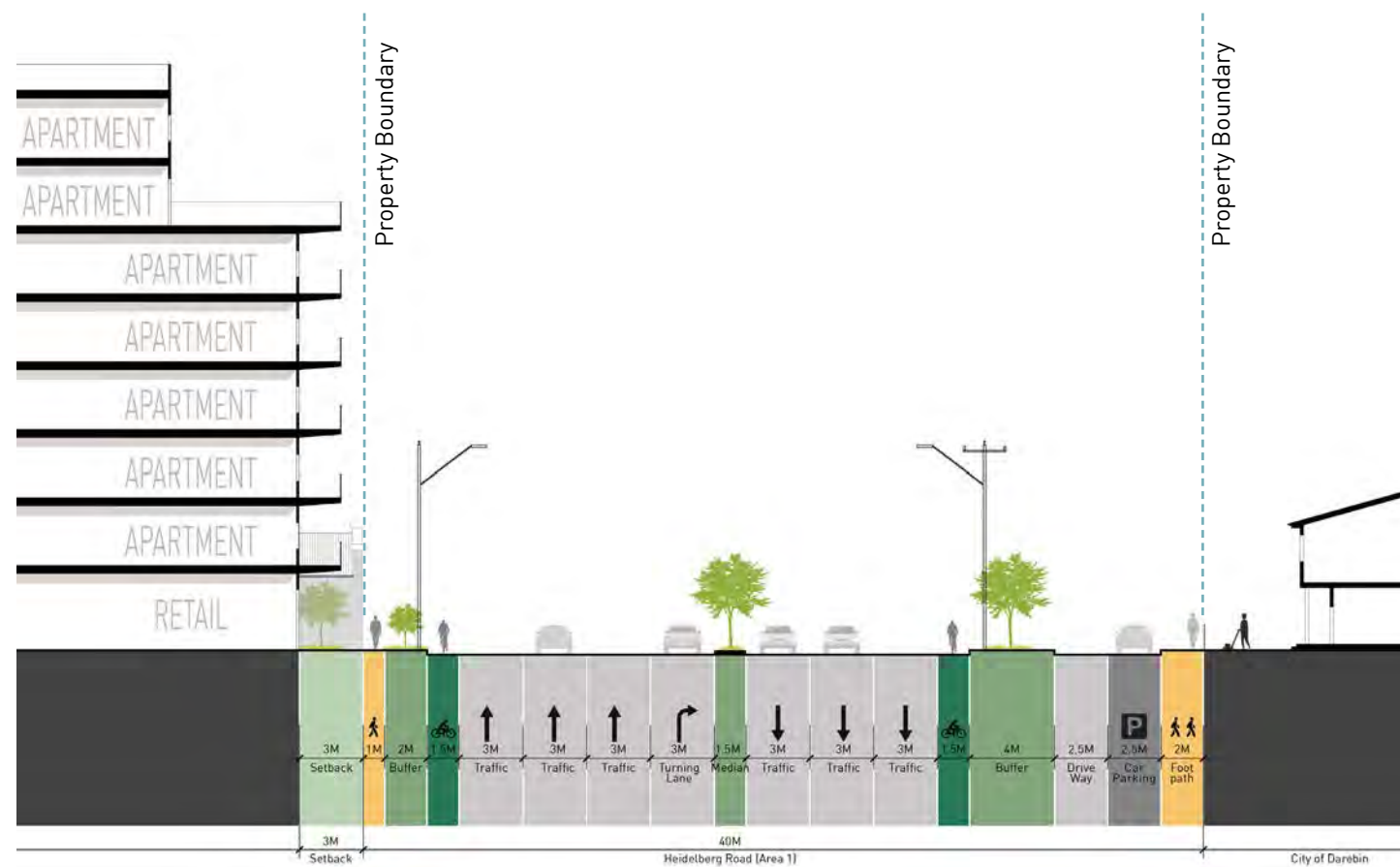


Figure 43. Proposed interface to Heidelberg Road - full street section

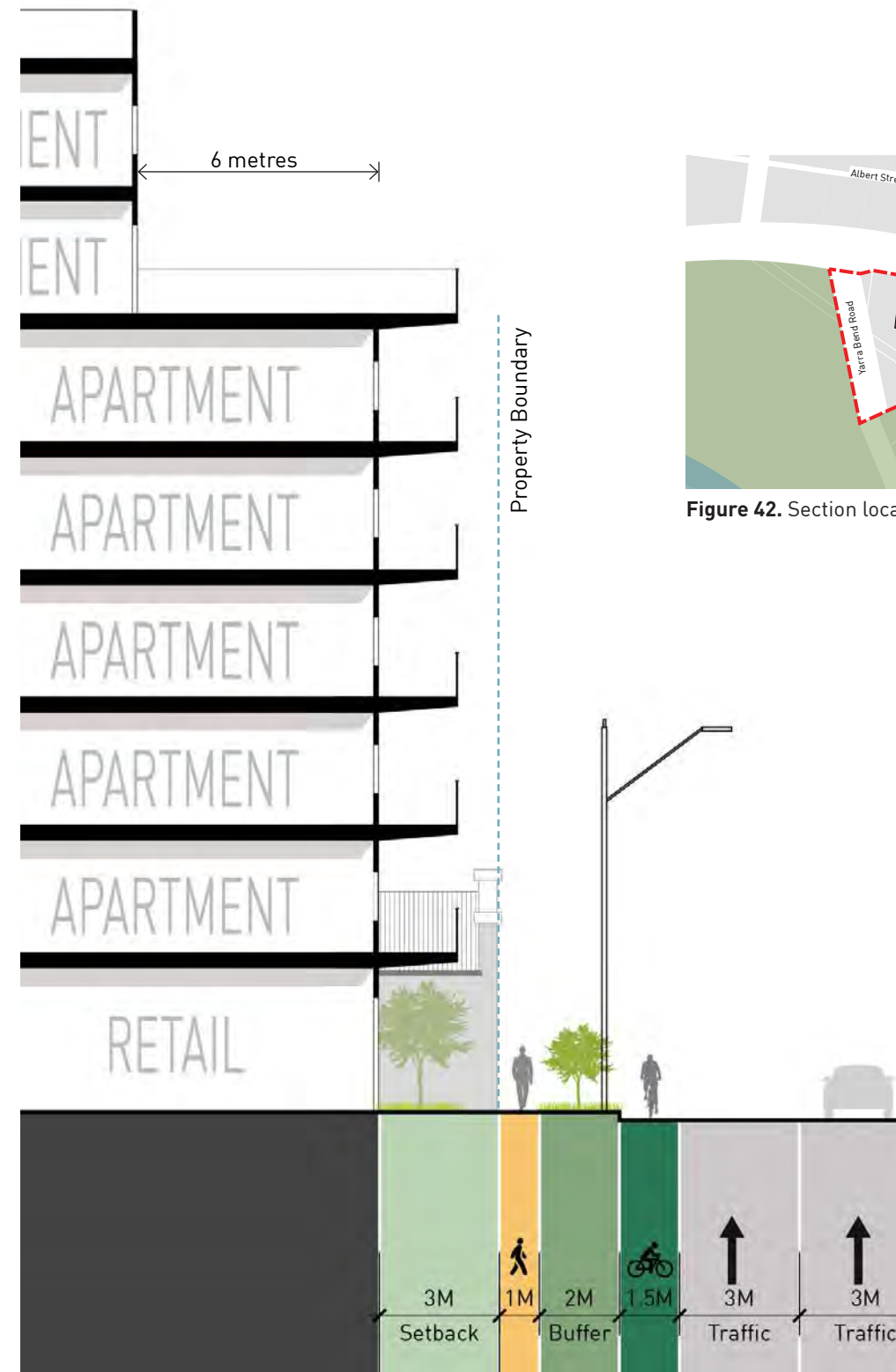


Figure 44. Proposed interface to Heidelberg Road - detailed street section



Figure 42. Section location plan

D. Determining development scale to park interface

The following outcomes are considered acceptable to meet the design principles and precinct-specific design objective:

- Overshadowing falls predominantly on the car park to the south and Yarra Bend Road reserve (see Figure 45).
- An appropriate balance between providing an urban edge and activation and overlooking of the car park area and ensuring that the buildings are set within the landscape and are not the dominant feature - this is demonstrated effectively by the existing four-storey apartment development which sits within the scale of the large canopy trees.

The preferred development scale that achieves this outcome is:

- A 4 storey building height along the park interface
- Above 4 storeys, upper level setbacks are determined by a 45 degree angle.

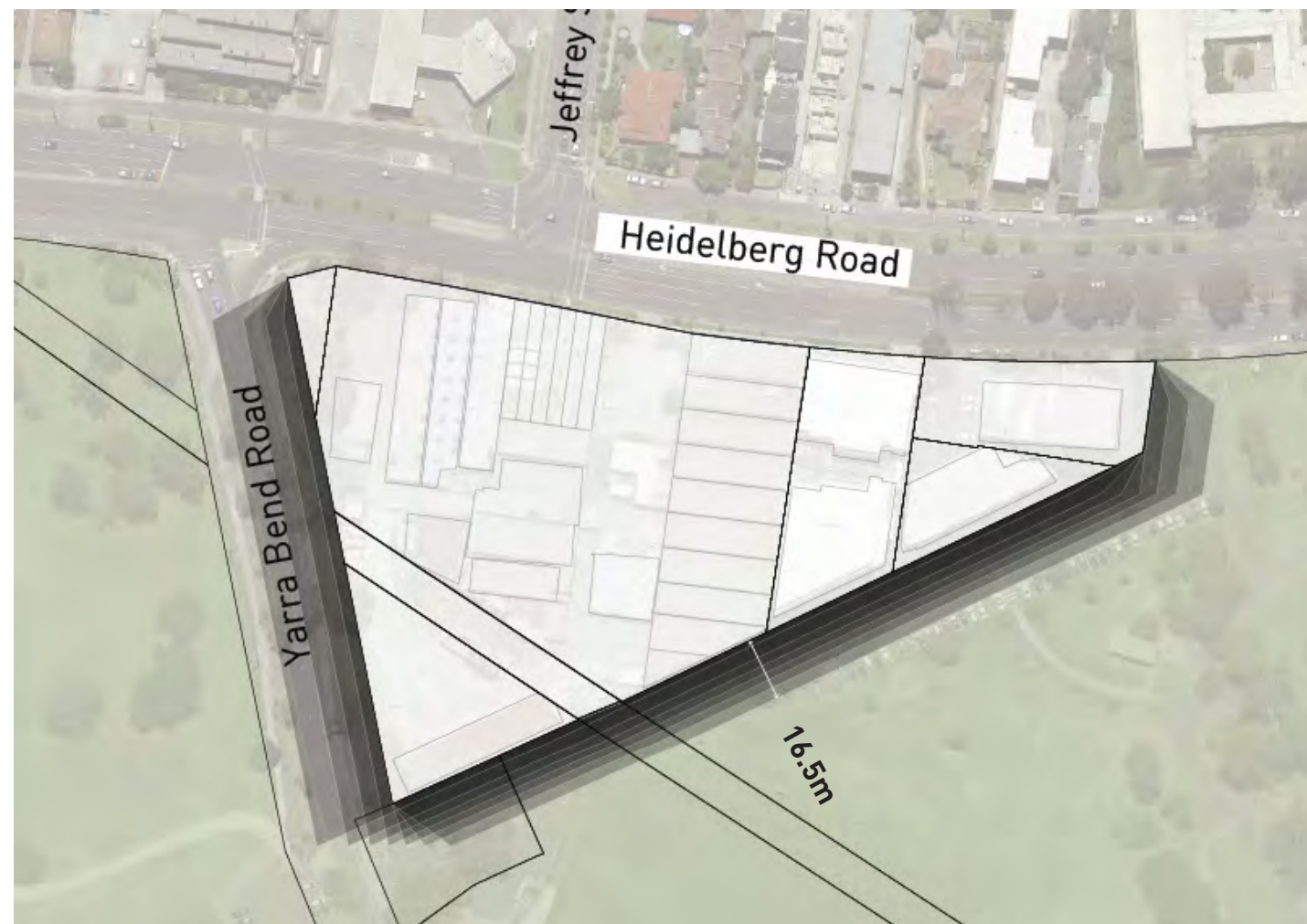


Figure 45. Extent of shadow for a 4 storey high building.

Precinct 1 - Yarra Bend

E. Building envelope controls

The following building envelopes are proposed for Precinct 1.

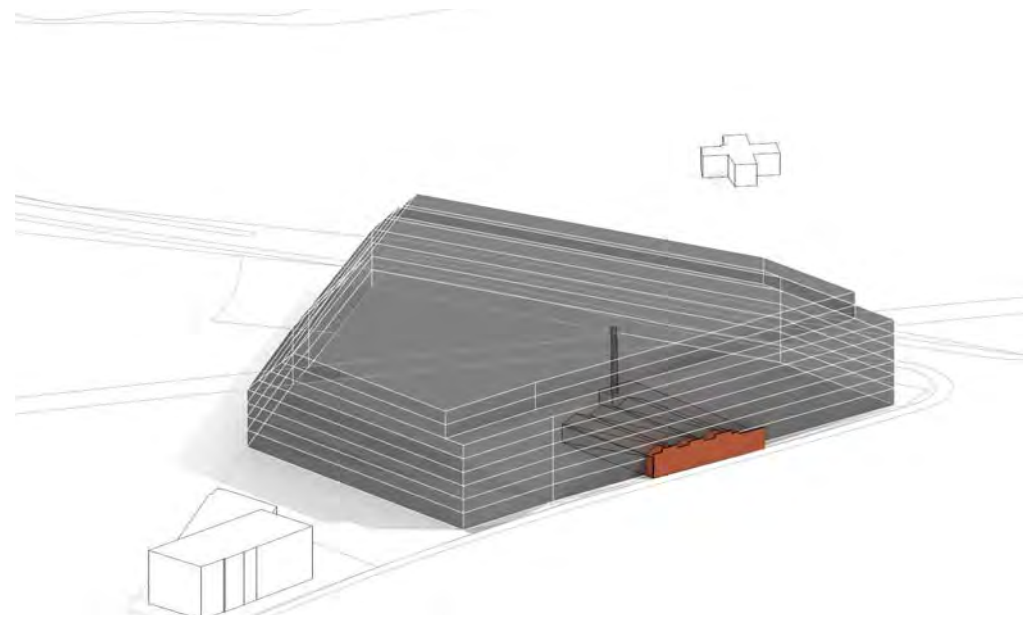


Figure 46. Demonstration of the 3d building envelope controls applied to the Porta site
*Note: the full heritage building is to be retained. Envelope for the whole site shown for illustrative purposes only.

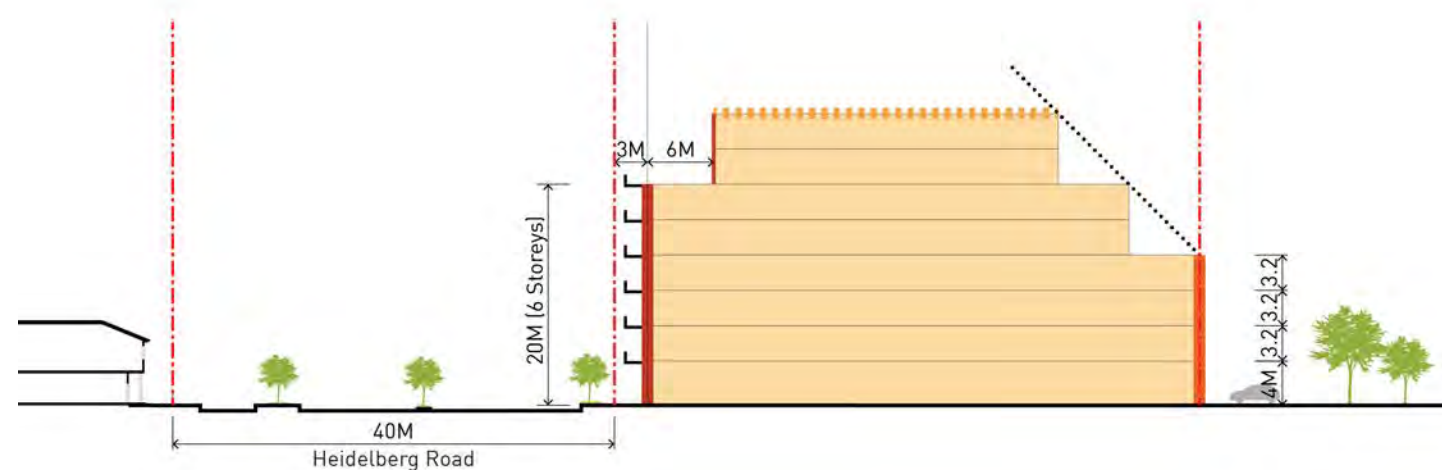


Figure 47. Proposed built form envelope controls (section)



The building envelope controls have been determined considering the overall precinct conditions. The existing heritage building warrants a more tailored response to the street wall condition to ensure that any proposed new development respects the existing qualities and presence of the heritage building. A step down in street wall height and introduction of a new public pedestrian link through the site will give the heritage building more prominence.

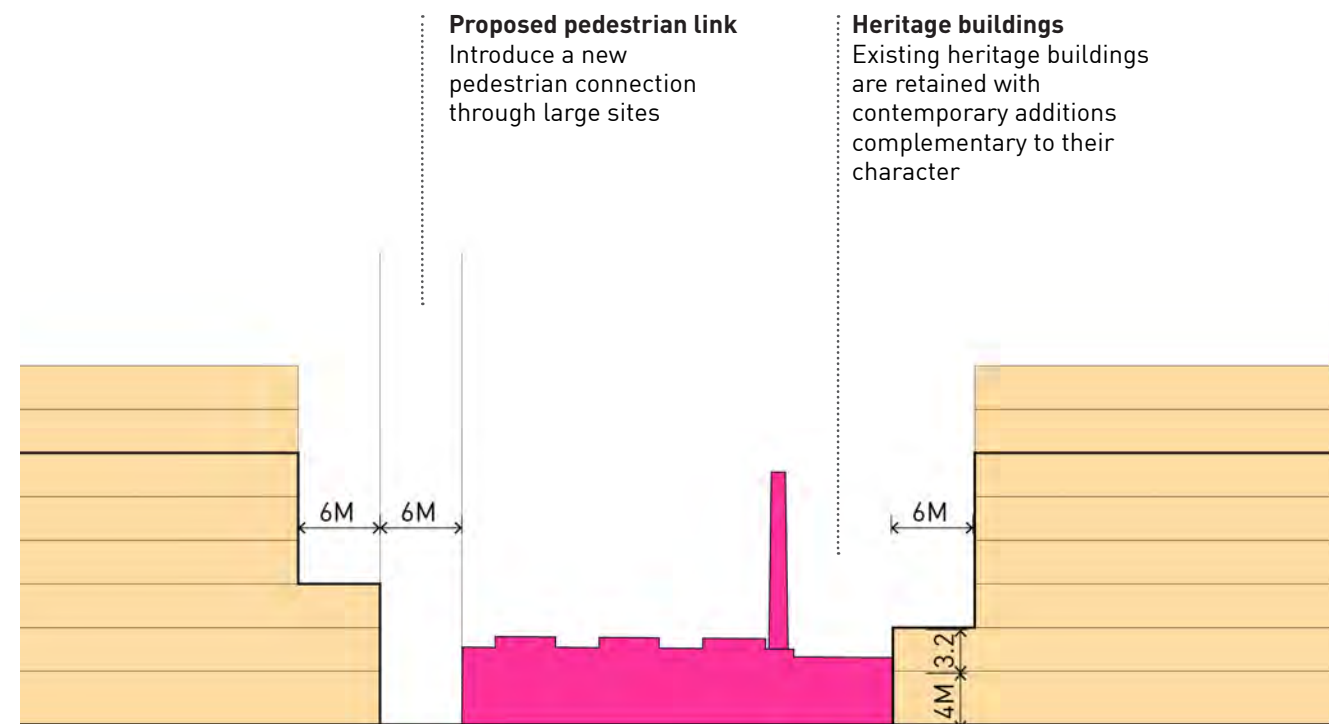


Figure 48. Proposed built form envelopes (elevation) in response to existing heritage building

F. Precedent examples - Precinct 1



Figure 49. Proposal for 342-348 Victoria Street - Brunswick (Source: Fieldworks Architects)



Figure 50. Proposal for 342-348 Victoria Street - Brunswick (Source: Fieldworks Architects)



Figure 51. Hawke & King Street development, North Melbourne (Source: Six Degrees Architects)

Precinct 1 - Yarra Bend

G. Built form testing of proposed building envelopes

Testing site

Built form testing has been undertaken for the Porta site to both assess and communicate the proposed built form controls.

Additional sensitivity testing of taller forms were also assessed (see figures 57 - 65). Taller forms above 8 storeys are considered to be too visually dominant adjacent to the existing heritage chimney. 10 storey buildings become too visually dominant when viewed from within Heidelberg Road.

Location:
224-256 Heidelberg Road

Site area:	Lot width:	Lot depth:	Characters:
11,725m ²	125M	40-135M	Heritage overlay Include easement

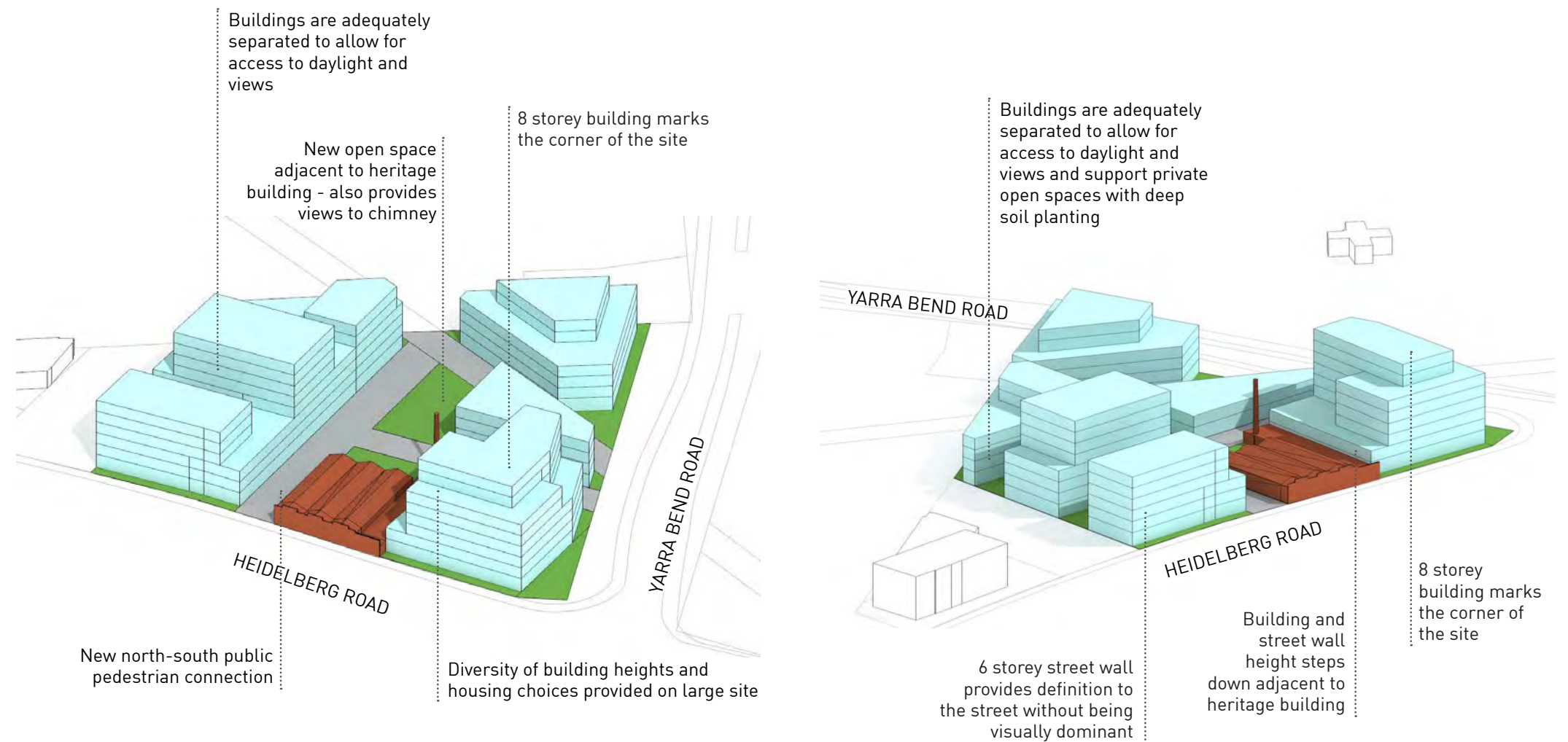


Figure 52. Built form testing - perspective views

Precinct 1

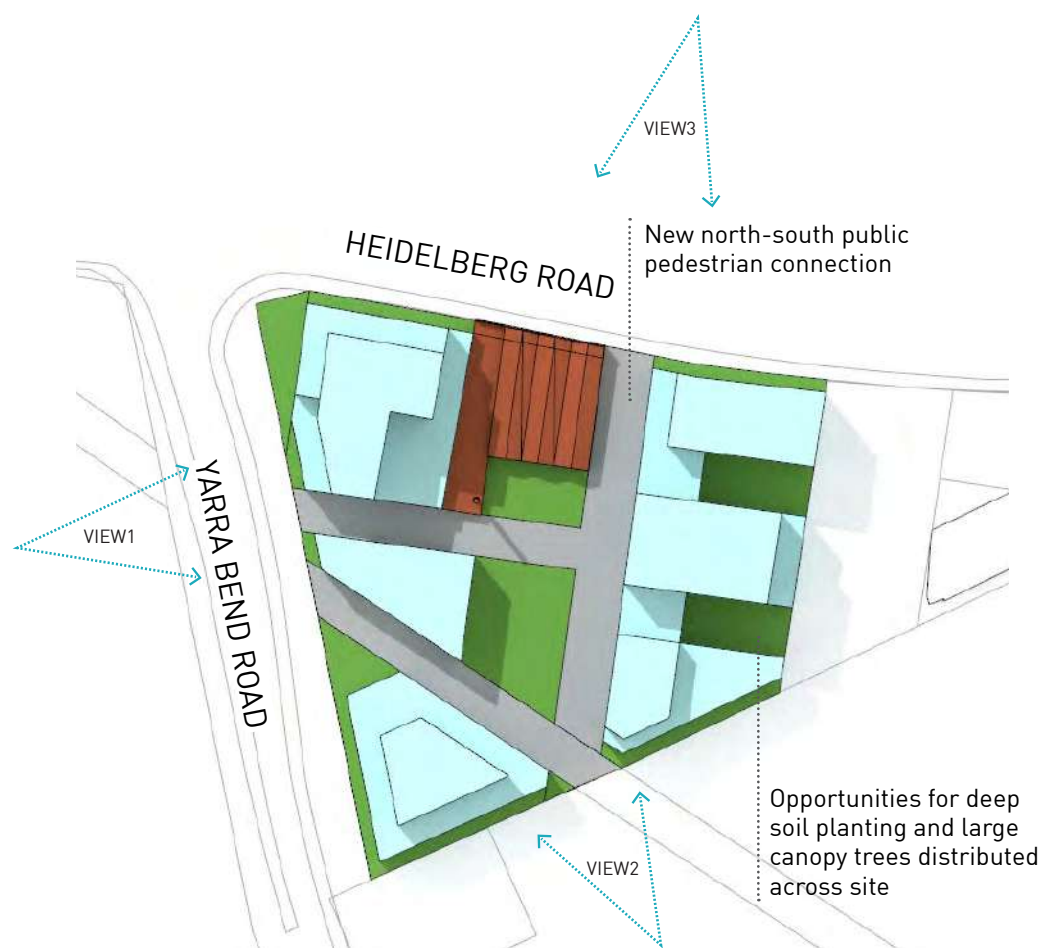


Figure 53. Built form testing - plan view with 2pm shadow at the equinox

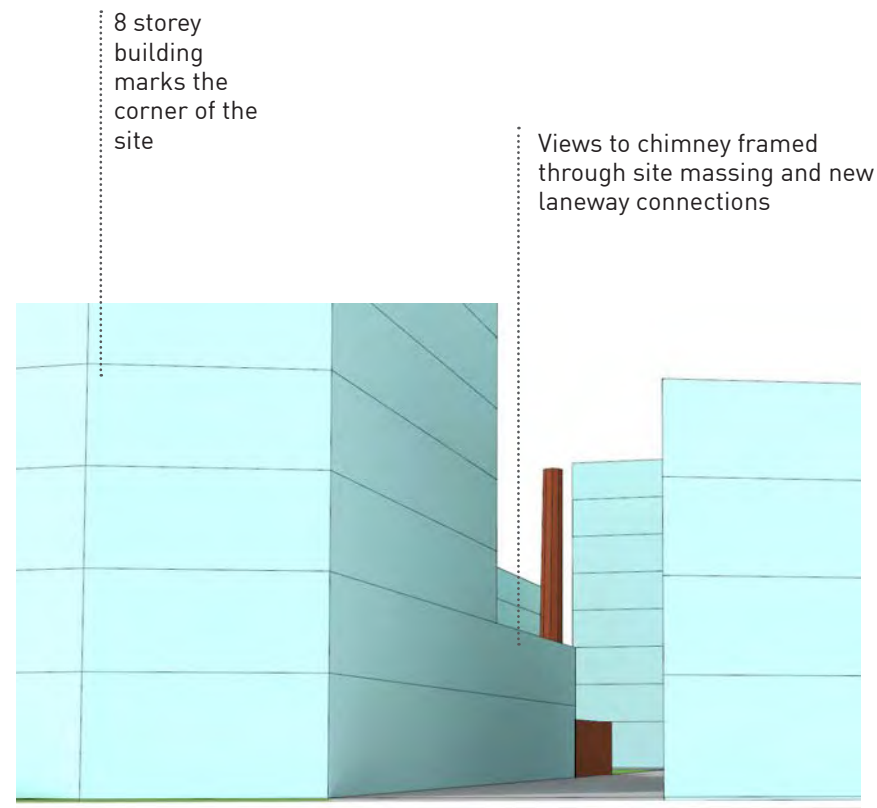


Figure 54. View 1 - View to chimney from TH Westfield Reserve

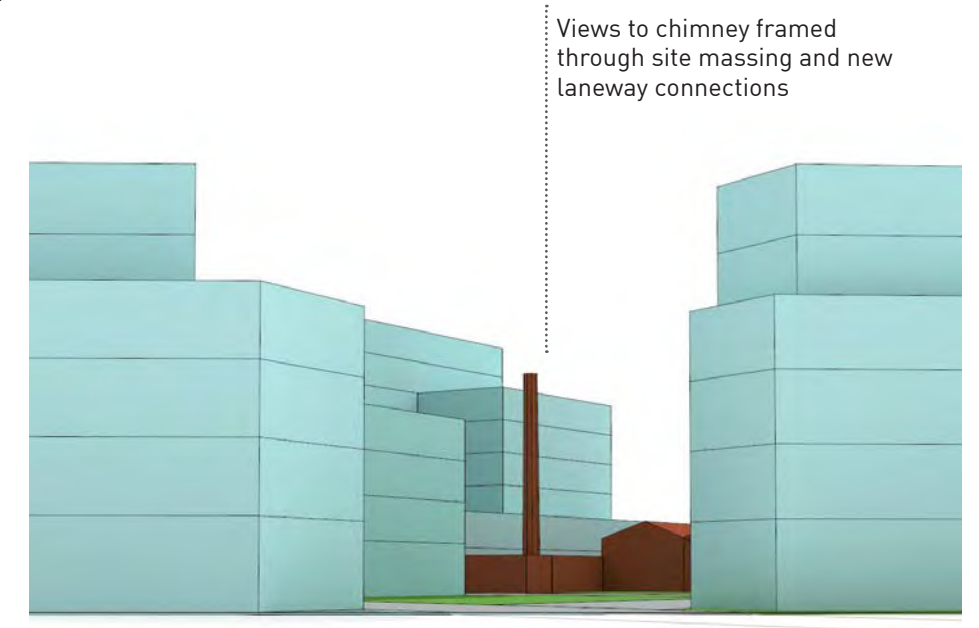


Figure 55. View 2 - View to chimney from Yarra Bend Park

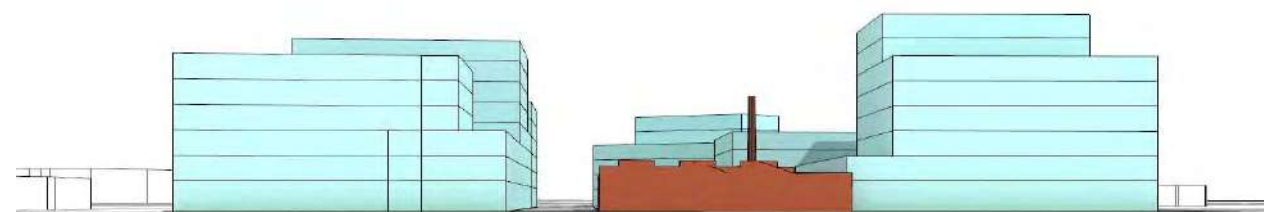


Figure 56. View 3 - View to chimney from Jeffrey Street retains prominence

Precinct 1 - Yarra Bend



Figure 57. Perspective view

Composition of heritage building and surrounding new development is in balance.



Figure 58. View A - View from Jeffrey Street



Figure 59. View from location B



Figure 60. Perspective view

New development is visually dominant over the heritage building.



Figure 61. View A - View from Jeffrey Street



Figure 62. View from location B



Figure 63. Perspective view

New development is visually dominant over the heritage building.



Figure 64. View A - View from Jeffrey Street



Figure 65. View from location B

H. Proposed built form controls

The proposed building envelope controls for Precinct 1 are illustrated in the following plans.

Considering the unique site attributes and the need to support design flexibility with certainty that minimum amenity standards are met, a mix of mandatory and discretionary controls are proposed as follows:

Discretionary

- Overall height limits
- Street wall heights to Heidelberg Road and Yarra Bend Road
- Upper level setbacks from Heidelberg Road and Yarra Bend Road

Mandatory

- 3 metre front setback to Heidelberg Road, Yarra Bend Road and the park.
- 4 storey building height at the park interface with all upper levels setback with a 45 degree angle.



Figure 66. Precinct 1 - Proposed overall building heights



Figure 67. Precinct 1 - Proposed street wall heights / building heights along park interface boundaries



Figure 68. Precinct 1 - Proposed ground floor setbacks

Precinct 2 - Fairfield Commercial

A. Key valued character attributes



Figure 69. Precinct 2 - Aerial image

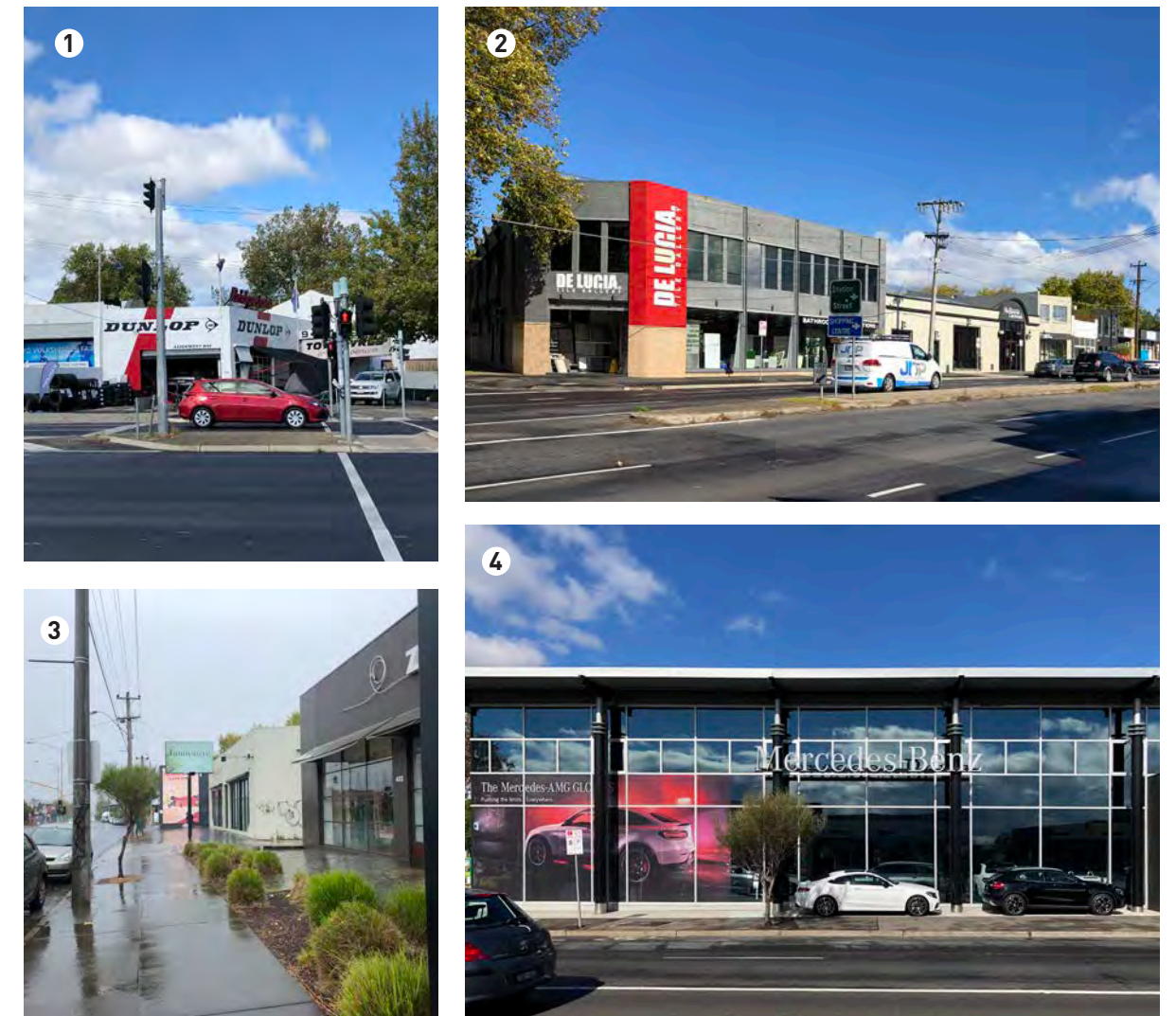


Figure 70. Key character attributes

The Commercial 2 zone area along Heidelberg Road plays an important economic role within the City of Yarra. The existing businesses include large format retail outlets, automotive businesses and warehouses. The existing character reflects this pattern of use.

1. Potential heritage building (including automotive business).
2. Large format showrooms which support the cluster of restricted retail outlets.
3. View along Heidelberg Road - existing landscape design is of varying quality, however provides visual relief and additional space for pedestrians within the heavily trafficked street.
4. Large format showrooms which incorporate large glass shopfront areas and contemporary building design.
5. Residential side streets, including large mature trees and significant setbacks.

B. Design Strategy

Creation of a mid-rise, commercial precinct that frames Heidelberg Road with active uses and additional greening opportunities.

Design Objectives

Improve the pedestrian experience on Heidelberg Road through a 3 metre front setback - Location 1.

The existing landscape setback within the front of some properties improves the quality of the pedestrian experience by greening the otherwise largely asphalt landscape and by providing additional sense of openness/relief for pedestrian movement.

Improve the character of Heidelberg Road by creating a comfortable sense of enclosure and definition to the street - Location 2.

This can be achieved through the introduction of a street wall height that provides a positive interface to the street but which does not visually dominate. This balance is particularly important to achieve considering the poor quality of the street environment. Buildings that are visually overwhelming will exacerbate the impact of heavy traffic on the pedestrian experience.

Ensure development does not visually dominate or unreasonably overshadow private open space in adjacent residential areas - Location 3.

The neighbouring residential properties all incorporate private open space at the rear of each dwelling. Sunlight should be provided at the equinox according to the current level of provision required in Clause 54 and 55 of the planning scheme.

Minimise the impact of vehicular crossovers to Heidelberg Road and Yarra Bend Road - Location 4.

Vehicular access to most sites is provided from Heidelberg Road. This includes shared access for a number of sites. No additional vehicular crossovers are supported. Where possible vehicular access from residential side streets should be provided.



Figure 71. Design Strategy

- Potential heritage buildings
 - Existing vehicular access (retained/consolidated)
 - Vehicular access (removal preferred as alternate access is available)
 - Existing landscape setback (retained)
 - Proposed 3m landscape setback
 - Create urban street wall and activated edges along Heidelberg Road
- Minimise visual bulk, overshadowing and privacy on the sensitive interface:
- Rear to rear boundary condition
 - Rear to side boundary condition
 - Side to side boundary condition

Precinct 2 - Fairfield Commercial

C. Determining Heidelberg Road development scale

Heidelberg Road varies in width along its length. In Precinct 2 it reduces to approximately 27 metres in width.

A 'moderate scale' of development is supported through existing planning policy.

The potential street wall and overall building heights are tested. This demonstrates the following scenarios:

8 storey developments with:

- 4 storey street wall and 3 metre upper level setback.
- 6 storey street wall with 3 metre setback.
- 6 storey street wall with 6 metre setback.
- 8 storey street wall height.

6 storey developments with:

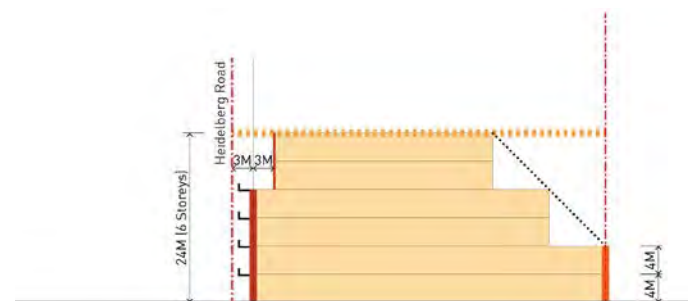
- 4 storey street wall and 3 metre upper level setback.
- 4 storey street wall with 6 metre setback.
- 6 storey street wall height.

In each option, the 3 metre ground level front setback has been adopted.

An assessment of each option is provided against the design principles. The scenario that best delivers the design principles is 6 Storey - Option 2. This includes a 4 storey street wall with upper 2 storeys set back 6 metres.

6 Storeys - Option 1

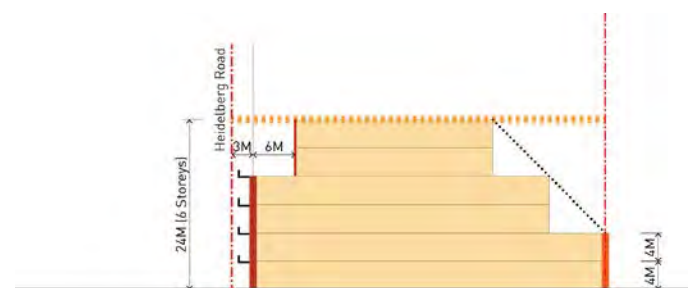
Street wall: 4 storeys
Upper level setback: 3 metres



- Effectively frames Heidelberg Road with the potential for high-quality development.
- The 4-storey street wall provides a balanced street wall height against the street width.
- The 3 metre setback provides a negligible improvement on reducing the upper level dominance.

6 Storeys - Option 2

Street wall: 4 storeys
Upper level setback: 6 metres

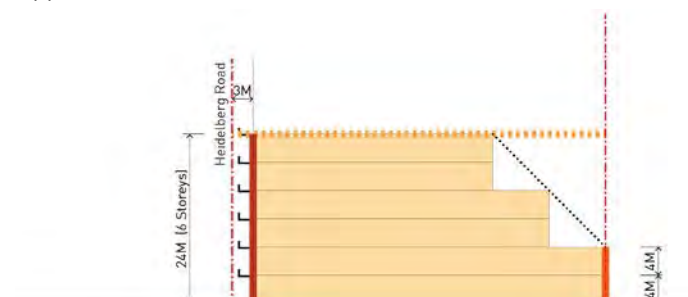


Preferred development outcome

- Effectively frames Heidelberg Road with the potential for high-quality development.
- The 4-storey street wall provides a balanced street wall height against the street width.
- Supports the design of well-proportioned buildings where the upper levels are a recessed, lighter element above a stronger base building form. The increased upper level setback to 6 metres provides a marginal improvement on the 3 m setback as the base building form becomes more prominent and the upper levels less visible.

6 Storeys - Option 3

Street wall: 6 storeys
Upper level setback: N/A

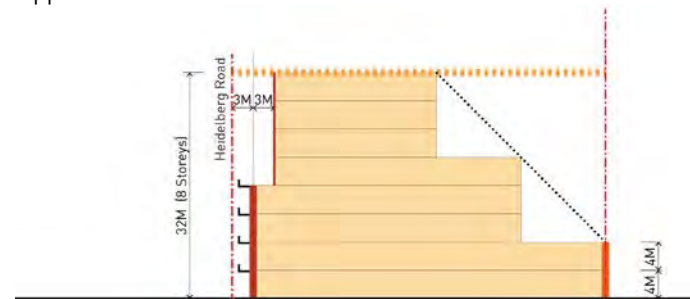


- The 6-storey street wall is too visually dominant. Together with the high levels of traffic this could create a poor quality public realm.

Figure 72. 6 storey built form testing to Heidelberg Road

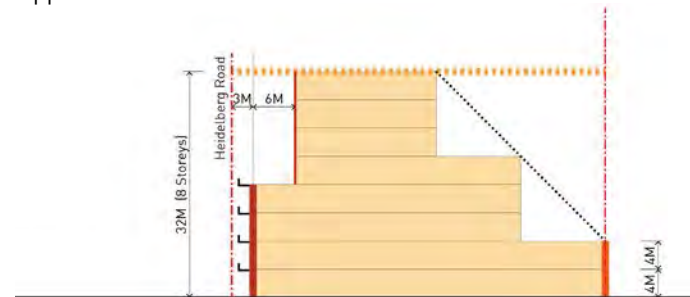
8 Storeys - Option 1

Street wall: 4 storeys
Upper level setback: 3 metres



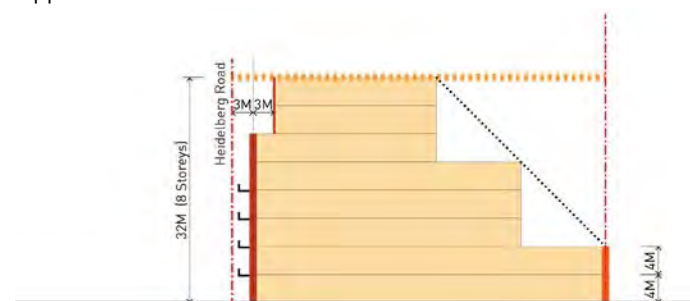
8 Storeys - Option 2

Street wall: 4 storeys
Upper level setback: 6 metres



8 Storeys - Option 3

Street wall: 6 storeys
Upper level setback: 3 metres



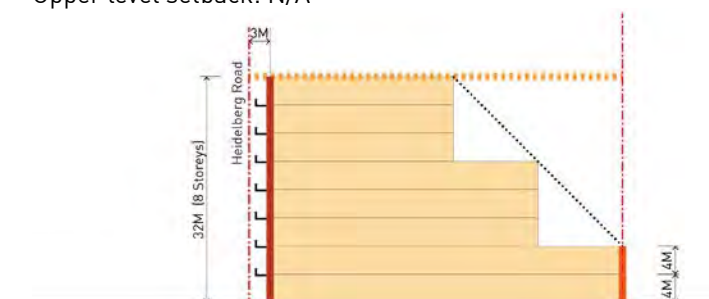
- 8 storey buildings are visually dominant, creating a wall of development.
- Creates an uncomfortably proportioned building where the lower and upper levels are of equal heights.

- 8 storey buildings are visually dominant.
- The 6 metre setback provides a negligible improvement on reducing this dominance.
- Creates an uncomfortably proportioned building where the lower and upper levels are of equal heights.

- The 6-storey street wall is too visually dominant. Together with the high levels of traffic this could create a poor quality public realm.

8 Storeys - Option 4

Street wall: 8 storeys
Upper level setback: N/A



- The eight-storey street wall is too visually dominant. Together with the high levels of traffic this could create a poor quality public realm.

Key recommendation

Introduce a 6-storey building height control in Precinct 2 with a 4-storey street wall height and upper 2 levels to be setback 6 metres.

Figure 73. 8 storey built form testing to Heidelberg Road

Precinct 2 - Fairfield Commercial

The proposed relationship to Heidelberg Road of the proposed built form outcome is illustrated below.

This demonstrates a balanced degree of enclosure to the 27 metre wide street, without creating visually dominant buildings, and the benefit of the 3 metre front setback on improving the composition of the street and the quality of the pedestrian environment at ground level.

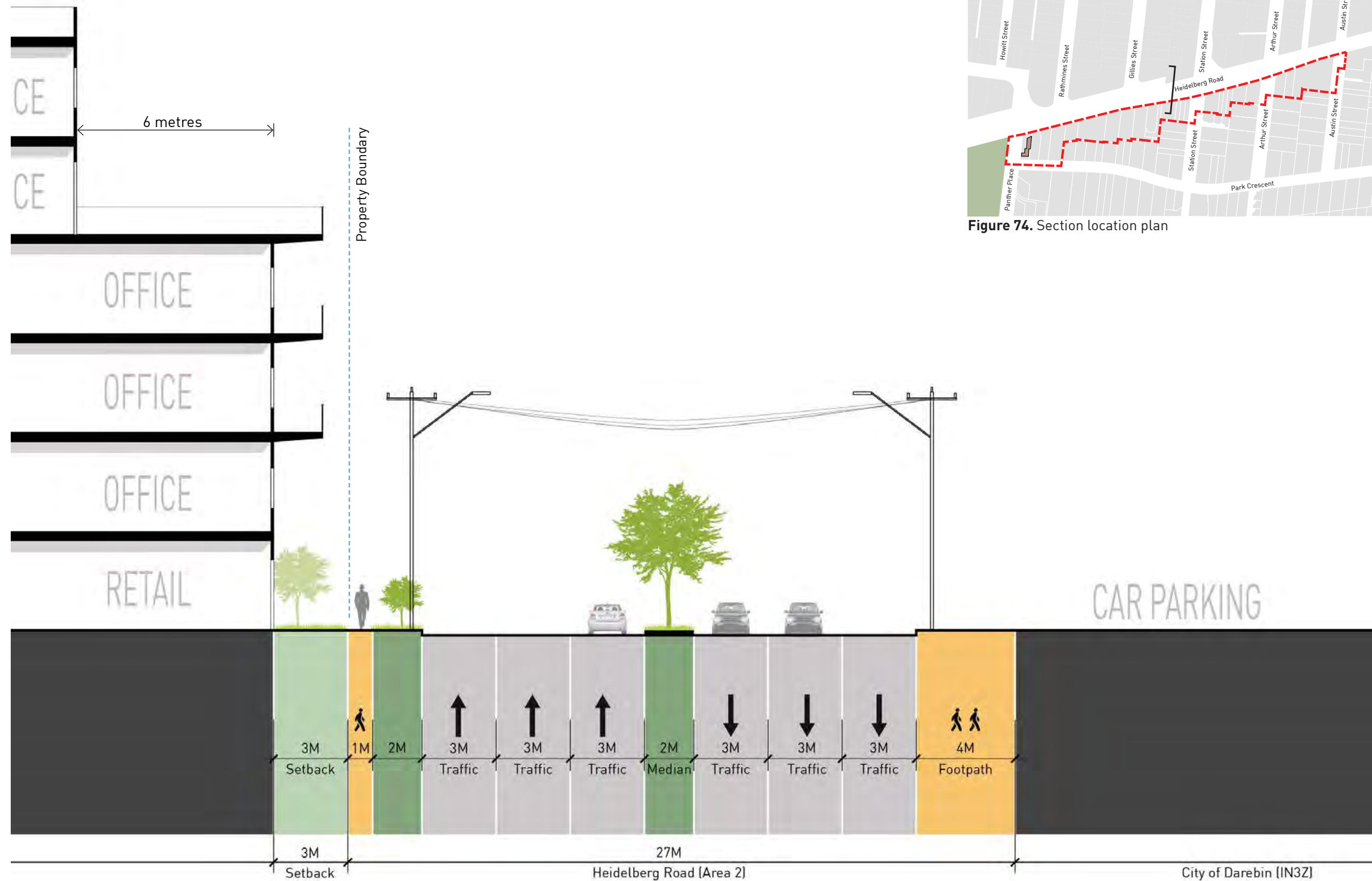


Figure 75. Proposed street section - full street section

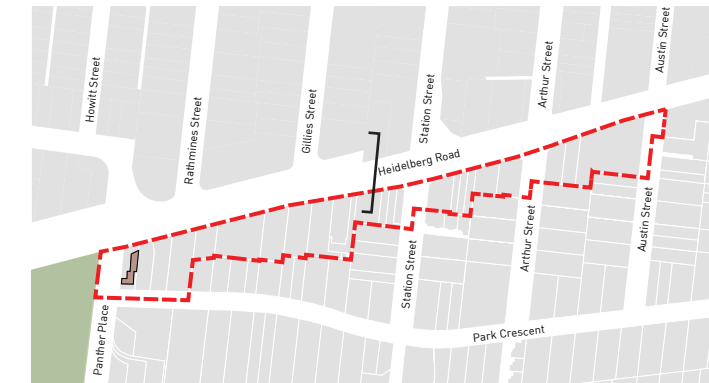


Figure 74. Section location plan

D. Confirming overshadowing requirements

Figure 76 illustrates how the introduction of an 8 metre high boundary wall condition at the rear interface of new development will enable the overshadowing requirements of Clause 54 and 55 to be met.

Note, that this does not mean that the visual impact requirements are also met (refer Chapter 2 which provides detailed guidance on rear interface conditions).



Figure 76. Cumulative shadow impact of 8 metre high boundary wall condition

- Private open space has more than 5 hours sunlight between 9 am and 3 pm on 22 September
- Shadow between 9 am and 3 pm on 22 September

Note: Assessment utilises the building footprints that are documented in Council's GIS mapping.

Precinct 2 - Fairfield Commercial

E. Building envelope controls

The following building envelope is proposed for Precinct 2.

The application of the rear interface and the Heidelberg Road interface result in a maximum 5-6 storey building height.

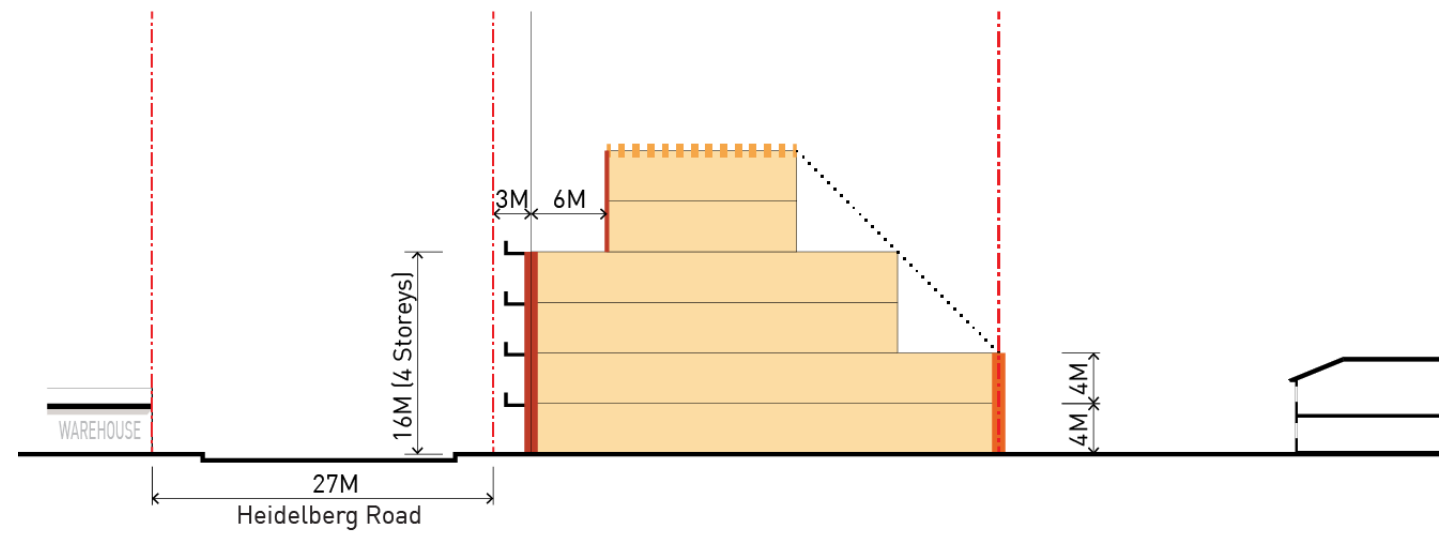
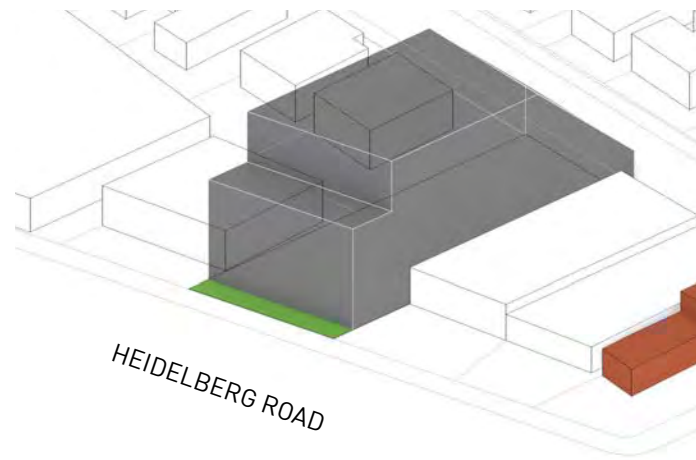


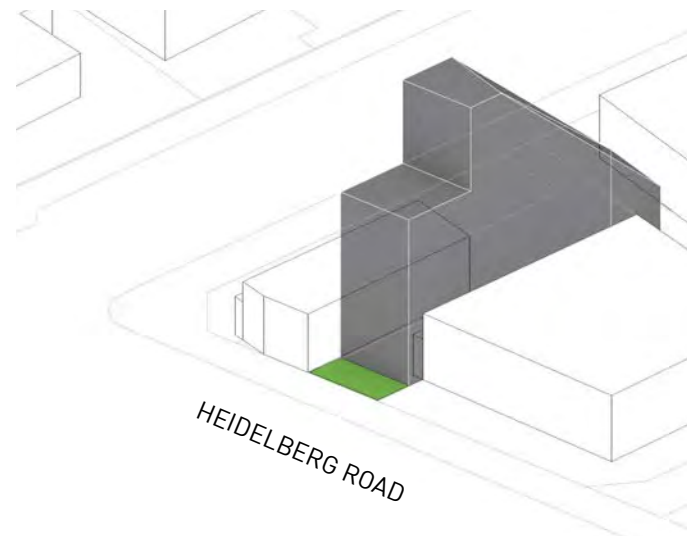
Figure 77. Proposed building envelope controls in Precinct 2

Note: Ground level setback to rear boundary may be required depending on relationship to existing dwelling location (refer to Figure 15 on page 13)

Testing site 2-1



Testing site 2-2



Testing site 2-3

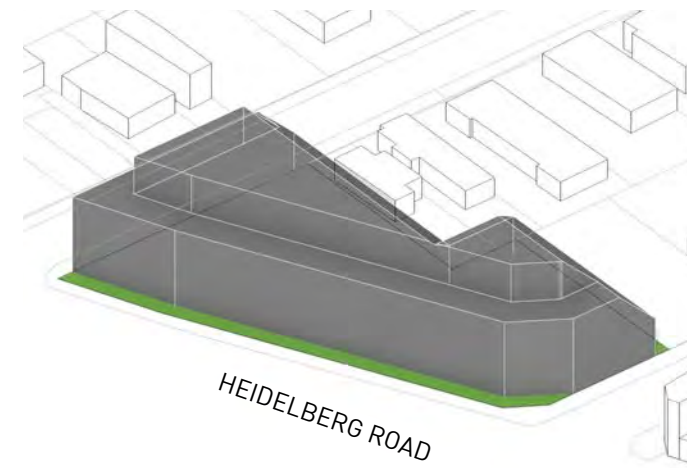


Figure 78. Demonstration of the 3d building envelope controls applied to the selected testing sites (refer over page)

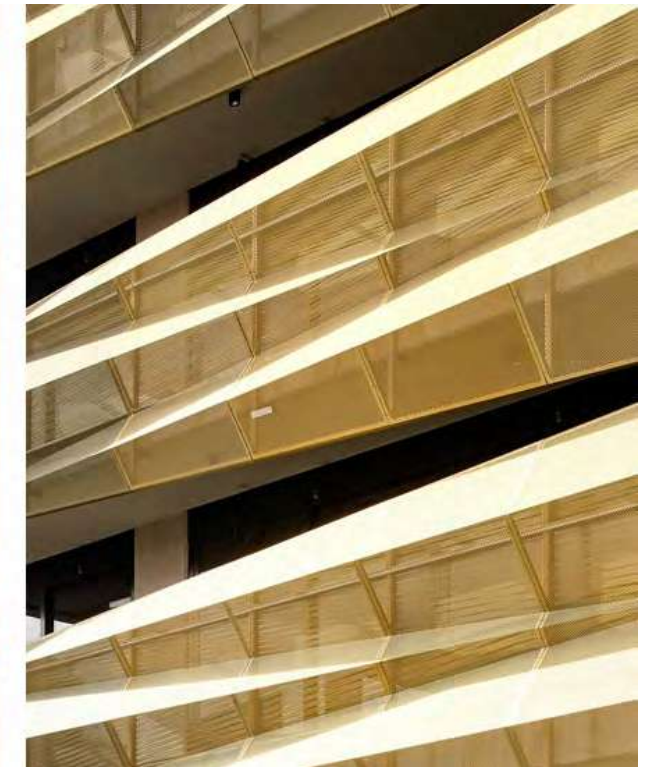
F. Precedent examples - Precinct 2



Figure 80. 9-15 Inkerman Street, St Kilda (Source: Neometro)



Figure 79. Commercial development in Cremorne (Source: EAT Architects)



Precinct 2 - Fairfield Commercial

G. Built form testing of proposed building envelopes

Testing site

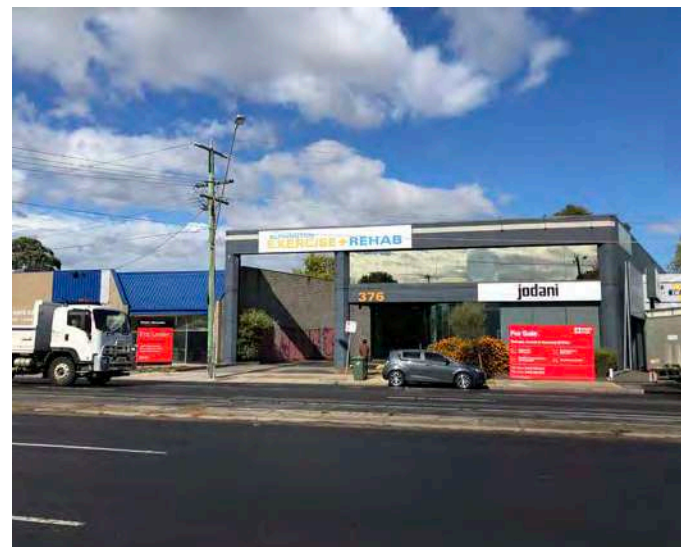
Built form testing has been undertaken for three sites in Precinct two to both assess and communicate the proposed built form controls.

This includes a range of site sizes and mid-block and corner sites.

Testing site 2-1

Location:
376 Heidelberg Road

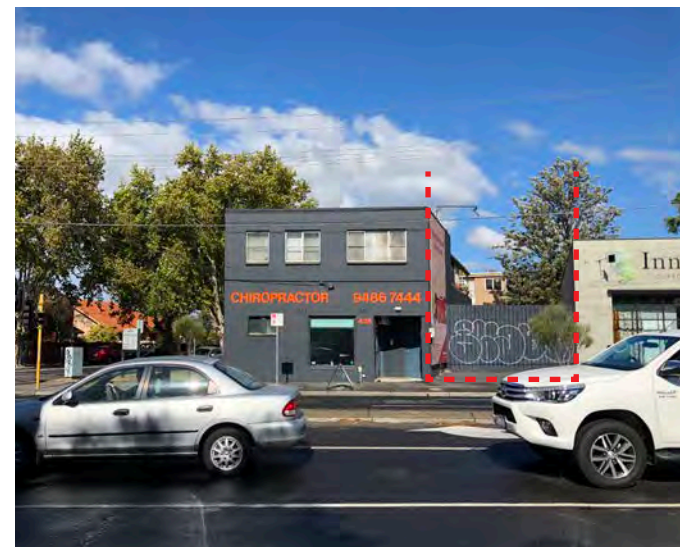
Site area:	Lot width:	Lot depth:	Character/use:
1,080m ²	21.9M	50M	Large format retail Vehicular access



Testing site 2-2

Location:
434 Heidelberg Road

Site area:	Lot width:	Lot depth:	Character/use:
203m ²	6.5M	31.5M	Vacant



Testing site 2-3

Location:
484 Heidelberg Road

Site area:	Lot width:	Lot depth:	Character/use:
3,640m ²	90M	33-55M	3 Point Motors Large showroom



Testing site 2-1

376 Heidelberg Road

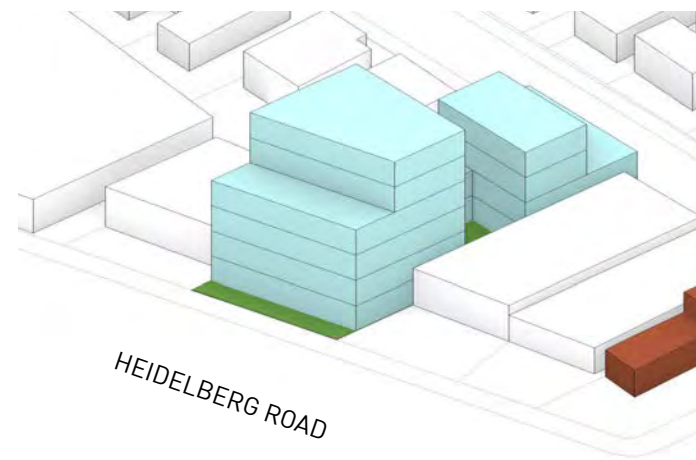
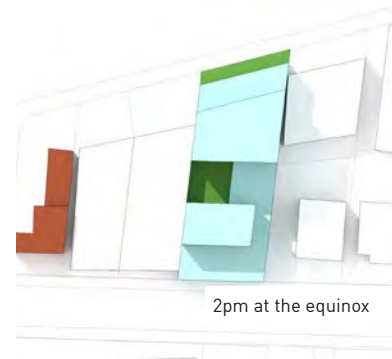


Figure 81. Built form testing – plan and perspective view

Key positive features:

- Buildings are adequately separated to allow for access to daylight and views within generous internal light well
- Building transitions to the lower residential areas to the south
- 4 storey street wall provides definition and enclosure to the street without being visually dominant.

