

Attachment 1 - PLN21/0042 - 46 - 52 Smith Street Collingwood - Advertising S52 - Plans



46-52 SMITH STREET,
COLLINGWOOD, VIC 3066
DRAWING LIST

- A0001 - LOCATION PLAN & DRAWING LIST
- A0002 - EXISTING CONDITIONS SURVEY
- A0003 - WEST DEMOLITION ELEVATION - SMITH STREET
- A0004 - NORTH DEMOLITION ELEVATION - DERBY STREET
- A0005 - EAST DEMOLITION ELEVATION - LANEWAY
- A0006 - DEMOLITION PLAN

- A0099 - B1 - BASEMENT FLOOR PLAN
- A0100 - L00 - GROUND FLOOR PLAN
- A0101 - L01 - FLOOR PLAN
- A0102 - L02 - FLOOR PLAN
- A0103 - L03/05 - FLOOR PLAN
- A0104 - L04/L06 - FLOOR PLAN
- A0107 - ROOF PLAN
- A0108 - PV PLAN

- A0900 - WEST ELEVATION - SMITH STREET
- A0901 - NORTH ELEVATION - DERBY STREET
- A0902 - EAST ELEVATION - LANEWAY
- A0903 - SOUTH ELEVATION

- A0950 - SECTION AA
- A0951 - SECTION BB

TOWN PLANNING RFI -
SCHEDULE OF CHANGES

LEVEL BASEMENT

- B.1 THE ANNOTATION OF "30 BICYCLE SPACES 15 x 60RA E3DT-GT =30 (DOUBLE TIER BIKE RACK)" ADDED
- B.2 THE LOCATION OF BICYCLE CHARGING PORT ZONE ADDED
- B.3 THE ANNOTATION OF THE CONNECTION OF THE RAINWATER TANK ADDED

LEVEL 00

- 0.1 THE DOOR OF FIRE BOOSTER CHANGED FROM METAL TO GLASS
- 0.2 REDUCED SIZE OF CANOPY ALONG SMITH STREET TO COMPLY WITH THE 750 SETBACK FROM PEDESTRIAN KERB, THE DIMENSION OF CANOPY ADDED
- 0.3 A SOLID CANOPY ON THE GROUND LEVEL AT THE NORTH WESTERN ENTRANCE ADDED, REFER TO THE WIND CONSULTANT'S RECOMMENDATION
- 0.4 ANNOTATION TO INDICATE THE LOCATION OF THE LIGHT AND ELECTRICITY POLE ADDED

LEVEL 08 (PV PLAN)

- 8.1 ADDING THE ANNOTATION TO INDICATE THE CAPACITY OF SOLAR PANELS

DEMOLITION ELEVATIONS

- DE.1 INCLUDES THE DEMOLITION OF THE ENTRANCE CANOPY
- DE.2 EXISTING FIRE BOOSTER SOLID METAL DOORS DEMOLISHED

ELEVATIONS

- E.1 ADDING THE COLOR ELEVATION SET
- E.2 THE DOOR OF FIRE BOOSTER CHANGED FROM METAL TO GLASS
- E.3 AMENDED ELEVATIONS TO DEMONSTRATE THE EXTENT OF GLAZING PROPOSED ADJACENT TO THE HERITAGE PARAPET OF No.52 SMITH STREET
- E.4 A SOLID CANOPY ON THE GROUND LEVEL AT THE NORTH WESTERN ENTRANCE ADDED, REFER TO THE WIND CONSULTANT'S RECOMMENDATION
- E.5 DESIGNED VISUAL INTEREST ADDED TO THE SOUTH BLANK SIDE WALL

SECTIONS

- S.1 A-A SECTION, THE ANNOTATION OF BASEMENT CHANGED FROM F/B TO GYM
- S.2 REDUCED THE SIZE OF CANOPY ALONG SMITH STREET TO COMPLY WITH THE 750 SETBACK FROM PEDESTRIAN KERBS, THE DIMENSION FOR THE HEIGHT OF CANOPY ADDED.
- S.3 A SOLID CANOPY ON THE GROUND LEVEL AT THE NORTH WESTERN ENTRANCE ADDED, REFER TO THE WIND CONSULTANT'S RECOMMENDATION



XO PROJECTS
XO TRS
TEL +61 (0)399 237 462
602/232 LA TROBE STREET
MELBOURNE, VIC 3000
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XO-PROJECTS.COM

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A	16/10/20	PLANNING APPLICATION	SH				
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PROJECT TITLE
46-52 SMITH STREET, COLLINGWOOD, VIC
3066

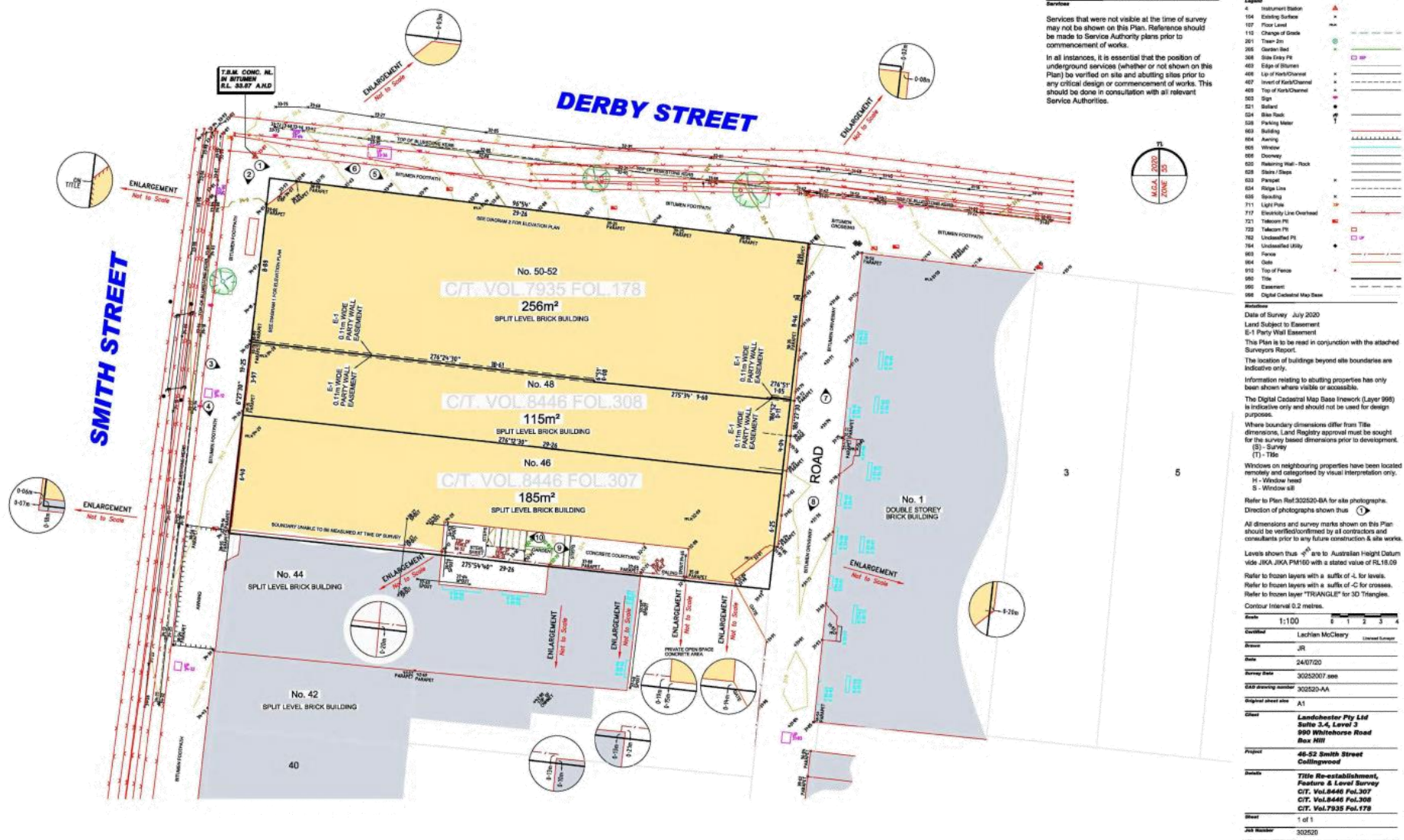
CLIENT
THREE TOWERS GROUP

DRAWING TITLE
LOCATION AND DRAWING LIST

PROJECT NUMBER
XO20009

DRAWING NUMBER
A00001

Attachment 1 - PLN21/0042 - 46 - 52 Smith Street Collingwood - Advertising S52 - Plans



Services
 Services that were not visible at the time of survey may not be shown on this Plan. Reference should be made to Service Authority plans prior to commencement of works.
 In all instances, it is essential that the position of underground services (whether or not shown on this Plan) be verified on site and adjoining sites prior to any critical design or commencement of works. This should be done in consultation with all relevant Service Authorities.

Legend

Instrument Station	+
Existing Surface	•
Floor Level	•
Change of Grade	•
Tree Top	•
Garden Bed	•
Site Entry Pt	•
Edge of Footway	•
Top of Kerb/Chamber	•
Invert of Kerb/Channel	•
Top of Kerb/Channel	•
Sign	•
Ballast	•
Blade Road	•
Parking Meter	•
Building	•
Awning	•
Window	•
Downspout	•
Roofing Half-Rack	•
Stairs / Steps	•
Parapet	•
Ridge Line	•
Spouting	•
Light Pole	•
Boundary Line Overhead	•
Telephone Pt	•
Telephone Pt	•
Unsealed Pt	•
Unsealed Utility	•
Fence	•
Gate	•
Top of Fence	•
Tile	•
Essential	•
Digital Cadastral Map Base	•

Metadata
 Date of Survey July 2020
 Land Subject to Easement
 S-1 Party Wall Easement
 This Plan is to be read in conjunction with the attached Surveyors Report.
 The location of buildings beyond site boundaries are indicative only.
 Information relating to adjoining properties has only been shown where visible or accessible.
 The Digital Cadastral Map Base (Layer 998) is indicative only and should not be used for design purposes.
 Where boundary dimensions differ from Title dimensions, Land Registry approval must be sought for the survey based dimensions prior to development.
 (S) - Survey
 (T) - Title
 Windows on neighbouring properties have been located remotely and categorised by visual interpretation only.
 W - Window head
 S - Window sill
 Refer to Plan Ref:322520-BA for site photographs.
 Direction of photographs shown thus ①
 All dimensions and survey marks shown on this Plan should be verified/confirmed by all contractors and consultants prior to any future construction & site works.
 Levels shown thus ⁺ are to Australian Height Datum vide JICA JKA PM100 with a stated value of RL18.09
 Refer to frozen layers with a suffix of -L for levels.
 Refer to frozen layers with a suffix of -C for crosses.
 Refer to frozen layer "TRIANGLE" for 3D Triangles.
 Contour Interval 0.2 metres.

Scale
 1:100

Drawn Lachlan McCleary
Checked J.R.
Date 24/07/20
Survey Date 30/25/2007 see
Original drawing number 302520-AA
Original sheet size A1
Client Landchester Pty Ltd
 Suite 2.4, Level 3
 990 Whitehorse Road
 Box Hill
Project 46-52 Smith Street
 Collingwood
Details Title Re-establishment,
 Features & Level Survey
 C/T. Vol.8446 Fol.307
 C/T. Vol.8446 Fol.308
 C/T. Vol.7935 Fol.178
Sheet 1 of 1
Job Number 302520

Level 3, 1 Southbank Boulevard
 Southbank, Victoria 3005
 03 7018 8600
 www.veris.com.au
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PROJECT TITLE
 46-52 SMITH STREET, COLLINGWOOD, VIC
 3066

CLIENT
 THREE TOWERS GROUP

DRAWING TITLE
 EXISTING CONDITIONS SURVEY

PROJECT NUMBER
 XO20009

DRAWING NUMBER
 A00002

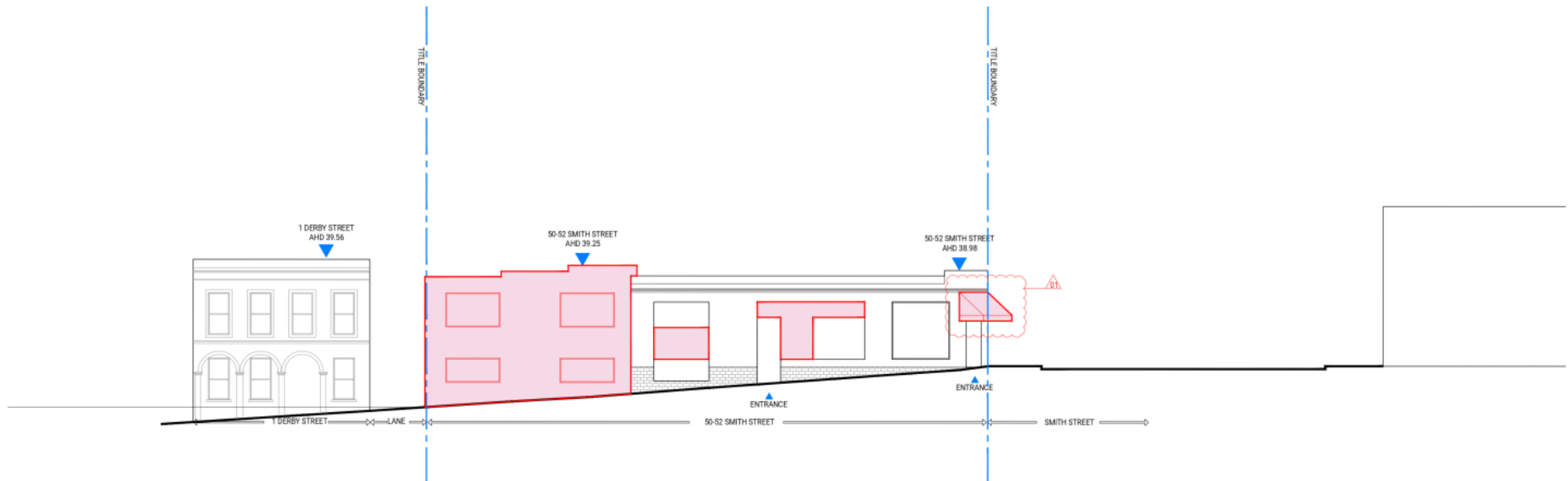
DRAWING STATUS
 TP

REVISION

Attachment 1 - PLN21/0042 - 46 - 52 Smith Street Collingwood - Advertising S52 - Plans

REVISION NOTES

01 - THE DEMOLITION OF THE ENTRANCE CANOPY ADDED



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602/232 LA TROBE STREET
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NORTH
PROJECT TITLE
46-52 SMITH STREET, COLLINGWOOD, VIC
3066
CLIENT
THREE TOWERS GROUP

DRAWING TITLE
NORTH DEMOLITION ELEVATION - DERBY STREET
PROJECT NUMBER
XO20009
DRAWING NUMBER
A0004
REVISION STATUS
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REVISION NOTES

01 - THE DEMOLITION OF THE ENTRANCE CANOPY ADDED



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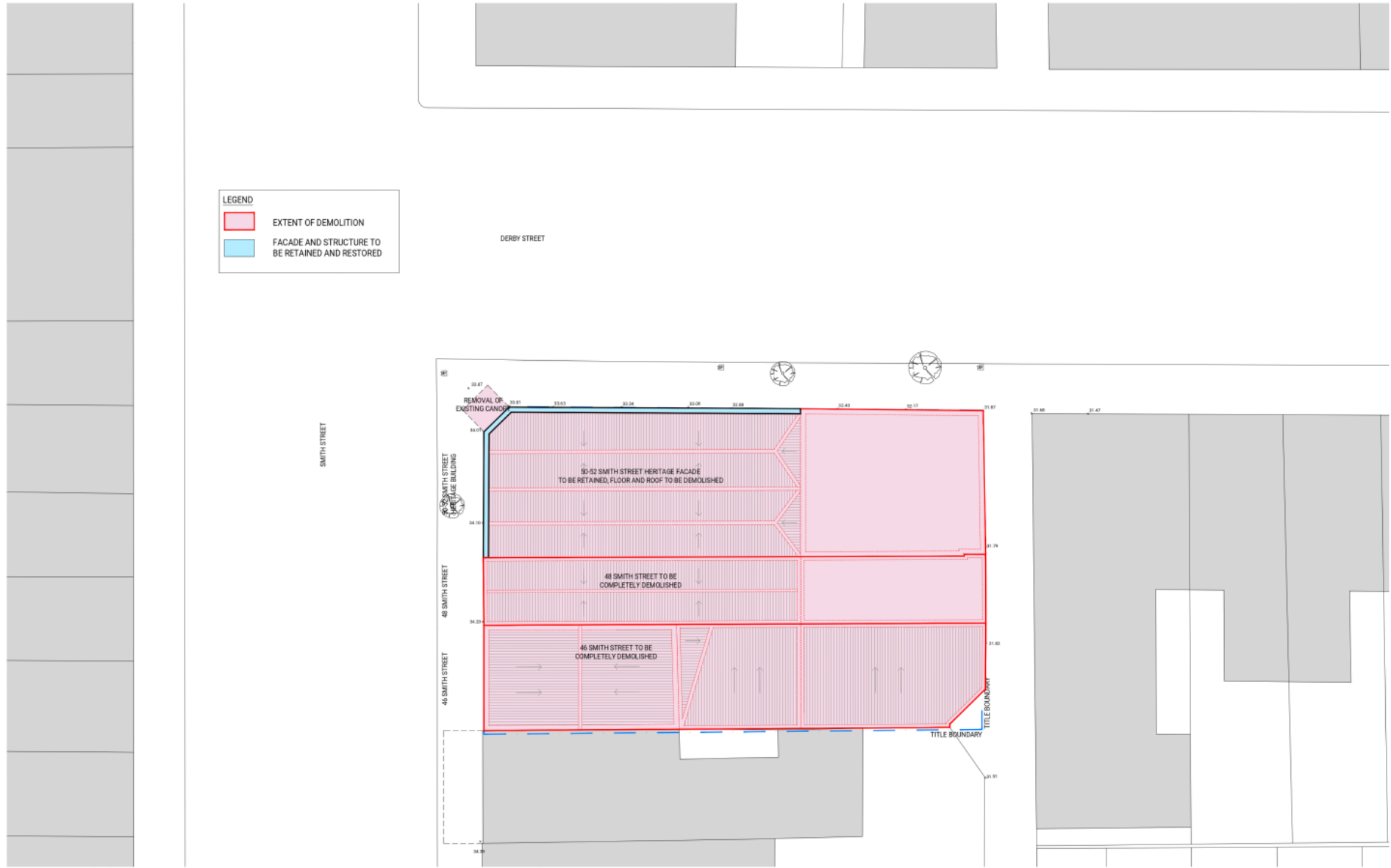
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3066
CLIENT
THREE TOWERS GROUP

DRAWING TITLE
EAST DEMOLITION ELEVATION - LANEWAY
PROJECT NUMBER
XO20009
DRAWING NUMBER
A0005
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CLIENT
THREE TOWERS GROUP

DRAWING TITLE
DEMOLITION PLAN

PROJECT NUMBER
XO20009

DRAWING NUMBER
A0006

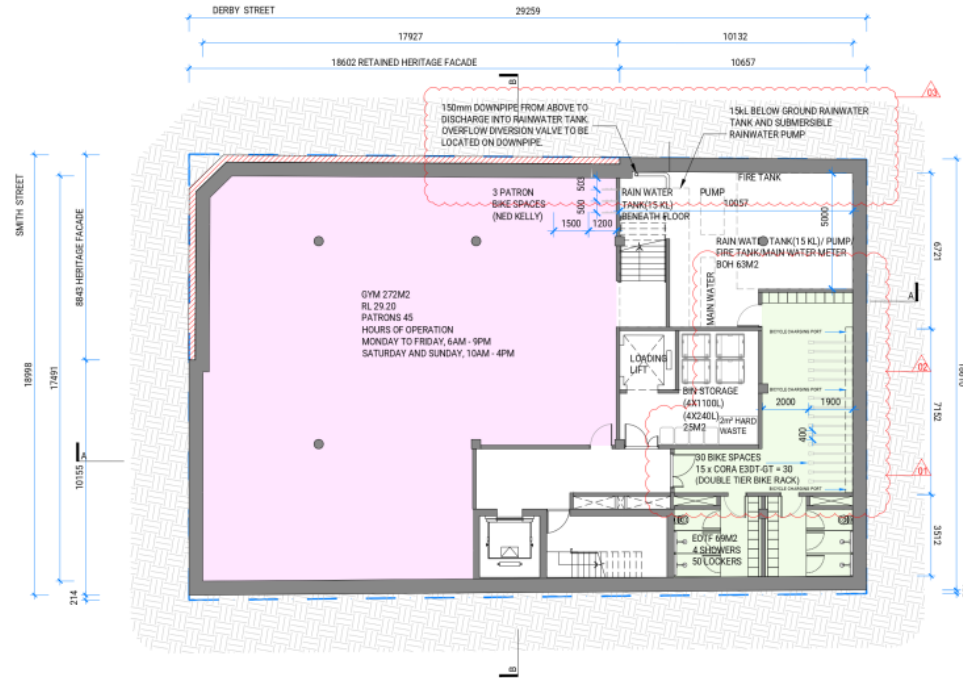
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REVISION NOTES

- 01 - THE ANNOTATION OF "30 BICYCLE SPACES 15 x CORA E3DT-GT +30 (DOUBLE TIER BIKE RACK)" ADDED
- 02 - THE LOCATION OF BICYCLE CHARGING PORT ZONE ADDED
- 03 - THE ANNOTATION OF THE RAINWATER TANK CONNECTION ADDED

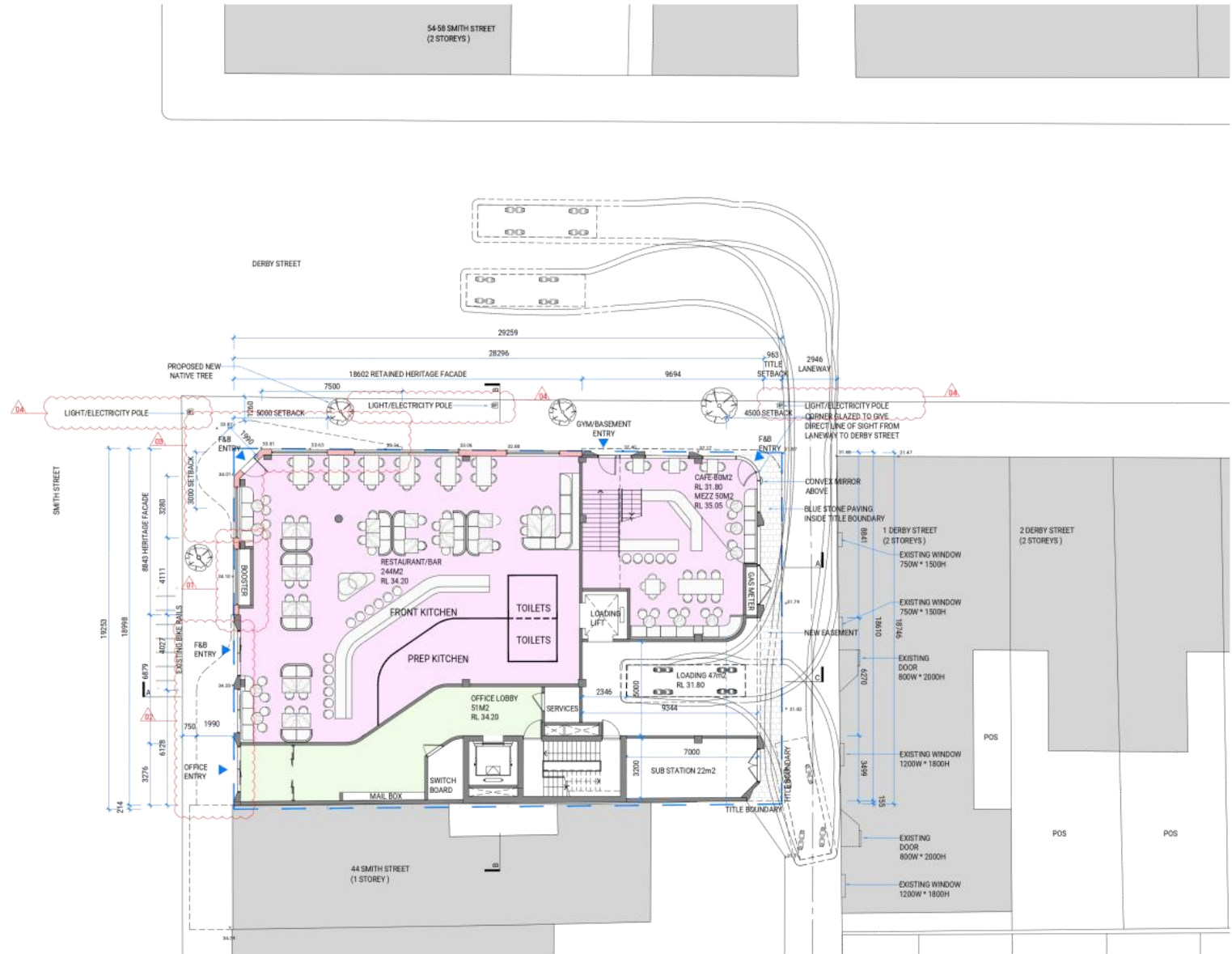


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	<p>Agenda Page 7</p>						

Attachment 1 - PLN21/0042 - 46 - 52 Smith Street Collingwood - Advertising S52 - Plans

REVISION NOTES

- 01 - THE DOOR OF FIRE BOOSTER CHANGED FROM METAL TO GLASS
- 02 - REDUCED SIZE OF CANOPY ALONG SMITH STREET TO COMPLY WITH THE 750 SETBACK FROM PEDESTRIAN KERB, THE DIMENSION OF CANOPY ADDED
- 03 - A SOLID CANOPY ON THE GROUND LEVEL AT THE NORTH WESTERN ENTRANCE ADDED, REFER TO WIND CONSULTANTS RECOMMENDATION
- 04 - ANNOTATION TO INDICATE THE LOCATION OF THE LIGHT AND ELECTRICITY POLE ADDED



