



Ordinary Meeting of Council Agenda

to be held on Tuesday 2 October 2012 at 6.30 pm
at the Richmond Town Hall

Disability - Access and Inclusion to Committee and Council Meetings:

Facilities/services provided at the Richmond and Fitzroy Town Halls:

- Entrance ramps and lifts (off Moor Street at Fitzroy, entry foyer at Richmond)
- Hearing loop (Richmond only), the receiver accessory may be accessed by request to either the Chairperson or the Governance Officer at the commencement of the meeting, proposed resolutions are displayed on large screen and Auslan interpreting (*by arrangement, tel. 9205 5110*)
- Electronic sound system amplifies Councillors' debate
- Interpreting assistance (*by arrangement, tel. 9205 5110*)
- Disability accessible toilet facilities

Order of business

- 1. Statement of recognition of Wurundjeri Land**
- 2. Attendance, apologies and requests for leave of absence**
- 3. Declarations of conflict of interest (Councillors and staff)**
- 4. Confirmation of minutes**
- 5. Petitions and joint letters**
- 6. Public question time**
- 7. General business**
- 8. Delegates' reports**
- 9. Questions without notice**
- 10. Council business reports**
- 11. Notices of motion**
- 12. Urgent business**
- 13. Confidential business reports**

1. Statement of Recognition of Wurundjeri Land

“Welcome to the City of Yarra. Council acknowledges the Wurundjeri community as the first owners of this country. Today, they are still the custodians of the cultural heritage of this land. Further to this, Council acknowledges there are other Aboriginal and Torres Strait Islander people who have lived, worked and contributed to the cultural heritage of Yarra.”

1A. Presentation to Officers in Receipt of Recent Awards

2. Attendance, apologies and requests for leave of absence

Anticipated attendees:

Councillors

- Cr Geoff Barbour (Mayor)
- Cr Alison Clarke
- Cr Jackie Fristacky
- Cr Josh Funder
- Cr Sam Gaylard
- Cr Stephen Jolly
- Cr Anthony Main
- Cr Dale Smedley
- Cr Amanda Stone

Council officers

- Vijaya Vaidyanath (Chief Executive Officer)
- Jack Crawford (Director Corporate and Financial Services)
- Ivan Gilbert (Executive Manager Governance)
- Craig Kenny (Director Community Programs)
- Bruce Phillips (Director City Development)
- Guy Wilson-Browne (Director Infrastructure Services)
- Margaret Elvey (Governance Officer)

3. Declarations of conflict of interest (Councillors and staff)

4. Confirmation of minutes

Recommendation

That the minutes of the Special Council Meeting held on Monday 17 September 2012 and the Ordinary Council Meeting held on Tuesday 18 September 2012 be confirmed.

5. Petitions and joint letters

6. Public question time

Yarra City Council welcomes questions from members of the community.

Public question time is an opportunity to ask questions, not to make statements or engage in debate.

Questions should not relate to items listed on the agenda. (Council will consider submissions on these items separately.)

Members of the public who wish to participate are to:

- (a) state their name clearly for the record;
- (b) direct their questions to the chairperson;
- (c) ask a maximum of two questions;
- (d) speak for a maximum of five minutes;
- (e) refrain from repeating questions that have been asked previously by themselves or others; and
- (f) remain silent following their question unless called upon by the chairperson to make further comment.

7. General business

8. Delegates' reports

9. Questions without notice

10. Council business reports

| Item | | Page | Rec. Page | Report Presenter |
|------|---|------|-----------|---|
| 10.1 | 1 - 21 Robert Street Collingwood VIC 3066 - Planning Permit Application PLN11/0750 | 6 | 93 | Mary Osman - Manager Statutory Planning |
| 10.2 | Economic Development Strategy 2009 - 2014 Annual Progress Report Year 3 | 177 | 180 | Kim Swinson - Coordinator Economic Development |
| 10.3 | Audit Committee Annual Report and Appointment of New Independent Member | - | - | Philip Mason – Manager Finance |

To be distributed separately

Public submissions procedure

The public submission period is an opportunity to provide information to Council, not to ask questions or engage in debate.

When the chairperson invites verbal submissions from the gallery, members of the public who wish to participate are to:

- (a) state their name clearly for the record;
- (b) direct their submission to the chairperson;
- (c) speak for a maximum of five minutes;
- (d) confine their remarks to the matter under consideration;
- (e) refrain from repeating information already provided by previous submitters; and
- (f) remain silent following their submission unless called upon by the chairperson to make further comment.

11. Notices of motion

Nil

12. Urgent business

Nil

13. Confidential business reports

13.1 Matters prejudicial to Council and/or any person

**10.1 1 - 21 Robert Street Collingwood VIC 3066 - Planning Permit Application
PLN11/0750**

Executive Summary

The recommended decision is not a “Major Policy Decision” within the context of Local Government Act 1989. The recommended decision is a “Significant Decision” within the meaning of the Councillors Code of Conduct, but an exception should be made due to the statutory time limits in which to determine the application under the Planning and Environment Act 1989

This item has been deferred from the Special Council Meeting held Monday 17 September. Additional information will not be available until after the meeting with Developer on Monday 1 October and as such will be circulated to Councillors on Tuesday.

Purpose

This report provides Council with an assessment of Planning Permit Application No. PLN11/0750, which affects land at 1-21 Robert Street, Collingwood; more commonly known as the former Yorkshire Brewery site, and recommends approval subject to conditions.

The Applicant has also sought to remove the Section 173 Agreement that applies to the site. This report addresses this request and recommends that further discussions are undertaken between Council, Heritage Victoria and the Permit Applicant before this occurs. A resolution would likely result in parties entering into a new Section 173 Agreement to ensure that the intent of the original agreement is maintained and up-to-date.

Background

The Applicant is proposing to undertake a redevelopment of the site for a mixed use development comprising a number of buildings (up to 17 levels plus two basement levels); use of the land as a food and drink premises (cafe), and convenience shop (deli); reduction in the car parking requirements; and waiver of the loading and unloading bay provisions.

The site is listed on the Victorian Heritage Register (VHR number H0807) meaning the site is of state heritage significance. A comparable development has already obtained approval from Heritage Victoria via permit P16751 issued on 20 April 2012. It is noted that Council objected to the Heritage Victoria application via resolution of Council on 8 November 2011.

Heritage Victoria has subsequently amended the permit and endorsed plans on 3 September 2012 to align with the amended plans being considered under this application. Additionally, conditions have been amended to vary the staging works proposed and to include a requirement for a bank guarantee worth 1 Million dollars.

As a consequence of the Heritage Victoria Permit, the considerations of this application outlined within this report therefore do not include heritage matters.

Should a planning permit issue, the Applicant may need to seek a variation/amendment to the Heritage Victoria approval already granted as a result of the changes as a result of conditions recommended in this report.

Since the application was advertised, a planning consultation meeting was held with the Applicant, Objectors and Ward Councillors in attendance. Since that meeting and upon review of the objections and Council's referral advice, the Applicant formally amended the plans pursuant to Section 57A of the *Planning and Environment Act 1987* [the Act].

The amended plans (received by Council on 4 July 2012) were readvertised on 13 July 2012 and detail the following changes to the original application plans:

- (a) total number of apartments increased from 349 to 350;
- (b) total number of car parking spaces reduced from 348 to 336;
- (c) increased setbacks between the central apartment building and the north-west apartment building (14 storeys) to a minimum of 7.2m;
- (d) an additional setback of 6m average is provided to levels 13-16 of the 17 storey apartment building from Waterloo Road;
- (e) a reduced setback of the north-western apartment building to a minimum of 15.7m from the north-south carriageway; and
- (f) a total of 200 onsite storage cages.

These plans form the basis of this assessment [**decision plans**].

The Applicant has also undertaken a Road Safety Audit, in consultation with some residents/businesses in the area. The Audit made a number of recommendations, however not all could be included as permit conditions as these recommendations fall outside the scope of planning consideration. These matters are considered in the traffic section of the assessment.

At the time of writing, 103 objections had been received, along with 1 letter of support. It is noted however, that 9 of the 'objections' received to the amended plans were now conditionally supportive of the development.

The application required formal referral to the Department of Transport [**DOT**], now Public Transport Victoria [**PTV**]. No objection was received.

Key Issues

The key issues for Council in considering the proposal relate to:

- (a) policy and physical context;
- (b) land uses;
- (c) public open space contribution;
- (d) urban design;
- (e) on-site amenity;
- (f) off-site amenity;
- (g) environmental sustainability;
- (h) car parking / traffic;
- (i) bicycle parking and facilities;
- (j) loading facilities;
- (k) waste management;
- (l) cancellation of Section 173 agreement; and
- (m) Objector concerns.

Council Plan, Strategy and Policy Implications

The former Yorkshire Brewery Site is located within a Mixed Use Zone [**MUZ**]. The purpose of the MUZ is to implement State and Local Policy and to provide for a range of residential, commercial, industrial and other uses which complement the mixed-use function of the locality, whilst encouraging residential development that respects the neighbourhood character. In addition,

Council's Local Policy also identifies the site as a Strategic Redevelopment Site [**SRS**]. There are also a number of other overlays and controls which apply to the site.

Conclusion

With the changes outlined in the recommendation section of this report, it is considered that the redevelopment of the site would provide an appropriate built form outcome for the site.

The proposed development incorporates a series of towers with a central area of open space that takes into account the unique interfaces and long range views afforded to the site. On balance, the proposal is an acceptable planning outcome and is considered to meet the relevant elements of the Yarra Planning Scheme.

CONTACT OFFICER: Sarah Thomas
TITLE: Principal Planner
TEL: 92055005

**10.1 1 - 21 Robert Street Collingwood VIC 3066 - Planning Permit Application
PLN11/0750**

Trim Record Number: D12/71239

Responsible Officer: Director City Development

| | |
|-----------------------------|---|
| Proposal: | Demolition of the silos buildings and part demolition of existing buildings on site (no permit required) for the development of the land for a mixed use development comprising a number of buildings (up to 17 levels plus two basement levels); use of the land as a food and drink premises (cafe) and convenience shop (deli); reduction in the car parking requirements; and waiver of the loading and unloading bay provisions. |
| Existing use: | Vacant |
| Applicant: | SMA Projects Australia Pty Ltd |
| Zoning / Overlays: | Mixed Use Zone Part Design and Development Overlay (Schedule 2 – Main Roads and Boulevards) Environmental Audit Overlay Heritage Overlay (Schedule 141 – 88 Wellington Street Collingwood – Former Yorkshire Brewery) |
| Date of Application: | 12 September 2011 |
| Application Number: | PLN11/0750 |

Background

1. This report provides Council with an assessment of Planning Permit Application No. PLN11/0750, which affects land at 1-21 Robert Street, Collingwood, more commonly known as the former Yorkshire Brewery site, and recommends approval subject to conditions.
2. Since the application was advertised, a planning consultation meeting was held with the Applicant, Objectors and Ward Councillors in attendance. Since that meeting and upon review of the objections and Council's referral advice, the Applicant formally amended the plans pursuant to Section 57A of the *Planning and Environment Act 1987* [the Act].
3. The amended plans (received by Council on 4 July 2012) were readvertised on 13 July 2012 and detail the following changes to the original application plans:
 - (a) total number of apartments increased from 349 to 350;
 - (b) total number of car parking spaces reduced from 348 to 336;
 - (c) increased setbacks between the central apartment building and the north-west apartment building (14 storeys) to a minimum of 7.2m;
 - (d) an additional setback of 6m average is provided to levels 13-16 of the 17 storey apartment building from Waterloo Road;
 - (e) a reduced setback of the north-western apartment building to a minimum of 15.7m from the north-south carriageway; and
 - (f) a total of 200 onsite storage cages.
4. These plans form the basis of this assessment [decision plans].
5. The Applicant has also undertaken a Road Safety Audit, in consultation with some residents/businesses in the area. The Audit made a number of recommendations, however not all could be included as permit conditions as these recommendations fall outside the

scope of planning consideration. These matters are considered in the traffic section of the assessment.

Permit History (Planning and Heritage Victoria)

6. The relevant planning history for the site is as follows:

- (a) On 15 July 1999, Heritage Victoria issued Permit P4379 for a residential and commercial redevelopment of the site and a three lot subdivision.
- (b) Planning Permit Application PL01/0833 was withdrawn at the Applicant's request due to Council's traffic and urban design issues with the proposal.
- (c) During 2001, Allom Lovell and Associates prepared a Conservation Management Plan [**CMP**] for the site at the request of Heritage Victoria. The CMP established what works would need to be undertaken prior to the redevelopment of the site and established two potential development models for the site (low rise and high rise schemes).
- (d) On 5 December 2002, Planning Scheme Amendment C39 (Part 1) deleted the Development Plan Overlay Schedule 1 (amongst others) [**DPO1**], relating to an approved development plan affecting the subject site. The removal of the DPO1 followed a Ministerial direction to remove the DPOs in the Yarra Planning Scheme [**the Scheme**] as they related to superseded architectural proposals.
- (e) Planning Permit Application PL02/1091 was subsequently lodged. Although Council raised urban design concerns with the application, Heritage Victoria approved a permit (HOP5606) for a 2 to 9 storey development containing 121 dwellings and 146 car parking spaces. The Applicant prepared a set of 'urban design principals' (Fastnet Consulting) in consultation with Council in an attempt to address the issues. The principals were not adopted or formally endorsed by Council.
- (f) Planning Permit PL03/1302 was issued on 26 April 2004 for the use and development of the land for the purpose of a car park. This permit expired on 26 April 2006 and raises questions regarding the legitimacy of the current car park use. This matter has been referred to Council's Community Amenity Unit and the owner has since lodged a planning permit application (PLN12/0727 - yet to be considered by Council) to rectify this.
- (g) Heritage Victoria issued a permit (H7870) on 25 March 2004 for the adaptive re-use of the heritage buildings and the construction of a residential tower. The endorsed plans detail:
 - (i) retention of the brew tower, former stables and part of the south boundary wall (forming part of the former boiler house and cellar buildings);
 - (ii) a 2 to 4 storey building along Waterloo Place;
 - (iii) a 4 storey form in the north-west corner adjacent to Robert Street;
 - (iv) an 11 storey tower in the north-east corner adjacent to Robert Street; and
 - (v) 3 basement car park levels.
- (h) Council issued a Notice of Decision to Grant a Planning Permit (PL03/0978) on 15 April 2004 with conditions resulting in a 8-9 storey (maximum) tower in the north-east corner of the site and 2-4 storey forms around the remainder of the perimeter. The Applicant appealed this condition (along with a number of others relating to car parking, traffic and design detailing). The matter was mediated at VCAT and resulted in a permit issuing for an 11 storey tower in the north-east corner of the site. Plans were never endorsed in accordance with this permit and following a number of extensions to the expiry date of the permit, the permit expired on 18 June 2011.
- (i) Lovell Chen Architects and Heritage Consultants prepared an updated CMP during 2011. Since the 2001 report was prepared, the physical condition of the building which

make up the former brewery site have deteriorated and the conservation management approach for the site was revisited.

- (j) An application for the redevelopment of the site was lodged with Heritage Victoria on 31 August 2011. The application was referred to Council for comment by way of letter 14 September 2011. At the Council meeting of 8 November 2011, Council resolved that from a heritage perspective it objects to the proposed redevelopment of the former Yorkshire Brewery site based on the following grounds:
 - (i) *The impact of the external stairwell to the brew tower will impact on the heritage significance of the building and should be reviewed and an alternative sought.*
 - (ii) *The 14 and 17 storey buildings are of a mass, height and design that would dominate and detract from the heritage fabric of the brew tower.*
 - (iii) *The impact of the interjection of the 17 storey building in to the stables building will negatively impact on the remnant heritage fabric.*
- (k) On 20 April 2012, Heritage Victoria issued a permit (P16751) for the redevelopment of the site as a residential 'urban village' including partial demolition and adaption of heritage buildings and construction of new buildings. The permit requires:
 - (i) conservation works (as outlined in the 2011 Construction Management Plan) to be completed before the development commences;
 - (ii) artefact identification and management;
 - (iii) an archival photographic record before any works commence; and
 - (iv) an updated CMP within 6 months of the completion of the development.
- (l) Heritage Victoria has subsequently amended the permit and endorsed plans on 3 September 2012 to align with the amended plans being considered under this application. Additionally, conditions have been amended to vary the staging works proposed and to include a requirement for a bank guarantee worth 1 million dollars (copy of permit and endorsed attached).
- (m) This planning permit application before Council was lodged on 12 September 2011.

Existing Conditions

Subject Site

- 7. The former Yorkshire Brewery, known as 1-21 Robert Street (refer to figure 1 below), is located approximately 1.5km north-east of the Melbourne CBD within a former industrial area of Collingwood between Wellington Street to the west and Hoddle Street to the east.
- 8. The site was previously operated by Carlton and United Breweries and manufacturing ceased in 1996.
- 9. The subject site is listed on the Victorian Heritage Register, with the following statement of significance:
 - (a) *The Yorkshire Brewery was established in 1858 by John Wood who commenced operations in a timber building on a two acre site. Wood, a former builder, lived on site and also owned the Yorkshire Hotel. In 1875 a son, the engineer and architect James Wood, prepared drawings for the present complex, the foundation stone being laid in 1876. The most prominent feature of the complex was a multi-level polychrome brick tower with a large mansard roof and a viewing platform. This still survives. A smoke stack has since been demolished. The Yorkshire Brewery became a major Melbourne brewing enterprise which obtained prizes for ale and porter at the Melbourne Centennial Exhibition of 1888. Significantly, it was located in Collingwood, the centre of the brewing industry, the complex experienced numerous changes of ownership but maintained its identity in the early twentieth century period. This period saw*

amalgamations and an overall reduction of breweries in Melbourne. The Yorkshire Brewery was purchased by the dominant Carlton and United group in 1909 and used as a standby plant, then a cooperage. In 1954 it became a malt house. The complex survives as a substantially intact large mid-Victorian brewing complex consisting of a brewing tower, brewery cellars and former stables, with additions. Its significance is summarised as follows:

- (i) *for comprising remnant elements of a large nineteenth century brewing complex, including a brewing tower, adjacent buildings and stables;*
- (ii) *as a complex expressive of the first generation of industrial development in the Collingwood area which was noted for its manufacturing activity in the nineteenth century;*
- (iii) *as the most notable surviving brewery complex in the municipality of Collingwood, a district noted for its breweries in the nineteenth century and for reflecting the development of the brewing industry from the early days of the existence of the colony of Victoria;*
- (iv) *for comprising the largest and most prominent brewing tower in the state the tower may also be considered important as a notable work of the architect James Wood;*
- (v) *as expressive of the nineteenth century brewing process;*
- (vi) *as a symbol of industrial activity and the personal success of the wood family;*
- (vii) *as a colonial echo of similar structures in Great Britain;*
- (viii) *as a vantage point and symbol, and one of the tallest structures in the district in the nineteenth century;*
- (ix) *for its architectural detail, including polychrome brick and a mansard roof, the only substantially intact example remaining in Victoria of a feature typically associated with nineteenth century brewing towers;*
- (x) *as an example of French "Second Empire" architectural style;*
- (xi) *for comprising one of the few surviving nineteenth century stables associated with a large manufacturing complex; and*
- (xii) *as an archaeological site which may reveal further evidence of industrial processes, buildings and activities on the site, including the Yorkshire Hotel and other properties owned by John Wood.*

10. The site is slightly irregular in shape and sits alone as an 'island' with an area of approximately 4,560m² bounded by:
 - (a) north – Robert Street with a frontage of approximately 78m;
 - (b) east – abuttal to commercial tenancies fronting Rokeby Street with a site depth of 56m;
 - (c) south – Waterloo Road with a frontage of approximately 85m;
 - (d) west – carriageway easement on title with a frontage of approximately 56m.

11. The site comprises several existing buildings that are described as follows and indicated at figure 1:
 - A – series of silos
 - B – existing Kiln building
 - C – Brew Tower
 - D – former Boiler House and remanent wall
 - E – former Cellar
 - F – former Stables



Figure 1: Aerial image of the subject site and surrounds

12. These buildings are in various states of disrepair and all are vacant (see figures below). The internal hard stand area through the centre of the site and continuing to the north-east is currently being used as a commercial at grade car park on a lease arrangement.