This testing demonstrates that a height of 6 storeys is possible on this site due to rear interface and Heidelberg Road street wall height and setback requirements.

Testing site 2-2

434 Heidelberg Road

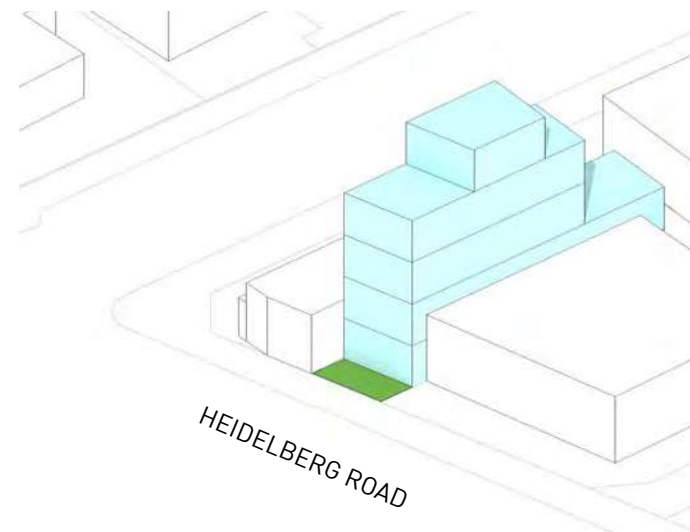
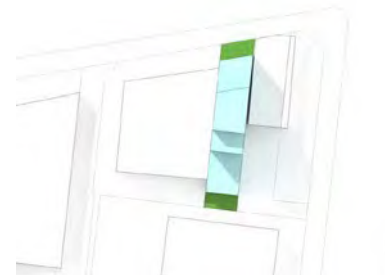


Figure 82. Built form testing – plan and perspective view

Key positive features:

- 3 metre ground floor setback at rear at interface to side boundary of existing dwelling.
- 4 storey street wall provides definition and enclosure to the street without being visually dominant.

This testing demonstrates that a height of 5 storeys is possible on this site due to rear interface and Heidelberg Road street wall height and setback requirements.

Testing site 2-3

484 Heidelberg Road

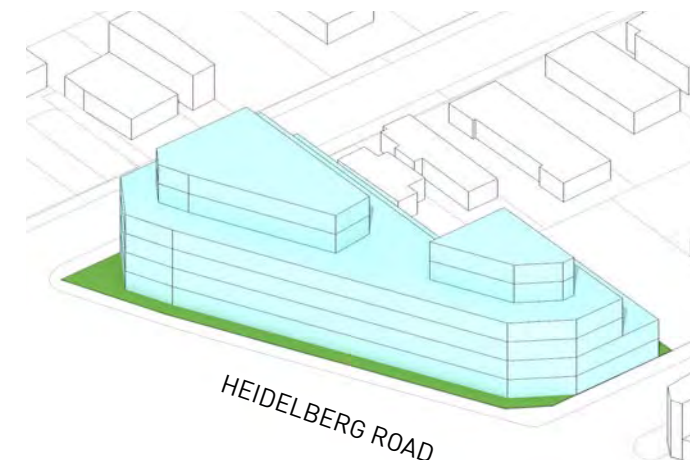
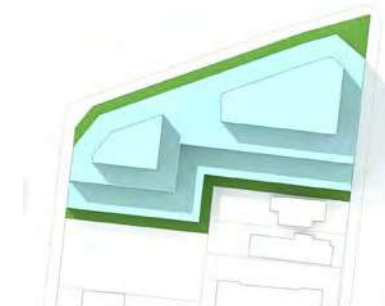


Figure 83. Built form testing – plan and perspective view

Key positive features:

- 3 metre ground floor setback at rear at interface to side boundary of existing dwelling.
- 4 storey street wall provides definition and enclosure to the street without being visually dominant.

This testing demonstrates that a height of 6 storeys is possible on this site due to rear interface and Heidelberg Road street wall height and setback requirements.

Precinct 2 - Fairfield Commercial

The study area is intersected by a number of side streets which are residential and low-scale in character. It is important that any taller buildings on the corners of Heidelberg Road and the side streets interface with these more sensitive, character environments appropriately. The requirement for a 45 degree angle above a two storey interface provides an appropriate transition in building scale (refer Figure 52).

Testing site 2-3

484 Heidelberg Road (rear view from Austin Street)

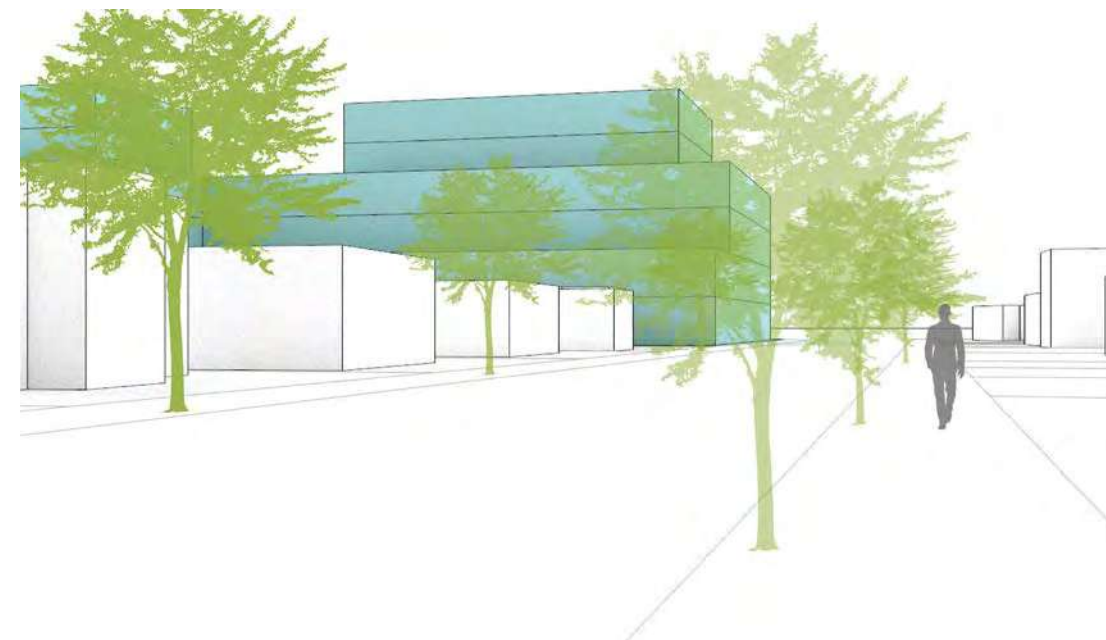
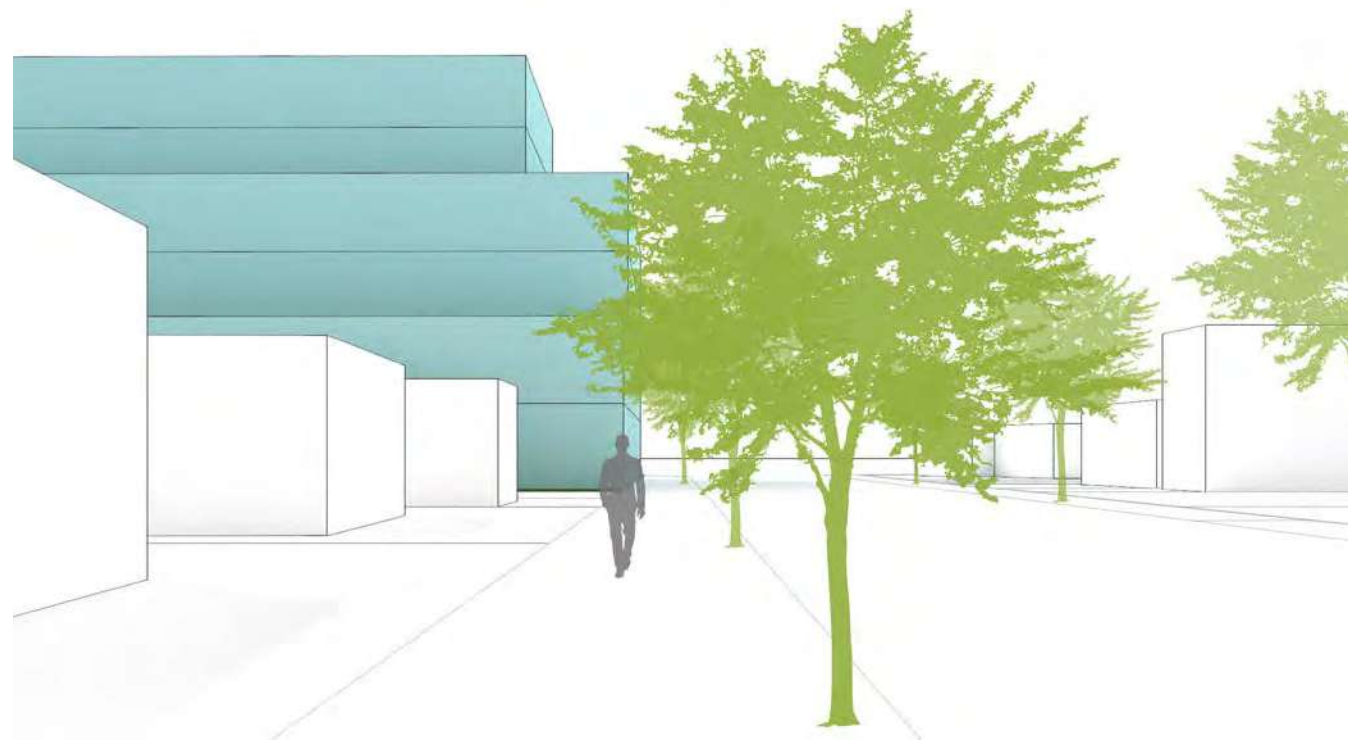


Figure 84. 3d modelling of the transition between a corner site and a low-scale residential side street. The transition from a 6-storey building height to a 2 storey interface is illustrated in a view from across the street (right) and on the same side of the street (left)

- Existing heritage buildings
- Potential heritage buildings

H. Proposed built form controls

The proposed building envelope controls are illustrated in the following plans.

Considering the consistent site attributes and constraints and a high degree of certainty that the proposed controls have been tailored to maximise development potential while considering the amenity of residential areas to the south, all development controls are proposed as mandatory.

Building heights



Figure 85. Precinct 2 - Proposed overall building heights

Ground floor setbacks



Figure 87. Precinct 2 - Proposed ground floor setbacks

Street wall heights

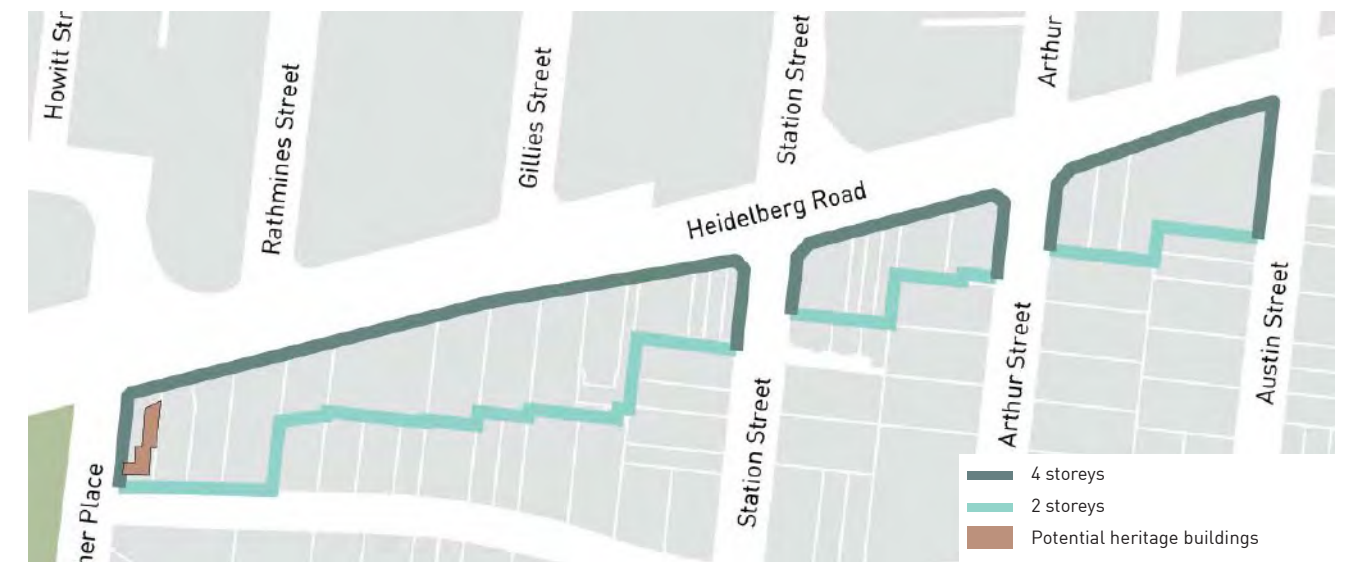


Figure 86. Precinct 2 - Proposed street wall heights / building heights along residential interface boundaries

Precinct 3A - Alphington West

A. Key valued character attributes



Figure 88. Precinct 3A - Aerial image

1. Prominent location on major road intersection provides the opportunity to introduce a taller building that holds the corner with a high quality, well-designed building. The site is immediately adjacent to the Alphington Paper Mills site - the opposite corner site has an approved permit for a 17 storey building.
2. Larger street trees along Heidelberg Road frontage provide greening of Heidelberg Road.
3. The site fronts Coate Avenue to the west, a quiet residential street that incorporates predominantly 1-2 storey buildings with front landscaped gardens.
4. The existing landscape setback to Heidelberg Road and Chandler Highway provides visual relief within the streetscape and a more pedestrian-friendly environment.








Figure 89. Key character attributes

B. Design Strategy

Provide a well-designed mid-rise, mixed-use building that marks the prominent corner location and respects the character of the neighbourhoods to the south and west. Incorporate a landscape setback to all boundaries to provide an attractive, garden setting to Coate Avenue and the southern boundary and to significantly improve the pedestrian experience to Heidelberg Road and Chandler Highway.



Figure 90. Design Strategy

-  Existing vehicular access (retained/consolidated)
-  Proposed 3m landscape setback
-  Proposed 4.5m landscape setback to Coate Avenue and the southern boundary
-  Create urban street wall and activated edges along Heidelberg Road
-  Minimise visual bulk, overshadowing and privacy on the sensitive interface (Rear to side boundary condition)

Design Objectives

Improve the pedestrian experience on Heidelberg Road and Chandler Highway through a 3 metre front setback - Location 1

The existing landscape setback improves the quality of the pedestrian experience by greening the otherwise largely harsh, asphalt landscape and by providing additional sense of openness/relief for pedestrian movement.

Ensure the development integrates with the existing character of Coate Avenue through inclusion of a 4.5 metre setback and 3 storey street wall height - Location 2

Coate Avenue is fronted by 1-3 storey dwellings that incorporate a front garden setback of approximately 4.5 metres. This is a consistent and valued character.

Introducing a three storey street wall behind this setback will ensure that development integrates with the existing context. Upper levels above this height should be setback at a 45 degree angle from the rear neighbourhood boundary and 30 degree angle from Coate Avenue to minimise the impact of visual bulk above this height.

Respond to the prominent intersection with a taller form located at the corner that transitions in height down towards the west and south - Location 3

The tallest building element should be located on the intersection of Heidelberg Road and Chandler Highway and step down in height to the lower scale residential neighbourhoods.

The site needs to accommodate a transition from the 17 storey development context to the east and the single storey context to the west. A building height in the order of 8 storeys on the corner would provide this transition. This is a similar approach to transition that is adopted within the Paper Mill site which transitions from 14 storeys (the Heidelberg Road and Chandler Highway intersection), to 6-8 storeys and down to low-rise building (3 - 4 storeys) within the centre, eastern and southern portions of the Alphington Paper Mill site.

Ensure development does not visually dominate or unreasonably overshadow private open space in adjacent residential areas - Location 4

The neighbouring residential properties all incorporate a single private open space that is located at either the front or rear of each building. The primary outlook and main light/sunlight source for the main living spaces front these outdoor areas.

While sunlight is only required to be provided at the equinox according to the current level of provision required in Clause 54 and 55 of the planning scheme, inclusion of a 4.5 metre landscape setback and 2 storey building height on this interface will ensure that some winter sunlight reaches the northern dwellings.

Locate vehicular crossover from Coate Avenue - Location 5

Locate a singular vehicular access from Coate Avenue. The width of the vehicular entry should be minimised.

Precinct 3A - Alphington West

C. Determining appropriate development scale - all interfaces

Heidelberg Road varies in width along its length. In Precinct 3A it broadens to approximately 44 metres in width.

This site was recently considered at VCAT where acceptance of a taller built form on the corner of the Chandler Highway and Heidelberg Road interface was accepted. The need to step down in height from this intersection towards the west and south towards the existing residential areas was also supported. The lack of an appropriate transition and the visual dominance of the proposed 13 / 8 storey building when viewed from within Coate Avenue, however, was considered to have a negative impact on local, valued character and led to a VCAT decision that supported Council's refusal to grant a planning permit.

Figures 64-66 demonstrate the proposed development that was considered at VCAT, alternate proposal provided by the applicant's expert witness and the proposal supported by Leanne Hodyl as Council's expert witness.

Further testing has been undertaken to consider the appropriate scale of building transition to the west and south. The visual impact of different building proposals are illustrated in figures 97-100.

They confirm that an overall building height in the order of 8 storeys that steps down to 5, then 3 storeys at Coate Avenue provides a balance between supporting development intensification and managing the visual impact on local character.

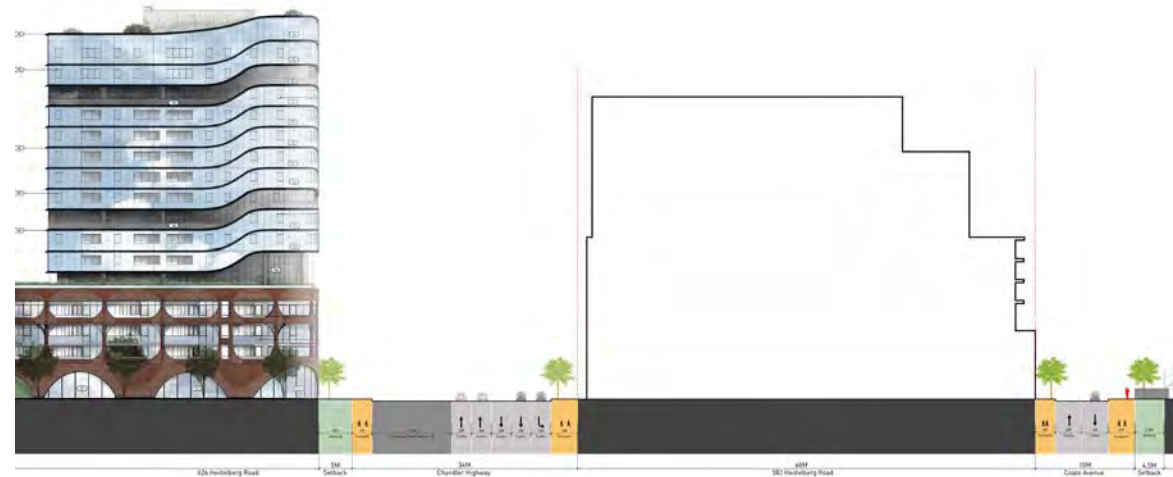


Figure 91. Building enveloped of development proposal not supported at VCAT - An overall building height of 13 storeys that transitions to 11, 7 and then 3 storeys at Coate Avenue.

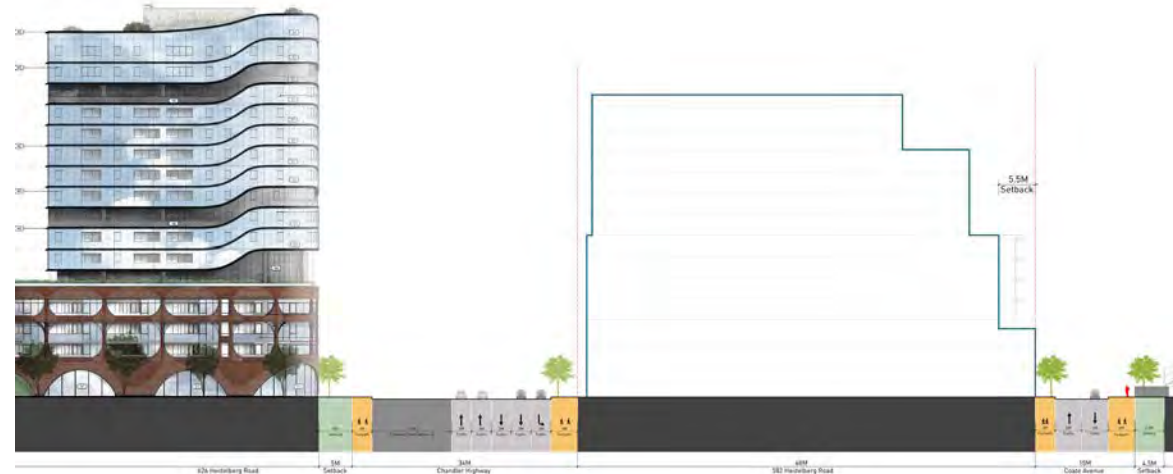


Figure 92. Alternate proposal supported by the applicant's expert witness for increasing the upper level setbacks on Coate Avenue to 5.5 metres

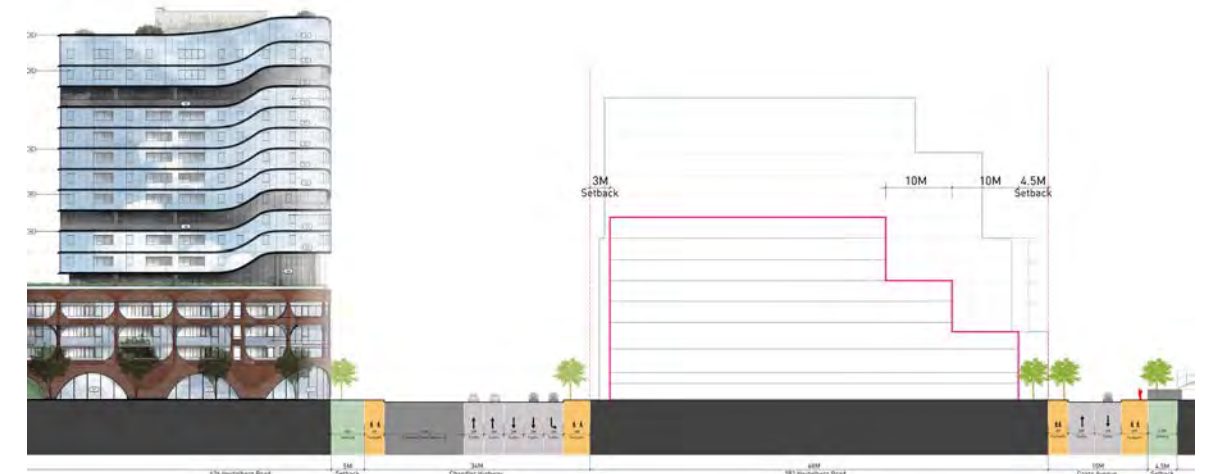


Figure 93. Alternate proposed supported by Leanne Hodyl (Council's expert witness) for reducing the overall building height and increasing the setbacks from Coate Avenue.

8 Storeys - Option 1

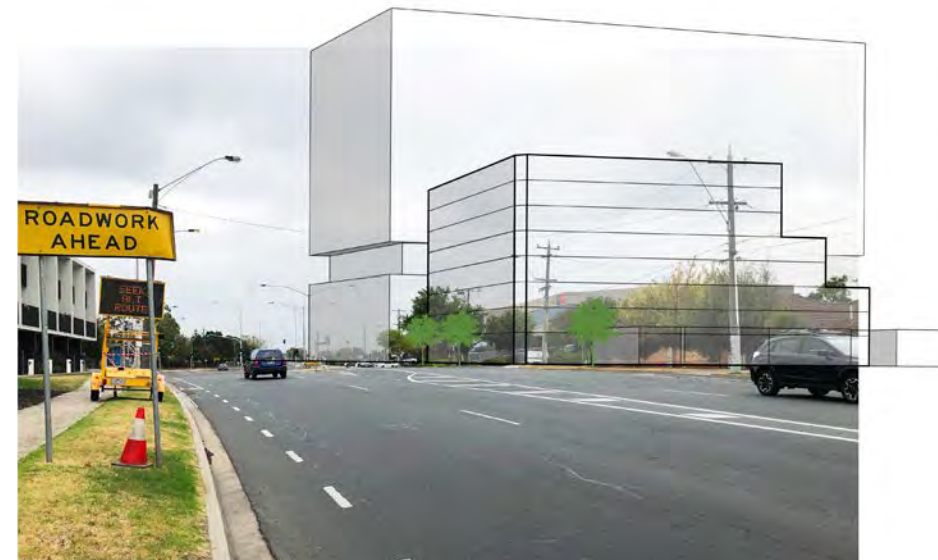
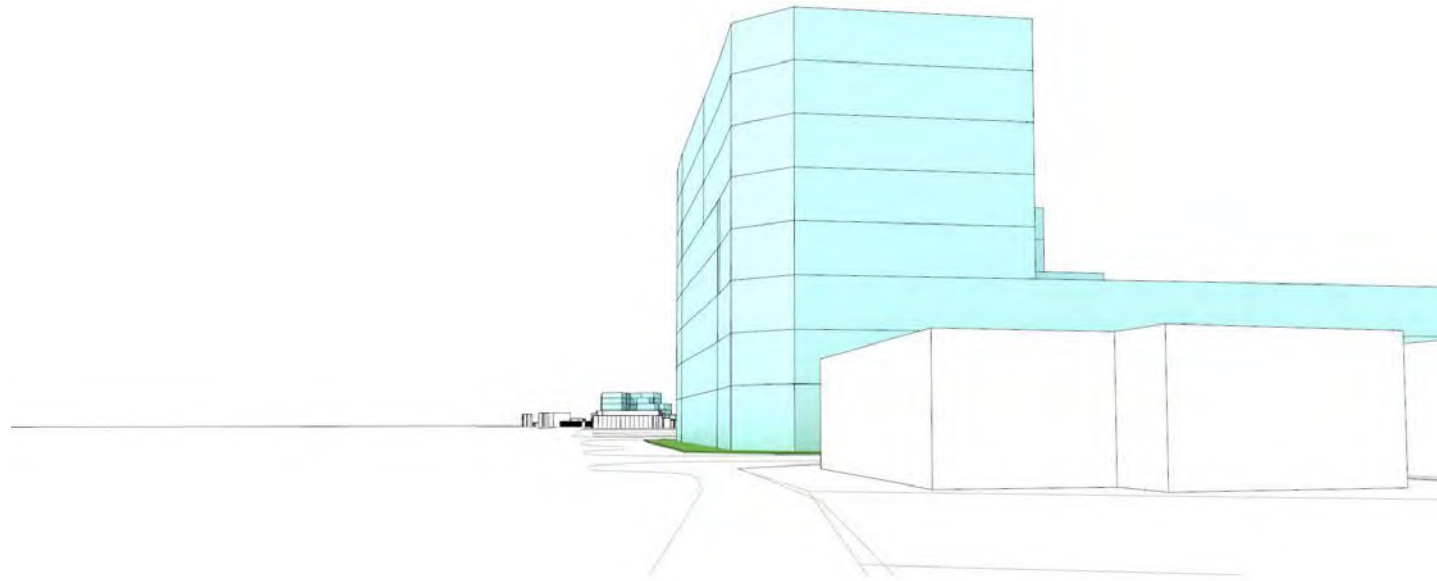


Figure 94. 8 storey street wall height along the full length of Heidelberg Road viewed from south side of street (left) and from across the street further to the west - looking east (right)

8 Storeys - Option 2

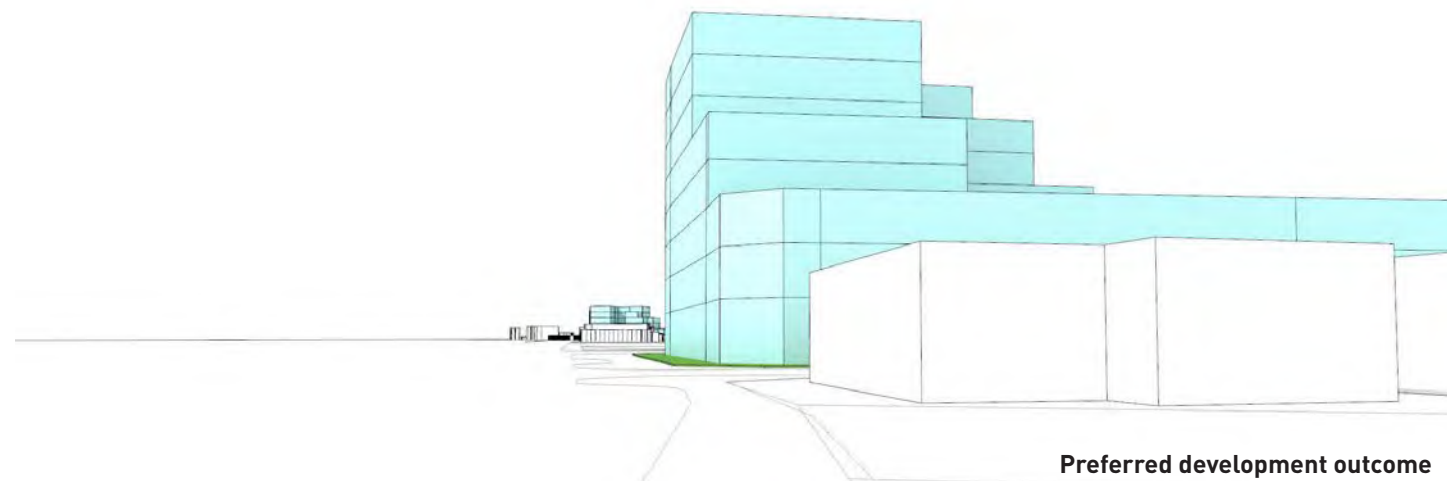


Figure 95. 8 storey street wall height stepping down to 5 then 3 storeys viewed from south side of street (left) and from across the street further to the west - looking east (right)

Precinct 3A - Alphington West

Upper level setback - 8 Storeys - Option 1

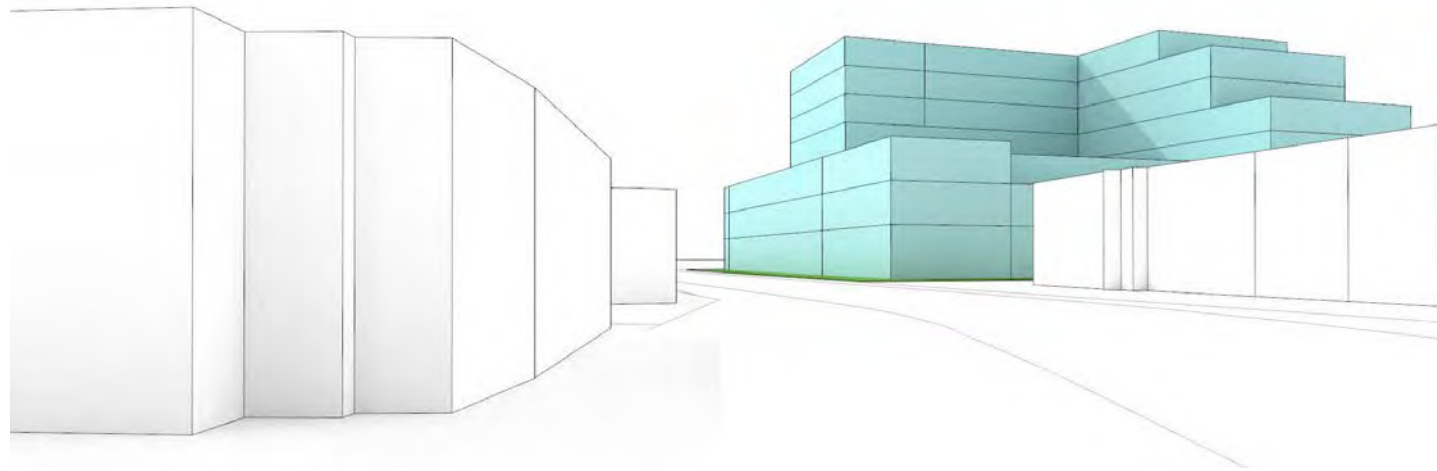


Figure 96. View from Coate Avenue - 8 storey development stepping directly to a 3 storey interface at Coate Avenue. The upper levels of the building become visually dominant in the street.

Upper level setback - 10 Storeys - Option 3

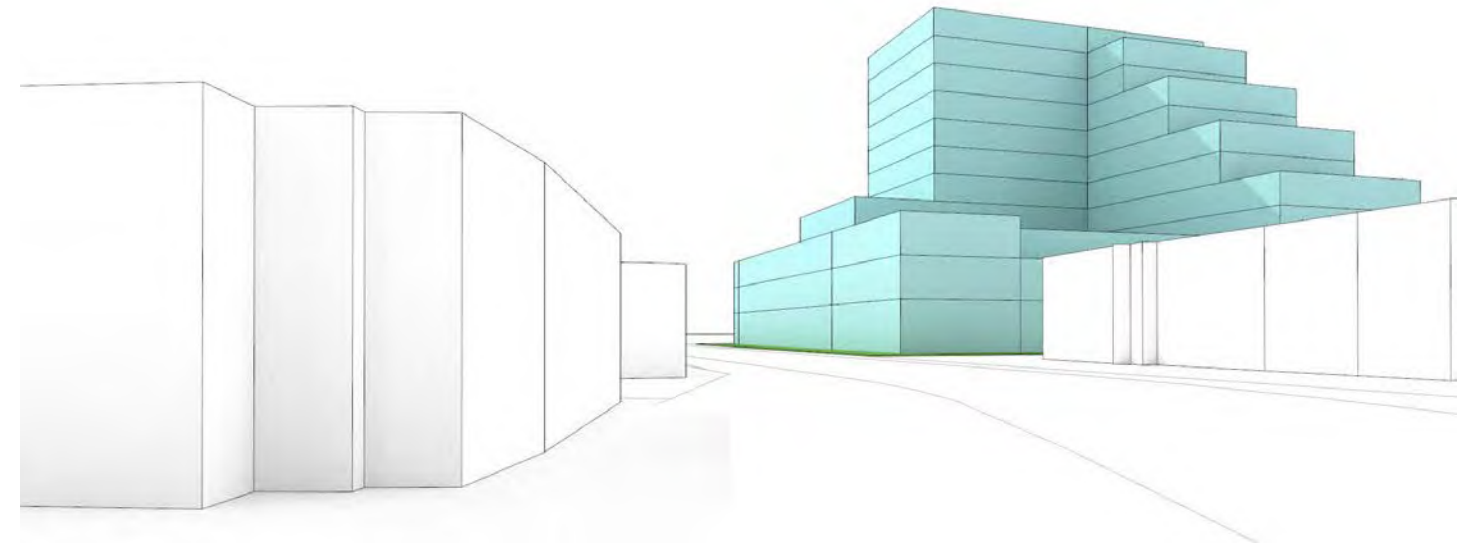
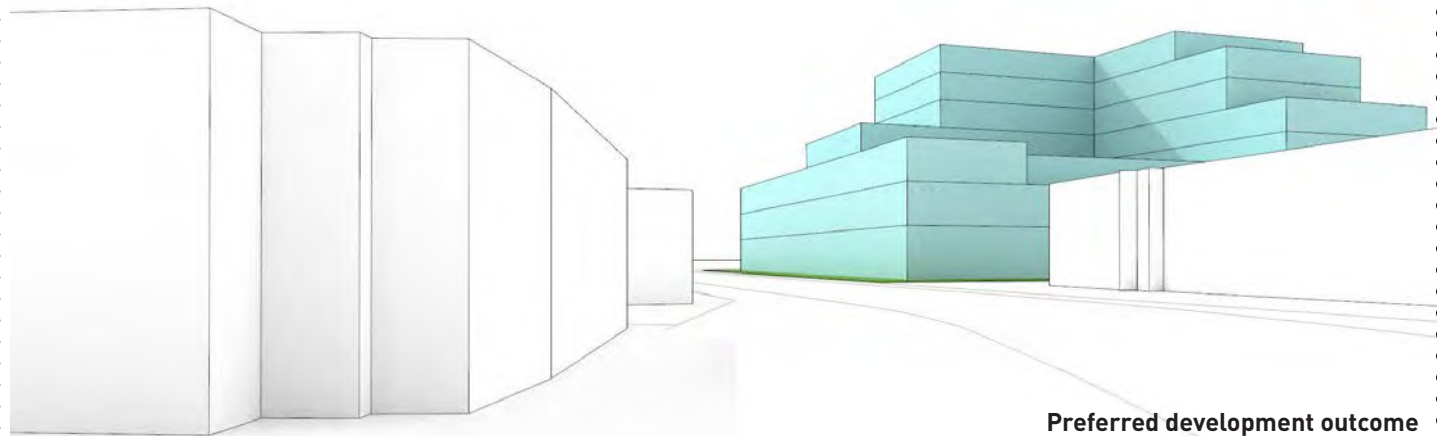


Figure 98. View from Coate Avenue - 10 storey development stepping to 5, then 3 storeys at Coate Avenue. The upper levels of the building become visually dominant in the street.

Upper level setback - 8 Storeys - Option 2



Preferred development outcome

Figure 97. View from Coate Avenue - 8 storey development stepping to 5, then 3 storeys at Coate Avenue. The upper levels of the building become part of the overall building composition and are not visually dominant.

Upper level setback - 10 Storeys - Option 4

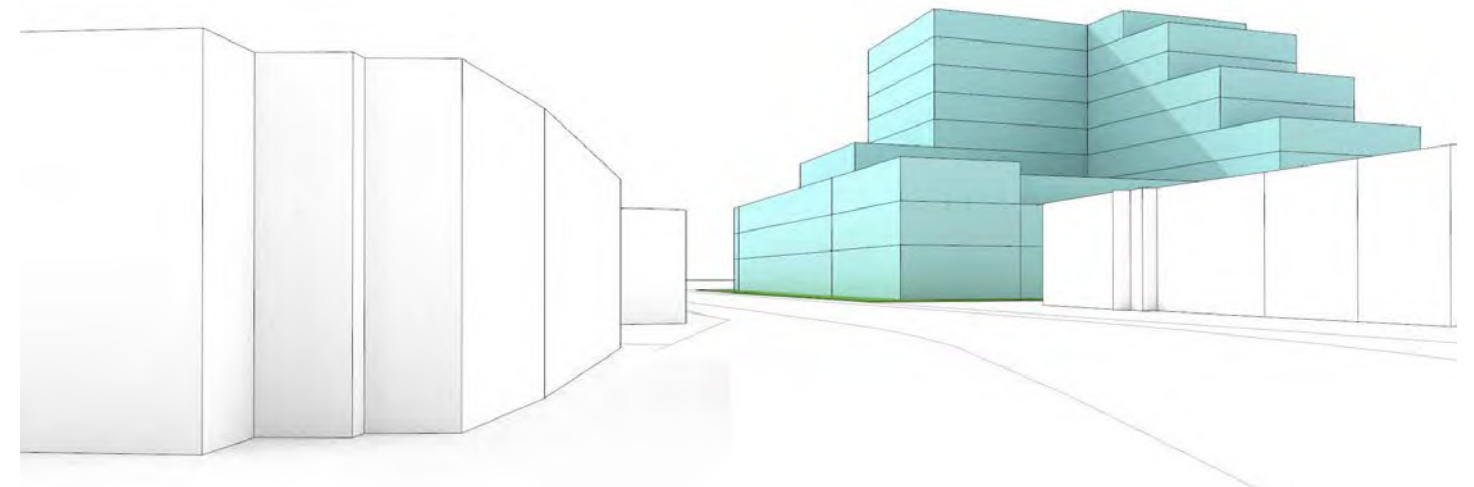


Figure 99. View from Coate Avenue - 10 storey development stepping to 5, then 3 storeys at Coate Avenue. The upper levels of the building become visually dominant in the street.

Precinct 3A



Figure 100. Section location plan

Key recommendation

Introduce a discretionary 8 storey height control, requiring the building to step down to 3 storeys at Coate Avenue and to 2 storeys on the southern boundary.

Upper levels to be set back at a 45 degree angle from the southern boundary.

Upper levels to be set back 10 metres above the 3rd floor on Coate Avenue, with an additional setback of 10 metres about the 5th floor.

Interface to Heidelberg Road

The proposed relationship to Heidelberg Road (at the intersection with Chandler Highway) is illustrated below.

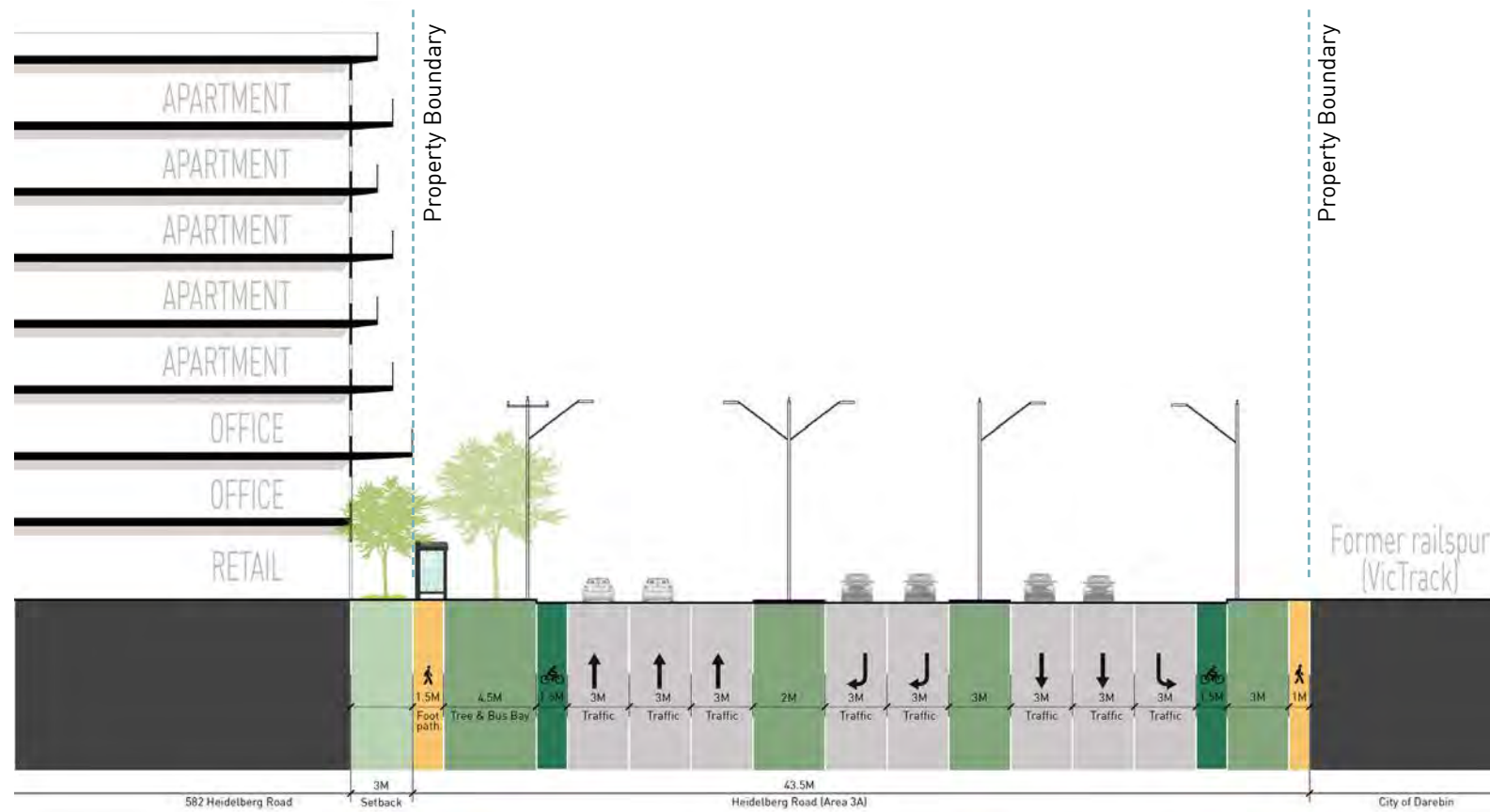


Figure 101. Proposed street section - full street section

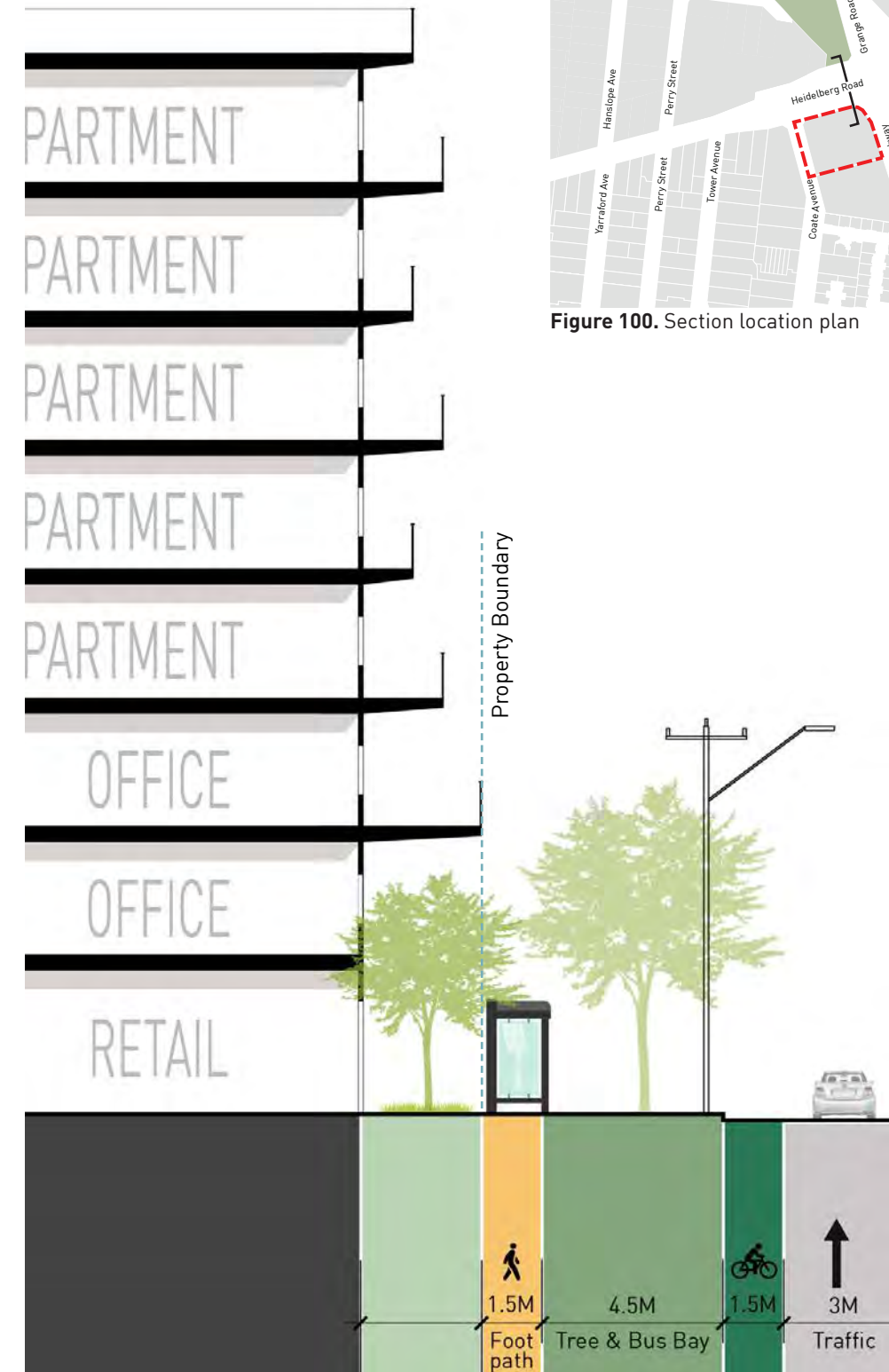


Figure 102. Proposed street section - detailed street section

Precinct 3A - Alphington West

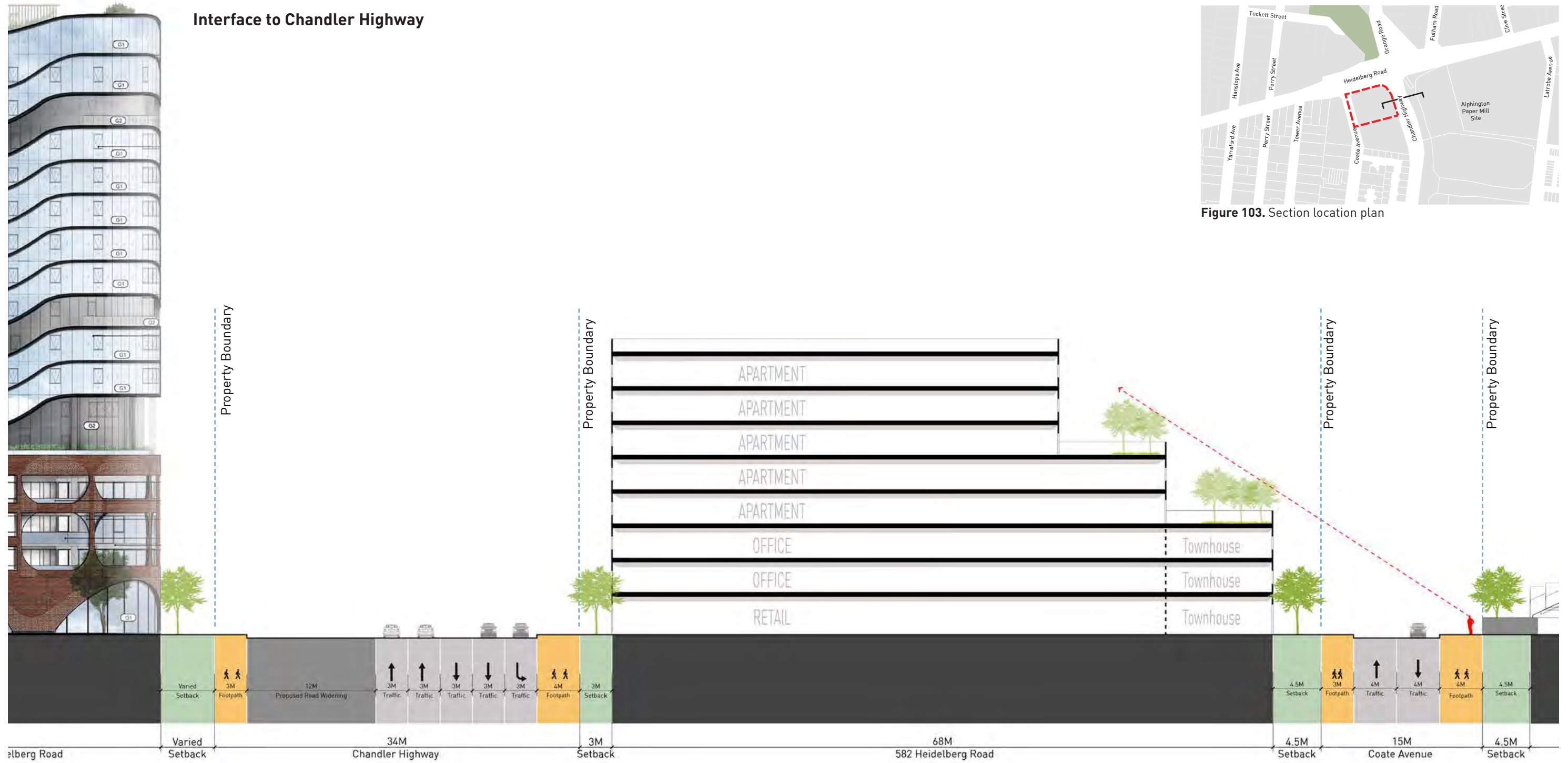


Figure 104. Proposed street section - full street section with indicative floor levels illustrated within this envelope for reference only. These are not intended to illustrate acceptable building designs.

D. Confirming overshadowing requirements



Figure 105. Cumulative shadow impact of 8 metre high boundary wall condition

■ Private open space has more than 5 hours sunlight between 9 am and 3 pm on 22 September
■ Shadow between 9 am and 3 pm on 22 September
 Note: Assessment utilises the building footprints that are documented in Council's GIS mapping.

E. Building envelope controls

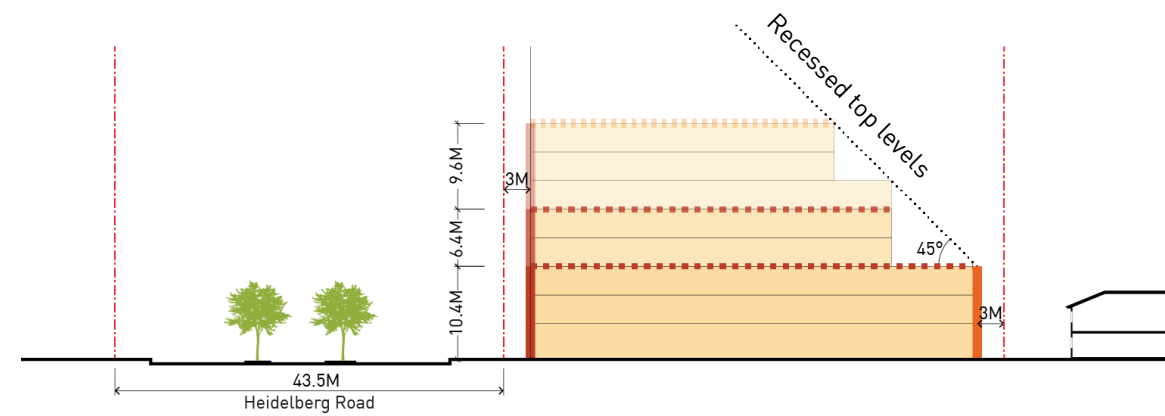


Figure 106. Proposed built form envelopes (section A-A) with indicative floor levels illustrated within this envelope for reference only. These are not intended to illustrate acceptable building designs.

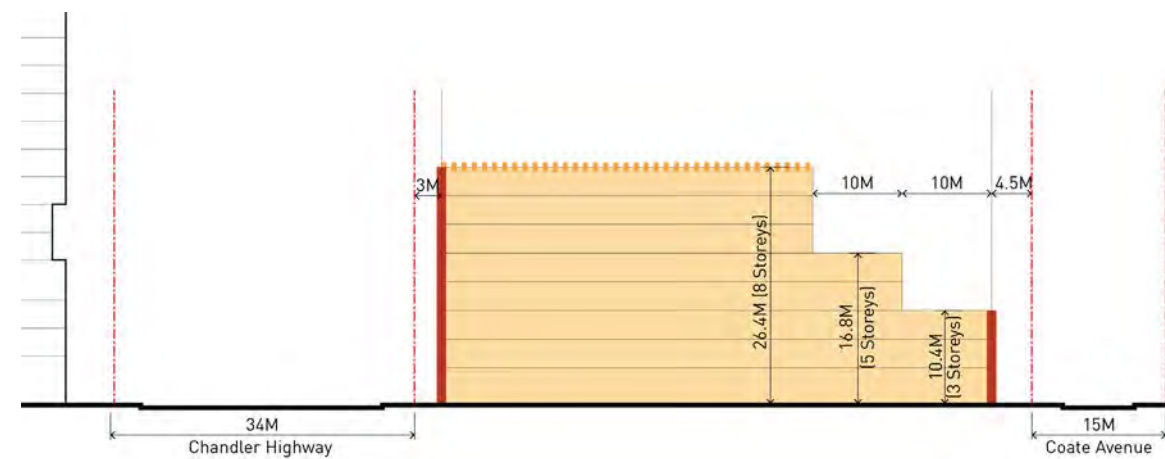


Figure 107. Proposed built form envelopes (section B-B) with indicative floor levels illustrated within this envelope for reference only. These are not intended to illustrate acceptable building designs.

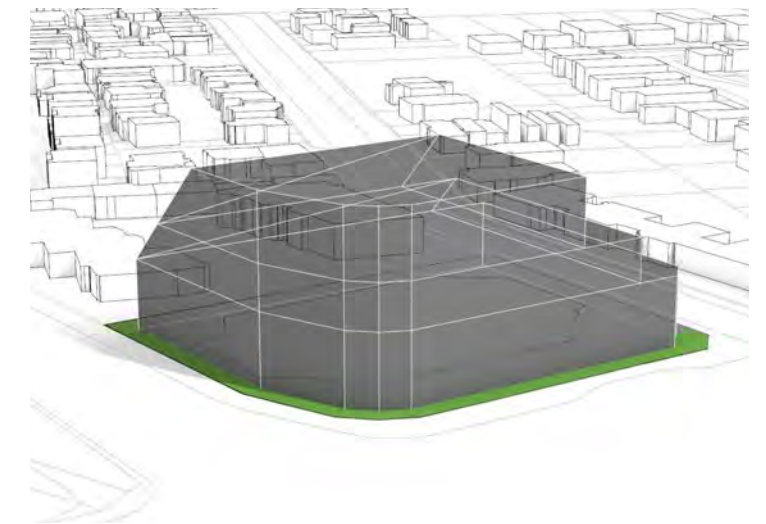


Figure 108. Demonstration of the 3d building envelope controls

Precinct 3A - Alphington West

F. Precedent examples - Precinct 3A

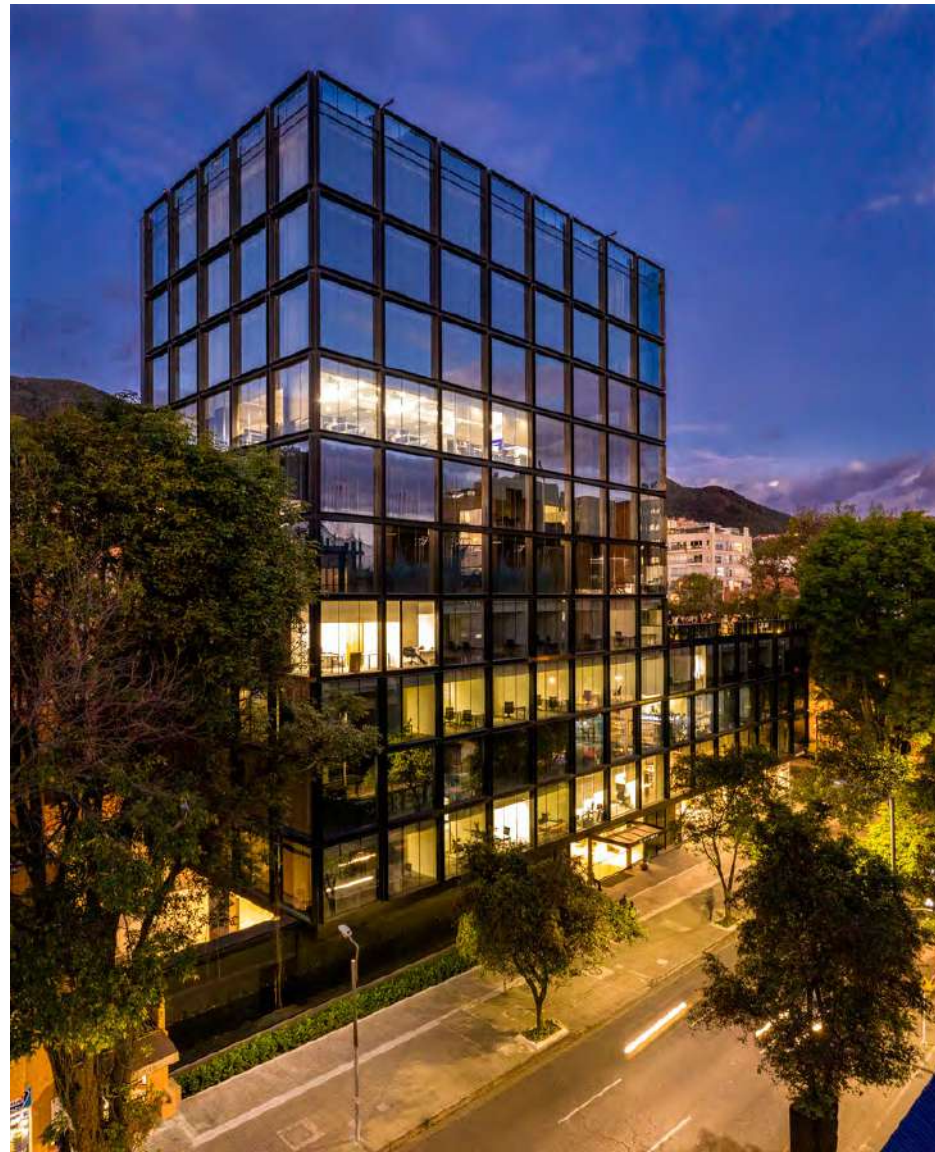


Figure 109. 80-ONCE Business & Living [Source: Sestral S.A]



Figure 111. Peel Street, Collingwood [Source: DKO Architecture]



Figure 110. Proposal for 342-348 Victoria Street - Brunswick [Source: Fieldworks Architects]

G. Built form testing of proposed building envelopes

Testing site 3A

Location: 582 Heidelberg Road			
Site area: 3,729m ²	Lot width: 68M	Lot depth: 56M	Characters: Singular site

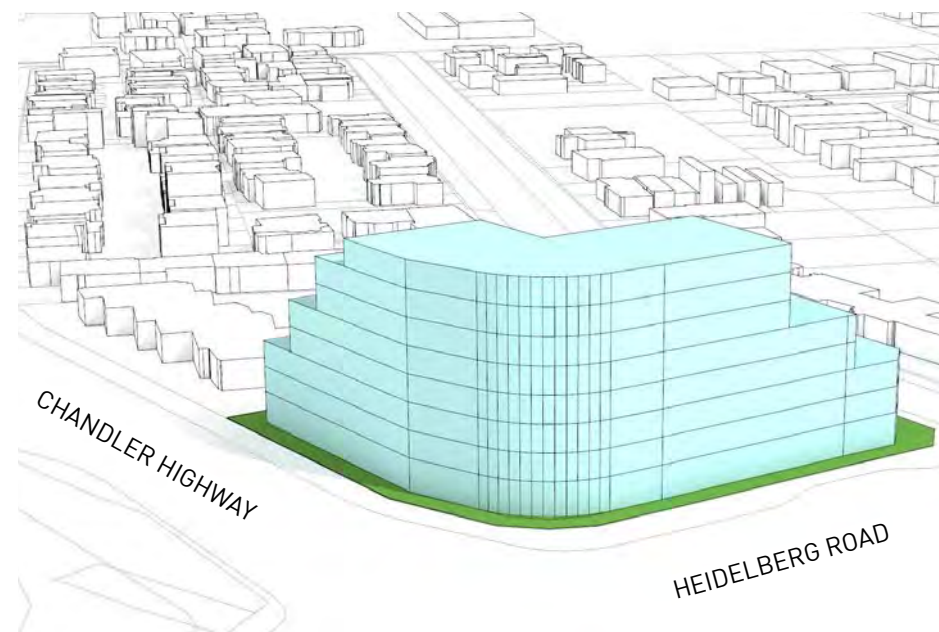


Figure 112. Built form testing - perspective view

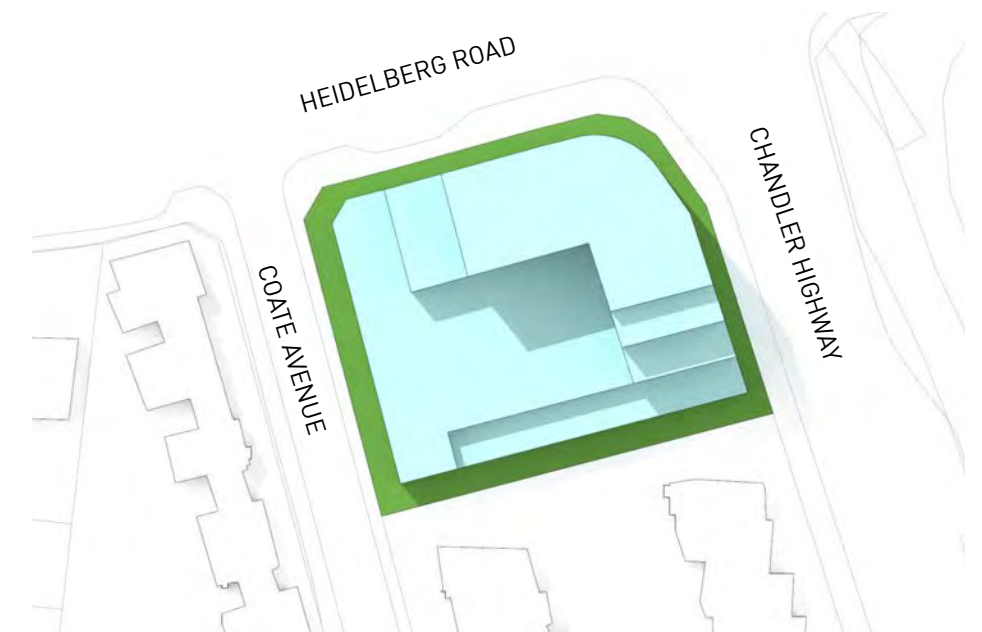


Figure 113. Built form testing - plan view



Figure 115. Existing building

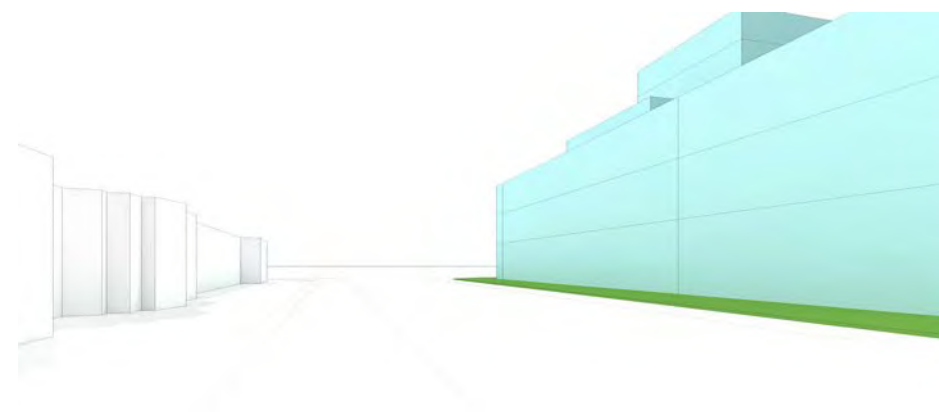


Figure 114. View from Coate Avenue illustrating the benefits of a three storey street wall height with significant upper level setbacks that ensure the building is not visually dominant in the street.