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 XO 18 ES
 TEL +61 (0)399 237 462
 602/232 LA TROBE STREET
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 46-52 SMITH STREET, COLLINGWOOD, VIC 3066

CLIENT
 THREE TOWERS GROUP

DRAWING TITLE
 GROUND FLOOR PLAN L00

PROJECT NUMBER
 XO20009

DRAWING NUMBER
 A0100

DATE
 24/02/21

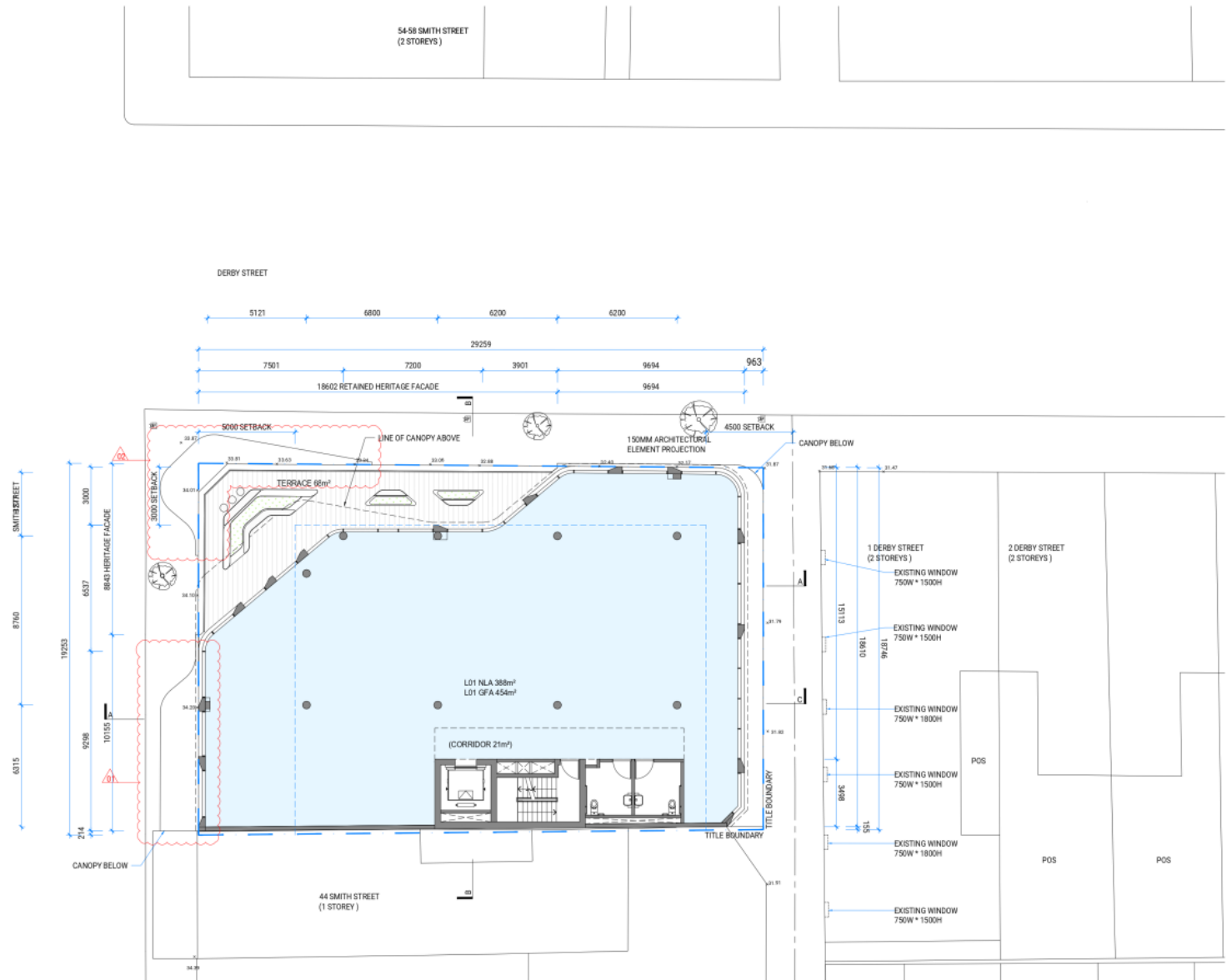
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REVISION NOTES

01 - REDUCED THE SIZE OF CANOPY ALONG SMITH STREET TO COMPLY WITH THE 750 SETBACK FROM PEDESTRIAN KREBS, THE DIMENSION OF CANOPY ADDED

02 - A SOLID CANOPY ON THE GROUND LEVEL AT THE NORTH WESTERN ENTRANCE ADDED, REFER TO WIND CONSULTANTS RECOMMENDATION



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CLIENT
THREE TOWERS GROUP

DRAWING TITLE
L01 FLOOR PLAN L01

PROJECT NUMBER
XO20009

DRAWING NUMBER
A0101

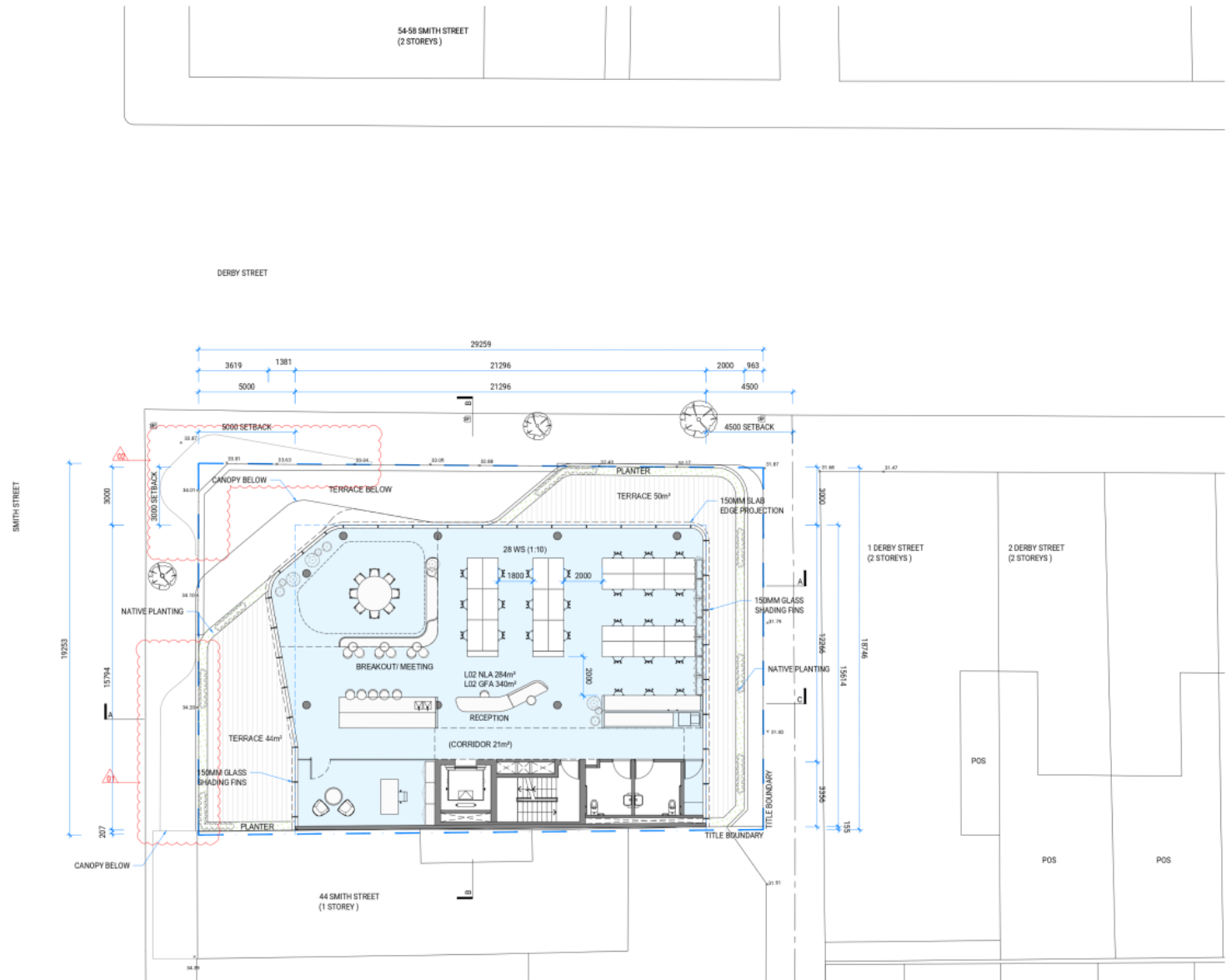
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CLIENT
 THREE TOWERS GROUP

DRAWING TITLE
 L02 FLOOR PLAN L02

PROJECT NUMBER
 XO20009

DRAWING NUMBER
 A0102

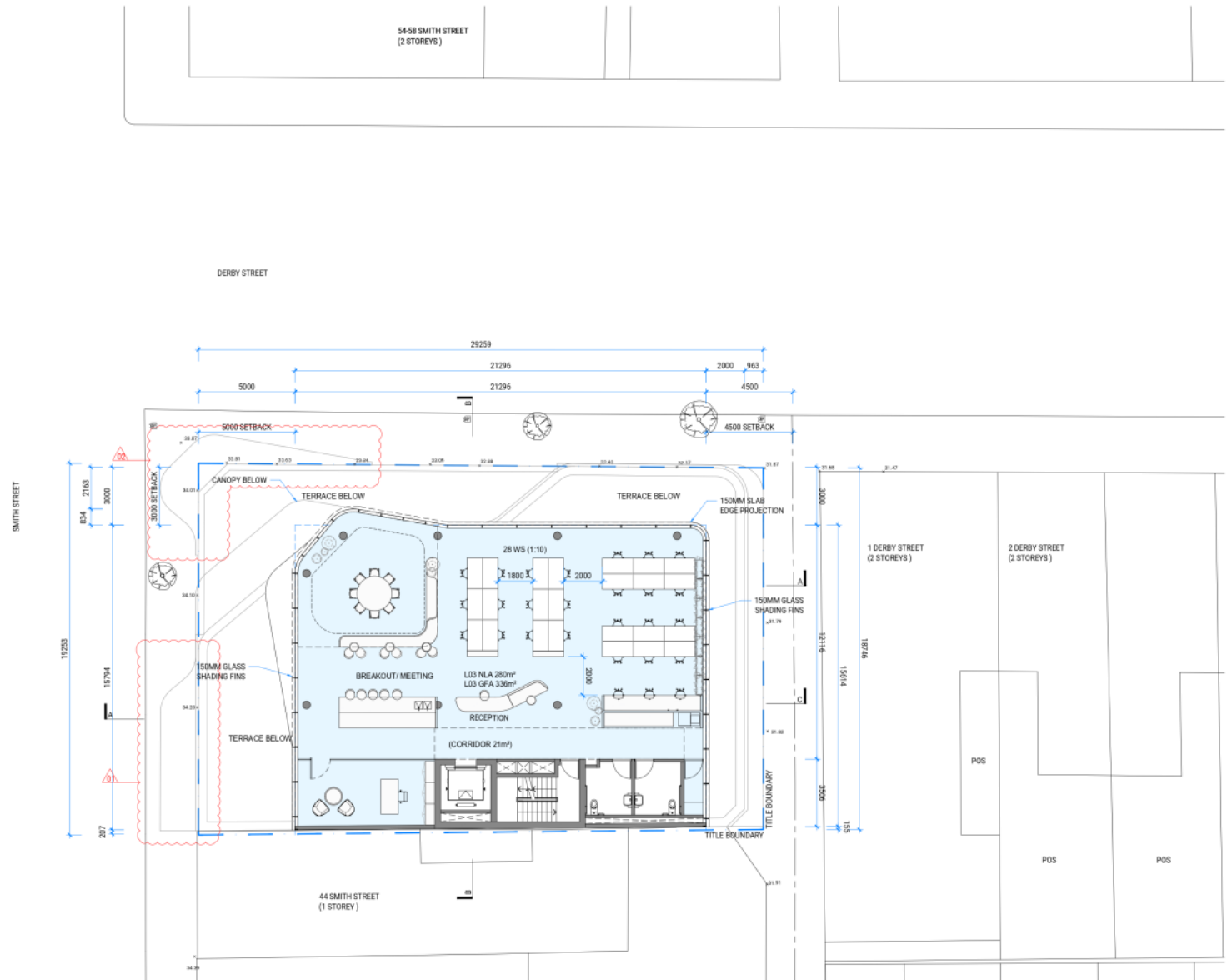
REVISION STATUS
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Attachment 1 - PLN21/0042 - 46 - 52 Smith Street Collingwood - Advertising S52 - Plans

REVISION NOTES

01 - REDUCED SIZE OF CANOPY ALONG SMITH STREET TO COMPLY WITH THE 750 SETBACK FROM PEDESTRIAN KERB, THE DIMENSION OF CANOPY ADDED

02 - A SOLID CANOPY ON THE GROUND LEVEL AT THE NORTH WESTERN ENTRANCE ADDED, REFER TO WIND CONSULTANTS RECOMMENDATION



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PROJECT TITLE
 46-52 SMITH STREET, COLLINGWOOD, VIC 3066

CLIENT
 THREE TOWERS GROUP

DRAWING TITLE
 L03/L05 FLOOR PLAN
 L03/L05

PROJECT NUMBER
 XO20009

DRAWING NUMBER
 A0103

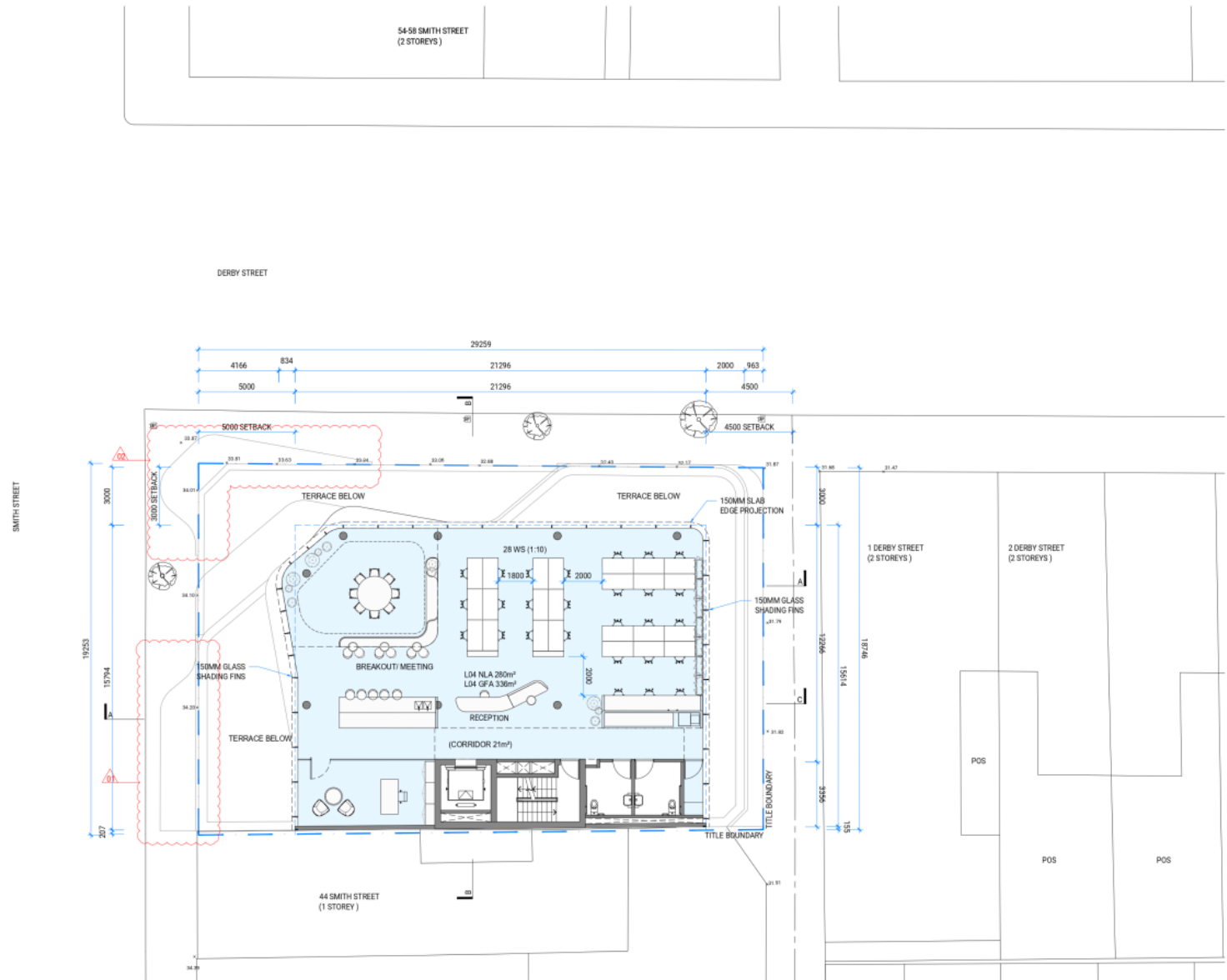
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REVISION NOTES

01 - REDUCED SIZE OF CANOPY ALONG SMITH STREET TO COMPLY WITH THE 750 SETBACK FROM PEDESTRIAN KERB, THE DIMENSION OF CANOPY ADDED

02 - A SOLID CANOPY ON THE GROUND LEVEL AT THE NORTH WESTERN ENTRANCE ADDED, REFER TO WIND CONSULTANTS RECOMMENDATION



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PROJECT TITLE
 46-52 SMITH STREET, COLLINGWOOD, VIC 3066

CLIENT
 THREE TOWERS GROUP

DRAWING TITLE
 L04 FLOOR PLAN L04/L06

PROJECT NUMBER
 XO20009

DRAWING NUMBER
 A0104

REVISION STATUS
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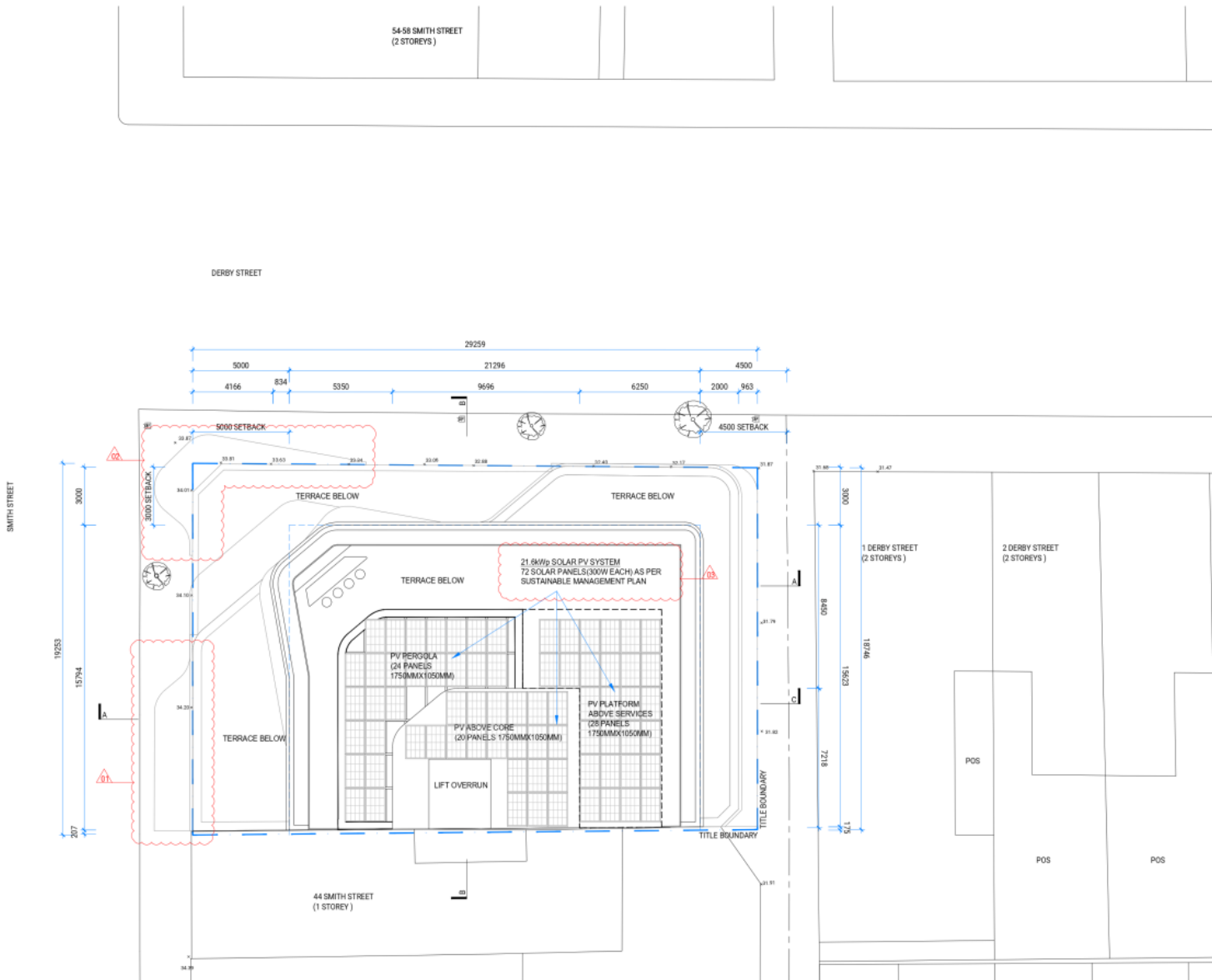
Attachment 1 - PLN21/0042 - 46 - 52 Smith Street Collingwood - Advertising S52 - Plans

REVISION NOTES

01 - REDUCED SIZE OF CANOPY ALONG SMITH STREET TO COMPLY WITH THE 750 SETBACK FROM PEDESTRIAN KERBS, THE DIMENSION OF CANOPY ADDED

02 - A SOLID CANOPY ON THE GROUND LEVEL AT THE NORTH WESTERN ENTRANCE ADDED. REFER TO WIND CONSULTANTS RECOMMENDATION

03 - THE ANNOTATION TO INDICATE THE CAPACITY OF SOLAR PANELS ADDED



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PROJECT TITLE
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CLIENT
 THREE TOWERS GROUP

DRAWING TITLE
 PV PLAN
 LOB

PROJECT NUMBER
 XO20009

DRAWING NUMBER
 A0108

REVISION STATUS
 TP

Attachment 1 - PLN21/0042 - 46 - 52 Smith Street Collingwood - Advertising S52 - Plans

REVISION NOTES

- 01- A SOLID CANOPY ON THE GROUND LEVEL AT THE NORTH WESTERN ENTRANCE ADDED, REFER TO WIND CONSULTANT'S RECOMMENDATION
- 02 - THE DOOR OF FIRE BOOSTER CHANGED FROM METAL TO GLASS
- 03 - AMENDED ELEVATIONS TO DEMONSTRATE THE EXTENT OF GLAZING PROPOSED ADJACENT TO THE HERITAGE PARAPET OF No.52 SMITH STREET

LEGEND

CODE	DESCRIPTION
GLS-01	GLAZING - LOW EMISSIVITY, DOUBLE GLAZED GREY TONED GLAZING. FRAMES: ALUMINIUM POWDER COATED DARK BRONZE
GLS-02	GLAZING - LOW EMISSIVITY, DOUBLE GLAZED CLEAR GLAZING. FRAMES: ALUMINIUM POWDER COATED DARK BRONZE
SP-01	SLAB EDGE DETAIL - DARK BRONZE ANODIZED ALUMINIUM
SP-02	BLACK MATT PV PANELS (BIPV)
SP-03	GREY/WHITE RENDER RAL 9002
SP-04	PLANTER BOX - DARK BRONZE ANODIZED ALUMINIUM
BR-01	LIGHT GREY/WHITE BRICK
BS-01	BLUE STONE PLINTH
HR-01	METAL HANDRAIL POWDER COATED DARK BRONZE
HR-02	CLEAR FRAMELESS GLASS HANDRAIL
SS-01	ALUMINIUM SCREEN POWDER COATED DARK BRONZE