Figure 2: Building A Silos

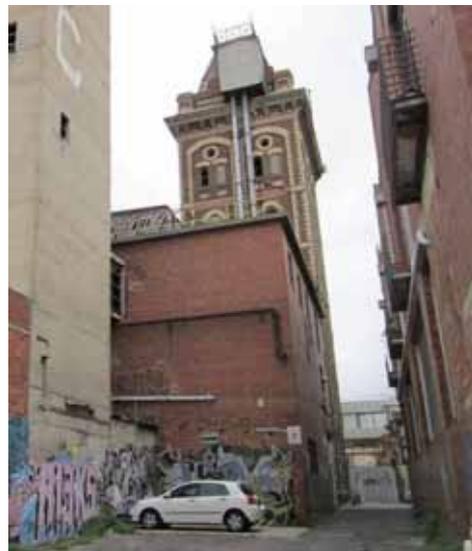


Figure 3: Building B Kiln Building



Figure 4: Building C Brew Tower



Figure 5: Building D Boiler House and remnant wall



Figure 6: Building E former Cellar



Figure 7: Building F former stables

13. Vehicular access to the site is via a cross over on Robert Street. Access to the site is also afforded across the carriageway easement on the western boundary, linking Robert Street and Waterloo Road. A roller door is integrated into the existing wall located on Waterloo Street between the cellar and stables buildings; however this is currently not in use.

Restrictive Covenants

14. The site is legally known as Lot S2 on Plan of Subdivision 411166X (volume 10460 folio 800). The title is affected by a Notice under Section 47(2) of the Heritage Act and a Section 173 Agreement (W235186R).**[the agreement]**
15. In addition to the above, the site includes a carriageway, light and ventilation easement in favour of lots 1 and 2 (the two buildings further west fronting Wellington Street) **[E-1 - carriageway easement]**. (See figure 2.)

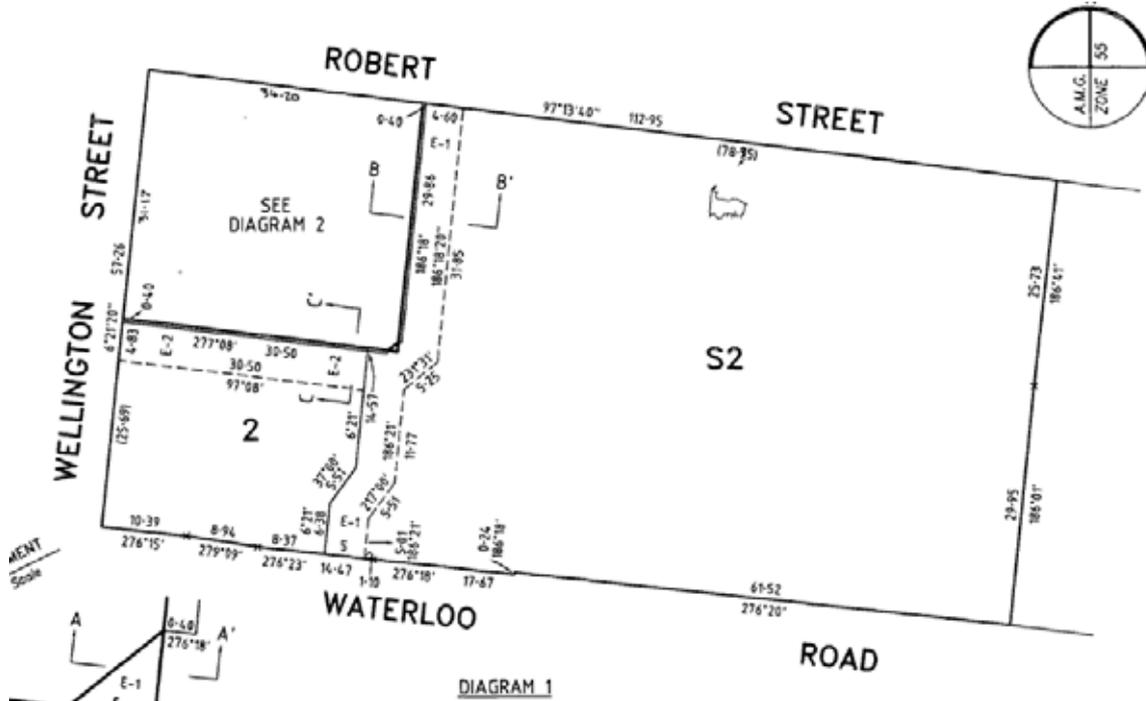


Figure 8: Easements

16. An easement of 'way' also extends from Wellington Street in favour of all lots on title (the subject site and two 'Wellington Street' buildings) [E-2 – **way easement**].

Notice under Section 47(2) of the Heritage Act

17. The Notice identifies the site is on the Victorian Heritage Register (number 807).

Section 173 Agreement (W235186R)

18. The agreement was executed on the 3 August 1999 and parties to the agreement include Council, Heritage Victoria and the then owner of the land 'Treeline Pty Ltd.
19. The agreement arose from conditions relating to an earlier heritage permit issued by Heritage Victoria (July 1999) and a subdivision permit (May 1999) issue by Council.
20. The agreement at section 3 states (specific obligations of the owner):
 - 3.1 *the Owner must perform the Conservation Works and must undertake the redevelopment of Lot S2 in accordance with the Permit to the reasonable satisfaction of Council and Heritage.*
 - 3.2 *must not carry out any development on or of the Land or do or permit to be done anything on or in relation to the Land which in the opinion of Council or Heritage adversely affects the cultural heritage significance of characteristics of the Land.*
 - 3.3 *the Owner must appoint a qualified conservation architect to supervise the Conservation Works. The appointment of the conservation architect must be approved in advice in writing by heritage.*
 - 3.4 *the Owner must commission the conservation architect appointed in accordance with clause 3.3 to provide written progress reports detailing the Conservation Works which have been completed. The progress reports must be submitted to Council and Heritage every six months after the Conservation Works first commence until the Conservation Works are complete to the satisfaction of Council and Heritage.*
 - 3.5 *once the Conservation Works are complete to the reasonable satisfaction of Council and Heritage, the Owner must maintain the Heritage Buildings to a reasonable and*

safe standard or repair and condition to the reasonable satisfaction of Council and Heritage.

- 3.6 *the Owner will not make application to Council or Heritage for permission to further subdivide Lot S2 until the Conservation Works specified in the Conservation Management Plan have been completed to the reasonable satisfaction of Council and Heritage.*
- 3.7 *the Owner agrees to perform and complete the Conservation Works within the time specified by condition 16 of the permit namely, the Conservation Works are to be commenced within eighteen months of 15 July 1999 and completed within three years of 15 July 1999 or such further time as may be agreed by Heritage.*

3.8 Public Open Space Contribution – Lot S2

3.8.1 *If Lot S2 is subdivided again, the owner of Lot S2 must pay Council in an amount not exceeding 5 per cent of the value of Lot S2 calculated in accordance with the procedures set out in the Subdivision Act 1988 Council prior to the issue of a statement of compliance pursuant to the Subdivision Act 1988 for the subdivision of Lot S2.*

21.

3.8.2 *If Lot S2 is not subdivided again, the owner of Lot S2 must pay to Council an amount not exceeding 5 per cent of the value of Lot S2 calculated in accordance with the procedures of the Subdivision Act 1988 within two years of the issue of a statement of compliance for the subdivision approved by the Subdivision Permit; and*

3.8.3 *if the money referred to in clause 3.8.1 and 3.8.2 is not paid to Council by the specified time, the amount payable will accrue interest from the day the statement of compliance is issued at the rate prescribed under section 227A of the Local Government Act 1989 until it is paid.*

22. Also applicable to the site are two Conservation Management Plans [**CMPs**] have been prepared for the site by the Allom Lovell and Associates (now known as Lovell Chen), with the 2011 report being an update to the 2001 report. A summary of each report is provided below:

Conservation Management Plan – Allom Lovell and Associates 2001

23. The 2001 Conservation Management Plan was prepared for the land owner by Allom Lovell and associates at the request of Heritage Victoria in 2001 to guide the on-going management and redevelopment of the site. The CMP describes the history of the site and identifies the important elements of the substantial landmark industrial brewing complex as well as providing direction on future redevelopment of the site. The key points of the statement of significance is that the Former Yorkshire Brewing Complex:
- (a) *is of state architectural and historical significance;*
 - (b) *is a rare surviving example of a c19th brewing complex in Victoria;*
 - (c) *continuously operated as a brewing establishment for over 140 years;*
 - (d) *is an important link to the early industrial development of Collingwood; and*
 - (e) *is an outstanding example of utilitarian industrial buildings of the period, distinguished by the use of polychromatic brick work. The buildings have individual significance.*
24. The 2001 Conservation Management Plan (at page 22) includes three principles for incorporating new development and buildings on the site:

- (a) any development should ensure that the heritage significance of the site is not diminished as a result of inappropriate new work or over development;
- (b) the landmark qualities of the brew tower should be retained, and preferably enhanced, amid the new development; and
- (c) new construction should only be undertaken in a manner which ensures that the cultural significance of the listed buildings is not compromised.

25. The 2001 report recommended two models for the redevelopment of the site which would satisfy the conservation criteria. Specifically:

- (a) a low rise model which proposes that a large proportion of the site may be covered with low rise buildings as originally existed with open space located around the curtilage of heritage buildings; and
- (b) a high rise model which proposes a taller structure that is no higher than the cornice line of the brew tower. Specifically, the following comments were also made for the high rise model:

any building which in actual vertical dimension exceeded the height of the brew tower cornice, should be located as far away from the brew tower as possible so that in any perspective view...its height appears less than it is in reality. To assist in the siting of any high elements as far away as possible from brew tower, any new building should concentrate the height in the north east corner of the site through stepping back of the upper levels from the brew tower. Elsewhere lower rise buildings could be constructed on the site provided that they did not impinge upon the visual setting of the brew tower, at the lower levels and the stables. (See page 65 of the 2001 CMP)

Conservation Management Plan – Lovell Chen Architects and Heritage Consultants 2011

26. Since the 2001 report was prepared, the physical conditions of the building which make up the former brewery site have deteriorated. Notably, access to the first and second floor spaces of the former cellaring and production building and the brew tower beyond the third floor is not possible, due to hazardous conditions. Additionally illegal demolition of the roof and some sections of external walls to the stables building have subsequently occurred.

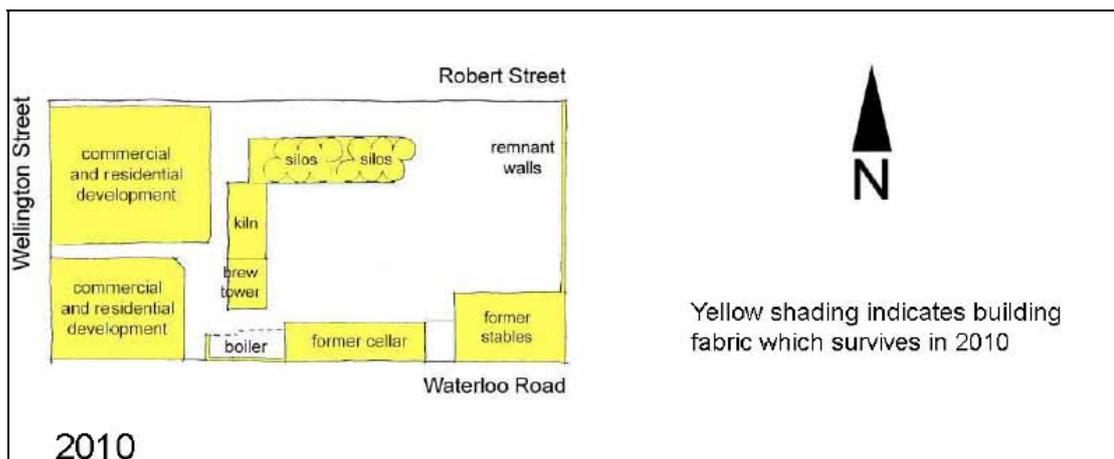


Figure 9: Existing buildings on site

27. A large part of the 2011 CMP is dedicated to providing an analysis of each individual building on site with specific recommendations made for any proposed adaptation /intervention. See following table that outlines the recommendations for each of the buildings of significance:

| Building | Essential elements | Preferred action | Adaptation/Intervention |
|--|--|--|---|
| <i>Brew Tower</i> | <i>All elements of original and early fabric, including the polychrome brickwork, bluestone plinth, roof form and fabric, including the form and fabric of the window openings.</i> | <i>Maintain, restore and preserve as required. Restore the missing flagpole to the roof. If this is not possible, reconstruct this element and reinstall.</i> | <i>There is considered to be little scope for additional openings to the brew tower. As a rule, existing openings should ideally be utilised for any interconnections with new buildings. There is also more latitude for adaptation of existing openings to the elevations which have previously at low level, been enclosed. These would be the east and north elevations in particular. If the adjoining kiln house is demolished, and the original foundation stone is found in situ, it should preferably be restored.</i> |
| | <i>All original and early fabric including the interior fabric of the mansard roof which reportedly includes beaded timber boards, chamfered purlins, timber floors and the original attic door. The original riveted iron girders at the top level support the timber attic floor, the sole original floor level which has survived the conversion of the brew tower to a maltings.</i> | <i>Retain, restore and preserve all surviving original fabric.</i> | <i>There is considered to be considerable scope to adapt the interior of the building, providing alterations do not require new openings to the exterior of the building, particularly to the upper levels. The 'Kembla' brand steel structure which formerly supported the maltings equipment and later floor levels, can be removed or retained as required. Should the insertion of new floor levels within the building be considered, these should not be externally visible through the existing windows.</i> |
| <i>Former Boiler House (south-west corner of the site)</i> | <i>All elements of surviving original and early fabric, including the full extent of the two surviving elevations including the north return, polychrome brickwork, bluestone plinth, and the form of the original window openings.</i> | <i>Restore the west and south elevations to the extent of all of the original tuck-pointed polychromatic brickwork. Reconstruct the window openings in the south elevation to their original form. Restore the form of the original opening to the west elevation. Remove the red brick addition to the south-west pier to the west elevation.</i> | <i>There is considered to be significant scope for the adaptation of the remnant boiler house structure.</i> |
| <i>Cellaring and Production Building (south elevation)</i> | <i>All elements of surviving original and early fabric, including the full extent of the two surviving elevations – the south and the east elevations – to the extent of the original polychrome brickwork, the bluestone plinth, surviving roof form and the form of the original window openings.</i> | <i>Maintain, preserve and restore as required. Restore and reconstruct the original form and fabric of the openings to the south and east elevations.</i> | <i>There is considerable scope for the adaptation of the interiors of the buildings, which have undergone a significant degree of alteration. The two storey central building which does not retain its original roof form, the two-storey west wing which retains only two of its outer walls and the north and west elevations of these portions of the building provide the greatest</i> |

| Building | Essential elements | Preferred action | Adaptation/Intervention <i>degree of flexibility for adaptation or incorporation with new build set above, behind or within their footprint.</i> |
|--------------------------------------|--|---|---|
| Brewery entrance (off Waterloo Road) | All elements of surviving original and early fabric, including the full extent of the south elevation, including the gable-form parapet and the original form of the opening to Waterloo Road. | Maintain, preserve and restore as required. Remove the later brick piers and restore the original form of this opening. Consider reconstructing the timber doors if evidence of their original form can be located. | There is considered to be little or no scope for adaptation other than the conservation actions outlined. |
| Stables | All elements of surviving original and early fabric, including the full extent of the four elevations, including the gable-form parapet walls to the east and west, the double height internal walls that frame the entry from the west and enclose the former harness and feed rooms. All polychrome brickwork and the form and rhythm of the window openings, including 'blind' window openings, surviving bluestone pitchers. | Maintain, preserve and restore as required. Reconstruct the missing gable to the east elevation. Consider reconstructing the original roof form, either in full or in part, in any future use of the building. Restore and/or reconstruct the timber doors to the west elevation. | Today there is little or no surviving original internal fabric, and there is reasonable scope for adaptation of the interior space to other uses. |

Table 1:- Extract from Conservation Management Plan – Lovell Chen Architects and Heritage Consultants 2011

28. Of the remaining buildings on site, the brew tower is identified as the most prominent element. A views analysis of the brew tower is provided in the 2011 CMP, with views to the tower possible from all sides, extending as far as Collingwood and North Richmond rail stations to the east; Powelett and Grey Streets, East Melbourne to the south; Otter Street to the north; and corner of Smith and Langridge Streets to the west. Given the land locked nature of the site, from ground level, only the upper portions of the brew tower are visible except from within the confines of the site.
29. The prominent view lines identified are along Langridge and Wellington Street (to both the north and south), and those from the west of Cambridge and Oxford Streets. Specifically, the following view lines are also identified:
 - (a) corner of Rokeby Street and Langridge Street: the top two levels of the brew tower is clearly visible.
 - (b) north side of Langridge Street as one travels towards the west: top level of the brew tower is clearly visible.
 - (c) south-west corner of Langridge Street and Smith Street: the roof form is clearly visible.

Surrounding Land

30. Land surrounding the subject site is zoned Business 3, with the subject site and the section west towards Wellington Street and beyond being zoned Mixed Use (see figure 10).



Figure 10: Zoning map (Mixed use = ■ and Business 3 = ■)

31. Amendment C102 to the Yarra Planning Scheme (Gipps Precinct, Collingwood) was approved by the Minister for Planning [**the Minister**] in June 2011 which rezoned the Gipps Precinct from Industrial 1 Zone to Business 3 Zone and introduced Design and Development Overlay 11 – Gipps Precinct.
32. Relevant overlays in the area also include the Heritage Overlay, Design and Development Overlay (schedules 2 [Main Roads and Boulevards] and 11 [Gipps Precinct]) and the Environmental Audit Overlay (see figures 11, 12, 13 and 14).



Figure 11: Heritage Overlay



Figure 12: Design and Development Overlay 2 (Main Roads and Boulevards) – Does not affect the proposed development



Figure 13: Design and Development Overlay 11 (Gipps Precinct) – Does not affect the subject site



Figure 14: Environmental Audit Overlay

33. The following is a detailed description of the built form and uses surrounding the subject site:

North

34. The subject site is boarded by Robert Street to the north, a one-way, east bound road. The street has an approximate 6m width which has brought rise to 'no stopping' and 'no parking' restrictions on either side.
35. The northern side of Robert Street features a range of uses within 1 to 2 storey buildings. The built form is hard edge with brick and corrugated iron roller doors being commonplace. Land uses include a martial arts studio, offices and warehouses/industry (a fashion/fabric distributor). Although upper level decks and a number of windows face the subject site, the Business 3 Zoning prohibits the use of these properties as dwellings (unless existing use rights can be established) which officers are not aware of this in this instance.
36. Being primarily commercial, a number of these properties require vehicular deliveries from Robert Street, as is evident by the number of roller doors providing on-site parking and loading opportunities.

East

37. The subject site has a common boundary with single/two storey commercial premises with a number of tenancies fronting Robert and Rokeby Streets and Waterloo Road. The immediately adjacent property is a bakery, extending between Robert Street and Waterloo Road. This property present boundary walls to the subject site and has recessed setbacks (approximately 6m) providing on-site car parking and loading opportunities from the north and south sides. Projecting flues are visible towards the shared boundary with the subject site.
38. Further east along Robert Street is a seafood processor.
39. Two recent planning applications are relevant to the consideration of this application. Planning application PLN12/0213 was recent approved which legitimised the use of 25 Rokeby Street as a seafood processor and enable buildings and works to 29 Rokeby Street. Planning application PLN12/0564 is currently on advertising and seeks approval for 'use of the land for food manufacturing, a packaged liquor licence and a reduction of the car parking requirements of clause 52.06 of the Yarra Planning Scheme.

South

40. The subject site is boarded by Waterloo Road to the south, a one-way, east bound road. The street has an approximate 3.8m to 4.6m carriageway width (varies) which has brought rise to 'no stopping' restrictions on either side.
41. The southern side of Waterloo Road opposite the subject site consists of 2 to 4 storey commercial buildings, which appear to be used as offices. The buildings are built hard edge to the side boundaries and upper level frontages, with most providing ground level front setbacks to facilitate vehicular access. Building materials include face brick, render, high gloss tiles and natural concrete, with panel lift vehicular entrance doors.
42. Again, although a number of balconies and windows face the subject site, the Business 3 Zoning prohibits the use of these properties as dwellings unless existing use rights can be established.