Precinct 3A - Alphington West

H. Proposed built form controls

The proposed building envelope controls are illustrated in the following plans.

- Rear interface controls (maximum rear interface heights, ground level and upper level setbacks)

Considering the unique site attributes and the need to support design flexibility with certainty that minimum amenity standards are met, a mix of mandatory and discretionary controls are proposed as follows:

Discretionary

- Overall height limits
- Street wall heights to Heidelberg Road, Chandler Highway and Coate Avenue
- Upper level setbacks from street

Mandatory

- Front setbacks to all streets

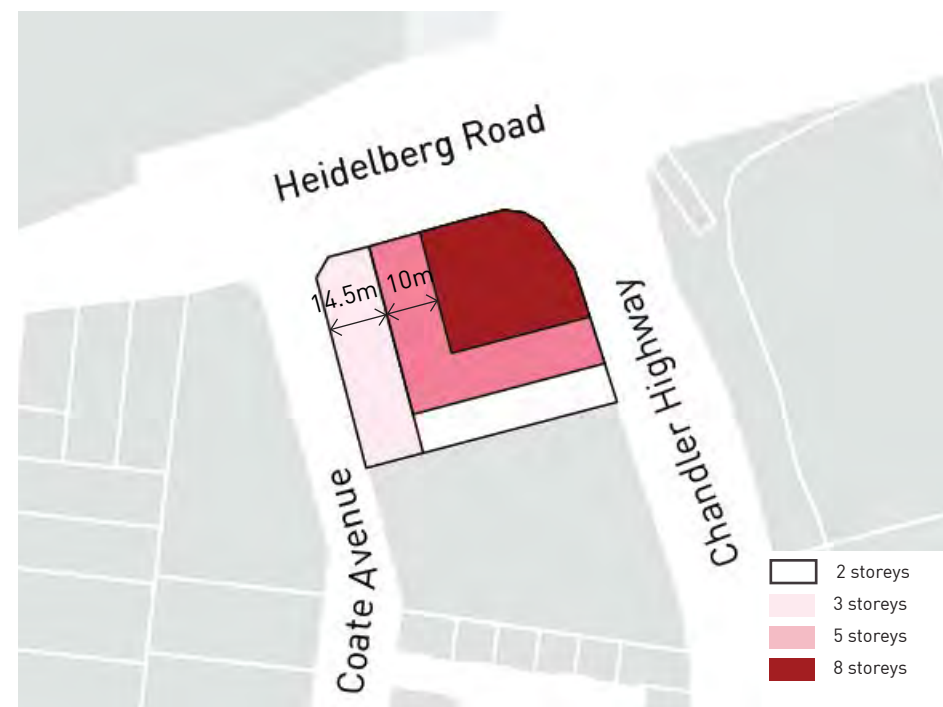


Figure 116. Precinct 3A - Proposed overall building heights



Figure 117. Precinct 3A - Proposed ground floor setbacks



Figure 118. Precinct 3A - Proposed street wall heights / building heights along residential interface boundaries

Precinct 3B - Heidelberg Road Neighbourhood Activity Centre

A. Key valued character attributes



Figure 119. Precinct 3B - Aerial image



Figure 120. Key character attributes

1. Recent mixed-use development - the overall height and massing responds to the urban context. The significant setback incorporates large trees and low-storey planting and improves the quality of the pedestrian environment.
2. Existing fine-grain shopfronts provide a pedestrian-friendly environment. This occurs in the block bounded by Yarralea Street and Park Avenue.
3. Existing and potential heritage buildings are located within this block. PAO applies in the corner and overlays five sites including an existing heritage building.
4. Smaller frontages/shops, narrow footpath, and verandahs give more sense of traditional and enclosure.
5. Residential side streets, including large mature trees and significant setbacks.



Precinct 3B - Neighbourhood Activity Centre

B. Design Strategy

Develop a new mid-rise character for the existing neighbourhood centre which complements the scale and facilities in the former Alphington Paper Mills site. Enhance the setting of heritage buildings and the fine-grain development patterns through a low-street wall height.



Figure 121. Design Strategy

- Existing heritage buildings
- Potential heritage buildings
- Existing medium-density, mid-rise housing
- Existing vehicular access (retained/consolidated)
- Vehicular access (removal preferred)
- Existing landscape setback
- Proposed 3m landscape setback
- Create urban street wall and activated edges along Heidelberg Road
- PAO overlay
- Neighbourhood Residential Zone

Minimise visual bulk, overshadowing and privacy on the sensitive interface:

- Rear to rear boundary condition
- Rear to side boundary condition
- Side to side boundary condition
- Rear to laneway boundary condition

Design Objectives

Introduce a generous landscape setback in the block bounded by Como Street and Yarralea Street - Location 1.

The existing PAO in this location requires buildings to setback from the street in the order of 12m. This provides an opportunity to create a landscape setback that could provide opportunities for retail, cafes (outdoor dining) that is setback from the traffic of Heidelberg Road.

West of Yarralea Street this opportunity has not been pursued as the location of existing heritage buildings conflict with the PAO location.

Respond to existing valued character, including heritage buildings and fine-grain shopfronts on the block bounded by Yarralea Street and Park Avenue - Location 2.

There are potentially five sites of heritage significance that have a zero metre setback to the street. This character should be continued along this street interface.

Improve the quality of Heidelberg Road by creating a comfortable sense of enclosure and definition to the street - Location 3.

This can be achieved through the introduction of a street wall height that provides a positive interface to the street but which does not visually dominate. This balance is particularly important to achieve considering the poor quality of the street environment. Buildings that are visually overwhelming will exacerbate the impact of heavy traffic on the pedestrian experience.

Ensure development does not visually dominate or unreasonably overshadow private open space in adjacent residential areas - Location 4.

The neighbouring residential properties all incorporate private open space at the rear of each dwelling.

Sunlight should be provided at the equinox according to the current level of provision required in Clause 54 and 55 of the planning scheme.

Minimise the impact of vehicular crossovers - Location 5.

Vehicular access to most sites is provided from Heidelberg Road. This includes shared access for a number of sites. There are five existing crossovers where alternative access can be provided. No additional vehicular crossovers are supported.

C. Determining Heidelberg Road development scale

700-718 Heidelberg Road

8 Storeys - Option 1

Street wall: 4 storeys
Upper level setback: 3 metres

- 8 storey buildings are visually dominant, creating a wall of development.
- Creates an uncomfortably proportioned building where the lower and upper levels are of equal heights.



8 Storeys - Option 2

Street wall: 4 storeys
Upper level setback: 6 metres

- 8 storey buildings are visually dominant.
- The 6 metre setback provides a negligible improvement on reducing this dominance.
- Creates an uncomfortably proportioned building where the lower and upper levels are of equal heights.



8 Storeys - Option 3

Street wall: 4 storeys
Upper level setback: 45 degree angle

- Creates awkward building forms that are uncomfortably proportioned.



Figure 122. Built form testing on 718 Heidelberg Road

Preferred development outcome

7 Storeys - Option 4

Street wall: 4 storeys
Upper level setback: 6 metres

- The 4-storey street wall provides a balanced street wall height against the street width.
- The 6m setback reduces the visual dominance of upper levels and supports heights of 7 storeys.

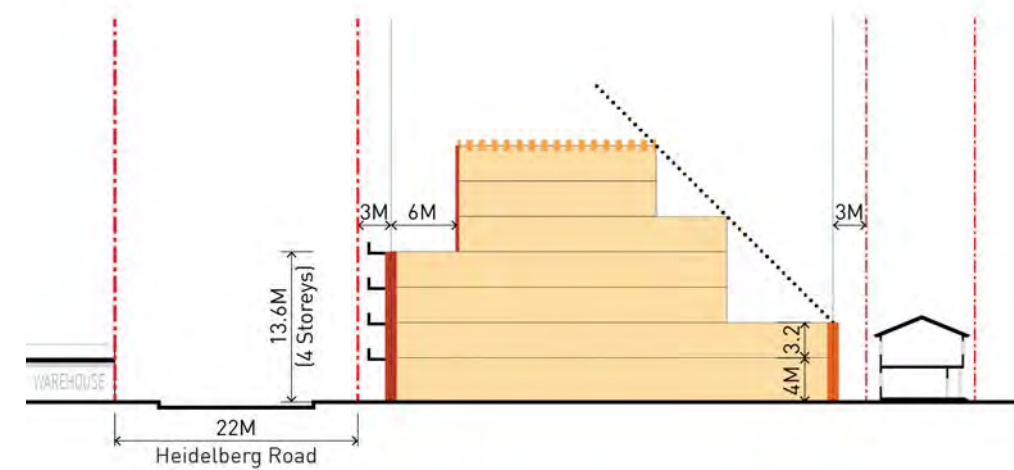


Figure 123. Proposed building envelope controls for Precinct 3B (718 Heidelberg Road)

Note: Ground level setback to rear boundary may be required depending on relationship to existing dwelling location (refer to Figure 15 on page 13)

Precinct 3B - Neighbourhood Activity Centre

Between Park Avenue and Yarralea Streets

6 Storeys - Option 1

Street wall: 1-2 storeys
Upper level setback: 3 metres

- 6 storey buildings are visually dominant, creating a wall of development.
- Upper levels are too dominant for the context.
- Creates an uncomfortably proportioned building above existing heritage buildings.



6 Storeys - Option 2

Street wall: 1-2 storeys
Upper level setback: 6 metres

- 6 storey buildings are visually dominant, creating a wall of development.
- Upper levels are too dominant for the context.
- Creates an uncomfortably proportioned building above existing heritage buildings.



6 Storeys - Option 3

Street wall: 1-2 storeys
Upper level setback: 45 degree angle

- Creates awkward building forms that are uncomfortably proportioned.



Figure 124. Built form testing on heritage block

Preferred development outcome

6 Storeys - Option 4

Street wall: 1-2 storeys
Upper level setback: 6 metres & 45 degree angle (above 5th floor)

- Overall 5 storey height frames the street without being overly visually dominant. The 6m setback creates a more distinctive street wall which enhances the existing character and heritage buildings.
- Creates a comfortably proportioned building where the lower levels support integration with the existing heritage buildings within the street.

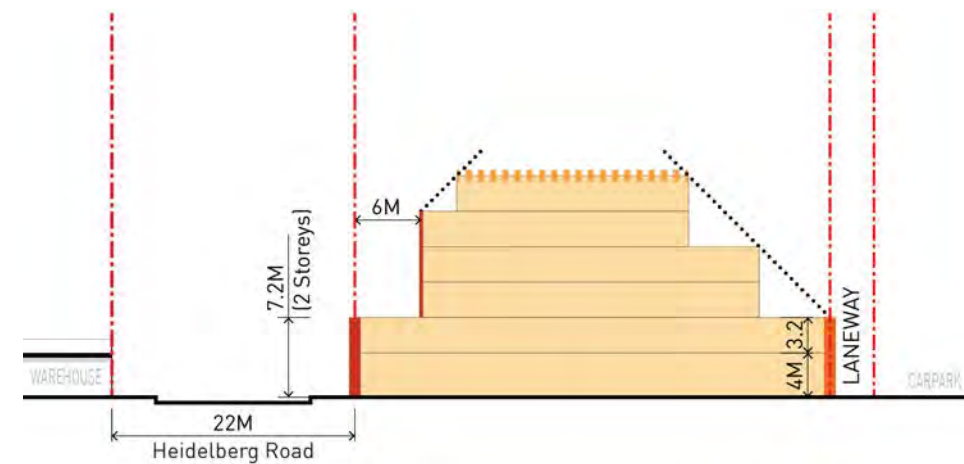


Figure 125. Proposed building envelope controls for Precinct 3B (heritage block)

Note: Ground level setback to rear boundary may be required depending on relationship to existing dwelling location (refer to Figure 15 on page 13)

Proposed built form envelopes (section) with indicative floor levels illustrated within this envelope for reference only. These are not intended to illustrate acceptable building designs.

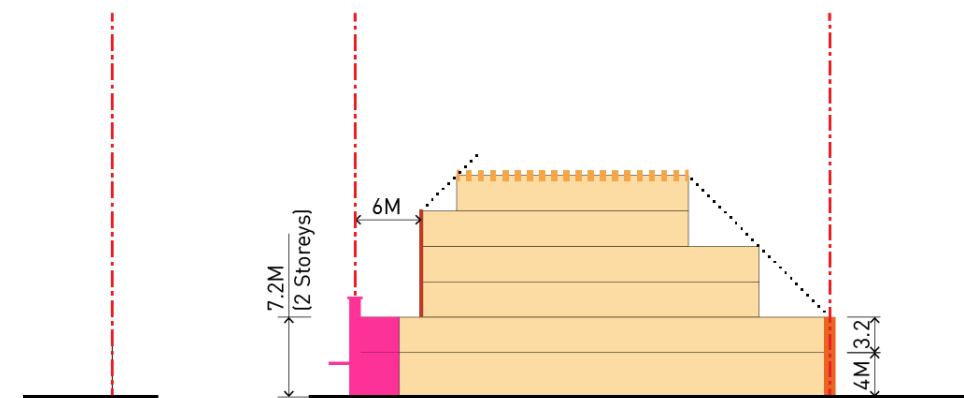


Figure 126. Proposed built form envelopes (section) in response to existing heritage building

Between Yarralea Street and Como Street (with PAO overlay)

6 Storeys - Option 1

Street wall: 4 storeys
Upper level setback: 3 metres

- Overall 6 storey height frames the street without being overly visually dominant.
- Creates a comfortably proportioned building where the lower levels support integration with the existing heritage buildings within the street.



Preferred development outcome

6 Storeys - Option 2

Street wall: 4 storeys
Upper level setback: 6 metres

- Overall 6 storey height frames the street without being overly visually dominant. The 6m setback creates a more distinctive street wall.
- Creates a comfortably proportioned building where the base of the building is prominent and upper levels are recessed.



6 Storeys - Option 3

Street wall: 6 storeys
Upper level setback: N/A

- Street wall height is too dominant for the context.



Figure 128. Built form testing for Precinct 3B (eastern block)

D. Building envelope controls

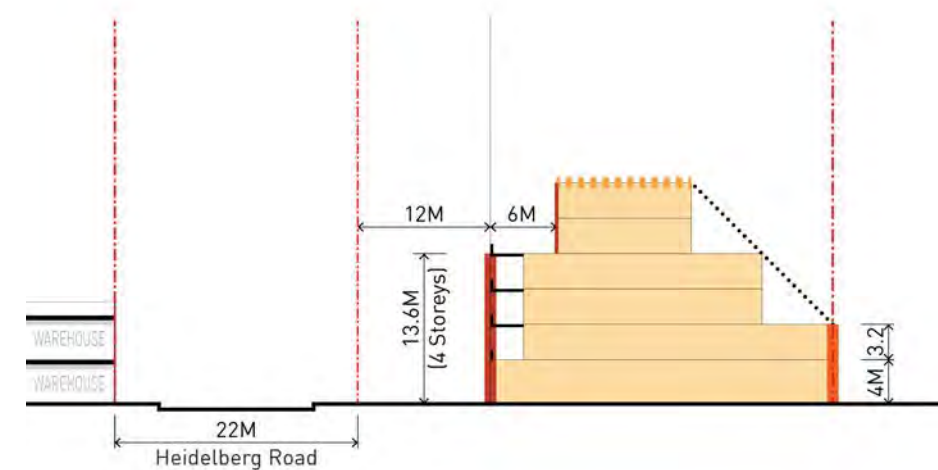


Figure 127. Proposed building envelope controls for Precinct 3B (eastern block)

Note: Ground level setback to rear boundary may be required depending on relationship to existing dwelling location (refer to Figure 15 on page 13)

Key recommendation

Precinct 3 - 700-718 Heidelberg Road

Introduce a 8-storey building height control in Precinct 3 (718 Heidelberg Road) with a 4 storey street wall and 6 metre setback. Above six storeys additional setbacks are required at a 45 degree angle.

Precinct 3 - Between Park Avenue and Yarralea Streets

Introduce a 5-storey building height control with a 2 storey street wall and 6 metre setback. Above five storeys additional setbacks are required at a 45 degree angle.

Precinct 3 - Between Yarralea Street and Como Street (with PAO overlay)

Introduce a 6-storey building height control with a 4 storey street wall and 6 metre setback.

Precinct 3B - Neighbourhood Activity Centre

Interface to Heidelberg Road – 718 Heidelberg Road

The proposed relationship to Heidelberg Road is illustrated below.

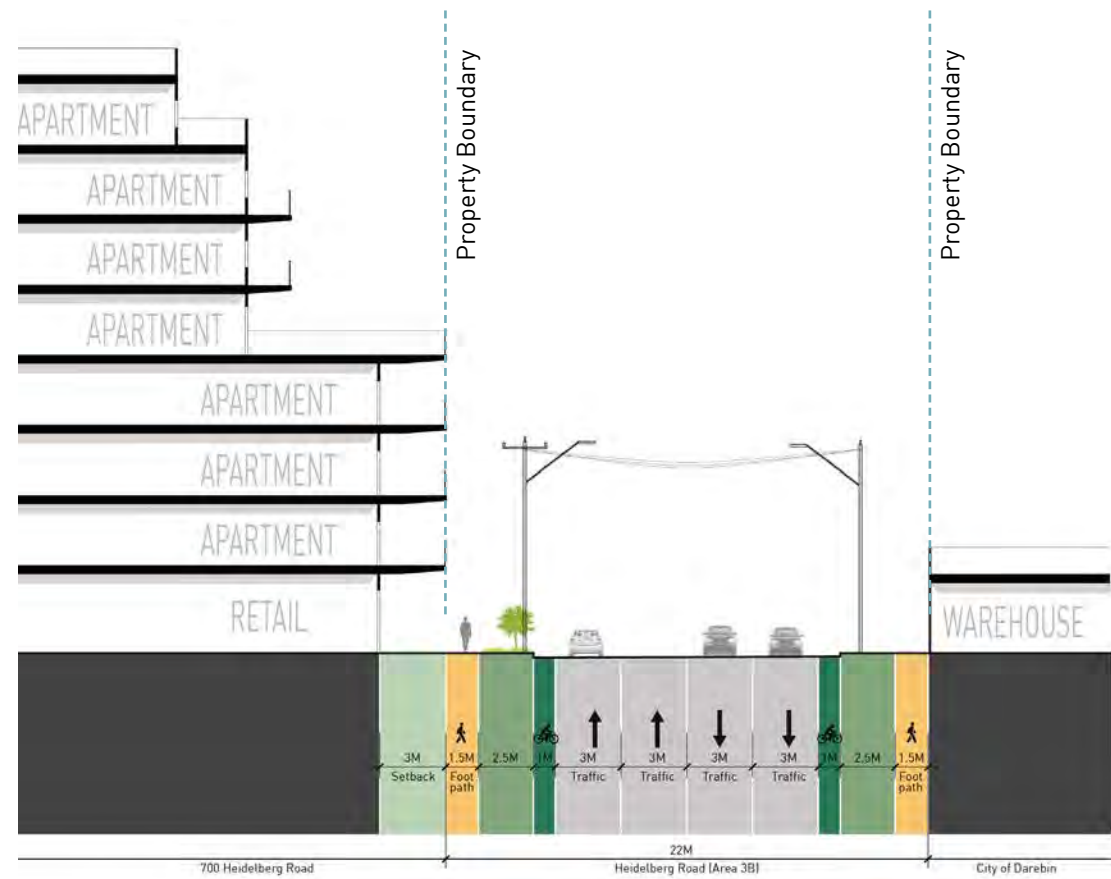


Figure 130. Proposed street section - full street section

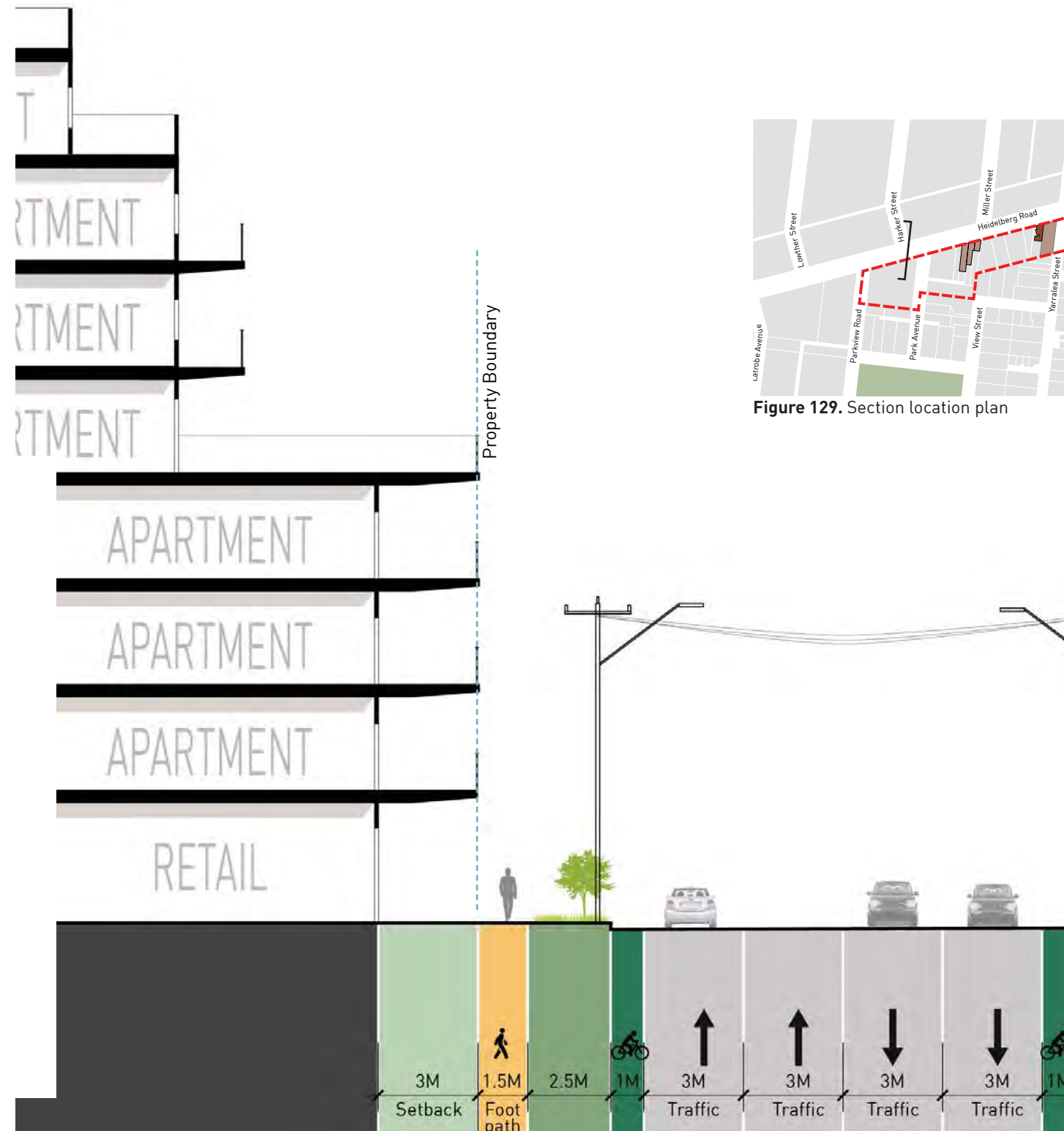


Figure 131. Proposed street section - detailed street section



Figure 129. Section location plan

Interface to Heidelberg Road – Between Park Avenue and Yarralea Street

The proposed relationship to Heidelberg Road is illustrated below.

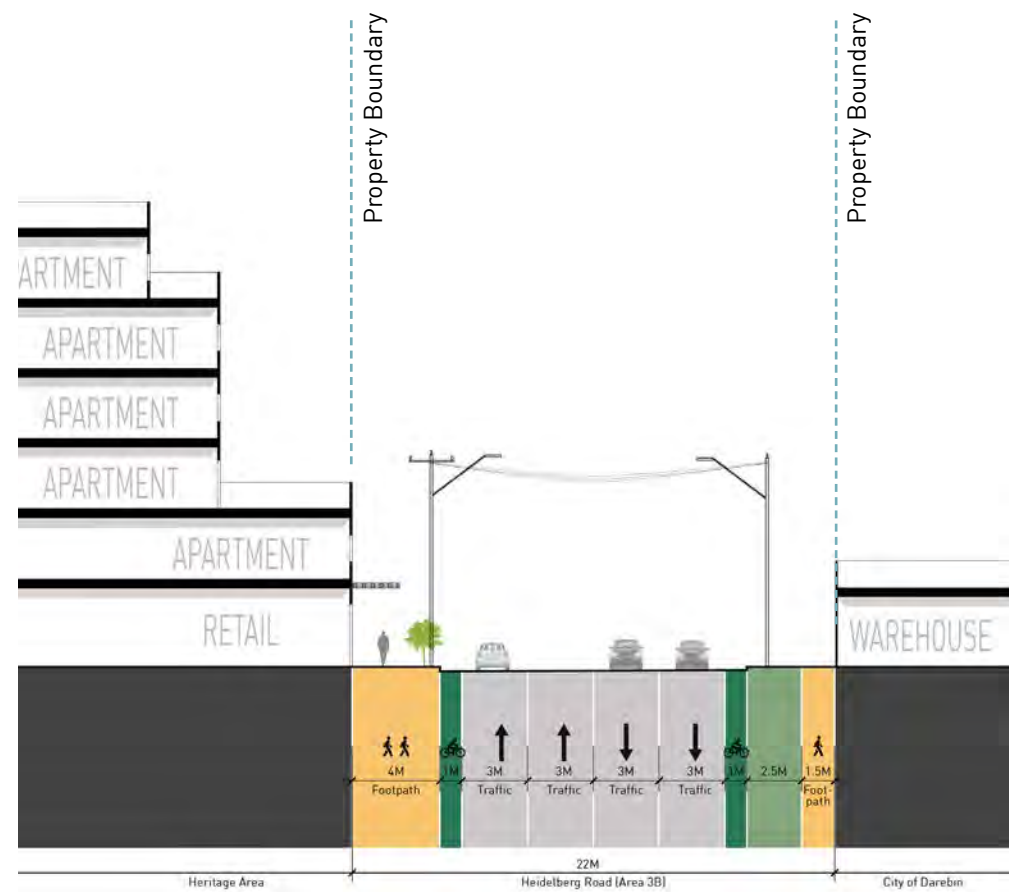


Figure 133. Proposed street section - full street section

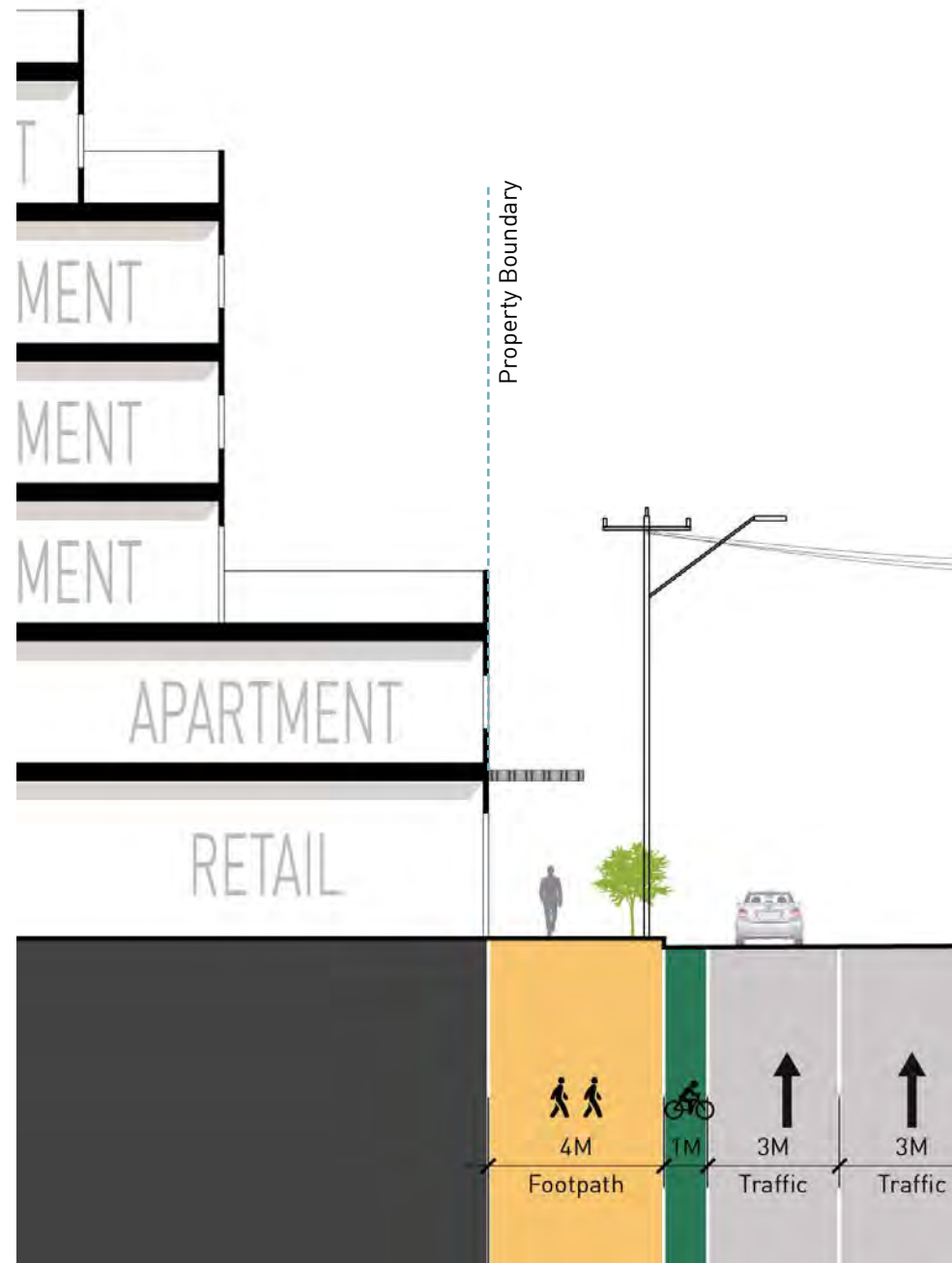


Figure 134. Proposed street section - detailed street section

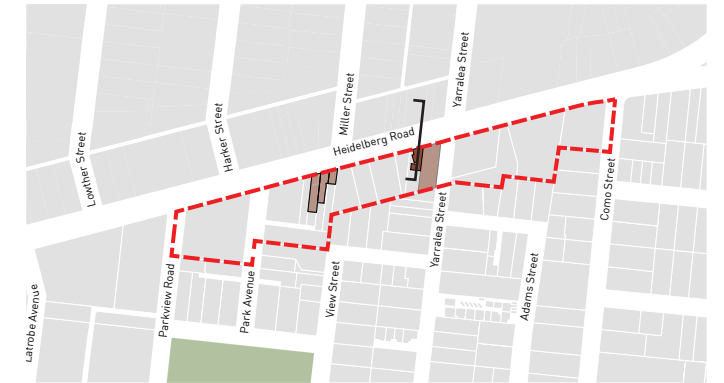


Figure 132. Section location plan

Precinct 3B - Neighbourhood Activity Centre

Interface to Heidelberg Road – East of Yarralea Street (PAO overlay)

The proposed relationship to Heidelberg Road is illustrated below.

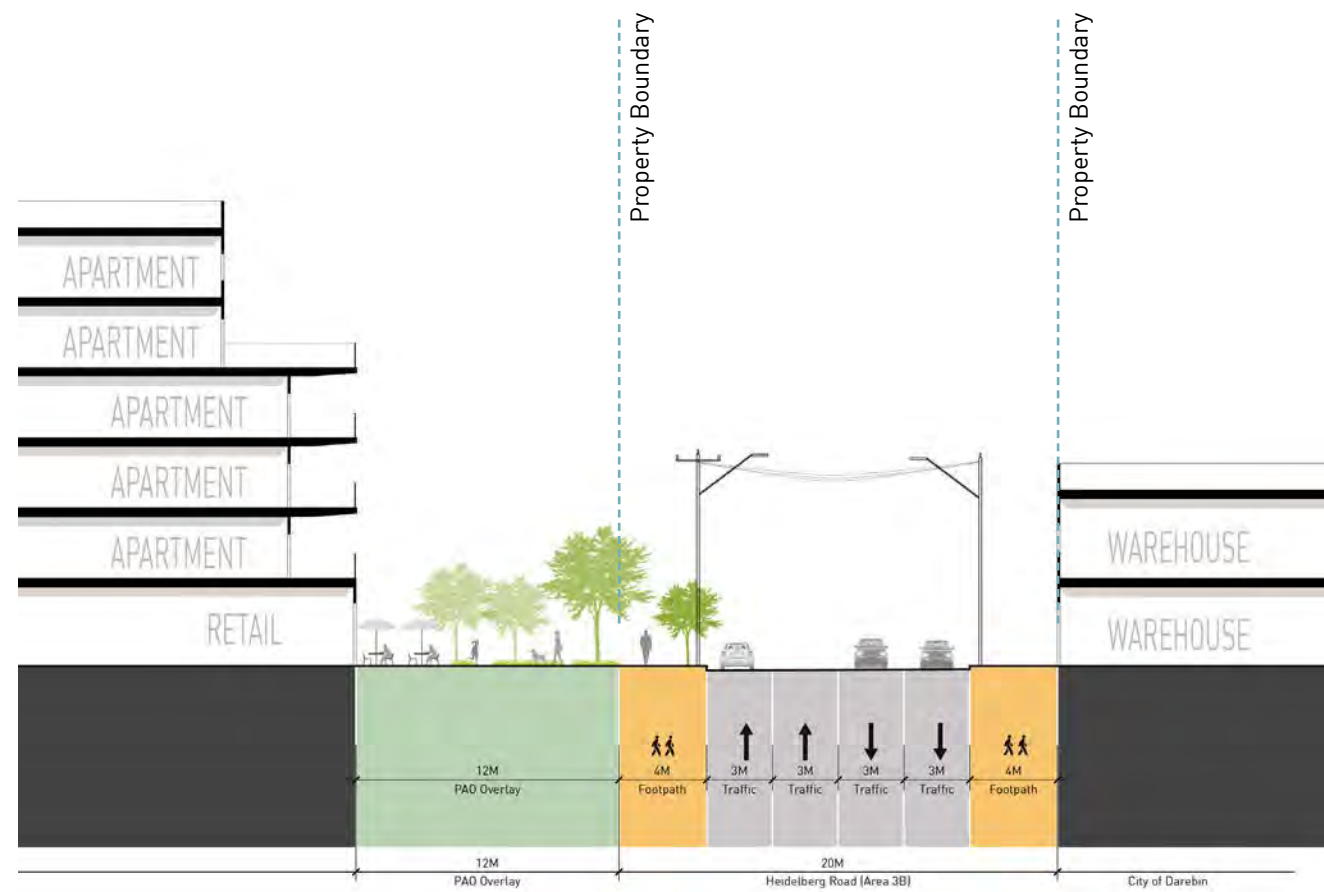


Figure 136. Proposed street section - full street section

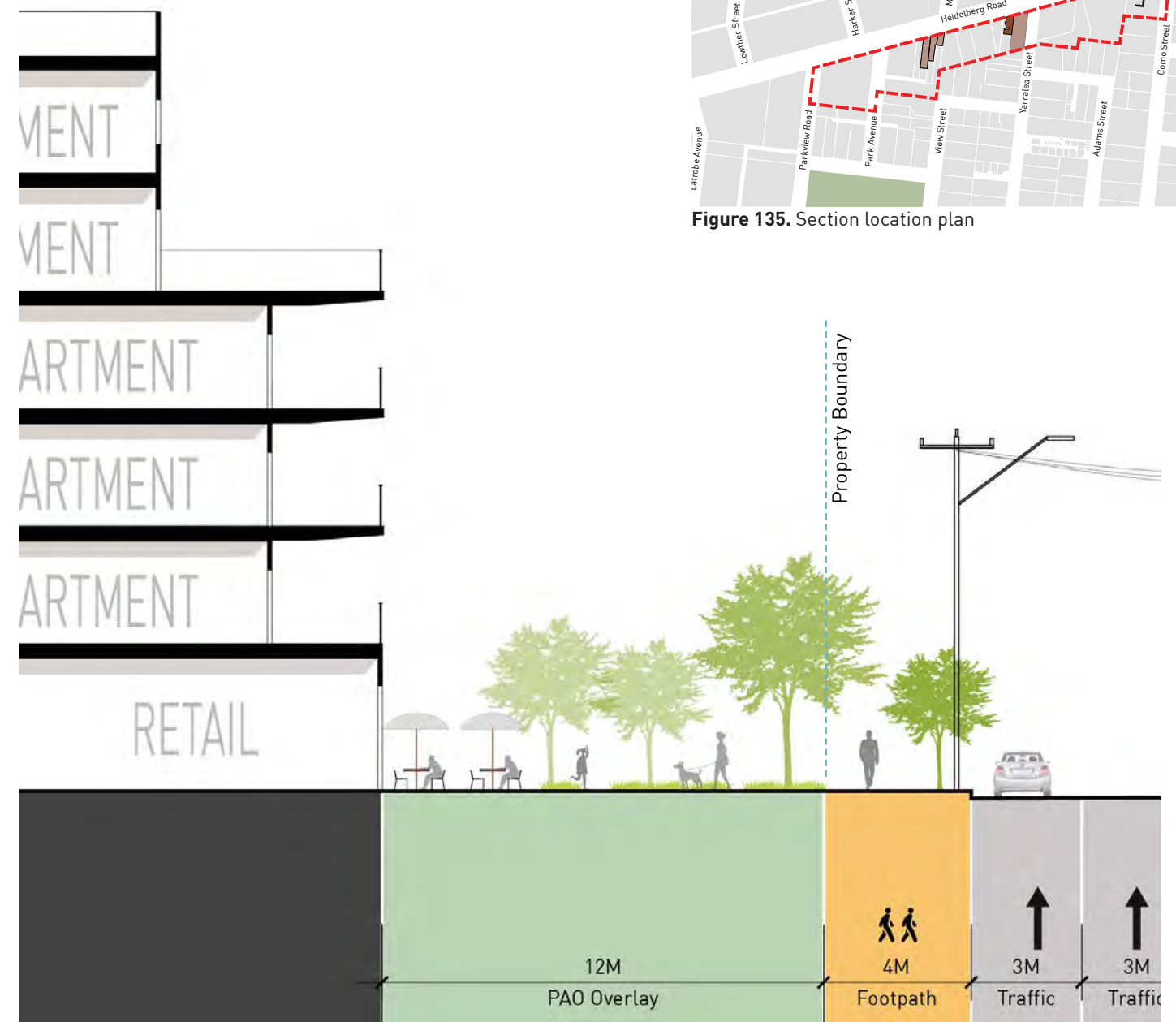


Figure 137. Proposed street section - detailed street section



Figure 135. Section location plan

E. Confirming overshadowing requirements



Figure 138. Cumulative shadow impact of 8 metre high boundary wall condition

- Private open space has more than 5 hours sunlight between 9 am and 3 pm on 22 September
- Shadow between 9 am and 3 pm on 22 September

Note 1: Assessment utilises the building footprints that are documented in Council's GIS mapping.

Precinct 3B - Neighbourhood Activity Centre

F. Precedent examples - Precinct 3B



Figure 139. Nine Smith St, Fitzroy (Source: Neometro)



Figure 140. George Corner, Fitzroy (Source: Neometro)



Figure 141. Nightingale 1, Brunswick (Source: Breathe Architecture)

G. Built form testing of preferred envelope controls

Testing site 3B-1 – the block between Yarralea Street and Parkview Avenue

Location: 730 Heidelberg Road			
Site area: 348m ²	Lot width: 6.4M	Lot depth: 50M	Character/use: Narrow and deep site Potential heritage site
Location: 732 Heidelberg Road			
Site area: 370m ²	Lot width: 8M	Lot depth: 53M	Character/use: Narrow and deep site Potential heritage site
Location: 734 Heidelberg Road			
Site area: 363m ²	Lot width: 7.3M	Lot depth: 56M	Character/use: Narrow and deep site Potential heritage site
Location: 736 Heidelberg Road			
Site area: 740m ²	Lot width: 12M	Lot depth: 38M	Character/use: Automotive service
Location: 738 Heidelberg Road			
Site area: 600m ²	Lot width: 16M	Lot depth: 38M	Character/use: Fitness
Location: 740 Heidelberg Road			
Site area: 864m ²	Lot width: 24M	Lot depth: 38M	Character/use: Warehouse
Location: 750 Heidelberg Road			
Site area: 316m ²	Lot width: 14M	Lot depth: 38M	Character/use: Electricity
Location: 756 Heidelberg Road			
Site area: 450m ²	Lot width: 12M	Lot depth: 38M	Character/use: Heritage overlay
Location: 760 Heidelberg Road			
Site area: 611m ²	Lot width: 16M	Lot depth: 38M	Character/use: Potential heritage site



Precinct 3B - Neighbourhood Activity Centre

Testing site 3B-1

732 Heidelberg Road

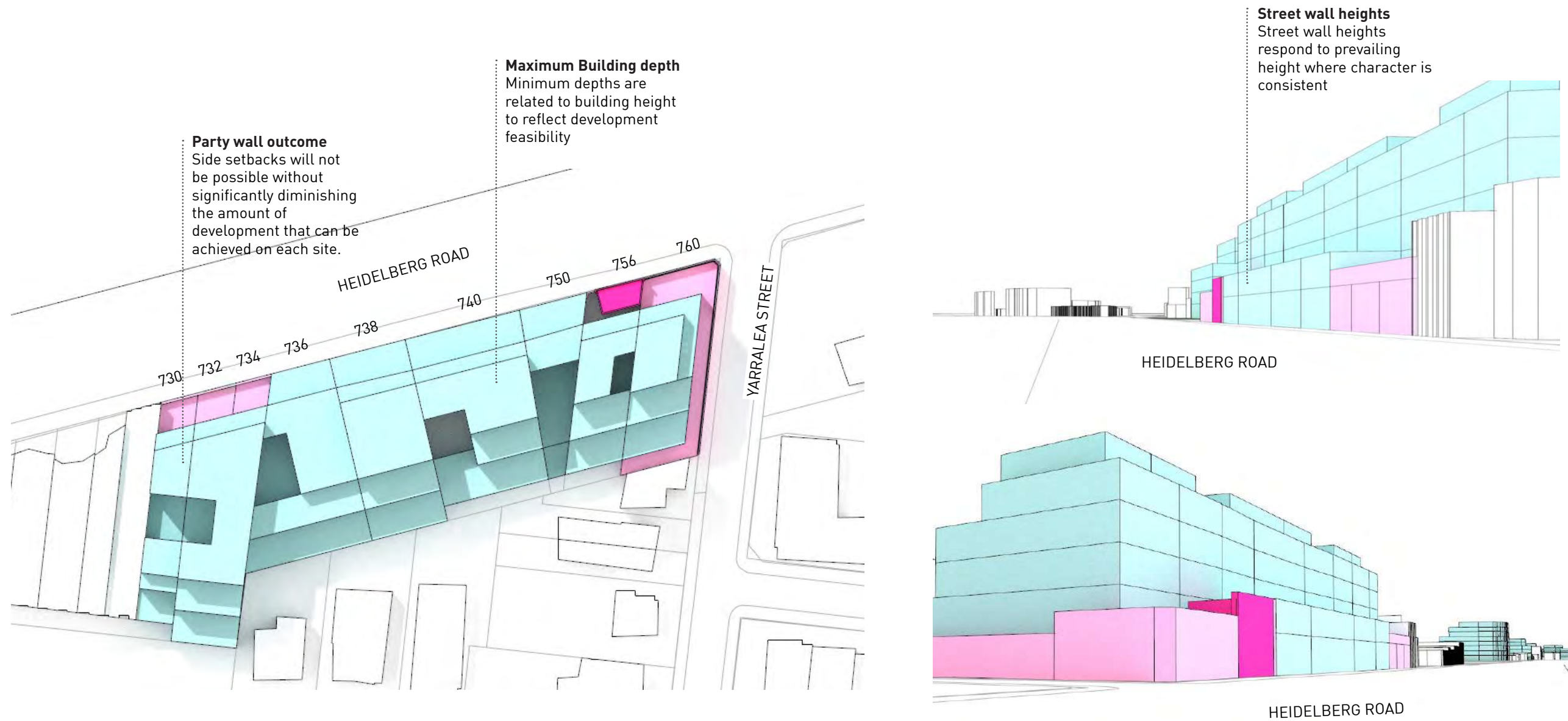


Figure 142. Built form testing – plan and perspective views

Testing site 3B-2 – PAO overlay site

Location: 800 Heidelberg Road			
Site area: 2,260m ²	Lot width: 53M	Lot depth: 55M	Characters: Red Rooster PAO overlay

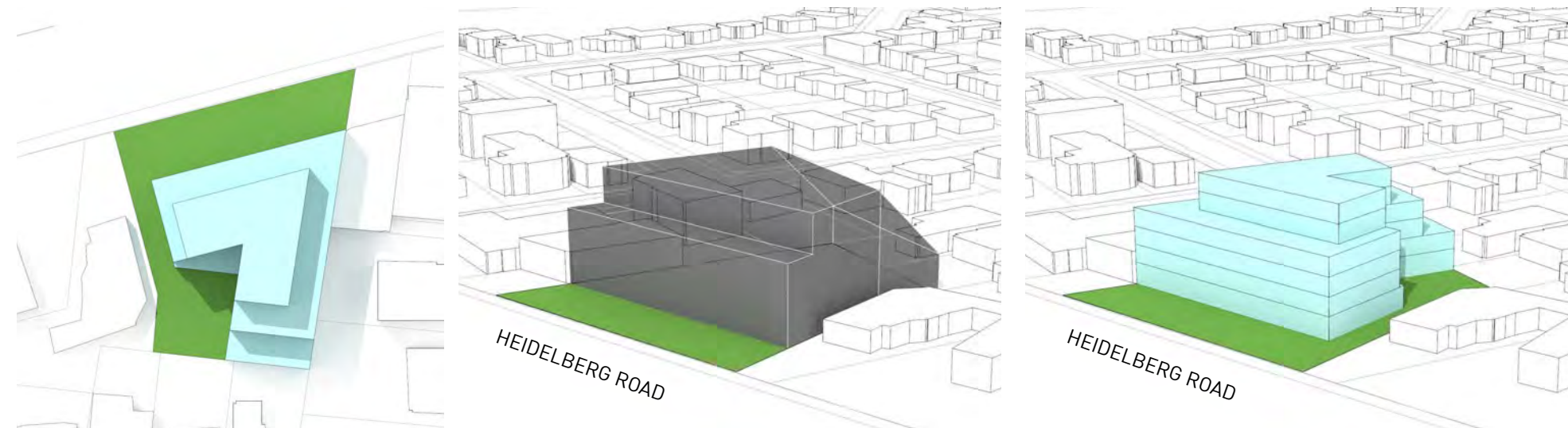


Figure 143. Built form testing – plan, built form envelope and perspective view)



Precinct 3B - Neighbourhood Activity Centre

H. Proposed built form controls

The proposed building envelope controls are illustrated in the following plans.

700-718 Heidelberg Road

Considering the unique site attributes and the need to support design flexibility with certainty that minimum amenity standards are met, a mix of mandatory and discretionary controls are proposed on 718 Heidelberg Road which is identified as a strategic site as follows:

Discretionary

- Overall height limit
- Street wall heights
- Upper level setback to street

Mandatory

- Front setbacks to all streets
- Rear interface controls (maximum rear interface heights, ground level and upper level setbacks)

Remaining sites in Precinct 3B

On all other sites, mandatory controls are proposed for all envelope controls.



Figure 146. Precinct 3B - Proposed ground floor setbacks



Figure 144. Precinct 3B - Proposed overall building heights

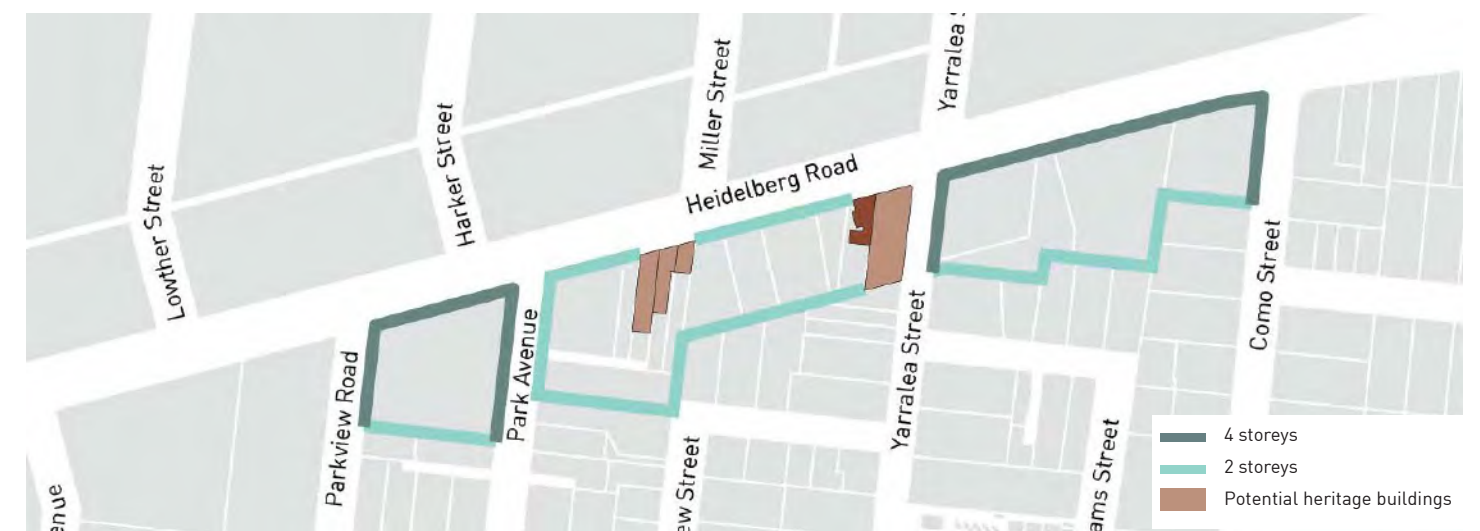


Figure 145. Precinct 3B - Proposed street wall heights / building heights along residential interface boundaries

4. Summary of recommendations

4.1 Summary of proposed controls

The following development controls are recommended within this report:

- Maximum building heights (refer Figure 147 and Table 1)
- Minimum ground floor setbacks (refer Figure 148 and Table 1)
- Maximum street wall heights (refer Figure 149 and Table 1)
- Minimum upper level setbacks from street (refer Table 1)

- Maximum rear interface building height (refer Table 1)
- Minimum upper level setbacks above the rear boundary building height (refer Table 1)
- Minimum rear boundary ground level setback (refer Table 1)
- Overshadowing requirements to residential interfaces (refer Table 1)
- Upper level building setbacks from side boundaries (refer Table 2)

- Building separation within sites (refer Table 2)
- Design principles (refer Table 3).

Proposed building heights

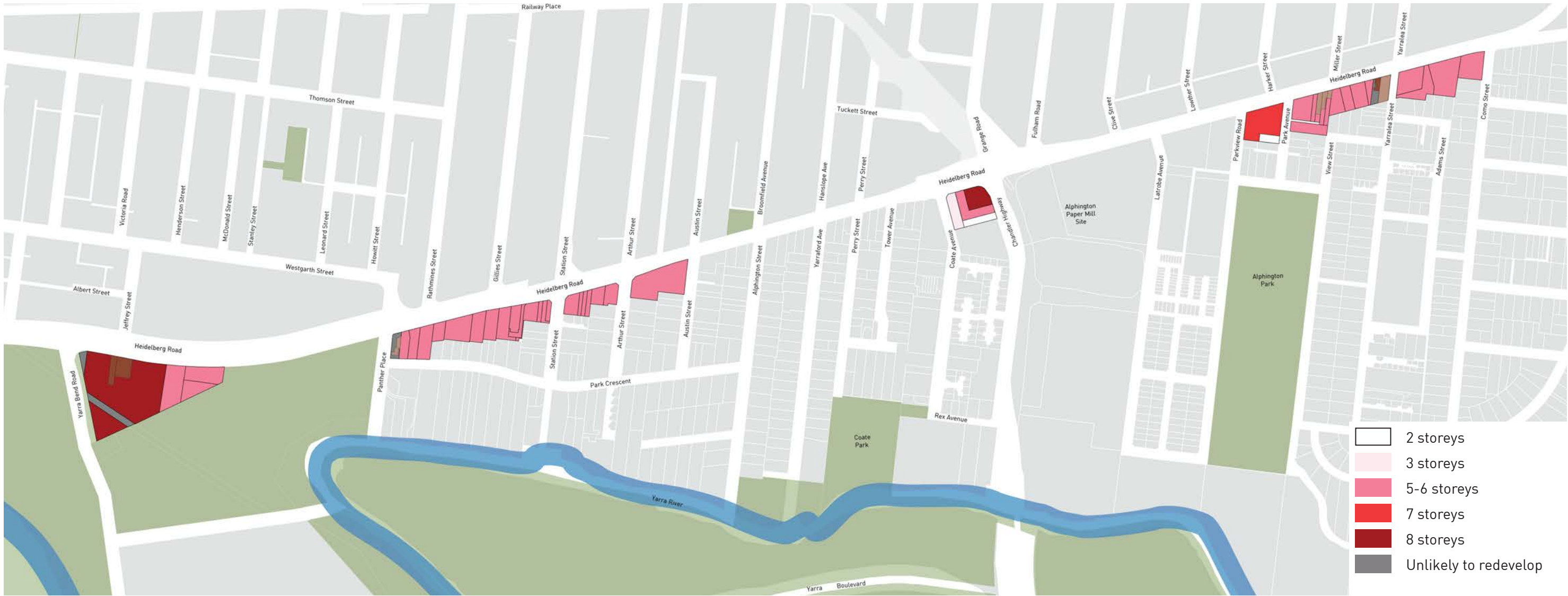


Figure 147. Proposed building heights (all precincts)

Proposed Ground Floor Setbacks

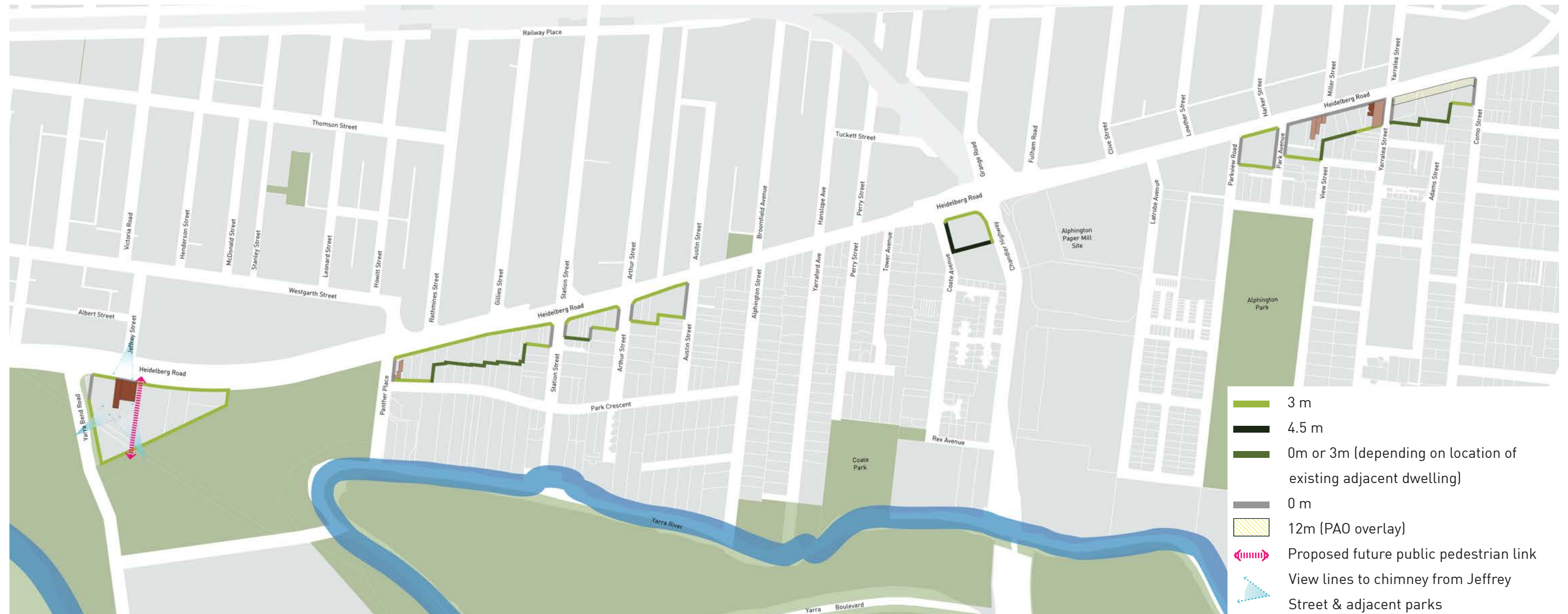


Figure 148. Proposed ground floor setbacks (all precincts)

Proposed street wall height and rear interface building heights



Figure 149. Proposed street wall heights (all precincts)

Summary of development controls (all precincts)

Precinct	Maximum building height	Preferred street wall height	Maximum street wall height	Ground floor setback to street(s)	Minimum upper level setback from street(s)	Maximum rear interface height	Upper level setbacks (above rear interface)	Minimum rear boundary setback	Overshadowing
Precinct 1	6 / 8 storeys (20m/27m)	4-6 storeys	6 storeys	3 metres	6 metres	4 storeys (park)	A setback of 45 degrees applies about the maximum rear interface building height. A maximum of two steps within the buildings are strongly encouraged.	3 metres (to park)	Overshadowing of adjacent residential properties to comply with Clause 54 and 55 of the Yarra Planning Scheme.
Precinct 2	6 storeys (20m)		4 storeys	3 metres	6 metres	2 storeys (8 metres)		0 metres (if adjacent dwelling is located 15m or more from the property boundary) 3 metres (if adjacent dwelling is located less than 15m from the property boundary)	
Precinct 3A	8 storeys (27m)		On Heidelberg Road, 3-8 storeys from Coate Avenue to Chandler Highway	3 metres to Heidelberg Road and Chandler Highway 4.5 metres to Coate Avenue	6 metres to Heidelberg Road and Chandler Highway 14.5 metres to Coate Avenue above 3rd storeys	2 storeys (8 metres)		4.5 metres	
Precinct 3B – 700-718 Heidelberg Road	7 storeys (24m)		4 storeys	3 metres	6 metres	2 storeys (8 metres)		0 metres (if adjacent dwelling is located 15m or more from the property boundary) 3 metres (if adjacent dwelling is located less than 15m from the property boundary)	
Precinct 3B – Between Park Avenue and Yarralea Street	6 storeys (20m)		2 storeys	0 metres	6 metres, with an additional 45 degree angle setback above level 5	2 storeys (8 metres)		0 metres (if adjacent dwelling is located 15m or more from the property boundary) 3 metres (if adjacent dwelling is located less than 15m from the property boundary)	
Precinct 3B – Between Yarralea Street and Como Street	6 storeys (20m)		4 storeys	12 metres	6 metres	2 storeys (8 metres)		0 metres (if adjacent dwelling is located 15m or more from the property boundary) 3 metres (if adjacent dwelling is located less than 15m from the property boundary)	

Table 1. Summary of building envelope controls for all precincts (excluding upper level building setbacks and building separation within a site)

Building height	Preferred separation (Suitable if there is a primary living space/ balcony facing the boundary)	Minimum separation (Suitable when the use is not a primary living space or balcony facing the boundary)
Up top 4 storeys	4.5m	3m
5-8 storeys	6m	3m

Table 2. Upper level building setbacks and building separation within a site - all precincts

Design principles

Active ground floor frontages required to all sites fronting Heidelberg Road
Multiple entrances to buildings on large sites is encouraged
Weather protection at entrances to buildings within Precincts 1, 2 and 3A (within 3 metre ground floor setback)
Continuous weather protection provided within Precinct 3B
Incorporate weather protection at entrances within the front setback and continuous weather protection in the Heidelberg Road Neighbourhood Activity Centre.
Locate all future carparking underground in basements.
Locate vehicular crossovers from rear lanes or side streets where possible.
Rationalise the number of existing crossovers to Heidelberg Road where multiple crossovers exist on single sites.
No additional vehicular crossovers are supported on Heidelberg Road.

Table 3. Design principles - all sites

Discretionary controls
 Mandatory controls

4.2 Extent of mandatory controls

The role of mandatory controls is guided through planning practice notes 59 and 60 (refer call-out box).

The detailed testing within this report leads to a recommendation for a combination of discretionary and mandatory controls on strategic sites, and mandatory controls on all other sites.

This is considered appropriate as:

- The mandatory controls support strategic objectives for development intensification.
- The rear interface controls for all development have been rigorously tested, are appropriate for the majority of proposals and provide for the preferred balance between development intensification and protection of amenity.
- The mandatory controls provide an efficient outcome - considering the interface between commercial and sensitive residential precincts, without certainty there will continue to be a significant number of objections and lack of clarity on the preferred development outcome. This has already been demonstrated through 3 recent VCAT cases.
- The majority of proposals not in accordance with the mandatory provisions will be clearly unacceptable. Considering the combination of a poor quality, heavily trafficked arterial and sensitive residential interfaces, the detailed testing in this report demonstrates the limited range of circumstances that provide a positive outcome to both interfaces.

Planning practice note 59: The role of mandatory provisions in Planning Schemes

This practice note sets out the criteria that can be used to decide whether mandatory provisions may be appropriate in planning schemes in Victoria.

It acknowledges that Victorian planning schemes are predominantly performance-based, with schemes specifying a clear objective and often a preferred development outcome while providing a degree of flexibility on how the objective is achieved.

Mandatory provisions are noted as the exception, however in circumstances where a mandatory provision will provide certainty and ensure a preferable and efficient outcome they can be supported.

The criteria that must be addressed include:

- Is the mandatory provision strategically supported (is there strategic basis)?
- Is the mandatory provision appropriate to a majority of proposals?
- Does the mandatory provision provide for the preferred outcome?
- Will the majority of proposals not in accordance with the mandatory provision be clearly unacceptable?
- Will the mandatory provision reduce administrative costs?

Planning practice note 60: Height and setback controls for activity centres

This practice note provides guidance on the state government's preferred approach to the application of height and setback controls for activity centres. It has been updated in response to the preliminary findings from the recent Activity Centre Pilot program.

It acknowledges the need to support development intensification. It notes that 'height and setback controls can be appropriate so long as they are not aimed at restricting the built form, but at facilitating good design outcomes'. The application of height and setback controls must be 'soundly based on the outcomes of strategic research and background analysis that demonstrates consistency with state and regional policy and includes a comprehensive built form analysis.

The Practice Note states that a combination of discretionary and mandatory height and setback controls may be appropriate. Discretionary height and setback controls are preferred, with mandatory provisions supported when they are justified by robust and comprehensive strategic work, or where exceptional circumstances warrant their introduction.