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 XO 1315
 TEL +61 (0)399 237 462
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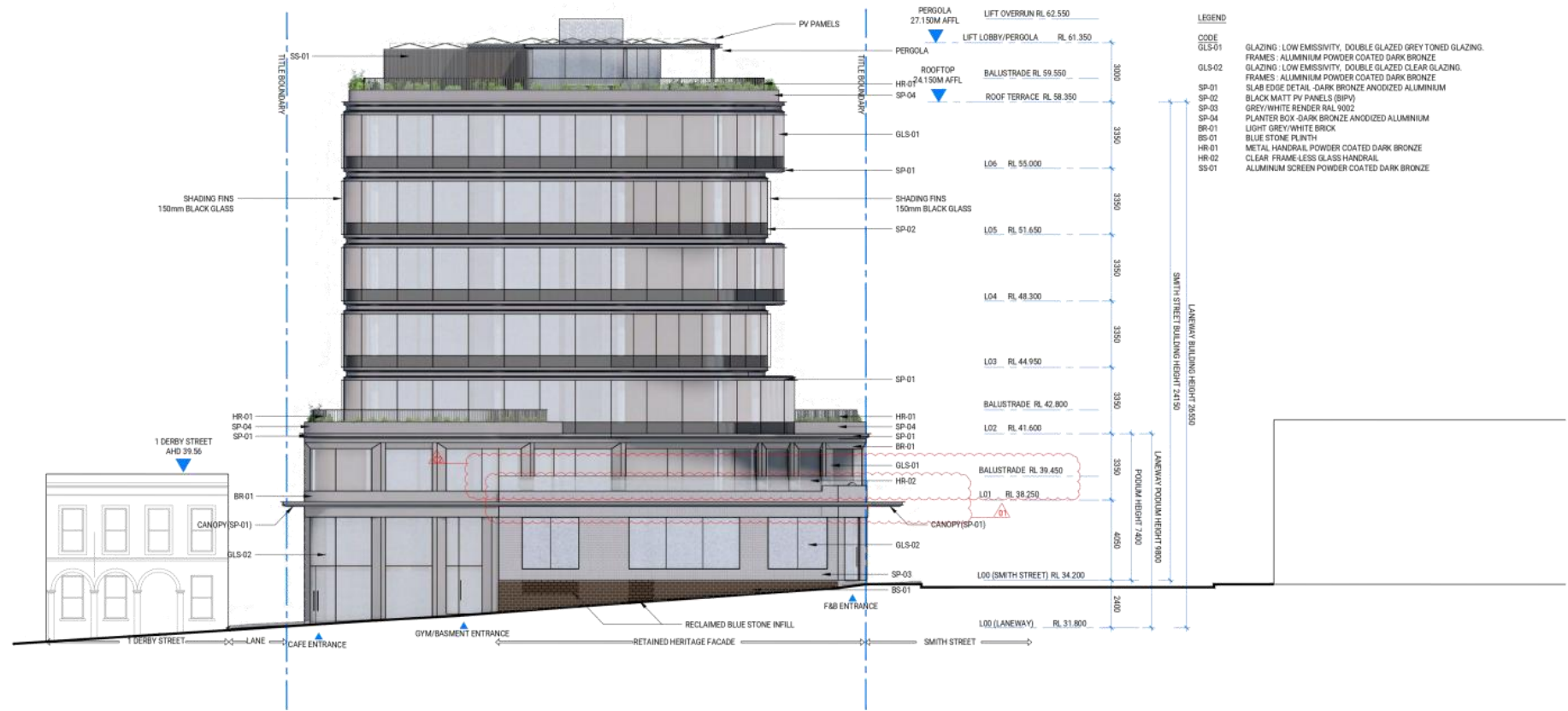
PROJECT TITLE
 46-52 SMITH STREET, COLLINGWOOD, VIC 3066
 CLIENT
 THREE TOWERS GROUP

DRAWING TITLE
 WEST ELEVATION - SMITH STREET
 PROJECT NUMBER
 X020009
 DRAWING NUMBER
 A0900
 REVISION STATUS
 TP

Attachment 1 - PLN21/0042 - 46 - 52 Smith Street Collingwood - Advertising S52 - Plans

REVISION NOTES

- 01- A SOLID CANOPY ON THE GROUND LEVEL AT THE NORTH WESTERN ENTRANCE ADDED, REFER TO WIND CONSULTANT'S RECOMMENDATION
- 02- AMENDED ELEVATIONS TO DEMONSTRATE THE EXTENT OF GLAZING PROPOSED ADJACENT TO THE HERITAGE PARAPET OF No.52 SMITH STREET



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CLIENT: THREE TOWERS GROUP

PROJECT TITLE: 46-52 SMITH STREET, COLLINGWOOD, VIC 3066

DRAWING TITLE: NORTH ELEVATION - DERBY STREET

PROJECT NUMBER: X020009

DRAWING NUMBER: A0901

DATE: 16/10/20

SCALE: 1/100

Attachment 1 - PLN21/0042 - 46 - 52 Smith Street Collingwood - Advertising S52 - Plans

REVISION NOTES

01- A SOLID CANOPY ON THE GROUND LEVEL AT THE NORTH WESTERN ENTRANCE ADDED, REFER TO WIND CONSULTANT'S RECOMMENDATION



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PROJECT TITLE
46-52 SMITH STREET, COLLINGWOOD, VIC 3066

CLIENT
THREE TOWERS GROUP

DRAWING TITLE
EAST ELEVATION - LANEWAY

PROJECT NUMBER
XO20009

DRAWING NUMBER
A0902

REVISION
01

DATE
24/02/21

SCALE
1/200

DATE
24/02/21

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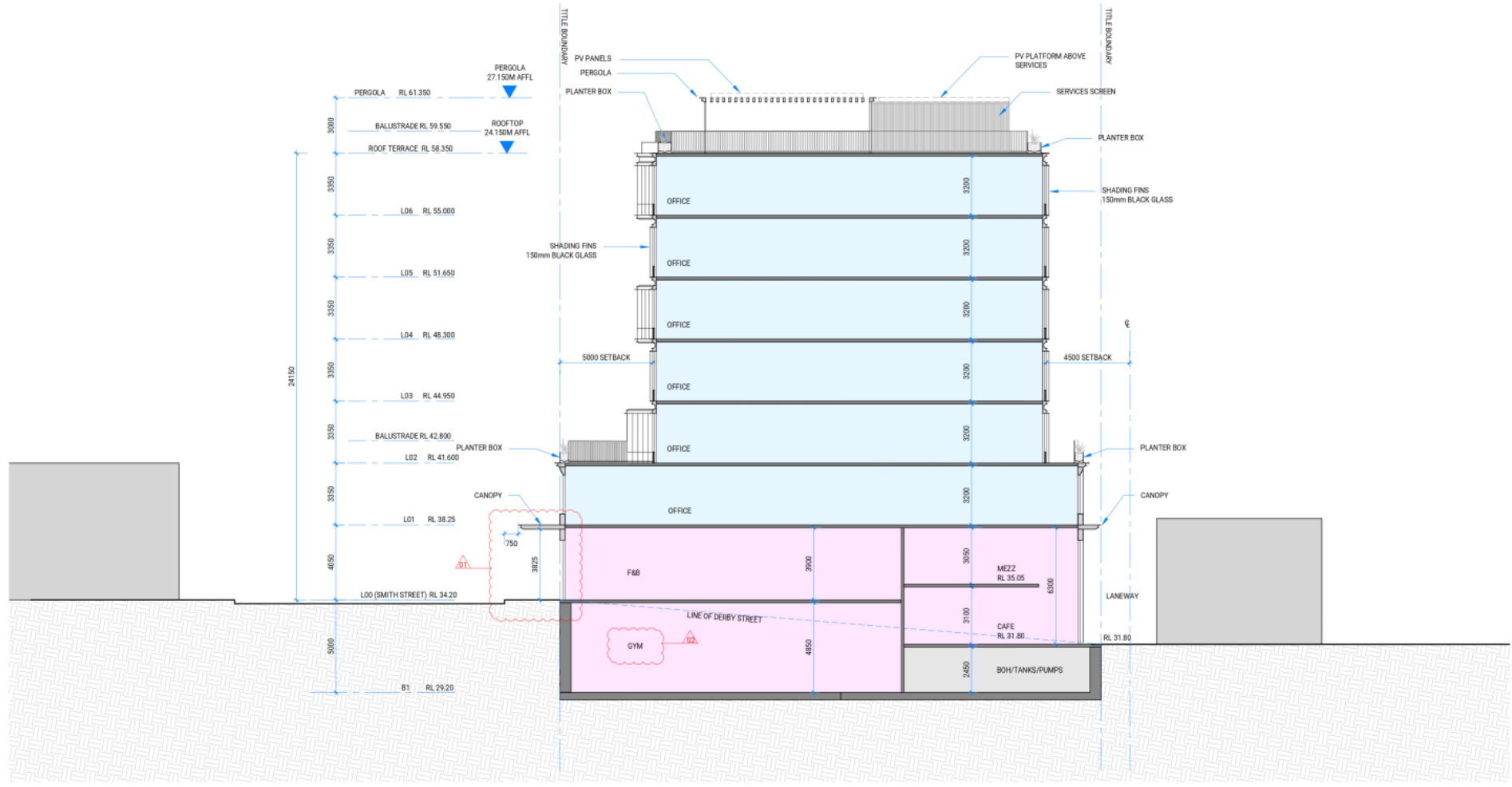
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24/02/21

SCALE
1/200

Attachment 1 - PLN21/0042 - 46 - 52 Smith Street Collingwood - Advertising S52 - Plans

REVISION NOTES

- 01 - REDUCED SIZE OF CANOPY ALONG SMITH STREET TO COMPLY WITH THE 750 SETBACK FROM PEDESTRIAN KERB, ADDED THE DIMENSION FOR THE HEIGHT OF CANOPY
- 02 - THE ANNOTATION OF BASEMENT CHANGED FROM F/B TO GYM



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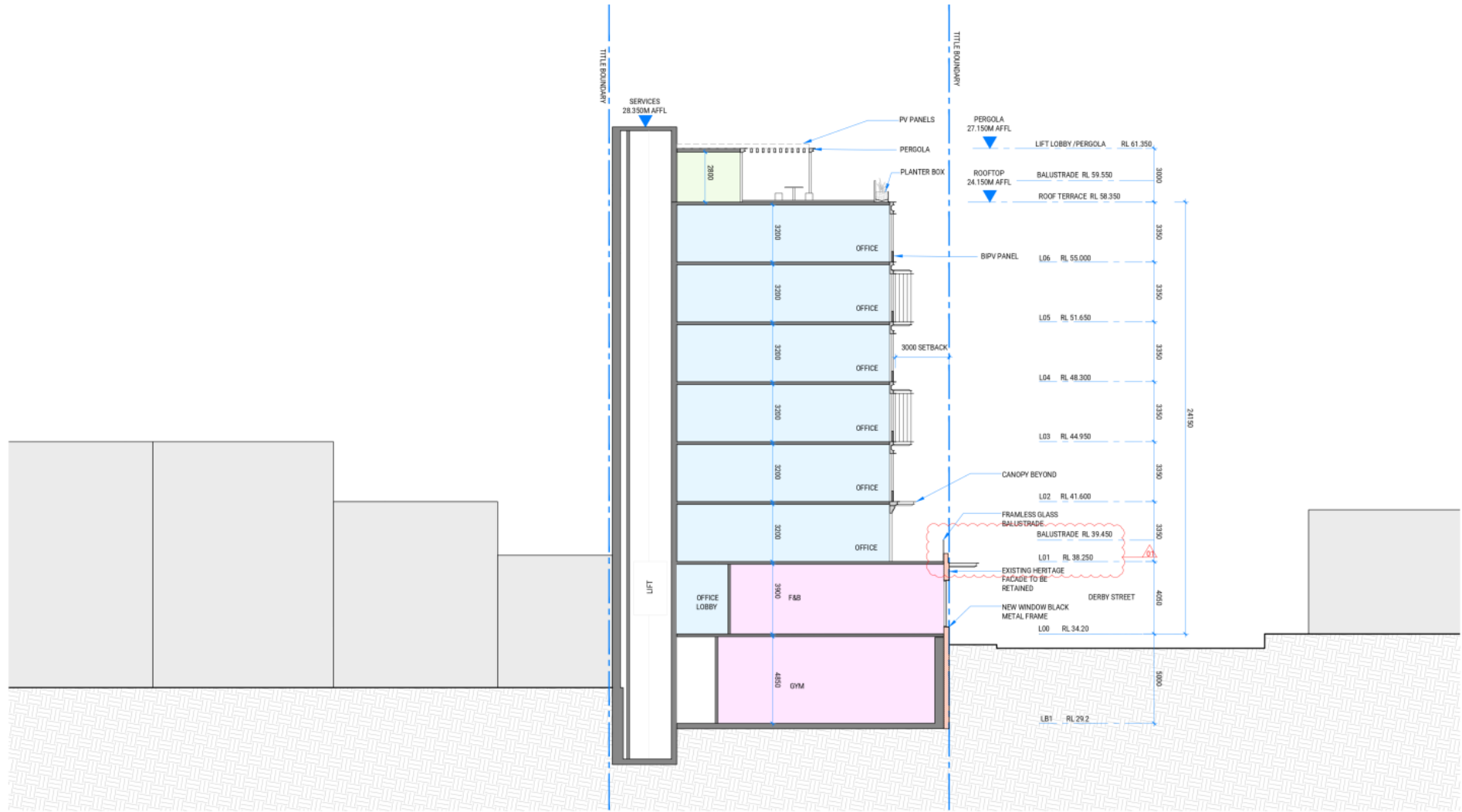
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 PROJECT TITLE
 46-52 SMITH STREET, COLLINGWOOD, VIC
 3066
 CLIENT
 THREE TOWERS GROUP

DRAWING TITLE
 SECTION AA
 PROJECT NUMBER
 X020009
 DRAWING NUMBER
 A0950
 SHEET NUMBER
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Attachment 1 - PLN21/0042 - 46 - 52 Smith Street Collingwood - Advertising S52 - Plans

REVISION NOTES

01 - UPDATED THE BALUSTRADE HEIGHT TO 1.2M



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PROJECT TITLE
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CLIENT
 THREE TOWERS GROUP

DRAWING TITLE
 SECTION BB

PROJECT NUMBER
 XO20009

DRAWING NUMBER
 A0951

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Attachment 2 - 46-52 Smith Street, Collingwood - Sketch Plan



Attachment 3 - 46-52 Smith Street, Collingwood - Wind referral comments



(ACN 004 230 013)

Ref: 45-21-DE-REV-00

05 May 2021

City of Yarra
PO Box 168
Cremorne VIC 3121

Attn: Lara Fiscalini

Dear Lara,

**46 – 52 Smith Street, Collingwood
Review of Global Wind Technology Services Pty Ltd (GWTS)
Desktop Pedestrian Level Wind Study
GWTS Document Number: GWTS-DPR-10180-2021-0
dated 5 March 2021**

The review of the Global Wind Technology Services Pty Ltd (GWTS) Desktop pedestrian level wind study is based on MEL Consultants' experience of wind flow around buildings and structures. This experience has been developed from a company experience of more than 40 years of desktop, wind tunnel, and full scale studies of environmental wind conditions in urban and sub-urban areas. No wind tunnel studies have been undertaken to support the review. Our comments are as follows:

- The GWTS Desktop Pedestrian Level Wind Study has been prepared based on the experience of the consultancy and no wind tunnel testing by GWTS has been carried out to support the report. MEL Consultants have no issue with this approach for a desktop study as this is a common approach to provide architects, developers, and responsible authorities advice on the wind effects of the design.

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Attachment 3 - 46-52 Smith Street, Collingwood - Wind referral comments

2

- MEL Consultants have no issue with the Analysis Approach, Site Exposure, and Regional Wind Climate that have been used as the basis for the assessment. GWTS has clearly identified the process for the desktop assessment and this is consistent with the approach that MEL Consultants would take to prepare a desktop wind impact assessment. A clear description of the 46-52 Smith Street Development, Collingwood has been provided along with reference drawings are listed in the Appendix of the report. The main building entrances are located on Derby and Smith Streets.
- MEL Consultants have no issue with the assessment criteria that GWTS have used for the desktop assessment. The recommended criteria for the immediate surroundings streetscapes would be walking comfort and standing comfort for the entrances to the building. The assessment clearly discusses the rationale for recommending the walking criterion for the terraces and there is no issue with this recommendation.
- GWTS have assessed that the wind conditions at the main entrances along Derby and Smith Streets would satisfy the standing comfort criterion, with the incorporation of a canopy of at least 1.5m wide at the northwest entrance. MEL Consultants would agree with the above assessment at entrances away from the northeast and northwest corners of the building but would query if the standing comfort criterion can be achieved at the northeast and northwest corner entrances with the proposed ~1m and ~2m wide canopies respectively. The downwash from the north and west face exposed to the strong northerly and westerly winds would be expected to flow around the corners and under the corner canopies. In MEL Consultants' experience, these canopies would need to extend further along Derby and Smith Streets to deflect downwash from the broad faces of the building. GWTS have noted that the canopy width at the northwest corner would need to be confirmed by a wind tunnel model study and MEL Consultants agree with this recommendation. MEL Consultants would also suggest extending the wind tunnel model study to confirm the wind conditions at the northeast corner entrance.

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Attachment 3 - 46-52 Smith Street, Collingwood - Wind referral comments

3

- GWTS has identified a substation entrance near the southeast corner of the building but have not provided an assessment of the wind conditions at that entrance. MEL Consultants would expect the wind conditions at the substation entrance to satisfy the standing comfort criterion.
- MEL Consultant also note that GWTS has not provided an assessment of the wind conditions in the streetscapes away from the entrances and whether these conditions would satisfy the recommended walking comfort criterion.
- GWTS have assessed the wind conditions on the terraces located on Levels 1, 2 and 7 may approach the walking comfort criterion and have recommended minimum balustrade heights of 1.2m for these terraces. GWTS have noted that these balustrade heights are to be confirmed by a wind tunnel model study and MEL Consultants have no issue with these recommendations.

In conclusion, the GWTS Desktop Pedestrian Level Wind Study has been prepared based on the consultant's experience of wind flow around buildings and structures. We have no issues with the Analysis Approach, Site Exposure, Regional Wind Climate, and description of the development used in the preparation of the assessment. This is consistent with the approach MEL Consultants would take to prepare a similar desktop environmental wind assessment. GWTS have assessed the wind conditions outside the building entrances as satisfying the standing criterion with the inclusion of a canopy near the northwest corner, the extent of which to be confirmed by a wind tunnel model study. GWTS have identified an entrance to the substation near the southeast corner of the building but have not provided an assessment whether the wind conditions at that entrance would achieve the standing comfort criterion. MEL Consultants, based on the assessment criteria, would expect the wind conditions at the substation entrance to satisfy the standing comfort criterion. GWTS have not provided an assessment whether wind conditions in the surrounding streetscapes would satisfy the walking criterion. GWTS have made recommendations for the minimum balustrade heights for the terraces to satisfy the walking criterion and have noted that the balustrade heights are to be confirmed by a wind tunnel model study. MEL Consultants

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Attachment 3 - 46-52 Smith Street, Collingwood - Wind referral comments

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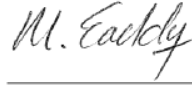
have no issue with the recommendations for the terrace balustrades and the confirmation by a wind tunnel model study.

Prepared by:



Y. Padayatchy
MEL Consultants Pty Ltd

Reviewed and Released by:



M. Eaddy
MEL Consultants Pty Ltd

TELEPHONE: (03) 8516 9680 : Intl +613 8516 9680 FAX: (03) 9544 0682: Intl +613 9544 0682

Attachment 4 - 46-52 Smith Street, Collingwood - Streetscapes comments



Memo

TO: Lara Fiscalini

Cc: Glen Williammes

From: Justin Bates

Date: 19 April 2021

Subject: PLN21/0042 – 46-52 Smith St Collingwood

diverse

vibrant

exciting

inclusive

Hi Lara,

The three street trees around the site have an appox value \$10,000, to cover the cost of amenity and replacement should they be damaged during the build.

A tree management plan will be required, as well as TPZ fencing as per AS4970.

There is a roadway tree that was remove due to being hit by a car in Derby st near the crn of Smith. This tree will be replaced this year during infill, and will also need to be protected. Should you require any more information, please contact me

Regards
justin

Attachment 5 - 46-52 Smith Street, Collingwood - Strategic Planning comments



Date: 7 June 2021

Property Address: 46-52 Smith Street, Collingwood

Application No: PLN21/0042

Officer: Joerg Langeloh

COMMENTS:

General

The site is within the requested interim DDO37 Smith St North and South as part of C272. The Minister for Planning has not made a decision at this point. As a result, the interim DDO37 is not part of the Yarra planning scheme at this point.

It is anticipated that the permanent amendment process be initiated later this calendar year.

Bulk Requirements of requested interim DDO37:

- The bulk requirements in the requested interim are:
 - o New discretionary street wall of max. 11.2m;
 - o Discretionary upper level setback of min. 8m;
 - o Discretionary max. building height of 20.8m (excl. lift overrun) – numerous conditions need to be met to exceed the discretionary building height, including not causing additional overshadowing;
 - o Common boundary separation of 4.5m, measured from the laneway;

Other Requirements:

- Also important to note is the mandatory overshadowing requirement to not overshadow the opposite footpath of Smith St at the equinox.
- Next to others, there are other design requirements in the interim DDO37, incl.:
 - o Horizontal and vertical emphasis;
 - o the avoidance of large expanses of horizontal glazing (except for the ground level); and
 - o achieving a suitable ratio of solid and void elements in the façade.

Conclusion

As a decision on interim DDO37 has not been made yet, it is Strategic Planning's understanding that the DDO cannot be relied upon for decision making as of yet.

Whether or not the overall height would be appropriate would depend on the analysis against the conditions for discretionary heights.

Attachment 5 - 46-52 Smith Street, Collingwood - Strategic Planning comments

The upper level setbacks from Smith and Derby St would not be compliant, but whether or not that could be appropriate depends on the overall assessment and how the proposal fits in with its surrounds.

The upper levels appear to over-emphasise the horizontal and rely on glazing. This could be amended by providing a different balance in solid & void elements through some vertical elements and warmer materials and colour.

Attachment 6 - 46-52 Smith Street, Collingwood - Open Space comments



Date: 13 April 2021

Property Address: 46-52 Smith Street, Collingwood

Application No: PLN21/0042

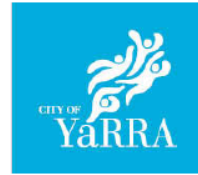
Officer: Julia Mardjuki

Open Space referral comments:

- (a) show the type, location, quantity, height at maturity and botanical names of all proposed plants;
- (b) provide details of the raised planters and terrace/rooftop planting (including planter box materials and dimensions, mulch layer – something that won't blow away, growing media – suitable in weight and content for roof top gardens, filter media, irrigation method, drainage system, root barrier / water proofing layer);
- (c) provide details of the *proposed method for irrigation and drainage*;
- (d) detail the *maintenance (duration, regime) and irrigation*;
- (e) show the materiality of the proposed spaces;
- (f) provide a specification of works to be undertaken prior to planting;
- (g) further detail on any sustainable treatments and water harvesting methods ;and
- (h) detail plant/planting maintenance schedules and requirements.

Attachment 7 - 46-52 Smith Street, Collingwood - Internal UD comments

MEMO



To: Lara Fiscalini (Statutory Planning)
From: Christian Lundh (Urban Design)
Date: 16 April 2021
Site Address: 46-52 Smith Street, Collingwood
Application No: PLN21/0042
Description: Full demolition of 46 & 48 Smith Street and partial demolition of 50-52 Smith Street, construction of an 8-storey development, use of the land for restricted recreation facility (gymnasium), food and drink premises and office (permit required for gymnasium) and a reduction in the statutory car parking rate.

COMMENTS SOUGHT

Urban Design comments have been sought on following matters:

- Public realm interface to Smith Street and Derby Street.
- Whether there are any capital works approved or proposed within the area of the subject site.

These comments are provided on Plans:

- *Architectural plans and elevations, prepared by XO Projects, dated 24 February 2021*
- *Landscape plan, prepared by XO Projects, dated 11 March 2021*

The extent of this review is limited to the proposed development's integration with the streetscape and public realm and excludes landscaping within the building and rooftop landscapes.

COMMENTS SUMMARY

This proposal is supported in principle, subject to the improvements outlined below, including the following:

- A landscape plan to be submitted to Council for review prepared by a suitably qualified Landscape Architect or Landscape Designer.
- All existing street trees adjacent to the subject site to be shown as retained and shown on all ground floor plans.
- Provision of additional on street bike parking on Derby Street and within the subject site boundary by the rear laneway.

Attachment 7 - 46-52 Smith Street, Collingwood - Internal UD comments

There are no known planned/approved capital works around the site being led by the Urban Design Team at the date of preparing this memo.

Additional Information Required

The following details are required on the drawings:

- Footpath dimensions.
- The exact location of the existing streetscape fixtures and street trees.
- On-street parking bays.
- Drainage including any side entry pits.

Additional details required on the drawings are also set out in the relevant sections below.

URBAN DESIGN COMMENTS

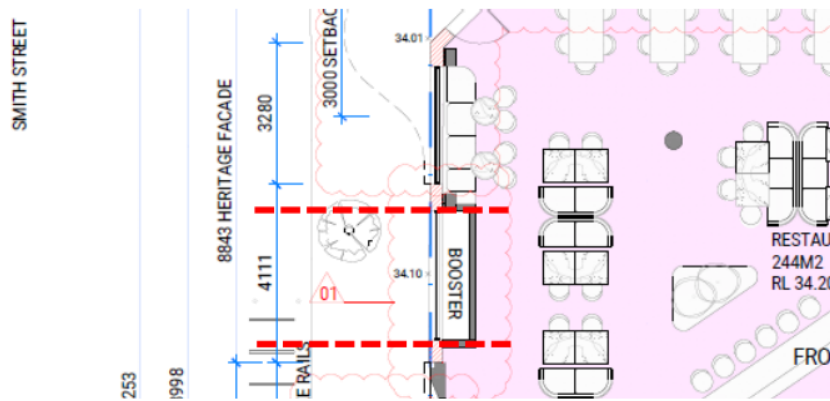
Ground Floor Interface

The ground floor interfaces are in principle supported, with the F&B venues activating the street frontages including the threshold to the laneway from Derby Street.

- Confirmation is required that the booster location is compliant with all relevant standards and access requirements, given that the proposed location has not a clear access from the street with bot an existing tree and existing on street bike rails within the access alignment (as indicated with the red lines below).

Tree removal / pruning to accommodate any access requirements to the booster will not be acceptable.

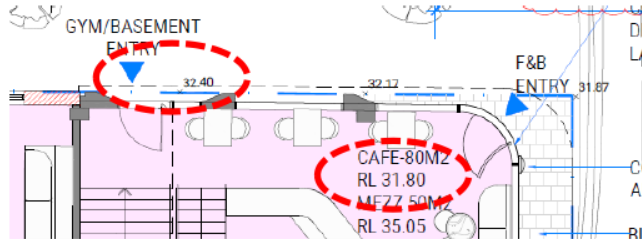
Repositioning of the existing bike rails to accommodate any access requirements must be negotiated including but not limited to Council Strategic Transport, Traffic, and Engineering teams accordingly.