West

43. To the west of the subject site, across the easement, is the rear of two buildings that front Wellington Street. These buildings are lawfully used as dwellings, with a copy centre and fashion wholesalers. The buildings present a number of first and second floor habitable room windows and balconies towards the subject site.
44. The buildings are separated by the way easement which is on the title for the southernmost building.
45. The buildings are both 2 storeys to Wellington Street, although the northernmost is slightly taller due to higher floor-to-ceiling heights and an accentuated parapet. The northernmost building is constructed of face brick and the parapet references a sawtooth industrial roof profile.
46. The southernmost building includes a similar parapet at the northern end, but further south transitions to a simplified, more contemporary office typology. The rear of this building is developed in a different manner, perhaps over two development stages, and contains a three storey, painted concrete form.
47. The southernmost building enjoys vehicular access off the carriageway easement on the subject site and the northernmost building enjoys vehicular access off Robert Street.

The Proposal

48. The application is for a mixed use development, up to 17 storeys (plus two basement levels) to be used as dwellings (no permit required), a café and deli, with a reduction in the car parking and waiver of the loading bay requirements. More specifically:

Demolition

- (a) demolition of the silos buildings and part demolition of existing buildings on site (no permit required);
- (b) the works retain the brew tower, stables and cellar buildings and the 'heritage wall' along the southern boundary with a 5m long return 'nib' wall at the western end;

Massing

- (a) the development comprises four general 'segments' above two basement levels (see figure 15):
- 49.

- (i) **Heritage Precinct** including the **brew tower** and buildings along Waterloo Road which will include new buildings up to 5 storeys;
- (ii) **Central building** rising to a height of 9 – 10 storeys;
- (iii) **North Precinct** podium of 3 – 4 storeys with buildings rising to 14 storeys along Robert Street; and
- (iv) **East Precinct** tower rising to 17 storeys.

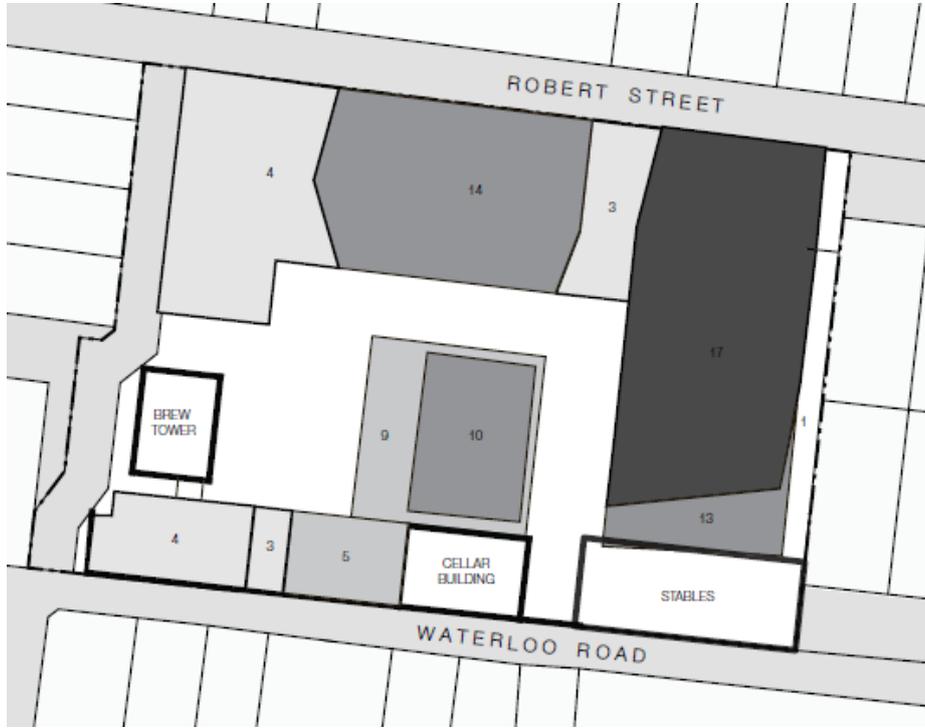


Figure 15: Massing diagram

- (b) the massing of the development includes a series of pedestrian and vehicular links being provided through the site, including a 17m by 28m (maximum) neighbourhood landscaped communal square located east of the brew tower;
- (c) a total area of approximately 922m² of open space is provided at ground level, including the 493m² neighbourhood square;

Access

- (d) pedestrian access to the site is via Robert Street, Wellington Street (through the 'way' easement) and Waterloo Road;
- (e) vehicular access is provided via:
 - (i) Robert Street (eastern end) leading to the basement levels (containing 260 car parking spaces);
 - (ii) Waterloo Road (central) leading to an at grade 31 space car park and a section of both basement levels (containing 45 car parking spaces);

Land use

- (f) use of the land as:
 - (i) dwellings (350 - no planning permit required):
 - studio and 1 bedroom = 146;
 - 2 bedroom = 201; and
 - 3 bedroom = 3.
 - (ii) the dwelling use includes the following ancillary components:
 - (iii) food and drink premises (cafe):
 - hours of operation – 7.00 am to 11.00 pm, seven days;

- capacity – 40 patrons inside and 40 outside (within the neighbourhood square); and
 - 5 staff members at any one time.
- (iv) convenience shop (deli):
- hours of operation – 7.00 am to 11.00 pm, seven days; and
 - 1 staff member at any one time.

Height and setbacks

(g) **Heritage Precinct:**

(i) Brew Tower

- Height of the Brew Tower to remain the same (maximum height 35.9m);
- Attached to the south elevation is a series of balconies stacked on top of each other (winter gardens) connected to the lift core contained within the boiler building;
- Located within the brew tower is the proposed café at ground level with 5 residential levels above.

(ii) Boiler House, Cellar and Stables Building;

- the Waterloo Road section is separated into three general segments (separation created by pedestrian and vehicular entrances). The segments are generally constructed hard edge to Waterloo Road and the east title boundary (utilising the existing brick wall along the boundary) and utilises the existing 5m deep 'nib' around the western edge:
 - (a) western end – 4 storey residential building, with a 7 level link at the northern end providing access to the brew tower (12.2m in height along the south boundary, extending to 25.6m for the link behind);
 - (b) central – 3 to 5 storeys in front of a 10 storey tower behind (see central precinct) 10.5m to 15.2m high; and
 - (c) eastern end – 2 to 3 story podium in front of the 17 storey tower (as outlined in the East Street section below).

(h) **Central Building:**

- (i) The central building is accessed from internally within the site with vehicular access from Waterloo Road;
- (ii) The tower rises to a maximum height of 32.6m which is lower than the overall height of the brew tower but higher than the cornice line; and
- (iii) The tower is separated 16.3m from the brew tower, 7.2m from the Robert Street building and 9m from the east precinct tower.

(i) **North Precinct:**

- (i) This building segment has a 3 to 4 storey podium, with a central 14 storey tower and a 17 storey tower at the eastern end which forms part of the East Precinct;
- (ii) The podium generally has a zero to 815mm setback from Robert Street at ground level and is to be constructed hard edge to the carriageway easement (western edge) and the east title boundary. A section of built form projects outside the northern boundary and the second and third floor balconies project 700mm into the carriageway easement. The podium rises from 9.4m (central segment) to 12.4m in height adjacent to the carriageway easement;
- (iii) The 14 storey tower is 31.7m in height (including plant and equipment screen) and is generally constructed to Robert Street with a minimum 15.4m setback from the edge of the carriageway easement and 6.6m from the 17 storey tower;

(j) **East Precinct:**

- (i) The 17 storey tower is 53.6m in height (including plant and equipment screen) and is setback 3m to 4m from the east title boundary, a minimum of 9.7m from

the south title boundary and is to be constructed up to 1m from the north title boundary; and

- (ii) At level 13, the setback from the southern boundary increases to between 14.8 – 18.8m.

Car parking/bicycle parking

- (k) 336 car parking spaces, provided at grade and within two basement levels:
 - (i) ground level (at grade) = 31;
 - (ii) basement 1 = 152; and
 - (iii) basement 2 = 153.
- (l) car parking allocation:
 - (i) dwelling – 323;
 - (ii) deli/café – 2;
 - (iii) multi-use – 9; and
 - (iv) car share – 2.
- (m) although some of the spaces appear to be ‘tandem’ on the plans, some car parking spaces incorporate independent stacker systems;
- (n) 13 motorcycle spaces within basements 1 and 2;
- (o) bicycle parking as follows:
 - (i) 88 resident bicycle parking spaces at ground floor;
 - (ii) 40 visitor bicycle parking spaces within the neighbourhood square; and
 - (iii) 7 bicycle share spaces adjacent to the brew tower.

Other

- (p) 200 storage cages in a combination of cage and above bonnet configurations (4m³ average);
- (q) proposed site coverage of approximately 73 per cent (excluding basement footprint);
- (r) the site would remain impervious;
- (s) most dwellings will be provided with 4.2m² to 91.6m² of private open space. Ten dwellings (within the former stables and cellar buildings) will not be allocated any private open space;
- (t) bin stores are located at the ground floor (within each commercial tenancy) and in basement 2. A bin holding room is located adjacent to the Robert Street vehicular entrance ramp. Waste collection will occur as follows:
 - (i) residential waste collection – on-site waste collection by a private contractor (small vehicle) within the:
 - Robert Street driveway, collecting rubbish from the bin holding room; and
 - Waterloo Road driveway, collecting rubbish from the bin holding room.
 - (ii) deli and café waste collection – on-site waste collection in the carriageway easement.

Colours and materials

- (u) the architectural style is a distinct ‘new’ and ‘old’, with the face brick (especially the polychromatic brickwork of the brew tower) being complemented by the simple, grey concrete and glazed forms of the new elements. Metal sliding louver screens and timber detailing accentuate the facades and complement the industrial character of the area;
- (v) towers – extensive use of white and clear glazing, with precast concrete panels accentuating the rectilinear style. The floor plates are slightly irregular, with reveals and dark concrete panels creating horizontal bands;

- (w) podium to Robert Street – mixture of glazing and concrete panels, continuing the rectangular forms, with timber cladding to the lower two levels to accentuate the ‘podium’;
- (x) Waterloo Road and towers beyond– retention of face brick along the boundary, with light grey concrete and glazing to the upper levels and towers;

Acoustic works

- (y) the Applicant’s acoustic report recommends:
 - (i) double glazing to the west façade and acoustic-rated windows to the east façade, or treatment to the mechanical plant on the rooftop of the adjacent facility to achieve nominate noise levels inside the proposed dwellings;
 - (ii) mechanical services plant associated with the deli and café must comply with SEPP N-1;
 - (iii) further consideration of the design and layout of the café and impact on residents;
 - (iv) further consideration of the design of the gym to ensure the noise and vibration does not impact the residents below;

Environmentally sustainable initiatives

- (z) 4 star Green Star residential rating – considered a ‘best practice’ rating under the Green Star scheme;
- (aa) average residential energy rating of 6.6 stars;
- (bb) STEPS score:

| Element | Required Score | Project Score |
|------------------------------|----------------|---------------|
| Greenhouse | 25% | 45% |
| Peak Energy Use | 10% | 84.6% |
| Mains Water Use | 25% | 37% |
| Stormwater Quality | 100% | 100% |
| Building Materials | 12% | 15% |
| Project Sustainability Score | | 281.6/500 |

Extract from Applicant’s ESD report.

- (cc) two water tanks (45,000L and 30,000L) under basement 1, servicing toilet flushing in a total of 110 bedrooms and irrigating landscaping;
- (dd) gas boosted solar hot water system;
- (ee) most rooms enjoy direct daylight, with only 1 borrowed light bedroom;
- (ff) 47 solar panels on the roof of the 14 storey Robert Street building;
- (gg) a green travel plan [**GTP**];

Section 173 Agreement

- (hh) removal of the Section 173 Agreement applying to the site (assessment at paragraphs 380 - 384).

Staging

- (ii) stage 1 – heritage quarter (heritage restoration and re-adaption, north and south basements and at-grade car parking and neighbourhood square); and
- (jj) stage 2 – urban quarter (north, east and north-west towers).

Planning Scheme Provisions

Zoning

Mixed Use Zone [MUZ]

50. Under clause 32.04-1 of the Scheme:
- (a) a permit is not required to use the site as a dwelling;
 - (b) a planning permit is required to use the site as a café (food and drinks premises, nested within 'retail premises'); and
 - (c) a planning permit is required to use the site as a deli (convenience shop, nested within 'shop').
51. Under clause 32.04-5 of the Scheme, a permit is required to construct two or more dwellings on a lot. Although not strictly applicable to developments of four or more storeys, clause 55 (ResCode) is a relevant decision guideline.
52. Under clause 32.04-7 of the Scheme, a permit is required to construct a building or construct or carry out works for a use in section 2 of clause 32.04-1 (deli and café).
53. Planning permission is therefore required under the MUZ for the café and deli uses and to construct a building and to construct or carry out works associated with the café, deli and dwellings.
54. The purpose of the MUZ is:
- (a) *To implement the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies;*
 - (b) *To provide for a range of residential, commercial, industrial and other uses which complement the mixed-use function of the locality; and*
 - (c) *To encourage residential development that respects the neighbourhood character.*

Overlay(s)

Part Design and Development Overlay (Schedule 2 – Main Roads and Boulevards) [DDO2]

55. The DDO2 only affects a 2.5m deep segment of the south-west corner of the site. As the works do not extend into this portion, a permit is not required pursuant to this overlay.

Environmental Audit Overlay [EAO]

56. Clause 45.03-1 of the Scheme states that before a sensitive use (residential use, child care centre, pre-school centre or primary school) commences or before the construction or carrying out of buildings and works in association with a sensitive use commences, either:
- (a) *a certificate of environmental audit must be issued for the land in accordance with Part IXD of the Environment Protection Act 1970; or*
 - (b) *an environmental auditor appointed under the Environment Protection Act 1970 must make a statement in accordance with Part IXD of that Act that the environmental conditions of the land are suitable for the sensitive use.*
57. As the proposal includes a sensitive use (residential), this requirement should be conveyed to the Applicant by way of a notation (as opposed to a condition as it is not a permit requirement).

Heritage Overlay

Heritage Overlay (Schedule 141 – 88 Wellington Street Collingwood – Former Yorkshire Brewery) [HO141]

58. No permit is required under this overlay. More specifically, clause 43.01-2 states that no permit is required to develop a heritage place which is included on the Victorian Heritage Register. A planning permit is therefore not required under this overlay and no consideration of heritage matters will form part of this assessment.

Particular Provisions

Clause 52.06 – Car Parking

59. The purpose of this provision (amongst others) is to ensure the provision of an appropriate number of car spaces are provided for a development having regard to the activities on the land and the nature of the locality. Under clause 52.06-2, prior to a new use commencing, a new building being occupied, the floor or site area of an existing use being increased or the number of patrons, seats or practitioners at an existing use being increased, the required car parking should be provided as per clause 52.06-5. However, clause 52.06-3 states that a permit may be granted to reduce (including reduce to zero) the requirement to provide the number of car parking spaces required under this clause.
60. Under clause 52.06-5, the following car parking rates are required:

| Use | Car parking requirement at clause 52.06-5 | Car parking proposed | Reduction sought |
|---|---|----------------------|------------------|
| Dwelling | Studio, 1 and 2 bedroom dwellings = 347 = 347 spaces 3 bedroom dwellings = 3 = 6 spaces TOTAL = 353 | 323 | 30 |
| Dwelling visitor | 70 | 0 | 70 |
| Café (food and drinks premises [123m ² including outdoor dining area]) | 5 | 1 | 4 |
| Deli (convenience shop - shop rate [45.5m ²]) | 2 | 1 | 1 |
| | TOTAL | 325 | 104 |

61. The car parking numbers above do not take into account the 2 car share and 9 multi-use spaces proposed, bringing the on-site car parking provision to 336.
62. A permit can be granted to reduce the car parking requirements. A detailed assessment is offered later in this report.

Clause 52.07 – Loading and unloading of vehicles

63. The purpose of this Clause is to set aside land for loading and unloading commercial vehicles to prevent loss of amenity and adverse effect on traffic flow and to manage road safety. The provisions set out the minimum dimension requirements for the design of loading and unloading facilities. A permit may be granted to reduce or waive these requirements if either the land area is insufficient; or adequate provision is made for loading and unloading vehicles to the satisfaction of the Responsible Authority.
64. In this case, the development does not provide an on-site loading bay. A detailed assessment is offered later in this report.

Clause 52.34 – Bicycle facilities

65. The purpose of this clause is to encourage cycling as a mode of transport and to provide secure, accessible and convenient bicycle parking spaces and associated shower and

change facilities. Clause 52.34-2 states that a permit may be granted to vary, reduce or waive any requirement of clause 52.34-3 and clause 52.34-4.

66. Under clause 52.34-3, the following parking rates are required:

| Use | Bicycle parking requirement | Proposed | Reduction sought |
|---|-----------------------------|----------|--|
| Dwelling | Resident = 70 | 135 | 0 (Exceeding minimum requirements by 30 spaces) |
| | Visitor = 35 | | |
| Café (food and drinks premises - retail rate) | 0 | | |
| Deli (convenience shop - shop rate) | 0 | | |
| TOTAL | | 135 | -30 |

67. The on-site provision exceeds the minimum statutory requirement and, therefore, a permit is not required for a dispensation from the statutory bicycle parking provision requirements of clause 52.34.
68. In addition to the above, clause 52.34 requires one change room. As this has not been provided, a permit is required to reduce this requirement.

Clause 52.35 – Urban Context Report and Design Response for Residential Development of Four or More Storeys

69. The purpose of this clause is to ensure that an urban context report is prepared before a residential development of four or more storeys is designed and that the design responds to the existing urban context and preferred future development of the area.
70. The application included an urban context report and design response in accordance with this clause.

Clause 52.36 – Integrate Public Transport Planning

71. These provisions seek to ensure that land use and development is appropriately integrated into the existing and future transport system. Under clause 52.36 an application to subdivide land, to construct a building or to construct or carry out works for a residential development comprising 60 or more dwellings or lots, must be referred to the Director of Public Transport (now PTV) in accordance with Section 55 of the Act.

General Provisions

Clause 65 – Decision Guidelines

72. The decision guidelines outlined at clause 65 of the Scheme are relevant to all applications. Before deciding on an application, the Responsible Authority must consider a number of matters, including the relevant State Planning Policy Frameworks, Local Planning Policy Frameworks and any Local Policy, as well as the purpose of the Zone, Overlay or any other Provision. An assessment of the application against the relevant sections of the Scheme is offered in further in this report.

Clause 66 – Referrals

73. These provisions set out the requirement to formally refer planning permit applications to specified bodies, as part of the planning scheme amendment process. In this case, this permit application must be referred to the Director of Public Transport [DOT] (now PTV) pursuant to the clause 52.36 (Integrated Public Transport) provisions.

State Planning Policy Framework [SPPF]

Clause 11.01-1 – Activity centre network

74. The objective and relevant strategy of this clause is *'To build up activity centres as a focus for high-quality development, activity and living for the whole community by developing a network of activity centres'*.
75. The site is located over 312m to the east of the Smith Street Major Activity Centre [**MAC**].

Clause 11.01-2 – Activity centre planning

76. The objective of this clause is *'To encourage the concentration of major retail, residential, commercial, administrative, entertainment and cultural developments into activity centres which provide a variety of land uses and are highly accessible to the community'*.

Clause 11.04-2 – Activity Centre hierarchy

77. The objective of this clause is *'To create a network of activity centres comprising the Central Activities Districts, Principal Activity Centres, Major Activity Centres, Specialised Activity Centres and Neighbourhood Activity Centres'*.
78. It is acknowledged that the site does not form part of the Smith Street MAC but the site is located proximate to it.

Clause 13.03-1 – Use of contaminated and potentially contaminated land

79. The objective of this clause is *'To ensure that potentially contaminated land is suitable for its intended future use and development, and that contaminated land is used safely'*.

Clause 13.04-1 – Noise abatement

80. The objective of this clause is *'To assist the control of noise effects on sensitive land uses'*.

Clause 15.01-1 – Urban design

81. The objective of this clause is *'To create urban environments that are safe, functional and provide good quality environments with a sense of place and cultural identity'*.

Clause 15.01-2 – Urban Design Principles

82. The objective of this clause is *'To achieve architectural and urban design outcomes that contribute positively to local urban character and enhance the public realm while minimising detrimental impact on neighbouring properties'*.
83. This clause outlines principles relating to context, the public realm, safety, landmarks, views and vistas, pedestrian spaces, heritage, consolidation of sites and empty sites, light and shade, energy and resource efficiency, architectural quality and landscape architecture. These principles will be addressed in the following urban design assessment.
84. This clause also states that planning must consider (as relevant) the *Design Guidelines for Higher Density Residential Development (Department of Sustainability and Environment, 2004)* in assessing the design and built form of residential development of four or more storeys.