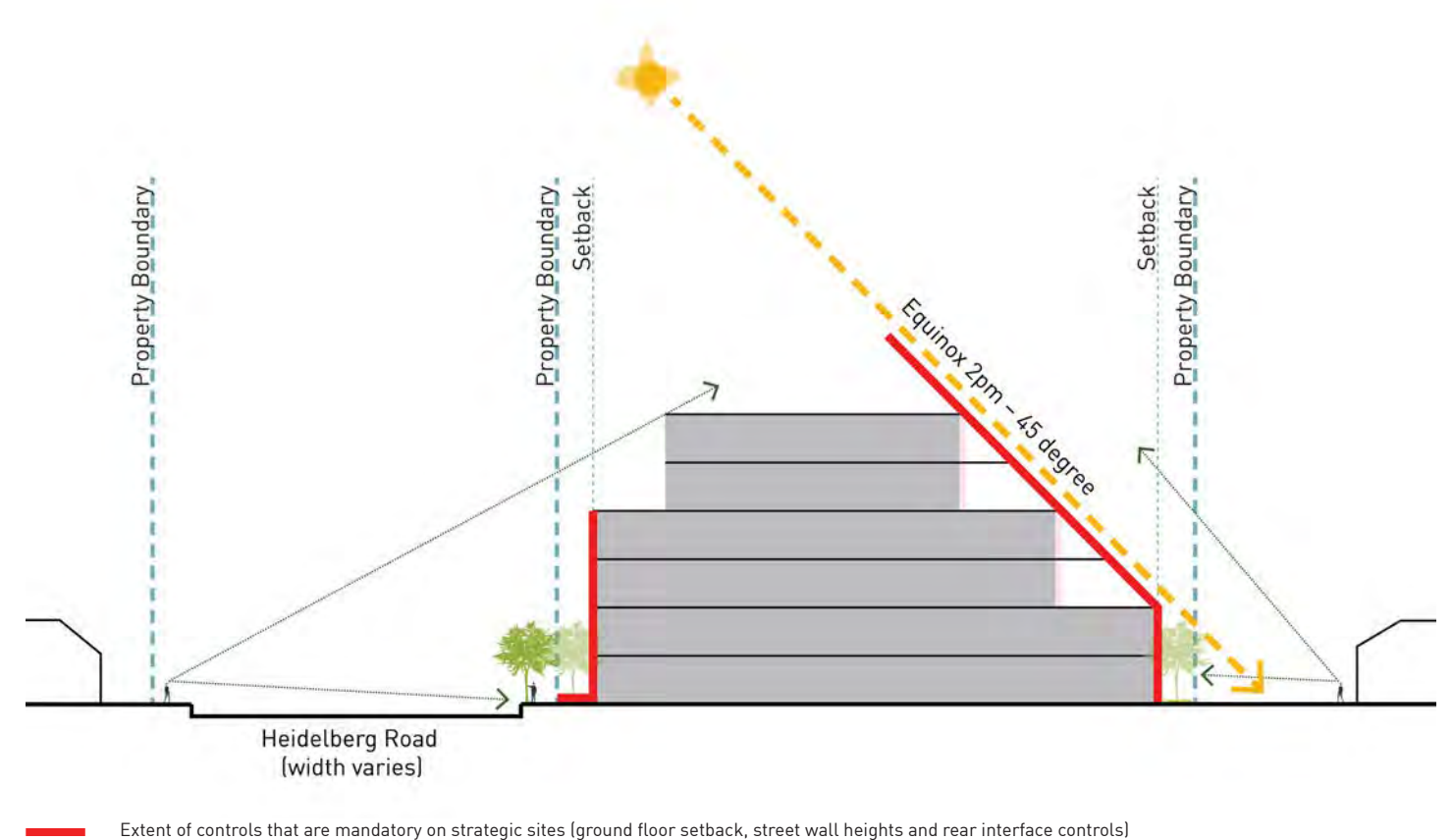


Figure 150. Summary of mandatory controls on strategic development sites. All controls are proposed as mandatory on all other sites

Appendix A - Overshadowing assessment

The overarching urban design strategy adopts the position that protecting existing secluded private open space to meet the requirements of Clause 54 and 55 is appropriate.

The following boundary wall heights have been tested to determine the maximum wall height that delivers this requirement.

- 4 metres
- 7.2 metres (4m commercial with one floor above)
- 8 metres (2 x 4m commercial floors)
- 12 metres (3 x 4m commercial floors)

The impact of these different boundary wall heights at the summer and winter solstice have been provided for illustrative purposes only.

Shadow study – Spring

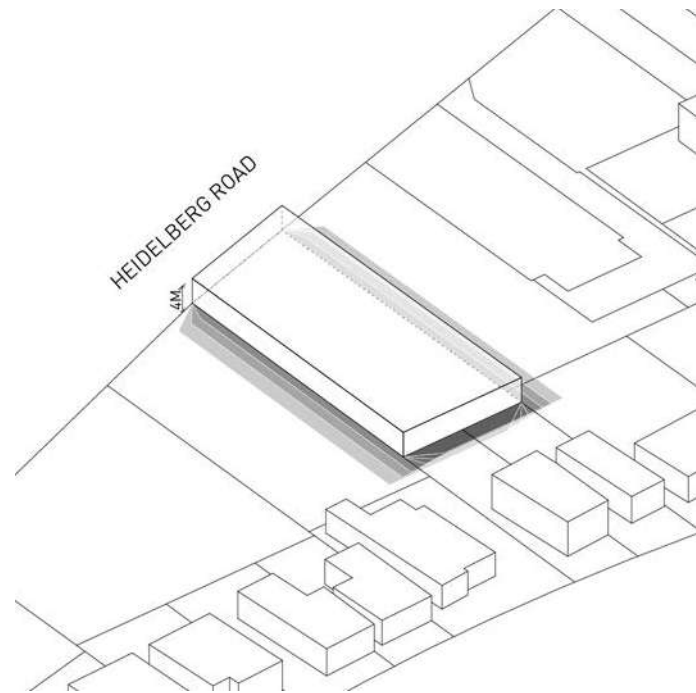


Figure 151. Extent of overshadowing of a 4 metre high wall on boundary. The overshadowing impacts are minimal.

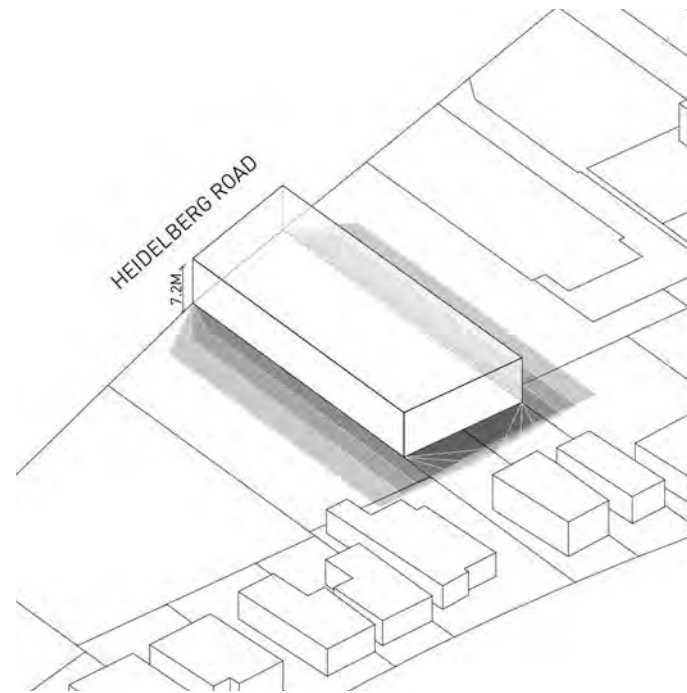


Figure 153. Extent of overshadowing of a 7.2 metre high wall on boundary. The overshadowing impacts are increased, however the minimum requirements of Clause 54 and 55 can be met.

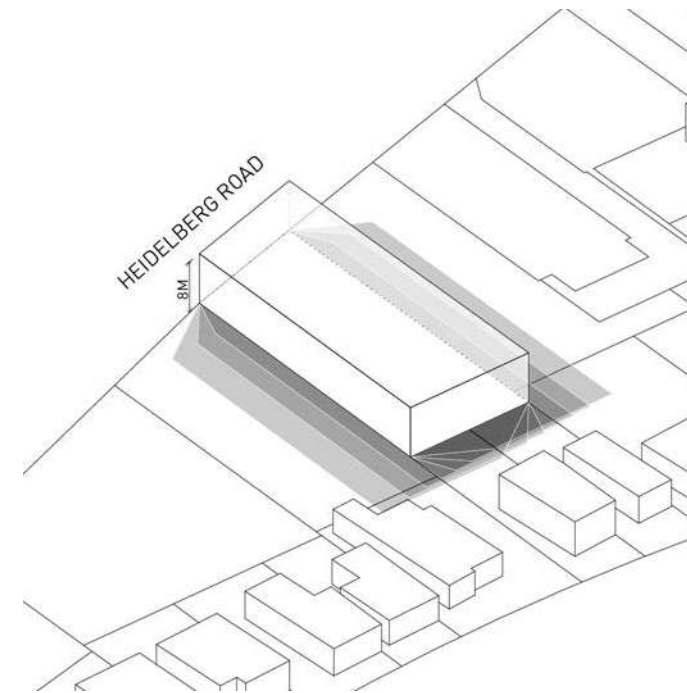


Figure 152. Extent of overshadowing of a 8 metre high wall on boundary. The overshadowing impacts are increased, however the minimum requirements of Clause 54 and 55 can be met.

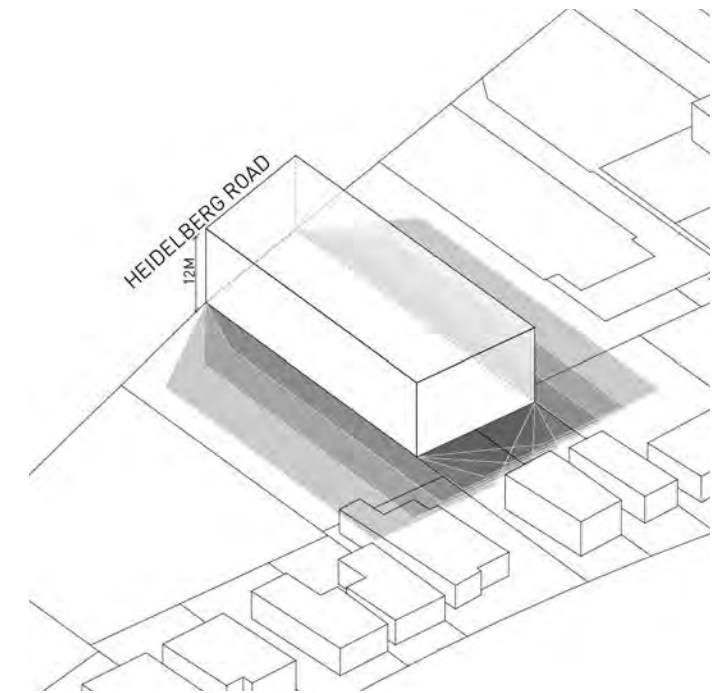


Figure 154. Extent of overshadowing of a 12 metre high wall on boundary. The overshadowing impacts are increased and the minimum requirements of Clause 54 and 55 can no longer be met.

Shadow study - Winter

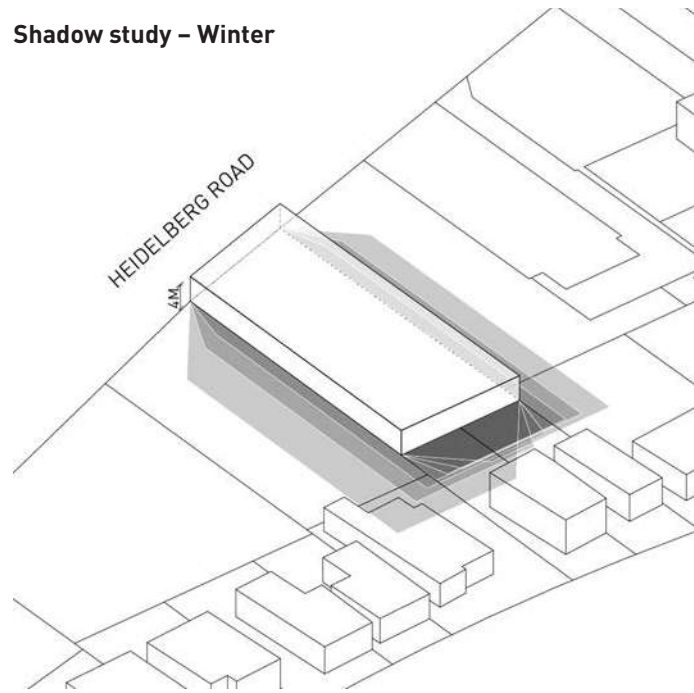


Figure 155. 4 metre high boundary wall

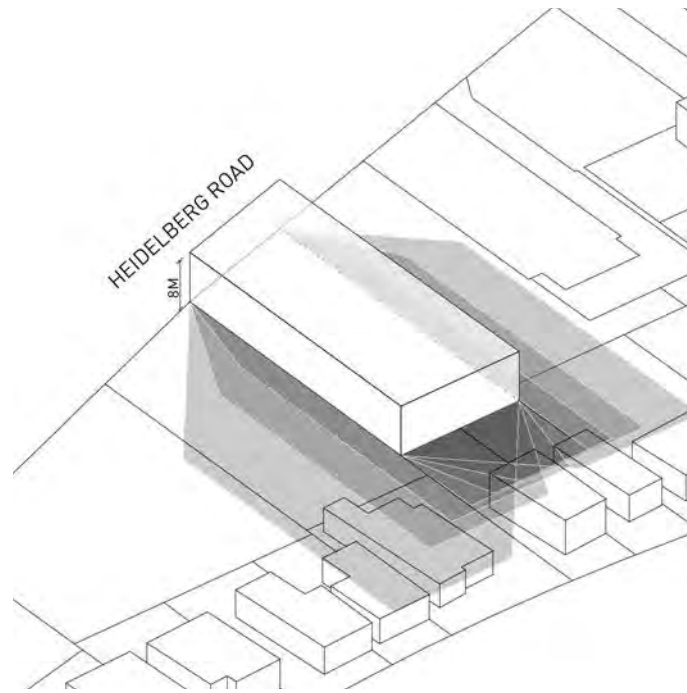


Figure 156. 7.2 metre high boundary wall

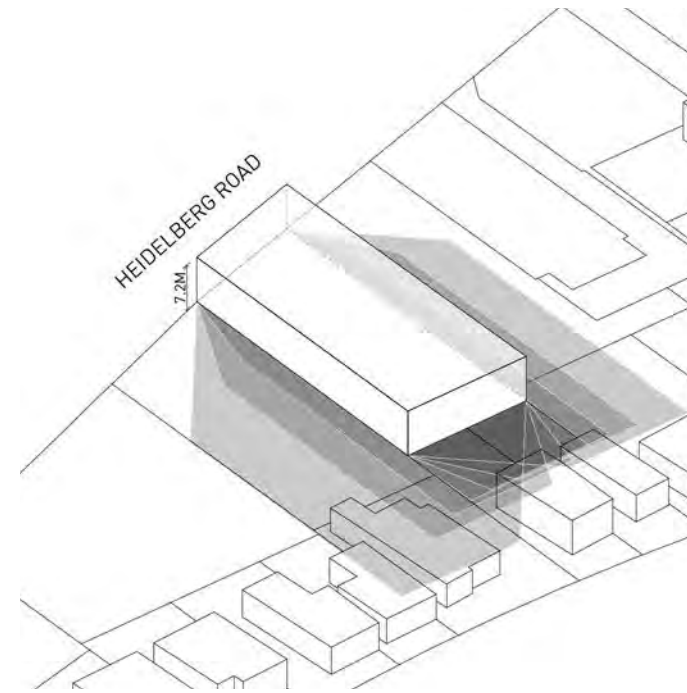


Figure 157. 8 metre high boundary wall

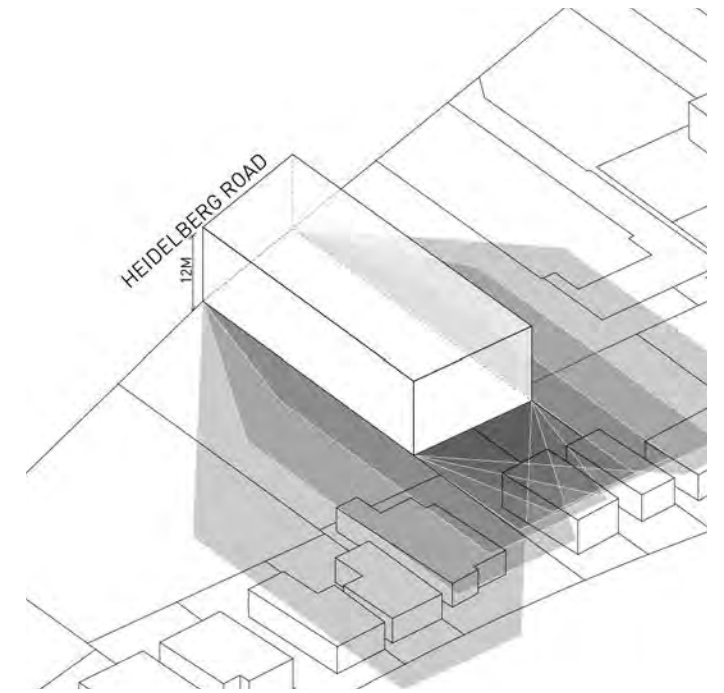


Figure 158. 12 metre high boundary wall

Shadow study - Summer

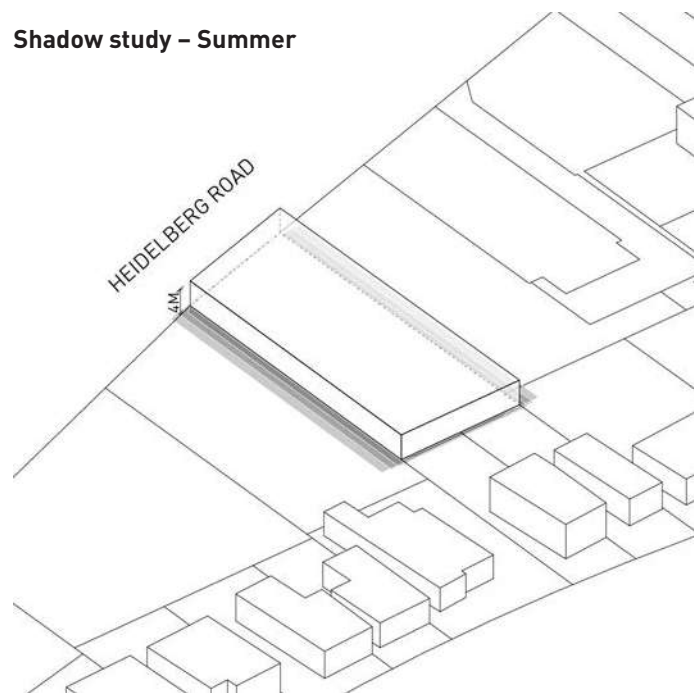


Figure 159. 4 metre high boundary wall

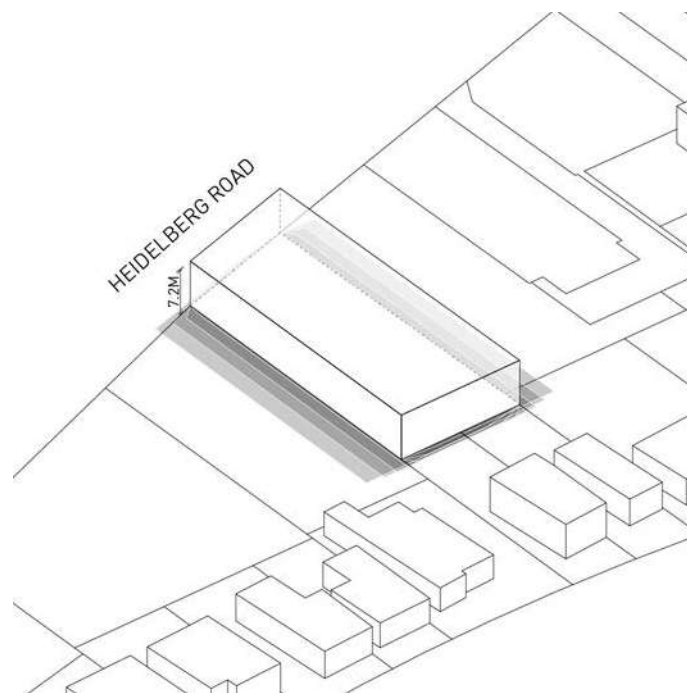


Figure 161. 7.2 metre high boundary wall

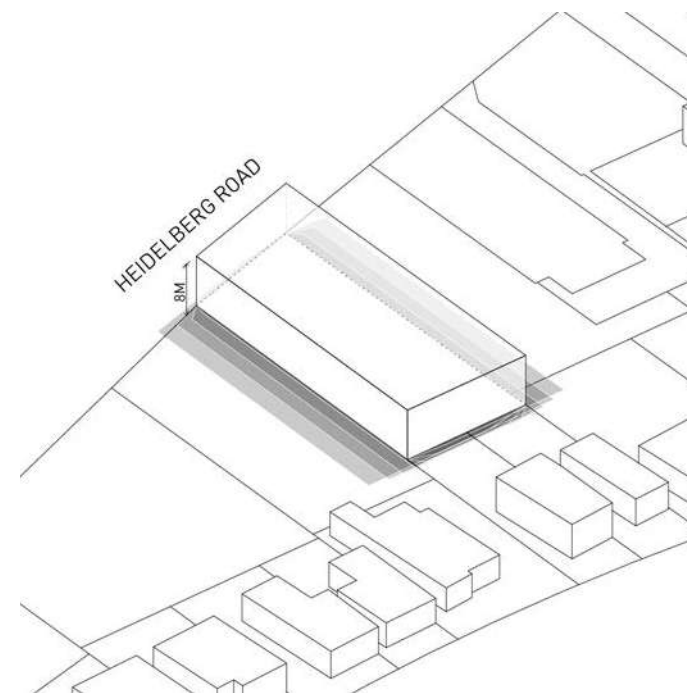


Figure 160. 8 metre high boundary wall

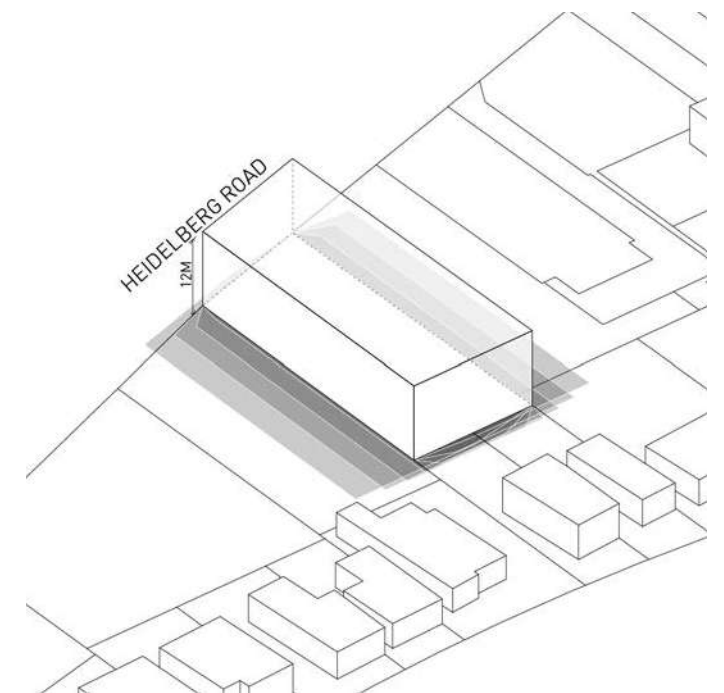


Figure 162. 12 metre high boundary wall

Precinct 1

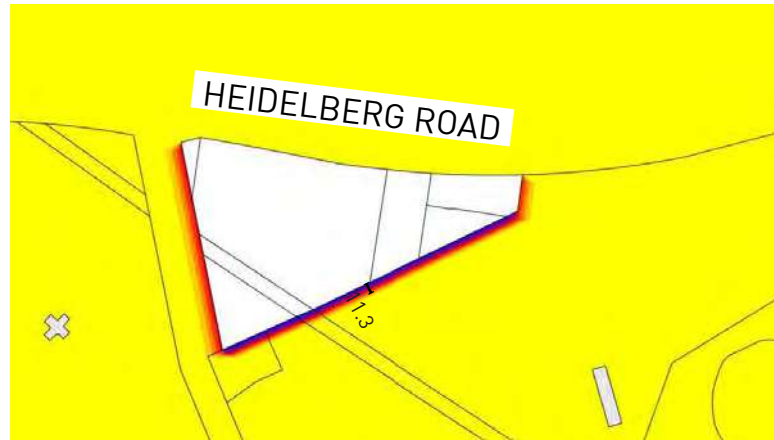


Figure 166. 7.2 metres rear street wall height



Figure 167. 10.4 metres rear street wall height

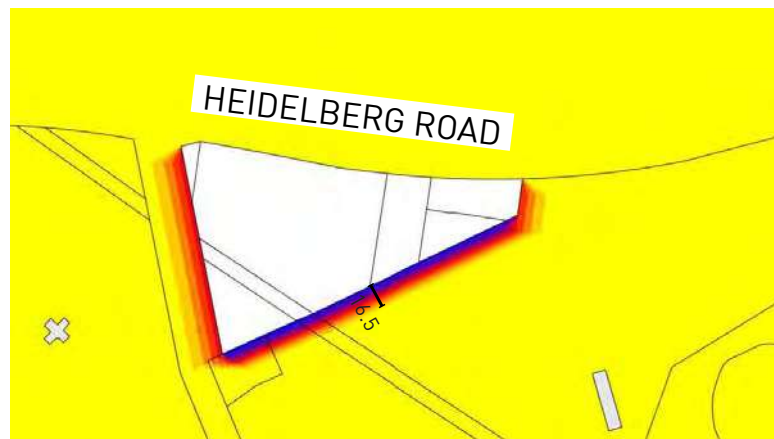


Figure 168. 13.6 metres rear street wall height

Precinct 3B



Figure 163. 4 metres rear street wall height



Figure 164. 7.2 metres rear street wall height with setback

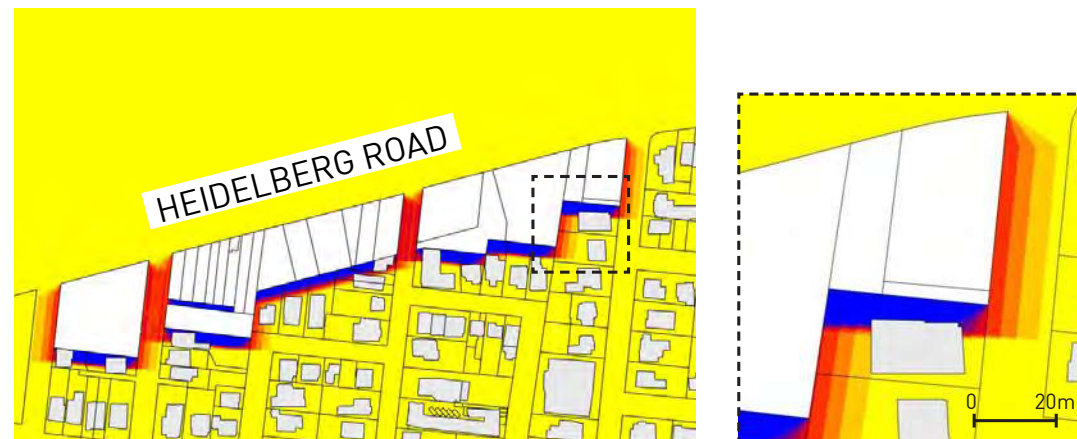
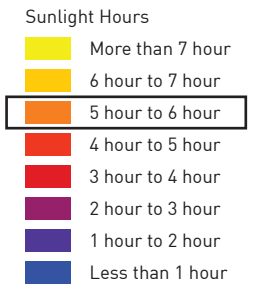


Figure 165. 10.4 metres rear street wall height



Precinct 2



Figure 171. Sunlight hours analysis against Clause 54/55 requirements - 8 metres rear boundary wall height (D=7.7 metres)

Floor heights:
Ground floor - 4M (Commercial)
Upper floors - 4M (Commercial)

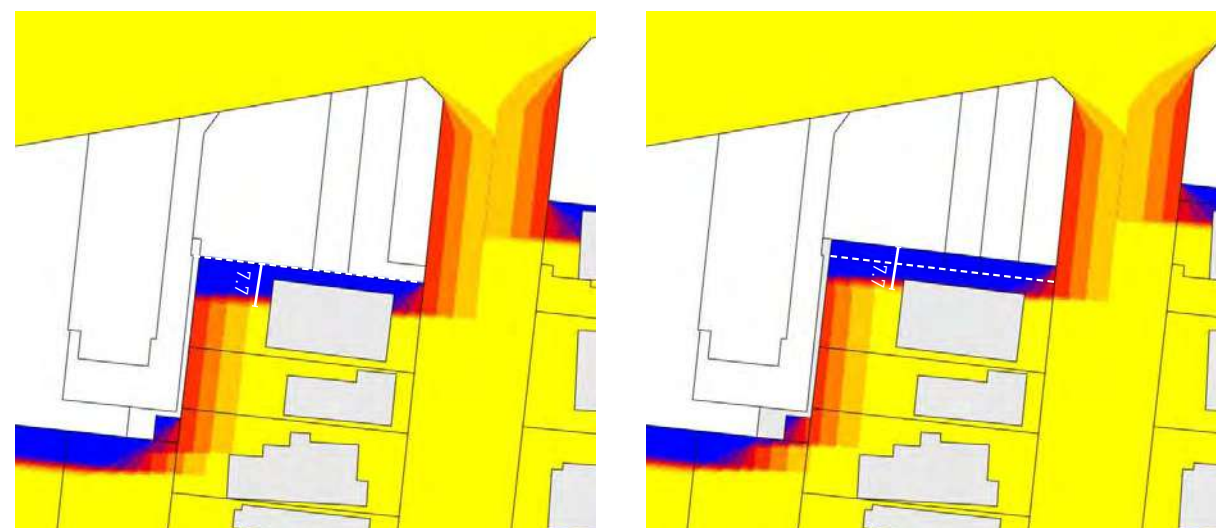


Figure 172. Comparison setback option on rear to side boundary - no setback (left) & 3 metres setback (right)



Figure 169. 4 metres rear street wall height (D=3.9 metres)



Figure 170. 12 metres rear street wall height (D=11.6 metres)

Precinct 3A

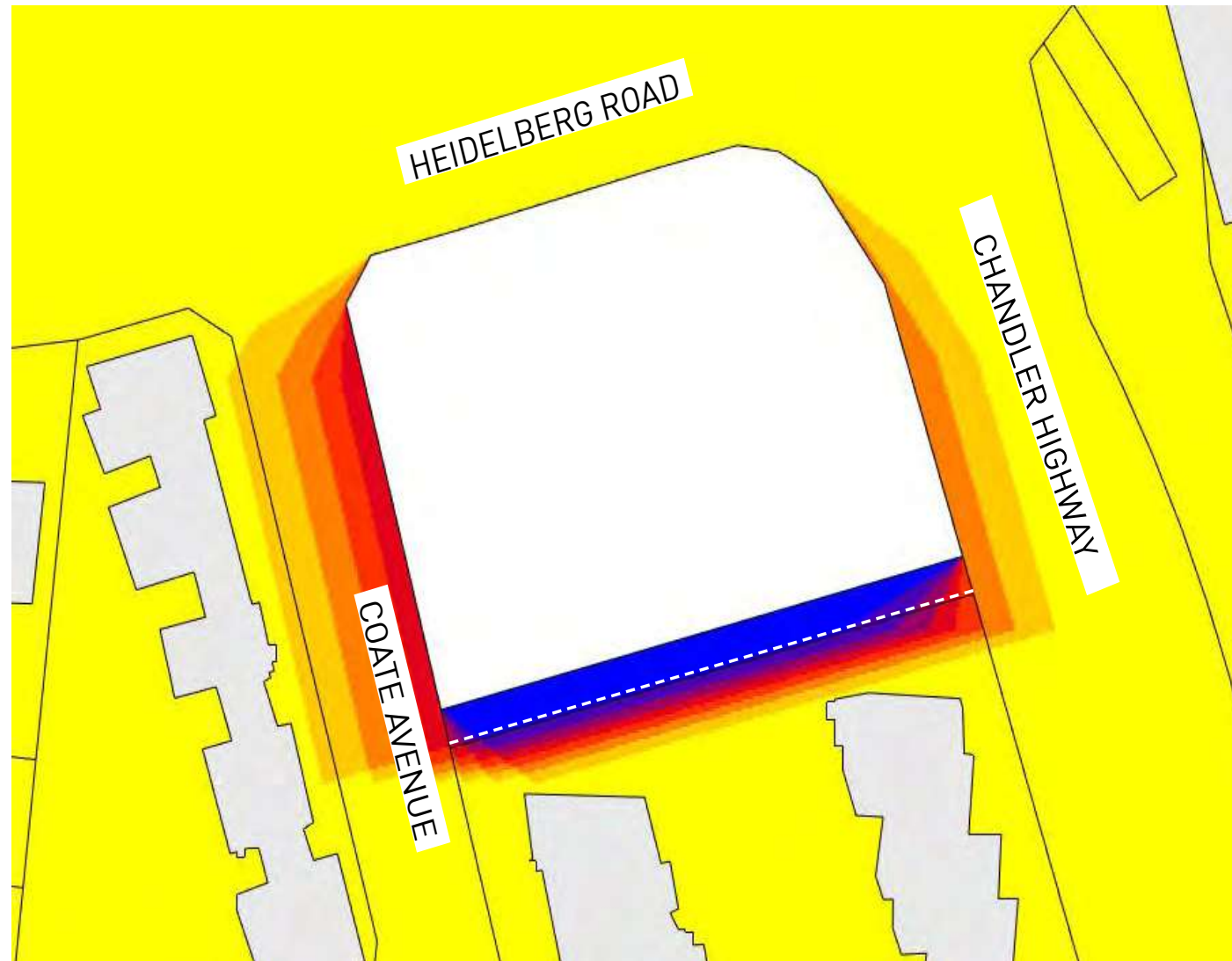


Figure 176. Sunlight hours analysis - 10.4 metres rear street wall height with 4.5m setback

Floor height:
 Ground floor - 4M (Commercial)
 Upper floors - 3.2M (Residential)

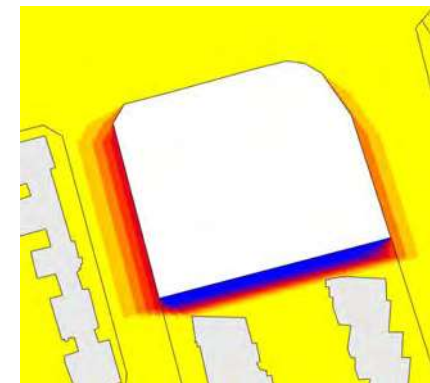


Figure 173. 7.2 metres rear street wall height

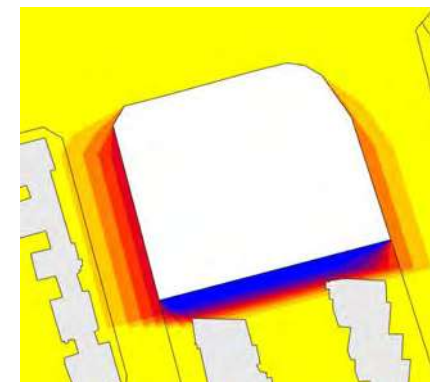


Figure 174. 10.4 metres rear street wall height

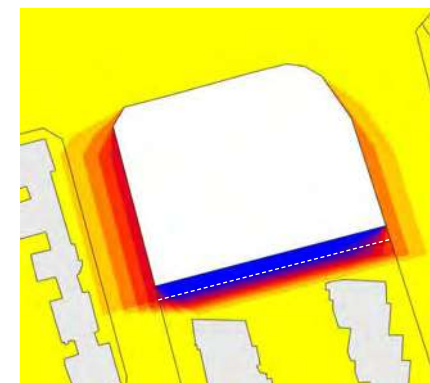
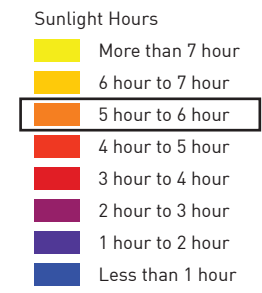


Figure 175. 10.4 metres rear street wall height with 3m setback



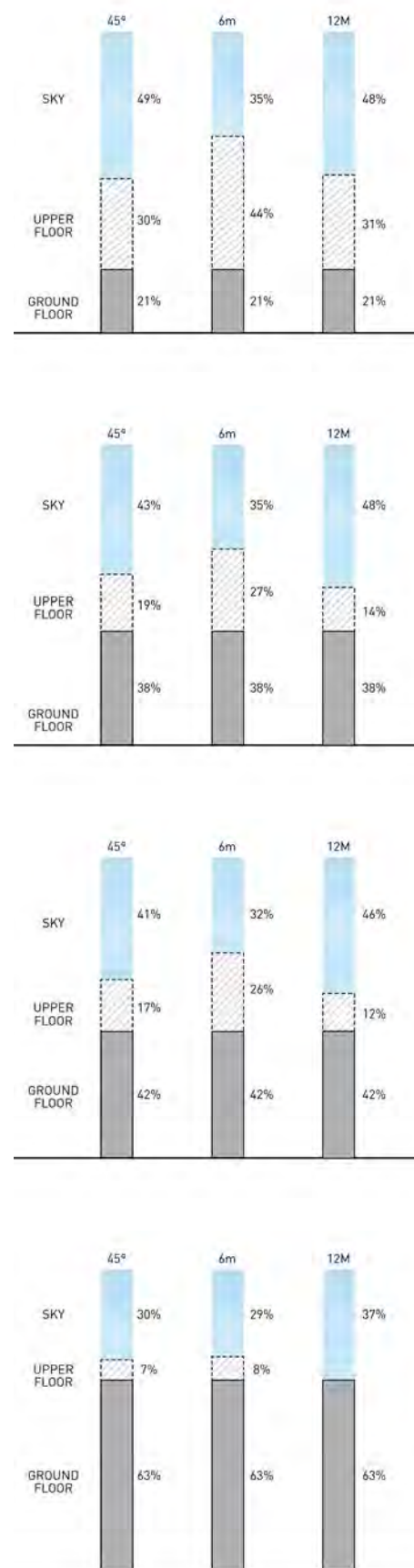
Appendix B - Visual Impact Assessment

Visual impact – 5 storeys (view from 15m)

Table 4. Visual impact assessment of each scenario (5 storeys - view from 15m)

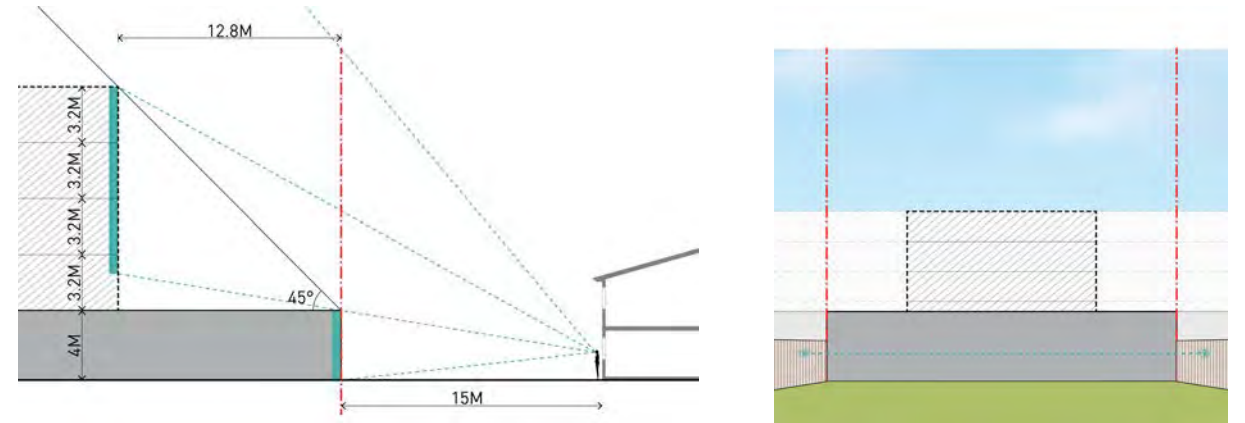
	45 degree angle	6 metre setback	12 metre setback
Urban Design Principle	4m boundary wall height condition		
Boundary wall height is not too visually dominant (50% or less)	Achieved	Achieved	Achieved
Upper levels are recessive (30% or less)	Achieved	Not achieved	Not achieved
Reasonable sky views (30% or more)	Achieved	Achieved	Achieved
Urban Design Principle	7.2m boundary wall height condition		
Boundary wall height is not too visually dominant (50% or less)	Achieved	Achieved	Achieved
Upper levels are recessive (30% or less)	Achieved	Achieved	Achieved
Reasonable sky views (30% or more)	Achieved	Achieved	Achieved
Urban Design Principle	8m boundary wall height condition		
Boundary wall height is not too visually dominant (50% or less)	Achieved	Achieved	Achieved
Upper levels are recessive (30% or less)	Achieved	Achieved	Achieved
Reasonable sky views (30% or more)	Achieved	Achieved	Achieved
Urban Design Principle	12m boundary wall height condition		
Boundary wall height is not too visually dominant (50% or less)	Not achieved	Not achieved	Not achieved
Upper levels are recessive (30% or less)	Achieved	Achieved	Achieved
Reasonable sky views (30% or more)	Achieved	Not achieved	Achieved

Comparison

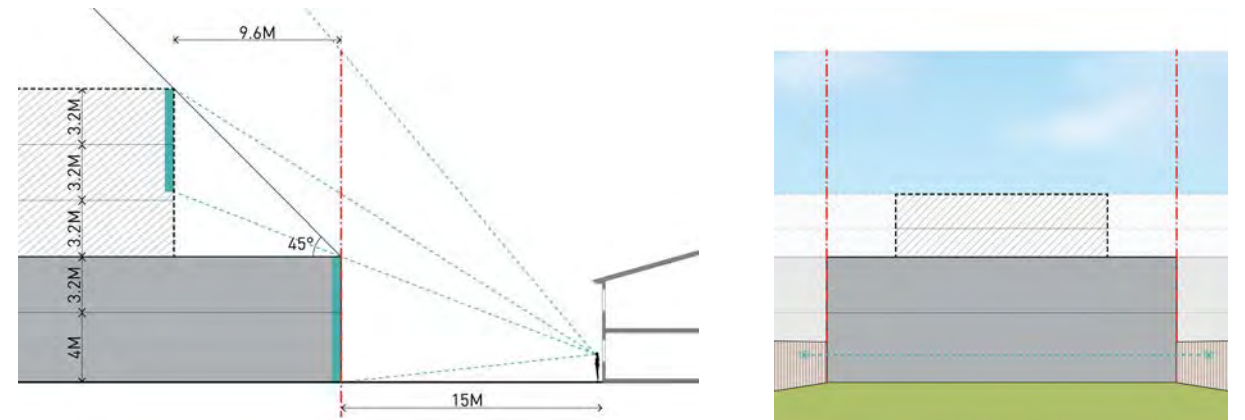


Upper level rear setback: 45 degree angle

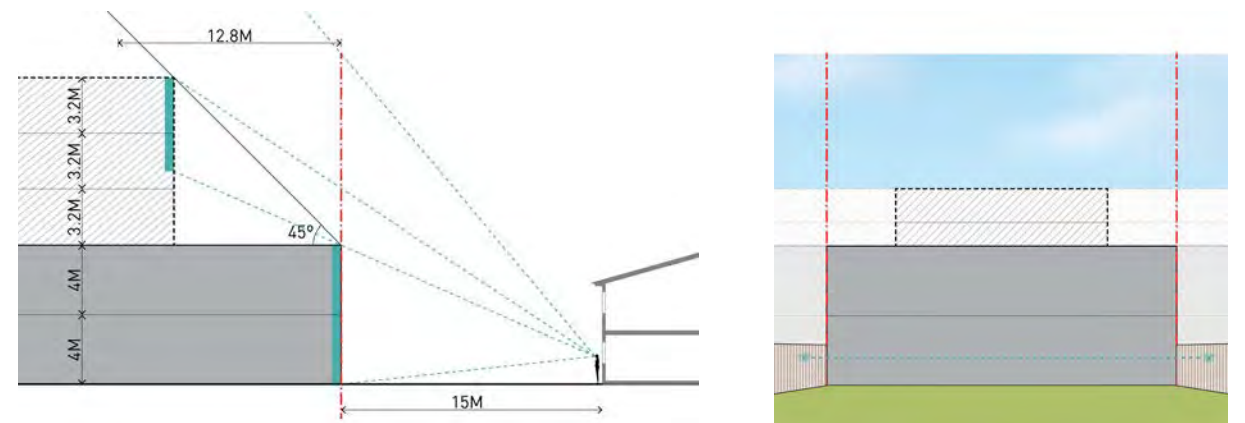
4m



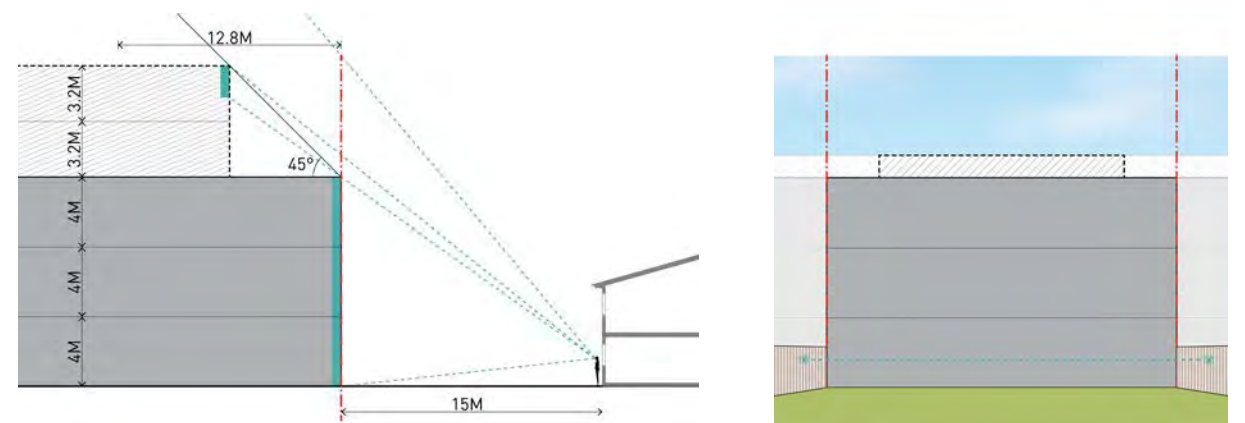
7.2m



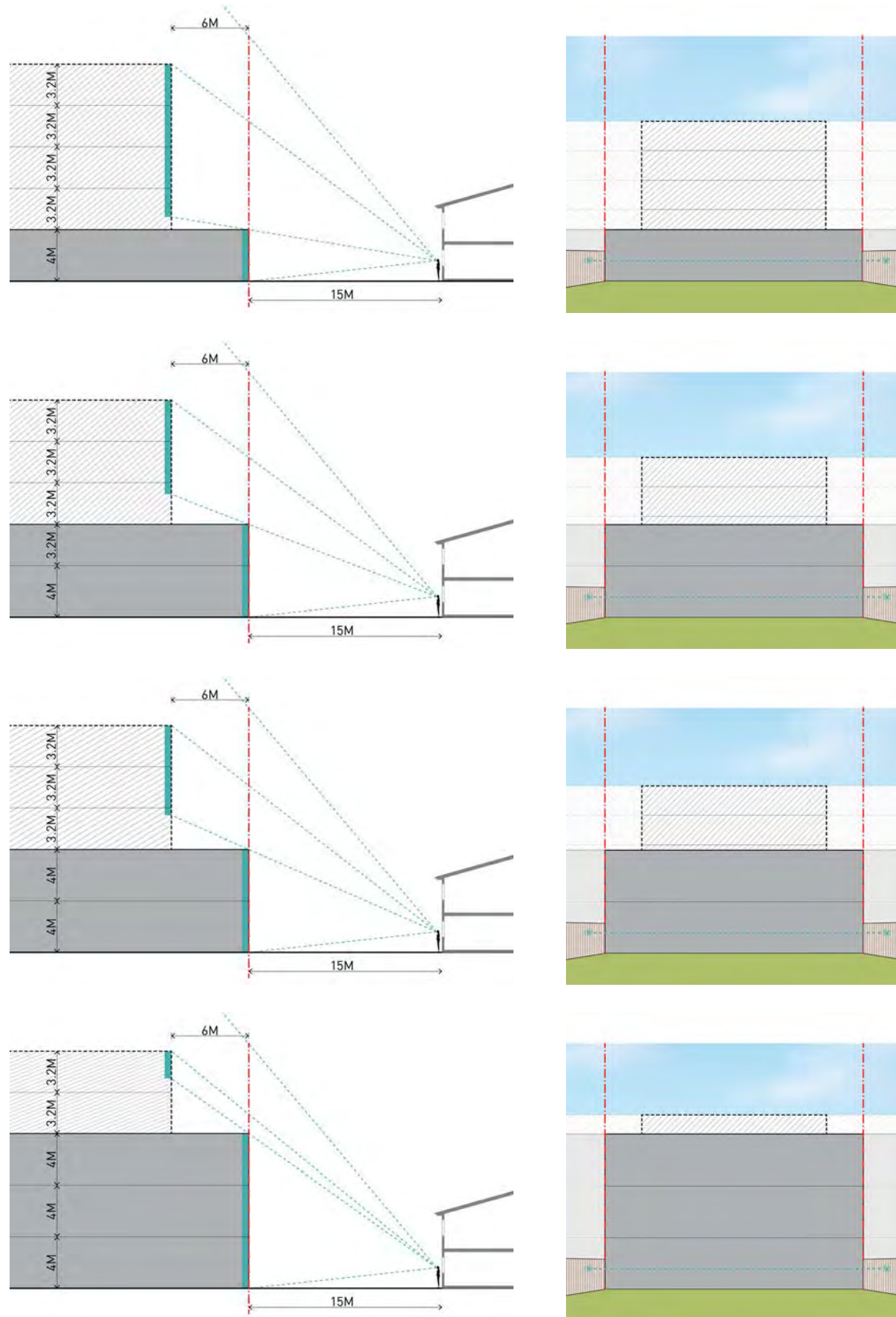
8m



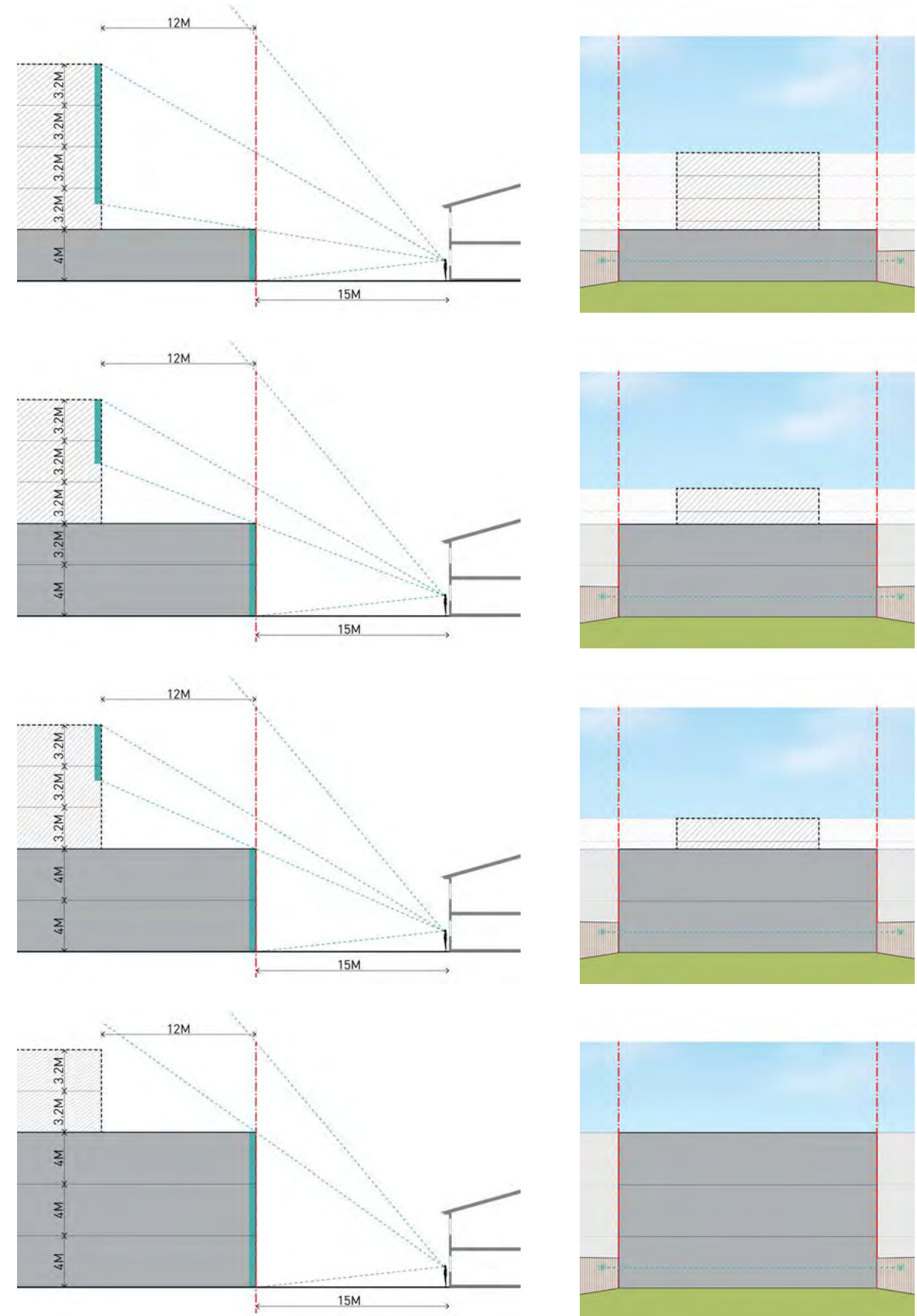
12m



Upper level rear setback: 6 metres



Upper level rear setback: 12 metres

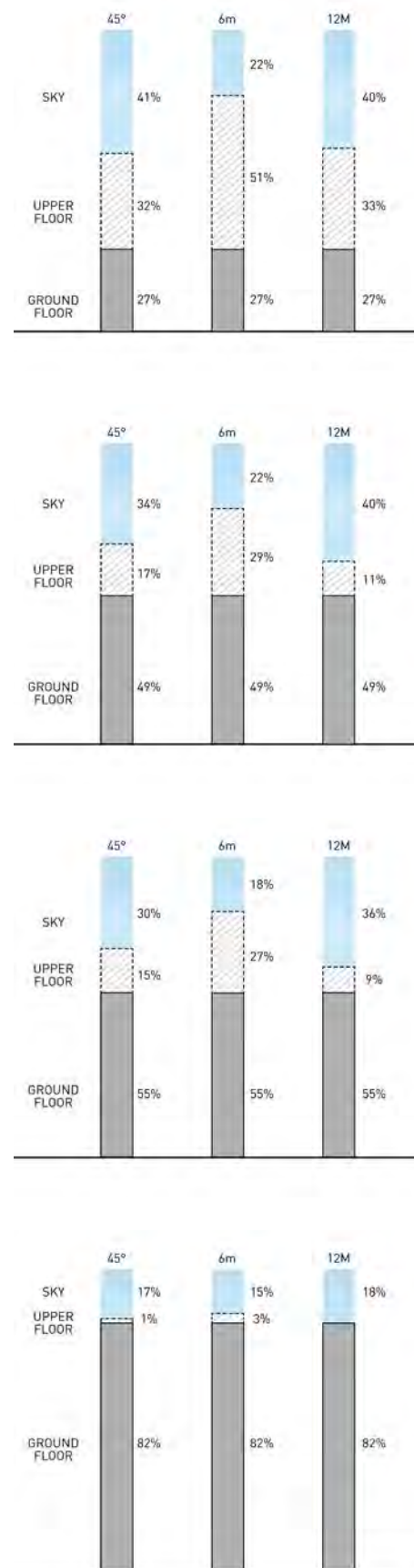


Visual impact – 5 storeys (view from 11m)

Table 5. Visual impact assessment of each scenario (5 storeys - view from 11m)

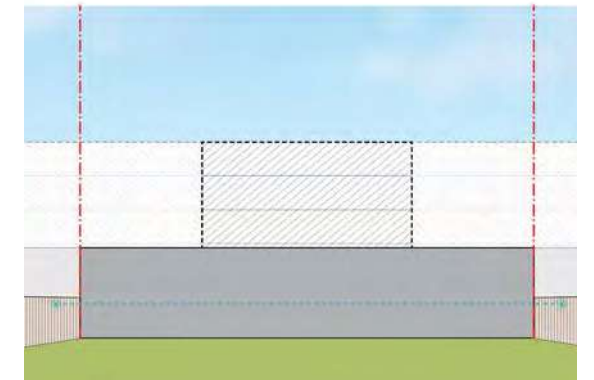
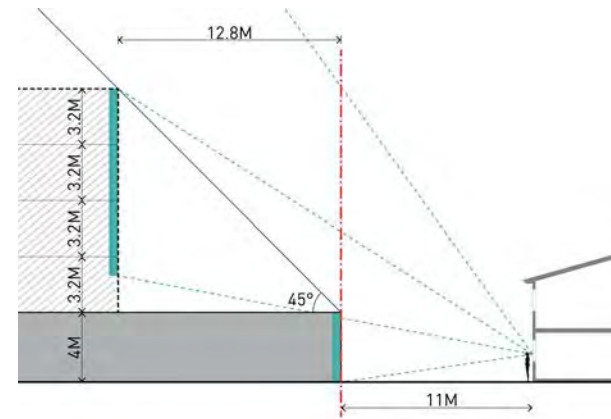
	45 degree angle	6 metre setback	12 metre setback
Urban Design Principle	4m boundary wall height condition		
Boundary wall height is not too visually dominant (50% or less)	Achieved	Achieved	Achieved
Upper levels are recessive (30% or less)	Not achieved	Not achieved	Not achieved
Reasonable sky views (30% or more)	Achieved	Not achieved	Achieved
Urban Design Principle	7.2m boundary wall height condition		
Boundary wall height is not too visually dominant (50% or less)	Achieved	Achieved	Achieved
Upper levels are recessive (30% or less)	Not achieved	Not achieved	Not achieved
Reasonable sky views (30% or more)	Achieved	Not achieved	Achieved
Urban Design Principle	8m boundary wall height condition		
Boundary wall height is not too visually dominant (50% or less)	Not achieved	Not achieved	Not achieved
Upper levels are recessive (30% or less)	Achieved	Achieved	Achieved
Reasonable sky views (30% or more)	Achieved	Not achieved	Achieved
Urban Design Principle	12m boundary wall height condition		
Boundary wall height is not too visually dominant (50% or less)	Not achieved	Not achieved	Not achieved
Upper levels are recessive (30% or less)	Achieved	Achieved	Achieved
Reasonable sky views (30% or more)	Not achieved	Not achieved	Not achieved

Comparison

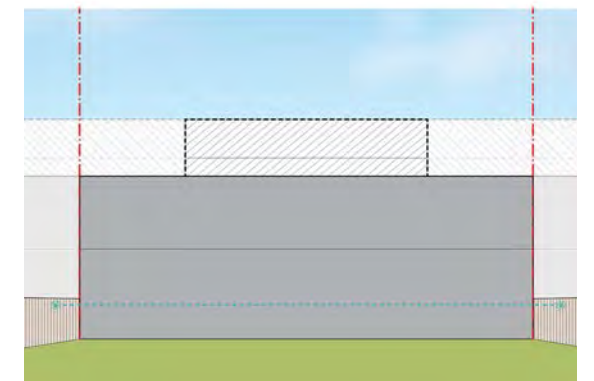
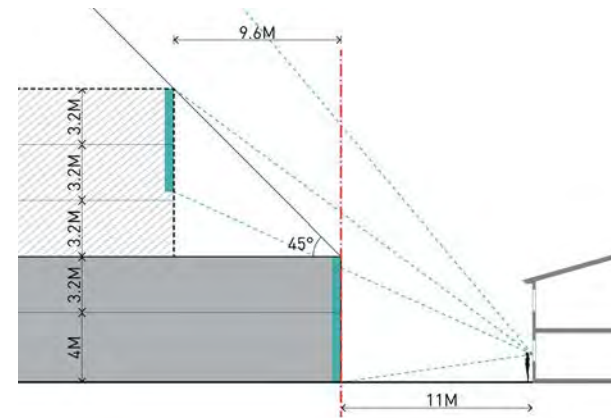


Upper level rear setback: 45 degree angle

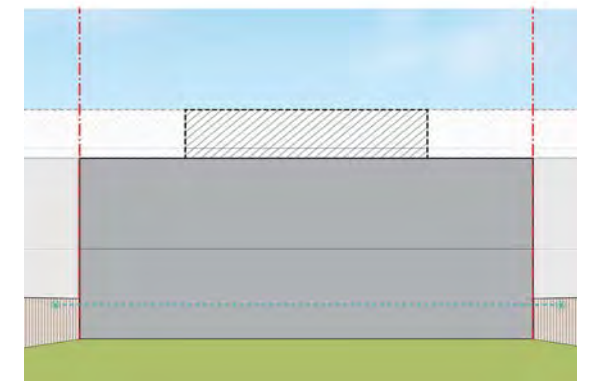
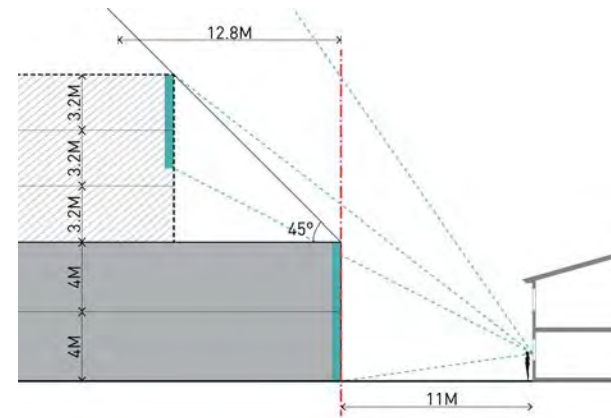
4m



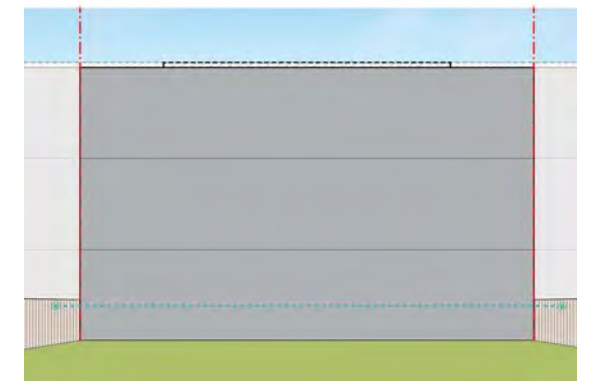
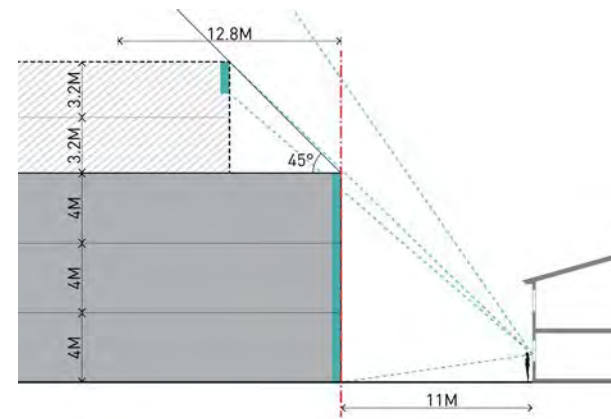
7.2m