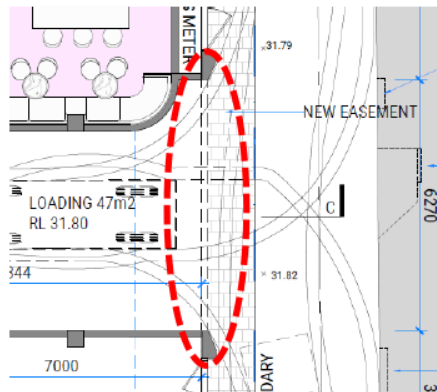
Attachment 7 - 46-52 Smith Street, Collingwood - Internal UD comments

Pedestrian and Vehicular Entrances

- Confirmation that the gym entrance design and be DDA compliant given the noted level difference between the existing footpath and proposed internal floor RL is 600mm, as highlighted in screenshot below.
 - If any steps, handrails, tactiles will be required, then these must be accommodated for within the subject sites boundary and must not protrude or be located within the public realm/footpath space.



- Confirm that the location for the proposed roller door to the loading area from the laneway is in-line with the dash lines on the plan, no call out on plan noted. Concern if the roller door is setback from the building line that a space for loitering will be created.



- Confirm if there will be any façade / external lighting in the laneway to improve the safety and amenity in this space, noting that consideration regarding light-spill into adjacent properties must be considered.

Street Furniture & Fixtures

Recommended integration of public accessible bike hoops (refer locations in screenshot overleaf);

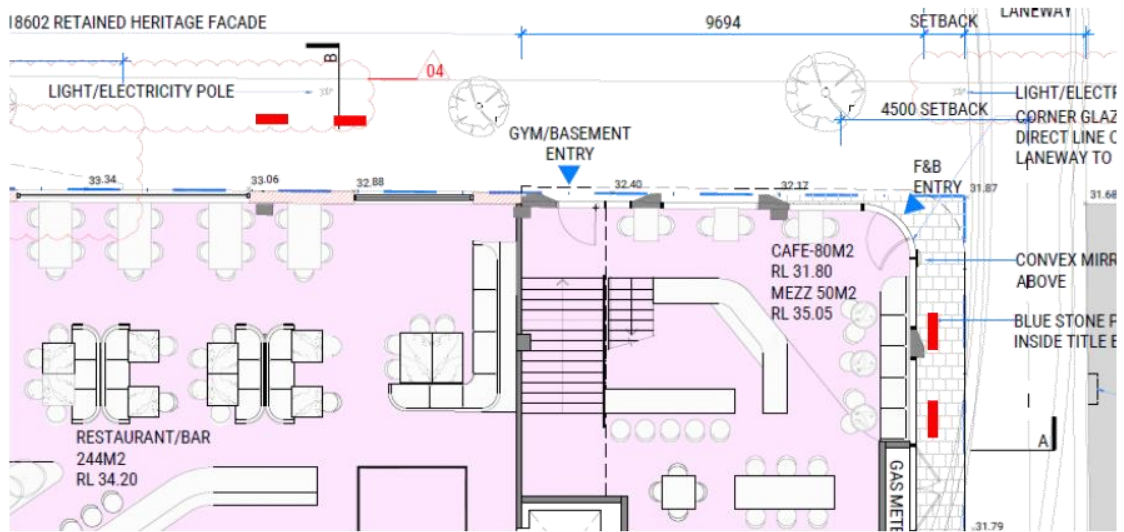
- Derby Street - two (2) no bike hoops on either side of the existing power pole, pending underground services assessment; and
- Rear laneway - two (2) no bike hoops located parallel with building within the subject site boundary, ensuring access requirements to gas meter and swept path clearances are not impaired.

The YCC standard details are currently being revised to ensure compliance with Australian Standards and required offsets are more apparent. Please refer to UD Standard Details - WIP-Bike Hoops drawing (attached) and further consider below set-out dimensions;

- As per Australian Standards bike hoops need a 1 metre by 1.8 metre clearance around them to allow enough space for parked bikes; and

Attachment 7 - 46-52 Smith Street, Collingwood - Internal UD comments

Distance between two bicycle boxes should be 0.6m to allow safe access to cyclist parked on the kerbside without a need to walk on the tree pit and/or the garden bed area.



Pavements

- Confirm paving material along slivers between building and asphalt footpaths.
- All pavements along Smith Street, Derby Street are to be reinstated to match the existing with asphalt pavement with bluestone kerbs and channels as per City of Yarra’s Infrastructure – Road Materials Policy.
- Further details are requested regarding the rear laneway including but not limited to the following items;
 - Proposed bluestone paving details within the subject site boundary including interface detail to the laneway asphalt pavement; and
 - Additional levels and drainage design for the laneway including potential requirements for a central bluestone channel as per Council standard details noting that this will require further review by Council engineering team.
 - All proposed pavement to comply with relevant Australian Standards including slip resistance.

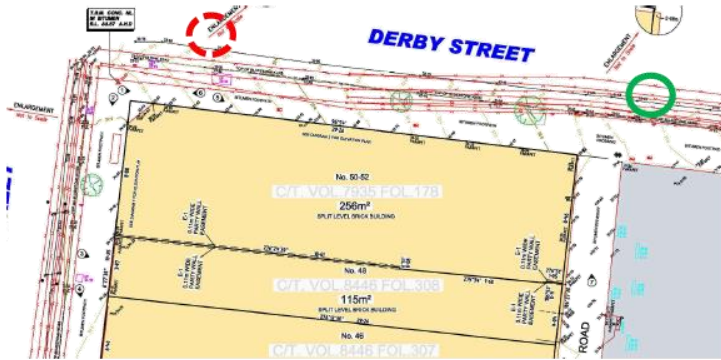
Street Tree Planting

- The tree highlighted in street view below on the corner Derby Street and Smith Street, is missing from the survey including tree cut out. From desk top review of aerials, the tree appears to have been removed. It can be anticipated that Council will replace the tree like for like in the same position in due course.



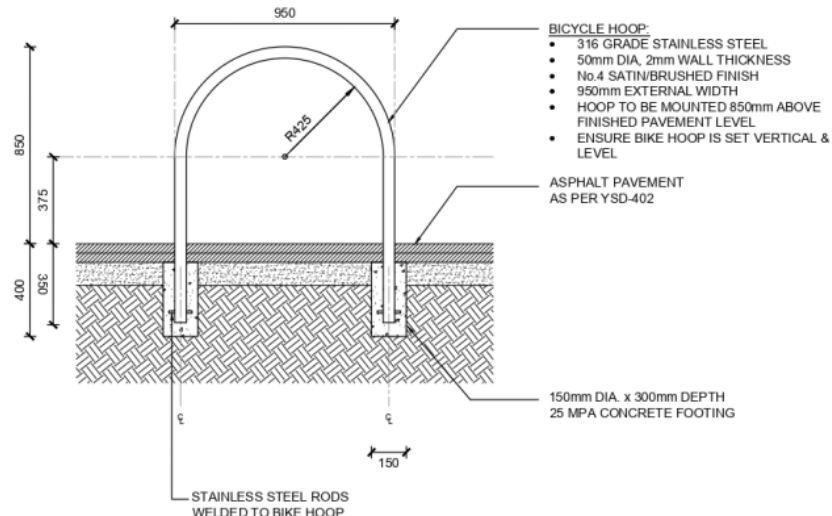
Attachment 7 - 46-52 Smith Street, Collingwood - Internal UD comments

- Show all existing trees and tree cut outs affected by the proposal, including cut out for 'missing' tree as noted in aforementioned comment, refer indicative mark-up below.



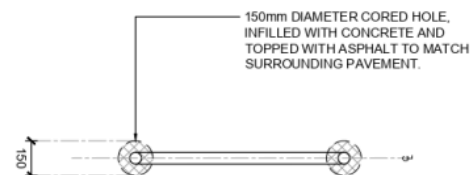
- The tree shown on the plans as proposed in the footpath by the corner on Derby Street and Smith Street, will not be required in lieu of the anticipated replacement of a removed street tree as per aforementioned comments.
- Tree protection plan must be submitted to Council for review.
- Note that trees managed by the Council must be protected during any construction activities in accordance with the Australian Standard for the Protection of Trees on Development Sites (AS 4970-2009).

Attachment 7 - 46-52 Smith Street, Collingwood - Internal UD comments

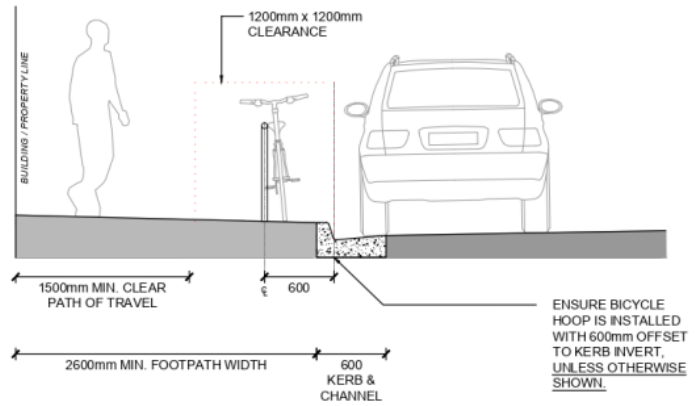


1 BICYCLE HOOP CONSTRUCTION - SECTION DETAIL
1:20

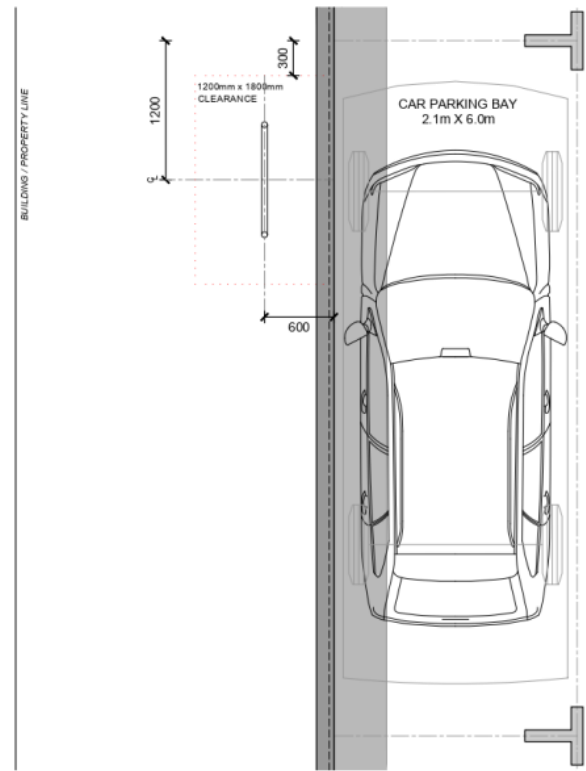
FOR INSTALLATION INTO EXISTING PAVEMENT:



2 BICYCLE HOOPS CONSTRUCTION - PLAN DETAIL
1:30



3 BICYCLE HOOP TYPICAL SETOUT - SECTION DETAIL
1:40



4 BICYCLE HOOP TYPICAL SETOUT - PLAN DETAIL
1:40

LEGEND

REV	BY	DESCRIPTION	DATE
		<p>WARNING BEWARE UNDERGROUND SERVICES</p> <p><small>THE LOCATION OF EXISTING SERVICES SHOWN ON THESE PLANS ARE APPROXIMATE ONLY & ARE THEIR EXACT POSITION SHALL BE PROVEN FIRST. THE APPROPRIATE AUTHORITY SHALL BE CONTACTED AND THE SERVICES LOCATED PRIOR TO COMMENCEMENT OF ANY WORK. NO GUARANTEE IS GIVEN THAT ALL EXISTING SERVICES ARE SHOWN.</small></p>	

PRELIMINARY

NOT FOR CONSTRUCTION

YARRA CITY COUNCIL
PO BOX 188
VIC 3046
T: 03 9333 5000
W: yarracity.vic.gov.au

CITY OF YARRA

PROJECT

PROJECT

DRAWING TITLE

FURNITURE DETAILS:
BICYCLE HOOPS

SCALE	SIZE	DESIGNED	DRAWN
AS SHOWN	A3	XX	XX
		CHECKED XX	APPROVED XX
		REV	DATE
		REV	DD/MM/YY

ORIENTED TO NORTH

####

Attachment 8 - 46-52 Smith Street, Collingwood - Heritage referral comments

ANITA BRADY HERITAGE

PO Box 1108
 Collingwood, 3066
anita@abheritage.com.au
 TEL 0411 057 271
 ABN 21 157 418 354

MEMORANDUM

TO	Lara Fiscalini	FROM	Anita Brady
RE	46-52 Smith Street, Collingwood	DATE	20 May 2021

Introduction

This memorandum has been prepared for the City of Yarra and includes a heritage assessment of proposed works to 46-52 Smith Street, Collingwood. The works include demolition of non-contributory buildings, partial demolition of contributory heritage buildings and works/modifications to the retained fabric, and construction of a seven/eight-storey building.

The assessment addresses the heritage considerations associated with the proposal, with the following tasks undertaken:

- Review of application documentation including:
 - Architectural plans and drawings, including a landscape plan, prepared by XO Projects (February 2021)
 - Heritage Impact Statement, prepared by Bryce Raworth Pty Ltd (December 2020); the HIS report contains a history and overview of the property which is cited below
 - Town Planning Report, prepared by Urbis (December 2020); this report includes montages/renderings of the proposed development
 - Urban Context Report, prepared by XO Projects (March 2021).
- Inspect property (external from the public realm) and the immediate heritage precinct/heritage context of the property.
- Review the Yarra Heritage Overlay considerations, including Clause 22.02 Development Guidelines for Sites Subject to the Heritage Overlay, Clause 43.01 Heritage Overlay and relevant heritage studies and reports.

Heritage considerations

The property is in the Smith Street South Precinct (HO464), which continues (and adjoins) to the north, south and west of the property (see Figure 1). As per the City of Yarra Review of Heritage Overlay Areas 2007 Appendix 8, the property gradings are as follows:

- Nos 46, 48 and the rear of 48 Smith Street are not contributory
- No 50 is contributory
- No 52 (to the corner and described as 'shop and residence') is contributory

Nos 50-52, and the rear boundaries of the other buildings on the site, adjoin (to the east) HO98 which is 'Derby House', at 1 Derby Street; and east again HO100 which is a Victorian terrace row at 3-7 Derby Street. The proposed development site, therefore, is effectively surrounded by properties included in the Heritage Overlay.

Attachment 8 - 46-52 Smith Street, Collingwood - Heritage referral comments

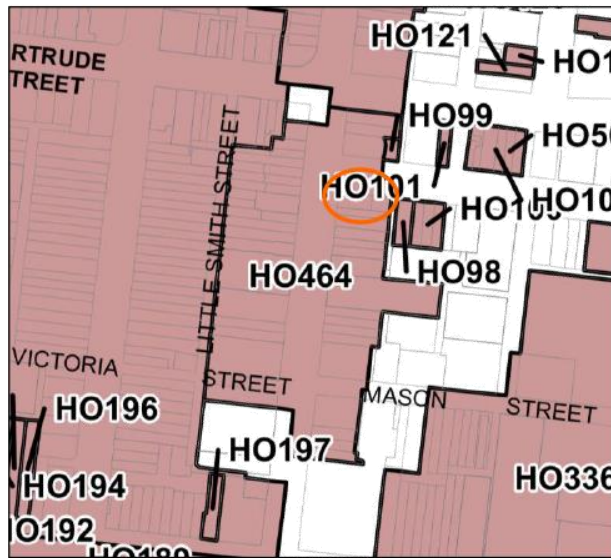


Figure 1 Extract from Heritage Overlay map, with subject property indicated
Source: Planning Schemes Online

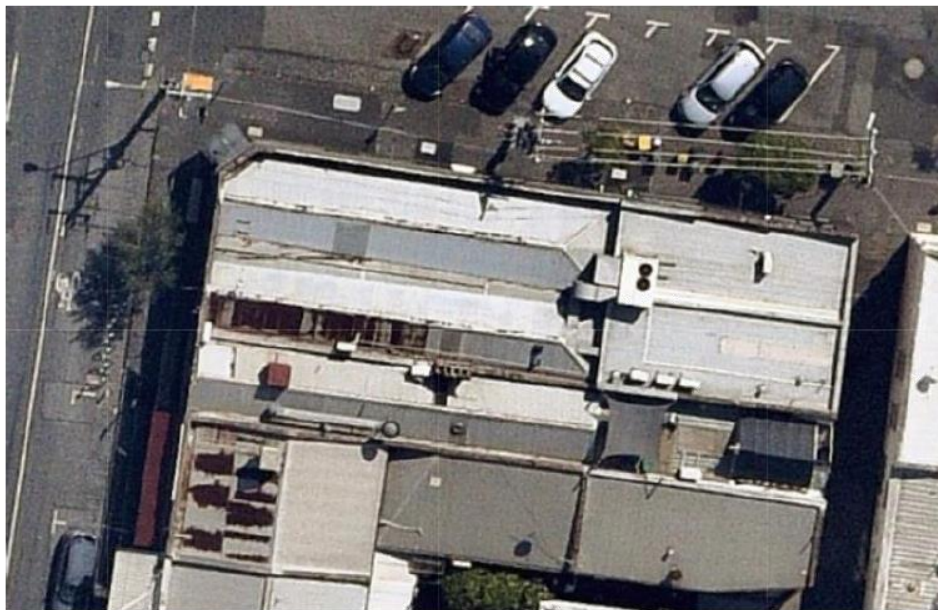


Figure 2 Recent aerial image of subject property
Source: Nearmap March 2021

Attachment 8 - 46-52 Smith Street, Collingwood - Heritage referral comments

ANITA BRADY HERITAGE



Figure 3 Subject property, western presentation to Smith Street; the contributory 50-52 Smith Street is at left



Figure 4 Subject property, northern presentation to Derby Street; showing the long north elevation of 52 Smith Street

Attachment 8 - 46-52 Smith Street, Collingwood - Heritage referral comments



Figure 5 Subject property, corner presentation



Figure 6 Derby Street, with east end of subject property at right

Attachment 8 - 46-52 Smith Street, Collingwood - Heritage referral comments**Description***Subject property*

The property comprises to its western half (Smith Street) several single-storey rendered and/or overpainted brick retail/commercial buildings (shops) with hipped and/or gabled roof forms. On its eastern half, and allowing for the drop in grade, the property comprises two-storey brick buildings (rear wings) which adjoin a narrow lane on the east side of the property. Derby Street adjoins the north side of the property. The property has no setbacks to the street or laneway frontages.

According to the Raworth HIS report (pp.3-4):

Sands and McDougall Directories indicate that the shop at 46 Smith Street dates to c1875-1880 with three shops at 48-52 Smith possibly built c1885-1890 as a matching group. An 1899 MMBW plan shows all of the shops with a continuous verandah to Smith Street and cellars under the rear wings. The MMBW plan also indicates that the shops formed part of a mixed streetscape containing a significant proportion of residential buildings, as well as parcels of vacant land. This is in marked contrast to Smith Street north of Gertrude Street where the tram line provided the impetus for intensive commercial development, including grand retail emporia. In the early decades of the twentieth century the dwellings on Smith Street south of Gertrude Street were gradually replaced with retail buildings and some small factories and workshops.

And:

In terms of the current state of the buildings on the subject site, the verandah to Smith Street has long since been removed and the shopfronts at 46 and 48 Smith Street have been entirely rebuilt (presumably in the post war period), leaving no evidence of their original Victorian architectural character. The shops at 50-52 Smith have a fairly plain rendered masonry Victorian facade with a quarry faced bluestone plinth to the Derby Street elevation. The hipped corrugated iron roofs are concealed by a parapet that is plainly detailed apart from a moulded string cornice and anthemion motifs. The corner door is modern and the shopfront to 50 Smith Street has been rebuilt without an entrance. Alterations to the Derby Street elevation include non-original window openings and the raising of the parapet using face brickwork. The double-storey wing on the Derby Street elevation may have been built c1940 when 50-52 Smith Street was occupied by an upholster.

Figures 5 and 7 of the Raworth HIS report identify original and modified elements of the Smith and Derby streets elevations of 50-52 Smith Street.

In summary, the contributory buildings at 50-52 Smith Street date from the period c.1875 to 1880; have modified rendered masonry facades, including modified openings, with a bluestone plinth to Derby Street; and have plainly detailed parapets with a moulded string cornice and anthemion motifs, which largely conceal the hipped roof forms. There is a chamfered corner entrance with a modern door set under a modern box awning. The Derby Street elevation of no. 52 is also modified with later windows and a door opening and a parapet which has been raised in brickwork. The rear two storey wings likely date to c.1940.

Attachment 8 - 46-52 Smith Street, Collingwood - Heritage referral comments*Heritage context*

As noted, the subject property is in the Smith Street South Precinct (HO464) and is adjoined to the east by the individual HO98 which is 'Derby House' at 1 Derby Street; and east again by HO100 which is a Victorian terrace row at 3-7 Derby Street.

The east side of Smith Street, in the HO464 precinct, was formerly in the HO333 Smith Street Precinct. Further, the current precinct citation and statement of significance for HO464¹ largely concentrates on the west side of Smith Street in Fitzroy, which was added to the Heritage Overlay to form HO464.

Acknowledging this, the subject precinct is largely of commercial heritage character, with buildings and development from the nineteenth century, together with some later development. As with HO333 further north, the boundary largely follows the rears of properties to both the Fitzroy and Collingwood sides of the street. Valued development in the precinct is mostly of brick and/or render construction, of two storeys, with some more limited single-storey buildings (as per the subject site). The ground floors (shopfronts) have mostly been altered, while the parapeted first floors tend to be more intact. When viewed from Smith Street there is rarely a sense of the depth of buildings or blocks excepting the corner sites, again as per the subject property. There is some recent infill/residential development in the precinct, of three to four storey scale, including some visible development to the rears of buildings fronting Smith Street, albeit not in proximity to the subject site.

Adjoining to the east, over the laneway, is 'Derby House' (HO98) at 1 Derby Street, also known as 'Adamson House'. This dwelling dates from about 1874 and is described as:²

...a two storey double-fronted asymmetrical rendered brick house, attached to the terrace at Nos. 3-7. The ground floor has a three-bay arcaded loggia, with moulded heads and keystones, at the eastern end, and a single window on the street alignment to its right. The first floor comprises four single windows with rendered moulded dressings and bracketed sills. There is a dentillated moulded string course at first floor level, and the first floor has quoining. The parapet has a moulded cornice. The windows are timber-framed double-hung sashes.

In terms of significance:

The house at 1 Derby Street is of local architectural significance. It is an unusual example of a double-fronted, arcaded terrace-style house, which forms a visual unit with the similarly-styled terrace at Nos. 3-7. As a group these buildings are an important heritage element in an area of Collingwood which has been substantially eroded by later development.

Regarding the terrace row at 3-7 Derby Street (HO100), this is referred to in the above quotes, and as:³

The terrace at 3-7 Derby Street, Collingwood, is a row of three attached double storey rendered brick houses, attached to Derby House at No. 1. The ground floors each have three-bay arcaded loggias, with moulded heads and keystones. The first floors each comprise three single windows with rendered moulded architraves and bracketed sills. There is a dentillated moulded string course at first floor level, and the first floor has quoined

Attachment 8 - 46-52 Smith Street, Collingwood - Heritage referral comments

panels dividing each dwelling's facade. The parapet has a moulded cornice. The windows are timber-framed double-hung sashes.

In terms of significance:

The houses at 3-7 Derby Street is of local architectural significance. It is an unusual and elegant example of an arcaded terrace, which forms a visual unit with the similarly-styled Derby House at No. 1. As a group, these buildings are an important heritage element in an area of Collingwood which has been substantially eroded by later development.

Proposed works

The proposed works include demolition of non-contributory buildings, partial demolition of contributory heritage buildings and works/modifications to the retained fabric, and construction of a seven/eight-storey building.

The demolition of the non-contributory buildings is not further described here.

The proposed partial demolition of the contributory heritage buildings at 50-52 Smith Street involves all the roof forms and roofing materials, and the rear wings including the north elevation of the later two-storey wing of no. 52 which is visible to Derby Street. The façade to Smith Street will be retained, together with the chamfered corner, and a section of the north elevation of no. 52 (extending for some 18m along Derby Street).

Works to the retained walls and fabric of 50-52 Smith Street include:

- replacement of a large infilled (blind) shopfront window to the façade of no. 50 with a fire booster cupboard
- removal of the modern box awning over the chamfered entrance
- works to the fabric and openings on the north elevation of no. 52 including:
 - (middle section of elevation) removal of wall fabric and door, and modification of existing window, to create a large new window; plus reinstatement of bluestone plinth to its original level
 - (east section of elevation) removal of wall fabric and modification of two existing windows, to create a large new window; plus reinstatement of bluestone plinth to its original level
- modern glazing suites will be inserted into the existing and modified openings to both street elevations.

The retained external walls of 50-52 are shown in the plans as being repainted or re-rendered in a grey-white colour. While 'reclaimed' bluestone is shown as being reinstated to the plinth, it is unclear if the existing paint is proposed for removal from the stone plinth. Also, the plans indicate a slender canopy is proposed for the retained walls, just below the cornice line; however the montages/renderings in the Town Planning Report do not show a canopy associated with the heritage fabric.

The new building will have hospitality and office uses and will comprise a basement level with seven/eight levels above including a rooftop level. The levels above the retained heritage building walls will have setbacks of 3m to 5m at levels 1 and 2, with some variation at levels 3 to 6 including some lesser setbacks. Above the space not defined by the retained heritage walls, the setbacks will also vary from limited to more generous, while generally maintaining greater setbacks to Smith as opposed to Derby streets. On the east side, where the new building

Attachment 8 - 46-52 Smith Street, Collingwood - Heritage referral comments

adjoins the significant heritage dwellings on Derby Street, albeit separated by the lane, the setback of the new building levels to the west elevation of 1 Derby Street will be in the order of 6m.

The overall effect will be of slightly 'overlapping' building levels, with some floor plates expressed as canopies. Terraces, with landscaped edges, will be sited in the setbacks including a large terrace at level 1 behind the parapet of the retained heritage walls.

Materials for the new building include extensive use of tinted glass with bronze detailing and black matt panels. Pale brickwork will also be used on the lower levels of the new building to Smith and Derby streets.