Clause 15.01-4 – Design for safety

85. The objective of this clause is '*To improve community safety and encourage neighbourhood design that makes people feel safe*'.

Clause 15.01-5 – Cultural identity and neighbourhood character

86. The objective of this clause is '*To recognise and protect cultural identity, neighbourhood character and sense of place*'.

87. Relevant strategies to achieve this objective include:

- (a) *Ensure development responds and contributes to existing sense of place and cultural identity;*
- (b) *Ensure development responds to its context and reinforces special characteristics of local environment and place by emphasising:*
 - (i) *The heritage values and built form that reflect community identity; and*
 - (ii) *The values, needs and aspirations of the community.*

Clause 15.02-1 – Energy and resource efficiency

88. The objective of this clause is '*To encourage land use and development that is consistent with the efficient use of energy and the minimisation of greenhouse gas emissions*'.

Clause 16.01-1 – Integrated housing

89. The objective of this clause is '*To promote a housing market that meets community needs*'.

Clause 16.01-2 – Location of residential development

90. The objective of this clause is '*To locate new housing in or close to activity centres and employment corridors and at other strategic redevelopment sites that offer good access to services and transport*'.

Clause 16.01-3 – Strategic redevelopment sites

91. The objective of this clause is '*To identify strategic redevelopment sites for large residential development in Metropolitan Melbourne*'.

92. The site is identified as a Strategic Redevelopment Site **[SRS]** in Council's MSS. The criteria for determining an SRS as outlined in the state section of the Scheme is outlined below:

- (a) *In or within easy walking distance of Principal or Major Activity Centres;*
- (b) *Along tram, train, light rail and bus routes that are part of the Principal Public Transport Network and close to employment corridors, Central Activities Districts, Principal or Major Activity Centres and around train stations; and*
- (c) *Able to provide 10 or more dwelling units, close to activity centres and well served by public transport.*

Clause 16.01-4 – Housing diversity

93. The objective of this clause is '*To provide for a range of housing types to meet increasingly diverse needs*'.

Clause 16.01-5 – Housing affordability

94. The objective of this clause is '*To deliver more affordable housing closer to jobs, transport and services*'.

Clause 17.01-1 – Business

95. The objective of this clause is '*To encourage development which meet the communities' needs for retail, entertainment, office and other commercial services and provides net community benefit in relation to accessibility, efficient infrastructure use and the aggregation and sustainability of commercial facilities*'.

Clause 18 – Transport

96. This clause provides a range of objectives to reduce private motor vehicle usage and encourage the use of public transport, cycling and walking. This in turn, will reduce pressure on road networks. In particular, clause 18.02-1 encourages sustainable personal transport, including walking, cycling and public transport.

Local Planning Policy Framework [LPPF]

Clause 21 – Municipal Strategic Statement (MSS)

97. The MSS provides a broad demographic summary of the municipality and is structured into 4 main themes; land use, built form, transport and sustainability. The MSS acknowledges that whilst Yarra has a growing population, the size of households is decreasing and there are relatively fewer children and elderly people when compared with the rest of Melbourne.
98. In relation to housing, Yarra has a higher percentage of flats and units than the rest of Melbourne and the MSS acknowledges that demand for inner-city living is high. An increased proportion of new housing development is to be encouraged on strategic redevelopment sites and in areas that are well located, close to public transport and activity centres.
99. The MSS acknowledges that Yarra's activity centres and SRSs are powerful economic engines that play a significant regional role and draw on tourists and destination shoppers from overseas, interstate and country Victoria as well as Greater Melbourne.
100. In relation to transport, the MSS outlines that whilst Yarra is very well serviced by different forms of public transport, the impacts of through traffic has negative impacts on residential amenity. Council is active in lobbying for change that will reduce the impact of traffic and improve public transport.
101. The MSS also acknowledges that Yarra is generally a low rise urban area with areas of higher density.
102. In terms of the natural environment and resources, the MSS acknowledges that Yarra is highly urbanised and the natural environment has been substantially modified. The importance of waterways, parklands and public open spaces are highlighted.
103. For each of the 4 main themes, objectives and strategies are offered to assist with decision making. Not all are of relevance to the redevelopment of the former Yorkshire Brewery.

Clause 21.04-1 – Accommodation and Housing

104. The relevant objectives and strategies of this clause are:
- (a) *Objective 1- To accommodate forecast increases in population.*
 - (i) *Strategy 1.1 Ensure that new residential development has proper regard for the strategies applicable to the neighbourhood in question identified in clause 21.08;*
 - (ii) *Strategy 1.2 Direct higher density residential development to Strategic Redevelopment Sites as identified at clause 21.08 and other sites identified through any structure plans or urban design frameworks.*
 - (ii) *Strategy 1.3 Support residual population increases in established*

neighbourhoods;

- (b) *Objective 2 - To retain a diverse population and household structure;*
 - (i) *Strategy 2.1 Support the provision of affordable housing for people of all abilities, particularly in larger residential developments and on Strategic Redevelopment Sites.*
- (c) *Objective 3 - To reduce potential amenity conflicts between residential and other uses.*
 - (i) *Strategy 3.1 Ensure new residential development in the Mixed Use Zone, Business 1, Business 2 and Business 5 Zone and near Industrial and Business Zones is designed to minimise the potential negative amenity impacts of existing non-residential uses in the vicinity.*
 - (ii) *Strategy 3.2 Apply the Interfaces Uses policy at clause 22.05.*

Clause 21.04-2 – Activity Centres

105. The relevant objective of this clause is *'To maintain the long term viability of activity centres'*.

Clause 21.04-3 – Industry, office and commercial

106. The objective of this clause is *'To increase the number and diversity of local employment opportunities'*.

Clause 21.04-5 – Parks, gardens and public open space

107. The relevant objective and strategy of this clause are:

- (a) *Objective 13 - To provide an open space network that meets existing and future community needs:*
 - (i) *Strategy 13.1 Apply the Public Open Space Contribution Policy at clause 22.12.*

Clause 21.05-2 – Urban design

108. The relevant objectives of this clause are:

- (a) *Objective 16 - To reinforce the existing urban framework of Yarra;*
- (b) *Objective 17 - To retain Yarra's identity as a low-rise urban form with pockets of higher development:*
 - (i) *Strategy 17.2 Development on strategic redevelopment sites or within activity centres should generally be no more than 5-6 storeys unless it can be demonstrated that the proposal can achieve specific benefits such as:*
 - *Significant upper level setbacks*
 - *Architectural design excellence*
 - *Best practice environmental sustainability objectives in design and construction*
 - *High quality restoration and adaptive re-use of heritage buildings*
 - *Positive contribution to the enhancement of the public domain*
 - *Provision of affordable housing.*
- (c) *Objective 18 - To retain, enhance and extend Yarra's fine grain street pattern;*
- (d) *Objective 20 - To ensure that new development contributes positively to Yarra's urban fabric.*
- (e) *Objective 22 – To encourage the provision of universal access in new development.*

Clause 21.05-4 Public environment

109. The relevant objective and strategies of this clause are:

- (a) *Objective 28 - To provide a public environment that encourages community interaction and activity:*
 - (i) *Strategy 28.1 Encourage universal access to all new public spaces and buildings.*
 - (ii) *Strategy 28.2 Ensure that buildings have a human scale at street level.*
 - (iii) *Strategy 28.3 Require buildings and public spaces to provide a safe and attractive public environment.*

- (iv) *Strategy 28.4 Require new development to consider the opportunity to create public spaces as part of new development.*
- (v) *Strategy 28.5 Require new development to make a clear distinction between public and private spaces.*
- (vi) *Strategy 28.6 Require new development to consider the creation of public access through large development sites, particularly those development sites adjacent to waterways, parkland or activity centres.*
- (vii) *Strategy 28.9 Apply the Public Open Space Contribution policy at clause 22.12.*

Clause 21.06-1 – Walking and cycling

110. This clause builds upon the objectives outlined at clause 18, promoting cycling, walking and public transport as alternatives to private motor vehicle usage.
- (a) *Objective 30 - To provide safe and convenient bicycle environments:*
 - (i) *Strategy 30.2 Minimise vehicle crossovers on street frontages.*
 - (ii) *Strategy 30.3 Use rear lane access to reduce vehicle crossovers.*
 - (b) *Objective 32 - To reduce the reliance on the private motor car:*
 - (i) *Strategy 32.1 Provide efficient shared parking facilities in activity centres.*
 - (ii) *Strategy 32.2 Require all new large developments to prepare and implement integrated transport plans to reduce the use of private cars and to encourage walking, cycling and public transport.*
 - (c) *Objective 33 - To reduce the impact of traffic:*
 - (i) *Strategy 33.1 Ensure access arrangements maintain the safety and efficiency of the arterial and local road network.*

Clause 21.07-1 – Ecologically sustainable development

111. The relevant objective of this clause is:

- (a) *Objective 34 To promote ecologically sustainable development:*
 - (i) *Strategy 34.1 Encourage new development to incorporate environmentally sustainable design measures in the areas of energy and water efficiency, greenhouse gas emissions, passive solar design, natural ventilation, stormwater reduction and management, solar access, orientation and layout of development, building materials and waste minimisation.*

Clause 21.08-5 - Collingwood

112. This clause describes the area as follows:

Much of Collingwood is industrial in character with the residential precincts surrounded by or interspersed with industrial buildings.

113. This clause identifies the site as being within a 'non residential area', where the objective is to improve the interface of development with the street.

114. The subject site is identified in this section of the MSS as a Strategic Redevelopment Site.

Local Policies

Clause 22.03 – Landmarks Policy

115. The objective of this clause is '*To maintain the prominence of Yarra's valued landmarks and landmark signs*'.

116. The clause does not identify the Yorkshire Brewery or the brew tower, however it is policy at clause 22.03-3 to:

- (a) *ensure the profile and silhouette of new tall structures adds to the interest of Yarra's urban form and skyline.*

Clause 22.05 – Interface Uses Policy

117. This policy applies to applications for use or development within the Mixed Use Zone (amongst others).
118. The relevant objective of this clause is '*To enable the development of new residential uses within and close to activity centres, near industrial areas and in mixed use areas while not impeding the growth and operation of these areas as service, economic and employment nodes*'.
119. This policy outlines recommendations for dwelling design to incorporate measures to protect future residents from noise, fumes, vibration, light spillage and other likely disturbances. Further, it encourages the location of noise-sensitive rooms and openings away from the interface; the provision of acoustic assessment reports where necessary; and appropriate siting, setbacks, articulation and screening to prevent overlooking.
120. Whilst inner city living creates vibrant and active communities, the mix of land uses can sometimes create conflict. Highlighted issues include noise, visual impact and appearance, overlooking, odour and air emissions, light spill, loading and unloading, rubbish removal and storage and construction noise, these issues will be discussed in the assessment section of this report.

Clause 22.12 – Public Open Space Contribution

121. The objectives of this clause are:
- (a) *To implement the Yarra Open Space Strategy;*
 - (b) *To identify when and where land contributions for public open space are preferred over cash contributions; and*
 - (c) *To ensure that where appropriate, land suitable for public open space is set aside as part of the design of a development so that it can be transferred to or vested in Council, in satisfaction of the public open space contribution requirement.*
122. The subject site is in an area where land in lieu of cash is the preferred method of public open space contribution (area 3066C).

Advertising

123. The application was advertised in accordance with Section 52 of the *Planning and Environment Act 1987 [the Act]* by way of 391 letters sent to the surrounding property owners/occupiers and by five signs displayed on the Robert Street, Waterloo Road and laneway frontages of the site.
124. A total of 45 objections and 1 letter of support were received. The grounds of objection included:
- (a) neighbourhood character, overdevelopment and heritage (including views to the heritage tower);
 - (b) height;
 - (c) urban design;
 - (d) zoning does not reflect character/history of the area;
 - (e) off-site amenity (overlooking, reduced daylight, overshadowing);
 - (f) on-site amenity (insufficient open space);
 - (g) interface with B3Z and commercial/industrial properties;
 - (h) safety (traffic and pedestrian along Waterloo Road and Robert Street, moss along laneway) and anti-social behaviour;

- (i) increased traffic / safety;
- (j) narrowing of Robert Street will restrict access into commercial properties to the north;
- (k) impact on the environment;
- (l) insufficient car parking (including loss of parking on-site and loss of parking should the Wellington Street bicycle lane be approved);
- (m) construction phase (noise, impact on access, pressure on on-street car parking and potential damage to surrounding properties);
- (n) waste removal;
- (o) no on-site loading bay;
- (p) insufficient infrastructure (public transport, car parking, childcare and education facilities);
- (q) application inaccuracies; and
- (r) property devaluation.

125. The letter of support stated:

- (a) the tower is in a state of disrepair;
- (b) this development would make it economically viable to restore the brew tower;
- (c) the proposal would provide much needed additional housing to the area; and
- (d) the development would revitalise the area.

126. A Consultation Meeting was held on 12 June 2012, where the key issues raised in the objections were discussed with the Permit Applicant, Objectors, a Ward Councillor and Planning Officers present.

127. Following the consultation meeting, the Applicant lodged amended plans (Section 57A of the Act) on 4 July 2012 (the decision plans). The amended plans made the following changes to the original plans:

- (a) total number of apartments increased from 349 to 350;
- (b) total number of car parking spaces reduced from 348 to 336;
- (c) increased setbacks between the central apartment building and the north-west apartment building (14 storeys) to a minimum of 7.2m;
- (d) an additional setback of 6m average is provided to levels 13-16 of the 17 storey apartment building from Waterloo Road;
- (e) a reduced setback of the north-western apartment building to a minimum of 15.7m from the north-south carriageway; and
- (f) a total of 200 onsite storage cages would be provided.

128. These plans were re-advertised and additional objections were received, bringing the total to 103. Along with objectors re-iterating concerns, one group of residents (9 objections) provided conditional support of the application. Suggested conditions included private agreements between the Applicant and objectors and road safety improvements (as per the Applicant's Road Safety Review prepared by GTA). For the purpose of this assessment, these submissions offering conditional support will continue to be treated as 'objections'.

129. The advertised material indicated that the deli would open on Monday between 7.00 am and 11.00 pm. This has been confirmed by the Applicant as an error, where the deli would be open every day. Given the advertised information included Monday trading (generally a more sensitive time, being a weeknight) it is not considered that re-advertising is required as a

result of the Applicant's clerical error. It is also noted that the café was advertised as trading seven days, 7.00 am to 11.00pm, and did not generate any specific objections.

Referrals

130. The application was referred to:

Internal departments

- (a) Engineering Services Unit (traffic and parking);
- (b) Engineering Services (assets);
- (c) Urban Design Unit;
- (d) ESD Advisor;
- (e) Strategic Transport Unit;
- (f) Open Space Unit;
- (g) Services Contracts Unit;

External consultants

- (h) traffic consultant (Traffix);
- (i) urban design (Rob McGauran);
- (j) acoustic (Marshall Day Acoustics);

External Referrals

- (k) Department of Transport;

131. The comments below are based on the original plans, unless otherwise noted. Given the nature of the changes detailed in the amended plans, re-referral to all internal departments, external consultants and external referrals was not considered necessary.

Engineering Services Unit (traffic and parking)

132. The following relevant comments were received:

Car parking provision – Residential Use, Convenience Store (Deli) and Café

- (a) *All residents and prospective property owners of the new dwellings will be ineligible to apply for on-street resident and visitor parking permits. On-street parking for residents is not a practical or viable option, and the local roads surrounding the site cannot sustain any resident parking.*
- (b) *In recent times, there has been a growing trend in the emergence of multi unit residential developments that are providing a proportion of residences without any on-site parking. When purchasing or intending to move into the dwellings, residents /occupants will know up-front that some of the dwellings have no on-site parking. These dwellings would be appropriate for persons who frequently use public transport, bicycles or other forms of sustainable transport – and it is also highly likely that the new dwellings would be specifically marketed towards this group.*
- (c) *The site is located on the south side of Robert Street, which a road frontage also on Waterloo Road, and is approximately 35 metres east of Wellington Street and 312 metres north of Victoria Parade. The site has no direct access to public transport; however tram series can be accessed from Smith Street and Victoria Parade a few hundred metres away. Similarly, the nearest shops, supermarket and essential facilities are located in Smith Street.*

- (d) *For the deli and café uses, it is agreed that much of the patronage would be drawn by employees from the local businesses and residents, rather than attract people extraneous to the area.*
- (e) *The level of on-street parking in the area is very high. Given that both Robert Street and Waterloo Road contain parking prohibitions, any likely overflow in visitor parking would probably be accommodated along Wellington Street or in nearby Peel Street, Langridge Street and possibly Derby Street. It is agreed that visitor parking demands would peak in the even times and weekends,*
- (f) *The implementation of a green travel plan and initiatives to promote more sustainable travel modes would certainly be a positive way of off-setting the use of a motor vehicle.*
- (g) *The parking provision provided for this development is not dissimilar to that of many other large scale developments we have reviewed in the past. From a traffic engineering perspective, the development should be able to function with its on-site parking provision if green travel initiatives are actively pursued and implemented.*

Traffic Generation

- (h) *Robert Street and Waterloo Road both have one-way traffic operations in the eastbound directions, and connect with Rokeby Street, which has a one-way traffic operation in the southbound direction, intersecting with Langridge Street. Vehicles exiting the property should be able to be dispersed easily, since these roads have one-directional flows. A total of 272 on-site spaces would be accessed from the Robert Street entrance (around 78% of the total on-site parking) and remaining 76 spaces would be accessed from the Waterloo Road entrance.*
- (i) *The traffic generation rate (0.3 vehicle movements per dwelling per peak hour) adopted by GTA is considered reasonable and is consistent with the RTA NSW's Guide to traffic generating developments (a reputable source) and other large scale developments we have reviewed in the past. The residences would be expected to generate 105 vehicle trips in each peak hour. The deli and the café would each generate one trip per peak hour.*
- (j) *Figures 7.3 and 7.4 of the report show development traffic volumes superimposed on existing conditions and provide a good snapshot of anticipated post development traffic trips and movements in the AM and PM peaks. According to GTA, the operation of the development would result in minor delays at the intersections of no more than one second. The road network has capacity to accommodate the volumes generated by the development.*

Access Arrangements

Robert Street Frontage

- (k) *There is no objection to the construction of a new vehicle crossing on the south side of Robert Street. The crossing must comply with the following:*
 - (i) *The vehicle crossing shall be constructed in accordance with City of Yarra Standard Drawings and Specifications;*
 - (ii) *The development's finished floor levels relative to the existing footpath and road levels must be such that pedestrian and vehicular access accord with the Australian/New Zealand Standard AS/NZS 2890.1:2004;*
 - (iii) *Existing kerb and channel, and road pavement surface levels should not be altered. Council may permit the adjustment of Building Line levels to provide access in accordance with AS/NZS 2890.1:2004; and*
 - (iv) *The design and construction of the vehicle crossing must also satisfy the requirements of Council's Community Amenity unit's Vehicular Access into*

Properties (Info Sheet and Application Form) before a vehicle crossing permit can be issued.