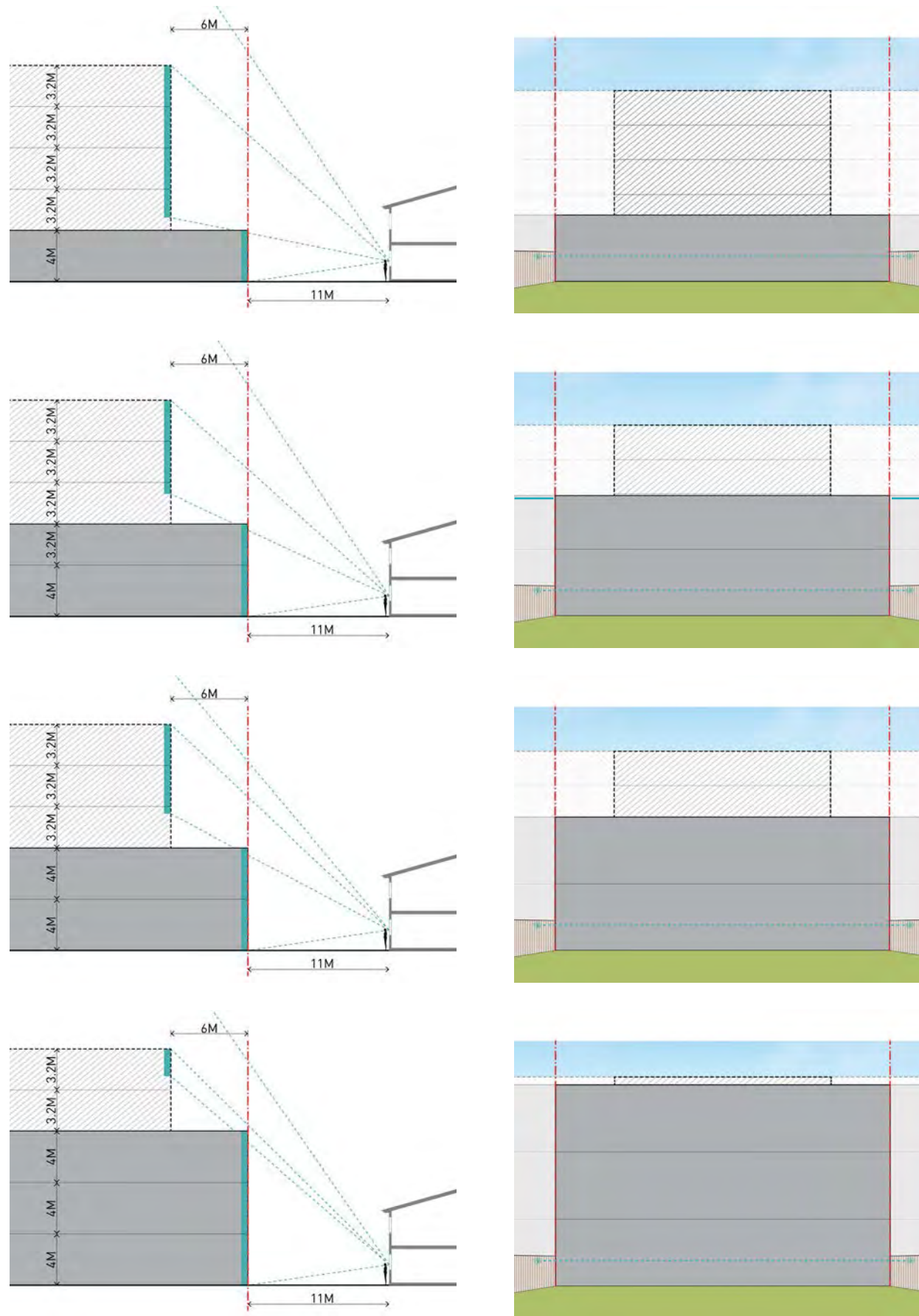
8m



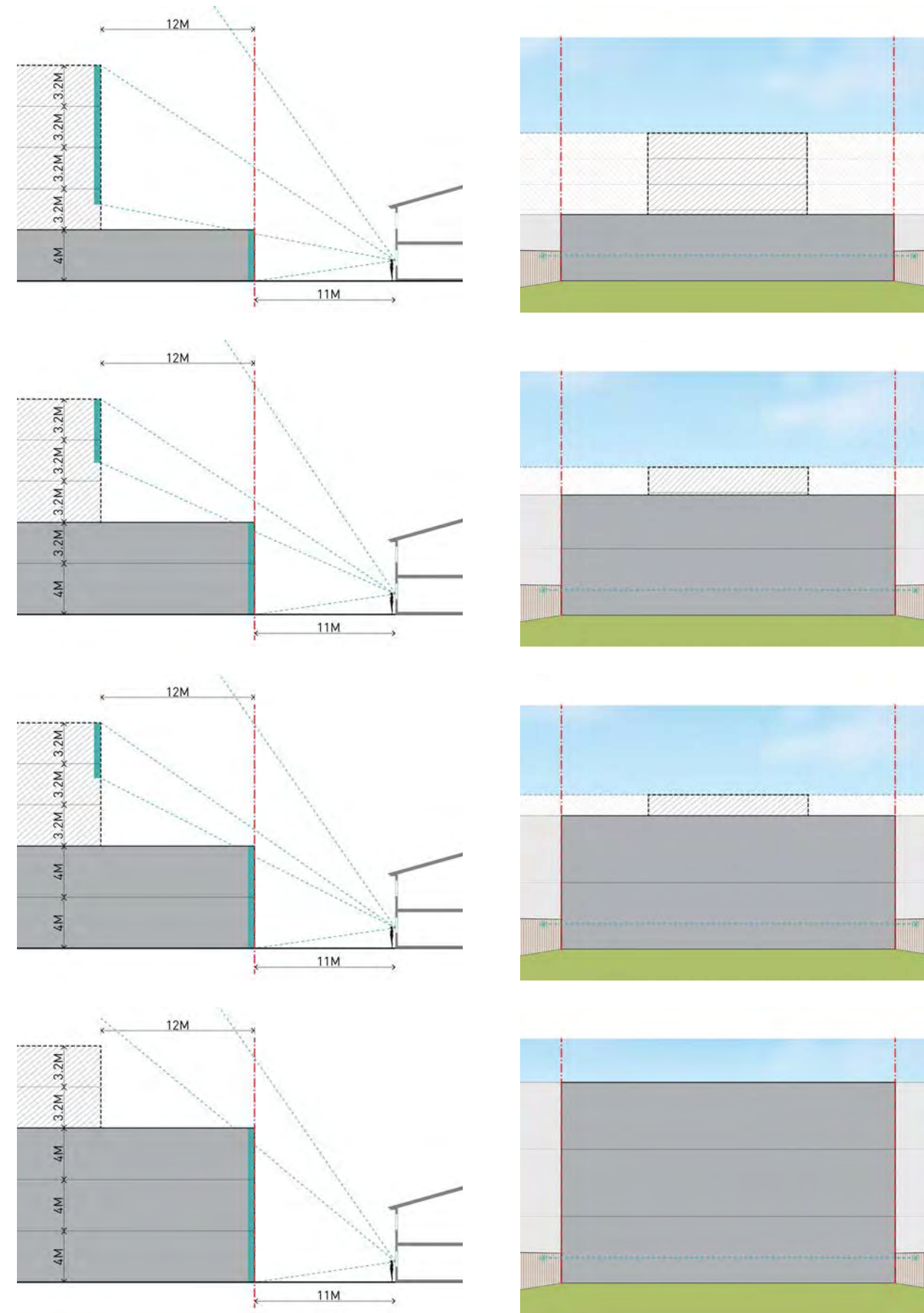
12m



Upper level rear setback: 6 metres



Upper level rear setback: 12 metres

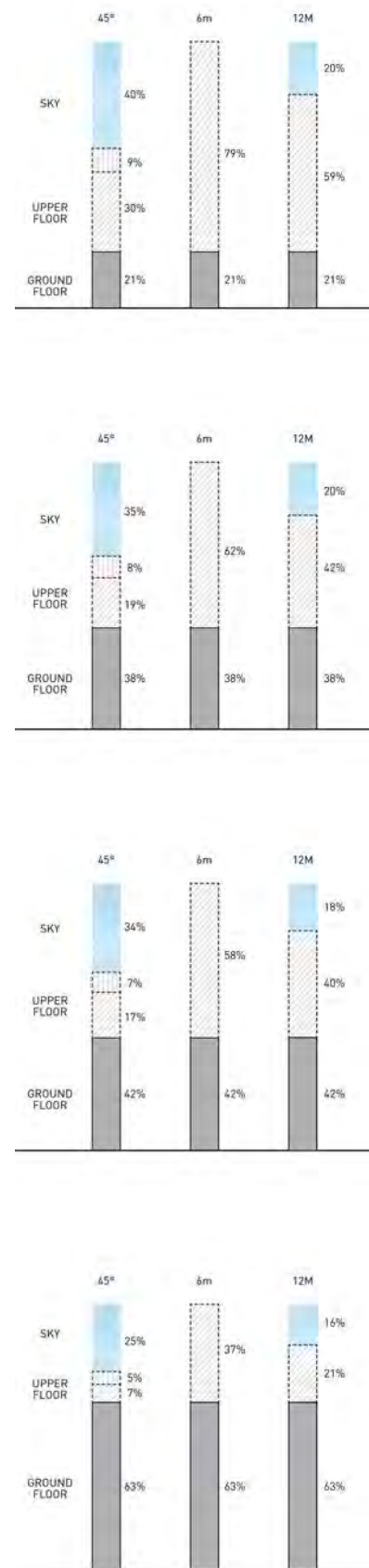


Visual impact – 8 storeys (view from 15m)

Table 6. Visual impact assessment of each scenario (8 storeys - view from 15m)

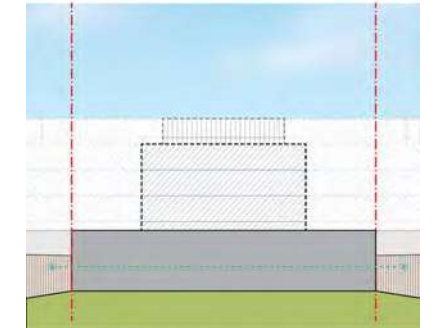
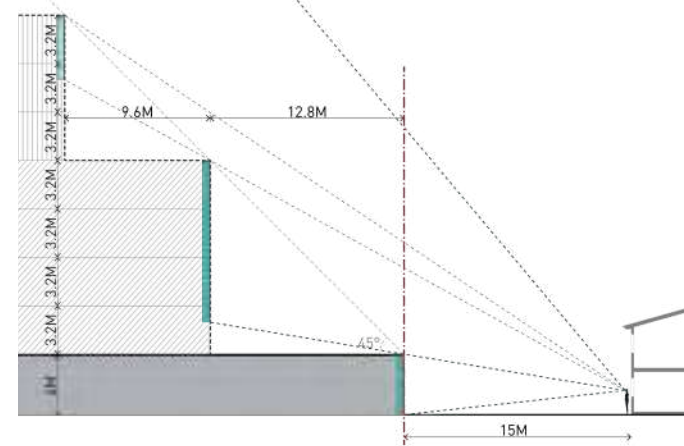
	45 degree angle	6 metre setback	12 metre setback
Urban Design Principle	4m boundary wall height condition		
Boundary wall height is not too visually dominant (50% or less)	Achieved	Achieved	Achieved
Upper levels are recessive (30% or less)	Not achieved	Not achieved	Not achieved
Reasonable sky views (30% or more)	Achieved	Not achieved	Not achieved
Urban Design Principle	7.2m boundary wall height condition		
Boundary wall height is not too visually dominant (50% or less)	Achieved	Achieved	Achieved
Upper levels are recessive (30% or less)	Achieved	Not achieved	Achieved
Reasonable sky views (30% or more)	Achieved	Not achieved	Not achieved
Urban Design Principle	8m boundary wall height condition		
Boundary wall height is not too visually dominant (50% or less)	Achieved	Achieved	Achieved
Upper levels are recessive (30% or less)	Achieved	Not achieved	Achieved
Reasonable sky views (30% or more)	Achieved	Not achieved	Not achieved
Urban Design Principle	12m boundary wall height condition		
Boundary wall height is not too visually dominant (50% or less)	Not achieved	Not achieved	Not achieved
Upper levels are recessive (30% or less)	Achieved	Not achieved	Achieved
Reasonable sky views (30% or more)	Not achieved	Not achieved	Not achieved

Comparison

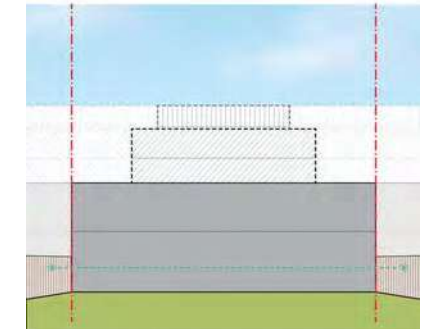
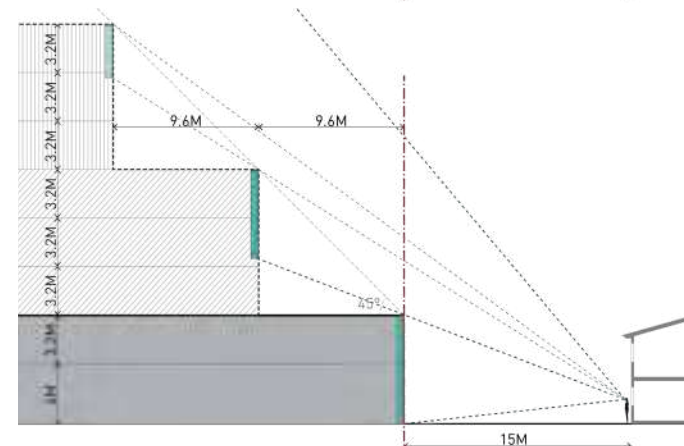


Upper level rear setback: 45 degree angle

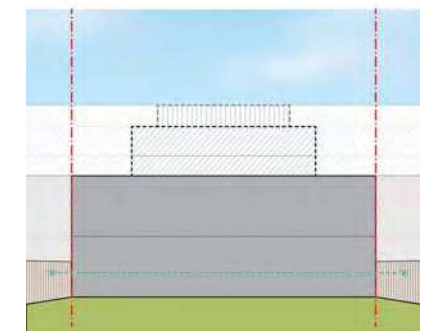
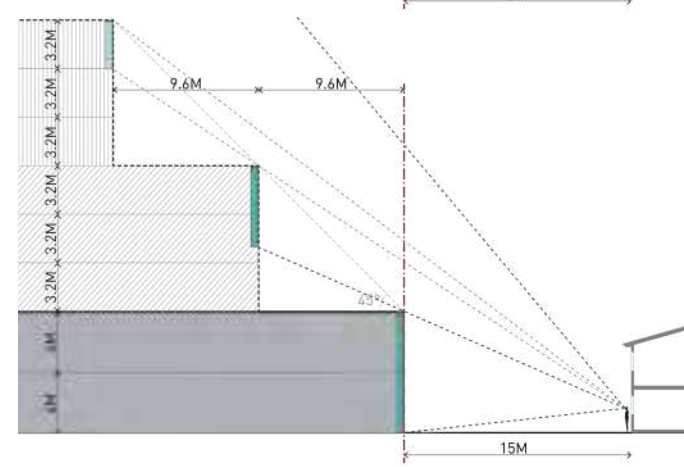
4m



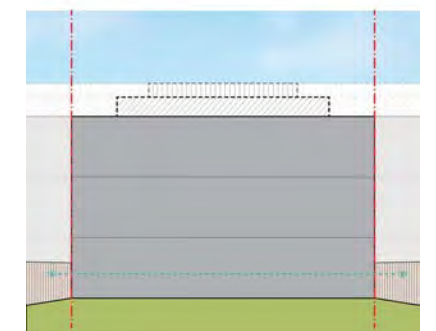
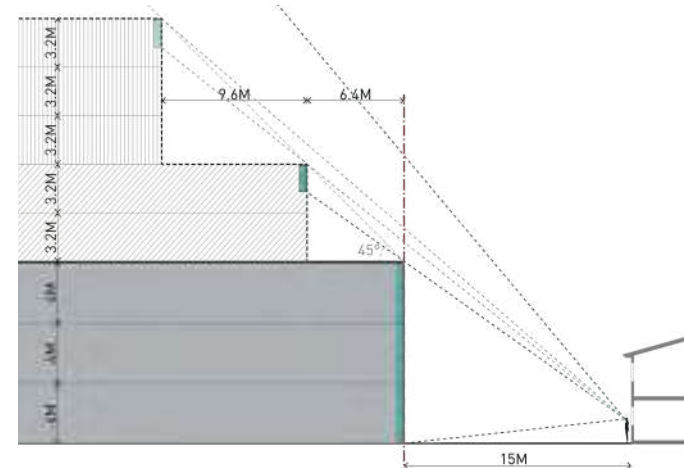
7.2m



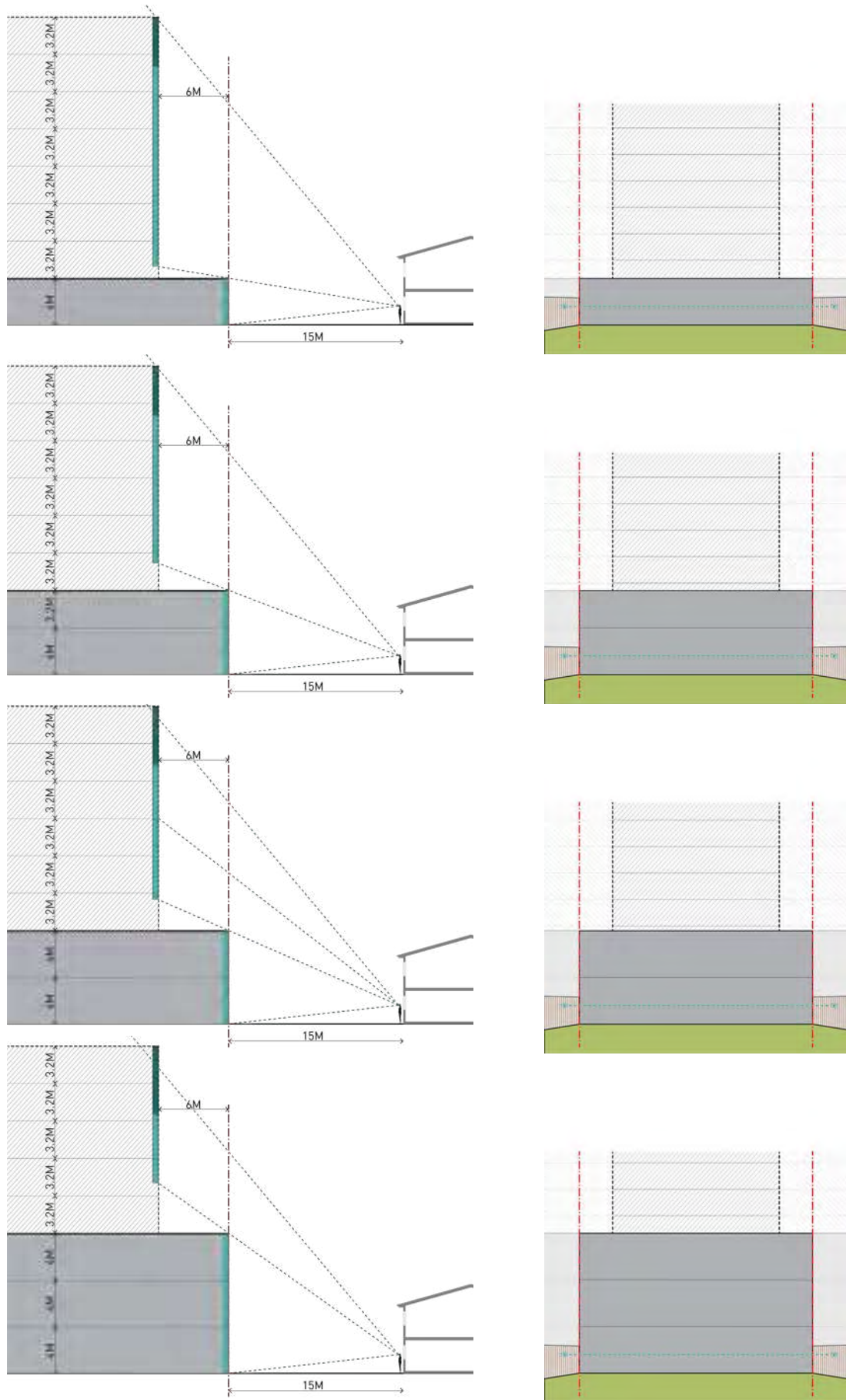
8m



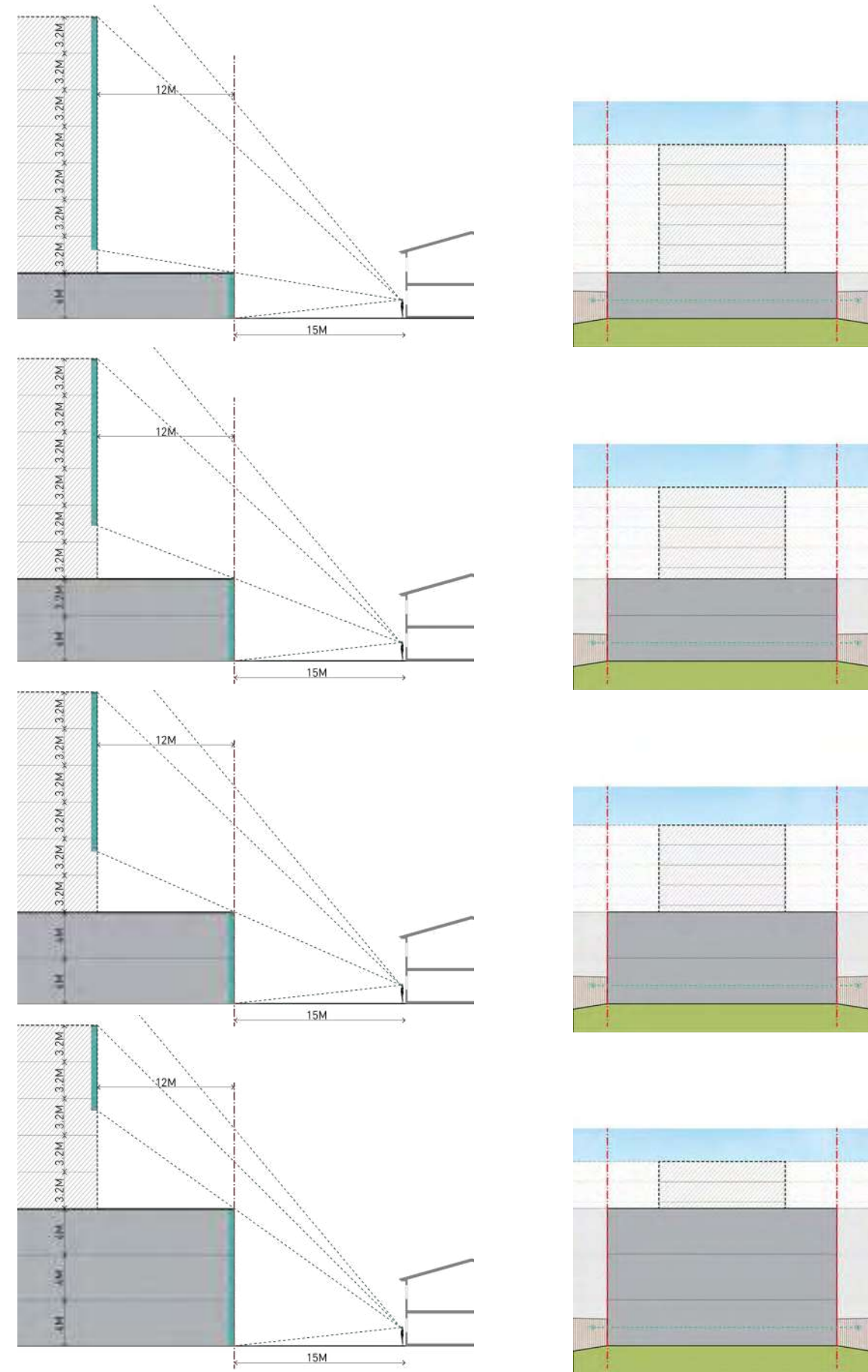
12m



Upper level rear setback: 6 metres



Upper level rear setback: 12 metres

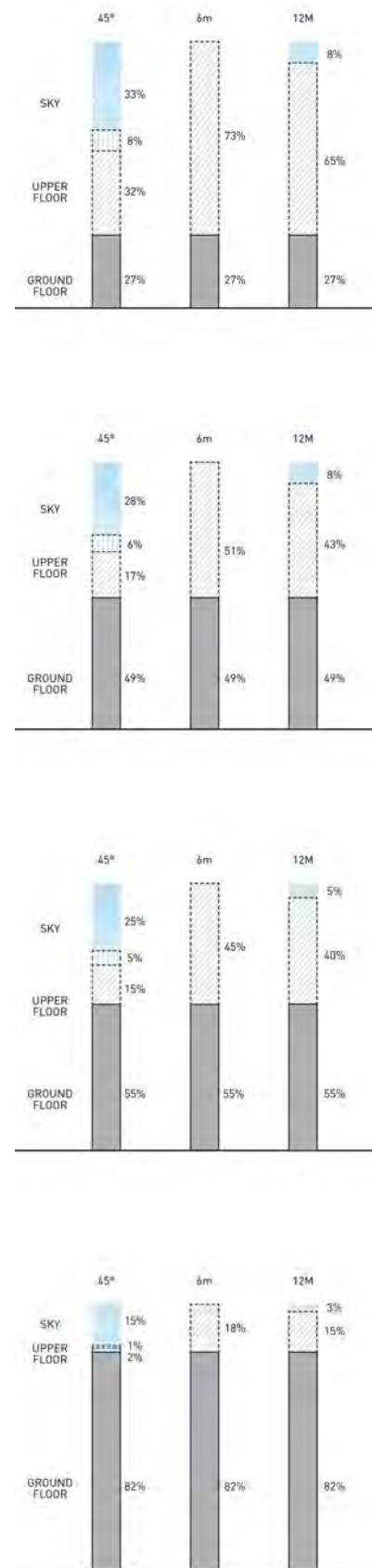


Visual impact – 8 storeys (view from 11m)

Table 7. Visual impact assessment of each scenario (8 storeys - view from 11m)

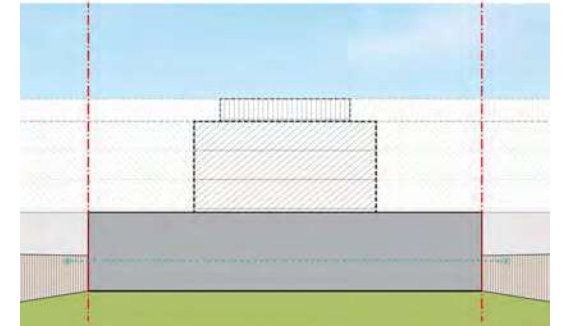
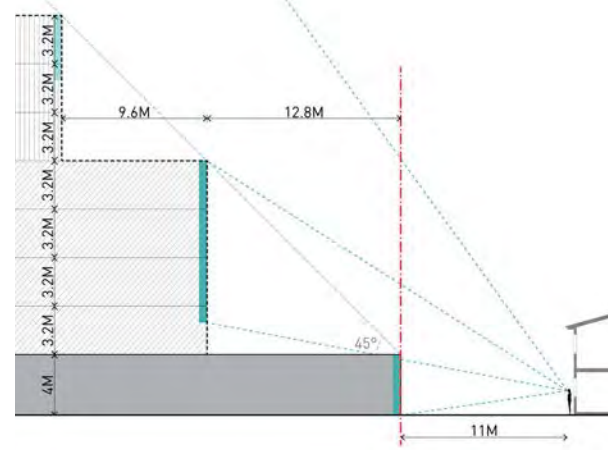
	45 degree angle	6 metre setback	12 metre setback
Urban Design Principle	4m boundary wall height condition		
Boundary wall height is not too visually dominant (50% or less)	Achieved	Achieved	Achieved
Upper levels are recessive (30% or less)	Not achieved	Not achieved	Not achieved
Reasonable sky views (30% or more)	Achieved	Not achieved	Not achieved
Urban Design Principle	7.2m boundary wall height condition		
Boundary wall height is not too visually dominant (50% or less)	Achieved	Achieved	Achieved
Upper levels are recessive (30% or less)	Achieved	Not achieved	Not achieved
Reasonable sky views (30% or more)	Not achieved	Not achieved	Not achieved
Urban Design Principle	8m boundary wall height condition		
Boundary wall height is not too visually dominant (50% or less)	Not achieved	Not achieved	Not achieved
Upper levels are recessive (30% or less)	Achieved	Not achieved	Not achieved
Reasonable sky views (30% or more)	Not achieved	Not achieved	Not achieved
Urban Design Principle	12m boundary wall height condition		
Boundary wall height is not too visually dominant (50% or less)	Not achieved	Not achieved	Not achieved
Upper levels are recessive (30% or less)	Achieved	Achieved	Achieved
Reasonable sky views (30% or more)	Not achieved	Not achieved	Not achieved

Comparison

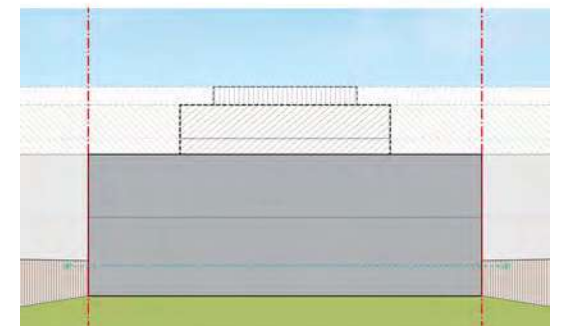
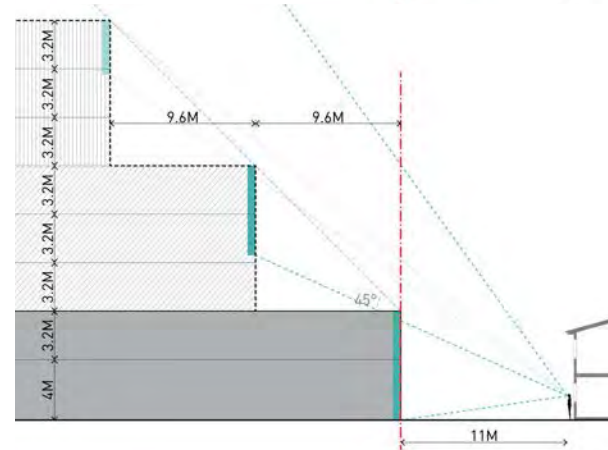


Upper level rear setback: 45 degree angle

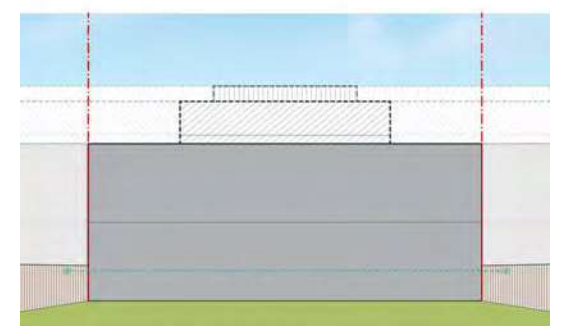
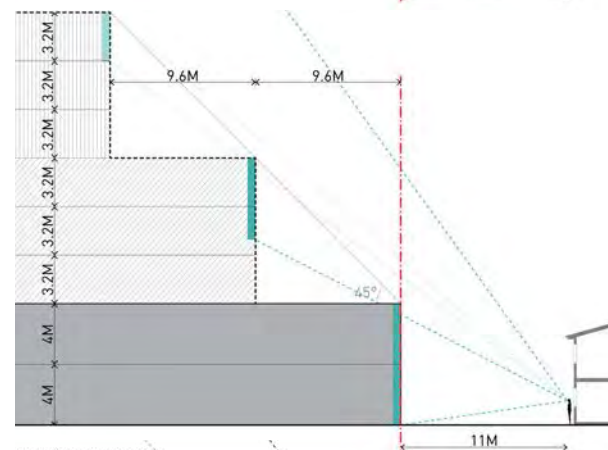
4m



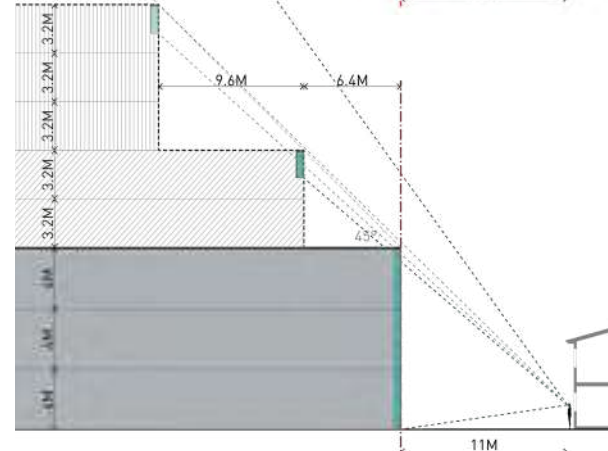
7.2m



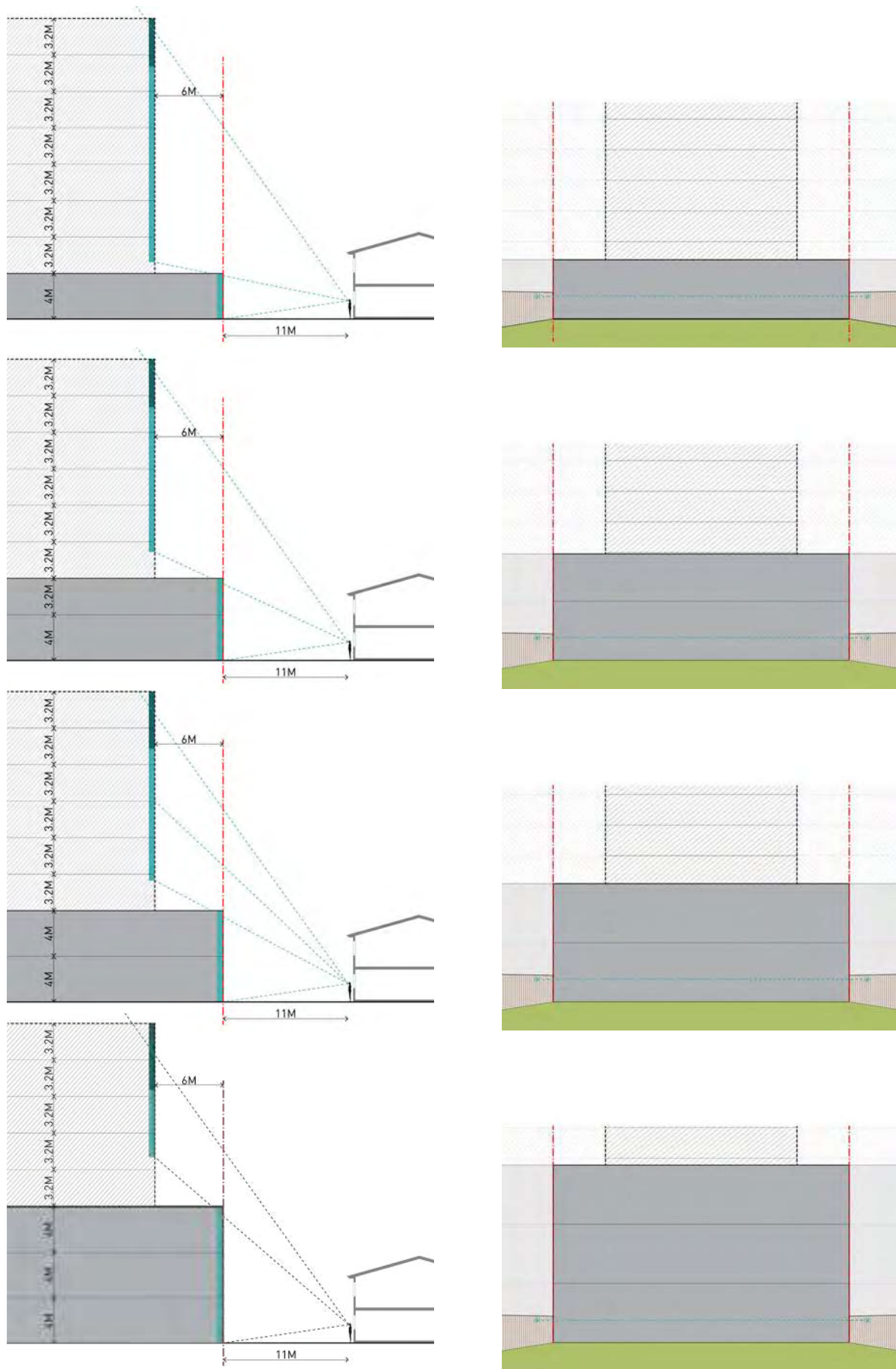
8m



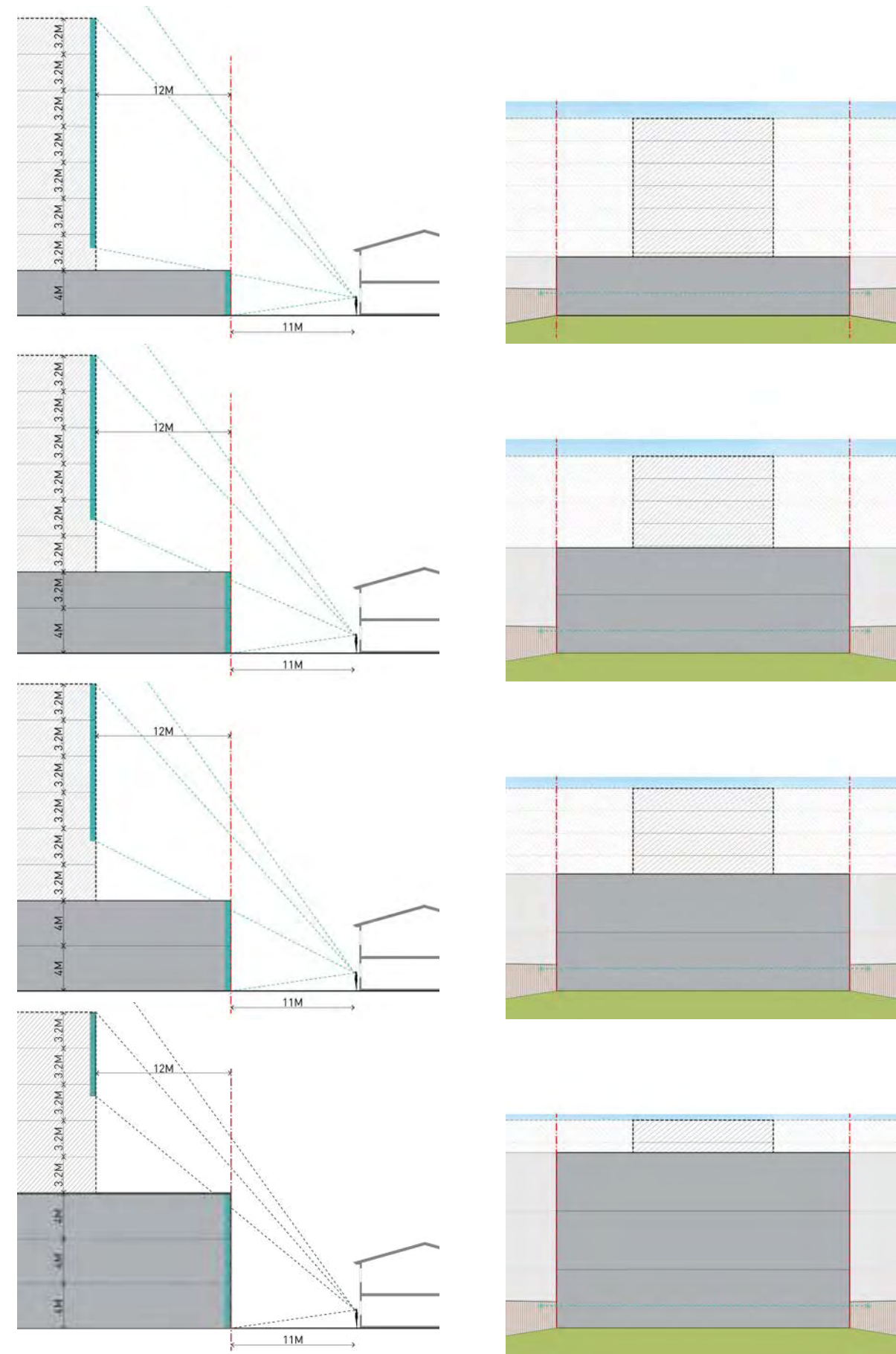
12m



Upper level rear setback: 6 metres

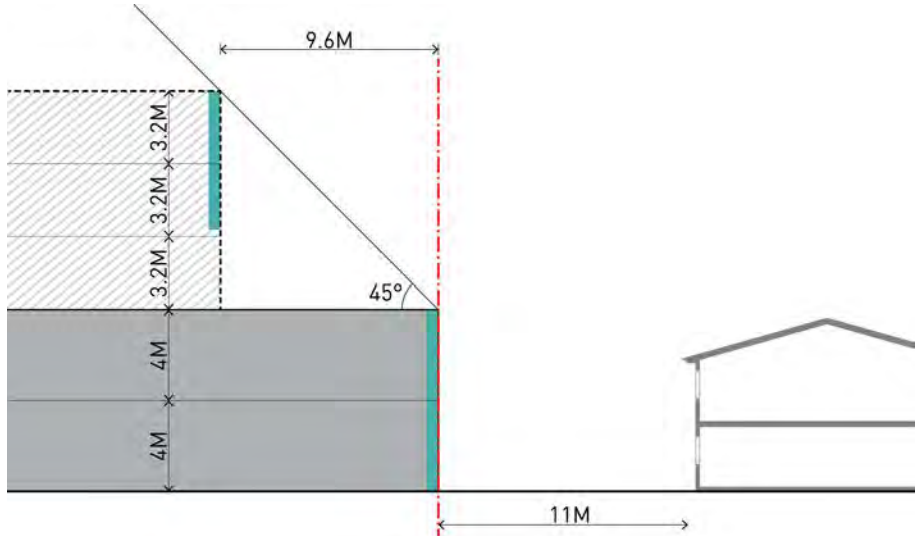


Upper level rear setback: 12 metres

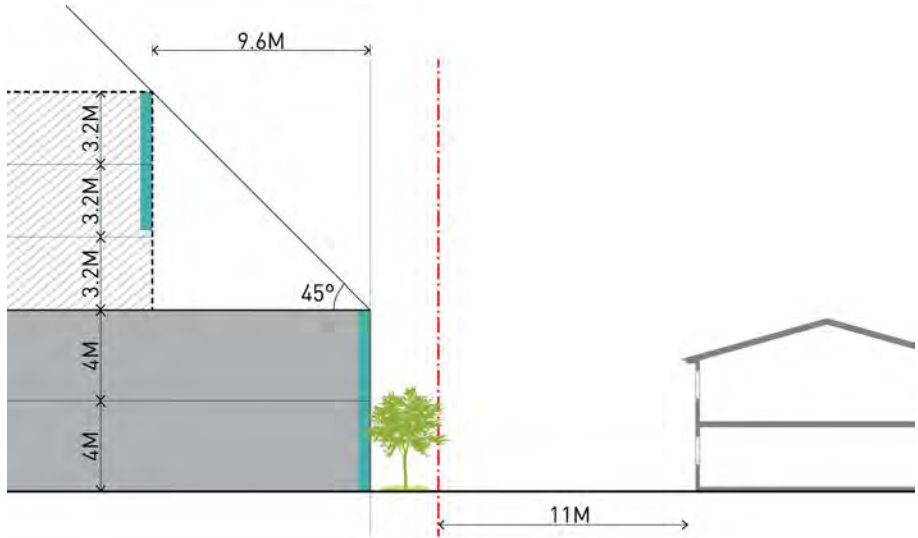


Visual impact – Introduction of a 3 metre setback

No
Setback



3m
Setback



View from 11 metres

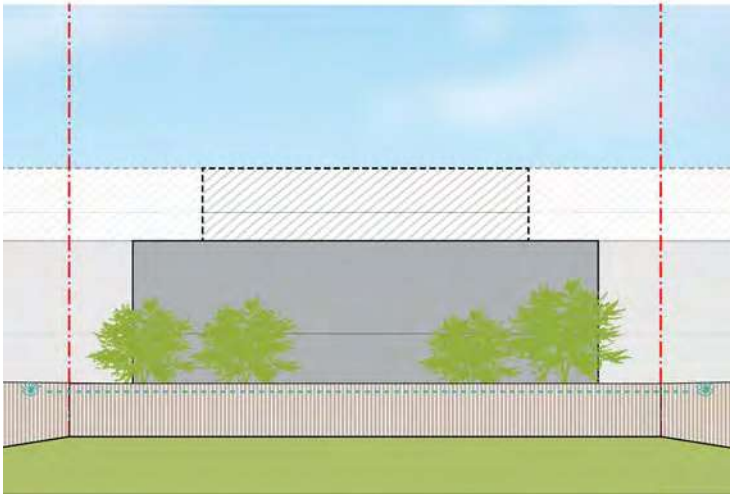
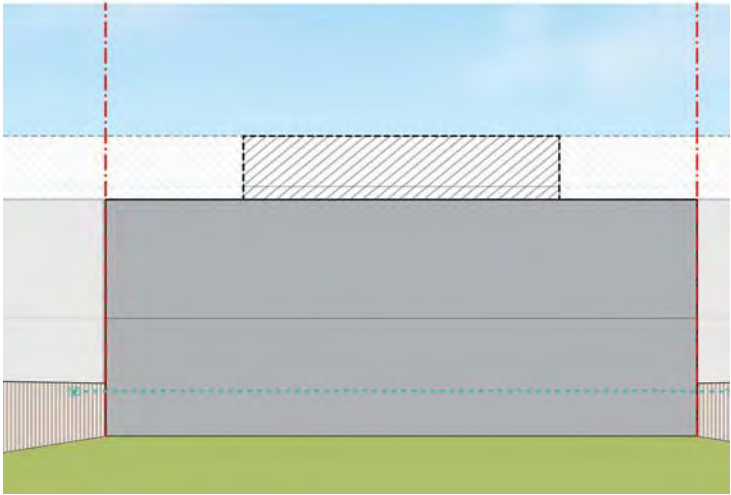


Figure 177. Impact of no setback at ground floor and introduction of a 3m setback

Appendix C - Existing examples of front ground floor setbacks

The existing range of setback conditions provides guidance on the appropriate landscape setback that should be incorporated into private development to improve the quality of the streetscape.

On balance, a 3 metre setback provides the opportunity for sufficient volume of landscape (in depth and height) to make a meaningful contribution to the street while also provide a 'hard' urban edge to provide overall street definition.



0 METRE

Appropriate to align with heritage buildings and existing shopfronts in Precinct 3.



2 METRES

This example demonstrates that a 2 metre setback can provide meaningful amount of landscape, however the opportunity to plant medium sized trees is compromised by the insufficient depth with the street canopy intruding into the footpath space to a great degree.

Increasing this to 3 metres will improve the volume of space available for a tree planting.



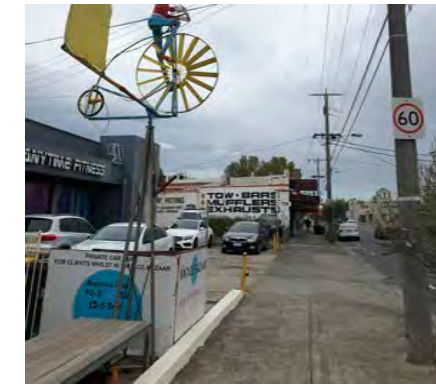
4 METRES

Increasing the setback to approximately 4 metres starts to diminish street definition as the building is located too far from the footpath edge.



5 METRES

The loss of an defined edge to the street is further exacerbated by increasing the setback to 5 metres.



9/12 METRES

Larger setbacks create the opportunity for semi-public open space that can be utilised for more active uses such as outdoor eating, socialising or community events. These examples include at grade car parking which is not desirable in the street frontage.

All setbacks distances are approximate, rounded to the nearest metre.

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Traffic and Vehicle Access Assessment

Heidelberg Road, Fairfield / Alphington

Prepared for
City of Yarra

November 2019

G27460R-01B

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1. Introduction

Yarra City Council has completed a Built Form Framework study for the Heidelberg Road Corridor in Fairfield and Alphington. This Built Form Framework defines the preferred future built form character of the precincts and include principles, guidelines and requirements to guide future development and to manage the level of change. Importantly, this framework will inform the preparation of Design and Development Overlay (DDO) controls and policy for the area.

The study area is detailed in the following figure, as set out within the Urban Design Strategy – Summary Plan (prepared by Hodyl & Co), and comprises Heidelberg Road between Merri Creek and Darebin Creek, development/access to C1Z, C2Z and MUZ areas on the south side of Heidelberg Road (the Yarra CC side of the road) as well as impacts to the adjacent local road network. We note that the study area does not include the Former Alphington Paper Mill Site.



The current use of the land within the study area is currently predominantly commercial in nature. This will change towards a greater proportion of residential development within the commercially zoned land.

The likely increase in residential development throughout the area poses transport challenges for all modes along the Heidelberg Road Corridor and the immediate area. Specific issues which have arisen as part of the local area plan insofar as they relate to transport matters include:

- The increase in overall traffic movements is likely to exacerbate existing issues, including potentially increasing conflicts with other vulnerable road users, increase to bus travel times. The development of the Alphington Paper Mill site is likely to further exacerbate these issues.
- The limited bus operating times, lack of bus shelters and lack of priority movements along Heidelberg Road and at intersections are likely detractors from utilising bus services.
- Inconsistent, shared nature and non-existent provision of bicycle lanes along Heidelberg Road through the precinct discourages bicycle riders (particularly less confident riders).
- A lack of separation from fast moving traffic, lack of crossing opportunities and short traffic light cycles and associated long waiting times for pedestrians make walking less attractive, limit accessibility to services and reduce the potential synergies between businesses on either side of Heidelberg Road.
- Uncertainty with regard to the intention of the Public Acquisition Overlay to potentially widen Heidelberg Road.
- Challenges with regard to vehicle access to potential development sites which do not have frontages to local roads or laneways.

While the traffic impacts of growth along the Heidelberg Road Corridor is acknowledged as a consideration, there is strategic policy support to facilitate increased commercial and residential development in this area. In considering the planning of similar centres across Melbourne, Planning Panels have acknowledged that “future congestion should not stifle development” and the “challenge of managing the road network should not prevent the Amendment from progressing”.

It is important that this project recognises the network constraints, the strong strategic support for development in the precinct, and the approach of Planning Panels in the discussion and advice on the future traffic conditions and future performance of Heidelberg Road and the local road network. In particular, this project must help to ensure that future consideration of traffic issues is focused on how best to manage the impacts of future development through improved access arrangements and measures to promote sustainable and active modes of travel through new development.

Traffic Group has been engaged by Yarra City Council to undertake a high level assessment of the future traffic conditions and performance of Heidelberg Road and the local street network taking into account the planned future development, prepare access and movement plans and provide input into the content of the future Design and Development Overlay to facilitate appropriate access and movement throughout the Heidelberg Road Corridor. The objective of the access and movement plans and the DDO is to facilitate ‘best practice’ access controls to properties abutting Heidelberg Road.

2. Scope & Methodology

The purpose of this study is to provide:

- a high level assessment of the future traffic conditions and performance of Heidelberg Road and local street network, with the planned future development,
- access and movement plan for the study area showing the location and form of new, altered and retained access arrangements and laneways required to provide appropriate access to future developments,
- advice on the content of the future Design and Development Overlay to facilitate appropriate access and movement through new development, and
- undertake a review of the existing public transport, bicycle and pedestrian considerations and infrastructure within the study area.

2.1. Methodology

The adopted methodology for undertaking this study was as follows:

- Undertake a case study of Victoria Street, which is a similar nearby inner urban area which has experienced significant growth in residential development along the corridor over the past 10 years, focusing on the “before” and “after” data for key transport measures, including traffic volumes, bicycle usage and public transport changes.
- Use the Victoria Street example as a basis for assessing the potential impacts additional development may have on the transport network, including the network performance of Heidelberg Road and the local road network as well as increased public transport use and the like.
- Undertake thorough site inspections of the entire study area to document and map:
 - existing access arrangements for each individual property,
 - existing traffic management treatments for all arterial and local roads and laneways/carriageway easements within the study area,
 - existing configuration of each road and laneway/carriageway easement within the study area (including carriageway width and road reservation width), and
 - foreseeable access constraints to each individual property should development occur.
- Liaise with representatives from Council to understand the relevant concerns and desirable access outcomes having regard to the potential impact on the safety and efficiency of the road network.
- Prepare “access” maps showing the preferred location and form of new, altered and retained access arrangements and laneways required to provide appropriate access to future developments, in consultation with Council.
- Review the traffic engineering and transport aspects of the future Design and Development Overlay, which sets out design objectives and outcomes, permit application requirements, and decision guidelines for assessing future planning permit applications, based on the desired access outcomes for future development.

2.2. Reference documents

The following reference documents were used in relation to this assessment

- Heidelberg Road Built Form Framework, Urban Context Analysis - Part 1, prepared by Hodyl & Co (dated September, 2019),
- Heidelberg Road Built Form Framework, Design Strategy & Recommendations - Part 2, prepared by Hodyl & Co (dated September, 2019),
- Heidelberg Road Corridor – Background Issues and Discussion Paper (dated 10th September, 2019),
- Heidelberg Road Corridor Draft Local Area Plan (dated 15th August, 2019), and
- Heidelberg Road – Transport Relevant Sections of proposed interim Design and Development Overlay.

3. Policy Context

3.1. Plan Melbourne 2017-2050

Plan Melbourne is the State Government plan that will guide the growth of Melbourne city for the next 35 years. It sets the strategy for supporting jobs, housing and transport, while building on Melbourne’s legacy of distinctiveness, liveability and sustainability.

The plan includes a number of key transport and urban planning objectives that are relevant to the Heidelberg Road Corridor. The most relevant objectives are listed in the table below.

Table 1: Key Objectives of Plan Melbourne in relation to the Heidelberg Road Corridor

Outcome	Directions	Policy
Outcome 2 Melbourne provides housing choice in locations close to jobs and services.	Manage the supply of new housing in the right locations to meet population growth and create a sustainable city.	Facilitate an increased percentage of new housing in established areas to create a city of 20-minute neighbourhoods close to existing services, jobs and public transport.
	Deliver more housing closer to jobs and public transport.	Facilitate well-designed, high-density residential developments that support a vibrant public realm in Melbourne’s central city. Direct new housing and mixed-use development to urban renewal precincts and sites across Melbourne. Support new housing in activity centres and other places that offer good access to jobs, services and public transport Provide support and guidance for greyfield areas to deliver more housing choice and diversity.

Outcome	Directions	Policy
Outcome 3 Melbourne has an integrated transport system that connects people to jobs and services and goods to market.	Transform Melbourne's transport system to support a productive city.	Provide high-quality public transport access to job-rich areas. Improve arterial road connections across Melbourne for all road users. Provide guidance and certainty for land use and transport development through the Principal Public Transport Network and the Principal Freight Network. Improve the efficiency of the motorway network. Support cycling for commuting.
	Improve local travel options to support 20-minute neighbourhoods.	Create pedestrian-friendly neighbourhoods. Create a network of cycling links for local trips. Improve local transport choices.
Outcome 5 Melbourne is a city of inclusive, vibrant and healthy neighbourhoods.	Create a city of 20-minute neighbourhoods.	Create mixed-use neighbourhoods at varying densities. Support a network of vibrant neighbourhood activity centres.
	Create neighbourhoods that support safe communities and healthy lifestyles.	Improve neighbourhoods to enable walking and cycling as a part of daily life.

3.2. State Planning Policy Framework (SPPF)

Clause 18 of the SPPF details state-wide objectives, strategies and policy guidelines relating to transport, including land use and transport planning, the transport system, walking, cycling, the principal public transport network, management of the road system, car parking ports, airports and freights.

The SPPF Transport objectives that are relevant to Yarra are set out in Table 2 below.

Table 2: SPPF Transport Objectives

Clause	Objectives
18.01-1 Land Use and Transport Planning	To create a safe and sustainable transport system by integrating land-use and transport.
18.01-2S Transport System	To coordinate development of all transport modes to provide a comprehensive transport system.
18.02-1S Sustainable Personal Transport	To promote the use of sustainable personal transport.
18.02-2S Cycling	To integrate planning for cycling with land use and development planning and encourage as alternative modes of travel.

Clause	Objectives
18.02-2R Principal Public Transport Network	To upgrade and develop the Principal Public Transport Network and local public transport services in Metropolitan Melbourne to connect activity centres, link activities in employment corridors and link Melbourne to the regional cities.
18.02-3S Management of the Road System	To manage the road system to achieve integration, choice and balance by developing an efficient and safe network and making the most of existing infrastructure.
18.02-4S Car Parking	To ensure an adequate supply of car parking that is appropriately design and located.

Detailed state-wide requirements in relation to car parking, loading and bicycle parking are set out at Clause 52.06, 65.01 and 52.34 of the Planning Scheme respectively.

3.3. Local Planning Policy Framework

While Clause 18 sets out the state-wide planning policy in relation to transport, each Council also sets its own local policies at Clauses 20, 21 and 22 of the Planning Scheme.

Clause 21 sets out the Municipal Strategic Statement (MSS).

Clause 21.03 sets out the vision for the municipality, as follows:

Land Use

- *The City will accommodate a diverse range of people, including families, the aged, the disabled, and those who are socially or economically disadvantaged.*
- *Yarra will have increased opportunities for employment.*
- *There will be an increased provision of public open space.*
- *The complex land use mix characteristic of the inner City will provide for a range of activities to meet the needs of the community.*
- *Yarra's exciting retail strip shopping centres will provide for the needs of local residents, and attract people from across Melbourne.*

Built Form

- *Yarra's historic fabric which demonstrates the development of metropolitan Melbourne will be internationally recognised.*
- *Yarra will have a distinctive identity as a low-rise urban form, with areas of higher development and highly valued landmarks.*
- *People will safely get together and socialise in public spaces across the City.*
- *All new development will demonstrate design excellence.*

Transport

- *Local streets will be dominated by walkers and cyclists.*

- *Most people will walk, cycle and use public transport for the journey to work.*

Environmental sustainability

- *Buildings throughout the City will adopt state-of the-art environmental design.*
- *Our natural environment will support additional species of flora and fauna.*

This vision is pursued by the objectives and strategies set out in the land use, built form, transport, environmental sustainability and neighbourhood sections under Clauses 21.04-21.08.

Clause 21.06 sets out Yarra’s detailed local Transport policy. The preamble states the following:

Yarra needs to reduce car dependence by promoting walking, cycling and public transport use as viable and preferable alternatives. This is also a key message of Melbourne 2030 and fundamental to the health and well-being of the community.

While the scope of the planning scheme in managing an integrated transport system is limited, Council will work towards improving the quality of walking and cycling infrastructure as a priority. Note that the term “walking” includes people who use wheelchairs.

Parking availability is important for many people, however in Yarra unrestricted car use and parking is neither practical nor achievable. Car parking will be managed to optimise its use and to encourage sustainable transport options.

The specific objectives and strategies for Transport management in Yarra are detailed in Table 3 below.

Table 3: LPPF Transport Objectives & Strategies

Clause	Objective	Strategies
21.06-1 Walking & Cycling	To provide safe and convenient pedestrian and bicycle environments.	30.1 Improve pedestrian and cycling links in association with new development where possible. 30.2 Minimise vehicle crossovers on street frontages. 30.3 Use rear laneway access to reduce vehicle crossovers.
21.06-2 Public Transport	To facilitate public transport usage.	31.1 Require new development that generates high numbers of trips to be easily accessible by public transport.
21.06-3 The Road System & Parking	To reduce the reliance on the private motor car.	32.1 Provide efficient shared parking facilities in activity centres. 32.2 Require all new large developments to prepare and implement integrated transport plans to reduce the use of private cars and to encourage walking, cycling and public transport.
	To reduce the impact of traffic.	33.1 ensure access arrangements maintain the safety and efficiency of the arterial and local road networks. 33.2 Ensure the level of service needed for new industrial and commercial operations does not

Clause	Objective	Strategies
		prejudice the reasonable needs of existing industrial and commercial operations to access Yarra’s roads.

The City of Yarra is currently undertaking a review of a number of Municipal Strategic Statement (MSS) policy themes, including Transport.

Yarra’s Planning Scheme Review – Report on Findings (October 2014) sets out the following in relation to the current Transport policy in the Planning Scheme:

An effective and efficient transport network is at the heart of a vibrant, equitable and prosperous municipality. In inner city environments, the management of the limited road and transport space and resources can require balancing of a number of objectives. This is a particular challenge in Yarra, due to the travel demands generated by:

- *the strategic location of the municipality on the edge of the central city*
- *the significant and growing mobile population, and*
- *the presence and proximity of major event attractors.*

Transport is currently addressed separately in the Context and Vision provisions of the Scheme as well as in strategy at Clause 21.06. It is also addressed in some specific policies such as the parking, access and traffic provisions of Built Form and Design Policy (Clause 22.10).

The current policy expresses a preference to reduce car dependency and encourage walking, cycling and public transport use. This appears to have had some success, with Yarra having a higher bicycle use rate than other parts of Melbourne.

There are still, however, inconsistencies regarding the requirement for Green Travel Plans, the use of car share schemes and reductions or waiving of on-site car parking.

Carparking was considered a particularly contested political issue in the initial consultation; any position or strategy regarding carparking is unlikely to satisfy all stakeholders. The Parking Strategy and Local Area Transport Management Policy provides a framework for the development of local area traffic management schemes.

The Scheme would be assisted with clear direction about how Council seeks to facilitate greater use of public transport, walking and cycling, and how and in what circumstances this will translate into reduced car parking, car sharing schemes and the like. The approach should include consideration of car parking in activity centres on a precinct wide basis (rather than site-by-site) as well as strategies relating to visitor car parking and increased bicycle parking.

3.3.1. Clause 22.07 – Development Abutting Laneways

The City of Yarra has a specific policy in relation to development abutting laneways.

The local policy identifies the need to retain existing laneways and enhance their amenity. It also states that, where appropriate, laneway access for vehicles is to be used in preference to street frontages to reduce vehicle crossovers.

Objectives

- *To provide an environment which has a feeling of safety for users of the laneway.*
- *To ensure that development along a laneway acknowledges the unique character of the laneway.*
- *To ensure that where development is accessed off a laneway, all services can be provided to the development.*
- *To ensure that development along a laneway is provided with safe pedestrian and vehicular access.*

Policy

It is policy that:

- *Where vehicular movement in the laneway is expected to cause a material traffic impact, a traffic impact assessment report be provided to demonstrate that the laneway can safely accommodate the increased traffic.*
- *Where alternative street frontage is available, pedestrian access from the street be provided.*
- *Pedestrian entries be separate from vehicle entries.*
- *Pedestrian entries be well lit to foster a sense of safety and address to a development. Existing lights may need to be realigned, or have brackets or shields attached or additional lighting may be required.*
- *Lighting be designed to avoid light spill into adjacent private open space and habitable rooms.*
- *Vehicle access be provided to ensure ingress and egress does not require multiple vehicular movements.*
- *Windows and balconies overlook laneways but do not unreasonably overlook private open space or habitable rooms on the opposite side of the laneway.*
- *Development respect the scale of the surrounding built form*
- *Development not obstruct existing access to other properties in the laneway.*
- *Doors to car storage areas (garages) not protrude into the laneway.*
- *The laneway not be used for refuse storage.*
- *All laneway upgradings which provide improved access to the development be funded by the developer.*
- *The laneway meet emergency services access requirements.*

Relevant additional policies and studies (which do not form part of the Planning Scheme) are summarised below.

3.3.2. Council Transport Statement 2006

City of Yarra's Strategic Transport Statement 2006 sets out a clear desire to reduce car dependence in the City of Yarra by promoting walking, cycling and public transport use as viable and preferable alternatives.

The Strategic Transport Statement sets out the following hierarchy of transport modes which forms the basis for decision making and actions related to transport in the City:

1. Pedestrians (including wheelchairs and walking with prams)
2. Cyclists
3. Tram
4. Bus/train
5. Taxi users/car sharers
6. Freight vehicles
7. Motorcyclists
8. Multiple occupants local traffic
9. Single occupants local traffic
10. Multiple occupants through traffic
11. Single occupants through traffic

The vision of Council's Transport Statement 2006 is ... *"to create a city which is accessible to everyone irrespective of levels of personal mobility and where a fulfilling life can be had without the need for a car"*.

There are seven key Strategic Transport Objectives (STO) to achieve this vision.

Of particular relevance is STO 5, which is to ... *"ensure Council's response to parking demand is based on Yarra's hierarchy and sustainable transport principles"*.

3.3.3. Transport Statement Review 2012

The City of Yarra's Strategic Transport Statement was reviewed in 2012.

Relevant key actions include the following:

- *Develop guidelines for assessing planning permit applications for car parking dispensation.*
- *Develop guidelines for car share operators that address the issues of location, number of bays and signage so that operators are clear as to the process and responsibilities.*

3.3.4. Yarra Parking Management Strategy

The Yarra Parking Management Strategy provides the framework around Yarra's policies for parking permit schemes, parking enforcement, the provision of disability access parking, managing parking around shopping strips, signage and all other parking-related issues and topics.

Council's website states that the fundamental aims of the Strategy are:

- to reduce the number of cars parking in Yarra,
- to promote public transport as an alternative to driving, and
- to ensure visitors contribute to the cost of providing Yarra's parking infrastructure.

A key aim underpinning this strategy is Council's desire to promote sustainable travel, such as cycling, walking and public transport.

Action Area 4 of Council's Parking Management Strategy is an integrated approach for Municipal Parking Strategy and in particular identifies a need to further develop Yarra's policy to provide a disincentive to car ownership and use by working with other sections of Council to promote behaviour change, sustainable transport and introduce more sustainable transport infrastructure.

3.3.5. Liveable Yarra Project

In 2015 Council undertook an extensive community engagement process known as the "Liveable Yarra Project". The consultation consisted of a number of elements including a People's Panel, Advisory Committees, and Targeted Community Workshops, and covered a range of topics, one of which was "Access and Movement".

The "engagement summary" document prepared by Capire Consulting Group (January 2016) summarised the consultation in relation to access and movement as follows:

"Access and movement received the highest number of priority votes at 64. Actions around the improvement of cycling, walking and non-automotive transport modes were strongly supported. Panel members suggested trialling street closures to "reclaim" street share for cyclists and pedestrians. The trade-off of busier arterials was seen as largely acceptable pending the trials. Panel members were very supportive of Council efforts to lobby for public transport upgrades."

The specific Access and Movement recommendations which were summarised in the "engagement summary" document are as set out in Table 4 below.

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Table 4: Summary of Parking Recommendations from Liveable Yarra Project

Action No.	Action	Support from People's Panel
1	Articulate targets for street share. Develop a municipality wide plan for transport and access.	86% support 12% not sure 2% disagree
2	Close local (residential) streets to through traffic including living streets.	36% support 48% not sure 16% disagree
3	Increase space for pedestrians and bikes, dedicated lanes/corridors. Decrease car space on the streets.	63% support 22% not sure 15% disagree
4	Require better bicycle parking as part of major development.	76% support 14% not sure 10% disagree
5	Reduce barriers that discourage riding, improve safety, connections, lighting. Council to provide additional cycling infrastructure – a comprehensive network that consistently provides a good level of service.	75% support 18% not sure 7% disagree
6	Move away from a “predict and provide” approach to providing car parking in new development.	86% support 12% not sure 2% disagree
7	Continue to work with State Government to improve performance of current public transport infrastructure assets.	36% support 48% not sure 16% disagree
8	Continue lobbying for improved public transport (new infrastructure and services).	63% support 22% not sure 15% disagree

4. Existing Conditions

4.1. Study Area

The study area extends for an approximately 1.2km long section of Heidelberg Road between Yarra Bend Road and Como Street as shown in the locality plan provided on the following page at Figure 1. The total study area is spread out along this stretch of Heidelberg Road and comprises of 4 precincts (Precincts 1, 2, 3a and 3b). The study area straddles City of Yarra and City of Darebin with Heidelberg Road separating the two municipalities.