Assessment of proposal

The following assessment has had regard for Clause 22.02 Development Guidelines for Sites Subject to the Heritage Overlay and Clause 43.01 Heritage Overlay.

Of the former, the relevant policy on partial demolition is cited (summarised):

Generally discourage the demolition of part of a contributory building or removal of contributory elements unless:

- That part of the heritage place has been changed beyond recognition of its original or subsequent contributory character(s).
- For a contributory building:
 - that part is not visible from the street frontage (other than a laneway), abutting park or public open space, and the main building form including roof form is maintained; or
 - the removal of the part would not adversely affect the contribution of the building to the heritage place.

And for the new development/additions:

Corner Sites and Sites with Dual Frontages

Encourage new building and additions on a site with frontages to two streets, being either a corner site or a site with dual street frontages, to respect the built form and character of the heritage place and adjoining or adjacent contributory elements to the heritage place.

Encourage new buildings on corner sites to reflect the setbacks of buildings that occupy other corners of the intersection.

And:

Industrial, Commercial and Retail Heritage Place or Contributory Elements

Encourage new upper level additions and works to:

- Respect the scale and form of the existing heritage place or contributory elements to the heritage place by being set back from the lower built form elements. Each higher element should be set further back from lower heritage built forms.

Attachment 8 - 46-52 Smith Street, Collingwood - Heritage referral comments

- Incorporate treatments which make them less apparent.

Introductory comment

This assessment has had regard for the non-contributory and contributory gradings of the buildings on the subject site, while also recognising the modified form and presentation of the contributory 50-52 Smith Street. This allows for some more flexibility to be applied than would be the case, for example, with a more intact and highly graded building.

However, there is also the heritage precinct and Heritage Overlay context, including the valued heritage character of the HO464 precinct and predominance of low-scale heritage buildings in the precinct; the fact of the precinct being comparatively small and contained and not including any of the more substantial recent developments referred to in the planning application; and the fact of the subject site being at a prominent and highly visible corner in the precinct.

The heritage sensitivities which arise from the adjoining individual Heritage Overlay places in Derby Street, again low-scale historic development, are also relevant.

The recent VCAT decision for 14-18 Smith Street Collingwood (reference No. P715/2020), which is another corner site in the HO464 precinct to the south, has also been referred to. While the Tribunal did not approve that proposed development, it is understood that the Tribunal was generally comfortable with a seven-storey development on that site, with greater setbacks to Smith Street than proposed here. That site, and development, also involved a significant graded building (British Crown Hotel).

Demolition/treatment of the retained heritage building facades

The overall outcome for the contributory 50-52 Smith Street is generally acceptable, including the proposal to restore areas of the original bluestone plinth, albeit some recommendations are made below regarding works to the north elevation.

The proposed removal of the rear wings and roof is also largely acceptable. The roof is not a highly visible element of the contributory building, and the rear wings are utilitarian later elements. The partial demolition also retains most of the remaining Victorian-era facades and visible fabric, which are the more contributory elements of the subject site.

However, while accepting that 50-52 Smith Street already has non-original and modified window openings, a combination of removal of wall and modification to existing openings on the highly visible and exposed north elevation will result in three large windows taking up most of the retained elevation. This will have the effect of giving the building an unsympathetic 'hollowed out' appearance. To be more sympathetic to the traditional form or condition, a reduction in the extent of glazing/opening is recommended, with more solid wall retained or introduced. Preferably this would at a minimum involve a reduction in the width/extent of the largest of the three windows (middle window).

Clarification is also sought on other works to the retained heritage building/walls, including whether the existing paint is proposed for removal from the bluestone plinth – which is recommended - and whether a canopy is proposed for the retained walls.

New building

The architectural design and expression of the new building/levels is generally acceptable, including the materials and the brick treatment of the new levels to Smith and Derby streets.

Attachment 8 - 46-52 Smith Street, Collingwood - Heritage referral comments

ANITA BRADY HERITAGE

However, removal of a level (one storey) from the proposed development, and a limited increase in the setbacks to Smith and Derby streets are recommended here and explained below.

The new building, at seven/eight storeys, is out of scale with development in this precinct. It is also out of scale with the adjoining heritage development on Derby Street. The nearby recently completed eight storey development to the east on Derby Street, to the corner of Oxford Street, is not in the Heritage Overlay.

With reference to the Town Planning Report (Urbis, December 2020), the 'nearby developments' identified at page 10 are also not all in a Heritage Overlay. Further, of the images of six recent developments on page 11 of the report, five are not in the Heritage Overlay in Yarra and one is a five-storey development in a different Yarra precinct (166-182 Gertrude Street). The report, at page 23, also cites a recent eight storey development which has been approved for 42-44 Oxford Street, but this again is not included in the Heritage Overlay. While the evolving character of Collingwood generally, and of some sites and areas in proximity to the subject property is acknowledged, there remains a distinction between what has been approved and is seen as acceptable or appropriate in Yarra's heritage precincts as opposed to areas and developments outside the precincts. This is particularly evident in Collingwood.

As noted, the proposal is broadly similar in its height and general approach to what VCAT considered reasonable for 14-18 Smith Street, albeit this development was not approved by the Tribunal. Relating that development and decision to the subject proposal, this is a situation where more limited setbacks are proposed for a lesser graded building but one which largely shares the heritage context and sensitivities of 14-18 Smith Street in terms of the height and interface with the HO464 precinct and Smith Street. This proposal also has the added heritage sensitivities of the adjacent Heritage Overlays on Derby Street.

Having regard for all the above, a reduction in the height of the proposed building, by one storey, will bring it in line with the VCAT decision for 14-18 Smith Street. This will also reduce the height of the building in the Derby Street context which is, as noted, a point of difference to the proposed development to the south. An increase in the western setback of the upper levels to Smith Street (levels 2 to 6) from 5m to 6m, is also recommended. This will help moderate the scale of the building in the lower-scale precinct and assist the new development to be less prominent to Smith Street. It will also help to reinforce the complementary streetscape height of the lower levels of the development to the precinct.

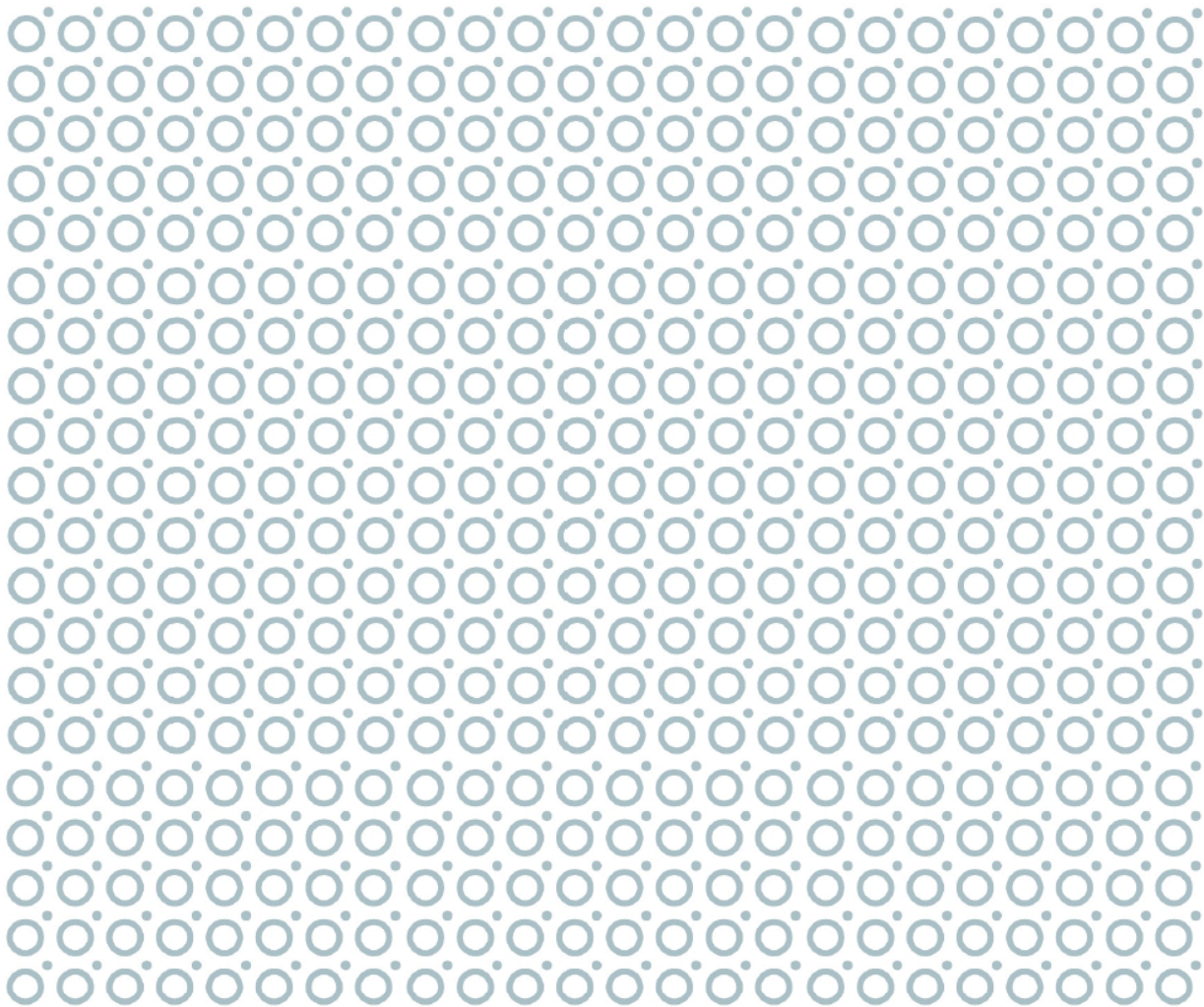
On Derby Street, an increase in the northern setback of the upper levels from 3m to 3.5m is also recommended. While minimal, this together with the height reduction, will improve the outcome for the sensitive heritage context on the north and east of the site, and help moderate the impact of the scale of the new building on this section of lower scale streetscape and adjoining heritage buildings.

¹ City of Yarra Heritage Gaps Study – Smith Street South, Anthemion Consultancies, 2014

² http://vhd.heritage.vic.gov.au/local/result_detail/103602?page=1&type=user, accessed 16 May 2021

³ http://vhd.heritage.vic.gov.au/local/result_detail/103774?page=1&type=user, accessed 16 May 2021

Attachment 9 - 46-52 Smith Street, Collingwood - External UD comments



Urban Design Review

Proposed Mixed-Use Development, 46-52 Smith Street, Collingwood
(PLN21/0042)

Prepared by Simon McPherson, for City of Yarra

30 April 2021

Attachment 9 - 46-52 Smith Street, Collingwood - External UD comments

Table of Contents

1.0 Introduction	3
1.1 Process and involvement	3
1.2 Qualifications and experience to prepare this Review	4
1.2.1 Qualifications and registrations	4
1.2.2 Experience	4
2.0 Context	6
2.1 Strategic context	6
2.1.1 Zoning and Overlays	6
2.1.2 Planning Policy Framework	6
2.1.3 Local Planning Policy Framework	7
2.2 Built form context	8
2.2.1 Site location and local context	8
2.2.2 On-site built form	8
2.2.3 Surrounding built form	10
2.3 The proposal	12
2.3.1 Configuration	12
2.3.2 Heights	12
2.3.3 Profile	12
3.0 Review of the proposed development	13
3.1 Is the built form siting appropriate?	13
3.2 Is the proposed land use appropriate?	13
3.2.1 Hospitality uses	14
3.2.2 Commercial office	14
3.3 Is the building height and massing appropriate?	14
3.3.1 Building height	14
3.3.2 Smith Street profile	16
3.3.3 Derby Street profile	18
3.3.4 Laneway interface	19
3.3.5 South interface	19
3.4 Are the public realm interfaces / frontages appropriate?	20
3.4.1 Interface to Smith Street	20
3.4.2 Interface to Derby Street	20
3.5 Is the architectural expression appropriate?	21
3.6 Are off-site amenity impacts acceptable?	22
4.0 Conclusion	23

Global South Pty Ltd

ACN 123 980 781
 ABN 81 123 980 781
www.globalsouth.net.au

M. +61 (0)448 201 344
 E. simon.mcpherson@globalsouth.net.au

Attachment 9 - 46-52 Smith Street, Collingwood - External UD comments

1.0 Introduction

1.1 Process and involvement

On 31 March 2021 I was asked by Yarra City Council officers to prepare a report comprising urban design review and advice, regarding the proposed mixed-use development at 46-52 Smith Street, Collingwood.

In preparing this review, I have:

- Obtained and reviewed the following Permit Application documents:
 - Architectural Plans (XO Projects, Revision B: Planning RFI, dated 24/02/2021);
 - Town Planning Report (Urbis, December 2020);
 - Urban Context Report (XO Projects, 16/03/2021);
 - Landscape Plan (XO Projects, Revision B: Planning RFI, dated 24/02/2021);
 - Heritage Impact Statement (Bruce Raworth, December 2020)
- Reviewed the applicable provisions of the Yarra Planning Scheme relating to urban design as listed below;
- Visiting the subject site and surrounding area on 25/04/2021. The photographs in this report are my own, except where specified. I note that I have visited the area around the subject site on several occasions, and am familiar with other developments and proposals nearby.

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ACN 123 980 781
ABN 81 123 980 781
www.globalsouth.net.au

M. +61 (0)448 201 344
E. simon.mcpherson@globalsouth.net.au

Attachment 9 - 46-52 Smith Street, Collingwood - External UD comments

1.2 Qualifications and experience to prepare this Review

1.2.1 Qualifications and registrations

- (1) My academic qualifications are as follows:
 - o **Executive Masters (MSc) in Cities**, inaugural programme (September 2016 - completed February 2018), London School of Economics and Political Sciences (LSE Cities), UK;
 - o **Master of Science (MSc): Built Environment - Urban Design** (Distinction), The Bartlett School, University College London, 2005-06, UK;
 - o **Bachelor of Architecture (BArch)** (First Class Honours), The University of Melbourne, 1996-97;
 - o **Bachelor of Planning and Design (BPD) (Architecture)**, The University of Melbourne, 1992-94.
- (2) My professional registrations and memberships are as follows:
 - o **Registered Architect**, Architects Registration Board of Victoria: individual registration number 15838;
- (3) I am engaged on the following professional organisations:
 - o Member, Victorian Design Review Panel;
 - o Member, Design Review Panel for South Australia;
 - o Member, Latrobe University Design Review Panel;
 - o Global Advisor, United Nations Global Compact – Cities Programme (discontinued);
 - o Member, Built Environment Task Force, Smart Cities Council – Australia/New Zealand (discontinued).

1.2.2 Experience

Professional experience

- (4) I hold over 15 years of dedicated professional experience in urban design, including:
 - o Urban Designer, Victorian State Government (2002-2007, including study leave);
 - o Director, SJB Urban (2007-2016);
 - o Director, Global South (2016-present).
- (5) I hold approximately 5 years of prior experience in architectural practice, in Australia and the UK.

Project experience

- (6) My urban design experience includes the following projects:
 - o Policy and guidelines:
 - Author/contributor, *Better Placed*, NSW Architecture and Urban Design Policy, Government Architect NSW (2016-17). Benchmark design policy, winner Australia Award for Urban Design 2017;
 - Contributor (State Government employee), *Design Guidelines for Higher Density Residential Development, Activity Centre Design Guidelines*;
 - Contributor, SA *Medium-Density Design Guidelines*;

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ACN 123 980 781
 ABN 81 123 980 781
www.globalsouth.net.au

M. +61 (0)448 201 344
 E. simon.mcpherson@globalsouth.net.au

Attachment 9 - 46-52 Smith Street, Collingwood - External UD comments

- Lead consultant, Urban Design Guidelines, Bowden, SA (SJB Urban, 2015);
- Urban Design Advice:
 - Eden/Haven/Sanctuary on the River, Abbotsford, for HAMPTON (complete), (SJB Urban, 2010). High-density, mid-rise (9-11 storeys) permeable courtyard development, winner UDIA President's Award, High-Density Housing Award (National, Victoria), Masterplanned Development Award (Victoria);
 - Richmond Plaza redevelopment, for Coles (SJB Urban, 2014);
 - Grocon FCAD redevelopment, Footscray Station Precinct (SJB Urban, 2011).
- Independent reviews:
 - Regular independent reviews of permit applications, for Councils including Melbourne, Yarra, Port Phillip, Banyule, Brimbank, Manningham and Casey.
- Strategic plans, structure plans and Urban Design Frameworks:
 - Footscray Built Form Review 2020, for Maribyrnong City Council;
 - Tarneit Major Town Centre: Economic Impact Assessment and Design Review 2018, for Wyndham City Council;
 - Oakleigh Activity Centre Transport Precinct: Design Review 2018, for Monash City Council;
 - 1160 Sayers Road, Tarneit, Structure Plan for Wyndham City Council (landowner) (SJB Urban 2014-15). Innovative, integrated plan for high-density, walkable precinct in greenfield setting;
 - Footscray Station Precinct Planning and Urban Design Framework (SJB Urban, 2008-09). Winner, PIA Transport Planning Award 2008;
 - Brighton Toyota Site UDF, for LEFTA Corporation;
 - Frankston Transit Interchange Precinct UDF and Master Plan, for DPCD (SJB Urban 2009-2012);
 - Wise Foundation 'Wellness Village' UDF, Mulgrave, for landowners (SJB Urban, 2015-16).
- Master Plans and Concept Designs
 - Caulfield Village Master Plan, for Beck Property / Probuild (SJB Urban, 2012);
 - Greensborough Activity Centre Concept Master Plan, for Banyule City Council (2017);
 - 433 Smith Street (Fitzroy Gasworks) Master Plan, for Places Victoria (SJB Urban, 2015);
 - Master Plan, Binks Ford Site and over-rail deck, Footscray, for Places Victoria (SJB Urban, 2012);
 - Caulfield-Dandenong corridor concept/feasibility studies, for VicTrack (SJB Urban, 2015).

Experience preparing expert evidence

- (7) I have presented evidence at VCAT and Planning Panels Victoria on numerous occasions.

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 ABN 81 123 980 781
www.globalsouth.net.au

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2.0 Context

2.1 Strategic context

2.1.1 Zoning and Overlays

The subject land is located within the Smith Street corridor, which is identified as a Major Activity Centre under *Plan Melbourne*.

The land is situated within the **Commercial 1 Zone (C1Z)**. The purposes of this Zone are to:

- *To create vibrant mixed use commercial centres for retail, office, business, entertainment and community uses.*
- *To provide for residential uses at densities complementary to the role and scale of the commercial centre.*

The site spans across precinct **Heritage Overlay HO464 (Smith Street South Precinct, Fitzroy and Collingwood)**.

2.1.2 Planning Policy Framework

The following clauses are applicable to the subject site and proposal. Relevant content from these clauses is raised below in the context of my assessment of the proposal.

Clause 15 Built Environment discusses Urban Design objectives and strategies:

- **15.01-1S Urban Design** provides strategies for safe, healthy, functional and enjoyable urban environments. Strategies include:
 - *Require development to respond to its **context in terms of character, cultural identity**, natural features, surrounding landscape and climate.*
 - *Ensure development **contributes to community and cultural life** by improving the quality of living and working environments, facilitating accessibility and providing for inclusiveness.*
 - *Ensure development **supports public realm amenity and safe access** to walking and cycling environments and public transport.*
 - *Ensure that the design and location of **publicly accessible private spaces**, including car parking areas, forecourts and walkways, is of a high standard, creates a safe environment for users and enables easy and efficient use.*
 - *Ensure that development provides **landscaping** that supports the amenity, attractiveness and safety of the public realm.*
- **15.01-2S Building design** guides buildings which contribute positively to context and enhance the public realm, including responding to the strategic and cultural context of the location.
- **15.01-4R Healthy neighbourhoods - Metropolitan Melbourne** seeks to create a city of 20-minute neighbourhoods;
- **15.01-5S Neighbourhood character** seeks to ensure development responds to its context and reinforces a sense of place and the valued features and characteristics of the local environment and place, including by emphasising the heritage values and built form that reflect community identity.
- **15.02-1S Energy and resource efficiency** promotes consolidation of urban development and integration of land use and transport.
- **15.03-1S Heritage conservation** encourages *appropriate development that respects places with heritage values*, and seeks to *ensure an appropriate setting and context for heritage places is maintained or enhanced*.

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ACN 123 980 781
 ABN 81 123 980 781
www.globalsouth.net.au

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 E. simon.mcpherson@globalsouth.net.au

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Clause 17.02-1S Business encourages *development that meets the community's needs for retail, entertainment, office and other commercial services*. Strategies include *ensuring commercial facilities are aggregated and provide net community benefit in relation to their viability, accessibility and efficient use of infrastructure; and locating commercial facilities in existing or planned activity centres*.

2.1.3 Local Planning Policy Framework

Yarra's Local Planning Policy Framework includes the following clauses applicable to the subject site and proposal. I have not exhaustively reproduced every policy below.

Clause 21.03 Vision states that *Yarra will have a distinctive identity as a low-rise urban form, with areas of higher development and highly valued landmarks, and that all new development will demonstrate design excellence*.

Clause 21.04-3 Industry, office and commercial seeks to *increase the number and diversity of local employment opportunities, including commercial and office use in existing industrial areas*.

Clause 21.05-1 Heritage seeks to protect and enhance Yarra's heritage places, and supports the restoration of heritage places. Heritage is not my area of expertise, and so this review does not address heritage directly.

Clause 21.05-2 Urban Design includes the following objectives and strategies:

- *To ensure that new development contributes positively to Yarra's urban fabric.*
- *Reflect the fine grain of the subdivision pattern in building design where this is part of the original character of the area.*
- *To enhance the built form character of Yarra's activity centres.*
- *Require development within Yarra's activity centres to respect and not dominate existing built form.*
- *Support new development that contributes to the consolidation and viability of existing activity centres.*

Clause 21.05-3 Built form character seeks to improve the built form character of transport corridors.

Clause 22.02 Development Guidelines for Sites Subject to the Heritage Overlay encourages the design of new development and alterations and additions to a heritage place or a contributory element to a heritage place to:

- *Respect the pattern, rhythm, orientation to the street, spatial characteristics, fenestration, roof form, materials and heritage character of the surrounding historic streetscape.*
- *Be articulated and massed to correspond with the prevailing building form of the heritage place or contributory elements to the heritage place.*
- *Be visually recessive and not dominate the heritage place.*
- *Be distinguishable from the original historic fabric.*

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ACN 123 980 781
 ABN 81 123 980 781
www.globalsouth.net.au

M. +61 (0)448 201 344
 E. simon.mcpherson@globalsouth.net.au

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2.2 Built form context

2.2.1 Site location and local context

The site is located on the eastern side of Smith Street, at the south-east quadrant of the T-intersection of Smith Street and Derby Street.

The site measures approximately 19.25m at the Smith street frontage, and 29.26m along the Derby Street frontage, with an overall area of approximately 556 sq.m.

2.2.2 On-site built form

The subject site is occupied by three (3) single-storey brick shop buildings fronting Smith Street



Figure 01: Subject site, Smith Street frontage.



Figure 02: Subject site, Derby Street frontage and rear laneway interface.

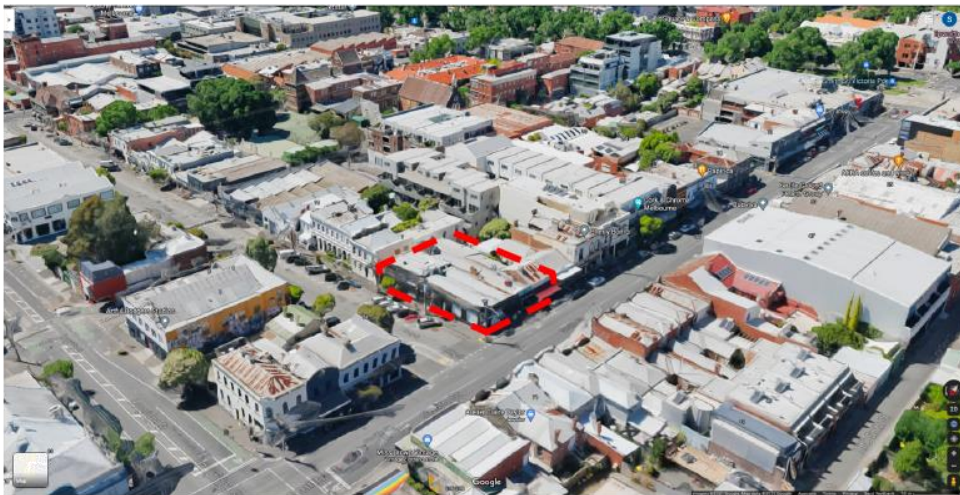


Figure 03: Aerial view of the subject land (centre) looking south-east, and the local context (image source Google)

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ACN 123 980 781
ABN 81 123 980 781
www.globalsouth.net.au

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E. simon.mcpherson@globalsouth.net.au

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Figure 04: Aerial view of the subject land (centre, foreground) looking north along Smith Street (image source Google).

The site's immediate interfaces comprise:

- **North:** Derby Street, with an ornate 2-storey rendered masonry, heritage building on the opposite corner of Smith and Derby Streets, interfacing to a small timber cottage to its rear, facing derby street.
- **East:** Laneway (approx. 3.5m wide) with ornate 2-storey rendered masonry, heritage buildings across the laneway fronting Derby Street.
- **South:** Single storey brick shop building at 44 Smith Street adjoining 2-storey buildings further south.
- **West:** Smith Street, comprising fine grain, one/two-storey frontage buildings with glazed shopfronts at ground floor generally.



Figure 05: The subject site's southern interface (Smith Street, east side), comprising single-level shop building (left) adjacent to double-storey built form.



Figure 06: The subject site's northern interface (Smith Street, east side, looking north, across derby Street), with double-storey heritage fabric, and timber cottage to the east, fronting Derby Street.