- (l) *The entrance width and ramp grades comply with AS/NZS 2890.1:2004 and are considered satisfactory.*
- (m) *Vehicles entering the development must enter on the left hand side of the entrance, and [exit] on the right hand side as shown on the drawing.*

Waterloo Road Frontage

- (n) *There is no objection to the existing opening on the north side of Waterloo Road (4.96 metres wide).*

Removal of Redundant Vehicle Crossings

- (o) *All redundant vehicle crossings along the property's Robert Street frontage shall be broken out and reinstated with paving, kerb and channel of the surrounding area. The cost of these reinstatement works shall be borne by the developer.*

Internal Layout

Ground Floor Plan – Hayball Pty Ltd Dwg No. A12.02 Rev 1 dated 13 October 2011

- (p) *The proposed ground floor car park of 31 spaces is to be serviced by TrendVario car stackers manufactured by Klaus Multiparking GmbH. North of the aisle, the multi stacker is intended to service small cars and the multi stacker along the south edge of the aisle would service 11 regular cars (85th percentile vehicles).*
- (q) *Given that it is proposed to accommodate 20 small cars, it is the onus and responsibility of the designer and the developer to ensure that each small car parking space size is clearly documented into its associated property's land information certificate. Small parking spaces must be clearly designated in the car park.*
- (r) *The blind aisle extension and aisle are considered adequate for this arrangement.*

Basement Level 1 Plan – Dwg No. A12.01 dated 16 August 2011

- (s) *A check of the Basement Level 1 drawing indicates that a further 20 small car parking spaces is proposed. This further addition of small cars is considered excessive, and this will not be supported by Council's Engineering Services unit. All the stackers and regular car parking spaces should be designed to accommodate 85th percentile vehicles on this level. The two stackers servicing small cars are to be upgraded to cater for 85th percentile vehicles.*
- (t) *The dimensions of the aisles, blind aisle extensions and stacker envelopes are generally satisfactory. The internal ramp grades are also considered satisfactory.*
- (u) *The disabled parking space should be designed with an associated shared area as required under the Australian/New Zealand Standard AS/NZS 2890.6:2009.*
- (v) *There is no objection to the use of a traffic signal system for cars negotiating the circular ramp.*

Basement Level 2 Plan – Dwg No. A12.01 dated 16 August 2011

- (w) *The dimensions of the aisles, blind aisle extensions and stacker envelopes are generally satisfactory. The internal ramp grades are also considered satisfactory.*
- (x) *All stackers are to accommodate 85th percentile vehicles.*

Use of Multi Car Stacker Devices

- (y) *Council's Engineering Services unit is usually supportive of the use mechanical parking devices for developments. We are generally satisfied with the comments prepared by Mr Simon Gilson, Director, Klaus Multiparking Australia (Appendix D of the GTA report).*

- (z) *With the exception of the 20 small car parking spaces on the ground floor, the designer is to ensure that all other car stackers for the development are to accommodate 85th percentile vehicles.*

Provision of Underground Utility Services and

- (aa) *The trenching and excavation for underground utility service connections by service authorities on Public Highways (Robert Street, Waterloo Road and any other road directly linked with service provisions) will require the reinstatement/rectification of Council road infrastructure.*
- (bb) *It is the developer's responsibility to coordinate and manage the service authorities and their works associated with underground utility provision and connections to the subject development. Trenches or areas of excavation made as result of laying underground services to the development will require the full width reinstatement of footpath and the replacement of damaged kerb and channel to the satisfaction of Council's Engineering Services unit.*

Reconstruction of Robert Street and Waterloo Road by the Developer

- (cc) *During the construction of this development, heavy trucks, plant and equipment will continuously traverse over the bluestone pavements of Robert Street and Rokeby Street, and will cause significant damage to these pavements and surrounding road infrastructure. These pavements were not originally designed to take heavy vehicle loadings, let alone on a frequent basis.*
- (dd) *Upon completion of all building works and underground service connections, the developer shall reconstruct the lengths of Robert Street and Waterloo Road to the satisfaction of council. The reconstruction will comprise full depth pavements, paving, and kerb and channel (where applicable). The new pavements are to be designed to accommodate the new traffic loading generated by the development.*
- (ee) *The costs of two road reconstructions as well as all road infrastructure reinstatements and rectification works associated with utility service provision and building works shall be borne by the developer.*

Drainage – Legal Point of Discharge

- (ff) *The applicant must apply for a Legal Point of Discharge under Regulation 610 – Stormwater Drainage of the Building Regulations 2006 from Yarra Building Services unit.*
- (gg) *Any storm water drainage within the property must be provided and be connected to the nearest Council pit of adequate depth and capacity (legal point of discharge), to Council's satisfaction under Section 200 of the Local Government Act 1989 and Regulation 610.*
- (hh) *Areas must be provided inside the property line and adjacent to the footpath to accommodate pits and meters. No private pits, valves or meters on Council property will be accepted.*

Waste Management Plan

- (ii) *The matter of waste management for the site should be referred to Council's Engineering Operations branch (based at the Collingwood Depot), who administer waste management in the municipality. Having reviewed the waste management plan, it is agreed that Robert Street and Waterloo Road should not be obstructed. Further comment should be sought from Engineering Operations.*

133. The following comments were received on the S57A amended plans (due to basement re-configurations and submission of a road safety audit):

Design of Ramp servicing Basement South

- (a) *Council's Engineering Services unit and traffic units have assessed the swept path diagrams provided by GTA Consultants for the operation of the ramp servicing the car*

parking levels for Basement South. We are satisfied that the swept paths for a 99th percentile vehicle (5.2 metres by 1.94 metres) can negotiate the circular sections of the ramp satisfactorily.

- (b) However, it is noted that there are no transition grades between the flat, circular sections and the 1 in 4 straight sections of the ramp. This will cause vehicles to scrape and bottom out. The designer needs to have this item addressed.

Design of Ramp servicing Main Basement Car Park

- (c) The curved ramp at the north east corner of the main basement car park is generally satisfactory. The outer radial of the curve should be provided with a outside kerb/clearance of 500 mm as required by the Australian/New Zealand Standard AS/NZS 2890.1:2004 Figure 2.9.

Proportion of Small Cars

- (d) The high number of small cars intended to be accommodated on site needs to be managed by the Owners Corporation of the site.

Road Profiles of Robert Street and Waterloo Street

- (e) Any geometry that is altered on any Public Highway under Council's management must be submitted to Council's Infrastructure and Special Projects branch for assessment and approval.
- (f) A site inspection of Robert Street revealed that the north footpath and carriageway measure approximately 0.92 metres and 4.28 metres respectively. Waterloo Street has an effective carriageway width of 4.11 metres.
- (g) The submitted concept detail sketches of the road profiles for Robert Street and Waterloo Street are insufficient and unsuitable for engineering assessment purposes. The designer does not appear to have taken into account the doorways which abut the commercial properties on the north side of Robert Street. The footpath width in Robert Street is excessive and restricts the carriageway width for manoeuvring commercial vehicle that have a legitimate right to access the properties abutting the north side of Robert Street. The proposed 3.2 metre wide carriageway of Robert Street will not be acceptable.
- (h) Preliminary engineering designs of the road profiles/cross sectional drawings of Robert Street and Waterloo Street should be submitted to Council before consent can be considered and granted by Council's Infrastructure and Special Projects branch under the powers of the Road Management Act 2004. It is the designer's responsibility to check the manoeuvrability of vehicles entering and exiting the doorways of individual properties directly opposite the development along the north side of Robert Street and the south side of Waterloo Street.

Road Safety Review conducted by GTA Consultants

- (i) GTA Consultants, in conjunction with the developer, conducted a safety review of the roads immediately surrounding the development. A meeting with the Mayor, Council officers, the developer, GTA, local residents and business operators was convened to discuss traffic concerns that exist in the local area and to find appropriate solutions and treatments where required.
- (j) Council's Infrastructure and Special Projects branch advises the following:
 - (i) A number of items in the traffic review centre on the conspicuity and reinforcement of One Way signs and No Entry signs in Robert Street, Waterloo Street and the north-south private carriageway easement. The review also recommends the installation of Give Way holding lines at the intersection of Robert Street and Rokeby Street. A recommendation was also made to the installation of a Left Turn Only sign on Langridge Street at the Rokeby Street intersection for east bound motorists. The Infrastructure branch has no objection to the provision of these items.

- (ii) *The traffic review recommends the installation of road humps in Robert Street and Waterloo Street. In principle, we have no objection to this option. However, the developer shall be responsible for proposing appropriate locations for the road humps and at the developer's expense, and consult with abutting properties. Approval for the installation for the humps will be subject to support from abutting property owners/occupiers.*
- (iii) *The suggestion to remove the kerb extension at the Rokeby Street approach to Langridge Street requires further analysis and collection of traffic data of the intersection to determine whether there is merit in removing the kerb extension. Traffic conditions at this intersection will need to be monitored over a period of time. Council had installed the kerb extension following community consultation. To remove the kerb extension (if it is warranted), the developer would also be required to consult the local community before approval can be granted.*
- (iv) *The suggestion to remove parking in order to facilitate truck turning movements out of Robert Street and Waterloo Street onto Rokeby Street requires approval from Council's Parking Services unit. Again, public consultation will be necessary. The suggestion of installing a Loading Zone on the east side of Wellington Street is not likely to be supported by Parking Services, especially in light of the possible installation of an on-road bicycle facility (Copenhagen type treatment) on Wellington Street.*
- (v) *In relation to the potential conflict between pedestrians and vehicles exiting the development's access off Robert Street, it is acknowledged that the heritage constraint on the existing building cannot allow for the provision of a visibility splay.*
- (vi) *The review had identified instances where vehicles have been deliberately travelling the wrong direction along One Way roads or trucks obstructing access. Council has no authority to enforce moving traffic violations as this is the responsibility of Victoria Police.*
- (vii) *The traffic review did not consider the request raised by the community to provide one-way southbound traffic arrangement along the Right of Way that connects Robert Street to Langridge Street. This needs to be further investigated by the developer's traffic consultant to determine whether this is warranted or not. If deemed appropriate, the developer must consult with adjacent properties before approval can be granted.*
- (k) *A plan showing area containing Robert Street, Waterloo Street, Rokeby Street Langridge Street and Wellington Street should be submitted, showing all the One Way and No Entry signs that are to be installed or relocated. The Give Way signs and associated line marking should also be shown on the plan. The Left Turn Only sign should also be shown on the plan.*
- (l) *It is important to note that Council's Infrastructure and Special Projects branch is not in a position to consider or give final approval for the road humps in Waterloo Street and Robert Street or the removal of the Rokeby Street kerb extension until the developer has obtained the necessary traffic data and a majority of support from the local community. It is considered advisable and prudent that these traffic management measures are investigated and assessed by the developer's traffic consultant once the development is fully operational.*

Engineering Services (assets)

134. The following comments were received:

General comments:

- (a) *It is not clear for me how pedestrians access from Wellington Street is shown through private land (In accordance to the GIS information there is no public laneway road leading from Wellington Street into the proposed neighbourhood square and food and drink premises).*

- (b) *I understood that some of the pedestrian access is considered through the easement created at the western boundary of 1- 21 Robert Street which is currently private property and will remain as a private property on the completion of the development.*

Comments on the street –lighting:

- (c) *No street lighting has been provided to the pedestrian’s access from Wellington Street. I am not sure how it can be provided given that the laneway is a private land. The planning application indicate that this project aimed to create “ perception of pedestrian safety” see point 4.8 of the submitted application. This perception required adequate level of lighting. At this stage no lighting is being considered.*
- (d) *No street lighting has been considered to Waterloo Road which had been designated as a pedestrian access to the Stables and Cellar building as well as to neighbourhood square and brew tower.*
- (e) *No plans has been submitted to Council in regards to the lighting for neighbourhood square including lighting in the vicinity of food and drink premises and deli.*

Other comments related to functionality of the development:

- (f) *I was unable to find any loading/unloading facilities allocated to the deli and/or food and drink premises*
- (g) *There is no “On Street” parking allocated to disabled/ “access for all” bays. Similar absence for the Victoria gardens development resulted in the inability to deliver “food on wheels” services to some of the residents, problem for the visitors with limited abilities etc.*
- (h) *I have concern with the circulation roadways in car park. Mark could please check whether they comply with the standard.*
- (i) *On the basement at level 1 entrance to the service room in the vicinity of circulation ramp (east side) does not look like is designed at the floor level*
- (j) *Council (Transport Unit) is currently undertaking survey for the proposed parking ban on one side of Wellington Street between Johnston Street and Victoria Parade in order to accommodate new on road bike facility. If adopted – it may have impact on this development.*
- (k) *Please alert the applicant that Council does require that street lighting for pedestrian areas has to be designed in accordance to the requirements of AS1158.3.1. Council will only accept luminaries which are approved as standard by our DB – Citi Power. It is Council requirements that new luminaire are from Energy Efficient list. Particular attention has to be given to the light spillage into the windows of the existing and proposed properties. In my opinion power supply to the proposed lighting poles should be underground no overhead cables.*

Urban Design Unit

135. The following comments were received, with the original plans not being supported:

- (a) *The overall massing of the proposal needs further punctuation. The length and height of the eastern tower needs to be reconsidered.*
- (b) *A pedestrian connection and view lines from Robert Street to the neighbourhood square would be preferred.*
- (c) *The proposed extension of the Cellar building needs to be set back further to attain views of the Brewery Tower at the junction of Rokeby and Waterloo Streets.*
- (d) *The proposal needs to provide landscaping details.*

ESD Advisor

136. The following comments were received:

Applicant Commitments:

- (a) *The provision of a rainwater tank of 75,000l, providing water for toilets of 110 bedrooms.*
- (b) *A target-recycling rate of 80% of construction and demolition waste will be achieved.*
- (c) *A solar contribution of at least 40% to the northern building's hot water demand will be met.*
- (d) *Provision of sliding shading louvers to northern, eastern and western facades (confirm full extent).*
- (e) *Common area corridors will have access to daylight and natural ventilation.*
- (f) *Two car share spaces will be provided.*

Application Deficiencies:

- (g) *Provide a vertical facade articulation without the use of tinted glazing. Tinted glazing impacts on internal amenities through reduced access to daylight and reduced passive heat gains in winter. Tinted glazing therefore increases an apartment's energy demand for heating and lighting.*
- (h) *Provide information about the provision of hot water for town houses and the central building.*
- (i) *Clearly identify all sliding shading louvers on architectural drawings.*
- (j) *Provide one bike rack per dwelling, encouraging all residents to use sustainable transport modes.*

Further Recommendations:

- (k) *The applicant is encouraged to consider the inclusion of ESD recommendations, detailed in this referral report. Further guidance on how to meet individual planning conditions has been provided in reference to the individual categories. The applicant is also encouraged to seek further advice or clarification from Council on the individual project recommendations.*

Updated Information:

- (l) *Should a permit be issued, it is recommended that all major ESD commitments, the deficiencies addressed and the outstanding information (Council Assessment Rating - 2) are outlined in an updated SMP report and are clearly shown on Condition 1 drawings.*

Strategic Transport Unit

137. The following comments were received (based on the amended S57A plans):

Green travel initiatives

- (a) *The submitted Green travel Plan is considered generally satisfactory. Sustainable transport information should be placed where it will be clearly visible to the residents, visitors, and employees of the developments.*
- (b) *The actions indicated in section 5 of Green Travel Plan to implement according to the submitted report need to be implemented, and managed and monitored by the body corporate.*

Bicycle parking provision

- (c) *According to the applicant's report, the development will be provided a total of 348 on-site parking spaces which are well short of the statutory car parking requirement of some 702 spaces.*

- (d) *The development will be provided in total of 128 on-site bicycle parking facilities including 40 visitors spaces, and a bike share station, which is just above the statutory requirements.*
- (e) *As the devolvement proposed well short of car parking, and it is not known how many dwellings will not have any on-site parking, it is recommending to increase the number of bike parking facilities on site.*

Layout of the bicycle facilities

- (f) *Access to the 88 bicycle storage spaces on ground floor is not satisfactory. The bike parking space is not close to the entrance of the development and it is not reasonable to expect cyclists to carry bikes to 6 set of stairs to access bike store spaces.*
- (g) *40 visitors bicycle parking spaces are also not satisfactory as cyclists need to carry their bikes to 5 set of steps to access visitors parking.*

It is noted that the following matters should also be considered in locating any bicycle facilities:

- (h) *Parking should be on same floor level, and be accessible with minimal risk of conflict with drivers accessing their ground parking spaces;*
- (i) *Ideally all bicycle storage will be closest to street level to encourage use of this transport mode.*

General (pedestrian and vehicular) access around the site

- (j) *The key pedestrian access has provided through Waterloo Road and which is only 4.96 metres wide. Poor pedestrian access has been provided, and no pedestrian improvements have been proposed to support the residents to walk locally to use public transport, recreation and retail facilities.*
- (k) *Waterloo Road should be remodelled as a shared zone, to visually identify it to all users as preferred area for pedestrians, and to facilitate bicycle entry to all bicycle parking areas.*

Others

- (l) *The plan does not indicate any disabled parking bays for the visitors or retail stores. The applicant should meet the requirements for the disabled parking bays for this development. Disabled parking spaces must be located close to lift wells.*
- (m) *There are poor provisions for people with disabilities, either as residents or visitors to the development.*

Open Space Unit

138. The following comments were received on the original plans:

Open Space Contribution

- (a) *The Yarra Open Space Strategy identifies the criteria for the provision of new Small Local open space as:*
 - (i) *Minimum land area of 0.03Ha.*
 - (ii) *Minimum width in any direction of 10 metres.*
 - (iii) *Entry from local access street (not a laneway).*
 - (iv) *Good visual access to the reserve, preferably with two sides accessible.*
 - (v) *Where possible, link to the existing open space network via key pedestrian and/or cycle links (Refer to Drawing No. YOSS06).*
 - (vi) *Receives reasonable sunlight access.*
 - (vii) *Adjoining buildings do not disproportionately overwhelm the open space.*
 - (viii) *Located where some borrowed green can potentially add value to the small reserve.*
 - (ix) *Demonstrate there is no soil contamination that will limit safe use as open space.*

- (b) *The proposed internal piazza/neighbourhood square does not meet a number of these criteria, specifically:*
- (i) *The space has no street frontage, with access is via laneways off the surrounding street network rather than directly from local access streets.*
 - (ii) *Whilst passive surveillance of the internal piazza/neighbourhood square from the surrounding residential and retail development that overlooks the space will be quite good, there is poor visual access of the space from the outside of the development.*
 - (iii) *The proposed internal piazza/neighbourhood square is heavily overshadowed by the surrounding development (both existing and proposed).*
 - (iv) *The proposed internal piazza/neighbourhood square is surrounded by multi-storey buildings on all sides which will dominate the space and create a sense of enclosure.*
 - (v) *Due to the internal nature of the space, there is limited opportunity to capitalise on 'borrowed green' outside of the development.*
- (c) *As identified above, the internal piazza/neighbourhood square does not satisfy the criteria for the provision of new open space, as outlined in the Yarra Open Space Strategy, and accordingly will not be considered as a land contribution in lieu of a cash open space developer contribution for the development.*

It is noted that it is not the intent of the permit application for the internal piazza/neighbourhood square to be considered as a land contribution.