Land within the study area is generally zoned either 'Commercial 1 Zone' or 'Commercial 2 Zone', as detailed in the Land Use Zoning Map at Figure 2.

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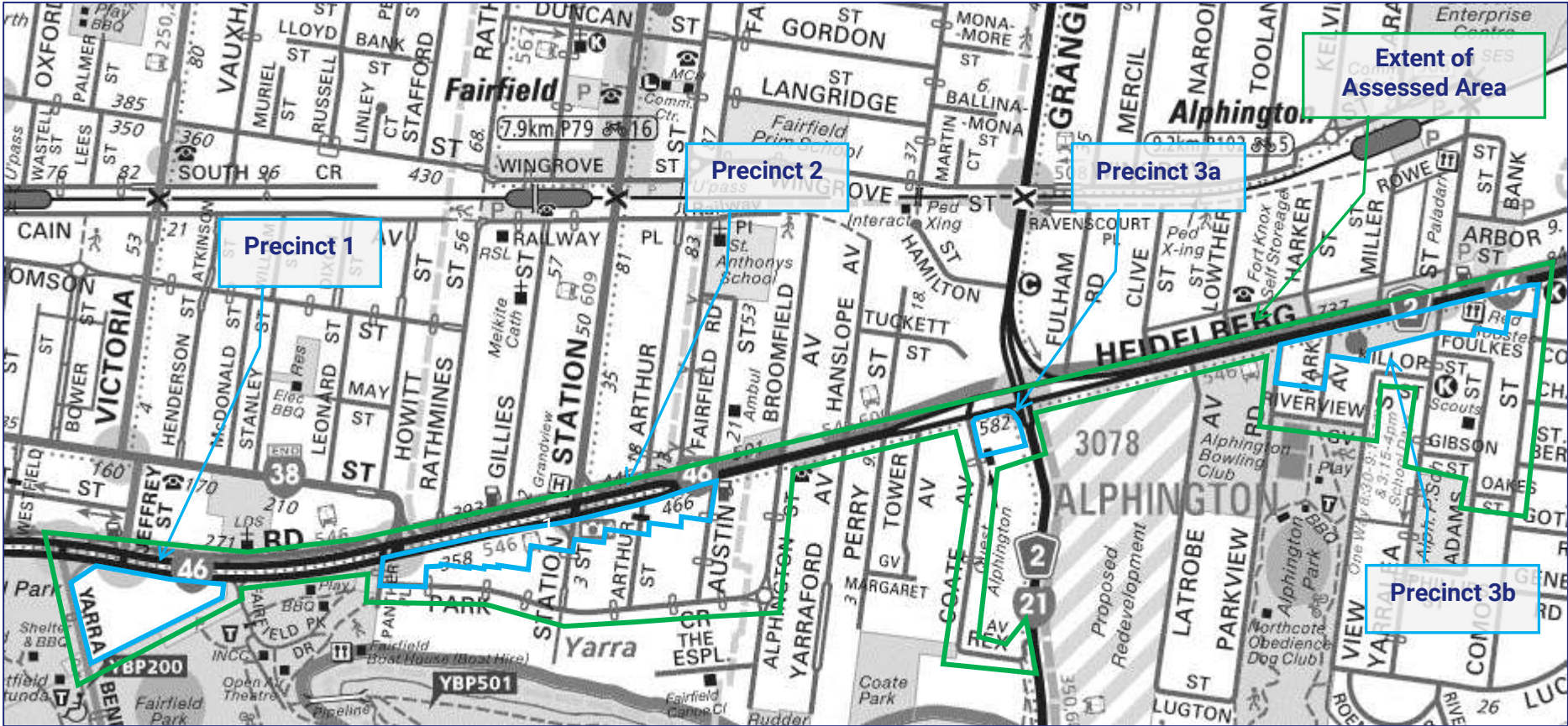


Figure 1: Locality Map

Source: Melway

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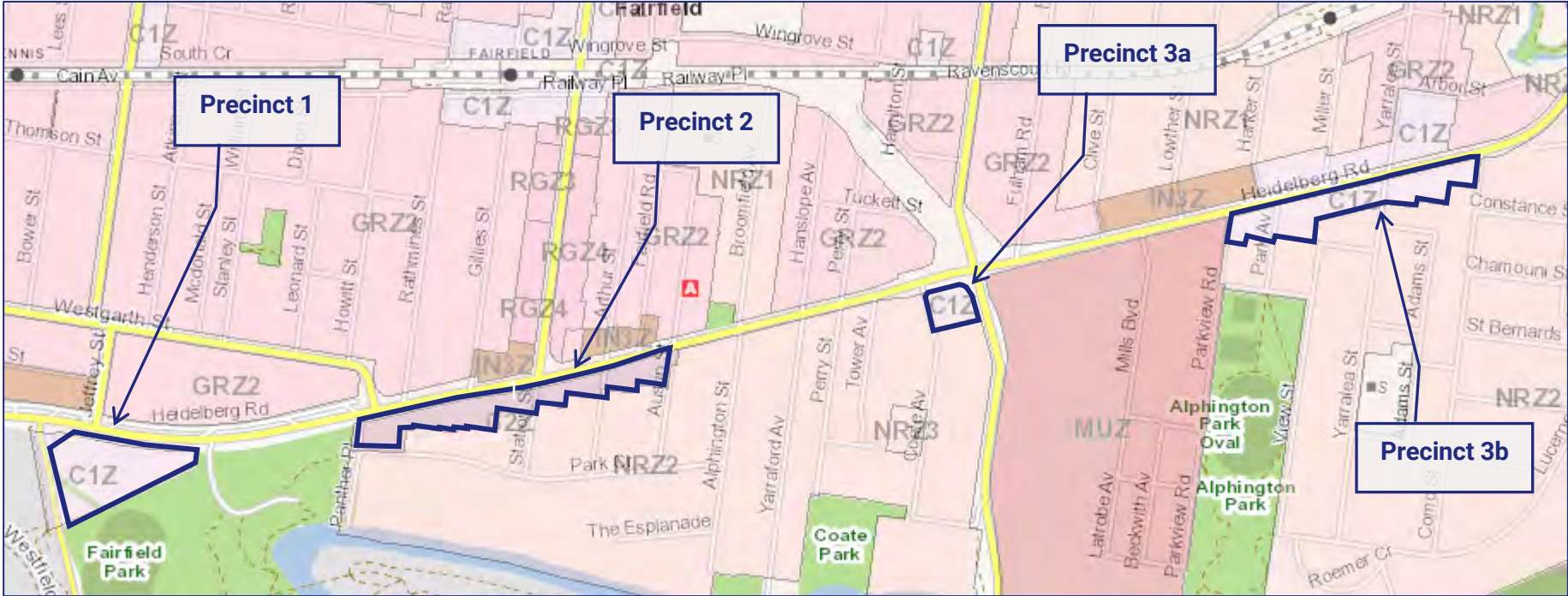


Figure 2: Land Use Zoning Map

Source: VicPlan

Land use within the study area generally comprises a mixture of commercial and industrial use along Heidelberg Road and residential areas immediately north and south of Heidelberg Road.

Key features and land uses located in close proximity of the study area include:

- Alphington Station, located at the north-east corner of the study area.
- Fairfield Station, located west of Station Street and centrally north of the study area.
- Former Alphington Paper Mill, a proposed redevelopment of a mixed use precinct within Precinct 3a on the corner of Chandler Highway and Heidelberg Road.
- Alphington Park, located at the intersection of Parkview Road and Riverview Grove.
- Fairfield Park, located west of Panther Place.
- Yarra Bend Park, located west of Yarra Bend Road.
- Yarra River, located south of the study area.
- Heidelberg Road Neighbourhood Activity Centre is located at the east end of the study area.
- Fairfield Village Neighbourhood Activity Centre is located approximately 450m north of Heidelberg Road.
- Eastern Freeway, located approximately 1km south of the study area.

All of these areas are readily accessible from various parts of the study area via walking, cycling or a short public transport trip.

4.2. Alphington Paper Mill Site

Whilst not located within the study area, the former Alphington Paper Mills site is located in between Precinct 3a and 3b, and accordingly the considerations of the associated development plan for this site is relevant to our assessment of the overall study area.

The Development Plan for the former Alphington Paper Mills site was endorsed in May, 2016, with the following key elements included within the overall plan (quoted from Council's website):

- *4.5% open space*
- *1700 square metres of community facilities and multi-purpose sports court*
- *30 metre wide buffer to the Yarra River*
- *5% affordable housing provision*
- *13,500 square meters of retail and commercial floor space*
- *Estimated 2500 dwelling in the form of town houses and apartments.*

The development plan has been informed by the following objectives (as set out within the overview of the Development Plan documentation:

- *A vibrant community that retains links to the former Alphington Paper Mill and industrial structures of heritage significance. These structures will be adapted and / or interpreted where practical to maintain a visual link to the site's industrial history.*
- *Thriving mixed use precincts, including a well-connected town centre, a village piazza and a community and learning hub. Provide increased live / work opportunities, education and community uses, affordable housing, higher density housing, retail and hospitality.*
- *An increased range of dwelling types that contribute to increased diversity within the local area and respond to changing household sizes, includes 5 per cent of the total dwellings as affordable housing.*
- *A traditional street pattern that efficiently utilises the existing street network, provides a street frontage to the heritage structures to be retained and responds to the topography of the site.*
- *A landscape character relative to the scale of development proposed, which brings the leafy character of Alphington Park and streets into the site before transitioning to the main street landscape envisioned for the northwest corner of the site. North / south corridors link to an industrial heritage landscape and the Yarra River as well as the 'Paper Trail' linear park. These distinctive landscapes contribute to the identification of a series of neighbourhoods with diverse identities and character.*

4.3. Road Network

The following describes the higher order roads within close proximity to the study area, and which have a direct impact on the study area. This study has also reviewed the local roads and laneways within the study area and a detailed review of the existing conditions of these streets is included at Appendix A of this report.

Due to the location of the Yarra River, and associated lack of north-south routes, travelling to and from the south from the Heidelberg Road corridor is somewhat restricted and can only be provided via Chandler Highway. This has impacts on all modes of transport, particularly on cycling and walking

The configuration of **Heidelberg Road** varies considerably throughout each of the precincts. Along its entirety, Heidelberg Road is a VicRoads declared arterial road and Road Zone Category 1 and extends throughout the study area in an east-west direction.

At **Precinct 1**, Heidelberg Road is configured with three lanes in each direction separated by a central median. The westbound carriageway accommodates a kerbside bicycle lane/car parking lane. The westbound carriageway accommodated a kerbside bicycle lane and a service road accommodating one lane for eastbound traffic and kerbside car parking.

The speed limit within this precinct is 60km/h.



Figure 3: Precinct 1 – Heidelberg Road – view east



Figure 4: Precinct 1 – Heidelberg Road – view west

At **Precinct 2**, Heidelberg Road is generally configured with three lanes in each direction separated by a central median with the kerbside lanes accommodating on-street car parking outside of Clearway times. Localised widening occurs at the signalised intersections to accommodate turn lanes. Towards the east end of the precinct, Heidelberg Road narrows to two-lanes in each direction.

The speed limit within this precinct is 60km/h.



Figure 5: Precinct 2 – Heidelberg Road – view east



Figure 6: Precinct 2 – Heidelberg Road – view west

Either side of **Precinct 3a**, Heidelberg Road is configured with two lanes in each direction, with the kerbside lanes accommodating on-street car parking outside of Clearway times. Localised widening occurs at the signalised intersection with Chandler Highway to accommodate three lanes and turn lanes.

The speed limit within this precinct is 60km/h.

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Figure 7: Precinct 3a – Heidelberg Road – view east



Figure 8: Precinct 3a – Heidelberg Road – view west

At **Precinct 3b**, Heidelberg Road is configured with two lanes in each direction, with the kerbside lanes accommodating on-street car parking outside of Clearway times. Localised widening occurs at the signalised intersection with Yarralea Street to accommodate right turn lanes from Heidelberg Road.

The speed limit within this precinct is generally 60km/h, with a 40km/h limit applying west of Park Avenue, relating to roadwork.



Figure 9: Precinct 3b – Heidelberg Road – view east

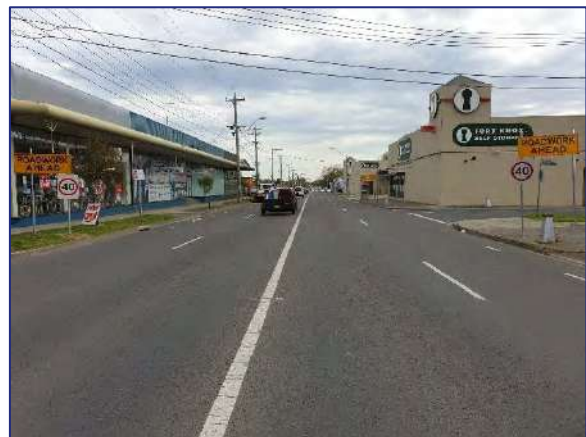


Figure 10: Precinct 3b – Heidelberg Road – view west

Chandler Highway is a VicRoads declared arterial road and Road Zone Category 1 which extends in a north-south direction between Heidelberg Road in the north (where it continues as Grange Road) and the Earl Street in the south (where it continues as Princess Street).

Chandler Highway is separated by a central median and typically provides three through traffic lanes in each direction, with kerbside bicycle lanes on both sides. No Stopping restrictions apply along both sides of Chandler Highway.

A posted speed limit of 60km/h applies to Chandler Highway in the vicinity of the study area.

Within the study area (south of Heidelberg Road), **Station Street** is a local road¹ which extends in a north-south direction between Heidelberg Road in the north, where it becomes a VicRoads declared arterial road and Road Zone Category 1, and a dead end in the south.

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Heidelberg Road, Fairfield/Alphington

Station Street typically provides one through traffic lane in each direction with indented kerbside parallel parking on both sides. On-street parking is typically short-term (2P).

A posted speed limit of 40km/h applies to Station Street south of Heidelberg Road.



Figure 11: Station Street - view north



Figure 12: Station Street - view south

Westgarth Street is a VicRoads declared arterial road and Road Zone Category 1 which extends in an east-west direction between Heidelberg Road in the east and Merri Parade in the west.

Westgarth Street typically provides one through traffic lane in each direction, with dedicated bicycle lane on both sides. Kerbside parking is provided adjacent to the bicycle lanes on both sides. On-street parking is generally unrestricted.

A posted speed limit of 60km/h applies to Westgarth Street.

Yarra Bend Road is a local road¹ which extends in a north-south direction between Heidelberg Road in the north and a loop road to the south, which provides access to the parklands.

Yarra Bend Road provides one through traffic lane in each direction. No kerbside parking is provided on both sides of Yarra Bend Road and limited indented parking are provided on the west side of the road.

The default suburban speed limit of 50km/h applies to Yarra Bend Road.

¹ As defined in the City of Yarra Road Management Plan Register of Public Roads, dated 4th July, 2017.



Figure 13: Yarra Bend Road - view north



Figure 14: Yarra Bend Road - view south

4.3.1. Arterial Road Traffic Volumes

The following table sets out the Average Annual Daily Traffic Volumes of the arterial roads within the study area. This information is sourced from the VicRoads Arterial Road Database (April, 2018).

Table 5: Arterial Road Traffic Volumes (Source: VicRoads Arterial Road Database – April 2018)

Road Name	Average Annual Daily Traffic Volume (two-way)
Heidelberg Road (each precinct)	
Btw Westfield St/Jeffrey St (Precinct 1)	30,000
Btw Jeffery St/Westgarth St (Precinct 1)	30,000
Btw Westgarth St/Station St (Precinct 2)	29,000
Btw Station St/Chandler Hwy (Precinct 3a)	28,000
Chandler Highway	
Btw Heidelberg Rd/Yarra Bvd	40,000
Grange Road	
Btw Chandler Hwy/Separation St	22,000
Station Street	
Btw Heidelberg Rd/Separation St	16,900
Westgarth Street (north and south)	
Btw Heidelberg Rd/Jeffrey St	6,400

4.3.2. Existing Parking Conditions

On-street parking within the study area is a mixture of short-term (2P or less), medium-term (3 & 4P), unrestricted and permit zone restrictions.

Parking along the south side of Heidelberg Road within the study area is generally unrestricted outside of Clearway 6:30am-9:30am Mon-Fri times, with some short-term parking within Precinct 3b.

Parking within the local streets in the vicinity of the study area is generally controlled by short-term restrictions.

A map detailing the various car parking restrictions throughout each precinct is provided at Appendix B.

4.4. Public Transport

The study area has access to a number of public transport services including rail and bus services within walking distance of the study area.

The existing public transport services within close proximity of the study area are shown on the Public Transport Map at Figure 15 and a summary provided at Table 6.

The study area is partially located within the PPTN Area, as detailed in the map at Figure 16.

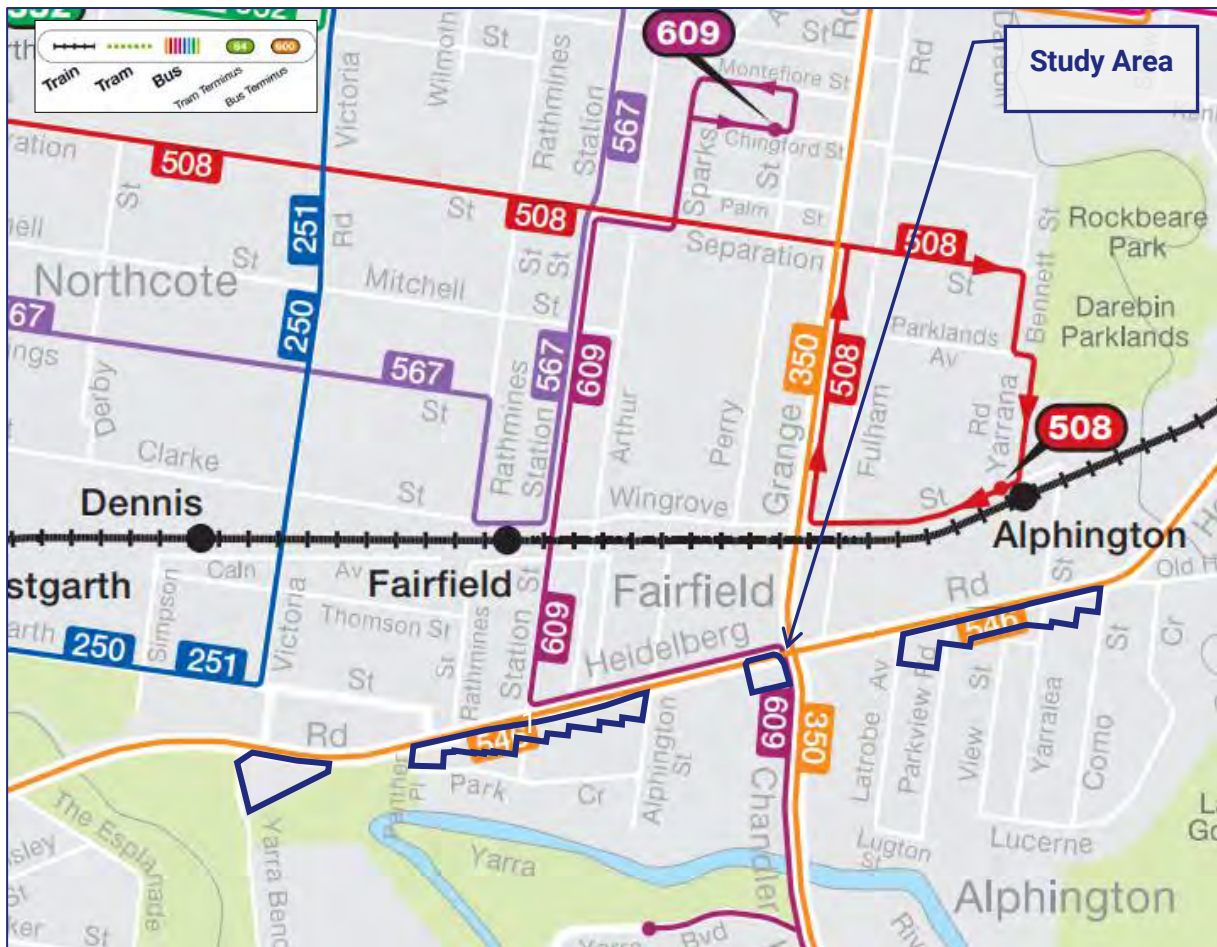


Figure 15: Public Transport Map

Source: Public Transport Victoria

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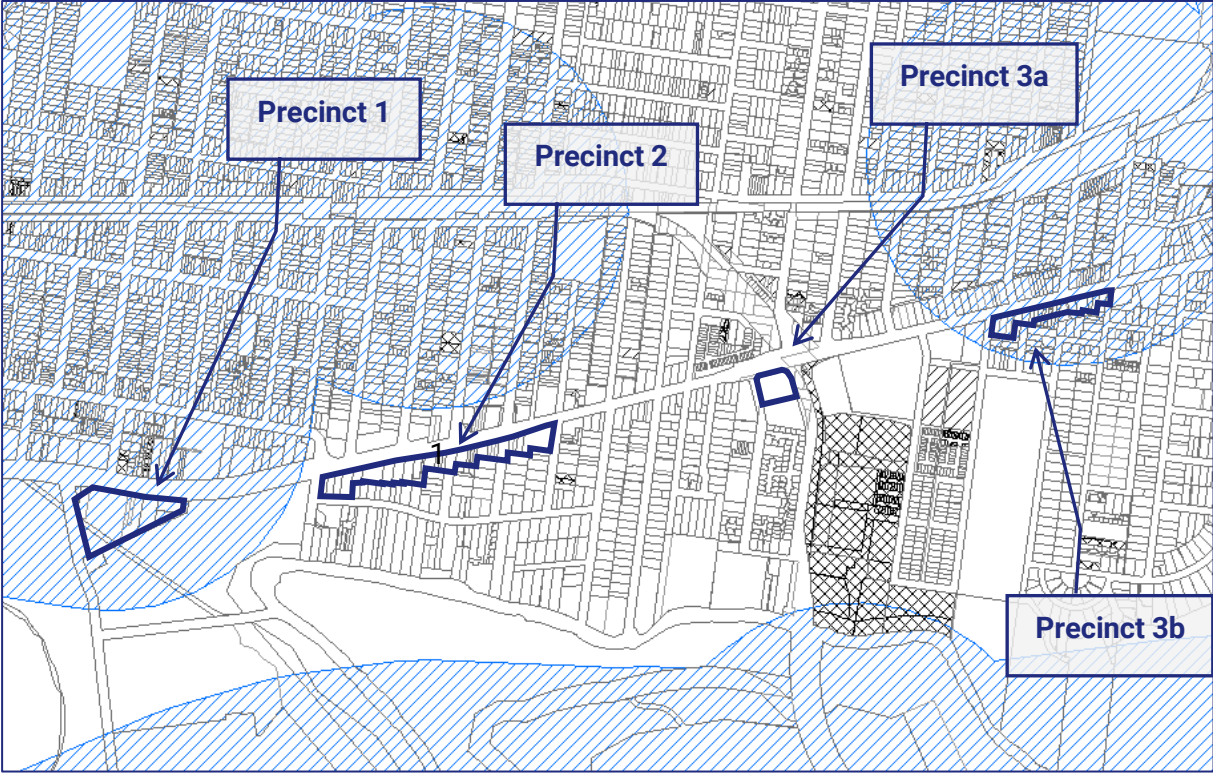


Figure 16: PPTN Map

Table 6: Summary of Public Transport Services

Service	Between	Via	Operating Times (Frequency)		
			Weekday	Saturday	Sunday
Dennis Station, Fairfield Station and Alphington Station – located north of study area					
Hurstbridge Line	Hurstbridge & City	Alphington	Operate at high frequency		
Heidelberg Road – operates through the study area					
Bus Route 546	Heidelberg Station & Queen Victoria Market	Clifton Hill & Carlton	6:20am-6:50pm 30 minutes	Does Not Operate	
Bus Route 609	Hawthorn & Fairfield	Kew	8:05am-1:55pm 60 minutes	Does Not Operate	
Grange Road – located north of the study area					

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Service	Between	Via	Operating Times (Frequency)		
			Weekday	Saturday	Sunday
Bus Route 350	La Trobe University & City	Eastern Freeway	7:05am-6:30pm 20 minutes	Does Not Operate	
Westgarth Street – located north of the study area					
Bus Route 250	La Trobe University & City	Westgarth	5:30am-11:20pm 20-30 minutes	6:15am-11:45pm 30-40 minutes	6:40am-10:45pm 30 minutes
Bus Route 251	Northland SC & City	Westgarth	6:50am-8:50pm 20 minutes	7:20am-7:10pm 30-40 minutes	8:25am-5:45pm 40 minutes
Wingrove Street – located north of the study area					
Bus Route 508	Alphington Railway Station & Moonee Ponds	Northcote & Brunswick	5:30am-10:35pm 10-20 minutes	6am-11:35pm 30-60 minutes	8:20am-10:40pm 40-60 minutes

We note that the bus services which travel south, along Chandler Highway, do not operate on the weekend and as such, it is not possible to travel south of the Yarra river efficiently on the weekends.

Similarly, the bus routes which operate along the Heidelberg Road throughout the study area do not operate on the weekends.

The remaining bus services do not operate at high frequency during peak or off-peak times.

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Heidelberg Road, Fairfield/Alphington

4.5. Sustainable Travel Modes

The study area is served by an adequate network of bicycle routes, albeit with some discontinuity and functionality issues. Figure 17 below shows the Travel Smart Map for the study area.



Figure 17: Travelsmart Map

Source: City of Yarra

4.5.1. Cycling

Heidelberg Road provides on-road bicycle lane between Yarra Bend Road/Station Street and Coates Avenue/Chandler Highway. The other sections of Heidelberg Road within the study area are nominated as an informal bicycle route. The bicycle lanes along Heidelberg Road within the study area are disconnected and inconsistent. They are relatively unsafe and not well suited to less experienced cyclists. While there are dedicated bike lanes in some sections many parts of Heidelberg Road either have no bike lane or one shared with parked cars outside the clearway times. In practice the discontinuous route will be of little value to inexperienced cyclists.

On-road bicycle lanes are provided on several of the north-south streets which intersect the study area including Westgarth Street, Station Street, and Chandler Highway. Key off-road bicycle routes include the Capital City Trail to the west, the Main Yarra Trail along the Yarra River to the south and a bicycle trail which extends along the Eastern Freeway.

Chandler Highway provides a good connection from the study area to the south, via dedicated on-street bicycle lanes from Heidelberg Road to the Eastern Freeway. To the south of the Eastern Freeway an off-road shared path continues to the south-east.

4.5.2. Car Share

As shown on the TravelSmart map at Figure 17, two car share vehicles located north of the study area, in the vicinity of Fairfield Railway Station.

4.5.3. Walking

The study area is somewhat walkable where only some services and destinations are within a convenient walking distance. The Walkscore² map for Fairfield and Alphington is illustrated in Figure 18, with a score of 69. This is a measure of the level of accessibility to local services by walking. The score is classified as 'Somewhat Walkable', which states that some errands can be accomplished on foot.

We note that the main pedestrian connection to local services within close proximity to the study area is via Heidelberg Road. The main shopping precinct in close proximity to the study area is Fairfield Village, which is located at least 500m from Precinct 2 of the study area, with greater walking distance for the remaining precincts. We note that level of accessibility to local services within the study area will increase when the development at Alphington Paper Mill site is complete. Upon completion, the development will add additional services including retail shops, restaurant, banks, post office and commercial uses.

The following railway stations are within close proximity to the study area:

- **Dennis Station**, located approximately 750m walking distance from Precinct 1 (10 minutes walk).
- **Fairfield Station**, located approximately 700m walking distance from Precinct 2 (10 minutes walk).

² <https://www.walkscore.com/AU-VIC/Melbourne/Alphington>

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- **Alphington Station**, located approximately 850m walking distance from Precinct 3a and 550m walking distance from the east end of Precinct 3b (7-10 minutes walk).

The study area also has access to several walking routes including The Main Yarra Trail along the Yarra River and walking paths through Coate Park and Alphington Park.

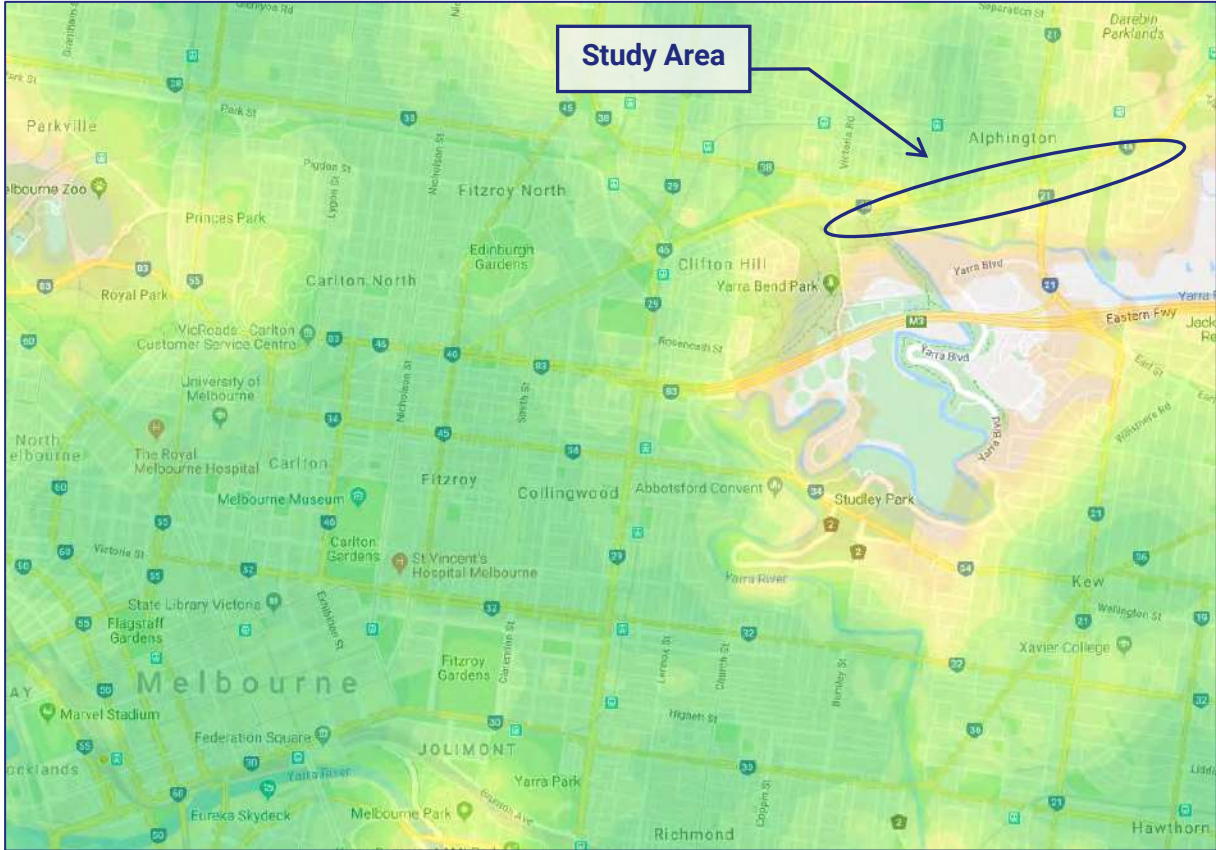


Figure 18: Walkscore Map - Fairfield & Alphington

4.6. Demographics

4.6.1. Car Ownership Statistics

The majority of new dwellings within the study area will be apartment style dwellings. A review of car ownership statistics for ‘flats units and apartments’ within the suburbs of Fairfield and Alphington and the City of Yarra highlights the following average car ownership statistics. This data was recorded by the Australian Bureau of Statistics (ABS) in the 2016 census.

We note that the sample size for these suburbs is quite limited, as the housing stock is still mostly semi-detached and detached dwellings, rather than apartments. However, the level of apartments is expected to increase in the near future.

These statistics indicate that the parking requirements for dwellings set out under Clause 52.06-5 of the Planning Scheme are generally higher than the car ownership statistics for one and three-apartments in this locality.

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Table 7: ABS Census Car Ownership Statistics (2016) – Flat/Unit/Apartment

Type of Dwelling	Number of Cars	Alphington Suburb	Fairfield Suburb	Yarra LGA
1 bedroom Flat/Unit/Apartment in one or more storey block	Average no. of cars per dwelling	0.9	0.8	0.7
	0 cars	20%	28%	38%
	1 car	71%	67%	55%
	2 or more cars	9%	5%	7%
2 bedroom Flat/Unit/Apartment in one or more storey block	Average no. of cars per dwelling	1.2	1.2	0.9
	0 cars	13%	15%	26%
	1 car	56%	61%	56%
	2 or more cars	31%	24%	19%
3 bedroom Flat/Unit/Apartment in one or more storey block	Average no. of cars per dwelling	1.8	1.6	1.2
	0 cars	8%	0%	20%
	1 car	22%	37%	48%
	2 or more cars	70%	63%	33%

4.6.2. Journey to Work Data

A review of Journey to Work data for the suburbs of Alphington and Fairfield, the City of Yarra and the Greater Melbourne highlights the following statistics. This data was recorded by the Australian Bureau of Statistics (ABS) in the 2016 Census.

This data highlights a stronger reliance on public transport, walking and cycling for those living (in particular) within the study area compared with the Melbourne metropolitan area.

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Heidelberg Road, Fairfield/Alphington

Table 8: ABS Census Journey to Work Data (2016)

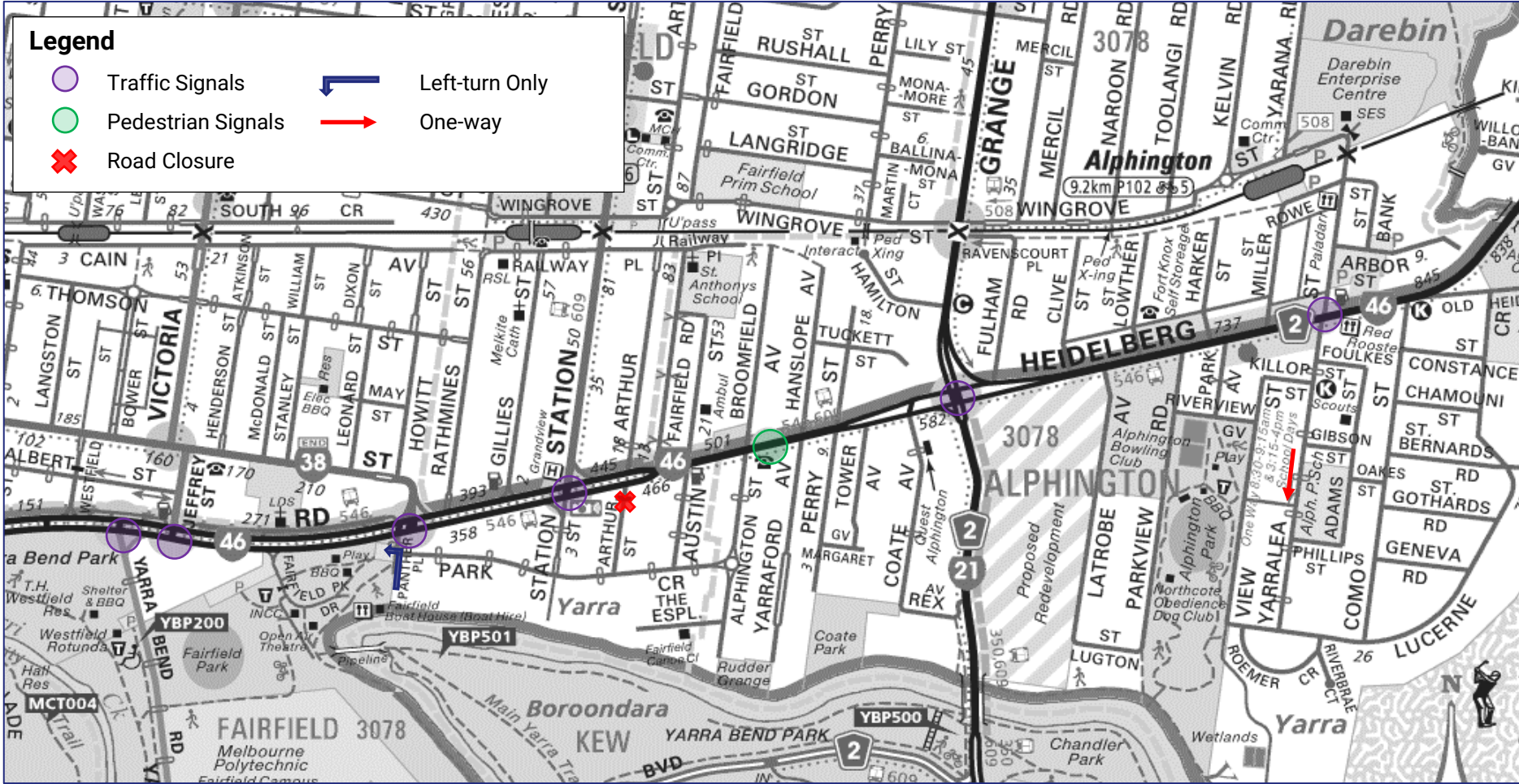
% mode of travel for 'journey to work' trip	Live within the area (i.e. place of residence)				Work within the area (i.e. place of employment)		
	Alphington	Fairfield	City of Yarra	Greater Melb.	Alphington-Fairfield SA2*	City of Yarra	Greater Melb.
Car as driver	48.3%	43.4%	32.8%	60.2%	68.8%	48.5%	59.8%
Public Transport	22.2%	27.4%	28%	15.4%	6.9%	23.7%	15.8%
Walking	2.6%	3.2%	12.4%	3%	3.5%	5.9%	3.1%
Cycling	6.4%	6.5%	8.6%	1.4%	1.9%	4.3%	1.4%
Other (car passenger, motorcycle, taxi)	3.3%	3.3%	2.9%	4.5%	3.4%	3.4%	4.4%
Other Data (worked at home, did not go to work, mode not stated)	15.6%	14.4%	13.1%	13.8%	14.5%	12.5%	13.9%

4.7. Traffic Management

A detailed review of the existing traffic management measures within the study area is provided at Appendix C. The following map summarises the traffic management measures along or immediately adjacent to Heidelberg Road.

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Heidelberg Road, Fairfield/Alphington



Source: Melway

Figure 19: Traffic Management Map

5. Transport Impacts

The primary purpose of this study is to review the traffic engineering implications of the implementation of the Design and Development Overlay, which introduces a range of built form controls to the Yarra Planning Scheme. This amendment is required to implement the recommendations of the Heidelberg Road Built Form Framework prepared by Hodyl & Co in order to allow for more intense development along the Heidelberg Road Corridor.

The key transport engineering impact of the proposed controls is the direction to use local roads and rear laneways for vehicle access to new developments wherever possible and avoid new crossovers to Heidelberg Road. As a result, the use of the laneways and local roads within the study area will increase. This study reviews the potential impacts of new development and makes recommendations to manage the increased use of local roads and laneways.

The following sections provide:

- An overview of the likely traffic impacts of increased development along Heidelberg Road, by reviewing a case study of Victoria Street, Richmond.
- A discussion regarding access to Heidelberg Road being avoided if possible, for vehicle access.
- Identification of locations which may require additional analysis to be completed as part of a development application.
- Recommendations for provision of car parking within each precinct.
- Analysis of potential capacity of the road network to accommodate on-street parking generated from developments.

This study does not seek to undertake detailed traffic modelling of Heidelberg Road or its key intersections. Traditional traffic modelling relies on estimates of future growth of land use intensity and assumptions about future trip generation rates and transport mode choice to assess the impact on a transport network. In our view, these critical modelling assumptions cannot be determined with any certainty for this area.

There are a number of factors that mean that preparing a detailed traffic model for this Activity Centre is not possible. At this time, Yarra City Council has not completed a detailed study regarding possible increases in dwelling numbers or commercial floor space on specific sites, which is an essential requirement of any model.

Future policy on car parking provision is expected to move away from a 'predicted and provide' approach to car parking provision (as identified by the Liveable Yarra Project) towards using car parking as a tool to encourage sustainable transport choices. Car parking provision rates are expected to be lower than have historically been required. The provision of car parking can have a significant impact on the traffic generated by a development site and the mode choice of trips generated by any development and this will greatly affect any assessment of future traffic conditions.

Fundamentally though, a detailed traffic model would not assist in achieving the key objectives of this study, which is to best manage the transport challenges posed by new

development. This is primarily achieved by applying best principles access management techniques to manage this new development.

This study aims to promote alternative transport modes in the following key ways:

Public Transport

- Maximise the accessibility of public transport, including fixed rail and bus services

Walking

- Provide a high quality pedestrian environment, including minimising the impact of vehicle access points along key pedestrian routes, especially Heidelberg Road
- To protect and enhance pedestrian connectivity to key destinations
- Promote public transport by providing good pedestrian and cycling links to public transport stops

Cycling

- Promote a safe cycling environment by minimising the number of conflict points with vehicles

Key outcome of this assessment is an Access and Movement Plan for properties abutting Heidelberg Road. This plan applies best practice vehicle access management techniques to properties abutting Heidelberg Road to manage the impacts of vehicle access to abutting properties on these three modes and maximise the efficiency of the arterial road network. A detailed model of traffic movement along Heidelberg Road would be of no assistance to this assessment. These techniques would be recommended notwithstanding any traffic model.

To take a historical example, a detailed traffic model of the Swan Street/Lennox Street intersection or Swan Street/Church Street intersection would have no impact on vehicle access locations adopted for the Dimmeys redevelopment at 140 Swan Street. Vehicle access to the rear and side of the property, rather than directly to Swan Street was chosen on best practice access management principles.

This report does include a detailed review of Victoria Street, Richmond, as a case study of how traffic conditions on Heidelberg Road are likely to change over time. Victoria Street has a number of parallels with Heidelberg Road and has and will experience some significant development. This case study provides a high level overview of how additional development on Heidelberg Road will change the transport conditions along Heidelberg Road. In our view, this case study provides a better guide to the likely future transport conditions along Heidelberg Road than any mathematical model, which would be based on highly uncertain assumptions regarding development scale, future trip generation rates and mode choices.

5.1. Traffic impacts along Heidelberg Road

In order to assess the likely traffic impacts of increased development along Heidelberg Road, we have undertaken a case study and review of Victoria Street, Richmond. The review generally covers the period between 2006 and 2016.

In April, 2010, Yarra City Council adopted the Victoria Street Structure Plan, a document that built on planning work that occurred between 2002 and 2010. Since that time, significant redevelopment has occurred, particularly within the eastern and western precincts identified by this structure plan.

The following reviews the changes to Victoria Street and the changes in transport along Victoria Street as a model for how Heidelberg Road may evolve over time.

It should be noted that there are some similarities and differences between Heidelberg Road and Victoria Street. Both are key arterial roads located within inner Melbourne providing important routes for travel from outer areas through to the CBD. However, a distinction needs to be made in comparing between Victoria Street as it is now and Heidelberg Road as it is currently.

Victoria Street does perform better in a number of areas compared to Heidelberg Road in terms of its sustainable transport characteristics. This includes closeness to the CBD, the availability of on-road public transport services, walkability and availability of local services. However, access to metropolitan rail services is higher for most of Heidelberg Road than Victoria Street. This provides a different level of public transport access to the CBD (and wider Melbourne).

However, it should be recognised that many of Victoria Streets sustainable transport characteristics have improved markedly in the last 20 years, including increased tram services, number and quality of bicycle connections and changing land use all occurred over the review period.

Heidelberg Road has significant scope for improvement in the areas of cycling, walking and access to local services. This includes the provision of increased on-road public transport services, where there is significant scope to extend the frequency and hours of operation of existing bus services. The full development of the Paper Mill site will provide a much higher level of access to local services within a walkable distance than at present. There are also opportunities to increase the walking and cycling environment along Heidelberg Road.

From a transport perspective, the study area has considerable potential to improve in the key areas that have assisted in achieving the shift in travel patterns seen in Victoria Street.

5.2. Case Study – Victoria Street

In order to assess the likely traffic impacts of increased development along Heidelberg Road, we have undertaken a case study and review of Victoria Street, Richmond. The review generally covers the period between 2006 and 2016. '

The case study is provided in detail at Appendix D.

5.2.1. Summary of Case Study

Based on the study, the following conclusions can be drawn from the development of Victoria Street over the last 10 years:

- Victoria Street has experienced significant development over the last 10 years, with over 3,000 new dwellings being constructed on properties that directly abut Victoria Street.
- The daily volume of traffic on Victoria Street has decreased, in some sections by up to 25%.
- Sustainable transport modes for journey to work purposes have significantly increased within the City of Yarra and Richmond for both residents and employees in Richmond.
- Public transport services (trams) on Victoria Street have doubled.
- Bicycle usage has increased significantly as a transport mode within Richmond and Victoria Street.
- Alternative transport modes such as car share vehicles have become available over time.

From the review of case study data, a modal shift is certainly occurring and it is modal shift that is accommodating the increased transportation activity within Richmond. While the population and development intensity along Victoria Street has increased, the daily traffic volumes along Victoria Street and parallel traffic routes has reduced over time and been taken up by alternative transport modes.

It is not evident from the arterial road volume data that non-local traffic is dispersing to other routes. The traffic volumes on Victoria Street, Bridge Road and Swan Street have all fallen over the last 10 years. While, locally generated traffic within Richmond would be displacing non-local or through traffic, however the main shift appears to be towards sustainable transport modes.

A key driver of this change is due to:

- Changes in land use over time along Victoria Street with a shift away from manufacturing towards service and professional industries.
- An increasing mix of land uses including a significant increase in dwellings and new mix of commercial uses in place of industrial uses.
- A change in demographic with the gentrification of Richmond. Residents of Richmond are increasingly younger persons employed in professional industries who live and work locally (including the CBD and nearby Activity Centres). Travel by private car is not necessarily the most convenient mode of travel for many trips to either work or everyday destinations (shopping, etc.). The increased number of dwellings on Victoria Street are well served for everyday needs by a short walk to Victoria Gardens.

5.3. Traffic Impacts to Local Road Network

The following sets out our high level review of the potential traffic impacts to local roads within the study area generated by the proposed height controls and level of development that could potentially occur within the area. The following highlights any locations that should be further analysed during the application process for vehicle access to certain streets.

5.3.1. Precinct 1

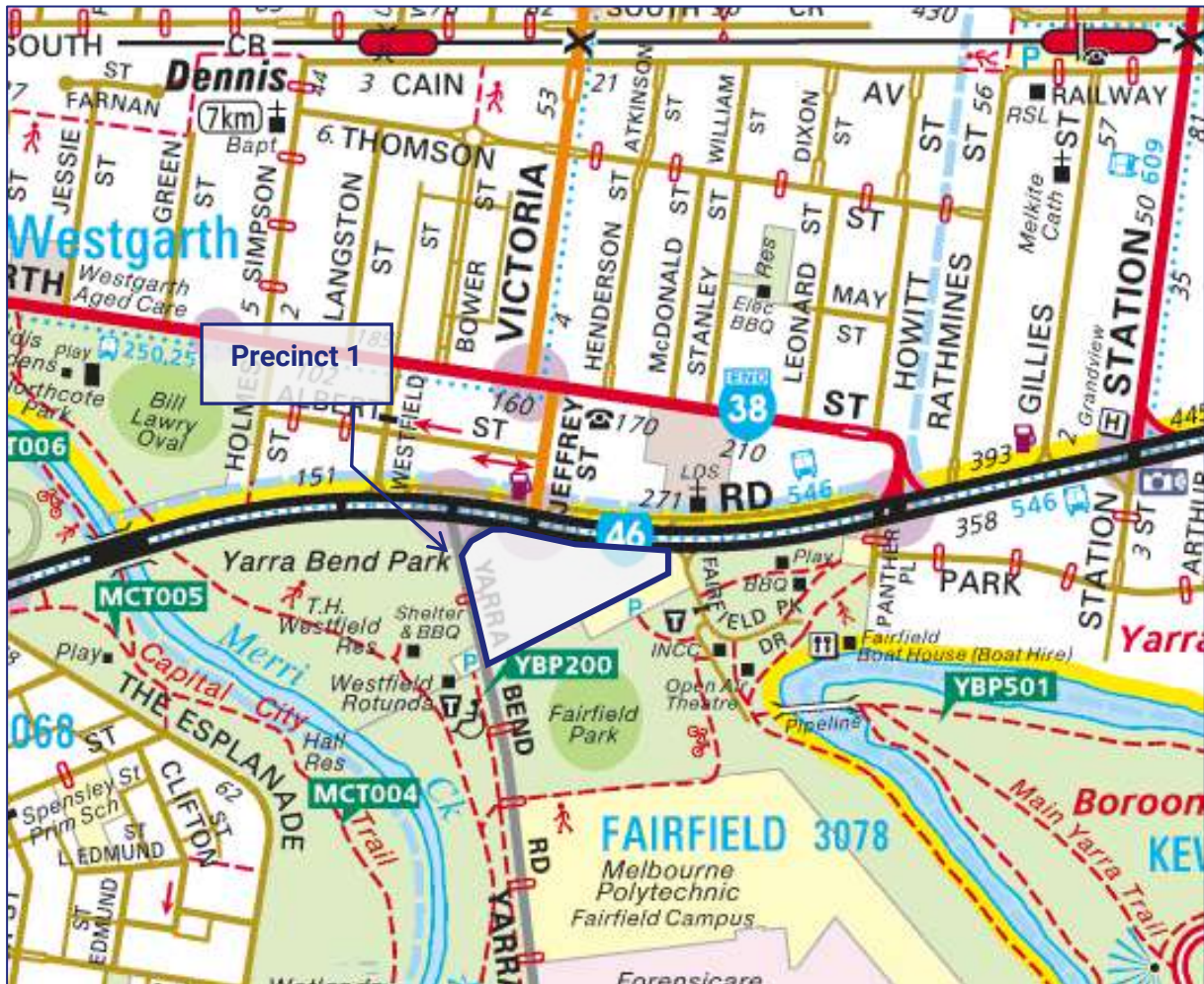


Figure 20: Precinct 1 Map

We understand that the redevelopment of the land in this precinct is likely to be largely residential, with a small amount of commercial. Based on the access and movement plan detailed in Section **Error! Reference source not found.**, vehicle access for the western part of the precinct should be undertaken via Yarra Bend Road, with the remaining properties accessing Heidelberg Road directly.

Existing usage of Yarra Bend Road includes recreational uses associated with Yarra Bend Park, as well as Melbourne Polytechnic and the Thomas Embling Hospital.

We are satisfied that the level of traffic likely to be generated by Precinct 1 to Yarra Bend Road is expected to be able to be accommodated by Yarra Bend Road, particularly given that the Yarra Bend Road/Heidelberg Road intersection is controlled by traffic signals.

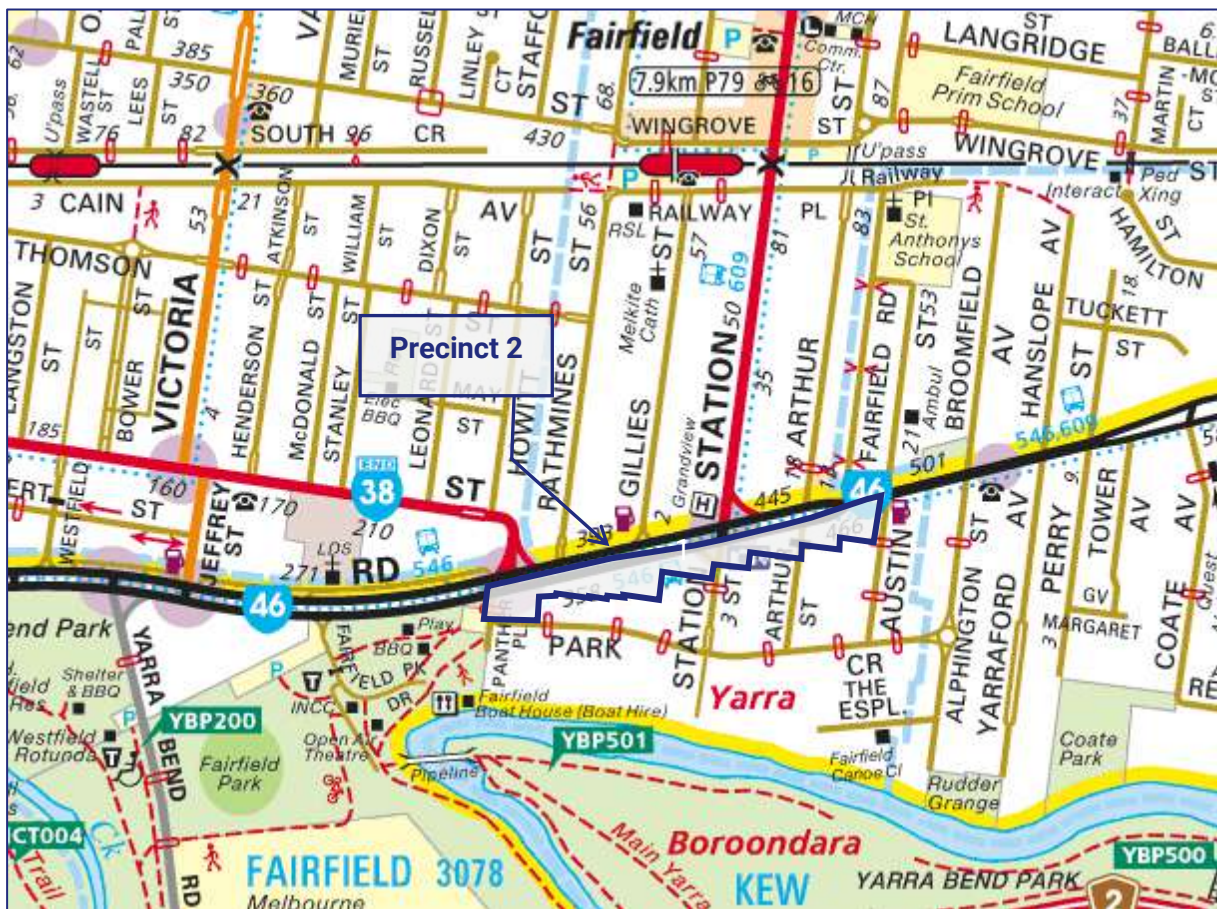
Further, the portion of the precinct which will take vehicle access to Yarra Bend Road is currently occupied by industrial uses, which are likely to be generators of traffic and would include heavy vehicle traffic.

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Accordingly, we do not believe the level of traffic generated by the level of development proposed for this precinct will pose a significant issue for the operation of Yarra Bend Road or its intersection with Heidelberg Road.

For any redevelopment of this site utilising Yarra Bend Road for access, the application material would need to include detailed traffic analysis including Sidra analysis of the intersection to ensure that the capacity of the intersection is not exceeded.

5.3.2. Precinct 2



The level of traffic generated as a result of the development proposed for this precinct of a 5 storey height limit is not expected to be significant. We do not expect that the level of traffic will increase to a detrimental level on any of the adjoining local roads.

The block bound by Panther Place and Station Street includes a significant number of properties which will be required to either continue to take vehicle access directly to Heidelberg Road, or not provide parking on the site. The remaining properties accessing the local road network directly do not have the development potential to cause a detrimental impact to Panther Place or Station Street.

We note that Panther Place and Station Street are both controlled by traffic signals at their intersections with Heidelberg Road.

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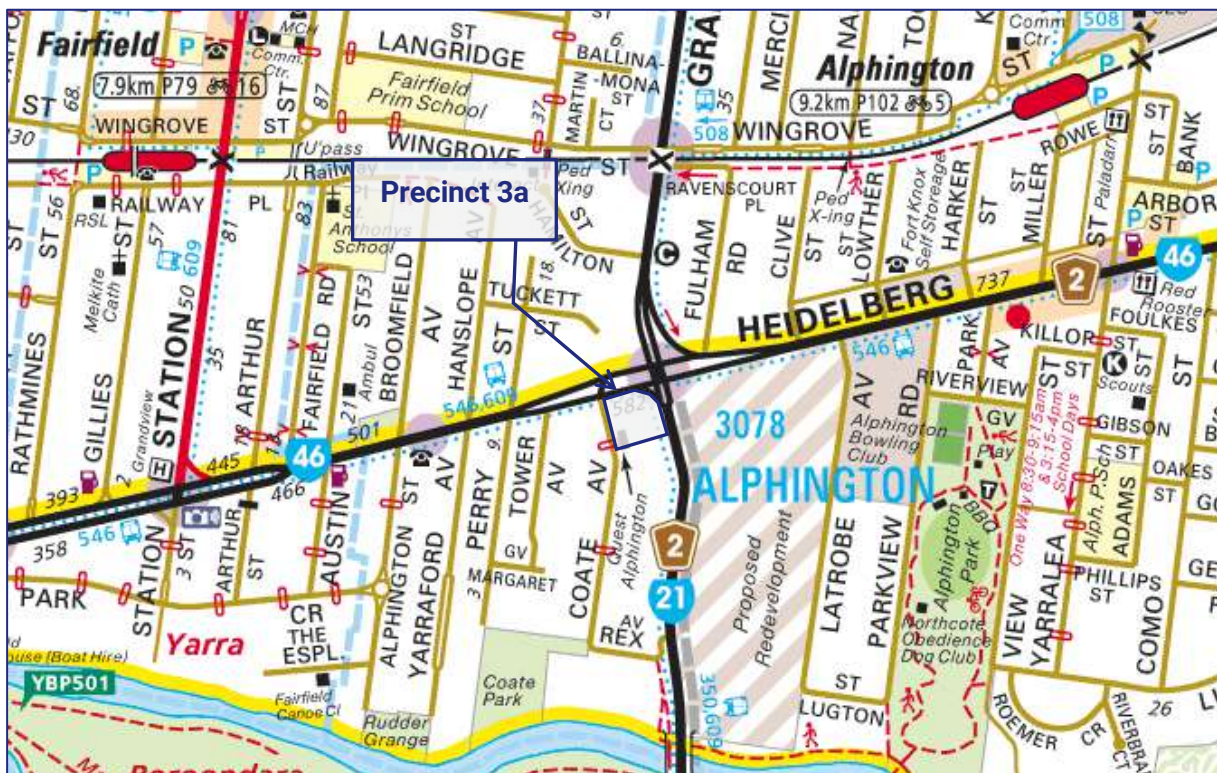
Arthur Street includes a road closure which is located close to the intersection with Heidelberg Road. This road closure will only allow for the properties within the DDO area to access Heidelberg Road. All other properties to the south (and outside the study area) cannot access Heidelberg Road from Arthur Street. Accordingly, it will only be the two sites on either side of Arthur Street which will take access to this section of Arthur Street.

Arthur Street is restricted to left-in/left-out and accordingly, we are satisfied that the likely traffic increase to the intersection will be minor and accommodated by the left-in/left-out nature of the road.

The Heidelberg Road Built Form Framework does not indicate that the intention is for vehicle access to be undertaken via Austin Street. In the access maps discussed at Section 6.3.1 and attached at Appendix F, we are satisfied that vehicle access could be taken to Austin Street and it would be beneficial to do so for the block bound by Austin Street and Arthur Street.

If vehicle access were taken to Austin Street we do not believe this would have a significant impact to Austin Street, as the current use of the site as a car dealership with service centre included would generate a level of traffic which would be potentially comparable to the development potential of this land.

5.3.3. Precinct 3a



The level of traffic impact from any redevelopment of this site would need to be assessed as part of any development proposal submitted.

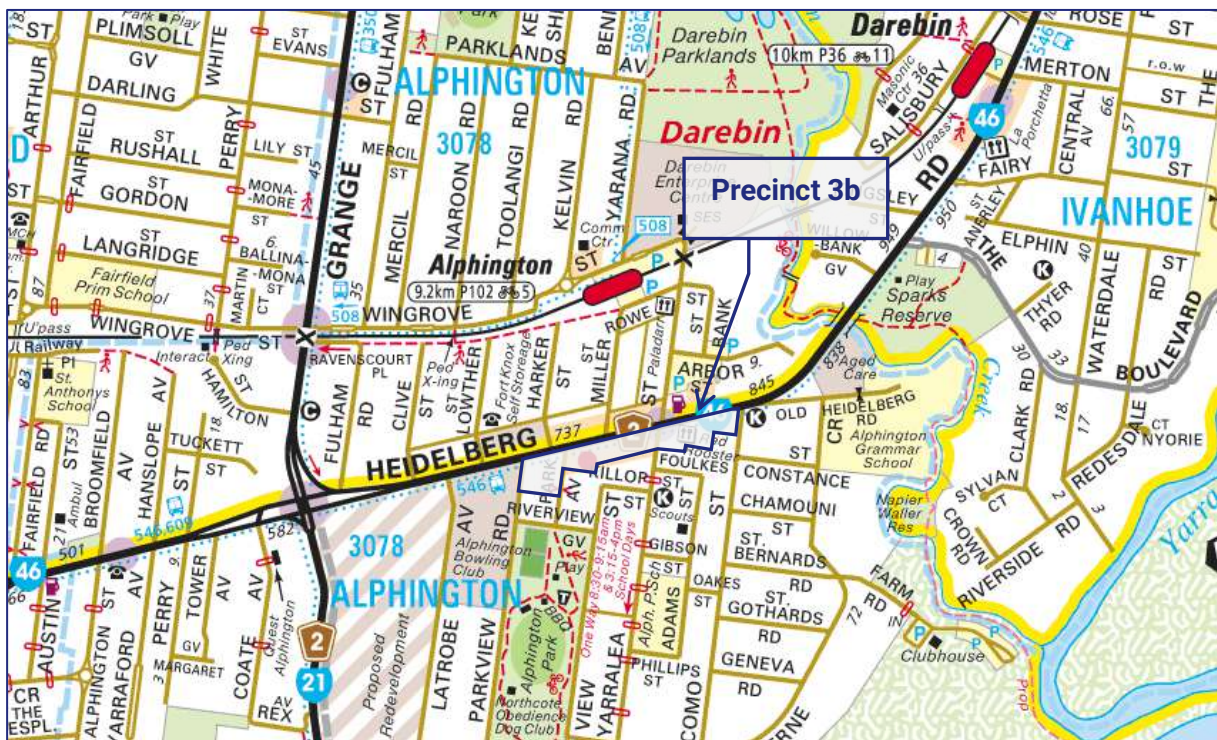
The level of development is potentially quite significant and given there is only one option for vehicle access, to Coate Avenue, this would need to be critically assessed.

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However, given the Coate Avenue/Heidelberg Road intersection is downstream from the Chandler Highway intersection, there are large gaps in the traffic which can be used by vehicles to enter (via right turn) or exit via left or staged right turn during the large breaks in the traffic.

The site is already occupied by a two-storey office development with associated carpark (94 spaces). Accordingly, it is likely that this development is already generating a moderate level of traffic. Any traffic surveys and analysis should ensure that the existing traffic generated by this site is taken into account during the assessment.

5.3.4. Precinct 3b



The level of traffic generated as a result of the level of development proposed for this precinct, of a mostly 5 storey height limit, is not expected to have a significant traffic impact to local roads. We do not expect that the level of traffic will increase to a detrimental level on any of the adjoining local roads.

The block bound by Parkview Road and Park Avenue, known as 700-718 Heidelberg Road has received a Planning Permit from VCAT (Aleks Nominees Pty Ltd v Yarra CC [2018] VCAT 1315) for an 8 storey mixed use building over 3 levels of basement parking. The building is to contain 2 retail tenancies, 105 dwellings and 153 car spaces, accessed via both Parkview Road and Park Avenue.

The traffic associated with the use of this land will effectively be split between two local roads. We do not believe that either of these roads will be greatly impacted by this development, or similar development in the event an amended permit was to be issued.

The block bound by Park Avenue and Yarralea Street includes a significant number of properties which will be required to either continue to take vehicle access directly to Heidelberg Road (due to no alternative), or not provide car parking on the site. The remaining properties accessing the local road network directly do not have the development potential to cause a detrimental impact to Park Avenue or Yarralea Street. We note that Yarralea Street is controlled by traffic signals at its intersection with Heidelberg Road.

Property No's 774 and 782 will each have vehicle access to Yarralea Street. Given that the intersection of Yarralea Street is controlled by a set of traffic signals we do not believe there would be a detrimental impact to Yarralea Street as vehicle access to Heidelberg Road is controlled already. However, for any redevelopment of this site utilising Yarralea Street for access, the application material would need to include detailed traffic analysis, including Sidra analysis of the intersection to ensure that the capacity of the intersection is not exceeded.

We do not expect any further increase to traffic along Como Street as the only site which would require vehicle access already provides what is described under the design strategy as 'Existing medium-density, mid-rise housing'. This is a relatively new development and is highly unlikely to be redeveloped further in the short/medium term.

5.3.5. Summary

Overall, we are satisfied that the traffic generated as a result of this fairly moderate level of development across each of the precincts will not have a detrimental impact to the operation of the local road network.

Given the level of development potential and number of properties which are served, the locations that may require intersection analysis to form part of any application material are:

- the Yarra Bend Road/Heidelberg Road intersection,
- the Coate Avenue/Heidelberg Road intersection, and
- the Yarralea Street/Heidelberg Road intersection.

5.4. Parking Impacts to Local Road Network

The following sets out the general approach to parking demands within each of the different precincts. Each of the precincts has different locational attributes which would be more or less conducive to allowing for car parking reductions.