Global South Pty Ltd

ACN 123 980 781
 ABN 81 123 980 781
www.globalsouth.net.au

M. +61 (0)448 201 344
 E. simon.mcpherson@globalsouth.net.au

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Figure 07: The subject site's eastern interface (Derby Street, south side), comprising double-storey heritage fabric adjoining the laneway.



Figure 08: Laneway at the rear (east) of the site, viewed from Derby Street looking south, which appears to be approximately 2.7m wide.



Figure 09: Smith Street streetscape (west side, looking south) opposite the subject site.



Figure 10: Smith Street streetscape (east side, looking north from Mason Street), towards the subject site at far left in the distance.

2.2.3 Surrounding built form

The area around the subject site comprises a diverse built context.

The Smith Street corridor in this area generally comprises low-scale (1-2 levels), fine grain, traditional built form, as shown above.

An emerging context of higher-scale redevelopment is evident in this locality, however more extensive redevelopment has occurred in the northern part of Smith Street. Nearby recent developments, shown below, range from 6-8 levels, with some higher built form along the Wellington Street corridor, about 200m to the east.

The approach of establishing 2-3-storey street walls, with higher form set back, is generally adopted for new development in the Smith street corridor and nearby streets, as shown below. This supports retention of the prevailing and relatively consistent low-height street wall condition, while accommodating higher-scale form above.

Global South Pty Ltd

ACN 123 980 781
 ABN 81 123 980 781
www.globalsouth.net.au

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Figure 11: Development at 15-21 Derby Street (8 storeys, 4-storey street wall), 50m east of the subject site.



Figure 12: Development at 7-15 Little Oxford Street (8 storeys, 2-storey street wall), 100m north of the subject site.



Figure 13: Development fronting Wellington Street, approximately 200m east of the subject site.



Figure 14: Development at corner Peel Street and Oxford Streets (6 storeys, full street wall, in response to immediate context), approximately 250m north-east of the subject site.



Figure 15: Development at 9-11 Smith Street (7 storeys, 3-storey street wall), approximately 120m south of the subject site.



Figure 16: Smith Street, west side, looking north, north of Gertrude Street, including wider parcels and higher-scale heritage buildings.

Global South Pty Ltd

ACN 123 980 781
ABN 81 123 980 781
www.globalsouth.net.au

M. +61 (0)448 201 344
E. simon.mcpherson@globalsouth.net.au

Attachment 9 - 46-52 Smith Street, Collingwood - External UD comments

2.3 The proposal

2.3.1 Configuration

The proposal is for a 7-storey building (plus roof terrace) configured as follows:

- o Basement: Gym, bike storage, services;
- o Ground Floor: Restaurant/Bar, separate Café, Lobby, loading bay and services;
- o Levels 1-2: Office spaces with outdoor terraces;
- o Levels 3-6: Office spaces;
- o Level 7: Roof terrace with pergola canopy.

The development includes retention of the 'contributory' heritage facades fronting Smith and Derby streets (50-52 Smith Street), but demolition of modern buildings and additions on the site.

2.3.2 Heights

The overall building height on Smith Street is 24.15m to rooftop level.

Because Derby street has a gradient down towards the east, the height at the rear laneway is 26.55m.

Floor to floor heights are 3.35m throughout, and 4.05m at Ground Floor.

2.3.3 Profile

The Ground Floor occupies the majority of the site area, but provides a setback to the rear/east of 0.96m to widen the existing laneway.

Level 1 extends to both street frontages, but is recessed across the corner to create a setback space and terrace.

Levels 2-6 establish continuous street setbacks of 5m to Smith street and 3m to Derby Street, with varying profiles at the corner, allowing the floors to overlap/overhang. Curved corners in the floor plates adds to this 'fluid' expression.

Global South Pty Ltd

ACN 123 980 781
ABN 81 123 980 781
www.globalsouth.net.au

M. +61 (0)448 201 344
E. simon.mcpherson@globalsouth.net.au

Attachment 9 - 46-52 Smith Street, Collingwood - External UD comments

3.0 Review of the proposed development

3.1 Is the built form siting appropriate?

Clause 21.05-2 Urban Design includes the following objectives and strategies:

- *Reflect the fine grain of the subdivision pattern in building design where this is part of the original character of the area.*
- *To enhance the built form character of Yarra's activity centres.*
- *Require development within Yarra's activity centres to respect and not dominate existing built form.*

Clause 22.02 Development Guidelines for Sites Subject to the Heritage Overlay encourages development to *respect the pattern, rhythm, orientation to the street, spatial characteristics, fenestration, roof form, materials and heritage character of the surrounding historic streetscape.*

By retaining the existing contributory facades to the corner of Smith and Derby Streets, the proposal effectively occupies the full extent of the site. The retention of street facades contributes to retaining the established character of the public realm interfaces.

The establishment of zero (0m) setbacks to street frontages reflects the existing conditions and consistent character of Smith Street and local streets generally.

The widening of the rear laneway does not provide significant public benefit because the laneway does not provide through-connection, but is accepted as a requirement for vehicular/loading access.

I am therefore supportive of the proposed siting of the development.

3.2 Is the proposed land use appropriate?

The **Commercial 1 Zone (C1Z)** seeks to *create vibrant mixed use commercial centres for retail, office, business, entertainment and community uses.*

Clause 15.01-1S Urban Design seeks to *ensure development contributes to community and cultural life by improving the quality of living and working environments, and supports public realm amenity and safe access.*

Clause 15.01-4R Healthy neighbourhoods - Metropolitan Melbourne seeks to *create a city of 20-minute neighbourhoods, bringing homes within 20-minute access to employment, education, community infrastructure and other regular destinations*

Clause 15.02-1S Energy and resource efficiency promotes consolidation of urban development and integration of land use and transport.

Clause 17.02-1S Business encourages *development that meets the community's needs for retail, entertainment, office and other commercial services, including locating commercial facilities in existing or planned activity centres.*

Clause 21.04-3 Industry, office and commercial seeks to *increase the number and diversity of local employment opportunities.*

Clause 21.05-2 Urban Design supports *new development that contributes to the consolidation and viability of existing activity centres.*

Global South Pty Ltd

ACN 123 980 781
 ABN 81 123 980 781
www.globalsouth.net.au

M. +61 (0)448 201 344
 E. simon.mcpherson@globalsouth.net.au

Attachment 9 - 46-52 Smith Street, Collingwood - External UD comments

3.2.1 Hospitality uses

The proposed Ground Floor Restaurant/Bar and Café are clearly appropriate in this Major Activity Centre setting, and will contribute active edges and visual/physical permeability to both street frontages. The Restaurant Bar will also provide activation during evening hours, while the office space would be expected to be less occupied, while the Café may generate more early-morning activity.

3.2.2 Commercial office

The proposed office space at upper levels will contribute to the mix of land uses in the activity centre, and to daytime activation and passive surveillance opportunities, while supporting employment outcomes.

I therefore consider the proposed land uses to be responsive to the zoning and context.

3.3 Is the building height and massing appropriate?

There is no definitive, prescriptive policy guidance or controls for built form in this location in the Yarra Planning Scheme. Consideration of any proposal needs to factor the current and emerging context and character, and how the proposal will contribute to or impact on these.

3.3.1 Building height

The emerging built form context in this locality is indicated by multiple developments nearby in the range of 5-8 levels, as indicated above, while higher forms exist further to the east on Wellington Street (8-14 levels approximately) and to the north on Smith Street (6-10 levels approximately).

Therefore the proposed height of 7 levels is considered to be within the general range of other developments nearby.

The height on Smith Street of 24.15m is commensurate with the 20m street width of Smith Street, and generates a width-to-height ratio (factoring in upper level setbacks) of 1:0.8, as shown in Figure 17 below (20m + 5m + 5m width; 24m height). I consider this proportional relationship to be acceptable.

The ratio at Derby Street would be approximately 1:0.9, due to the lesser upper-level setbacks of 3m. I also consider this proportion to be acceptable.

In considering the photomontage views in the applicant's Urban Context Report (below, Figures 18-21), I do not consider the height to be excessive or overbearing in relation to the streetscapes of existing built fabric. While the low-height and recessive street wall/podium configuration makes the upper-level form more visible (compared to a higher street wall), I consider the visual implications to be acceptable. I also consider the proportional relationship between the street wall/podium component and the upper-level form to be appropriate.

Further, the 7-storey height is within the range of visual facial recognition between the top level and the streetscape, and will ensure that occupants in the upper levels will have a strong visual connection with the public realm.

While I assess the building massing, profiles and setbacks further below, I consider the proposed building height to be acceptable, based on the above assessment.

Global South Pty Ltd

ACN 123 980 781
ABN 81 123 980 781
www.globalsouth.net.au

M. +61 (0)448 201 344
E. simon.mcpherson@globalsouth.net.au

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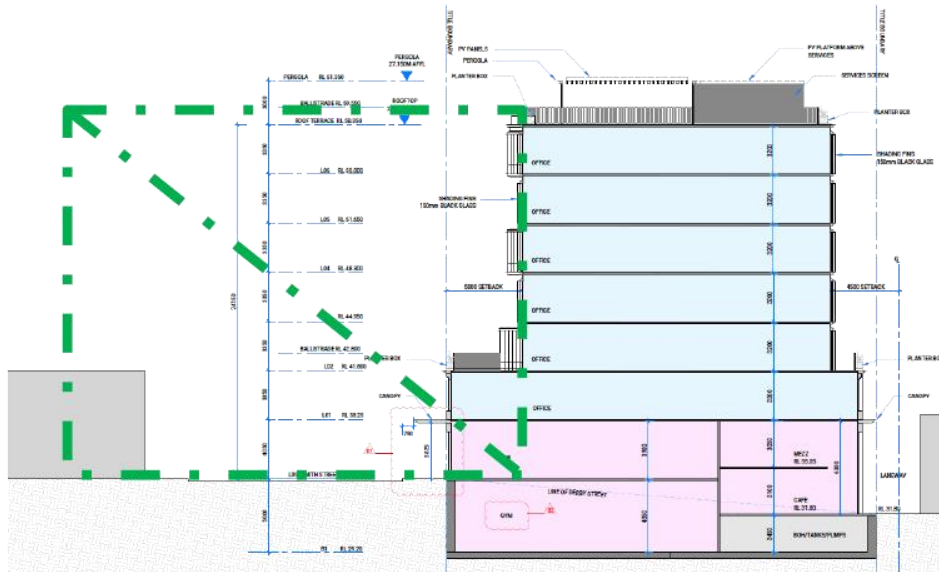


Figure 17: Excerpt from drawing A0950 (Section AA) indicating proportions of street width to building height ratio, including upper-level setbacks, to Smith Street. The approximate ration of with to height is 1:0.8 (height is 20% less than the width).



Figure 18: Photomontage view from street corner, looking south-east. The height is prominent but not visually overwhelming or excessive, in my view. The proportional relationship between street walls and upper-level form is considered appropriate.



Figure 19: In this photomontage looking along Derby Street, the upper-level form is quite vertical proportions and the visual prominence is emphasised by the street gradient. However, it is clearly set back from the streets, and is not considered over-scaled.

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 ABN 81 123 980 781
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Attachment 9 - 46-52 Smith Street, Collingwood - External UD comments



Figure 20: Photomontage view from Smith Street, showing the relatively modest vertical scape and substantial setback from the corner.



Figure 21: Longer-range 'whitecard' render looking south down Smith Street, indicating acceptable proportions of the building eight relative to street width.

3.3.2 Smith Street profile

Podium height

At both street frontages, the street wall/podium is 2-storeys in height at the site's outer edges, and single-storey centrally, due to the retention of the existing corner façade.

The gradient to Derby Street allows the Café tenancy to incorporate a mezzanine floor, but this reads externally as a high ground floor frontage.

While generally a consistent 2-storey street wall would be considered appropriate in this location, given the consistent streetscape context, in this case the single-storey retained façade provides rationale for a single-level street wall, for the extent of that façade.

The Level 1 frontage wall utilises angles and curved corners to reflect being 'peeled' away from the street frontage. This is reinforced by the design expression to the new Ground Floor and Level 1 facades, in light grey, patterned brickwork and corbelled window openings, in contrast to the metal banding of the upper levels.

The single-storey frontage height at the street corner retains the existing street wall condition, and gives the building an open, 'low-slung' feel at this interface, which I consider to be appropriate and effective in addressing the public realm.

I therefore support the street wall heights to Smith Street and the street corner with Derby Street.

Upper-level setbacks

The upper-level form is set back 5m from the Smith Street frontage, above Level 1, while the floor levels encroach into this setback around the corner, by 1.4m at Level 2 (3.6m clear setback), 0.8m at Levels 4 and 6 (4.2m clear setback).

However as shown in the renders above, the form still 'reads' as having a consistent, parallel setback, with a limited extent of variation at the corner as a visual or formal feature.

The nearby development at 9-11 Smith Street appears to have an upper-level setback to Smith Street of approximately 3m (see Figure 15), while the development at 15-21 Derby Street (Figure 11) appears to have an upper-level setback of 3-4m.

Global South Pty Ltd

ACN 123 980 781
 ABN 81 123 980 781
www.globalsouth.net.au

M. +61 (0)448 201 344
 E. simon.mcpherson@globalsouth.net.au

Attachment 9 - 46-52 Smith Street, Collingwood - External UD comments

Front setback profiles in other approved/completed Smith Street developments further to the north are as follows:

- 'MacRobertson Lofts', 365-379 Smith Street (not constructed) (170m from the subject site):
 - 8 storeys;
 - 4m upper-level setback behind heritage façade (L2);
 - 6m setback at L3-L6.
- 'Haus', 416-422 Smith Street: (completed) (opposite the subject site);
 - 9 storeys;
 - 7m setback upper-level setback to Smith Street;
 - 3m setback upper-level setback to side street.
- 'You & I', 444-452 Smith Street (completed) (90m from subject site):
 - 7 storeys;
 - 7m setback upper-level setback to Smith Street;
 - 3.0m setback upper-level setback to side street.
- 'Trilby', 466-482 Smith Street (completed) (150m from subject site):
 - 10 storeys;
 - 5m setback upper-level setback to Smith Street to balconies (7m to glass line);
- 221 Kerr Street: 7 storeys (under construction) (100m from the subject site);
- 423-425 Smith Street (not yet constructed):
 - 8-storey overall height;
 - 6m setback to Smith Street frontage to glazing line;
 - Uppermost level setback further 2.85m to Smith Street frontage.
- 221 Kerr Street (under construction):
 - 7-storey overall height;
 - New built form positioned above heritage building, set well back from Smith Street (approx. 35m).
- 368-374 Smith Street (not yet constructed):
 - 9-storey overall height;
 - 5m-6.5m setbacks to Smith Street;
 - Uppermost level setback much further from Smith Street.
- 365-379 Smith Street (not yet constructed):
 - 8 storeys overall height;
 - 6m predominant upper level setback from Smith Street.

Therefore, the emerging context indicates front setbacks of generally 5m-7m, or potentially less in nearby developments. The proposed setbacks are consistent with this pattern.

Further, I consider the proposed 5m setback to be appropriate in relation to the relatively modest upper-level height of 5 storeys above the 2-storey street wall/podium.

I therefore consider the Smith Street setback profile to be acceptable.

I have not included the roof deck in my consideration of building height, even though it incorporates an internal lobby and canopy above. The lift core and lobby are set against the southern boundary of the site, and the canopy is set well back from the building edges,

Global South Pty Ltd

ACN 123 980 781
 ABN 81 123 980 781
www.globalsouth.net.au

M. +61 (0)448 201 344
 E. simon.mcpherson@globalsouth.net.au

Attachment 9 - 46-52 Smith Street, Collingwood - External UD comments

ensuring it has limited visibility in streetscape views of the building, as the photomontages show (above).

3.3.3 Derby Street profile

Street wall height

As noted above, the street wall to Derby Street is single-storey in height for the extent of the retained façade, and two storeys for the eastern component. However, this form extends to approximately 3-storeys due to the streetscape gradient, allowing a mezzanine level in the Café tenancy.

The proposed laneway podium height is 9.8m, plus terrace balustrade above. This height is approximately 2.0m higher than the neighbouring 2-storey building to the east, across the laneway.

The expressed canopy line in the Derby Street facade, above Ground Floor, is slightly lower than the parapet and cornice of the neighbouring building, although there appears to be some discrepancy between this alignment in Elevation (drawing A0901) and photomontage (Urban Context Report, page 57), as shown below.

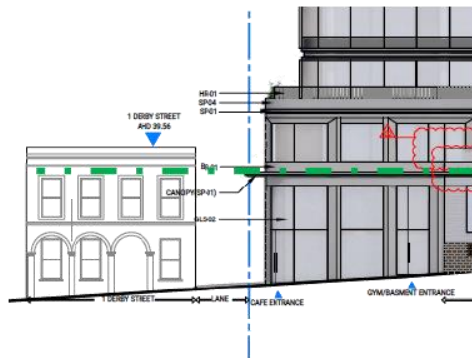


Figure 22: Excerpt from North Elevation (drawing A0901) highlighting alignment of expressed canopy line relative to neighbouring building (above that building's windows, close to the cornice line).



Figure 23: Excerpt from photomontage (Urban Context Report) highlighting different alignment of expressed canopy line relative to neighbouring building (middle of that building's windows).

I consider the difference in height relative to the neighbouring building to be acceptable, because it is a result of the streetscape gradient, and other buildings in this streetscape step down the gradient, as shown at Figure 11 above.

Recommendation 1:

I recommend, however, that the design further explore alignments with, and references to, the neighbouring buildings at 1 and 3-7 Derby Street, which presents as an opportunity to reflect a clearer 'fit' in the context, and which appears unresolved currently.

The neighbouring sites on Derby Street are in individual Heritage Overlays HO98 (Derby House, 1 Derby Street) and HO100 (3-7 Derby Street). The applicant's Heritage Impact

Global South Pty Ltd

ACN 123 980 781
 ABN 81 123 980 781
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Statement (Bryce Raworth) does not appear to mention these properties or their level of significance.

3.3.4 Laneway interface

The proposal incorporates a laneway widening of 0.96m with services at the Ground Floor interface (with glazing close to Derby Street) and full glazing at Level 1. The setback increases to 4.5m from the centreline of the laneway, or 2.96m from boundary, for Levels 2-6.

The existing building across the laneway at 1 Derby Street has windows on the laneway boundary at ground and first floors, noting these are setback approximately 3m from Derby Street (see Figure 8). I am not aware if these are habitable residential windows, but presume that some are, noting that they have external blinds.

The laneway height of 9.8m (equivalent 2.5-3 storeys) is higher than the existing wall on boundary on the subject site. However, I consider that this impact is adequately offset by the increased laneway width.

The laneway glazing to the café is limited in extent, and the Level 1 glazing is higher than the existing windows opposite.

While there may be some potential for café patrons to overlook existing windows across the lane, this is very limited, and I consider it acceptable in this Major Activity Centre context. I consider overshadowing impacts below.

I therefore consider the laneway interface and building profile to be acceptable.

3.3.5 South interface

The southern interface comprises a full height solid wall on the boundary, to predominantly 7-storeys in height, and 8-storeys in the central part. This wall is articulated with expressed floor edge lines or grooves which step up in height, with patterned or corbelled brickwork in between.

The geometry of this articulation reflects that of the building form and layout overall.

Given the single-storey height of the neighbouring building at 44 Smith Street, and the 2-storey height of buildings further south, this boundary wall will be highly visible in the streetscape.

Building up to the side boundaries is clearly a key characteristic of the local context, and the resulting 'black' side walls in larger buildings are evident in recent developments such as 9-11 Smith Street and 15-21 Derby Street (Figures 11 and 15)

In the subject proposal, the lift core is positioned against the southern boundary, which I consider to be appropriate, and this forms part of the black southern wall.

This side wall may be concealed in future if the neighbouring site(s) are redeveloped.

I consider the extent of this southern boundary wall to be acceptable, but its articulation and expression should be enhanced as follows.

I have considered the potential to introduce variation in this wall by 'peeling back' the upper 1-2 levels subtly, for a more varied three-dimensional interface, but this would potentially create unsatisfactory outcomes if the neighbouring site was to redevelop to similar scale.

Global South Pty Ltd

ACN 123 980 781
 ABN 81 123 980 781
www.globalsouth.net.au

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Recommendation 2:

I recommend removing the angled 'steps up' in the expressed floor edges in the southern façade, and extending these through horizontally, to better reflect the overall building expression of 'tacked' floor levels, and to avoid the awkward junction where these steps meet the top roof terrace level.

Recommendation 3:

I also suggest further resolution or clarification of the top (roof deck) level at the southern façade, including the wall and canopy details, to ensure these are integral with the overall façade expression, but have a lightweight expression which is not overly prominent visually.

The elevations identify this as an aluminium screen, but it is quite visually dark in the 'whitecard' render (Urban Context Report page 63).

3.4 Are the public realm interfaces / frontages appropriate?

3.4.1 Interface to Smith Street

The proposed development's ground floor frontage to Smith Street and Derby Street comprises a Restaurant/Bar tenancy addressing both street frontages, with several large windows within the retained heritage façade, and two entrances. This will provide extensive activation, visual permeability and passive surveillance opportunities, as noted above.

A separated entry lobby is located at the southern interface.

A fire booster cupboard is positioned within the Smith Street Ground Floor frontage, but otherwise services are confined to the rear laneway interface, which is appropriate.

At Level 1, the outdoor terrace space and office space windows will provide further passive surveillance potential, as they are closely linked to the adjoining public realm.

3.4.2 Interface to Derby Street

Alongside the Restaurant/Bar, the frontage to Derby Street incorporates the two-level Café tenancy with extensive frontage glazing in double-height windows which curve around the corner into the laneway, corner entrance, and a separate to the basement gym.

This frontage, combined with office windows above at Level 1 and the Level 1 outdoor terrace, will support passive surveillance and activation of Derby Street and the laneway.

The lower floor level of the Café, in response to the streetscape gradient, allows this tenancy to directly relate to and connect with the streetscape, which is appropriate.

The proposed interfaces to the public realm are highly responsive to the activity centre context and will support extensive interaction between the building and the streetscape. I am therefore supportive of the public realm interfaces as proposed.

Global South Pty Ltd

ACN 123 980 781
 ABN 81 123 980 781
www.globalsouth.net.au

M. +61 (0)448 201 344
 E. simon.mcpherson@globalsouth.net.au

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3.5 Is the architectural expression appropriate?

Clause 15.01-1S Urban Design requires development to *respond to its context in terms of character, cultural identity, natural features, surrounding landscape and climate.*

Clause 15.01-2S Building design guides buildings which contribute positively to context and enhance the public realm.

Clause 21.05-2 Urban Design includes the following objectives and strategies:

- *To ensure that new development contributes positively to Yarra's urban fabric.*
- *Reflect the fine grain of the subdivision pattern in building design where this is part of the original character of the area.*
- *Support new development that contributes to the consolidation and viability of existing activity centres.*

Clause 22.02 Development Guidelines for Sites Subject to the Heritage Overlay encourages new development to:

- *Respect the pattern, rhythm, orientation to the street, spatial characteristics, fenestration, roof form, materials and heritage character of the surrounding historic streetscape.*
- *Be articulated and massed to correspond with the prevailing building form of the heritage place or contributory elements to the heritage place.*
- *Be visually recessive and not dominate the heritage place.*
- *Be distinguishable from the original historic fabric.*

The external expression of the proposed built form incorporates the retained heritage facades at Ground Floor, including the bluestone plinth interfacing with the footpath, providing a level of texture and 'grain' to the streetscape interface.

The new street wall components combine extensive glazing, with curved corner glass, corbelled and patterned brickwork, and then, curved metal banding for canopies. The upper levels comprise more extensive façade glazing, also with curved metal banding along floor edges.

This approach is considered appropriate because:

- It provides a high level of detail, texture and tactility at the public realm interfaces, where it is directly experienced by pedestrians, and where it integrates with the masonry heritage facades;
- It reflects a high degree of design consideration and detail, and a sense of 'craft';
- It creates visual complexity
- The Level 1 'peeling pack' from the street frontages aligns with, and frames, the retained heritage facades;
- The upper-level facades and alignments create a sense of movement and complexity, supporting visual interest;
- The use of straight edges with curved corners achieves a balance of a regular overall form, with subtle shifting and 'disruption', while the curves indicate a sense of quality and craft in construction.

A fine-grain expression is established in the Smith Street frontage through vertical brick piers, and the new built forms clearly distinct from the heritage fabric on site and nearby.

The design of the new built form is bold and contemporary, but is adequately recessive and restrained to complement and not dominate the heritage fabric.

I therefore consider the external expression to be appropriate and supportable.

Global South Pty Ltd

ACN 123 980 781
 ABN 81 123 980 781
www.globalsouth.net.au

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 E. simon.mcpherson@globalsouth.net.au

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3.6 Are off-site amenity impacts acceptable?

Based on the overshadowing diagrams provided for the September equinox, the proposed development will affect neighbouring private open spaces (located in the Mixed Use Zone) at 2pm and 3pm. Before 2pm there are no significant public realm impacts, or impacts on residential properties.

At 2pm, significant impacts are limited to 2-3 townhouse private open spaces (full overshadowing, from existing 68%). I note that there appears to be a labelling error regarding the new shadow impact indicated at 2pm for the rear POS of the Derby Street property, but which is labelled as 0% extra shadowing.

At 3pm, four townhouse private open spaces and 3 rear courtyard spaces are significantly affected.

These impacts are recognised as detrimental to those properties. However, they are limited and occur relatively late in the day, in a Major Activity Centre and Commercial 1 / Mixed Use Zone context.

In this context, I consider these impacts to be reasonable and acceptable.

Global South Pty Ltd

ACN 123 980 781
ABN 81 123 980 781
www.globalsouth.net.au

M. +61 (0)448 201 344
E. simon.mcperson@globalsouth.net.au

Attachment 9 - 46-52 Smith Street, Collingwood - External UD comments

4.0 Conclusion

I consider the proposed development at 46-52 Smith Street, Collingwood to be an appropriate response to the evolving urban context of the Smith Street Major Activity Centre, and to the parameters of the subject site and its interfaces.