Landscape General

- (d) *Detailed landscape plans should be submitted for approval prior to the commencement of works showing:*
- (i) *All existing trees to be retained or removed including species, height and spread.*
 - (ii) *Planting plans and schedule with location, botanical name, common name, mature height and spread, installation size, spacings and quantities.*
 - (iii) *A legend indicating key features, materials and construction details.*
 - (iv) *Existing and proposed levels including top wall and fence heights.*
 - (v) *Existing underground and overhead services.*

The internal piazza

- (e) *The preliminary nature of the plans does not provide much information on the design of the internal piazza. While Council will not be responsible for owning or maintaining this space, we offer the following observations:*
- (i) *Hard surfaces seem to dominate the space. This, coupled with the surrounding building facades, may produce a cold and inhospitable environment. Additional planting could soften the feel of the piazza.*
 - (ii) *Additional planting might also provide some more visual interest to the plaza – which threatens to become a rather vacuous space once the café umbrellas come down and the farmers market tables are put away.*
 - (iii) *The piazza is heavily overshadowed by adjacent buildings, which will further impact on the amenity of the space (and should be considered if the space is to support a successful café premises that wishes to promote al fresco dining).*
 - (iv) *It is not possible to comment on the appropriateness of the proposed tree planting above the basement car parking as there is no detail to indicate this proposed treatment.*

The Wellington Street pedestrian link

- (f) *Aesthetically we have no objection to the addition of bluestone edging to the existing asphalt paving, however the design would have to be approved by Council's Engineering department.*

- (g) *Consideration should also be given to any impact these works might have on any existing trees that are to be retained.*

Rooftop landscapes

- (h) *Again, these landscapes do not fall within Council's responsibility, being private open spaces, however:*
 - (i) *There is little detail to indicate how the landscape will be established.*
 - (ii) *The "rooftop with patterned ballast" could display a little more design flair for visual interest.*

139. The following comments were provided in relation to the Robert Street 'shared zone':

- (a) *Generally the treatment of the shared street/lane looks good, as long as the materials etc are ok with Engineering.*
- (b) *It's not really clear from the plans, but I think they are showing two options, one with trees and one without. We feel there is not adequate space in the laneway to accommodate any street trees and we would not be prepared to maintain them in this location. For these reasons we would not accept the plan showing street trees.*
- (c) *Our comments in relation to detailed landscape plan required remain per previous.*

Services Contracts Unit

140. The following comments were received (based on the amended S57A plans):

- (a) *The Waste Management Plan (WMP) from Leigh Design, dated 2 July 2012, is satisfactory from the Engineering Operations Branch's perspective.*
- (b) *(However, I make the comment that, due to severe restrictions, such as heritage restrictions, town planning requirements, etc. the WMP is far from best practice from a waste management perspective. However, it can be also said that it is a reasonable outcome, all things considered.)*

Traffic consultant (Traffix)

141. The following comments were received (based on the amended S57A plans):

Based on our various investigations, we are satisfied that:

- (a) *under the parking requirements of Clause 52.06, the proposed development requires a dispensation of 94 car spaces,*
- (b) *the proposed carparking provision is considered to be satisfactory under the decision factors provided at Clause 52.06-6,*
- (c) *the level of bicycle parking provided for the development is satisfactory, subject to the appropriate design of the bicycle parking areas and it is recommended that the following two conditions should be included in any permit issued:*
- (d) *A minimum of 88 resident bicycle spaces and 40 visitor spaces to be provided on the site, or to the satisfaction of the Responsible Authority.*
- (e) *Bicycle parking spaces are to be designed to accord with the requirements of the Planning Scheme or to the satisfaction of the Responsible Authority.*
- (f) *the layout of the carpark is generally acceptable, subject to the following issues being resolved:*
 - (i) *the internal access ramp for the south carpark should be modified in order to accommodate the turning requirements of a B99 design vehicle in a single manoeuvre, in accordance with AS2890.1-2004,*

- (ii) *the ramp grades along the access ramp to the south carpark should be modified such that they accord with the requirements for grade changes under Design Standard 3 of Clause 52.06-8,*
 - (iii) *at least 25% of all car stacker spaces should be provided with a height clearance of at least 1.8m, in accordance with the Clause 52.06-8 (Design Standard 4) requirements, and*
 - (iv) *small car spaces are to be labelled as such on the plans.*
 - (g) *the concept plan prepared by Tract for Robert Street is generally acceptable, however Council should review whether the northern kerb should be retained,*
 - (h) *Waterloo Road should be signed as a shared zone with appropriate traffic management devices installed,*
 - (i) *we are satisfied with the waste collection arrangements for the site, subject to waste collection for the residential component being limited to off-peak times as a condition of permit, and*
 - (j) *a dispensation of loading activities for the commercial tenancies is appropriate.*
142. If a planning permit is to issue, the following should also be imposed by way of permit conditions (as suggested by the Applicant's Traffic Engineer):
- (a) *all transitions from a 1:4 (25%) grade should be increased to 2.5m; and*
 - (b) *the access way to the south basement levels should be increased from 6m to 6.1m wide.*

Urban design (Rob McGauran)

143. The following comments were received:

ASSESSMENT OF THE PROPOSAL

- (a) *In a strategic sense the site is suited to the proposed uses. It is well located relative to a range of public transport choices, albeit that all involve a level of walking.*
- (b) *The site is a significant one within its inner urban location and the adaptive reuse within a body corporate structure for a site containing heritage buildings such as this is an appropriate management structure and form of urban renewal that is likely to better retain the valued attributes of the heritage site than alternative uses. The nearby Foy and Gibson projects provide some history of success of similar models.*
- (c) *The pre-existing tall brew tower and silos collectively establish a history of higher density taller development on the site and visible built form that is supportive of a scale of renewal greater than that envisaged for the surrounding areas to the south, east and north and this higher level of density can be seen to help support and underpin viable regeneration and restoration of valued heritage attributes of the site. That being said this prevailing scale is in the order of 10 to 12 residential storeys.*
- (d) *The general configuration of the site, providing for a network of walking pathways and a central plaza within the site and clearly designed pedestrian linkages to the perimeter is consistent with good urban practice.*
- (e) *Likewise, the activation of the Robert Street frontage with residential development at elevated street level and upper levels reinstates a street life to these frontages supportive of the site's higher dependence on a safe and supervised environment for pedestrian movement in and around the site. The podium reference to the pre-existing perimeter two and three level scale references the existing streetscape attributes.*
- (f) *The underlying environmental and urban design strategies and the organisation of built form have also been informed by expert support in the areas of heritage, traffic, parking and green travel, environmental design, landscape, acoustics and environmental amenity. These have helped the designer articulate a built form solution that achieves*

noteworthy outcomes in a range of areas, most notably environmental design and green travel.

Areas of further investigation

- (g) *Having vindicated a number of the underlying design decisions I think the most significant issues to determine relate to:*

Is the proposed height of the taller buildings acceptable?

144. Is the juxtaposition of these taller buildings to the abutting neighbouring development and streets acceptable?

Are the impacts of these proposed buildings on key views acceptable?

Is the quality of the design resolution of these tower forms acceptable?

Is the juxtaposition of new development to the heritage forms acceptable?

Are there amenity issues arising from internal layouts that need to be addressed?

- (a) *In the other matters as previously noted, I believe there is clear strategic support for renewal of the site for primarily residential uses. The inclusion of taller built form as part of that resolution and the approaches taken on environmentally sustainable design, parking and public realm.*

LAYOUTS

- (b) *The site is configured for primary car access from the eastern end of the Robert Street frontage immediately abutting the adjacent properties. Robert Street is lined with two storey townhouses with access from the street to these residences.*
- (c) *A mid- frontage lobby extends to the upper level tower mid block tower, whilst at the eastern end a further lobby services the easternmost tower.*
- (d) *The south side sees the reuse of existing heritage form for apartments either side of the driveways to the small at grade car park and providing access to a basement car park.*
- (e) *South of the entrance to the basement car park a lift lobby provides access to the core for this medium rise central tower building.*
- (f) *To the western end of the Waterloo Road frontage a small existing annexe to the brew house incorporates a new lift and stair providing access to upper levels of the brew tower building.*
- (g) *Bike storage is located primarily at ground level at the eastern end of the frontage between the basement ramp to the car park off Robert Street and the at grade car park.*
- (h) *The organisation and access arrangements for the site are generally supported, in conjunction with the existing heritage buildings to the Waterloo Road frontage. Their retention is obviously consistent with heritage values ascribed to the site.*
- (i) *The designer has activated Robert Street with two storey townhouses elevated from the street frontage, thereby providing reasonable standards of amenity for residents, (with living areas on the lower floor and bedrooms above) whilst enhancing informal street surveillance and activation.*
- (j) *The central area of the site is partially occupied by the medium rise building, juxtaposed between the eastern tower and to the east, the brew tower to the west and the new Robert Street buildings to the north.*
- (k) *The reservations that I have about the site planning relate to the modest level of setback between the Robert Street podium form and the central courtyard building. It is my view that the 4.9m established at levels 1-8 is insufficient, creating an unnecessarily cavernous attribute to the development.*

- (l) *This could I think be mitigated through the removal of the northern apartment of this central block on all levels, opening up a more generous and less conflicted engagement between these central south facing apartments and the neighbourhood square and northern pedestrian link to the eastern tower. The layouts for the interiors of the units are generally satisfactorily managed.*
- (m) *The current relationship is depicted in Dwg A06.07 and again demonstrated in the internal east elevation on Dwg A06.08. It is an interface that I think needs to be expanded to approximately 9m and this could be achieved with the removal of these northernmost apartments and would result I think in a far better interconnection between the eastern tower and neighbourhood square and the northern towers, podium and neighbourhood square and moreover a more satisfactory relationship between the adjacent residential occupants in each of these levels.*

RECOMMENDATION

- (n) *Remove the north east and north western apartments of the central tower from ground level to the uppermost level.*
- (o) *Elsewhere I believe the footprint of development is reasonably satisfactory. A question does arise in relation to the eastern interface, where the development is proposing a setback of just under 3m to just under 4m from the eastern interface.*
- (p) *However balconies are indented further creating an overall setback in the order of 5m from this eastern interface. I am satisfied this is consistent with VCAT decisions and reasonable levels of development equity subject to there being a reverse amenity obligation on development along this interface to ensure that noise from adjacent industry is mitigated through appropriate window treatments to this interface. I note the acoustic report suggests one of two options facade treatments on the subject site or noise attenuation on the adjacent site. The latter outcome could be seen to substantially constrain change and development of the adjacent site for ongoing industrial operations and should not be supported raising the possibility of future objection arising from change and placing the onus on the existing use to upgrade at its cost the solutions. The onus should be recorded on the permit by condition and noted explicitly that external balcony zones to this frontage are unlikely to achieve necessary comfort levels and that windows, doors and facade treatments will require careful acoustic design with the criteria established to the satisfaction of the responsible authority as to the necessary technical criteria.*
- (q) *I note the applicant has stated that Mel Consultants [wind consultants] have been retained and that walking levels of comfort have been achieved to surrounding streets and lanes. I am not in receipt of this report. The neighbourhood square will require a higher standard as will shared community rooftop spaces in order to be fit for purpose for the extended sitting time that might be expected in these areas and conditions of permit should seek to secure this standard for these areas of the development, a standard that requires lower wind speed in these zones. This is an essential attribute of the development if the high density urban village is to work successfully.*

Internal issues

- (r) *The internal corridor widths vary between 1800mm and 1400mm. I am of the view that the 1400mm width is inadequate and note that in some instances this reduces further, such as in the southern end of the eastern tower, to a width of less than 1200mm. Corridors should typically be 1500mm in width with indents at entrances to apartments wherever possible. This provision would apply to each of the towers. I am aware the designers have adopted this methodology in other developments that I have reviewed both within the municipality and at Docklands.*
- (s) *The provision of storage for residents is ambiguous. 6 cubic metres per dwelling should be provided for in the development.*
- (t) *Mail areas need to be clearly defined for each building*

- (u) *Waste management would need to be managed to the satisfaction of the responsible authority.*

Traffic

- (v) *I rely on advice from Councils traffic engineer- clarity regarding how taxi drop off will be managed would be desirable for example.*

Height and heritage interface

- (w) *The final matters relate to height and interfaces with the heritage buildings.*
- (x) *The western Robert Street tower generally speaking replicates approximately the scale of the existing silos and is I believe satisfactory.*
- (y) *Subject to the reduced footprint previously noted, the central tower block building is also, at its nine levels, seen as a reasonable response. Siting within the view shadow of development to the north and setback from abutting street interfaces creating a clear delineation between the heritage podium and the Waterloo Street interface.*
- (z) *The matter of most conjecture will be the eastern tower.*
- (aa) *At 17 levels it represents a significantly greater height than the previous silos that occupied the site and other buildings in the immediate environment and occupies an area of the site more distant from the historic location of this taller form.*
- (bb) *In an urban design sense, support for greater height can be put on the basis that the location of the tower is well set back from the more significant adjacent streets of Wellington, Rokeby and Hoddle and is partially obscured by other buildings on the site from the south west, west and north west.*
- (cc) *The architect has sought to develop a design solution for this building as shifting plates and deeply expressed balconies with sliding screens to their frontage and a shaped and configured external envelope. These floor plates shift at intervals of three to four floors, creating a more diverse external form as a result.*
- (dd) *The applicant argues that the scope of development is necessary in order to fund the significant cost of restoration of the heritage elements that I understand the development sought within the proposal. I have not been a party to this economic analysis and would rely on the determination of Heritage Victoria as to its bona fides.*
- (ee) *In urban design terms there is arguably little difference in off-site impacts whether this building is 17 or 12 levels*
- (ff) *The substantiating arguments that could be put in an urban design sense arise around:-*
- (gg) *the existing presence of the silos and Brew tower buildings and the precedent for taller buildings a principle upheld in similar sites where change has been sought*
- (hh) *The high cost of restoration of severely degraded heritage stock on the site*
- (ii) *a scale of development that primarily corresponds to that of the existing silos and brew tower*
- (jj) *a modest stepping up from the adjacent 13 level building to the west.*
- (kk) *The question is whether the additional levels can be seen as supportable.*
- (ll) *In this latter context there are some precedents within the municipality and elsewhere in the city, where improvement to public space and local amenity in conjunction with high levels of sustainability performance and innovation has warranted some relaxation of scale.*
- (mm) *In their environmental design response to the site and their provision of public space the applicant has gone to a laudable level of outcome to build the case for support for this taller form, with buildings achieving exemplary levels of environmental performance and with there being a number of innovations within the development including:-*

- (nn) *visitor units*
- (oo) *significant investment in shared community spaces, both at ground level and within the individual buildings There is no doubt in my mind that considerable effort has been made by the applicant to create a visually engaging building at the detail level.*
- (pp) *The inclusion of sliding screens and forms to the south, east, north and western facades combined with the subtle shifting of floor plates create a built form with some visual interest.*
- (qq) *My concern however relates to the lineal nature of the building and the absence of any receding of the building from its southern edge to Waterloo Street. I am less concerned regarding the Robert Street frontage where significant built form has been the prevailing view from the east for many years and where the width of the street diminishes the ability to perceive the difference between a small number of levels.*
- (rr) *This juxtaposition is evident on images five and six on page 20 of the applicant's Town Planning Design and Context Report.*
- (ss) *In my view the design would be substantially improved in its response to context, with the removal of upper level tower form to the Waterloo Road frontage through the increased setback of the of the upper three floors of the development from Waterloo Road.*
- (tt) *however there is a lack of well put argument around why the site justifies a stepping up to this 17 level at the eastern interface against what is acknowledged as a prevailing perimeter form of five levels to the south, east and north.*

RECOMMENDATION

- (uu) *Remove the southernmost apartments to increase the setback for the uppermost four floors (levels 13 to 16) of the eastern tower by an additional 6m from the Waterloo Road interface to maintain the expressed indent (level 13) between mid rise lower level development and upper level development (levels 14 to 16 inclusive).*

RESPONSE TO THE HERITAGE MATTERS PREVIOUSLY RAISED

- (vv) *The applicant has noted in Dwg A07.02 the proposed interface of the tower and stables using precedents from other projects, the proposed interface and linkages with the tower and eastern heritage wall and the proposed juxtapositions between the remaining heritage buildings and new built form.*
- (ww) *The substantial setback for the most part of new development from the Waterloo Road street wall enables the existing buildings to be read with clarity.*
- (xx) *The choices of materials and finishes that are incorporated into the setback zone behind these heritage street edges are ones that have been tested in other environments such as the Melbourne GPO successfully and would similarly do so in my view in this instance and are supported in heritage design terms.*
- (yy) *The resolution of the edges with the stables is also quite sophisticated in its handling and is worthy in my view of support.*

CONCLUSION

- (zz) *Subject to the other adjustments previously recommended, the balance of the project can I think be supported in urban design terms based on the substantial contribution it makes to retention of valued heritage, the provision within the development made for public and community shared space and the relatively benign impact the development has other than in changing distant views from surrounding residential communities and more locally based light industrial uses. That being said it is a site not generously serviced by surrounding street capacity to its north and south and the location of access points will put increased pressure on these interfaces. This congestion and intensity needs to be weighed against the merits of retention and adaptive use of the*

heritage stock, a decision being determined by Heritage Victoria based on both the merits of the design and the economic cost of retention and restoration. On balance with the amendments suggested I am prepared to support the project based on the considerable research, analysis, design tactics and the program of the development. The tower expression has made the expression of scale more ambiguous and the visual interest achieved through the deep reveals and sliding screens provides considerable animation and visual interest. These measures combined with high quality public realm treatments, good unit planning and the recommended changes will I think deliver a satisfactory outcome.

Acoustic (Marshall Day Acoustics)

145. The following comments were received:

- (a) The Applicant's acoustic report incorrectly states the surrounding land is zoned Industrial 1 and that the nearest residential property is over 100m away in Peel Street. The surrounding land is zoned Business 3 and the closest residential properties are along Wellington Street to the west of the site.

The Applicant submitted an amended Acoustic report in light of these omissions.

- (b) Wellington Street workshop – although the development can incorporate double glazing to achieve SEPP N-1 compliance inside the proposed dwellings, SEPP N-1 also requires compliance within balconies and courtyards. The onus is on the workshop to achieve compliance with SEPP N-1 requirements.
- (c) Rooftop Plant – As with the Wellington Street workshop, the seafood processing plant in Robert Street must meet the requirements of SEPP N-1. Although the proposal has incorporated measures to achieve SEPP N-1 compliance inside the dwellings, the processing plant may need to attenuate roof top plant to ensure they meet the stringent SEPP N-1 requirements (including compliance in balcony and courtyard spaces).
- (d) On-site noise emissions – the proposal has generally addressed on-site noise sources. However, the following should also be required:
 - (i) A condition to ensure music noise associated with the café and deli meet SEPP N-2 noise limits.
 - (ii) Hours of operation and outdoor patron limits can mitigate amenity impacts associated with the café and deli.
 - (iii) Hours of operation can mitigate amenity impacts associated with the level 9 roof terrace.

Department of Transport

146. No objection was received by DOT subject to the following conditions being imposed on any permit:

- 1. *Before the use of the land commences, a Green Travel Plan must be prepared to the satisfaction of the Responsible Authority following consultation with the Department of Transport (Public Transport Division). The Plan must be prepared by a suitably qualified person and must encourage the use of non-private vehicle transport modes by the occupiers of the land. The Plan must include the following:*
 - a) *A description of the location in the context of alternate modes of transport and objectives for the Green Travel Plan;*
 - b) *Outline Green Travel Plan measures for the development including, but not limited to:*
 - i. *Household welcome packs – tram, train and bus timetables relevant to the local area must be included in the pack of information provided to purchaser's upon a purchaser's occupation of an apartment;*
 - ii. *Include a minimum to 10 by 2 hour Metcard (zone one / two) within the household welcome pack or a Myki pass (of equivalent value) and registration information;*

- iii. *Bicycle parking and facilities available on the land;*
 - iv. *Pedestrian routes to key destinations.*
2. *The Green Travel Plan must not be amended without written consent of the Responsible Authority following consultation with the Department of Transport (Public Transport Division).*
 3. *Once approved the Green Travel Plan must form part of the planning permit and any ongoing Management Plan for the land to ensure the Green Travel Plan continues to be implemented by residents / owners to the satisfaction of the Responsible Authority.*

Planning Considerations

147. The following key issues and policies will be used to frame the assessment of this planning permit application:
- (a) strategic policy;
 - (b) land uses;
 - (c) public open space contribution;
 - (d) urban design;
 - (e) on-site amenity;
 - (f) off-site amenity;
 - (g) environmental sustainability;
 - (h) car parking / traffic;
 - (i) bicycle parking and facilities;
 - (j) loading facilities;
 - (k) waste management;
 - (l) cancellation of section 173 agreement; and
 - (m) Objector concerns.

Strategic Policy

148. As a summary point, in relation to the SPPF and LPPF it is considered that the proposed development achieves the various land use and development objectives outlined earlier in this report, providing a good level of compliance with the relevant policies within the Scheme.
149. Broadly, the Scheme at both the State and Local levels support the intensive redevelopment of the site having regard to the Mixed Use Zoning of the land, clauses 11, 16.01, 18.01, 21.04 and the sites designation as a SRS in Council's MSS. At both State and Local levels, there is a clear preference to encourage higher density developments in established activity centres or on strategic redevelopment sites. This ensures efficient use of infrastructure and supports Council's preference that established residential areas experience residual increase in population density.
150. At a local level, Council's MSS (clause 21.04-1) also directs higher density development and forecast population increases to designated strategic redevelopment sites. Clause 21.05-2 provides guidance on built form outcomes specifically:

Strategy 17.2 Development on strategic redevelopment sites or within activity centres should generally be no more than 5-6 storeys unless it can be demonstrated that the proposal can achieve specific benefits such as:

- *Significant upper level setbacks*
- *Architectural design excellence*

- *Best practice environmental sustainability objectives in design and construction*
- *High quality restoration and adaptive re-use of heritage buildings*
- *Positive contribution to the enhancement of the public domain*
- *Provision of affordable housing.*

151. State Policy encourages the consolidation of existing urban areas in planning for urban growth with commercial development located near public transport. Specifically, the proposed development is supported by clauses 17.02 (Business) and 21.04-3 (Industry, office and commercial), which has an objective to encourage development of commercial services and provide more employment opportunities while efficiently using existing infrastructure.
152. To guide the process of redevelopment and urban renewal of the subject site and surrounding land, a range of built form controls apply to the land. The eleven (11) design principles outlined in clause 15.01 (Urban Design) and the DSE Guidelines for Higher Density Residential Development will be used to assess the appropriateness of the built form proposed. The DSE Guidelines and Council’s Interface Uses Policy (clause 22.05) will also be used to assess on and off-site amenity impacts. A detailed assessment against these policies/documents is offered in the following sections of this report.
153. State and Local Policy at clauses 15.02-1 and 21.07-1 aim to encourage energy efficient outcomes and the minimisation of greenhouse gas emissions. The Applicant has elected to participate in Council’s SDAPP initiative (Sustainable Design Assessment in the Planning Process) and has submitted a SMP. A detailed ESD assessment will follow later in this assessment.
154. Generally, the development has been found to have in principle strategic support given it’s classification as a strategic redevelopment site. The following sections of this assessment will provide an on balance assessment against all relevant decision guidelines within the Scheme.