Precinct 1 and 3b are located within the PPTN area and as such are more conducive to allowing for car parking reductions. Whereas, Precincts 2 and 3a are not, and may be less conducive.

Maps detailing the on-street car parking restrictions within the study area are provided at Appendix B.

The below recommendations assume that an improvement to the overall bicycle connectivity is improved generally in line with the recommendations set out within Section 7

5.4.1. Precinct 1

Whilst located within the PPTN area, the sites located within Precinct 1 are located 700m from the nearest railway station (Dennis Railway Station). The site is located within the PPTN due to its proximity to Westgarth Street, where Bus Routes 250 and 251 operate. These bus routes only operate at 20 minute frequencies during the peak periods and as such does not offer a high level of service.

Accordingly, whilst located within the PPTN, we do not believe there is much scope for properties within this area to receive a car parking reduction for long term parking (staff or residents).

Accordingly, consideration can be given to potential car parking reductions in this precinct. However, an application would need to include a robust assessment of the likely car parking demand so as to the proposed use.

For general retail uses, such as shops, food and drink or restaurants, a car parking reduction could be granted for the customer components (i.e. short term users), whereas the staff component should generally be provided.

Under existing conditions, the on-street carparking along Heidelberg Road in this precinct is unrestricted outside of Clearway Times. During our site inspection these car spaces were in high demand and were likely associated with long-term car parking for staff of the commercial/industrial uses of the precinct.

The residential uses permitted within this precinct will generate visitor car parking demands. These demands do not need to be provided on the sites as they are located within the PPTN, where there is no requirement for visitor parking. Accordingly, the most proximate area for visitor parking is along the site's frontage to Heidelberg Road. If this is occupied by long-term car parking which is not turning over during the day, there will be limited capacity for visitor or customer parking to occur and visitors will overflow into the car parking for Fairfield Park and Yarra Bend Park.

This should be avoided, and as such Council should explore restricting car parking along Heidelberg Road to short-term parking.

5.4.2. Precinct 2

Precinct 2 is located within a commercial precinct which comprises a mixture of retail and restricted retail uses. Accordingly, any redevelopment of this precinct will include an intensification of the commercial uses on the site.

The general approach to finer grain retail uses on narrower sites would be to ensure that staff parking is provided on the site, with all customer car parking accommodated on-street within the area consistent with a centre based approach to parking demands.

However, for larger sites and for bulky goods (i.e. restricted retail uses) an on-site customer car parking provision may be beneficial for a specific use which includes picking up of goods. Short term loading spaces could be included within the design of a specific site.

Generally speaking, customer car parking demands can be accommodated off-site in the nearby area.

For sites which do not have car parking along their site's frontage, it may be appropriate to require some level of on-site visitor parking to be provided. The provision of visitor car parking should be considered for property numbers 358 to 376, on the corner of Panther Place and Heidelberg Road, where the only street frontage with on-street parking is Park Crescent, a residentially sensitive area.

The remainder of the block between Panther Place and Station Street may also need to provide some level of visitor parking, as there is limited availability of on-street parking available to these sites. The majority of these sites are quite deep with limited street frontage and therefore limited car parking availability directly adjacent. Accordingly, the overflow generated by their development potential is likely to exceed the on-street parking adjacent to the land.

The remainder of the precinct has access to a higher number of street frontages and therefore on-street car parking. Accordingly, these sites could potentially achieve a higher level of car parking reduction for visitor and customer car parking.

The block between Station Street and Arthur Street includes a number of fine-grained sites and as such may not be conducive to providing on site car parking, particularly for visitors or customers. In some cases, commercial uses could also be provided without or with very limited car parking.

On narrow sites that are difficult to provide parking on, the inability to provide car parking should not be a limiting factor in the development potential of a site. Rather, the provision of alternative travel modes, such as bicycles, motorcycles and scooter parking should be explored as an alternative to car parking.

For example, the space that is required for a single car space, can accommodate up to 8 bicycle spaces (potentially more if you take into account the space required for manoeuvrability of a car), as detailed below. Accordingly, for some developments this may be more beneficial to providing a very small number of car spaces.

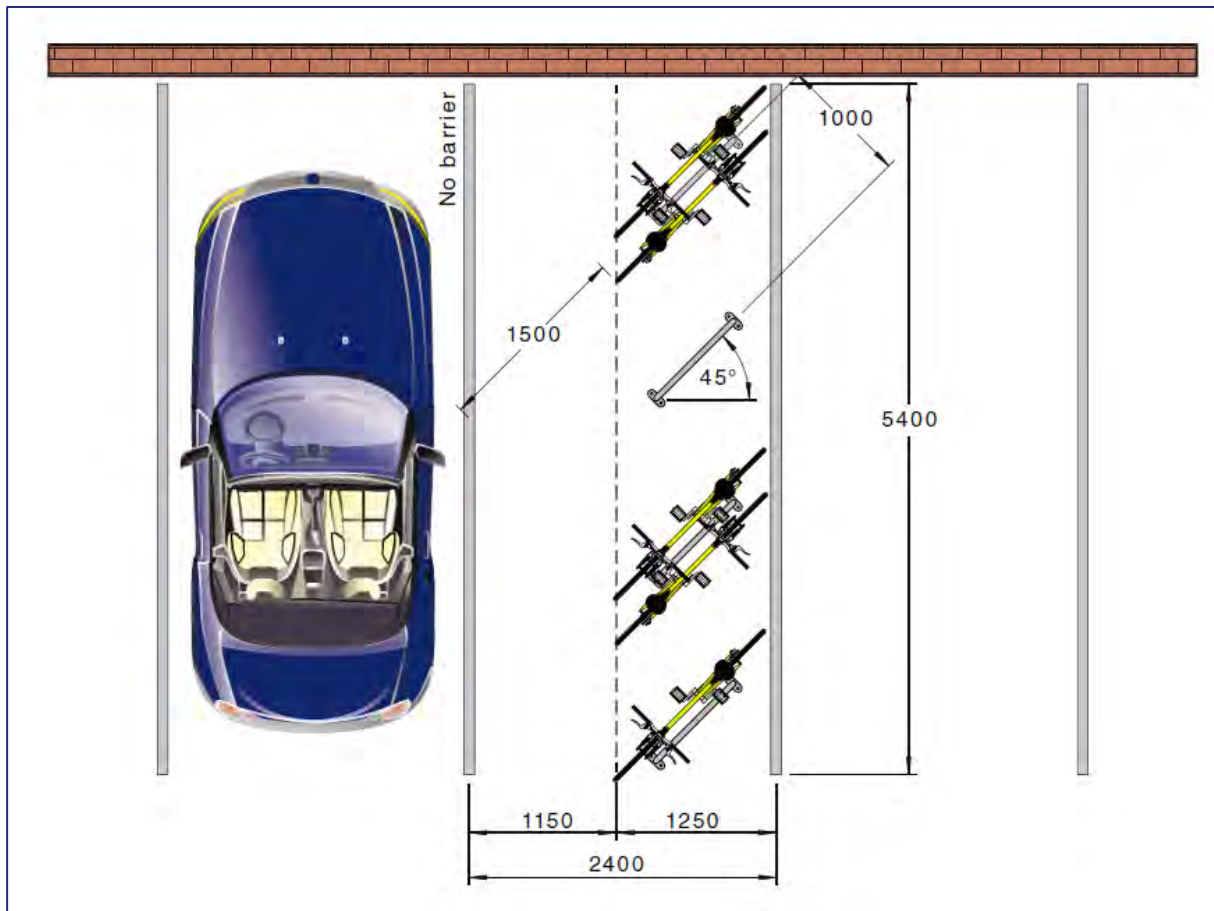


Figure 21: Conversion of Car Space to Bicycle Spaces (AS2890.3-2015)

5.4.3. Precinct 3a

The site located within Precinct 3a has only one street frontage that can accommodate on-street parking, Coate Avenue. The remaining street frontages are Heidelberg Road and Chandler Highway which do not provide for on-street car parking in this location.

The adjoining land uses to the south of the site are residential in nature. Accordingly, any overflow car parking demand should be confined to along the site's frontage to Coate Avenue, such that the car parking impact is limited.

Accordingly, the car parking demand generated by the site should be accommodated on the site, with a short-term overflow which is limited to the number of car spaces which can be accommodated along the site's frontage.

5.4.4. Precinct 3b

Precinct 3b is located within a commercial precinct which comprises a mixture of retail and restricted retail uses. Accordingly, any redevelopment of this precinct will include a retail presence on the ground floor and potentially residential uses above.

The general approach to finer grain retail uses on narrower sites would be to ensure that all staff parking is provided on the site, with all customer car parking accommodated on-street within the area consistent with a centre based approach to parking demands.

However, for larger sites and for bulky goods (i.e. restricted retail uses) an on-site customer car parking provision may be beneficial for a specific use which includes picking up of goods. Short term loading spaces could be included within the design of a specific site.

Visitor demands generally peak at opposing times to retail uses, with retail customers typically peaking during the day, whilst residential visitor parking typically peaks during the evening and on weekends. Accordingly, the sharing of the on-street car parking resources is appropriate in this case and can be accommodated within the area, without significant encroachment into residentially sensitive areas.

The precinct is located within the PPTN Area and as such, no residential visitor car parking is required on any of these sites.

The block between Park Avenue and Yarralea Street includes a number of fine-grained sites and as such may not be conducive to providing on site car parking. In some cases, depending on the site constraints and limitations, dwellings and retail uses could also be provided without or with very limited car parking.

On narrow sites that are difficult to provide parking on, the inability to provide car parking should not be a limiting factor in the development potential of a site. Rather, the provision of alternative travel modes, such as bicycles, motorcycles and scooter parking should be explored as an alternative to car parking.

The local streets to the south of Precinct 3b includes significant 'unrestricted' car parking. Accordingly, once development increases along the corridor, overflow car parking may encroach into these residentially sensitive areas.

Council could consider introducing short-term car parking for these residentially sensitive areas to protect existing residents from significant encroachment from new development, where unrestricted car parking prevails.

5.4.5. Summary

Overall, we are satisfied that the parking impacts of the redeveloped sites will not have a detrimental impact to the parking availability of the area.

Generally speaking all long-term car parking demand should be provided on the site, with short-term car parking accommodated on-street. Short-term car parking may be required to be provided on the site if:

- the site does not have access to on-street car parking adjacent to the site,
- any overflow car parking would encroach significantly into residentially sensitive areas, or
- access for customers to collect goods for larger retail uses.

We note that visitor parking cannot be requested to be provided on sites within the PPTN Area.

The residential areas which abut the commercial areas to the south generally have car parking restrictions which protect these areas from parking associated with the commercial

precincts. i.e. short/medium term and permit zones. Accordingly, these parking restrictions will continue to protect the residential areas when development increases along the corridor.

Those on-street car parking areas which are not currently protected by timed or 'Permit Zone' restrictions should be contemplated by Council.

A reduction of long-term car parking for staff or residents could be considered for fine grained sites where vehicle access would be required to Heidelberg Road or if the provision would be low due to the width of the site or other access constraints.

This would be appropriate in this area, as the area is served by fixed rail and it is not necessary that each individual development achieve an exact mix of parking rates as some will be able to readily provide parking and some will not.

6. Access and Movement Plans

A map of existing vehicle access points to properties within the study area is included at Appendix E of this report.

The following section sets out our recommended Access and Movement Plans for all properties within the study area. The detailed Access and Movement Plans are attached at Appendix F.

6.1. Access Management Principles

VicRoads generally adopts the AustRoads Guide to Traffic Management with regard to its access management principles for managing the arterial road network. In particular, the AustRoads Guide to Traffic Management Part 5: Road Management sets out the following relevant guiding principles:

- *Transport and other functions served by roads, the needs of abutting land use, along with wider government strategic objectives, all influence how roads are managed. The functional classification of a road relates to its role within the road network. There are two main functions of road networks and roads:*
 - *'mobility' that is concerned with the movement of through traffic and focussed on the efficient movement of people and freight, and*
 - *'access' that relates to the ease with which traffic from land abutting roads can enter or leave the road.*
- *Recent developments in policy and strategic planning initiatives are aimed at giving greater recognition to walking activity in road and transport planning. This has arisen from policy settings in the transport and health sectors recognising the need to move towards more sustainable forms of transport (by foot, bicycle or public transport) and towards healthier activity (walking, cycling) by the community generally (AustRoads 2013a).*
- *This has led to recognition of the need for planning and providing a road network which caters for the potential increase in active travel such as walking and cycling. This is a fundamental factor for consideration in striving for balance between the mobility and access functions of roads in the network.*

Importantly, in the context of the Heidelberg Road corridor, as an inner area, the move to sustainable forms of transport (foot, bicycle or public transport) has more than just health benefits. It is an integral component to the success of the implementation of the Heidelberg Road corridor DDO, having regard to the capacity constraints of Heidelberg Road to accommodate additional vehicle movements.

Accordingly, it is imperative that the planning for an increase in the density of development is accompanied by an access management strategy that recognises the importance of these sustainable transport modes, and also plans for the inevitable increase in pedestrians and cyclists as well as improvements to the public transport network along this important corridor.

The AustRoads Guide to Traffic Management Part 5: Road Management states the following in relation to the role of different road types:

- *The primary function or balance of different functions may be reflected in the classification of a road. In its purest form, road classification may consist of two basic road types which have fundamentally different traffic and environmental goals:*
 - *arterial roads, the main function of which is to provide for the safe and efficient movement of people and freight, and*
 - *local roads, which provide direct access to abutting land uses and which contribute to the overall functioning of areas bounded by arterial roads or other barriers. The basic function of a local road is to provide a good environment in which to live or conduct a business and to enable vehicular access to abutting land.*
- *The need for access planning and management arises because vehicle movements generated by abutting properties can potentially create interruptions in the traffic flow along a road. On many roads, these interruptions are of little or no concern. However, on arterial roads carrying high traffic volumes or fast moving traffic, where traffic efficiency is of greater importance, these interruptions can create a greater risk of crashes, inefficiencies and other costs to the community. An effective access management strategy for a road or site contributes to the best outcome for the community by protecting the level of traffic service on important through traffic routes while providing road users with safe and appropriate access to adjacent land.*

Heidelberg Road is an arterial road (Road Zone Category 1) and accordingly it has an important role in the broader arterial road network context to provide for through traffic. Heidelberg Road is also located on the Principle Bicycle Network (PBN).

The role of Heidelberg Road creates an environment which is not conducive to providing direct vehicular access to properties which could create interruptions in the flow of both vehicular and pedestrian traffic along Heidelberg Road.

Accordingly, taking into account Heidelberg Road's primary purpose, and noting that within the study area the majority of properties have alternative access potential (generally via local roads and some laneways/carriageway easements), there should be strong policy support within any Planning Scheme amendment (such as the DDO) to guide future access to development to be via the lower order road network.

Safety

Part 13 of the AustRoads Guide to Traffic Management addresses Road Environment Safety, as follows:

- *Managing safety in the road environment means managing the risk that injury will occur, whether it arises from the behaviour of road users, the performance of vehicles or the characteristics of the road environment. Making roads safer means reducing the risk. This applies to all road users – vehicle drivers, riders, passengers, cyclists, and pedestrians.*
- *Safe operation of the road and traffic system is a fundamental goal for road designers and traffic engineers who have a prime responsibility for addressing the safety factors related directly to the road environment itself.*

Fundamental principles for managing safety in road design, traffic management and remedial treatment practice include:

- speed management,
- conflict management,
- hazard management, and
- road user information management.

In the context of managing vehicular access to Heidelberg Road, conflict management is the primary safety principle which can be influenced.

Notably, it is important to provide a continuous safe environment for pedestrians at-grade along the Heidelberg Road public realm, and this can be achieved by minimising private property access points.

Policy Support

Council's Strategic Transport Statement sets out the following hierarchy of transport modes which forms the basis for decision making and actions related to transport in the City:

1. Pedestrians (including wheelchairs and walking with prams)
2. Cyclists
3. Tram
4. Bus/train
5. Taxi users/car sharers
6. Freight vehicles
7. Motorcyclists
8. Multiple occupants local traffic
9. Single occupants local traffic
10. Multiple occupants through traffic
11. Single occupants through traffic

Council's transport modal hierarchy for decision making places pedestrians and cyclists in the top 2, and places vehicular traffic at the bottom.

This hierarchy recognises the importance of sustainable modes into the future, and supports the recommended access management strategy to utilise lower order roads for vehicle access wherever possible, with direct access to Heidelberg Road being a last resort (with consideration for “no parking provision” potentially being preferable for some individual sites).

6.2. Benefits of Limiting Vehicle Access to Heidelberg Road

The principle of limiting direct vehicle access to Heidelberg Road provides the following key benefits:

- It promotes a safe and friendly pedestrian walking environment, by reducing breaks in the footpath, reducing pedestrian-vehicle conflict points and increasing the amount of active street frontage along Heidelberg Road. It also eliminates instances of vehicles blocking the footpath.
- It limits vehicle access to Heidelberg Road to public road intersections, where Council and VicRoads have a greater degree of control in the implementation of traffic management measures. This improves the efficiency and safety of the road network for all users.
- The reduced number of intersections allows the concentration of effort of traffic management measures and safety improvements at a limited number of locations.

However, the benefits of limiting vehicle access to Heidelberg Road need to be tempered against other competing demands, including:

- Some sites do not have alternative access options and have existing access points to Heidelberg Road. It is not possible to deny access to sites that already have direct access to Heidelberg Road and do not have viable alternatives. However, upon redevelopment these accesses can include new controls to limit their impact, in particular left-in/left-out restrictions. A left-in/left-out restrictions results in the smallest impact on the arterial road network from an efficiency and safety perspective. Noting that most sites (except in Precinct 3b) are opposite a central median separating east and west-bound movements and will need to be left-in/left-out regardless.
- For some land uses (such as supermarkets), convenient and direct access to the arterial road network is important for the viability of the use and to minimise impact on local roads.

6.3. Access and Movement Plans

The detailed access and movement plans are attached at Appendix F.

To implement these plans will require some changes to the existing traffic management treatments and the configuration of public roads and laneways. This includes widening laneways to accommodate additional vehicle movements, specifically to accommodate simultaneous two-way traffic flow. This would involve developments abutting certain laneways being required to setback at ground level (although the building could extend over the laneway at upper levels).

Proposed access management plans attached at Appendix F show the recommended traffic management changes and instances where laneways should be widened, to accommodate a rear outcome for redevelopment sites fronting Heidelberg Road.

The plans classify road frontages into three categories:

- **Access prohibited (unless there is no reasonable alternative)** – this category is where vehicle access is not desirable or supported. This classification generally relates to Heidelberg Road frontages (or Chandler Highway, in Precinct 3a).
- **Access not preferred** – this category relates to locations where access is not preferred in favour of alternatives. However, these sites may not have reasonable alternative access locations (i.e. vehicle access to these sections may be the only option available to the site). Vehicle access solutions that do not involve access to these locations are encouraged. This may include consolidation of sites that allow vehicle access to a preferred location or the non-provision of car parking for smaller development sites.
- **Access preferred** – vehicle access to these frontages is supported and encouraged.

It is noted that there are a number of areas, where access is not currently available via either a side (local) road or a rear laneway or are otherwise constrained, as follows:

6.3.1. Precinct 2

Location 1: corner of Heidelberg road and Panther Place (property numbers 358 to 376)

Location 2: south-west corner of Heidelberg road and Station Street (property numbers 416 to 438)

Location 3: south-east corner of Heidelberg road and Station Street (property numbers 440 to 452)

Location 4: block between Arthur Street and Austin Street (property numbers 468 to 484, currently occupied by Mercedes car dealership)

6.3.2. Precinct 3b

Location 5: block between Parkview Road and Park Avenue (property number 712)

Location 6: south-east corner of Park Avenue and Heidelberg Road (property numbers 720-734)

Location 7: property numbers 754 and 756

The following sets out our review of each of the abovementioned areas.

Location 1 - corner of Heidelberg road and Panther Place (property numbers 358 to 376)

Location 1 is shown in the figure below, indicated by the yellow line.



Figure 22: Aerial Photo – Location 1

Source: Nearmap

Property number 358 has three street frontages with a wide frontage to Panther Place, and two narrow frontages to Heidelberg Road and Park Crescent. This site currently has 3 vehicle access points, including one to Heidelberg Road and two to Panther Place.

The site is also noted as “Sensitive redevelopment of existing & potential heritage buildings” as set out within the Urban design strategy. Accordingly, any redevelopment of the site needs to also take into consideration the heritage aspects of the building and associated constraints.

The following discussion is undertaken purely from a traffic engineering and access planning perspective.

Vehicle access should not be permitted to Heidelberg Road as the intention of the DDO is to limit the number of vehicle access points to Heidelberg Road. The question then becomes where is the best location for vehicle access along Panther Place or Park Crescent. The options for vehicle access are as follows:

1. The northern most existing access location to Panther Place.
2. The existing approximate mid-block crossover to Panther Place.
3. Creating a new crossover at the eastern boundary of the site to Park Crescent.
4. Consolidating the site with the adjacent site at No. 364 and creating a new crossover at the site’s consolidated eastern boundary.

Traffic and Vehicle Access Assessment

Heidelberg Road, Fairfield/Alphington

Table 9: Review of Access Options – Location 1

Option	Advantages	Disadvantages/Challenges	Recommendation
1. The northern most existing access location to Panther Place	<ul style="list-style-type: none"> No impact to Heidelberg Road frontage. 	<ul style="list-style-type: none"> Unable to turn right out of the site due to the location of infrastructure associated with the traffic signals Limited sight distance to the Heidelberg Road intersection Difficult location to provide access whilst maintaining active street frontage to Heidelberg Road 	<p>Discard.</p> <p>The safety impacts associated with the limited sight distance is not acceptable.</p>
2. The existing approximate mid-block crossover to Panther Place.	<ul style="list-style-type: none"> No impact to Heidelberg Road frontage. Location with the best sight distance for this site. Facilitates safest vehicle access to this site 	<ul style="list-style-type: none"> Difficulty in turning right out of the site, due to likely queues forming across the access from Heidelberg Road. Keep Clear line marking could be provided to ensure that egress from the site can be provided Otherwise, may be required to be left-out only. Difficult location to maintain street presence to Heidelberg Road 	<p>Consider.</p> <p>May be problematic for building design reasons.</p>
3. New crossover at the eastern boundary of the site to Park Crescent	<ul style="list-style-type: none"> No impact to Heidelberg Road frontage. Maintains street presence at Heidelberg Road 	<ul style="list-style-type: none"> Limited sight distance to the bend at Panther Place/Park Crescent Will require removal of vegetation along verge on Park Crescent. 	<p>Discard.</p> <p>The safety impacts associated with the limited sight distance is not acceptable.</p>

Traffic and Vehicle Access Assessment

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Option	Advantages	Disadvantages/Challenges	Recommendation
<p>4. Consolidating with the adjacent site and new crossover at eastern boundary.</p>	<ul style="list-style-type: none"> No impact to Heidelberg Road frontage. Maintains street presence at Heidelberg Road Acceptable sight distance to the bend to the west 	<ul style="list-style-type: none"> Will require removal of vegetation along verge on Park Crescent. Complexity with regard to agreements between the two sites with regard to consolidation. 	<p>Preferred option.</p> <p>But only if Council deems it appropriate to consider approaching land owners regarding consolidation.</p>

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Heidelberg Road, Fairfield/Alphington

Property No's 364 and 376 should both create vehicle access to Park Crescent, in preference to Heidelberg Road.

The vehicle access points to No. 364 is too close to the Heidelberg Road/Panther Place/Westgarth Street intersection and as such should be removed and reinstated as kerb and channel. Entering traffic in this location would reduce the efficiency of through vehicle movements (including bicycles) through the intersection.

Similarly, access to and from the site would be problematic, as when accessing the site from the west (i.e. from the city) would require a vehicle to travel past the site and perform a U-Turn at the Station Street/Heidelberg Road intersection, impacting this intersection.

If vehicle access to this site is provided to Park Crescent, the issues regarding access from the west would be removed as access from the west could be facilitated via the right turn movement at Panther Place.

The Design Strategy for Precinct 2 indicates that the existing access to No. 376 should be retained for a redeveloped site. Our preferred access to this site is to Park Crescent. Whilst there is an existing two-way accessway to Heidelberg Road in this location, the crossover is of a substandard design and includes a street pole in the centre of the access, separating entry and exit movements. This is problematic in terms of clearances to the pole, particularly for larger vehicle access. The figure below shows this arrangement.



Figure 23: Existing Vehicle Access to No. 376

Maintaining access to Heidelberg Road for No. 376 creates the same issues with regard to vehicle access as described for No. 364

The preferred access location to Park Crescent is the existing crossover to this site, which is located at the eastern boundary.

This access location will increase traffic along Park Crescent. However, given this area is indicated as suitable for moderate redevelopment intensification for a height limit of 5 storeys, the associated traffic impact is expected to be low. We also expect traffic to be distributed to the east and west depending on direction of travel.

Vehicle access to Heidelberg Road for loading requirements may be maintained if cannot be facilitated to Park Crescent.

Traffic and Vehicle Access Assessment

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Location 2 - south-west corner of Heidelberg road and Station Street (property numbers 416 to 438)

Location 2 is shown in the figure below, indicated by the yellow line, with the indicative location of the ROW shown in green.



Figure 24: Aerial Photo – Location 2

Property No's 416 to 432 each have frontages to the ROW which extends in an L-shape from Heidelberg Road to the south-east corner of No. 416.

Each of these sites currently have vehicle access to the ROW, either by property access or by car parking within a setback arrangement.

Our recommendation is that vehicle access to the ROW continues, post redevelopment of each of these sites.

The ROW is currently provided at a width of 5.0m as it connects to Heidelberg Road. Under the ultimate arrangement of the ROW, it should be at least 6.1m wide to accommodate two-way movements.

The logical way to ensure that this occurs is to require sites No. 420 and No. 432 to set back their buildings equally to ensure that the necessary widening occurs and is equally distributed between the two sites. The necessary widening is detailed in the figure below.

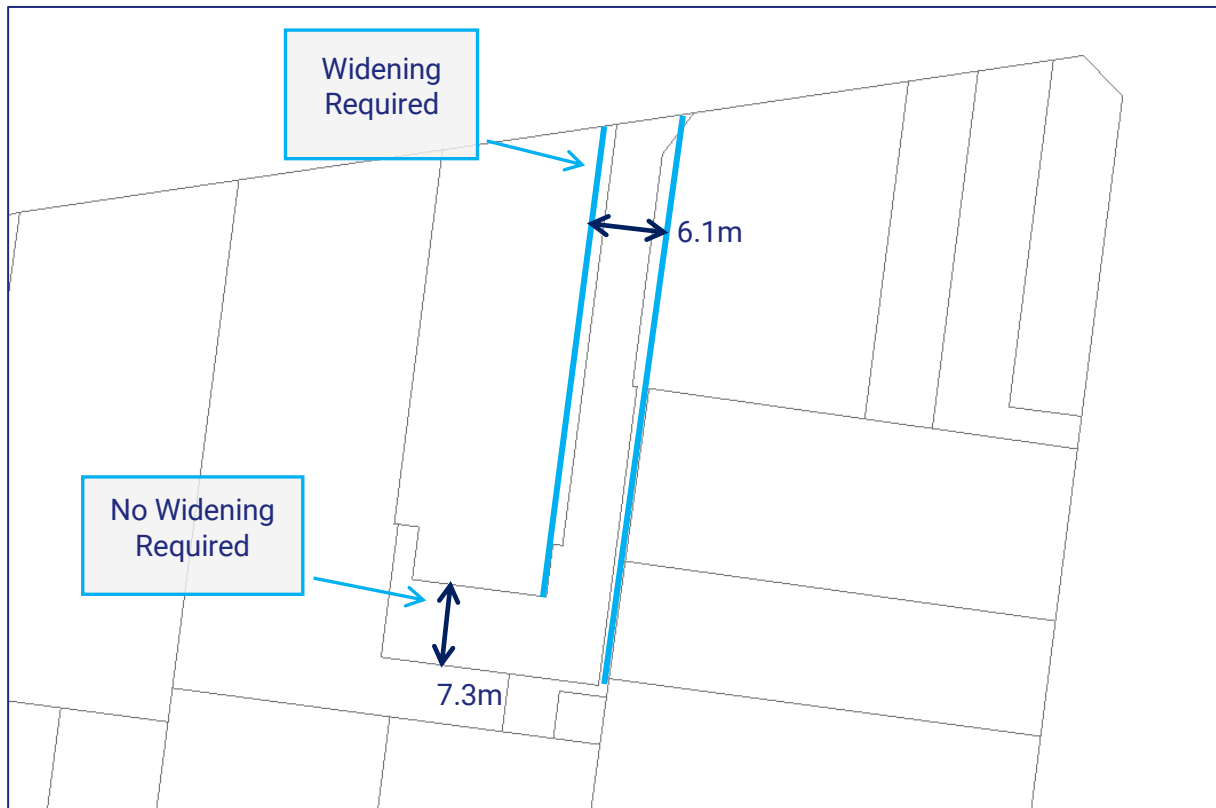


Figure 25: Require Lane Widening

We note that the current width of the lane is sufficient to provide for constrained two-way movements. Accordingly, we do not believe there will be an issue arising with regard to the order with which the site develop. For example, if No. 416 develops before the other two, the laneway will practically operate as allowing for two-way movements, such that the environmental capacity of the laneway will not be exceeded.

We understand that through discussions with Council that Property No 434-438 are likely to be developed as one consolidated site due to the ownership of the land. Accordingly, the access to the site should be provided to Station Street as far south as possible.

Traffic and Vehicle Access Assessment

Heidelberg Road, Fairfield/Alphington

Location 3 - south-east corner of Heidelberg road and Station Street (property numbers 440 to 452)

Location 3 is shown in the figure below, indicated by the yellow line, with the indicative location of the carriageway easement shown in green.



Figure 26: Aerial Photo – Location 3

We understand that a carriageway easement is provided in favour of each of the properties detailed below. Under existing conditions each of these properties have vehicle access to the carriageway easement either via property access or by car parking within a setback arrangement.

We recommend that each of these properties, upon redevelopment, continue to rely on this carriageway easement for vehicle access.

Given the level of development that is identified within this precinct, we do not expect these sites to generate the level of traffic that would necessitate the need for widening the easement to accommodate two-way movements (i.e. over 30 movements per peak hour), particularly given the size of No's 448, 450 and 452.

However, if No. 440 did develop beyond the 5 storey height limit and generate significant traffic that would cause the easement to exceed its environmental capacity, this may necessitate the need to, at a minimum, create a passing opportunity along this property's

frontage. As the largest site, with access to the easement, No. 440 is the driver as to whether widening of the laneway were to be required.

It may not be possible to provide for a passing area at the entrance to the carriageway easement due to the presence of a significant street tree which may need to be preserved (others to advise on).

However, a passing bay along the frontage to No. 440 would suffice if this situation arose. It is highly unlikely that the development of No's 448, 450 and 452 would be the driving force behind widening the easement.

As discussed at Section 5.4, given their width, No's 448, 450 and 452 are ideally dimensioned for the provision of low or no parking to be provided. Accordingly, if this were the case, no traffic impacts would result.

Vehicle access to Heidelberg Road should be prohibited for each of the abovementioned sites.

Location 4 - block between Arthur Street and Austin Street (property numbers 468 to 484, currently occupied by Mercedes car dealership)

We note that the Design Strategy details vehicle access to No's 468 to 484 is to Arthur Street, but not to Austin Street.

From an accessibility perspective, vehicle access to Austin Street is a more preferable option. The Austin Street/Heidelberg Road intersection allows for all movements, as opposed to the Arthur Street/Heidelberg Road intersection, which is restricted to left-in/left-out movements.

Whilst either location would be acceptable for access to No's 468 to 484, from an access perspective Austin Street would be preferable.

Location 5 - block between Parkview Road and Park Avenue (property number 700-718)

We note that the Design Strategy does not detail an access location to No. 700-718. This site has three street frontages, including Heidelberg Road, Park Avenue and Parkview Road.

Heidelberg Road would not be acceptable from a vehicle access perspective, given.

The former Paper Mills site development site lies directly to the west of this land parcel, and has vehicle access to Parkview Road. The Paper Mills development site is significant and will accordingly generate significant traffic to the network

Accordingly, it would be preferential if vehicle access to No. 712 were to occur to Park Avenue, rather than Parkview Road in order to distribute traffic throughout the network rather than concentrate it to Parkview Road.

However, both local streets would be appropriate for vehicle access provided the necessary capacity analysis was completed for the Parkview Road.

We note that this site received a Planning Permit from VCAT (Aleks Nominees Pty Ltd v Yarra CC [2018] VCAT 1315) for an 8 storey mixed use building over 3 levels of basement parking. The building is to contain 2 retail tenancies, 105 dwellings and 153 car spaces, accessed via both Parkview Road and Park Avenue. This is a satisfactory arrangement and is likely to distribute the traffic appropriately.

Traffic and Vehicle Access Assessment

Location 6 - south-east corner of Park Avenue and Heidelberg Road (property numbers 720-734)

Location 6 is shown in the figure below, indicated by the yellow line, with the ROW location shown in green.



Figure 27: Aerial Photo – Location 6

Properties 720 to 732 each have vehicle access to the laneway which extends to Park Avenue under exiting conditions. In addition to these properties, the carpark (accommodating approximately 23 car spaces) located on the south side of the laneway associated with the office on the north side also has vehicle access to the laneway.

We recommend that each of these properties, upon redevelopment, continue to rely on this laneway for vehicle access.

Assuming the continuing use of the carpark (or redevelopment of the carpark maintaining vehicle access to the ROW) and taking into account the development potential of the sites on the north side of the ROW, it is likely that the ROW will require a passing area to be provided at the entrance to the ROW.

This would increase the capacity such that vehicle access to each of the sites can be accommodated.

Traffic and Vehicle Access Assessment

Heidelberg Road, Fairfield/Alphington

The passing area should be designed such that a vehicle can enter the ROW, prop clear of the footpath and allow another vehicle to pass. It should be a minimum of 6.1m wide to accord with AS2890.1-2004 for two-lane, two-way access.

As discussed at Section 5.4, given their width and challenges associated with providing adequate access to Heidelberg Road, No's 728, 730, 732 and 734 are suited for consideration for a zero-parking solution. Accordingly, if this were the case, no traffic impacts would result.

Vehicle access to Heidelberg Road should be prohibited for each of the abovementioned sites.

Location 7 - property numbers 754 and 756

Location 7 is shown in the figure below, indicated by the yellow line.



Figure 28: Aerial Photo – Location 7

As discussed at Section 5.4, given their width and challenges associated with providing adequate access to Heidelberg Road, a Road Zone Category 1, No's 754 and 756 are ideally suited for the provision of no parking to be provided. Accordingly, if this were the case, no traffic impacts would result.

These sites are particularly difficult to provide access to, as an accessway would need to be at least 6.1m wide (assuming that over 10 car spaces are provided, being the threshold for a two-way accessway to a Road Zone).

Accordingly, the majority of their 12-14m frontages would be occupied by vehicle accessways, within close very close proximity, which would compromise the pedestrian experience/safety and other non-traffic engineering related considerations.

There is added complexity associated with No. 756 as it also identified as 'Sensitive redevelopment of existing & potential heritage buildings', as well as being restricted by an existing heritage overlay.

The option of providing a development with zero parking may be preferential to providing vehicle access to Heidelberg Road.

We also note that this site is located within 400m of Alphington Railway Station and is one of the most well located with respect to public transport in the study area.

7. Alternative Transport

7.1. Bicycle Infrastructure

As set out within Section 4.5.1, the current bicycle conditions throughout the study area vary at different points along Heidelberg Road and is discontinuous. Accordingly, this results in a confusing and potentially dangerous environment for cyclists, which discourages use of bicycles along Heidelberg Road.

The bike lanes through the corridor commonly share the kerb space with parked vehicles outside of Clearway times. Accordingly, outside of Clearway times, the bicycle lanes are not able to be used.

We note that during clearway times in some locations, the bicycle lanes are narrow, resulting in cyclists travelling quite close to adjacent traffic lanes.

The treatments at signalised intersections is inconsistent throughout the study area. This ranges between a high level at the Heidelberg Road/Chandler Highway intersection which includes kerbside bicycle lanes on the arrival and departure lanes, head-start areas and hook turn storage boxes, compared with a very poor level of infrastructure at a number of intersections.

In each of the precincts all redevelopment should provide for bicycle parking with provisions at least in line with Clause 52.34 of the Planning Scheme. If dwellings without car parking are provided, additional bicycle parking should be provided to ensure no dwelling is at a transport disadvantage.

The design of the bicycle parking facilities should be provided in accordance with the requirements of AS2890.3-2015, including the provision of 20% of spaces designed as a floor mounted space.

Any development within the study area should ensure that bicycles are logically placed with respect to the bicycle paths and bicycle lanes to ensure easy access to designated bicycle routes.

7.1.1. Precinct 1

The sites located within Precinct 1 are particularly well located with respect to bicycle infrastructure, with bicycle lanes located adjacent the site on Heidelberg Road (both directions) and off-road shared paths located to the south of the site throughout the parklands.

There are also good cycling connections from the site to Dennis Railway Station via Jeffrey Street and Victoria Street.

The design strategy for Precinct 1 demonstrates that the bicycle lanes will be maintained along Heidelberg Road.

Whilst the existing infrastructure is suitable, some improvements can be made in some areas. We recommend liaising with the relevant authorities, i.e. Department of Transport (PTV and VicRoads) and Darebin City Council in relation to considering the following, which is currently lacking:

- head-start areas for bicycles at the Heidelberg Road/Yarra Bend Road and Heidelberg Road/Jeffrey Street intersections, and
- additional bicycle parking for Dennis Railway Station to encourage bicycle access to the railway station.

7.1.2. Precinct 2

The bicycle lanes which extend along Heidelberg Road within Precinct 2 do not continue through the intersection with Station Street creating an unsafe arrangement. This is a deterrent for bicycle travel along this stretch of Heidelberg Road.

Bicycle lanes are provided along Westgarth Street and Station Street, providing for convenient bicycle access to Dennis Railway Station (via Westgarth Street and Victoria Street) and Fairfield Railway Station via (Station Street).

The existing bicycle infrastructure is lacking in this area. We recommend liaising with the relevant authorities, i.e. Department of Transport (PTV and VicRoads) and Darebin City Council in relation to the following:

- bicycle consideration at the Station Street/Heidelberg Road intersection to redesign the intersection to incorporate high quality bicycle infrastructure to encourage bicycle usage and increase safety at the intersection for cyclists, and
- additional bicycle parking for Fairfield Railway Station to encourage bicycle access to the railway station.

7.1.3. Precinct 3a

The recent upgrade to the intersection of the Heidelberg Road/Chandler Highway intersection has included significant provisions for bicycles including kerbside bicycle lanes on the arrival and departure lanes, head-start areas and hook turn storage boxes. Accordingly, the bicycle infrastructure in this precinct is good.

7.1.4. Precinct 3b

Bicycle lanes extend along Heidelberg Road within the western portion of Precinct 3b, but do not continue east of Miller Street and through the intersection with Yarralea Street and then do not continue to the east of this intersection. The non-provision of bicycle lanes in this area and restriction of bicycle lanes outside of clearway times by parked cars acts as a deterrent for bicycle travel along this stretch of Heidelberg Road.

Similarly, bicycle lanes are not present on Yarralea Street to assist access to Alphington Railway Station.

The existing bicycle infrastructure is lacking in this precinct. We recommend liaising with the relevant authorities, i.e. Department of Transport (PTV and VicRoads) and Darebin City Council in relation to considering the following, which is currently lacking:

- investigate the provision of bicycle lanes if the Public Acquisition Overlay is acted upon by the Department of Transport,
- bicycle consideration at the Yarralea Street/Heidelberg Road intersection to redesign the intersection to incorporate high quality bicycle infrastructure to encourage bicycle usage and increase safety at the intersection for cyclists, and
- additional bicycle parking for Alphington Railway Station to encourage bicycle access to the railway station.

7.1.5. Summary

Overall the bicycle infrastructure within the study area is lacking in most areas and should be improved. We recommend liaising with the relevant authorities, i.e. Department of Transport (PTV and VicRoads) and Darebin City Council in relation to considering the following, which is currently lacking:

- bicycle consideration at signalised intersections within the study area, intersection to redesign intersections to incorporate high quality bicycle infrastructure to encourage bicycle usage and increase safety at the intersection for cyclists. This could include continuous bicycle lanes through the intersection or head start areas for bicycles,
- additional bicycle parking at Dennis, Fairfield and Alphington Railway Stations to encourage bicycle access to the railway station, and
- creating a be a continuous safe bike lane which is not interrupted at intersections or by parked vehicles.

7.2. Public Transport

7.2.1. Fixed Rail

The study area has access to three railway stations within close proximity, including Dennis, Fairfield and Alphington Railway Stations. These railway stations are located on the Hurstbridge line and offer a high level of service to and from the City with services operating every 5-10 minutes during peak periods and every 20 minutes during off-peak times.

We do note however, that there appears to be a lack of bicycle parking at these railway stations which could create a detraction for potential users of the train services, given the lack of car parking at the stations.

As stated on Metro Train's website:

Parkiteer bike cages provide a convenient, undercover and secure place to park your bike, allowing fast access to the station to continue your journey by public transport.

Accordingly, providing a secure undercover space to park a bicycle would potentially attract additional users of fixed rail for the existing population, as well as any new residents to the area.

7.2.2. Bus Services

A detailed summary of the bus routes available within close proximity to the study area is provided at Section 4.4. The majority of the bus services provided within the study area do not provide a high level of service, with services ranging from every 20 minutes to every 60 minutes during the peak periods. Some services do not operate on the weekend, including along Heidelberg Road. i.e. on the weekend no bus services operate along Heidelberg Road.

Furthermore, there is a lack of bus shelters provided along Heidelberg Road which could be a detractor for potential users of the services.

We recommend liaising with PTV to increase the frequency of services for the existing bus routes within the area and potentially for services to operate during the weekend. Particularly Bus Route 546, which could create a convenient connection through the study area, enabling access to the retail and community services which will be offered as part of the redevelopment of the former Paper Mills site.

We also recommend liaising with the Department of Transport in relation to incorporating considerations for buses within any widening of Heidelberg Road, as well as additional bus shelters at regular intervals along Heidelberg Road.

7.2.3. Walking

The study area is somewhat walkable where only some services and destinations are within a convenient walking distance. We note that the main pedestrian path connecting the study area to local services in close proximity is via Heidelberg Road.

We also note that access to the nearest railway stations within the study area is not ideal, with Precinct 3a located at least 850m walking distance to the nearest railway station. Given its greater distance, this could discourage potential users of the train services.

Accordingly, providing a possible pedestrian link to Fairfield Station along the disused Outer Circle train line can improve pedestrian connectivity to Precinct 2 and 3a (both of which are located outside the PPTN area). This could potentially attract additional users of the train services and Fairfield Village shopping precinct for the existing population, as well as any new residents to the area. This is consistent with the recommendation put forward in the Alphington Paper Mill Development Plan.

8. Interim Design and Development Overlay – Working Draft

The following table sets out our discussion and recommendations for the transport engineering aspects of the interim Design and Development Overlay. This Extract is provided at Appendix G.

Table 10: Review of Design and Development Overlay

Current Text from Working Draft	Comments	Potential Re-Wording
Access, parking and loading areas requirements Car parking should be located within a basement or concealed from the main and side streets.	Agree	-
Providing recessed parking spaces at the ground floor of buildings and onsite parking spaces at the front of properties should be avoided, except for development east of Yarralea Street, Alphington.	There will be some instances where car parking may be recessed on the ground level from laneways or carriageway easements. We assume that the reasoning behind providing no parking within the front setbacks of buildings east of Yarralea Street is due to the PAO, which if enforced, would need to be removed. Accordingly, any car parking which is provided within the front setbacks in these areas should be of little consequence to the overall viability of the developments, and should include car spaces such as visitors or customers. Rather than resident or staff parking.	Providing recessed parking spaces at the ground floor of buildings and onsite parking spaces at the front of properties should be avoided, except for <u>development which includes vehicle access to laneways and for</u> development east of Yarralea Street, Alphington.
Pedestrian access to buildings should be achieved via Heidelberg Road or side streets and must be clearly visible, secure and have an identifiable sense of address. Residential and commercial entrances should be distinguishable from each other. Primary access from laneways should be avoided.	Agree.	-

Traffic and Vehicle Access Assessment

Heidelberg Road, Fairfield/Alphington

Current Text from Working Draft	Comments	Potential Re-Wording
The common pedestrian areas of new buildings should be designed with legible and convenient access, with hallway and lobby areas of a size that reflects the quantity of apartments serviced and which can be naturally lit and ventilated.	Agree.	-
Bicycle parking should be located and designed to be secure and conveniently accessible from the street and associated uses.	Agree.	-
Vehicle ingress and egress into development, including loading facilities and building servicing, should be designed to ensure a high quality pedestrian amenity and limit potential conflict between vehicle movements and pedestrian activity.	Agree.	-
Development must not provide new vehicular access from Heidelberg Road.	As detailed extensively at Section 6.3, there will be instances when vehicle access to Heidelberg Road is the only viable option for vehicle access. The word 'must' does not allow for any variation to allow for access to Heidelberg Road in the aforementioned situations.	Development must not provide new vehicular access from Heidelberg Road, <u>unless there is no reasonable alternative.</u> <u>In the event that access is taken to Heidelberg Road, only one crossover to a development site will be permitted to Heidelberg Road.</u>
Development with redundant vehicle access points to Heidelberg Road should reinstate the kerb, linemarked parking bays, and relocate any parking signs.	Agree.	-

Current Text from Working Draft	Comments	Potential Re-Wording
<p>Application Requirements The following application requirements apply to an application for a permit under Clause 43.02, in addition to those specified elsewhere in the scheme and must accompany an application, as appropriate, to the satisfaction of the responsible authority: A Traffic and Parking Assessment Report which includes an assessment of the cumulative impacts of traffic and parking in the Precinct including an assessment of the ongoing functionality of laneway/s, where applicable.</p>	<p>We agree with these requirements for an application. We also recommend that the cumulative impact should extend to any other developments which may not be located within the precinct, but would still impact upon the proposed development. As well as the functionality of laneway/s, the assessment should also assess the impact to any relevant intersections with Heidelberg Road.</p>	<p>A Traffic and Parking Assessment Report undertaken <u>by a suitable qualified traffic engineer</u> which includes an assessment of the cumulative impacts of traffic and parking in the <u>nearby area</u> including an assessment of the ongoing functionality of laneway/s, <u>any relevant intersection and local roads</u> where applicable.</p>
<p>Decision Guidelines The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specified in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority: The cumulative impact of development on traffic and parking in the nearby area, including on the functionality of laneway's.</p>	<p>See above.</p>	<p>The cumulative impact of development on traffic and parking in the nearby area, including on the functionality of laneways, <u>any relevant intersection and local roads</u>.</p>

9. Conclusions and Recommendations

Traffic Group has been engaged by Yarra City Council to undertake the following:

- a high level assessment of the future traffic conditions and performance of Heidelberg Road and local street network with the planned future development,
- access and movement plans for the study area showing the location and form of new, altered and retained access arrangements and laneways required to provide appropriate access to future developments,
- advice on the content of the future Design and Development Overlay to facilitate appropriate access and movement through new development, and
- undertake a review of the existing public transport, bicycle and pedestrian considerations and infrastructure within the study area.

The assessment of future traffic conditions is in the form of a case study regarding the changes to transport patterns over the last 10 years along Victoria Street, where significant development has been undertaken. This review found that there has been a significant mode shift in the area, resulting in reduction in traffic on the arterial road network. This is due to a combination of factors including changes in land use patterns, the changing demographics of Richmond and additional residents working and living locally.

The Heidelberg Road corridor is placed to encourage each of these outcomes. Heidelberg Road is readily accessible by public transport and alternative transport modes and can readily encourage local living through a greater diversity of land uses. As such, we are satisfied that the traffic impacts of new development on Heidelberg Road can be managed, with a large proportion of the new trips generated, being taken up by travel modes other than private car.

Access and Movement Plans have been prepared for all properties abutting Heidelberg Road to map out how vehicle access to new developments can be managed to reduce the impact of vehicle access directly to Heidelberg Road. Suitably designed and controlled vehicle access is a key component in achieving the objectives of maximising the efficiency of Heidelberg Road for vehicles, cyclists and providing a high quality pedestrian environment.

Additional studies may be required for some locations to determine whether laneway widening is required as a result of some development. Additional studies may also be required to determine any detrimental impacts on signalised intersections within the precincts.

Recommendations have been made in relation to liaising with other stakeholders in relation to upgrading bicycle infrastructure throughout the precincts, improving the connectivity to nearby railway stations and improving the level of service for buses within the area.

This report also undertakes a review of the transport related aspects of the interim Design and Development Overlay and outlines any alterations required in this regard.