The overall built form massing is generally consistent with other recent developments in this locality, and responsive to the adjoining public realm, neighbouring built fabric and heritage setting.

The external expression reflects a refined, considered design, with an interesting formal approach, which will contribute to the experience of this location.

I therefore consider that this proposal warrants support from an urban design perspective, while also making some minor recommendations for further refinements.

Global South Pty Ltd

ACN 123 980 781
ABN 81 123 980 781
www.globalsouth.net.au

M. +61 (0)448 201 344
E. simon.mcperson@globalsouth.net.au

Attachment 10 - 46-52 Smith Street, Collingwood - ESD referral comments

Sustainable Management Plan (SMP)

Referral Response by Yarra City Council



ESD in the Planning Permit Application Process

Yarra City Council's planning permit application process includes Environmentally Sustainable Development (ESD) considerations. This is now supported by the ESD Local Policy Clause 22.17 of the Yarra Planning Scheme, entitled *Environmentally Sustainable Development*.

The Clause 22.17 requires all eligible applications to demonstrate best practice in ESD, supported by the Built Environment Sustainability Scorecard (BESS) web-based application tool, which is based on the Sustainable Design Assessment in the Planning Process (SDAPP) program.

As detailed in Clause 22.17, this application is a 'large' planning application as it meets the category *Non-residential 1. 1,000m² or greater*.

What is a Sustainable Management Plan (SMP)?

An SMP is a detailed sustainability assessment of a proposed design at the planning stage. An SMP demonstrates best practice in the 10 Key Sustainable Building Categories and;

- Provides a detailed assessment of the development. It may use relevant tools such as BESS and STORM or an alternative assessment approach to the satisfaction of the responsible authority; and
- Identifies achievable environmental performance outcomes having regard to the objectives of Clause 22.17 (as appropriate); and
- Demonstrates that the building has the design potential to achieve the relevant environmental performance outcomes, having regard to the site's opportunities and constraints; and
- Documents the means by which the performance outcomes can be achieved.

An SMP identifies beneficial, easy to implement, best practice initiatives. The nature of larger developments provides the opportunity for increased environmental benefits and the opportunity for major resource savings. Hence, greater rigour in investigation is justified. It may be necessary to engage a sustainability consultant to prepare an SMP.

Assessment Process:

The applicant's town planning drawings provide the basis for Council's ESD assessment. Through the provided drawings and the SMP, Council requires the applicant to demonstrate best practice.

Attachment 10 - 46-52 Smith Street, Collingwood - ESD referral comments

Sustainable Management Plan (SMP)
Referral Response by Yarra City Council



Table of Contents

Assessment Summary:..... 3

1. Indoor Environment Quality (IEQ) 6

2. Energy Efficiency 7

3. Water Efficiency..... 9

4. Stormwater Management 10

5. Building Materials 11

6. Transport 12

7. Waste Management 13

8. Urban Ecology 14

9. Innovation 15

10. Construction and Building Management 16

Applicant Response Guidelines 17

Attachment 10 - 46-52 Smith Street, Collingwood - ESD referral comments

Sustainable Management Plan (SMP)

Referral Response by Yarra City Council



Assessment Summary:

Responsible Planner:	Lara Fiscalini
ESD Advisor:	Gavin Ashley
Date:	26.04.2021
Subject Site:	PLN21/0042 46-52 Smith Street, Collingwood VIC 3066
Site Area:	Approx. 556 m ²
Project Description:	7 storey building comprising a basement level for bicycle parking and a gymnasium, ground floor café and restaurant and 6 levels of commercial office space and a rooftop terrace.
Pre-application meeting(s):	Unknown.
Documents Reviewed:	<ul style="list-style-type: none"> • Sustainability Management Plan (Issue C – 23.12.20) by Ark Resources • Architectural Plans (Rev B – 24.02.21) by XO Projects • Urban Context Report (n.d) by XO Projects • Traffic Engineering Assessment (Iss C – 10.12.20) by Traffix Group • Waste Management Plan (13..09.20) by Leigh Design

The standard of the ESD does not meet Council's Environmental Sustainable Design (ESD) standards. Should a permit be issued, the following ESD commitments (1) and deficiencies (2) should be conditioned as part of a planning permit to ensure Council's ESD standards are fully met.

Furthermore, it is recommended that all ESD commitments (1), deficiencies (2) and the outstanding information (3) are addressed in an updated SMP report and are clearly shown on Condition 1 drawings. ESD improvement opportunities (4) have been summarised as a recommendation to the applicant.

(1) Applicant ESD Commitments:

- The proposal achieves an equivalent 5-star Green Star and 5.5-star NABERS rating (SMP, p. 4).
- Metering and monitoring strategy applied (SMP, p. 8).
- Independent commissioning agent directly engaged by client for review, advise, monitoring and verification roles commencing from schematic design stage (SMP, p. 8).
- Specifications around low VOC, PVC and formaldehyde products and materials (SMP, p. 9).
- Thermal and visual comfort is enhanced through architectural integration of insulated non-vision glazing into the faceted façade ensuring an appropriate window-wall ratio. All office facades will incorporate internal blinds (SMP, p. 9).
- Air-cooled 3-pipe VRF system, and hot water produced by an electric heat pump with minimum seasonal CoP of 3.0 (SMP, p. 9).
- 39kWp total installed solar PV capacity comprising: 25kW rooftop array using east-west oriented PV configuration, and bespoke 14kW façade-integrated thin-film CIGS cell modules (SMP, p. 9).
- 30 cycle racks in end of trip facilities spaced in accordance with AS2890.3 with a minimum of 20% horizontal racks at grade (SMP, p.).
- A STORM report with a 153% STORM score has been submitted that demonstrates best practice and relies on ~499m² of roof connected to a 16,000-litre rainwater tank connected to toilet flushing and irrigation (SMP, p. 10 & 19).
- Concrete mixes to incorporate reclaimed water and 30% cement reduction, and environmentally responsible steel design and procurement (SMP, p. 10).
- A hazardous materials survey will be carried out prior to the demolition of the existing building to minimise construction health risks (SMP, p. 10).
- A number of innovation credits such as: Specifications around construction waste, indoor plant requirements, air-tightness testing, e-bikes and site-wide leak detection system (SMP, p. 11).

Sustainable Management Plan (SMP)

Referral Response by Yarra City Council



(2) Application ESD Deficiencies:

- There remain concerns for the overall shading properties of the façade along the north (and east and west). Consider increasing the depth and extent of vertical fins (greater than 150mm), or providing an external shading solution for floors not shaded by overhanging storey above.
- While the SMP and STORM report indicate a 16,000-L rainwater tank, the plans (A0950) articulate a 15,000-L tank. Amend plans to match STORM report.

(3) Outstanding Information:

- Clarify provision of outdoor air to office spaces on all levels, and indicate operability of doors (to access terraces) and windows on plans.
- Clarify standard upon which low VOC and formaldehyde limits are specified.
- Clarify provision of operable windows, and natural ventilation to reduce reliance on mechanical systems and lower energy use.
- Provide a JV3 report which articulates comparison with a reference building in terms of building fabric and proposed services.
- Clarify reductions in heating and cooling associated with the proposal in JV3 report.
- Clarify reductions in GHG emissions associated with the proposal in JV3 report.
- Clarify reduction in peak energy demand associated with the proposal in JV3 report.
- Provide more details on HVAC system and energy use and include within JV3 report.
- Confirm reduction in IPD and provide more details on lighting energy efficiency within JV3 report.
- Include solar generation within JV3 report.
- Confirm floor by floor metering for water loads.
- Confirm post-development flows will not exceed pre-development levels.
- Confirm whether any additional retention tanks are included in the design.
- Confirm whether any additional treatment devices (i.e. SPEL) are proposed.
- Clarify whether the proposed design intends to use recycled materials, and if not consider their inclusion (i.e. recycled bricks, or insulation with post-consumer content).
- Clarify strategy to reduce cement and meaning of environmentally sustainable steel design. Consider conducting an LCA on these strategies to identify carbon reductions.
- Confirm extent of sustainably sourced timber (by weight or cost).
- Confirm extent of PVC strategy (by components, weight or cost).
- Clarify provision of visitor bicycle parking, and consider street-grade hoops to service this need.
- Confirm the provision of a unisex/non-gendered shower/changeroom for non-binary building occupants. Consider amending design to provide unisex access.
- Include details of peripheral car share services within Green Travel Plan.
- Provide a Green Travel Plan with performance targets and monitoring and reporting components included.
- Provide a Landscape Plan that details planting schedule, planter and irrigation design, and maintenance considerations.
- Provide a statement as to how the proposal aims to reduce the impacts of urban heat associated with the increase in urban mass. Consider greater plantings and ground level to help cool the streetscape and light coloured (high SRI) roofing materials.
- While all welcome initiatives, ultra-low VOC paints, indoor plant targets and staircase design are not considered innovative in terms of building performance. Provide more information on each innovation to justify inclusion in GS pathway.
- Confirm the provision of a Building Users Guide explaining optimal usage of building services to minimise energy and water consumption.
- Clarify that an Environmental Management Plan will be developed by the building contractor to monitor and control activities undertaken during construction.

(4) ESD Improvement Opportunities

- Consider a small pallet of materials and construction techniques that can assist in disassembly.
- Consider a green roof or wall to improve the ecological value of this site.
- Consider a head contractor that is ISO 14001 accredited.

Attachment 10 - 46-52 Smith Street, Collingwood - ESD referral comments

Sustainable Management Plan (SMP)

Referral Response by Yarra City Council



Further Recommendations:

The applicant is encouraged to consider the inclusion of ESD recommendations, detailed in this referral report. Further guidance on how to meet individual planning conditions has been provided in reference to the individual categories. The applicant is also encouraged to seek further advice or clarification from Council on the individual project recommendations.

Attachment 10 - 46-52 Smith Street, Collingwood - ESD referral comments

1. Indoor Environment Quality (IEQ)

Objectives:

- to achieve a healthy indoor environment quality for the wellbeing of building occupants.
- to provide a naturally comfortable indoor environment will lower the need for building services, such as artificial lighting, mechanical ventilation and cooling and heating devices.

Issues	Applicant's Design Responses	Council Comments	CAR*
Natural Ventilation and Night Purging	No information has been provided.	Clarify provision of outdoor air to office spaces on all levels, and indicate operability of doors (to access terraces) and windows on plans.	3
Daylight & Solar Access	Good. 75.3% of the nominated floor area achieves a DF>2.0 with a VLT of 0.53 (SMP, p. 27 & 28).	Satisfactory.	1
External Views	Green Star credit 12.2 'views' has been claimed, and given above daylight results this is acceptable.	Satisfactory.	1
Hazardous Materials and VOC	The SMP indicates the specification of low VOC paints, adhesives, sealants and carpets, and includes an innovation credit for ultra-low VOC paints. Low formaldehyde engineered wood products also identified (SMP, p. 9).	Clarify standard upon which low VOC and formaldehyde limits are specified.	3
Thermal Comfort	Mechanical ventilation, integration of insulated non-vision glazing into the faceted façade ensuring an appropriate window-wall ratio and incorporated internal blinds (SMP, p. 9).	Clarify provision of operable windows, and natural ventilation to reduce reliance on mechanical systems and lower energy use.	3

* Council Assessment Ratings:

1 – Design Response is **SATISFACTORY**; 2 – Design Response is **NOT SATISFACTORY**
 3 – **MORE INFORMATION** is required; 4 – **ESD IMPROVEMENT OPPORTUNITIES**

References and useful information:

SDAPP Fact Sheet: [1. Indoor Environment Quality](#)
 Good Environmental Choice Australia Standards www.geca.org.au
 Australian Green Procurement www.greenprocurement.org
 Residential Flat Design Code www.planning.nsw.gov.au
 Your Home www.yourhome.gov.au

Attachment 10 - 46-52 Smith Street, Collingwood - ESD referral comments

2. Energy Efficiency

Objectives:

- to ensure the efficient use of energy
- to reduce total operating greenhouse emissions
- to reduce energy peak demand
- to minimize associated energy costs.

Issues	Applicant's Design Responses	Council Comments	CAR*
NCC Energy Efficiency Requirements Exceeded	The proposal sets a 5.5-star NABERS target, which indicates improvement above NCC requirements (SMP, p. 18).	Provide a JV3 report which articulates comparison with a reference building in terms of building fabric and proposed services.	3
Thermal Performance	No information has been provided.	Clarify reductions in heating and cooling associated with the proposal in JV3 report.	3
Greenhouse Gas Emissions	No information has been provided.	Clarify reductions in GHG emissions associated with the proposal in JV3 report.	3
Hot Water System	Hot water will be produced by an electric heat pump with minimum seasonal CoP of 3.0 (SMP, p. 9).	Good. Include details of energy use within JV3 report.	1 / 3
Peak Energy Demand	No information has been provided.	Clarify reduction in peak energy demand associated with the proposal in JV3 report.	3
Effective Shading	The articulate façade at the north-west provides shading to level 1, 3 & 5 – with horizontal PV strips providing minimal shading across each level (Urban Context Report, p. 54).	There remain concerns for the overall shading properties of the façade along the north (and east and west). Consider increasing the depth and extent of vertical fins (greater than 150mm), or providing an external shading solution for floors not shaded by overhanging storey above.	2
Efficient HVAC system	Air-cooled 3-pipe VRF system (SMP, p. 9).	Provide more details on HVAC system and energy use and include within JV3 report.	3
Car Park Ventilation	No basement carpark proposed.	Good.	1
Efficient Lighting	Lighting systems designed to meet best practice illuminance levels (SMP, p. 9).	Confirm reduction in IPD and provide more details on lighting energy efficiency within JV3 report.	3
Electricity Generation	39kWp total installed solar PV capacity comprising: (SMP, p. 9). <ul style="list-style-type: none"> • 25kW rooftop array using east-west oriented PV configuration • bespoke 14kW façade-integrated thin-film CIGS cell modules 	Include solar generation within JV3 report.	1 / 3
Other	-	-	

*** Council Assessment Ratings:**

1 – Design Response is **SATISFACTORY**; 2 – Design Response is **NOT SATISFACTORY**
 3 – **MORE INFORMATION** is required; 4 – **ESD IMPROVEMENT OPPORTUNITIES**

References and useful information:

Attachment 10 - 46-52 Smith Street, Collingwood - ESD referral comments

SDAPP Fact Sheet: [2. Energy Efficiency](#)

House Energy Rating www.makeyourhomegreen.vic.gov.au

Building Code Australia www.abcb.gov.au

Window Efficiency Rating Scheme (WERS) www.wers.net

Minimum Energy Performance Standards (MEPS) www.energyrating.gov.au

Energy Efficiency www.resourcesmart.vic.gov.au

Attachment 10 - 46-52 Smith Street, Collingwood - ESD referral comments

3. Water Efficiency

Objectives:

- to ensure the efficient use of water
- to reduce total operating potable water use
- to encourage the collection and reuse of rainwater and stormwater
- to encourage the appropriate use of alternative water sources (e.g. grey water)
- to minimise associated water costs.

Issues	Applicant's Design Responses	Council Comments	CAR*
Minimising Amenity Water Demand	Minimum WELS star rating of fixtures: (SMP, p. 9) <ul style="list-style-type: none"> • Taps: 5 star • Toilets: 4 star • Showers: 3 star • Urinals: 5 star 	Satisfactory.	1
Water for Toilet Flushing	Rainwater harvesting system draining to 16kl rainwater tanks serving: (SMP, p. 10). <ul style="list-style-type: none"> • WC and urinal flushing • Sub-soil drip irrigation of landscaped areas • Bin wash area 	Satisfactory.	1
Water Meter	Floor-by-floor metering plus all loads >5% of annual energy use or > 100kVA plus metering of all demands >10% of water use (SMP, p. 8).	Confirm floor by floor metering for water loads.	3
Landscape Irrigation	Water efficient sub-soil drip irrigation system with moisture sensors and timer control (SMP, p. 10).	Satisfactory.	1
Other	Fire test system water storage and re-use (SMP, p. 10).	Satisfactory.	1

*** Council Assessment Ratings:**

- 1 – Design Response is **SATISFACTORY**; 2 – Design Response is **NOT SATISFACTORY**
 3 – **MORE INFORMATION** is required; 4 – **ESD IMPROVEMENT OPPORTUNITIES**

References and useful information:

- SDAPP Fact Sheet: [3. Water Efficiency](#)
 Water Efficient Labelling Scheme (WELS) www.waterrating.gov.au
 Water Services Association of Australia www.wsaa.asn.au
 Water Tank Requirement www.makeyourhomegreen.vic.gov.au
 Melbourne Water STORM calculator www.storm.melbournewater.com.au
 Sustainable Landscaping www.ourwater.vic.gov.au

Attachment 10 - 46-52 Smith Street, Collingwood - ESD referral comments

4. Stormwater Management

Objectives:

- to reduce the impact of stormwater runoff
- to improve the water quality of stormwater runoff
- to achieve best practice stormwater quality outcomes
- to incorporate Water Sensitive Urban Design principles.

Issues	Applicant's Design Responses	Council Comments	CAR*
STORM Rating	A STORM report with a 153% STORM score has been submitted that demonstrates best practice and relies on ~499m ² of roof connected to a 16,000-litre rainwater tank connected to toilet flushing and irrigation (SMP, p. 10 & 19).	While the SMP and STORM report indicate a 16,000-L rainwater tank, the plans (A0950) articulate a 15,000-L tank. Amend plans to match STORM report.	2
Discharge to Sewer	GS credit 26.1 claimed for stormwater discharge (SMP, p. 15).	Confirm post-development flows will not exceed pre-development levels.	3
Stormwater Diversion	A catchment area of approx. 499 m ² proposed (SMP, p. 19).	Satisfactory.	1
Stormwater Detention	A 15,000-L rainwater tank proposed.	Confirm whether any additional retention tanks are included in the design.	3
Stormwater Treatment	Stormwater system and treatment as per Rainwater Design & Installation Handbook published by the National Water Commission (SMP, p. 21).	Confirm whether any additional treatment devices (i.e. SPEL) are proposed.	3
Others	-	-	-

*** Council Assessment Ratings:**

- 1 – Design Response is **SATISFACTORY**; 2 – Design Response is **NOT SATISFACTORY**
 3 – **MORE INFORMATION** is required; 4 – **ESD IMPROVEMENT OPPORTUNITIES**

References and useful information:

- SDAPP Fact Sheet: [4. Stormwater Management](#)
 Melbourne Water STORM calculator www.storm.melbournewater.com.au
 Water Sensitive Urban Design Principles www.melbournewater.com.au
 Environmental Protection Authority Victoria www.epa.vic.gov.au
 Water Services Association of Australia www.wsaa.asn.au
 Sustainable Landscaping www.ourwater.vic.gov.au

Attachment 10 - 46-52 Smith Street, Collingwood - ESD referral comments

5. Building Materials

Objectives:

- to minimise the environmental impact of materials used by encouraging the use of materials with a favourable lifecycle assessment.

Issues	Applicant's Design Responses	Council Comments	CAR*
Reuse of Recycled Materials	GS credit 21.1 for Product Transparency and Sustainability has been claimed, however no firm commitments around recycled products/content identified.	Clarify whether the proposed design intends to use recycled materials, and if not consider their inclusion (i.e. recycled bricks, or insulation with post-consumer content).	3
Embodied Energy of Concrete and Steel	Concrete mixes to incorporate reclaimed water and 30% cement reduction, and environmentally responsible steel design and procurement (SMP, p. 10).	Clarify strategy to reduce cement and meaning of environmentally sustainable steel <i>design</i> . Consider conducting an LCA on these strategies to identify carbon reductions.	3
Sustainable Timber	Specification of sustainable sourced timber (FSC or PEFC) (SMP, p. 10).	Confirm extent of sustainably sourced timber (by weight or cost).	3
Design for Disassembly	No information has been provided.	Consider a small pallet of materials and construction techniques that can assist in disassembly.	4
PVC	Specification of common use PVC products that meet Best Practice Guidelines for PVC in the Built Environment (SMP, p. 10).	Confirm extent of PVC strategy (by components, weight or cost).	3

*** Council Assessment Ratings:**

- 1 – Design Response is **SATISFACTORY**; 2 – Design Response is **NOT SATISFACTORY**
 3 – **MORE INFORMATION** is required; 4 – **ESD IMPROVEMENT OPPORTUNITIES**

References and useful information:

SDAPP Fact Sheet: [5. Building Materials](#)
 Building Materials, Technical Manuals www.yourhome.gov.au
 Embodied Energy Technical Manual www.yourhome.gov.au
 Good Environmental Choice Australia Standards www.geca.org.au
 Forest Stewardship Council Certification Scheme www.fsc.org
 Australian Green Procurement www.greenprocurement.org

Attachment 10 - 46-52 Smith Street, Collingwood - ESD referral comments

6. Transport

Objectives:

- to minimise car dependency
- to ensure that the built environment is designed to promote the use of public transport, walking and cycling.

Issues	Applicant's Design Responses	Council Comments	CAR*
Minimising the Provision of Car Parks	No carparking proposed.	Good.	1
Bike Parking Spaces	30 bike parking spaces provided in the basements (15x Cora E3DT-FGT) for employees along with a bike maintenance station and floor pump (SMP, p. 9).	Clarify provision of visitor bicycle parking, and consider street-grade hoops to service this need.	3
End of Trip Facilities	End of trip facilities have been provided in the form of 4x showers and 50 lockers (A00099).	Confirm the provision of a unisex/non-gendered shower/changeroom for non-binary building occupants. Consider amending design to provide unisex access.	3
Car Share Facilities	No information has been provided.	Include details of peripheral car share services within Green Travel Plan.	3
Electric vehicle charging	n/a due to absence of car parking, however 2x e-bike chargers provided in basement 1 (SMP, p. 9).	Satisfactory.	1
Green Travel Plan	A Green Travel plan has not been provided.	Provide a Green Travel Plan with performance targets and monitoring and reporting components included.	3

*** Council Assessment Ratings:**

1 – Design Response is **SATISFACTORY**; 2 – Design Response is **NOT SATISFACTORY**
 3 – **MORE INFORMATION** is required; 4 – **ESD IMPROVEMENT OPPORTUNITIES**

References and useful information:

- SDAPP Fact Sheet: [6. Transport](#)
 Off-setting Car Emissions Options www.greenfleet.com.au
 Sustainable Transport www.transport.vic.gov.au/doi/internet/icy_nsf
 Car share options www.yarracity.vic.gov.au/Parking-roads-and-transport/Transport-Services/Carsharing/
 Bicycle Victoria www.bv.com.au

Attachment 10 - 46-52 Smith Street, Collingwood - ESD referral comments

7. Waste Management

Objectives:

- to ensure waste avoidance, reuse and recycling during the design, construction and operation stages of development
- to ensure long term reusability of building materials.
- to meet Councils' requirement that all multi-unit developments must provide a Waste Management Plan in accordance with the *Guide to Best Practice for Waste Management in Multi-unit Developments 2010*, published by Sustainability Victoria.

Issues	Applicant's Design Responses	Council Comments	CAR*
Construction Waste Management	The SMP indicates a target to divert 90% of demolition and construction waste from landfill (SMP, p. 10).	Satisfactory.	1
Operational Waste Management	An operational Waste Management Plan has been provided, and a 25 m ² bin room located in the basement (A00099).	Satisfactory.	1
Storage Spaces for Recycling and Green Waste	The WMP caters for both recycling and food organic waste, with spatial provision in the bin room (WMP, p. 4).	Satisfactory.	1
Others	-	-	-

*** Council Assessment Ratings:**

1 – Design Response is **SATISFACTORY**; 2 – Design Response is **NOT SATISFACTORY**
 3 – **MORE INFORMATION** is required; 4 – **ESD IMPROVEMENT OPPORTUNITIES**

References and useful information:

SDAPP Fact Sheet: [7. Waste Management](#)
 Construction and Waste Management www.sustainability.vic.gov.au
 Preparing a WMP www.epa.vic.gov.au
 Waste and Recycling www.resourcesmart.vic.gov.au
 Better Practice Guide for Waste Management in Multi-Unit Dwellings (2002) www.environment.nsw.gov.au
 Waste reduction in office buildings (2002) www.environment.nsw.gov.au

Attachment 10 - 46-52 Smith Street, Collingwood - ESD referral comments

8. Urban Ecology

Objectives:

- to protect and enhance biodiversity
- to provide sustainable landscaping
- to protect and manage all remnant indigenous plant communities
- to encourage the planting of indigenous vegetation.

Issues	Applicant's Design Responses	Council Comments	CAR*
On Site Topsoil Retention	There is no productive topsoil on this site.	-	N/A
Maintaining / Enhancing Ecological Value	Vegetation on terraces on level 1, 2 and 7 (as per plans), however no Landscape Plan provided.	Provide a Landscape Plan that details planting schedule, planter and irrigation design, and maintenance considerations.	3
Heat Island Effect	No information has been provided.	Provide a statement as to how the proposal aims to reduce the impacts of urban heat associated with the increase in urban mass. Consider greater plantings and ground level to help cool the streetscape and light coloured (high SRI) roofing materials.	3
Other	A hazardous materials survey will be carried out prior to the demolition of the existing building to minimise construction health risks (SMP, p. 10).	Satisfactory.	1
Green wall, roofs, facades	No information has been provided.	Consider a green roof or wall to improve the ecological value of this site.	4

*** Council Assessment Ratings:**

1 – Design Response is **SATISFACTORY**; 2 – Design Response is **NOT SATISFACTORY**
 3 – **MORE INFORMATION** is required; 4 – **ESD IMPROVEMENT OPPORTUNITIES**

References and useful information:

SDAPP Fact Sheet: [8. Urban Ecology](#)
 Department of Sustainability and Environment www.dse.vic.gov.au
 Australian Research Centre for Urban Ecology www.arcue.botany.unimelb.edu.au
 Greening Australia www.greeningaustralia.org.au
 Green Roof Technical Manual www.yourhome.gov.au

Attachment 10 - 46-52 Smith Street, Collingwood - ESD referral comments

9. Innovation

Objective:

- to encourage innovative technology, design and processes in all development, which positively influence the sustainability of buildings.