Land uses

155. The café and deli are the only uses which require a planning permit. The hours and patron numbers are outlined below:

| | Cafe | Deli |
|--|--|---------------------------------|
| Hours of operation | 7.00 pm to 11.00 pm, seven days | 7.00 pm to 11.00 pm, seven days |
| Patron numbers (maximum at any one time) | 40 patrons inside and 40 outside (within the neighbourhood square) | N/A |
| Staff (maximum at any one time) | 5 | 1 |

156. The purpose of the Mixed Use Zone includes ‘To provide for a range of residential, commercial, industrial and other uses which complement the mixed-use function of the locality’.
157. The proposed café and deli would both cater to the needs of existing and future residents and businesses in the surrounding area. However, having regard to the most relevant local policy (clause 22.05), it must be considered whether the uses would unreasonably impact the amenity of the future residents and existing residents along Wellington Street. Off-site amenity considerations include:
- (a) overlooking of private open space and habitable room windows;
 - (b) noise (hours of operation, patron numbers and location of plant and equipment);
 - (c) construction materials (e.g. masonry as opposed to curtain walling);

- (d) views to the proposed café and deli (avoid unsightly views);
- (e) waste management; and
- (f) light spill.

Each of these issues will be discussed in turn.

Overlooking of private open space and habitable room windows

158. Although the café and deli incorporate windows which face the residential properties along Wellington Street, the proposed windows are at the ground level. The habitable room windows to the west of the site, are either at the first floor (90 Wellington Street) or over 10m away (82 Wellington Street). Screening is therefore not necessary. The proposal is considered to activate and improve the interface of the site with the carriageway easement, as is supported by clause 15.01-4 (Design for safety).

Noise (hours of operation, patron numbers and location of plant and equipment)

159. Council's external acoustic advice suggested:
- (a) *A condition to ensure music noise associated with the café and deli meet SEPP N-2 noise limits.*
 - (b) *Hours of operation and outdoor patron limits can mitigate amenity impacts associated with the café and deli.*
160. The Applicant's Acoustic Report (AECOM) stated:
- (a) Plant and services that are to be installed as part of the convenience store and business centre will need to comply with the noise limits prescribed by SEPP N-1.
 - (b) Plant may include exhaust fans, air conditioning units, or comms equipment. All equipment will be acoustically assessed as part of the design development, and appropriate measures devised such that compliance with the noise limits will be achieved.
 - (c) Noise due to patrons entering and leaving the food and beverage outlet [cafe] may also cause a nuisance to residents located nearby, such as the southern apartments located directly south of the food and beverage outlet. The design of the food and beverage outlet, as well as the design of the Southern Apartments should be such that noise intrusion from patrons outside the food and beverage outlet is minimised.
161. Conditions can ensure that plant and equipment would be selected, located and treated to meet SEPP N-2 and music noise can be controlled (or lowered) to ensure compliance with SEPP N-1. Should a permit issue, a condition should also ensure that speakers are not located outside the building.
162. In relation to patrons leaving the café at night, Local Policy does not guide what are 'reasonable' hours of operation in a MUZ. Clause 22.01 guides discretionary uses in the R1Z (8.00 am to 8.00 pm – except for a convenience shop), however this site has different characteristics to a pure residential area. Clause 22.09 (Licenced Premises Policy) states that premises in a Residential or Mixed Use Zone or within 30 metres of a residential area should generally not trade beyond 11pm, however a liquor licence is not sought in this instance.
163. Whilst the indoor seating may be appropriate until 11.00 pm, the outdoor seating is not. Once patrons are in the neighbourhood square, there are no mitigating measures (i.e. walls or other barriers) to address noise from patrons.
164. A condition on any permit issued should therefore require the outdoor seating area associated with the café use to cease at 8.00pm.

construction materials (e.g. masonry as opposed to curtain walling)

165. With the café being in the brick brew tower and the deli being in a 'brick type' tile on top of cement sheeting, the development has selected materials to reasonably mitigate noise impacts associated with material selection.

views to the proposed café and deli (avoid unsightly views)

166. Views to the café would be to the brew tower with contemporary, plain glass infill panels. Views to the deli would be to the side wall, which is part black/brown brick tiles, part clear glazing. The views would be a significant improvement to existing conditions.

Waste management

167. Waste management considerations are offered later in this assessment.

Light spill

168. Although lighting details are not shown on the plans, a condition on any permit issued should ensure that any external lighting is located and baffled to ensure unreasonable glare is not directed towards nearby residents.
169. Should a permit issue, a condition should also require the plans to show 'convenience shop (deli)' and 'food and drinks premises (café)' to mirror the Scheme terminology and avoid confusion.
170. Subject to the conditions contained in this report, the proposed deli and café uses would not unreasonably impact the amenity of the surrounding area.

Public open space contribution

171. The subject site is in an area where land (in lieu of cash) is the preferred method of public open space contribution (area 3066C as outlined at clause 22.12). However, Council's Open Space Unit has not requested land in this instance.
172. Although it was not the Applicant's intention, the Open Space Unit noted that the internal piazza / neighbourhood square does not meet the Yarra Open Space criteria because:
- (a) it does not have a street frontage;
 - (b) there is poor visual access of the space from outside the development;
 - (c) the square would feel 'enclosed' if it were to be considered as a public space; and
 - (d) due to the internal nature of the square, there is limited opportunity to capitalise on 'borrowed green' outside the development.
173. These are locational factors which the development cannot change.
174. In this instance, the provision of cash in lieu of land is considered acceptable by way of a public open space contribution. This cash payment would be addressed in any future subdivision stage. A note will be included on any permit issued outlining a requirement for a cash contribution pursuant to clause 22.12 of the Scheme. It is also noted that this approach is consistent with the current S173 agreement which requires a cash payment at the time of subdivision.

Urban design

Site Analysis Plan and context

175. The Applicant provided a site analysis plan and urban context report with the application. The policy and physical context section above has summarised the key elements which should be considered.

Urban form and character

176. The site is an identified SRS at both State and Local level of the scheme and is within close proximity to the Smith Street MAC. Local Policy does not support heights on these sites greater than 5 to 6 storeys unless it can be demonstrated that the proposal can achieve specific benefits such as:
- (a) significant upper level setbacks;
 - (b) architectural design excellence;
 - (c) best practice environmental sustainability objectives in design and construction;
 - (d) high quality restoration and adaptive re-use of heritage buildings;
 - (e) positive contribution to the enhancement of the public domain;
 - (f) provision of affordable housing.
177. The obvious benefit in this instance is the restoration and adaptive re-use of heritage buildings. The Heritage Victoria permit already holds the developer to undertaking the conservation works prior to the commencement of the remainder of the development. This is a significant benefit and although heritage is not a relevant planning considered, is an important fact which applies to the site.
178. Notwithstanding the above, the site is approximately 4,560m² and the proposal has been massed to locate the taller forms away from the sensitive residential interface to the west (detailed later in the 'off-site amenity' assessment) and away from the most sensitive heritage building on the site being the brew tower.
179. The development is also considered to have architectural design excellence, as the later section of this urban design assessment discusses.
180. The development has achieved the 'best practice' rating under Green Star for the residential component and as the later 'environmental sustainability' assessment establishes, would achieve a reasonable level of environmental efficiency and would minimise greenhouse gas emissions.
181. The proposal demonstrates an improved public realm relationship as it is accepted that the site is currently blighted by dilapidated buildings, some with obvious heritage character, albeit in poor condition. The proposal would activate all street and laneway/carriageway frontages, as the following urban design assessment outlines and will restore the dilapidated state significant heritage buildings on site.
182. Although the MSS list is not definitive, the only element this proposal (arguably) does not achieve is affordable housing. The Applicant has not engaged an affordable housing association, however the proposed dwellings are of varying sizes and orientations to provide a range of options for potential purchasers/occupants which does address the issue of affordability and housing choice.
183. Contextually, built form in the area varies from 1 to 4 storeys, although the silos on the subject site are 32m and the brew tower is 35.9m. Council's external urban design advice noted that these structures have a prevailing scale in the order of 10 to 12 residential storeys.
184. When assessing the application the immediate interfaces with the site include:
- (a) commercial premises on the north side of Robert Street (B3Z);

- (b) offices and what appears to be either caretaker's residences or illegal dwellings on the south side of Waterloo Road (B3Z);
- (c) commercial premises to the east (B3Z), with a boundary wall built for the majority of the shared boundary, save north and south setbacks for car parking; and
- (d) dwellings in the 'Wellington Street buildings' adjacent to the site's carriageway easement along the western boundary (MUZ) with some commercial tenancies on the lower levels.

185. Amenity impact to the dwellings to the west are addressed in detail throughout the following 'off-site amenity' assessment section of this report. This urban design assessment however, addresses the interfaces from an urban design perspective, addressing matters such as height, setbacks, pedestrian experience and architectural quality.
186. Physically, the site and its context provides both opportunities including the sites designation as a SRS, relatively limited sensitive interfaces [west] and limited view lines to the site given it's positioning behind the major road networks and constraints heritage [not a planning consideration in this instance], narrow road access and limited pedestrian access.
187. Although not a planning consideration, heritage matters have to a great extent driven the massing and height of the development. The Applicant has indicated (as is a consideration in the Heritage Victoria permit process) that the yield has been driven by the feasibility of undertaking the required conservation works. The result is a 17 storey tower, pushed as far away from the historic brew tower as possible (44m to the east).
188. However, the above is not a valid planning consideration and the application must be assessed against the relevant planning tools.

Height and setbacks

189. With the above context in mind and acknowledging that policy-wise the site can accommodate at least 5-6 storeys (pending an off-site amenity assessment), the issues of setbacks and height must be carefully considered. Each podium form is addressed, followed by a discussion on tower heights.

Robert Street podium levels

190. To Robert Street, the podium ranges from 3 to 4 storeys (10.2m to 12.4m). The podium complements the 1-2 storey heights on the north side of the street and the one to two storey forms on the south (see figures 16 and 17).
191. Although 1-2 storeys taller, this provides a reasonable transition in height and is only 4m taller than the commercial premises to the east and 400mm taller than the residential building to the west.

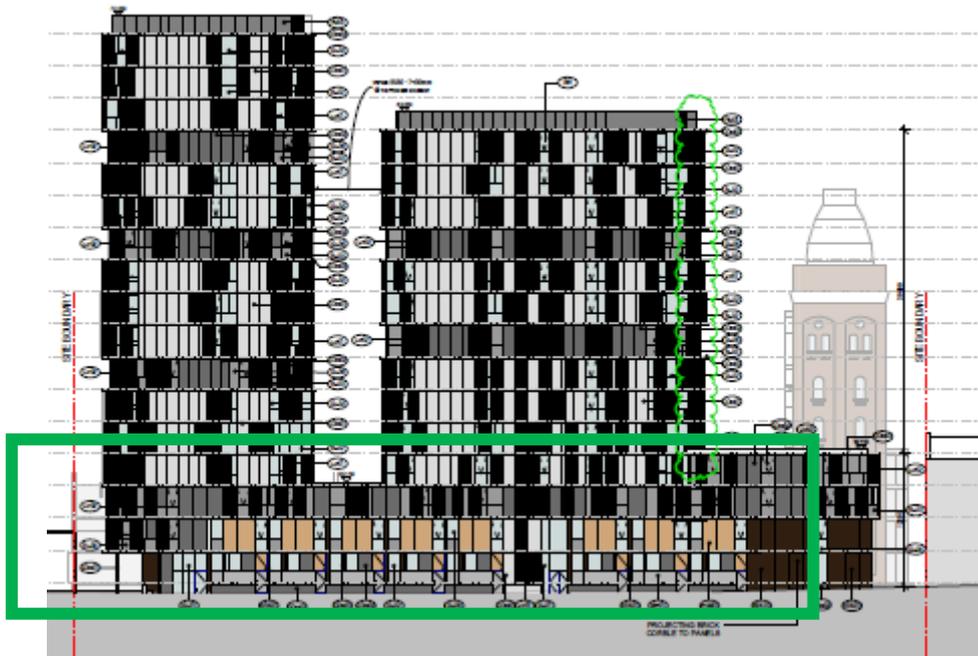


Figure 16 – Robert Street elevation



Figure 17: Robert Street looking east

192. This podium level also serves to provide a pedestrian scaled element presenting to the street, with the 14 and 17 storey towers sited above.
193. It is considered that the podium levels hard edge form respects the character of this area and appropriately responds to Robert Street by orientating access points, balconies and windows the street. Additionally the materiality of the podium includes a composition which is solid at the lower levels by using a combination of brick cladding, timber and metal and lightens as the building rises above ground level.
194. The pedestrian impact is addressed in the following sections of this assessment.

Carriageway easement



Figure 18: West Elevation

- 195. To the carriageway easement, the proposed built form is 4 storeys (12.4m). The Robert Street return of the building maintains the same façade composition presenting to Robert Street. The south western corner of the easement and Waterloo Road building also presents as four storey with minimal setbacks however adopts a different façade presentation using white seraphic and clear glass adopting a distinctive design typology which complements the brick associated with the retained heritage buildings.
- 196. This height and ‘hard edge’ form to the carriageway easement are supported in this context where boundary walls are commonplace. As with the Robert Street podium, the 4 storey height is comparable to the residential buildings to the west (400mm to 1.6m variations in height).

Waterloo Road podium levels

- 197. To Waterloo Road, the proposed built form associated with the podium ranges from 4 to 5 storeys (12.4m to 16m). Again, the built form is hard edged in response to the character of the area.
- 198. The 4 to 5 storey height is supported in this instance due to the 2 to 4 storey forms adjoining the south side of the road. These properties are in the B3Z, which does not bring rise to off-site amenity considerations (compared to a residential zone, for example), further supporting a taller podium form along the south boundary.

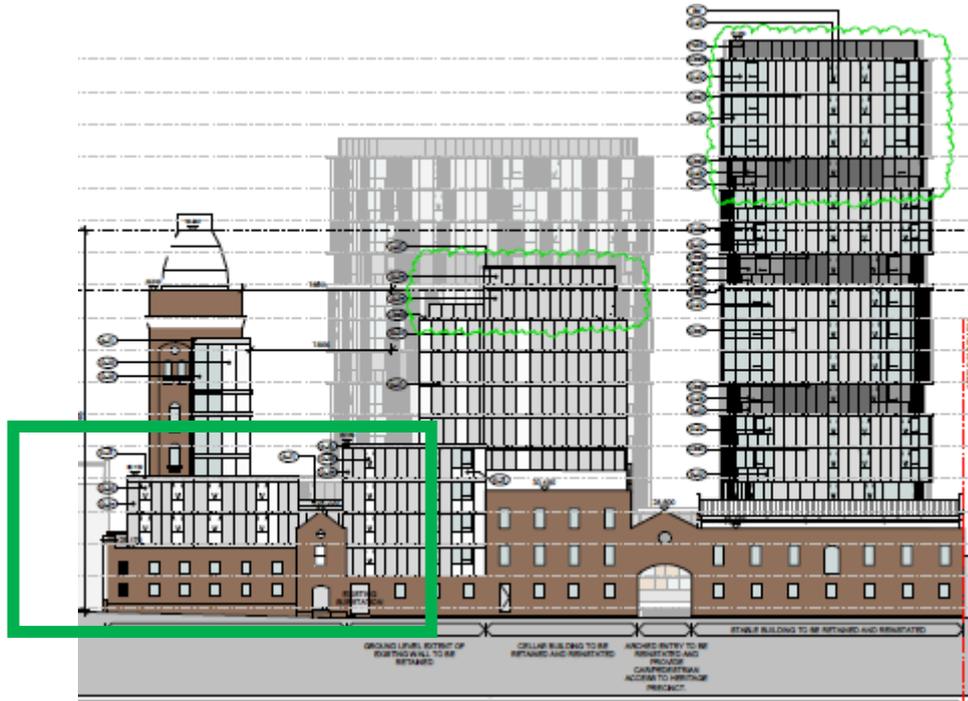


Figure 19: Waterloo Road elevation



Figure: 20: Intersection of the carriageway easement and Waterloo Road, looking north-east

199. The 4 to 5 storey Waterloo Road podium also acknowledges the contrast in the site's immediate interface with Robert Street. Robert Street contains lower built forms and is more 'pedestrian friendly' with a footpath (albeit narrowed). Waterloo Road does not contain a designated footpath and presents a different character through a shared road way which facilitates both vehicles and pedestrians, and a different scale relationship with the south side of the street. The proposal has responded accordingly with the proposed massing and use of two glazing types to activate the façade when viewed above the heritage building.
200. Where 4-5 storey buildings on either side of a narrow road could normally result in a 'canyon effect', the extensive use of clear and white glazing along Waterloo Road results in this element becoming a 'lightweight' addition to the heritage fabric. As can be seen in the

perspective above, rather than becoming a 'canyon', the Waterloo Road podium would activate the streetscape and add visual interest.

Tower forms

- 201. The subject site has both policy and physical support for higher built forms. The subject site is in a unique location accessed via narrow, one way streets which can be considered a limiting factor when access is considered, however also restricts immediate view lines to the site. Council's MSS acknowledges that the Municipality is 'a low-rise urban form punctuated by pockets of higher development' and view lines afforded to the site are considered further below.
- 202. The site also currently contains taller elements including the 32m high silos and a 35.9m brew tower. Viewlines to these elements are provided along Wellington Street, Rokeby Street, Robert Street, Waterloo Road, Oxford Street and Cambridge Street. Glimpses are also provided from Langridge Street and even Hoddle Street in some positions. Essentially, the site is already characterised as one with 'pockets of higher development'. This assessment must consider what view lines would be provided to the 10, 14 and 17 storey forms and whether this would be acceptable in this context.
- 203. Outlined below in figures 21 – 25 are the elevations and perspective views of the proposed tower elements.

17 Storey Robert Street Tower

- 204. The proposed tallest element of the proposal is the construction of a 17 storey tower in the north east corner of the site.
- 205. The siting of this element has been deliberate (by the architect) in locating the tallest building the furthest from the brew tower. This locational attribute of the building is consistent with the siting of the earlier approved 11 storey tower approved under planning permit PL03/0978 which has now expired and is consistent with CMP for the site.

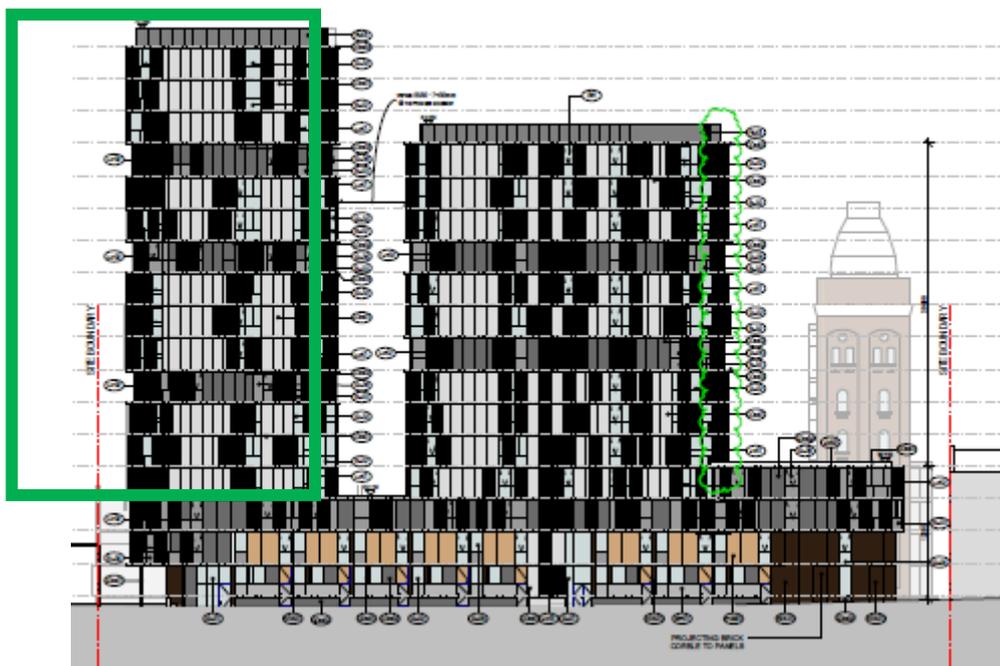


Figure 21 Elevation Robert Street



Figure 22: Robert Street looking west– NOTE this perspective has not been updated with the modified level 13 to 16 floor plate (setback from south boundary)



Figure 23: Waterloo Road looking west (based on S57A amended plans)

206. The composition of the façade has been broken into segments using deep reveals intermittently with 2 and 3 level stacked floor plates as a design tool to make the floor level ambiguous to lessen the expressed 17 storey scale of this tower.
207. The amended plans also include the set back of the upper four levels including a greater setback from the southern boundary as shown in Figure 23 above.
208. This element would be visible from the surrounding area and viewlines are explored later in the report.