Appendix A

Road Network

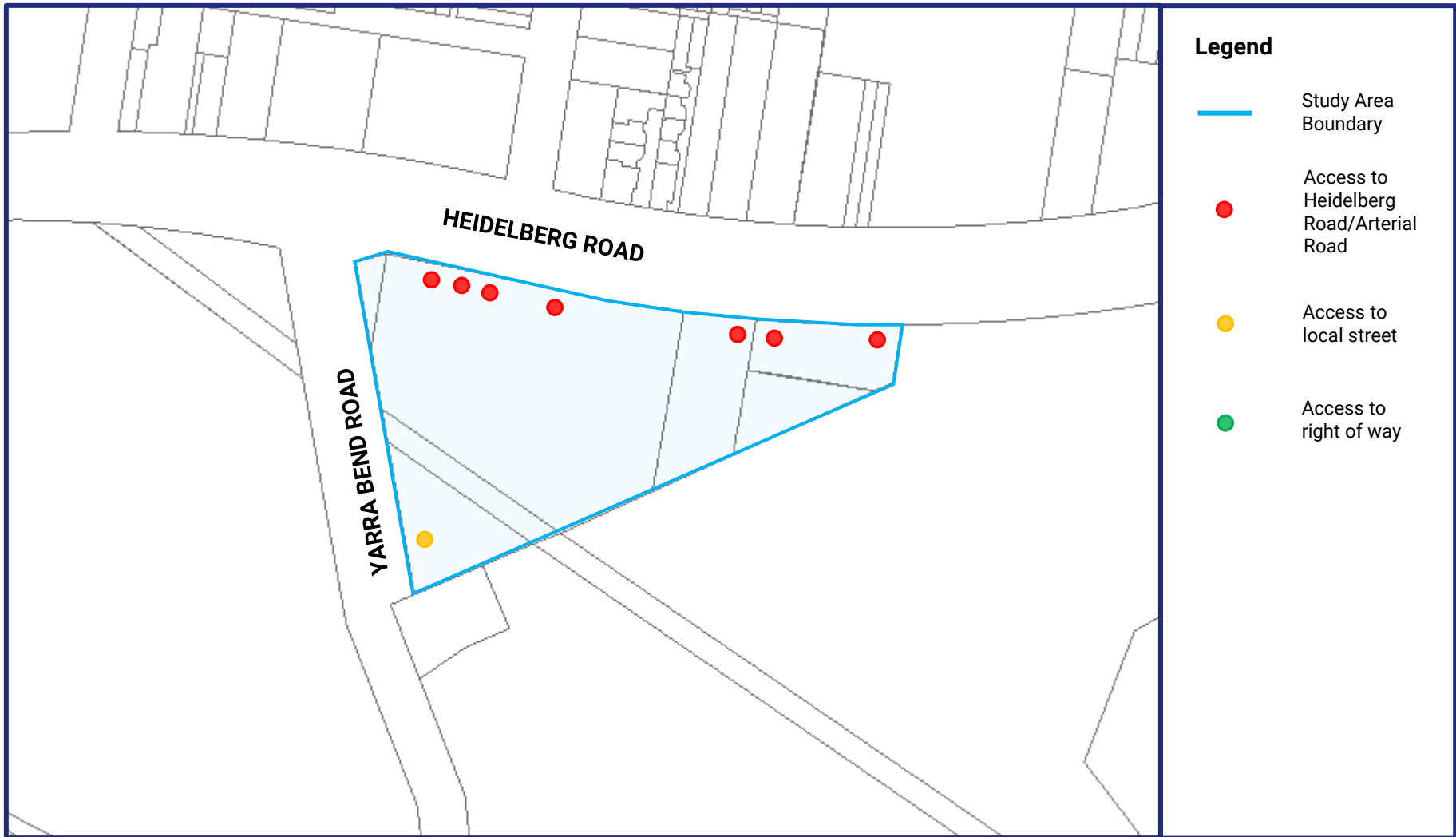


Figure E1: Precinct 1



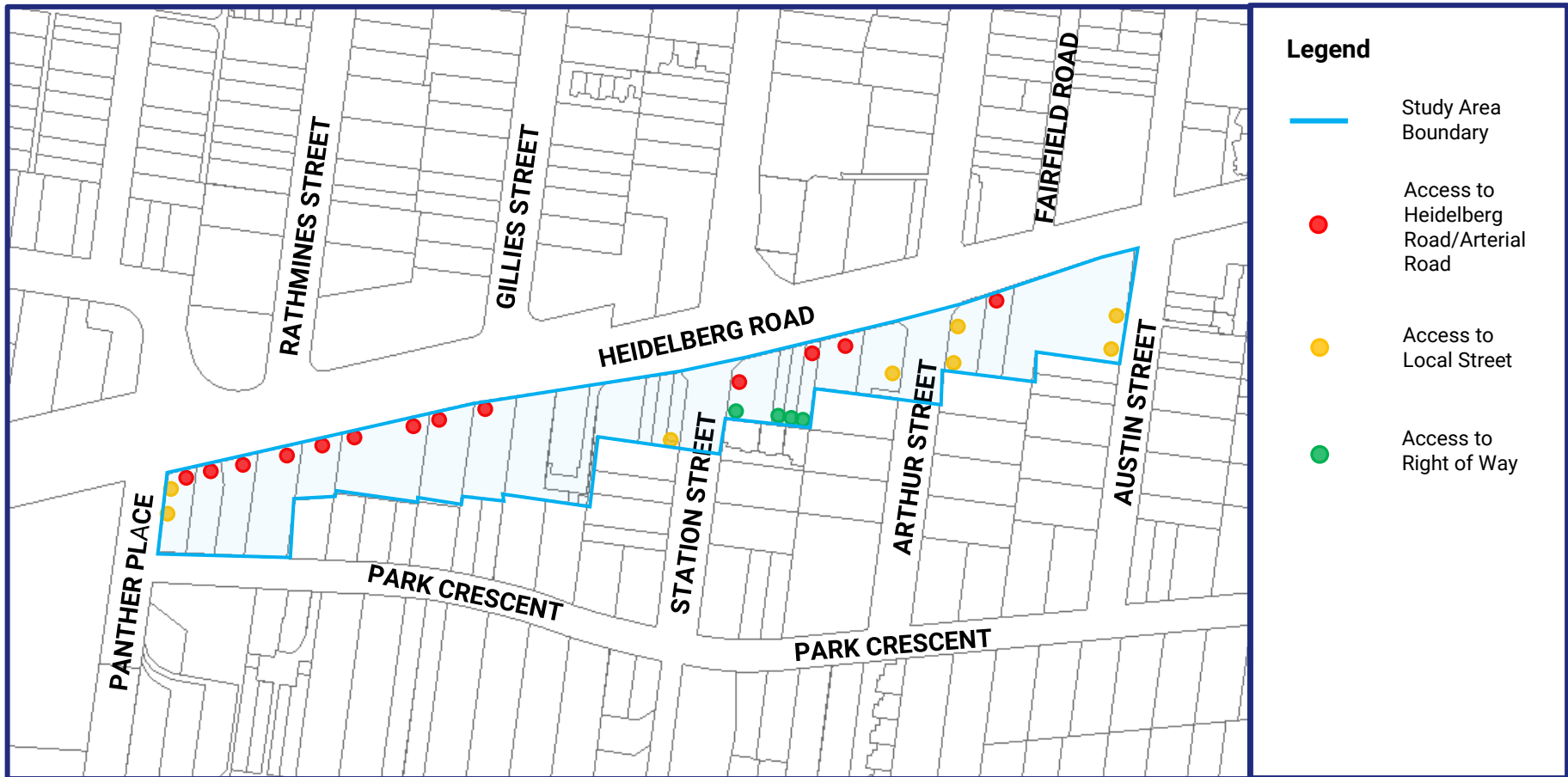


Figure E2: Precinct 2



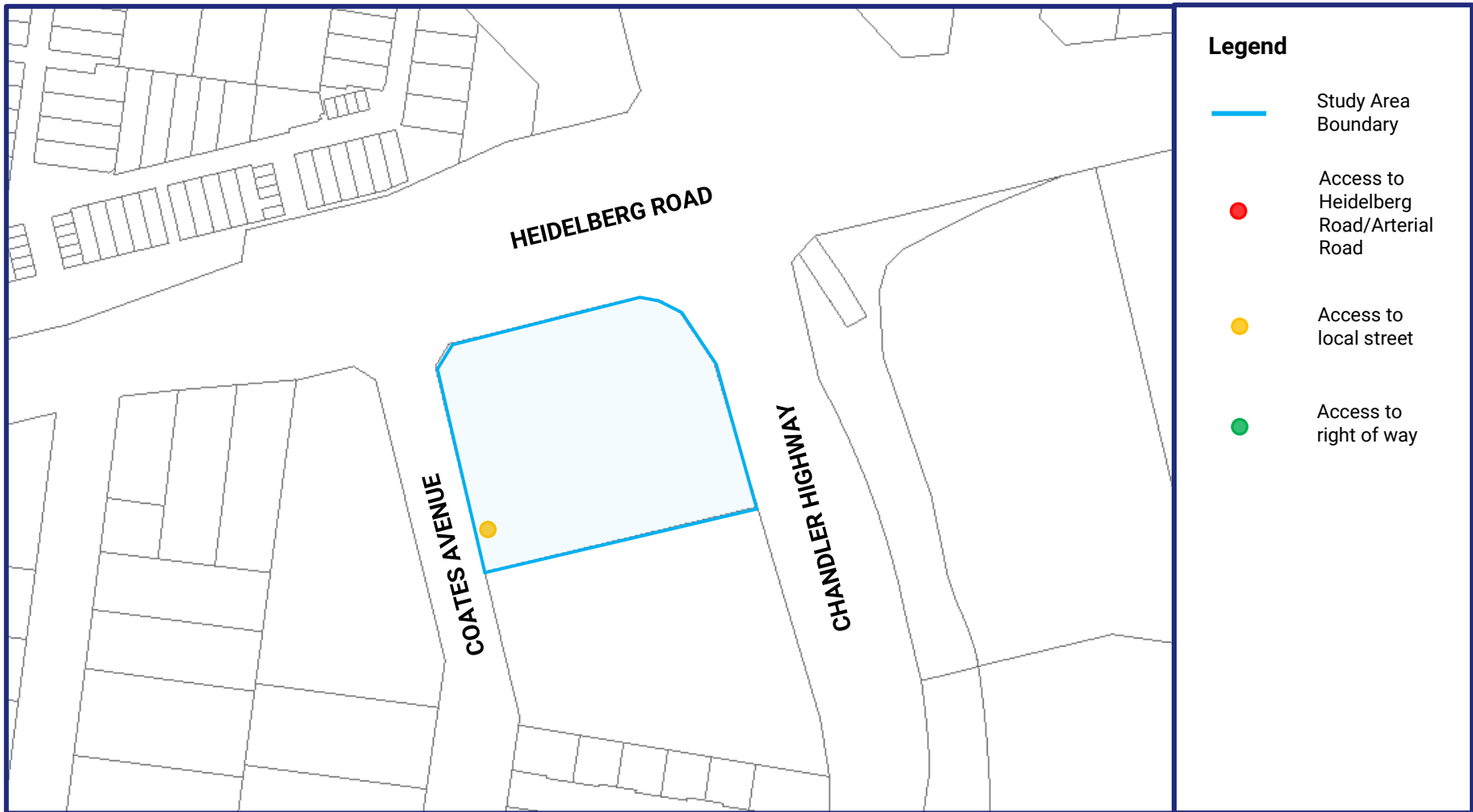


Figure E3: Precinct 3a



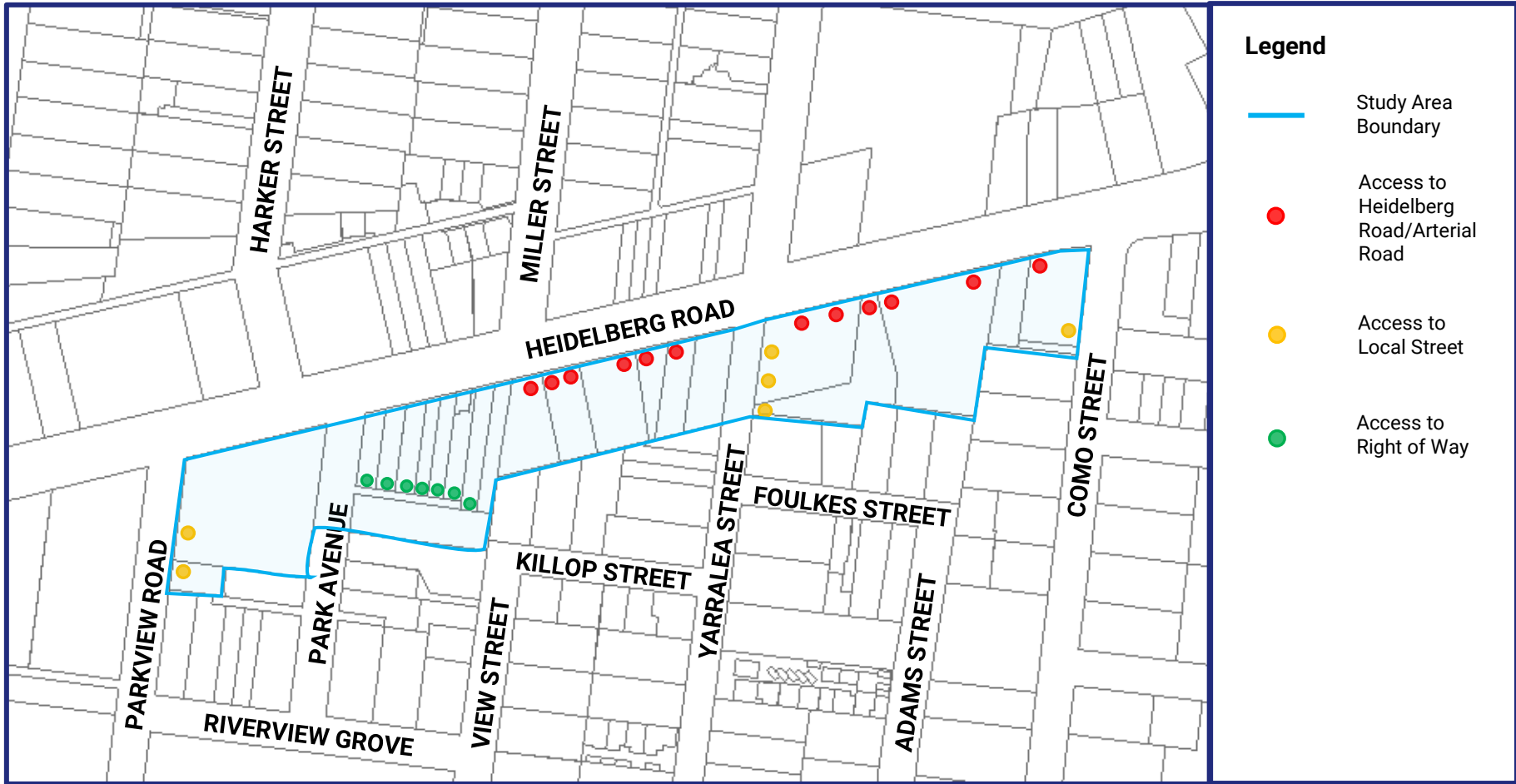


Figure E4: Precinct 3b





Appendix B

Existing Car Parking Restrictions

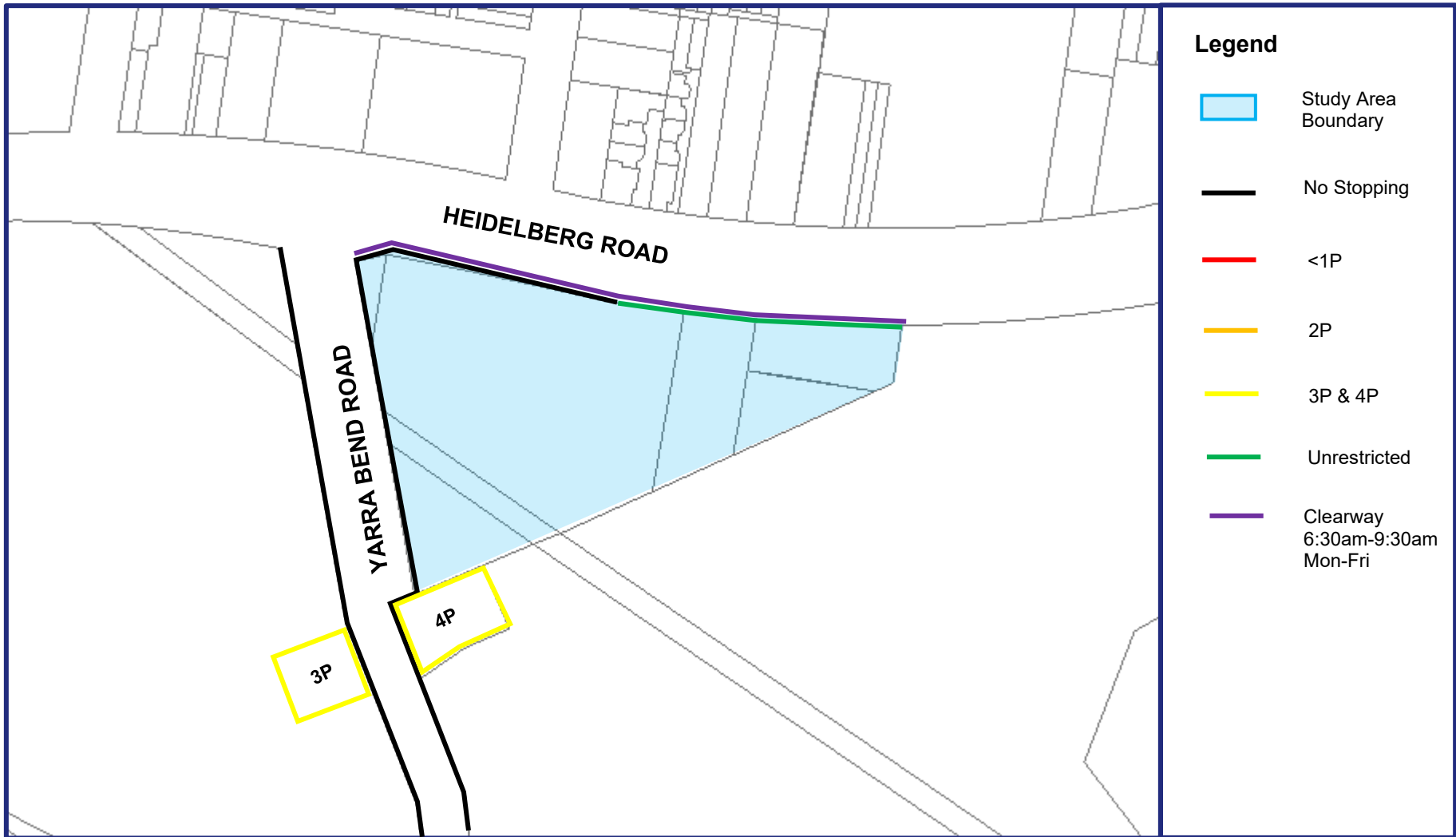


Figure B1: Precinct 1



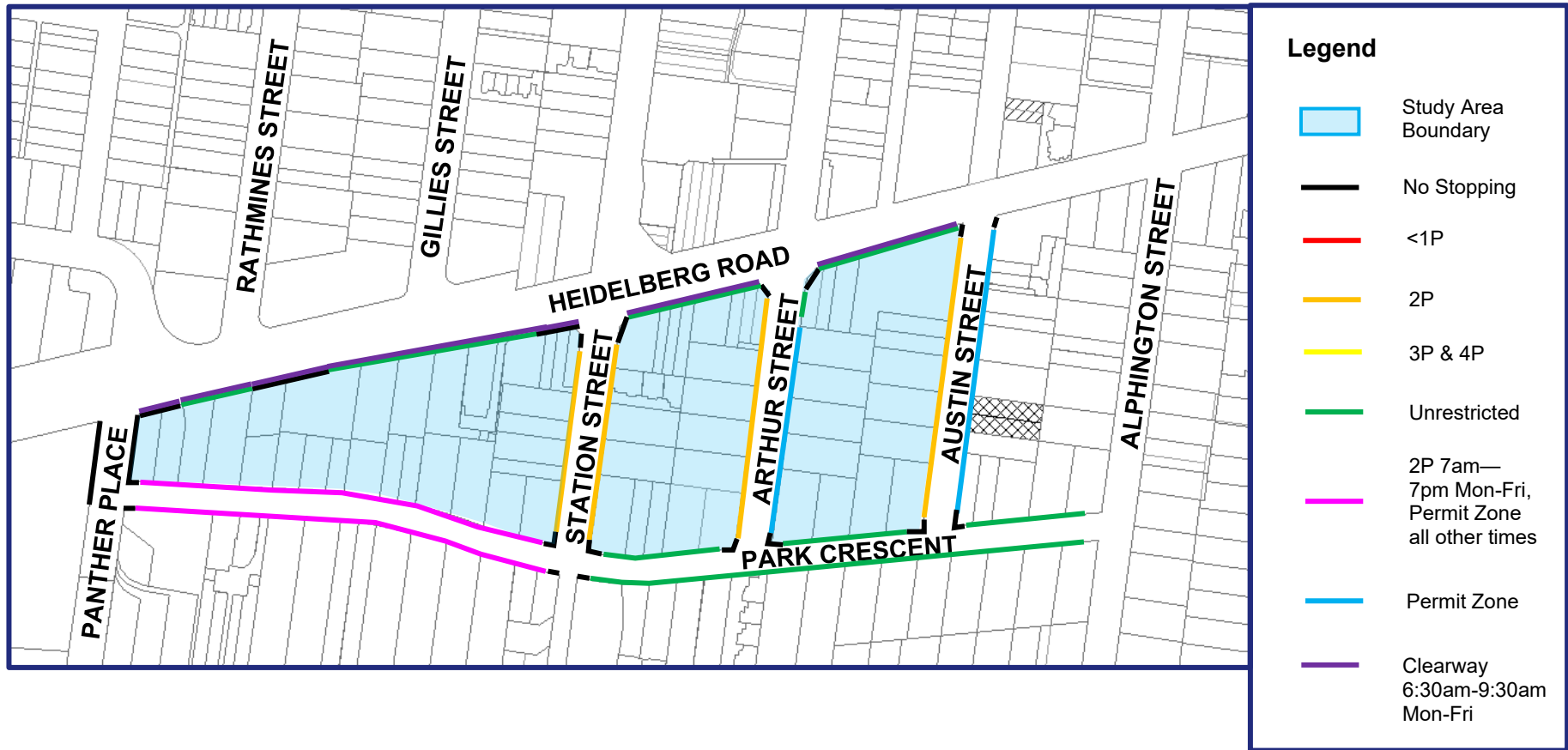


Figure B2: Precinct 2



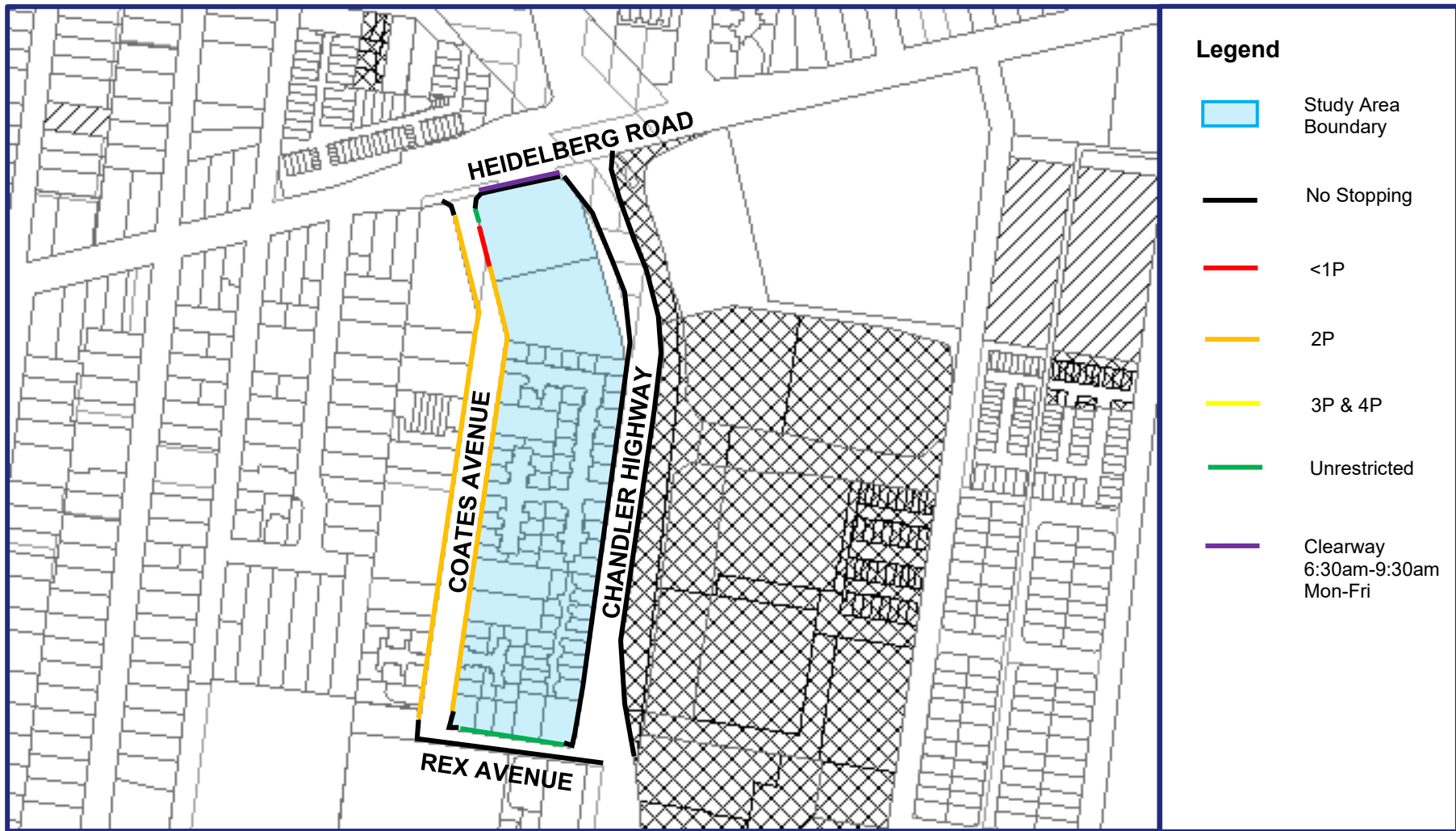


Figure B3: Precinct 3A



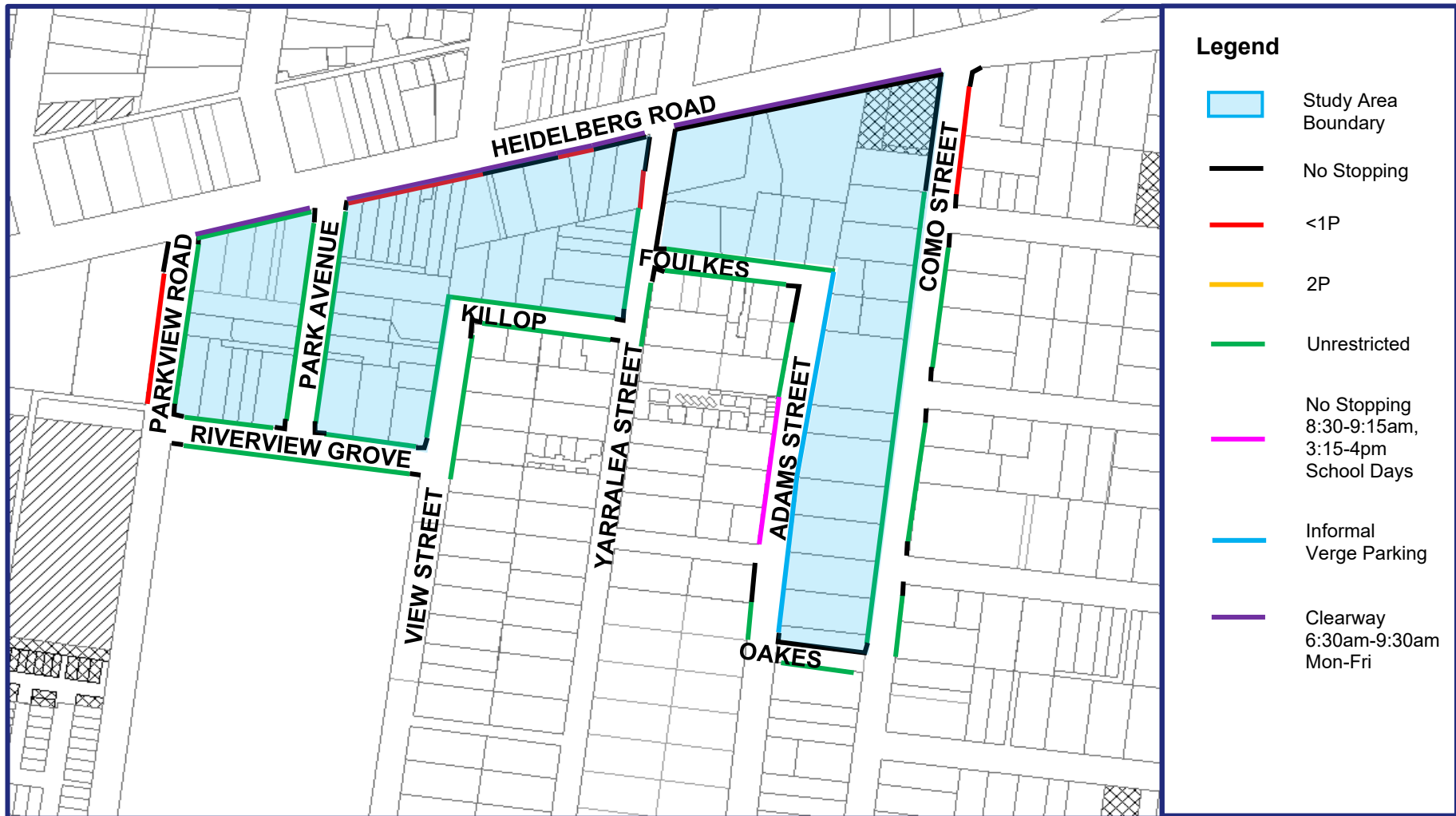


Figure B4: Precinct 3B





Appendix C

Existing Traffic Management

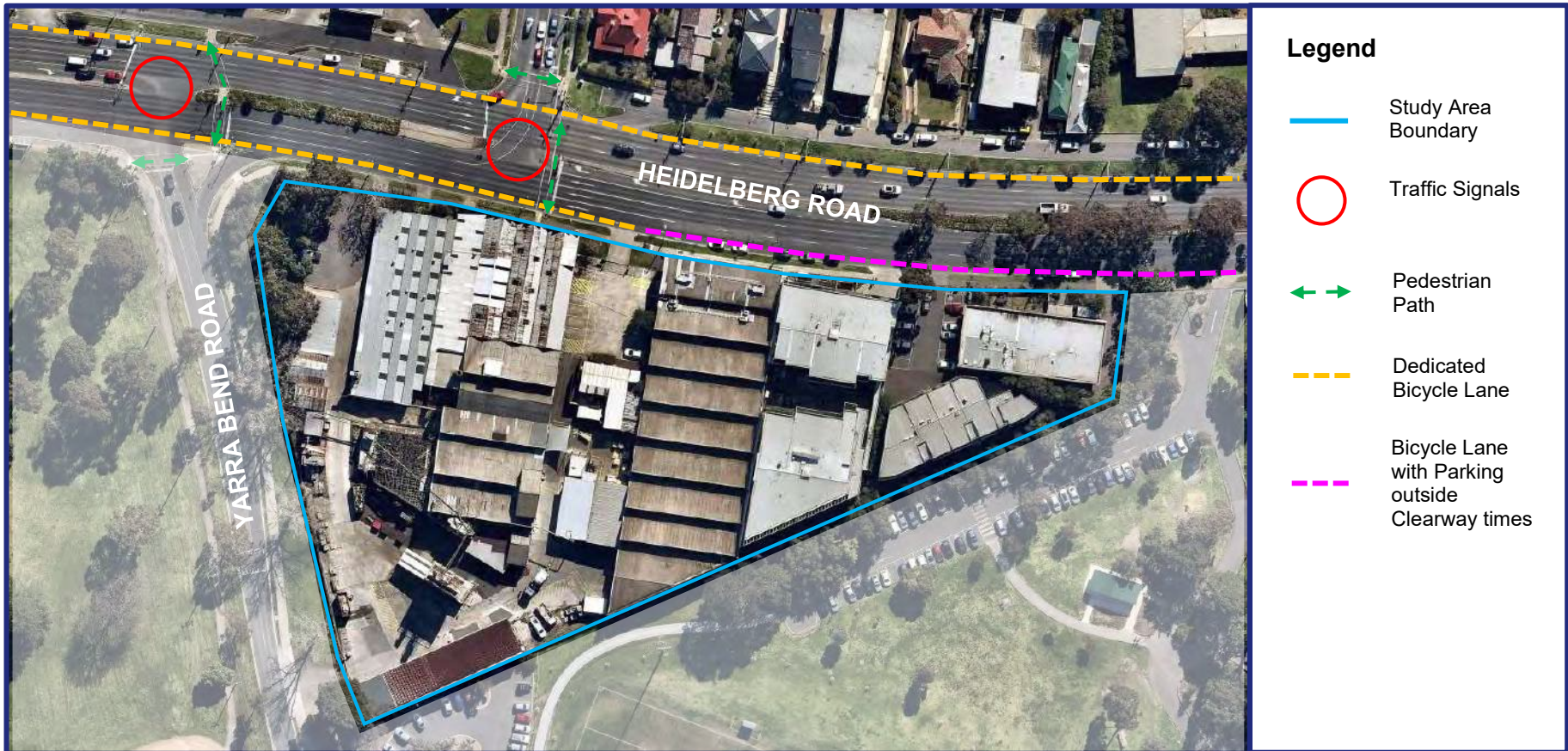


Figure C1: Precinct Area 1





Figure C2: Precinct 2





Figure C3: Precinct 3A





Figure C4: Precinct 3B





Appendix D

Victoria Street Case Study

Victoria Street Description

Victoria Street is similar to Heidelberg Road in many respects, including:

- It is a parallel east-west transport route between Melbourne’s inner eastern suburbs and the CBD.
- It has a large number of intersections with minor local roads.
- Land use is a mixture of residential, commercial and industrial land uses.

In April, 2010, Yarra City Council adopted the Victoria Street Structure Plan, a document that built on planning work that occurred between 2002 and 2010. Since that time, significant redevelopment has occurred, particularly within the eastern and western precincts identified by this structure plan.

The following reviews the changes to Victoria Street and the changes in transport along Victoria Street as a model for how Heidelberg Road may evolve over time.

Increase in Activity along Victoria Street

The number of people living within the Richmond Statistical Local Area has increased from 23,797 people in 2001 to 26,121 in 2011, which is a 9.7% increase over that time period.

Yarra City Council has provided data on the increased development that has occurred directly adjacent to Victoria Street in the last 10 years. This data was sourced from the valuation and permit information data by Council and Housing Dwelling Development data provided by the State Government.

Table D1 sets out the change in dwelling numbers along Victoria Street and Table D2 sets out the change in commercial floor space along Victoria Street.

Table D1: Change in Dwelling Numbers along Victoria Street – 2007-2016

Year	Total Dwellings	Yearly Change	Net Change Since 2007
2007	135		
2008	139	+4	+4
2009	200	+61	+65
2010	254	+54	+119
2011	347	+93	+212
2012	626	+279	+491
2013	1499	+873	+1364
2014	2119	+620	+1984
2015-2016	2490	+371	+2355

The change in dwelling density is highlighted in the following two maps.



Figure D1: Change in dwelling density – 2007-2016

Table D2: Change in Commercial Floor Space along Victoria Street – 2007-2013

Year	Commercial Floor Space	Yearly Change	Net Change Since 2007
Pre-2007	46,737m ²		
2009	45,006m ²	-1,731m ²	-1,731m²
2010	46,609m ²	1,603m ²	-128m²
2013	42,814m ²	-3,795m ²	-3,923m²

Review of Arterial Road Traffic Volumes

The following presents a review of arterial road traffic volumes over the last 10 years of available data for the three key parallel traffic routes through Richmond, Swan Street, Victoria Street and Bridge Road. This is set out in detail in Table D3.

Table D3: Arterial Road Traffic Volumes (Source: VicRoads Arterial Road Database - Feb 2017)

Road Name	Two-Way Annual Average Daily Traffic Volume ¹ by Year					
	2006	2013	2014	2015	2016	Change 2006-2016
Swan Street						
Btw Church/Lennox	18,000	17,800	17,300	17,200	17,200	-800
Btw Coppin/Church	21,000	21,000	20,600	20,300	20,300	-700
Btw Burnley/Coppin	19,600	20,300	20,200	20,300	20,200	+600
Btw Madden/Burnley	15,300	15,600	15,600	15,600	15,200	-100
Victoria Street						
Btw Church/Hoddle	22,700	18,600	18,300	18,200	18,000	-4,700
Btw Burnley/Church	22,000	20,000	18,800	18,500	18,300	-3,700
Btw High/Burnley	24,000	23,000	23,000	23,000	23,000	-1,000

Road Name	Two-Way Annual Average Daily Traffic Volume ¹ by Year					
	2006	2013	2014	2015	2016	Change 2006-2016
Bridge Road						
Btw Hoddle/Lennox	20,000	18,400	18,300	18,300	17,900	-2,100
Btw Lennox/Church	19,500	18,700	18,500	18,400	18,200	-1,300
Btw Church/Coppin	22,000	20,800	19,500	19,500	18,600	-3,400
Btw Coppin/Burnley	23,000	20,700	20,600	20,600	20,600	-2,400
Btw Burnley/Yarra	27,000	24,000	24,000	23,000	23,000	-4,000
Note: Annual Average Daily Traffic Volume is the sum of all traffic over the year divided by 365						

The above illustrates that arterial road traffic volumes have generally fallen between 2006 and 2016. Traffic volumes on Victoria Street in particular have fallen substantially over the last 10 years. There has not been a significant change to the traffic carrying capacity of these streets within this time period.

Furthermore, this decrease in traffic volumes is also reflected at key intersections during the commuter peak hours. Table D4 provides a comparison between current and historical data for two key intersections along Victoria Street and illustrates a drop in traffic volumes at these locations during peak hours. The Burnley Street/Victoria Street and Flockhart Street/Victoria Street intersections are the closest signalised intersections to where the highest level of development has occurred.

Table D4: Review of Peak Hour Traffic on Victoria Street

Intersection & Year of Survey	Two-Way Peak Hour Traffic Volume on Victoria Street	
	AM Peak	PM Peak
Flockhart Street (west of)		
2006 ¹	2,203	2,267
2015 ²	1,827	1,957
Change	-376 (-21%)	-310 (-16%)
Burnley Street (east of)		
2012 ³	1,933	1,831
2016 ⁴	1,709	1,649
Change	-224 (-13%)	-182 (-11%)

Notes:

1. Data collected by Grogan Richards dated 11th July, 2006.
2. Data sourced from VicRoads by Cardno, dated 11-15th May, 2015.
3. Data sourced from VicRoads by Traffix Group, dated 7th June, 2012.
4. Data collected by Ratio Consultants dated 14th April, 2016.

Review of Travel to Work Behaviour

The follow tables review the journey to work data sourced from the Australian Bureau of Statistics for the period from 2001 to 2016.

Table D5 presents data for journey to work based on place of residence within the City of Yarra.

Table D6 presents data for journey to work for people working within the Richmond Statistical Local Area (workers do not necessarily need to reside within Richmond).

The data indicates a clear trend over time for a decrease in the mode share of private cars. For people living within the City of Yarra, this decrease is realised by an increase in bicycle and walking trips. This is a strong indication of local living and working locally.

For people working within Richmond, the decrease in mode share of cars is higher. The change has resulted in a significant increase in public transport use (a relative 60% increase) and to a lesser extent walking and cycling. This is reflective of residents outside of Richmond travelling further and accordingly cycling and walking in particular are not a suitable mode for these longer trips.

Table D5: Journey to Work Data - Place of Residence within City of Yarra

Mode of Travel	Year				Change 2001-2016
	2001	2006	2011	2016	
Car as Driver	48%	43%	40%	38%	-10%
Car as Passenger	4%	3%	3%	2%	-2%
P/Trans	30%	28%	30%	32%	+2%
Motorcycle	1%	1%	1%	1%	-
Bicycle	5%	8%	10%	10%	+5%
Walked	11%	15%	13%	14%	+3%
Other	1%	2%	3%	3%	+2%
Total	100%	100%	100%	100%	

Table D6: Journey to Work Data - Place of Work within Richmond SLA

Mode of Travel	Year				Change 2001-2016
	2001	2006	2011	2016	
Car as Driver	73%	67%	61%	41%	-32%
Car as Passenger	5%	4%	4%	2%	-3%
P/Trans	15%	19%	24%	34%	+19%
Motorcycle	0%	1%	1%	1%	+1%
Bicycle	1%	2%	3%	6%	+4%
Walked	5%	6%	6%	14%	+9%
Other	1%	1%	1%	2%	-
Total	100%	100%	100%		

Change in Public Transport Services

The key public transport service for Victoria Street is tram services that run the length of the Activity Centre. Victoria Street is currently serviced by the following tram routes:

- Route 109 – service between Box Hill and Port Melbourne via the CBD.
- Route 12 – service between Victoria Gardens and St Kilda. This route commenced operation in July, 2014.

The changes in July, 2014 doubled the number of services between Victoria Street, Richmond and the CBD. While Tram Route 24 was removed at the same time, this service only operated during the AM and PM peak periods (approximately 7-9am and 4:30-6:30pm).

On Church Street, the peak hour only service Route 79 was terminated with Route 78 being extended to operate more than 18 hours per day.

Bus Route 684 used to operate along Victoria Street, however this service did not stop along Victoria Street (service between the CBD and Eildon via Healesville).

The key public transport service on Victoria Street is the tram services along Victoria Street and these have significantly improved in frequency over the last 10 years.

Increase in Bicycle Use

As set out above, the mode share of bicycles for journey to work purposes has increased from 5% to 10% by residents of Yarra and increased from 1% to 6% for employees within Richmond.

For Victoria Street, the Super Tuesday bicycle counts undertaken by Bicycle Network illustrate an increase in cycling numbers. The Super Tuesday counts are undertaken on an annual basis over the surveyed two hour, 7-9am commuter peak hour.

For the intersection of Victoria Street/Burnley Street/Walmer Street (which connects to the Capital City Trail along the Yarra River), the number of cyclists increased from 298 to 483 cyclists over the two hour period between 2011 and 2015 (62% increase).

Rise of Car Share

Car sharing schemes provide an alternative to car ownership for residents and actively encourage the use of alternative transport modes. Residents within Richmond do not need a car for everyday trips as they have easy access to public transport and are within convenient walking and cycling distance of many activities within the Melbourne CBD and Activity Centres. Car share vehicles provide a car on demand for those trips that specifically require a vehicle.

A study by Phillip Boyle & Associates (dated 18th June, 2015) was recently completed on behalf of the City of Melbourne, which reviewed car share policy in the City of Melbourne. This review found that car share significantly reduced car ownership and car use by members. The review identified that each new car share vehicle results in residents disposing of 10 privately owned vehicles (a net reduction of nine vehicles).

The study found that car ownership is reduced by:

- People replacing a private car with a car share membership as it is more cost-effective if you travel low kilometres (less than 15,000km per annum) and use alternative modes for many trips, and
- People who do not own a car, postpone or avoid purchasing a car by using a car share service.

In 2006, car share was in its infancy. The two leading car share company's today in Melbourne are Fleixcar (founded in 2004) and GoGet (arrived in Melbourne in 2004).

There are now multiple car share pods operated by three companies within close proximity of Victoria Street. The availability of these car share pods supports residents who do not own a car and businesses by providing a share car for work-based business trips (which allows employees not to drive to work).



Appendix E

Existing Vehicle Access Points

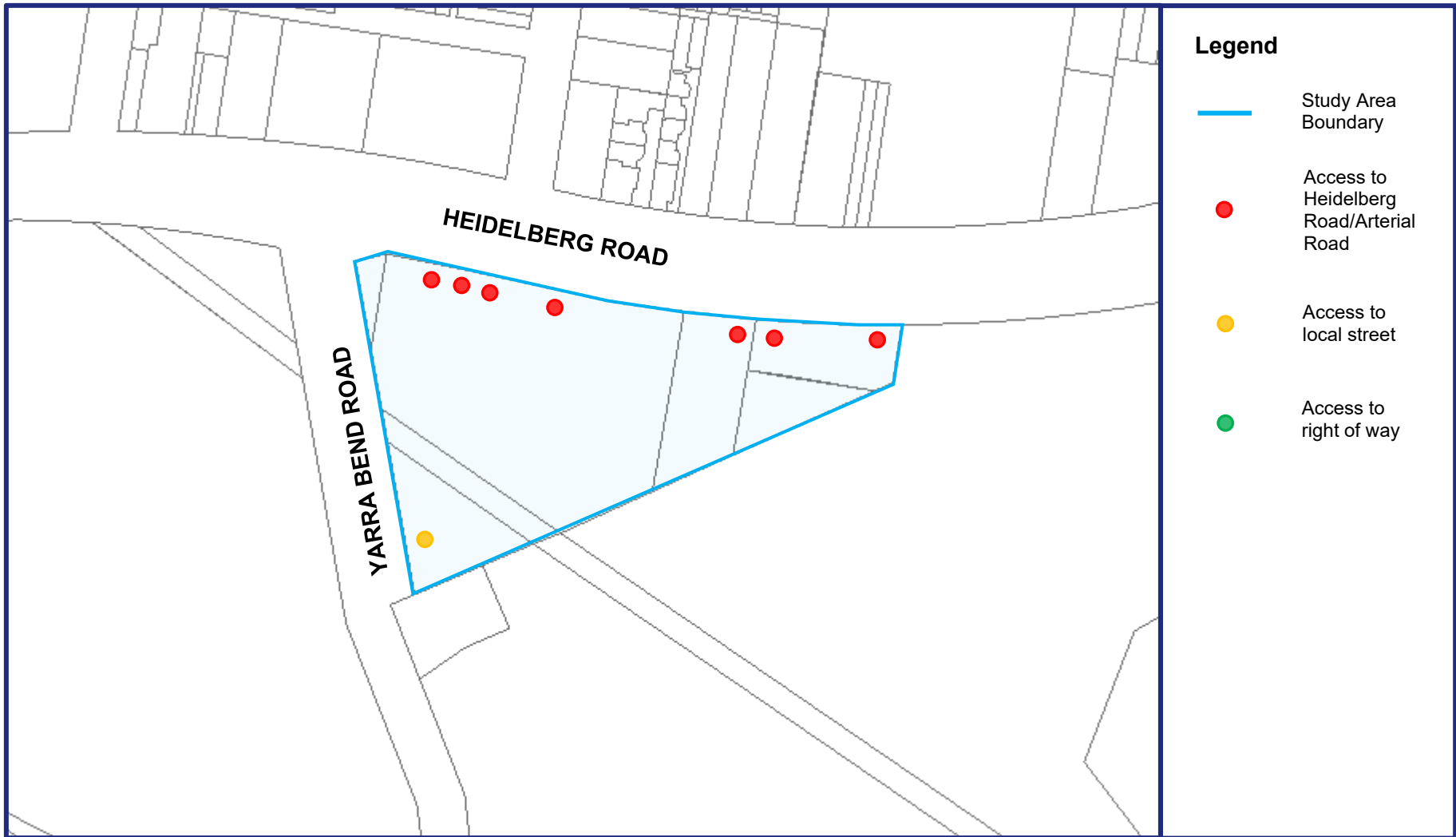


Figure E1: Precinct 1



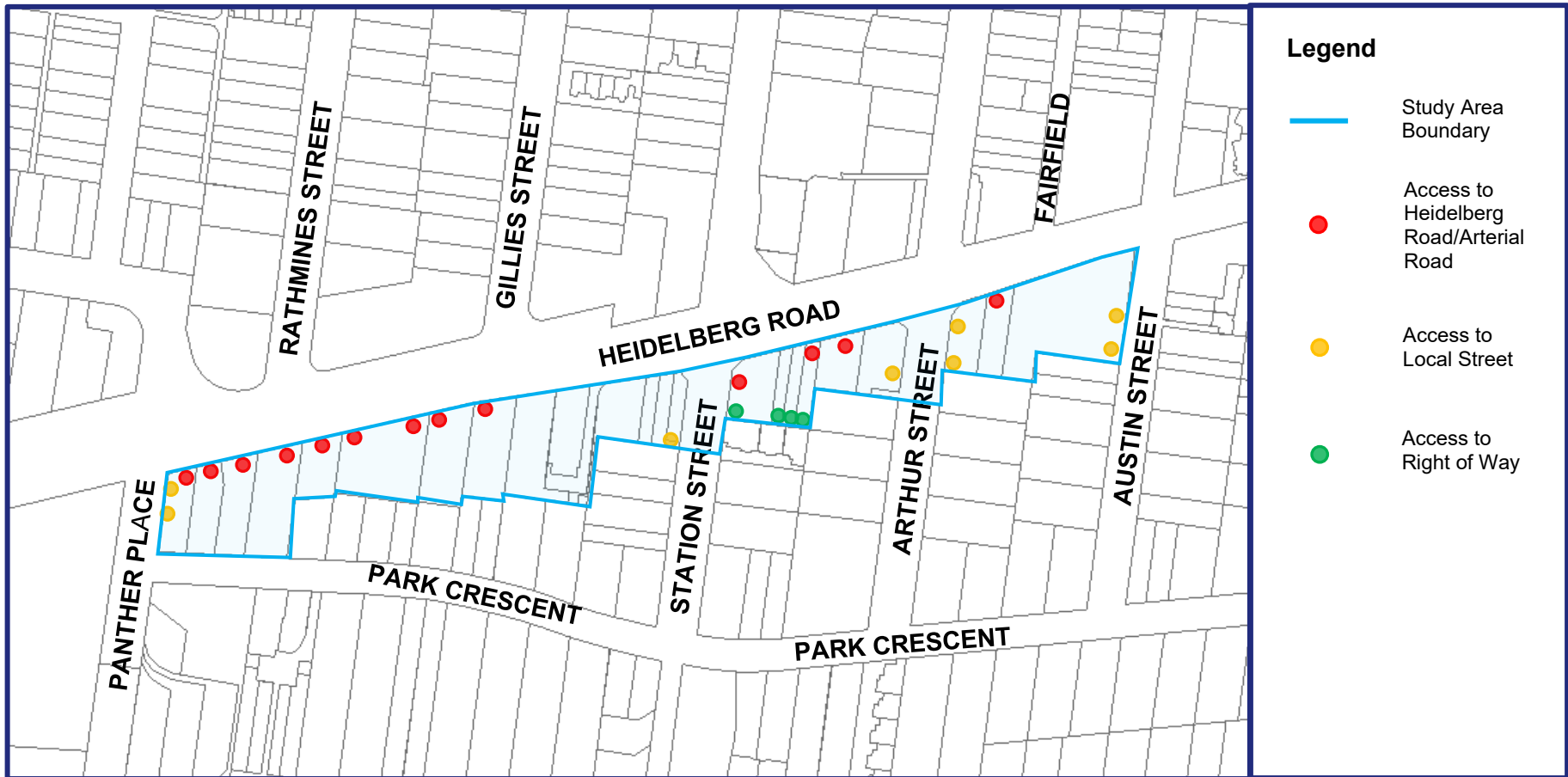


Figure E2: Precinct 2



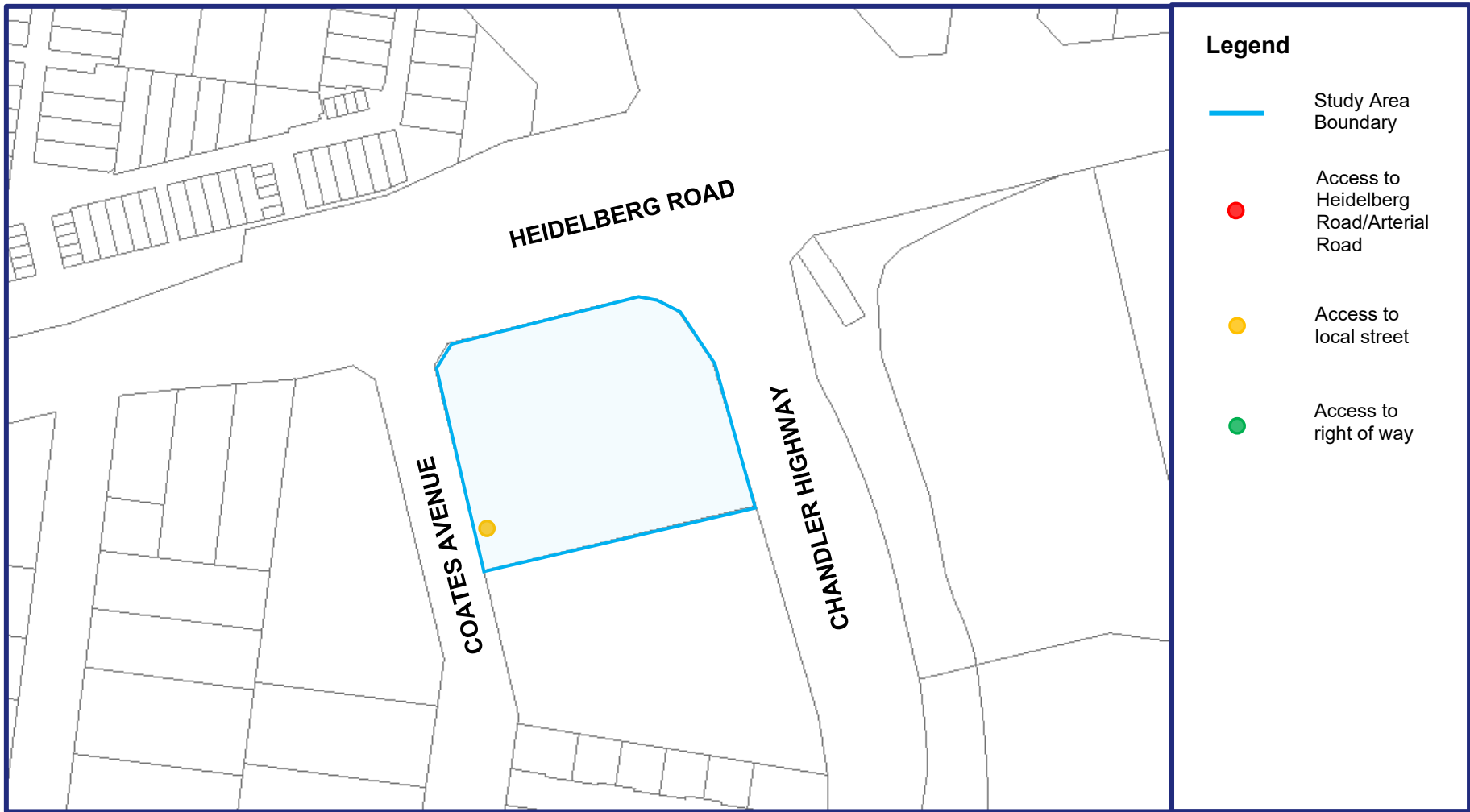


Figure E3: Precinct 3a



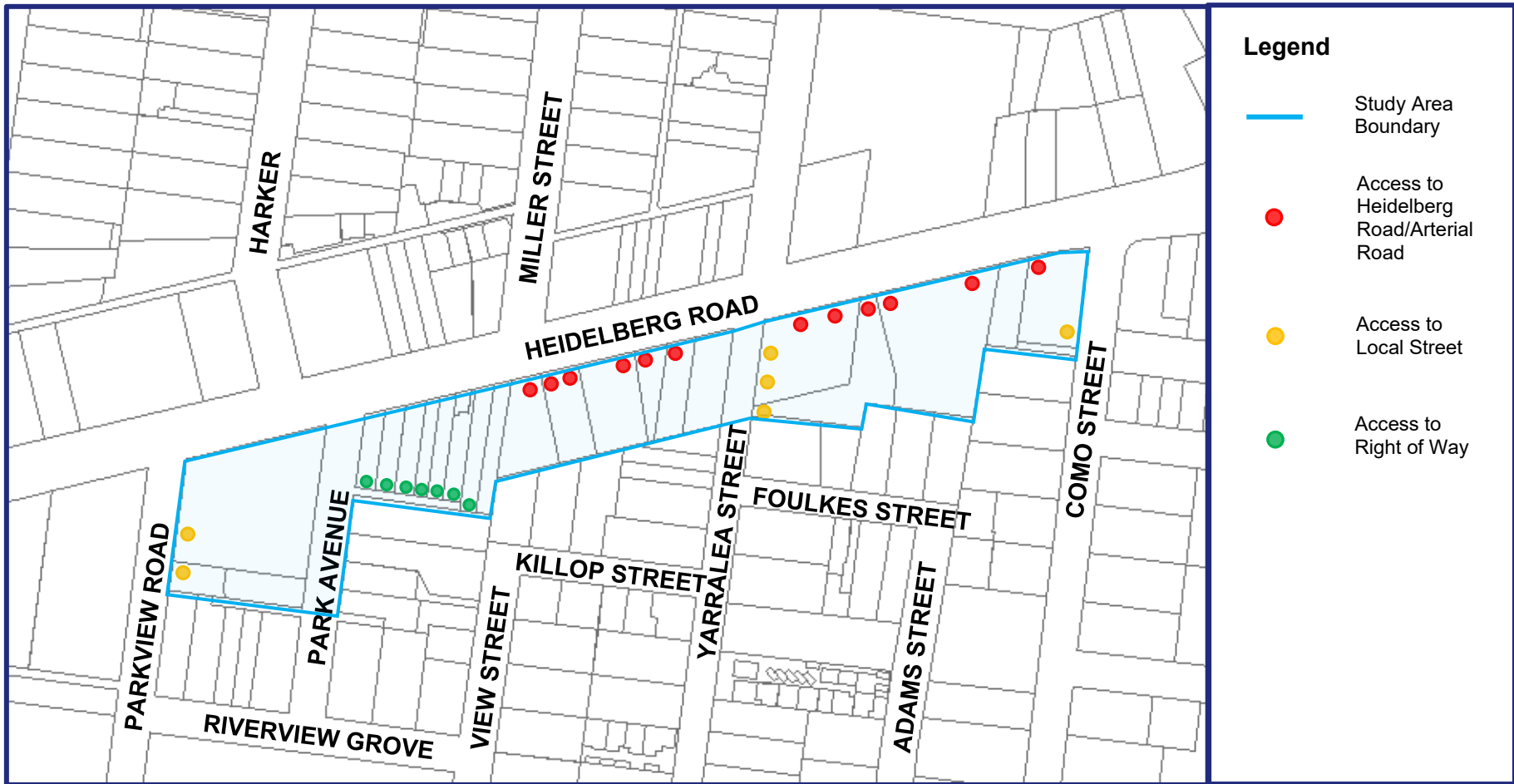
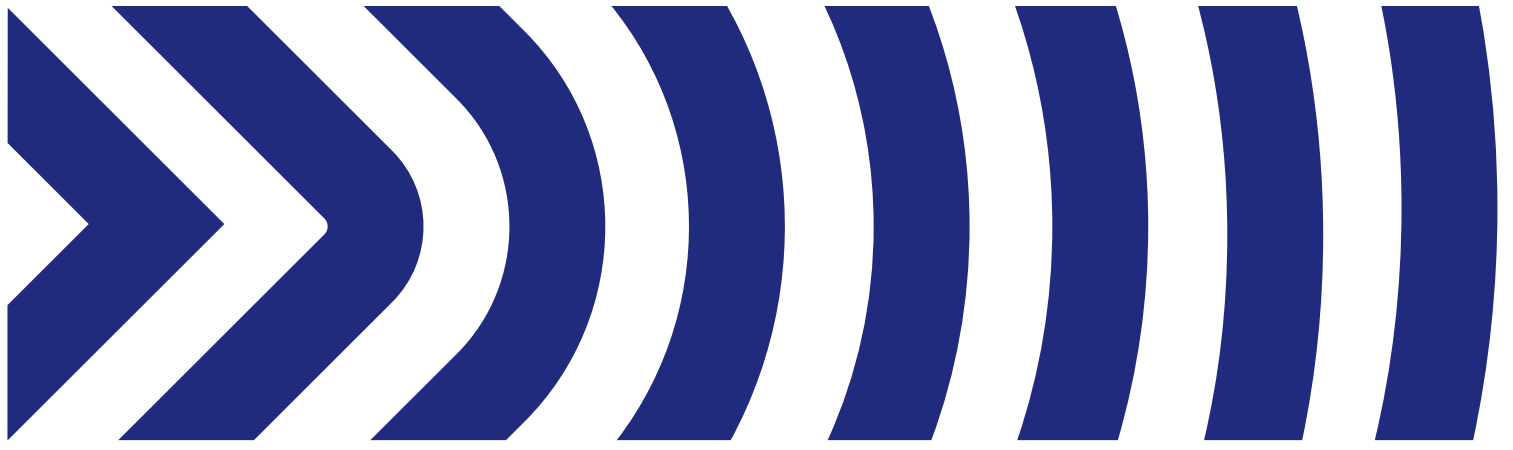


Figure E4: Precinct 3b





Appendix F

Access and Movement Plans

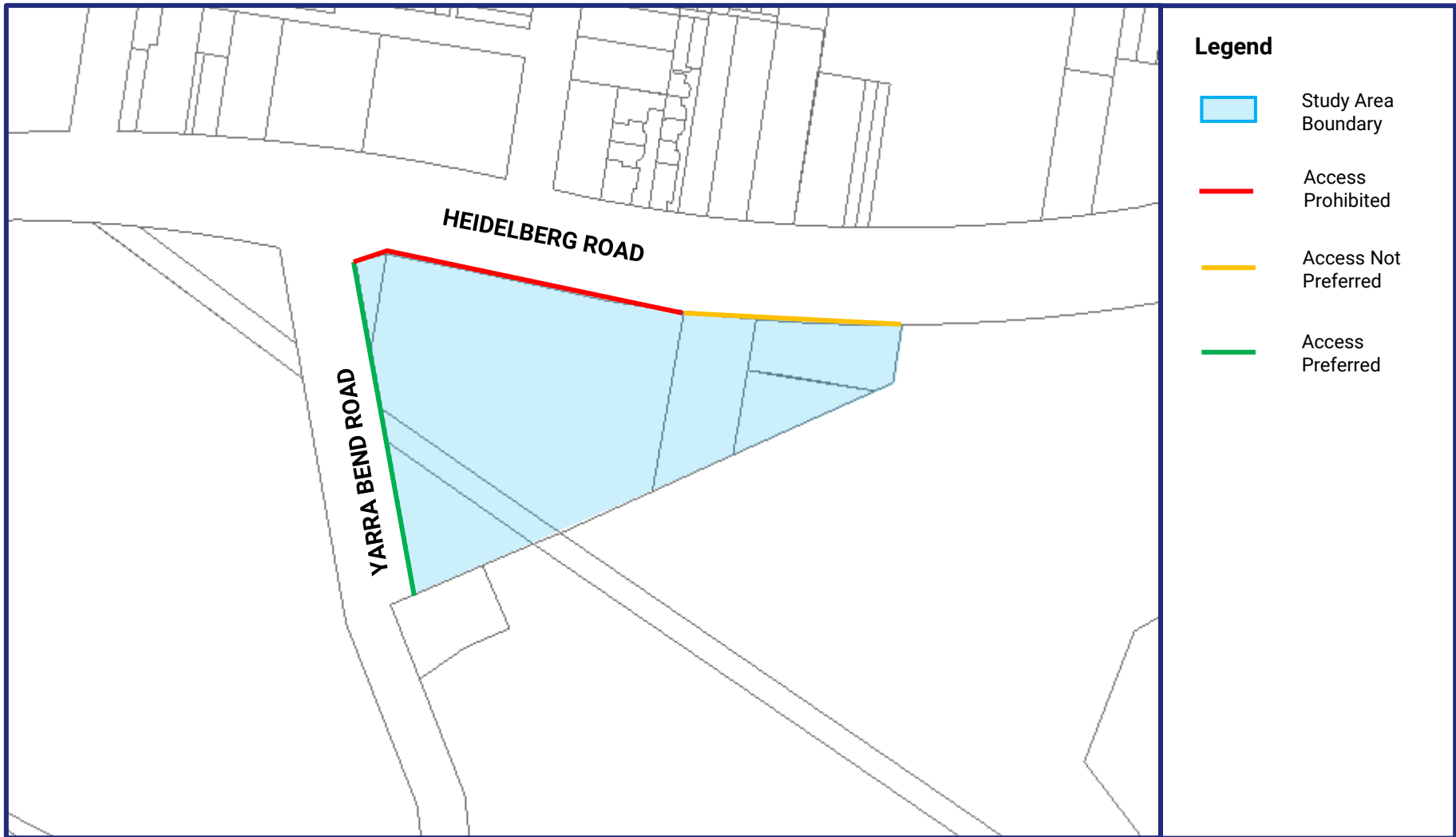


Figure F1: Precinct Area 1



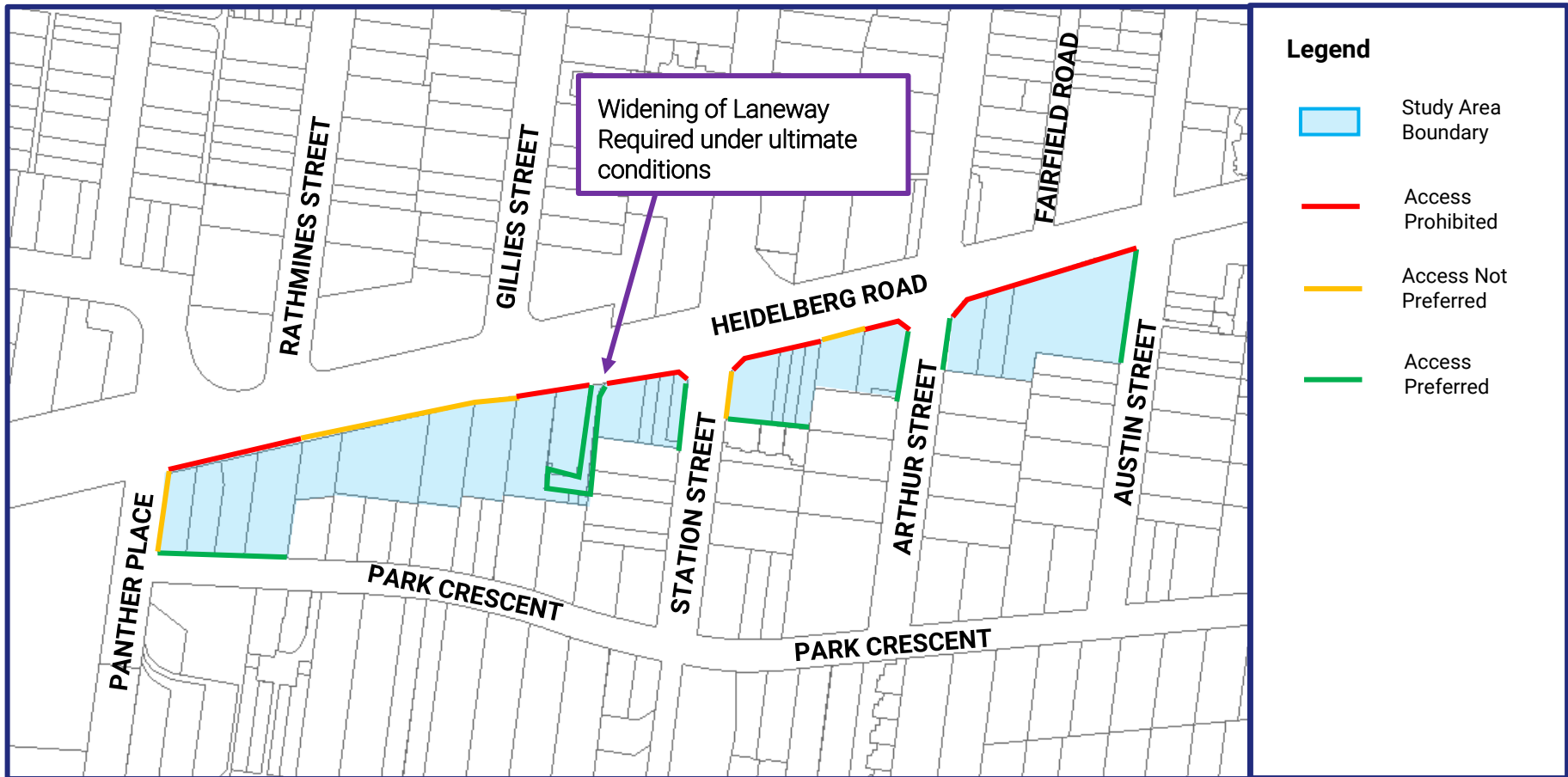


Figure F2: Precinct 2



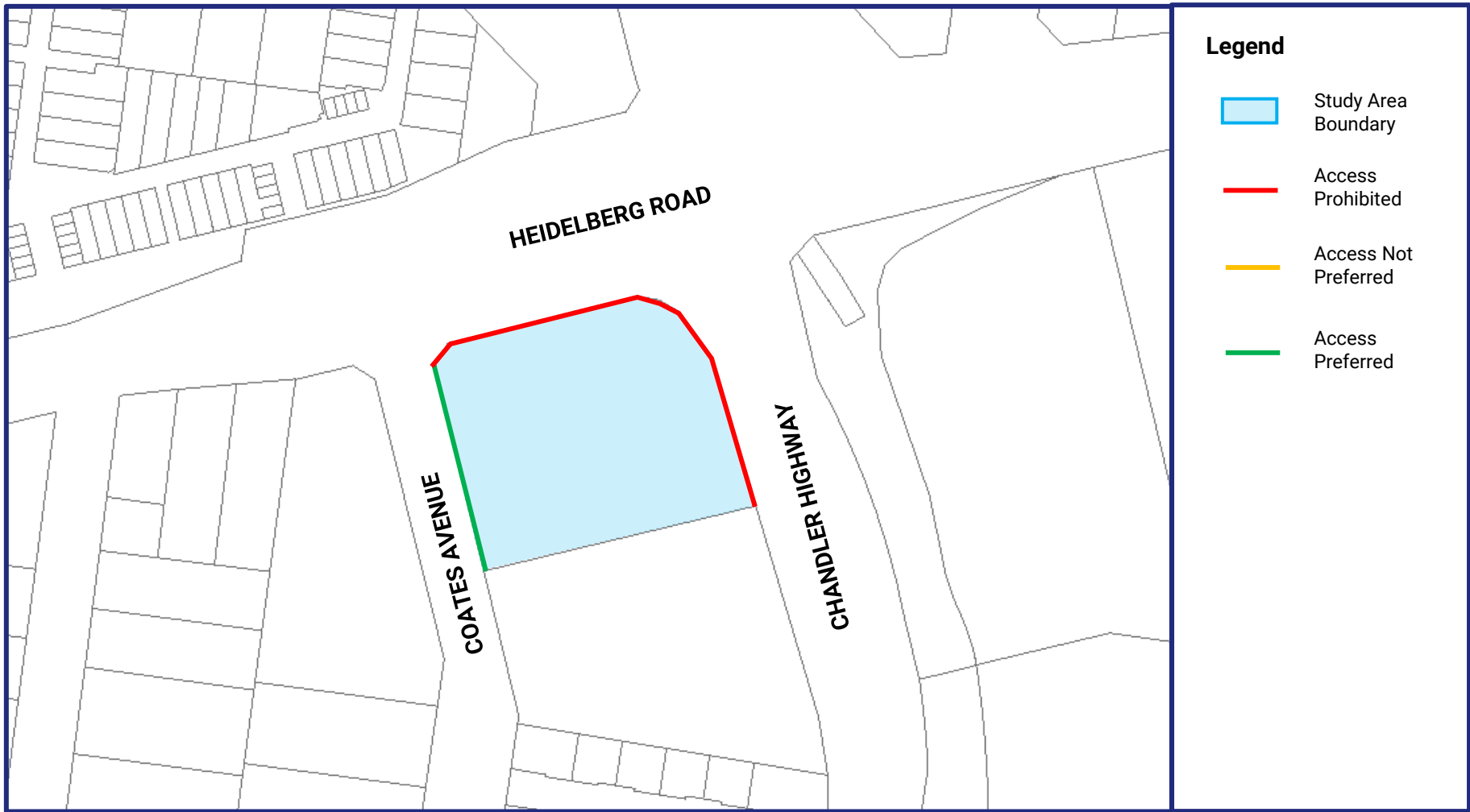


Figure F3: Precinct 3A



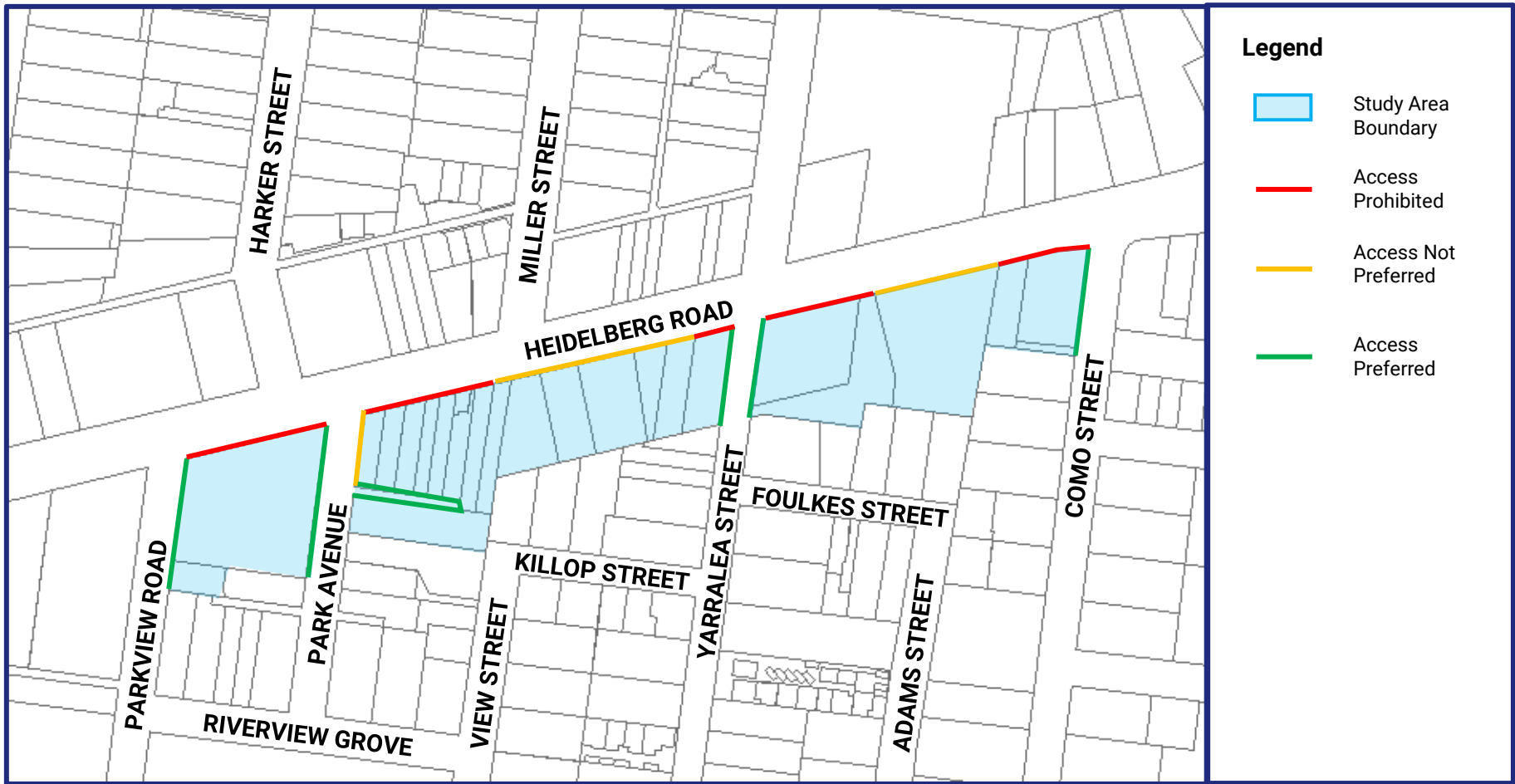
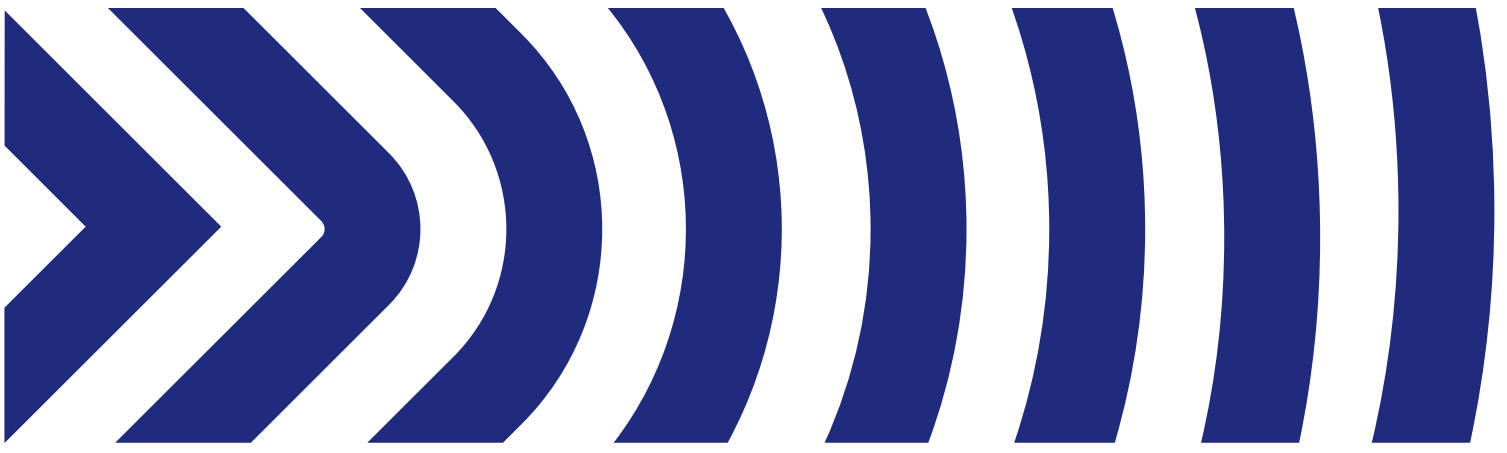


Figure F4: Precinct 3B





Appendix G

Interim Design and Development Overlay

Heidelberg Road – Transport Relevant Sections of an interim Design and Development Overlay

The below is an extract from the working draft of the proposed interim Design and Development Overlay that, if Council adopts it, would apply to the Commercial 1 Zone areas within the City of Yarra along Heidelberg Road.

The most transport-relevant passages have been extracted.

Objectives

Provide a greater sense of openness towards the Heidelberg Road footpath via small front setbacks and still achieves activated, pedestrian-oriented façades and passive surveillance.

Definitions

Laneway means a road reserve of a public highway 9 metres or less wide.

Road boundary means the boundary between the public road and the private property.

Shared zone means a road or network of roads where pedestrians, cyclists, and vehicles share the roadway.

Street-wall means the facade of a building at the street boundary, or, if the existing heritage building is set back from the street boundary, the front of the existing building.

General Requirements

A permit cannot be granted under this Design and Development Overlay to vary a requirement expressed with the term 'must'.

The below requirements apply to an application to construct a building or construct or carry out works.

Street Wall Height and Boundary Set Back Requirements

Towards Heidelberg Road, development

- in Precincts 1 and 3A must achieve a minimum 3 metres front setback, including side boundary walls, to provide better separation with Heidelberg Road and space for circulation and canopy landscaping
- in Precincts 2 should achieve a minimum 3 metres front setback, including side boundary walls, to provide better separation with Heidelberg Road and space for circulation and canopy landscaping.
- in Precinct 3B, between Park Avenue and Yarralea Street, should achieve a continuous street wall with no front setback.

Front Setback Design Requirements

Front setbacks should be designed to create a sense of openness and clear line of sight at pedestrian level between the public footpath and street wall and include:

- unobstructed access by avoiding steps, fences and narrow spaces between planting areas
- canopy trees
- creating a subtle distinction towards the public realm, including but not limited to different paving material, pattern and/or placement of planting.

Access, parking and loading areas requirements

Car parking should be located within a basement or concealed from the main and side streets.

Providing recessed parking spaces at the ground floor of buildings and onsite parking spaces at the front of properties should be avoided, except for development east of Yarralea Street, Alphington.

Pedestrian access to buildings should be achieved via Heidelberg Road or side streets and must be clearly visible, secure and have an identifiable sense of address. Residential and commercial entrances should be distinguishable from each other. Primary access from laneways should be avoided.

The common pedestrian areas of new buildings should be designed with legible and convenient access, with hallway and lobby areas of a size that reflects the quantity of apartments serviced and which can be naturally lit and ventilated.

Bicycle parking should be located and designed to be secure and conveniently accessible from the street and associated uses.

Vehicle ingress and egress into development, including loading facilities and building servicing, should be designed to ensure a high quality pedestrian amenity and limit potential conflict between vehicle movements and pedestrian activity.

Development must not provide new vehicular access from Heidelberg Road.

Development with redundant vehicle access points to Heidelberg Road should reinstate the kerb, linemarked parking bays, and relocate any parking signs.

Application Requirements

The following application requirements apply to an application for a permit under Clause 43.02, in addition to those specified elsewhere in the scheme and must accompany an application, as appropriate, to the satisfaction of the responsible authority:

- A site analysis and urban design context report which demonstrates how the proposal achieves the Design Objectives and Requirements of this schedule.
- To the satisfaction of the Responsible Authority, development proposals should be accompanied by a wind study analysis to demonstrate that pedestrian spaces, balconies, communal areas and secluded private open spaces will not be adversely affected by wind effects.

- A Traffic and Parking Assessment Report which includes an assessment of the cumulative impacts of traffic and parking in the Precinct including an assessment of the ongoing functionality of laneway/s, where applicable.

Decision Guidelines

The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specified in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

- Whether the design of the streetscape interface makes a positive contribution to an active, pedestrian-oriented street environment and/or public realm.
- The design of the retail streetscape interface along the main street frontage.
- Whether the development delivers design excellence, including but not limited to building siting, scale, massing, articulation and materials.
- Whether the development mitigates negative wind effects for the public realm and surrounding sites.
- The cumulative impact of development on traffic and parking in the nearby area, including on the functionality of laneway's.