Issues	Applicant's Design Responses	Council Comments	CAR*
Significant Enhancement to the Environmental Performance	5 innovation credits have been claimed in the GS pathway, with the SMP identifying: ultra-low VOC paints, low construction waste target, Stormwater Pollution reduction to Column B, Indoor plant targets, air-tightness, staircase design, 2x e-bikes & charging and site-wide leak detection.	While all welcome initiatives, ultra-low VOC paints, indoor plant targets and staircase design are not considered innovative in terms of building performance. Provide more information on each innovation to justify inclusion in GS pathway.	3
Innovative Social Improvements	-	-	-
New Technology	-	-	-
New Design Approach	-	-	-
Others	-	-	-

*** Council Assessment Ratings:**

- 1 – Design Response is **SATISFACTORY**; 2 – Design Response is **NOT SATISFACTORY**
 3 – **MORE INFORMATION** is required; 4 – **ESD IMPROVEMENT OPPORTUNITIES**

References and useful information:

- SDAPP Fact Sheet: [9. Innovation](#)
 Green Building Council Australia www.gbca.org.au
 Victorian Eco Innovation lab www.ecoinnovationlab.com
 Business Victoria www.business.vic.gov.au
 Environment Design Guide www.environmentdesignguide.com.au

Attachment 10 - 46-52 Smith Street, Collingwood - ESD referral comments

10. Construction and Building Management

Objective:

- to encourage a holistic and integrated design and construction process and ongoing high performance

Issues	Applicant's Design Responses	Council Comments	CAR*
Building Tuning	The SMP indicates the provision of commissioning specification, along with a Commissioning and Tuning Plan, and comprehensive O&M manual for FM team (SMP, p. 8).	Satisfactory.	1
Building Users Guide	No information has been provided.	Confirm the provision of a Building Users Guide explaining optimal usage of building services to minimise energy and water consumption.	3
Contractor has Valid ISO14001 Accreditation	No information has been provided.	Consider a head contractor that is ISO 14001 accredited.	4
Construction Management Plan	No information has been provided.	Clarify that an Environmental Management Plan will be developed by the building contractor to monitor and control activities undertaken during construction.	3
Others	-	-	-

*** Council Assessment Ratings:**

- 1 – Design Response is **SATISFACTORY**; 2 – Design Response is **NOT SATISFACTORY**
 3 – **MORE INFORMATION** is required; 4 – **ESD IMPROVEMENT OPPORTUNITIES**

References and useful information:

SDAPP Fact Sheet: [10. Construction and Building Management](#)
 ASHRAE and CIBSE Commissioning handbooks
 International Organization for standardization – ISO14001 – Environmental Management Systems
 Keeping Our Stormwater Clean – A Builder's Guide www.melbournewater.com.au

Sustainable Management Plan (SMP)
for planning applications being considered by Yarra Council



Applicant Response Guidelines

Project Information:

Applicants should state the property address and the proposed development's use and extent. They should describe neighbouring buildings that impact on or may be impacted by the development. It is required to outline relevant areas, such as site permeability, water capture areas and gross floor area of different building uses. Applicants should describe the development's sustainable design approach and summarise the project's key ESD objectives.

Environmental Categories:

Each criterion is one of the 10 Key Sustainable Building Categories. The applicant is required to address each criterion and demonstrate how the design meets its objectives.

Objectives:

Within this section the general intent, the aims and the purposes of the category are explained.

Issues:

This section comprises a list of topics that might be relevant within the environmental category. As each application responds to different opportunities and constraints, it is not required to address all issues. The list is non-exhaustive and topics can be added to tailor to specific application needs.

Assessment Method Description:

Where applicable, the Applicant needs to explain what standards have been used to assess the applicable issues.

Benchmarks Description:

The applicant is required to briefly explain the benchmark applied as outlined within the chosen standard. A benchmark description is required for each environmental issue that has been identified as relevant.

How does the proposal comply with the benchmarks?

The applicant should show how the proposed design meets the benchmarks of the chosen standard through making references to the design brief, drawings, specifications, consultant reports or other evidence that proves compliance with the chosen benchmark.

ESD Matters on Architectural Drawings:

Architectural drawings should reflect all relevant ESD matters where feasible. As an example, window attributes, sun shading and materials should be noted on elevations and finishes schedules, water tanks and renewable energy devices should be shown on plans. The site's permeability should be clearly noted. It is also recommended to indicate water catchment areas on roof- or site plans to confirm water re-use calculations.

Attachment 11 - 46 - 52 Smith Street, Collingwood - Strategic Transport comments



Planning Referral

To: Lara Fiscalini
From: Chloe Wright
Date: 15/04/2021
Subject: Strategic Transport Comments
Application No: PLN21/0042
Description: Construction of an 8-storey development, use of the land for restricted recreation facility (gymnasium), food and drink premises and office (permit required for gymnasium) and a reduction in the statutory car parking rate.
Site Address 46 – 52 Smith Street, Collingwood

I refer to the above application and the accompanying Traffic report prepared by Traffix Group in relation to the proposed development at 46 – 52 Smith Street, Collingwood. Council's Strategic Transport unit provides the following information:

Access and Safety

No access or safety issues have been identified.

Bicycle Parking Provision

Statutory Requirement

Under the provisions of Clause 52.34-3 of the Yarra Planning Scheme, the development's bicycle parking requirements are as follows:

Proposed Use	Quantity/ Size	Statutory Parking Rate	No. of Spaces Required	No. of Spaces Allocated
Office	2,168 sqm	1 employee space to each 300 sqm of net floor area if the net floor area exceeds 1000 sqm	7 employee spaces	
		1 visitor space to each 1000 sqm of net floor area if the net floor area exceeds 1000 sqm	2 visitor spaces.	
Restaurant	244 sqm	1 employee space to each 100 sqm of floor area available to the public	2 employee spaces	
		2 plus 1 visitor space to each 200 sqm of floor area available to the public if the floor area exceeds 400 sqm	0 visitor space	
Food and Drink Premises	130 sqm	1 employee space to each 100 sqm of floor area available to the public	1 employee spaces	
		2 plus 1 space to each 200 sqm of floor area available to public if floor area available exceeds 400 sqm	0 visitor spaces.	
Minor sports and rection facility (Gym)	302 sqm 2 staff	1 employee space per 4 employees	1 employee spaces	
		1 space per 200 sqm of net floor area	2 visitor spaces	

Attachment 11 - 46 - 52 Smith Street, Collingwood - Strategic Transport comments

Bicycle Parking Spaces Total		11 employee spaces	30 employee spaces
		4 visitor spaces	3 visitor spaces
Showers / Change rooms	1 to the first 5 employee spaces and 1 to each additional 10 employee spaces	1 showers / change rooms	4 showers / change rooms

Adequacy of visitor bicycle spaces

The following comments are provided in relation to provision of visitor bicycle spaces:

- 3 visitor spaces are provided within the gym and no publicly accessible visitor spaces are proposed. Council’s best-practice rate¹ recommends a total of 6 visitor spaces.
- It is recommended that additional visitor spaces are provided at the street level, including 2 hoops at the Derby Street footpath and 2 hoops at the easement, as per the locations marked up in blue on the screenshot below.
- All visitor spaces should be provided as a horizontal at-grade space and positioned in accordance with access and clearance requirements of AS2890.3 and Council’s urban design standard hoop detail (attached).

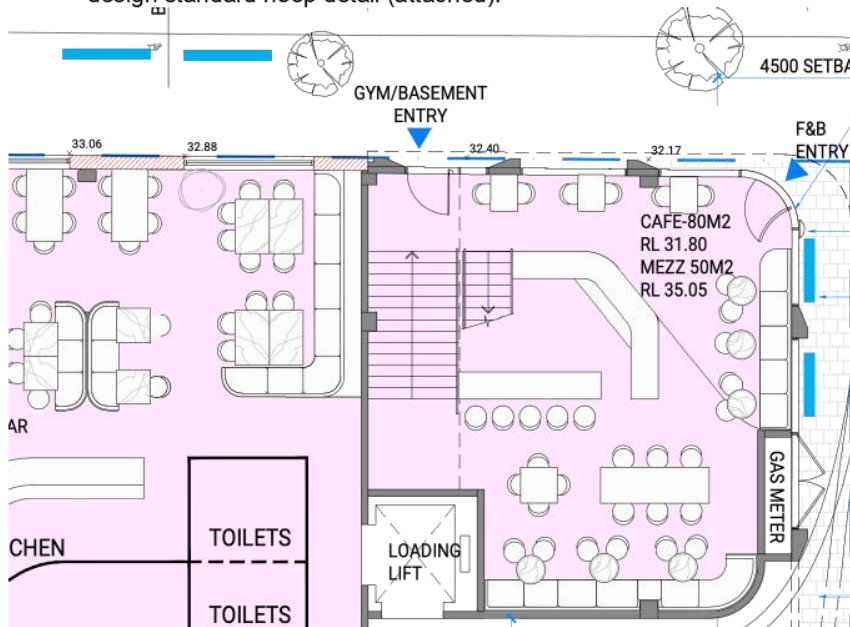


Figure 1 – locations for additional visitor bicycle spaces

Adequacy of employee spaces

Number of spaces

30 employee spaces are proposed, which exceeds Council’s best practice recommendation² of 26 employee spaces.

Design and location of employee spaces and facilities

The location and design of employee bike parking is adequate for the following reasons:

¹ Category 6 of the Built Environment Sustainability Scorecard (BESS) recommends 1 visitor space to each 500sqm of office floor space.

² Category 6 of the Built Environment Sustainability Scorecard (BESS) offers the following for best-practice guidance for employee office rates: ‘Non-residential buildings should provide spaces for at least 10% of building occupants.’ Assuming a floor-space occupancy of 1 staff member to 10sqm (which is the maximum rate allowed under the National Construction Code for fire safety), providing bicycle spaces for 10% of occupants results in a rate of 1 space per 100sqm of floor area

Attachment 11 - 46 - 52 Smith Street, Collingwood - Strategic Transport comments

- All employee bicycle spaces are provided within a secure facility at the basement level, with access via the entrance at Smith Street and lift shaft;
- The provision of two-tier bicycle racks satisfies the AS2890.3 requirement for at least 20% of bicycle storage spaces to be provided as horizontal at ground-level spaces;
- Dimensions of bicycle spaces are noted on the plans and demonstrate the layout is in accordance with access and clearance requirements of AS2890.3;
- An end of trip facilities area is located adjacent to the bicycle parking and includes four shower / change rooms and 50 lockers, which meets Council's best practice recommendation of 3 shower / change rooms.

Green Travel Plan

Given the development has a total non-residential floor area of more than 1,000sqm, pursuant to Clause 22.17-4 a Green Travel Plan (GTP) must be provided. The following information should be included:

- (a) Description of the location in the context of alternative modes of transport;
- (b) Employee welcome packs (e.g. provision of Myki/transport ticketing);
- (c) Sustainable transport goals linked to measurable targets, performance indicators and monitoring timeframes;
- (d) A designated 'manager' or 'champion' responsible for co-ordination and implementation;
- (e) Details of bicycle parking and bicycle routes;
- (f) Details of Green Travel funding and management responsibilities;
- (g) The types of bicycle storage devices proposed to be used for employee and visitor spaces (i.e. hanging or floor mounted spaces);
- (h) Security arrangements to access the employee bicycle storage spaces;
- (i) Signage and wayfinding information for bicycle facilities and pedestrians pursuant to Australian Standard AS2890.3; and
- (j) Provisions for the GTP to be updated not less than every five years.

Recommendations

The following should be shown on the plans before endorsement:

1. Six visitor bicycle spaces, including four spaces at Derby Street and two spaces at the easement. All visitor spaces should be a horizontal at-grade bike hoop and in accordance with access and clearance requirements of AS2890.3 or otherwise to the satisfaction of the responsible authority.

A Green Travel Plan should be provided with the information outlined previously.

Regards

Chloe Wright

Sustainable Transport Officer
Strategic Transport Unit

Attachment 12 - 46-52 Smith Street, Collingwood - Engineering comments



MEMO

To: Lara Fiscalini
From: Artemis Bacani
Date: 23 April 2021
Subject: Application No: PLN21/0042
 Description: Mixed Use Development
 Site Address: 46-52 Smith Street, Collingwood

I refer to the above Planning Application received on 30 March 2021 in relation to the proposed development at 46-52 Smith Street, Collingwood. Council's Engineering Referral team provides the following information:

Drawings and Documents Reviewed

	Drawing No. or Document	Revision	Dated
XO Projects	A00002 <i>Existing Conditions Survey</i>	B	24 February 2021
	A00099 <i>Basement Floor Plan B1</i>	B	24 February 2021
	A0100 <i>Ground Floor Plan L00</i>	B	24 February 2021
	A0900 <i>West Elevation – Smith Street</i>	B	24 February 2021
	A0901 <i>North Elevation – Derby Street</i>	B	24 February 2021
	A0902 <i>East Elevation – Laneway</i>	B	24 February 2021
	A0950 <i>Section AA</i>	B	24 February 2021
Traffix Group	<i>Traffic Engineering Assessment</i>	C	10 December 2020

Attachment 12 - 46-52 Smith Street, Collingwood - Engineering comments

CAR PARKING PROVISION

Proposed Development

Under the provisions of Clause 52.06-5 of the Yarra Planning Scheme, the development's parking requirements are as follows:

Proposed Use	Quantity/ Size	Statutory Parking Rate*	No. of Spaces Required	No. of Spaces Allocated
Office	2,168 m ²	3.0 spaces per 100 m ² of net floor area	65	0
Restricted Recreation Facility	302 m ²	Rate Not Specified	To the satisfaction of the Responsible Authority	
Food and drink premises	130 m ²	3.5 spaces per 100 m ² of leasable floor area	4	
Restaurant	244 m ²	3.5 spaces per 100 m ² of leasable floor area	8	
Total			77 Spaces plus Spaces for the Restricted Recreation Facility	0 Spaces

* Since the site is located within the Principal Public Transport Network Area, the parking rates in Column B of Clause 52.06-5 now apply.

Since no on-site car spaces will be provided, a full waiver in the car parking requirements is sought by the applicant.

To reduce the number of car parking spaces required under Clause 52.06-5 (including to reduce to zero spaces), the application for the car parking reduction must be accompanied by a Car Parking Demand Assessment.

Car Parking Demand Assessment

In reducing the number of parking spaces required for the proposed development, the Car Parking Demand Assessment would assess the following:

- *Parking Demand for Office Use.*
The office use would have no on-site car parking. Staff and clients to the site would be inclined to park on-street or commute by sustainable transportation modes. The actual parking demand generated by the office use is expected to be lower than the statutory parking rate of 3.0 spaces per 100 square metres of floor space, since the area has very good access to public transport services and located along an activity centre. Throughout the municipality, a number of developments have been approved with no car spaces or a reduced office rate, as shown in the following table:

Attachment 12 - 46-52 Smith Street, Collingwood - Engineering comments

Development Site	Approved Office Parking Rate
Cremorne	
60-88 Cremorne Street PLN17/0626 issued 21 June 2018	0.85 spaces per 100 m ² (233 on-site spaces; 27,306 m ²)
9-11 Cremorne Street PLN16/0171 (Amended) issued 13 June 2017	0.85 spaces per 100 m ² (20 on-site spaces; 2,329 m ²)
Collingwood	
2-16 Northumberland Street PLN16/1150 issued 14 June 2017	0.89 spaces per 100 m ² (135 on-site spaces; 15,300 m ²)

The site's good accessibility to public transport services and proximity to Melbourne would encourage visitors to the site to utilise more sustainable forms of transportation such as catching public transport or riding a bicycle.

- *Parking Demand for the Restricted Recreational Facility Use.*
All parking for patrons to the restricted recreational facility would be accommodated off-site. Restricted recreation facilities in the municipality (including pilates studios, gymnasiums, dance schools etc) have been approved with either no on-site parking or with one or two on-site spaces. The table below provides some recently approved recreation facilities in Yarra:

Development Site	Approved Parking
Cremorne	
Yoga Studio 94 Cubitt Street PLN15/0019 issued 4 May 2016	2 on-site spaces (32 patrons)
Abbotsford	
Gymnasium (24 hour) 563 Victoria Street PLN16/0948 issued 3 February 2017	No on-site car parking (40 patrons)
Yoga Studio 96 Nicholson Street PLN14/1065 issued 27 July 2015	No on-site car parking (12 patrons)
Fitzroy	
Gymnasium (24 hour) 224 Brunswick Street PLN14/0892 issued 14 January 2015	No on-site car parking (40 patrons)

The site is located close to tram and bus services allowing staff and patrons to commute to and from the site by public transport.

- *Parking Demand for Food & Drink Use and Restaurant Use*
A staff parking demand of 1 space per 100 square metres of floor area for each use could be adopted. Using this rate would equate to 3 spaces. This reflects the precinct approach for similar food and drink and restaurant uses and acknowledges that a proportion of customers for this use will be drawn from employees and residents from surrounding businesses and residences.

Attachment 12 - 46-52 Smith Street, Collingwood - Engineering comments

- *Availability of Public Transport in the Locality of the Land.* The following public transport services can be accessed to and from the site by foot:
 - Smith Street-Gertrude Street tram services – 70 metre walk
 - Victoria Parade tram services – 200 metre walk
 - Victoria Parade bus services – 220 metre walk
 - Nicholson Street tram services – 500 metre walk
 - Punt Road bus services – 810 metre walk
 - North Richmond railway station – 900 metre walk
 - Parliament railway station – 960 metre walk

- *Multi-Purpose Trips within the Area.*
Clients and customers to the development might combine their visit by engaging in other activities or business whilst in the area.

- *Convenience of Pedestrian and Cyclist Access.*
The site has good pedestrian access to public transport nodes and the Smith Street and Gertrude Street activity centres. The site also has good connectivity to the on-road bicycle network.

Appropriateness of Providing Fewer Spaces than the Likely Parking Demand

Clause 52.06 lists a number of considerations for deciding whether the required number of spaces should be reduced. For the subject site, the following considerations are as follows:

- *Availability of Car Parking.*
Due to the COVID-19 restrictions, car parking occupancy surveys cannot be undertaken at this time as it would not provide an accurate representation of the car parking demands in the surrounding area.

The on-street parking in this part of Collingwood/Fitzroy is generally high during business hours and in the evenings. The area surrounding the subject site is blanketed in time-based parking restrictions which ensure that parking turns over frequently. Visitors to the site during business hours should be able to find an on-street car space near the site.

- *Relevant Local Policy or Incorporated Document.* The proposed development is considered to be in line with the objectives contained in Council's *Strategic Transport Statement*. The site is ideally located with regard to sustainable transport alternatives and the reduced provision of on-site car parking would potentially discourage private motor vehicle ownership and use.

Adequacy of Car Parking

From a traffic engineering perspective, the full waiver of parking associated with the site is considered appropriate in the context of the development and the surrounding area. The site is located close to various forms of public transport and close to the Smith Street and Gertrude Street activity centres. The operation of the development should not adversely impact on existing on-street parking conditions in the area.

The Engineering Referral team has no objection to the reduction in the car parking requirement for this site.

Attachment 12 - 46-52 Smith Street, Collingwood - Engineering comments

**DEVELOPMENT LAYOUT DESIGN
Layout Design Assessment**

Item	Assessment
Access Arrangements	
Width of Loading Bay Entrance	The entrance to the loading bay is 6.27 metres in width.
Width of Laneway	The laneway is 2.94 metres in width.
Vehicle Turning Movements – 6.4 metre Long Waste Wise Mini	The swept path diagrams for the 6.4 metre long was collection vehicle demonstrates adequate access into the and out of the site off the laneway.
Headroom Clearance	The headroom clearance at the loading bay entrance is not dimensioned on the drawings.
Other Items	
Loading Bay Dimensions	The dimensions of the loading bay of 5.0 metres by 9.24 metres is adequate to accommodate a 6.4 metre long Small Rigid Vehicle as per Australian/New Zealand Standard AS/NZS 2890.2:2002.
Ground Clearance Check – Via Laneway	A clearance check for the loading bay entrance is to be undertaken by the applicant’s designer to confirm that a B99 design vehicle can enter and exit the property without scraping or bottoming-out (Please see under ‘ <i>Design Items to be Addressed</i> ’ section).
Canopy – South-West Corner of Site	The canopy along the Smith Street frontage is setback 750 millimetres from the edge of the kerb and has a clearance height above the footpath of 3.825 to satisfy the <i>Building Regulations 2008 – Reg 103</i> .
Canopy – North-West Corner of Site	The canopy along the Derby Street frontage is setback 1.26 metres from the edge of the kerb. The setback of the canopy along the Smith Street frontage is not dimensioned. The clearance height above the footpath is also not dimensioned.
Canopy – North-East Corner of Site	The elevation drawings show the canopy overhangs beyond the property line and into the laneway.

Design Items to be Addressed

Item	Details
Headroom Clearance	The headroom clearance at the loading bay entrance is to be dimensioned on the drawings.
Canopy – North-West Corner of Site	The clearance height above the footpath and setback from the edge of kerb for the section of canopy along the Smith Street frontage are to be dimensioned on the drawings.
Canopy – North-East Corner of Site	No part of the canopy is to encroach into the laneway. To allow a 6.4 metre long Small Rigid Vehicle with a height of 3.5 metres to pass under the canopy, the clearance height of the canopy along the laneway must be greater than 3.5 metres. The setback (from the edge of the kerb) and clearance height (above the footpath) of the canopy along the Derby Street frontage is to be dimensioned on the drawings

Attachment 12 - 46-52 Smith Street, Collingwood - Engineering comments

Design Items to be Addressed

Item	Details
Vehicle Crossing Ground Clearance Check	<p>To demonstrate adequate access for a B99 design vehicle and the provision of the 40 mm lip from the edge of the laneway to the finished floor level of the front edge of the slab at ground level, the applicant must prepare a cross sectional drawing showing the reduced level of the west and east edge of the laneway, the centreline of the laneway, and the finished floor level of the slab with the 40 mm lip from the edge of the laneway.</p> <p>These levels are to be shown on a 1 in 20 scale cross sectional drawing, with dimensions, together with the B99 design vehicle ground clearance template demonstrating access into and out of the development.</p> <p>Providing the ground clearance check early in the design phase can also determine whether further modification works are required, such as lowering the finished floor level inside the property or making any adjustments to Council's footpaths or road infrastructure.</p>
Utility Service Cabinet – Laneway	<p>To improve truck access along the easement section adjacent the laneway, it is recommended for the applicant to consider designing the utility service cabinet door to swing at 180-degrees and be latched onto the building wall when in service.</p>
Bicycle Considerations	<p>The bicycle requirements for this development are to be referred to Council's Strategic Transport unit for assessment.</p>
Proposed Street Tree – Derby Street	<p>The applicant is to liaise with Council's Open Space unit regarding the proposed tree in Derby Street.</p>

ENGINEERING CONDITIONS

Civil Works

Upon the completion of all building works and connections for underground utility services,

- The kerb and channel along the property's Smith Street and Derby Street frontages must be reconstructed to Council's satisfaction and at the Permit Holder's cost.
- The footpath along the property's Smith Street and Derby Street frontages must be reconstructed to Council's satisfaction and at the Permit Holder's cost. The footpath must have a cross-fall of 1 in 33 (for asphalt) or unless otherwise specified by Council.

Road Asset Protection

- Any damaged roads, footpaths and other road related infrastructure adjacent to the development site as a result of the construction works, including trenching and excavation for utility service connections, must be reconstructed to Council's satisfaction and at the developer's expense.

Construction Management Plan

- A Construction Management Plan must be prepared and submitted to Council. The Plan must be approved by Council prior to the commencement of works. A detailed dilapidation report should detail and document the existing and post construction conditions of surrounding road infrastructure and adjoining private properties.

Attachment 12 - 46-52 Smith Street, Collingwood - Engineering comments

Impact of Assets on Proposed Development

- Any services poles, structures or pits that interfere with the proposal must be adjusted, removed or relocated at the owner’s expense after seeking approval from the relevant authority.
- Areas must be provided inside the property line and adjacent to the footpath to accommodate pits and meters. No private pits, boundary traps, valves or meters on Council property will be accepted.

Discharge of Water from Development

- Only roof runoff, surface water and clean groundwater seepage from above the water table can be discharged into Council drains.
- Council will not permit clean groundwater from below the groundwater table to be discharged into Council’s drainage system. Basements that extend into the groundwater table must be waterproofed/tanked.

ADDITIONAL ENGINEERING ADVICE FOR THE APPLICANT

Item	Details
Legal Point of Discharge	The applicant must apply for a Legal Point of Discharge under Regulation 133 – Stormwater Drainage of the <i>Building Regulations</i> 2018 from Yarra Building Services unit. Any storm water drainage within the property must be provided and be connected to the nearest Council pit of adequate depth and capacity (legal point of discharge), or to Council’s satisfaction under Section 200 of the <i>Local Government Act</i> 1989 and Regulation 133.
Clearances to Electrical Assets	Overhead power lines run along the east side of Smith Street and south side of Derby Street, close to the property boundary. The developer needs to ensure that the building has adequate clearances from overhead power cables, transformers, substations or any other electrical assets where applicable. Energy Safe Victoria has published an information brochure, <i>Building design near powerlines</i> , which can be obtained from their website: http://www.esv.vic.gov.au/About-ESV/Reports-and-publications/Brochures-stickers-and-DVDs
Tree Protection	The applicant is to liaise with Council’s Open Space unit regarding the protection of the street trees along the property’s Smith Street and Derby Street frontages.

Attachment 13 - 46-52 Smith Street, Collingwood - City Works comments



Date: 8 April 2021

Property Address: 46-52 Smith Street, Collingwood

Application No: PLN21/0042

Officer: Atha Athanasi

City works referral comments:

The waste management plan for 46-52 Smith Street, Collingwood authored by Leigh Design and dated 13/11/20 is satisfactory from a City Works Branch's perspective.