14 storey Robert Street Tower

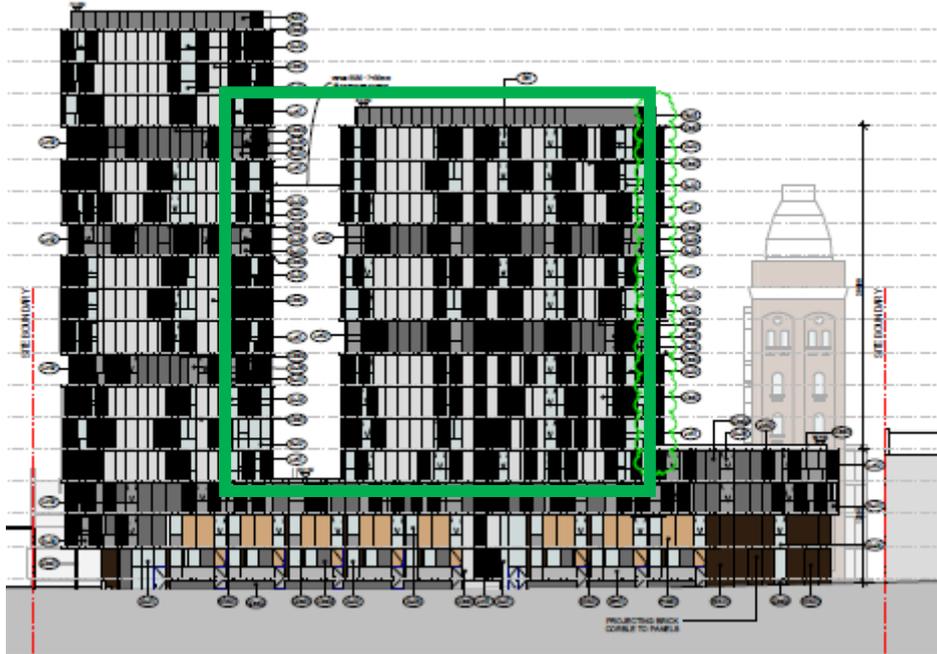


Figure 24: Robert Street, Elevation

- 209. The 14 storey component of the proposal sits midway along the Robert Street frontage. The composition of this tower adopts similar use of two deep reveals at levels 7 and 10 to break the vertical mass of the proposed tower.
- 210. This element sits proud of the brew tower by approximately 8.6 metres and presents a transitioning of scale between the 17 storey tower and the brew tower.
- 211. The siting of this building is roughly in the same location of the existing silos albeit presents as a taller form.

Waterloo Road 10 storey tower

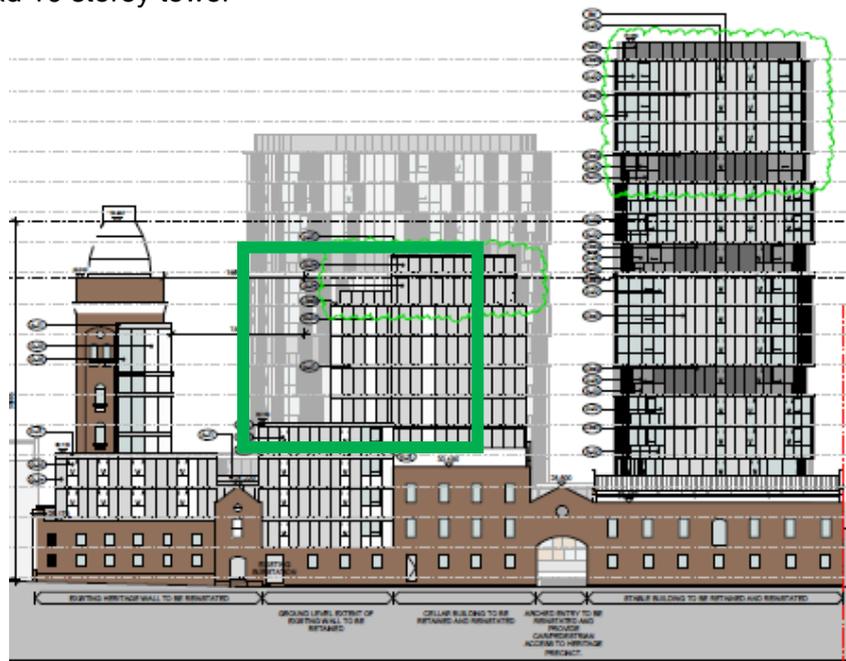


Figure 25: Waterloo Road Elevation

- 212. The part 9 part 10 storey component of the proposal sits midway along the Waterloo Road façade and is setback from this frontage by 9m and is located behind a part 4 part 5 podium level.
- 213. The scale of this building in part sits below the cornice line of the brew tower and where taller at the 10 storey part is 2.2 above the cornice line.
- 214. This tower is expressed differently to the 14 and 17 storey tower presenting as a clear glass and white seraphic glass curtain wall.

Addition to the Brew Tower

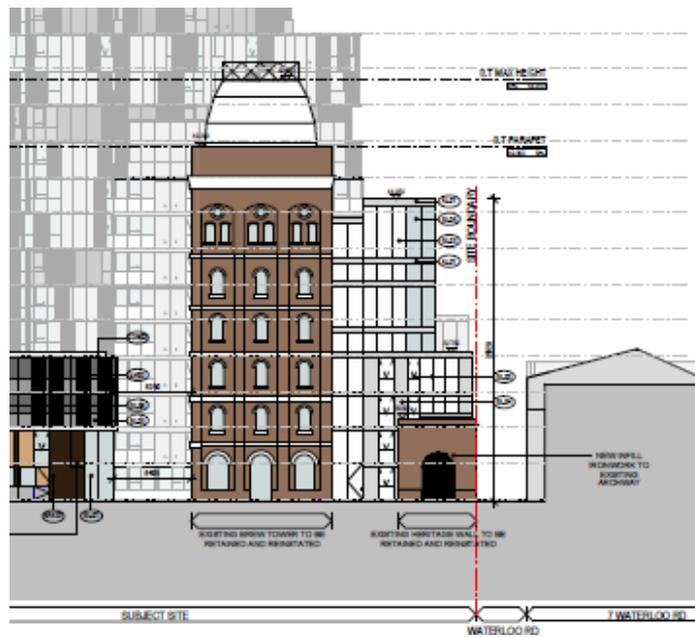


Figure 26: Easement (West Elevation)

- 215. The proposal incorporates a 7 storey building attached to the brew tower facilitating winter gardens associated with the dwellings located in the brew tower and a lift core providing access to the dwellings.
- 216. This element is designed similarly to the 10 storey tower being a glass curtain wall with clear and white seraphic glass. The width of this tower is narrower to that of the brew tower and while it would block some views to the brew tower maintains the three dimensional form of the brew tower due to its slender form.

North-west and south-west view lines

- 217. Accompanying the application is a series of long range view lines demonstrating the visibility of the proposed buildings and their relationship with the brew tower. Figures 27 - 30 demonstrate views along Wellington Street. Please note, these view do not incorporate the amendments included in the re-advertised plans.



Figure 27: Existing view Looking South on Wellington street near the corner of Peel Street



Figure 28: Proposed view Looking South on Wellington street near the corner of Peel Street



Figure 29: Existing view Looking North on Wellington street near the corner of Northumberland Street



Figure 30: Proposed view Looking North on Wellington street near the corner of Northumberland Street

218. The figures above demonstrate the visibility of the 10, 14 and 17 storey towers from Wellington Street, comparing them to the existing brew tower and silos. The 14 and 17 storey towers would appear taller than the brew tower from Wellington Street, however it is considered that the separation distance between the two elements do not overwhelm or detract from the prominence of the heritage tower. The 14 and 17 storey towers are setback over 54m from Wellington Street and would read as a backdrop to the 1-2 storey forms in the foreground. Reviewing these perspectives, only the top three levels of the 10 storey building would be visible (from the view lines provided being the balustrade of the level 9 roof terrace and two floors below).
219. The building heights are considered acceptable from these perspectives, with the architectural expression of the towers also creating visual interest, breaking the forms horizontally into segments though the use of tinted glazing and an alternating stacked floor plate.
220. Council's external urban design advice noted that 'The western Robert Street tower generally speaking replicates approximately the scale of the existing silos and is satisfactory'.

North-east and south-east

221. Figures 31 -34 similarly demonstrate the views at the intersection of Rokeby and Gipps and Rokeby and Langridge Street as these are some of the most exposed vantage points from Langridge and Gipps Street.



Figure 31: Existing View looking south west on Gipps Street near the corner of Rokeby Street



Figure 32: Proposed View looking south west on Gipps Street near the corner of Rokeby Street

222. From Rokeby and Gipps Streets, the 14 and 17 storey towers would appear as a background to the 1-2 storey forms in the foreground. While visible, the proposed building form is supported due to the high architectural merit of the proposal. From this view there is no impact on the presence of the brew tower, as it is not currently visible from this vantage point. The visible elements of the proposal have been segmented into horizontal elements to minimise the appearance of height or bulk.

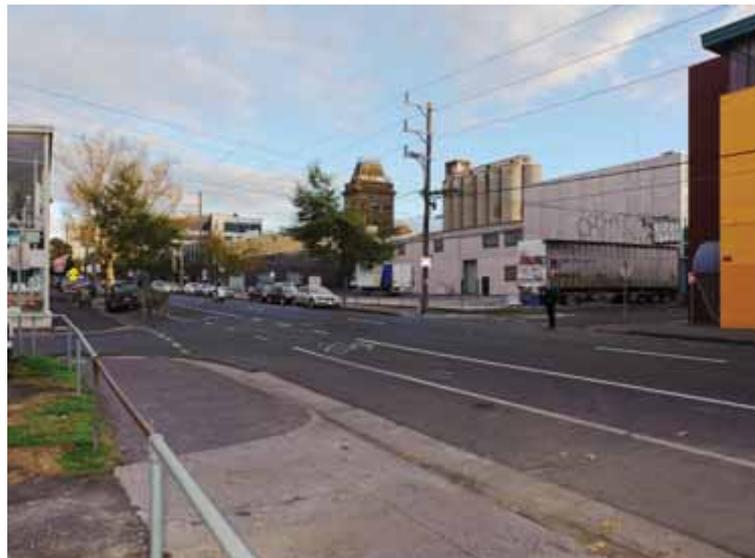


Figure 33: Existing View looking west on Langridge Street near the corner of Rokeby Street

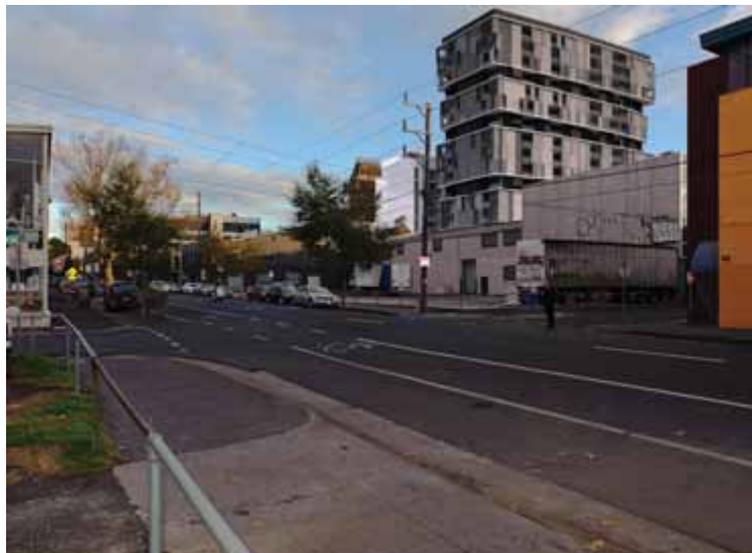


Figure 34: Proposed View looking west on Langridge Street near the corner of Rokeby Street

223. From Langridge Street both the 10 and 17 storey building will feature prominently within the streetscape. Council's external urban designer raised no issue with the expression of the 10 storey built form as it forms an appropriately scaled backdrop to buildings in the foreground. However concern was expressed with the 17 storey element. The image in figure 34 demonstrates the proposal prior to the amendments.
224. Nevertheless, the advice stated that the general location of a 17 storey form could be supported because *'...the tower is well set back from the more significant adjacent streets of Wellington, Rokeby and Hoddle and is partially obscured by other buildings on the site from the south west, west and north west.'*
225. Council's Urban Design Unit noted that *'the overall massing of the proposal needs further punctuation. The length and height of the eastern tower needs to be reconsidered.'*
226. Council's external urban design advice recommended that the upper three levels of the 17 storey tower be setback an additional 6m to provide a 'staggered' approach to the upper level capped form. The Section 57A amended plans have made this change, with the top four levels being setback an additional 5m to 8.7m from the south boundary. The result is as follows:



Figure 35: Original proposal



S57A plans with additional south boundary setback

227. Council's external urban design advice noted that *'...there is a lack of well put argument around why the site justifies a stepping up to this 17 level at the eastern interface against what is acknowledged as a prevailing perimeter form of five levels to the south, east and north.'* However, the plans have been amended to set away from the most exposed view lines (being Langridge and Rokeby Streets). The application must also be assessed on balance, with the overall streetscape and public realm improvements, heritage restoration improvements (although this is a consideration for Heritage Victoria) result in the conclusion that the proposal appropriately responds to the orderly planning of the area. This is also aided by the B3Z land to the south, east and north which limits off-site amenity impacts and provides opportunities for this redevelopment of this Strategic Redevelopment Site.
228. Council's Urban Design Unit noted that the proposed extension of the cellar building needs to be set back further to attain views of the brew tower at the junction of Rokeby and Waterloo Streets. However, view lines to the historic brew tower are a heritage matter and have

already been considered by Heritage Victoria (with a permit issued accordingly) and the change is considered to be unnecessary.

229. Having regard to the policy and physical context pertaining to the site, the development has been reasonably massed. The Section 57A amended plans have adopted the changes recommended to the 17 storey form in Council's external design advice. It is considered the proposal would provide an acceptable profile and silhouette to Yarra's urban form and skyline.

Light and shade

230. Having regard to shadowing of public spaces, the development would overshadow sections of the following:
- (a) 9.00 am – Wellington Street, Waterloo Road and the north-south laneway extending south off Waterloo Road;
 - (b) 11.00 am – Waterloo Road;
 - (c) 1.00 pm – Waterloo Road; and
 - (d) 3.00 pm – Waterloo Road and Rokeby Street.
231. Overshadowing of footpaths can be a marker of overdevelopment. Often, the height and setback of podium and tower configurations are guided by this impact (amongst other considerations).
232. Although Waterloo Road would be in shadow throughout the day, the road is currently in shadow by virtue of the existing buildings on the site. Waterloo Road also does not have a designated footpath, creating an atmosphere where any pedestrians would not currently expect the same level of amenity as along Wellington Street, for example. The other roads (Wellington, Rokeby and the north-south laneway) are only affected in either the early AM or PM and would not unreasonably impact the amenity of these spaces.

Street, public space quality and safety

233. Compared to existing conditions, the development would positively activate Waterloo Road, Robert Street and the carriageway easement in the western edge of the site.
234. In accordance with Objective 2.8 of the DSE Guidelines, the development would '...maximize informal or passive surveillance of streets and other public open spaces' through the:
- (a) presence of windows and balconies overlooking the surrounding streets and internal spaces;
 - (b) activation of the ground floor with the deli and café uses and dwelling with entrances to Robert Street;
 - (c) although the neighbourhood square will remain private land, residents and businesses in the area could still enjoy this space and the café and deli within. This would encourage pedestrian movement in and around the site; and
 - (d) removal of derelict non-heritage buildings and restoration of the heritage fabric improving perceived safety in the area.
235. The only exception to this is the 1.8m high fences/gates to the ground floor Robert Street 'podium' dwellings. Adjacent to these spaces are the living areas of dwellings, to which the north facing windows are their only source of light and ventilation. Objective 3.6 of the DSE Guidelines aims 'To avoid creating inactive frontages as a result of fencing private open spaces'. Design suggestion 3.6.1 is to 'use low height, transparent or partially open fences to create an impression of openness and permeability'. To address this, a condition on any permit should require these fences to be a maximum 1.5m high and at least 75 per cent

transparent to both provide an active street level and enable sufficient daylight to enter these spaces.

236. Given the proposed development would increase pedestrian movements into and around the site, pedestrian movements must also be considered. The creation of shared zones a recommendation made by Council's referrals in Waterloo Road and Robert Street is addressed in the car parking/traffic section of this assessment. Essentially, the surrounding road network needs to be modified to ensure that motorists are aware that pedestrians would frequent these streets as a result of increases to residential density in the area.
237. Council's Urban Design Unit suggested that pedestrian access to the neighbourhood square should be provided from Robert Street. However the site has benefit of a carriageway easement in the western edge to enable pedestrian access to the neighbourhood square. Access is also provided from the 'way' easement from Wellington Street and along Waterloo Road.
238. The Applicant originally applied for upgrade works to the 'way' easement in 80 Wellington Street (although this site was never included in the application form). This information remains on the landscape plans. The works were to upgrade this path as it would be a key pedestrian/bicycle entry point from Wellington Street. Considering that these works no longer form part of this application, it must be considered if this access way is suitable for pedestrians.



Figure 36: Way easement looking east

239. This easement is already used for pedestrian access to the two Wellington Street buildings and is illuminated and landscaped. Considering this, it is suitable for the key pedestrian/bicycle entry to the site from Wellington Street.
240. Council's Engineering Services (Assets) Unit requested details of street lighting (including the neighbourhood square, deli and café). Should a permit issue, a condition should require

a detailed lighting plan, confirming these spaces and the pedestrian and vehicular entries to the site from Robert Street, Waterloo Road and the carriageway easement would be illuminated. The Unit also noted that

- (a) Council requires street lighting for pedestrian areas to be designed in accordance to the requirements of AS1158.3.1;
- (b) Council will only accept luminaries which are approved as standard by our DB – Citi Power;
- (c) luminaire must be from the Energy Efficient list;
- (d) particular attention must be given to the light spillage into the windows of the existing and proposed properties; and
- (e) in my opinion power supply to the proposed lighting poles should be underground no overhead cables.

241. A condition should be imposed to require the following, noting that overhead cables would not be permitted in this location.

Wind

242. The Applicant provided a wind report (MEL Consultants), which outlined wind test results on a model of the proposed development. The testing established that design changes were required to address north-north-west and north-north-east wind conditions. The required changes were made to the submitted drawings and the report concludes that the development would be either on or within the criterion for walking comfort for all wind directions.

Landmarks, views and vistas

243. It has already been noted that the site is not within close proximity to an identified landmark or tall structure in clause 22.03 of the Scheme. More generally, views and vistas have already been addressed in the height and setback assessment above.

Site coverage/Permeability

244. The site would remain impervious, with the addition of to rain water tanks (75,000L capacity) reducing the amount of stormwater runoff from the site.

245. The site coverage would be increased to approximately 73 per cent. This is well under the 80 per cent recommended by clause 22.10-3.6 of the Scheme. (Although this clause is not relevant to site's covered by a Heritage Overlay, it is more appropriately tailored to Yarra's context than the standard ResCode rates of 60 per cent).

Architectural quality, colours and materials

246. The development is a high quality design, incorporating three general themes:

- (a) Robert Street podium;
- (b) Waterloo Road podium; and
- (c) the towers.

247. To Robert Street, a mixture of glazing and concrete panels are proposed, continuing the rectangular forms, with timber cladding to the lower two levels accentuating the 'podium'. This approach both respects the industrial forms on the north side of the street, whilst articulating the development as a residential form through the use of timber and a high degree of glazing which increases the higher up the built form.

248. To Waterloo Road, the design makes the distinction between 'old' and 'new' forms with clear and white glazed 'boxes' being positioned above the red brick base. The Waterloo Road tower continues this lightweight glazed box treatment and appropriately differentiates between Robert Street and the more heritage characteristic of Waterloo Road.
249. The Robert Street towers have adopted a cascading 'stack' design, with tinted glazing being used to accentuate horizontal bands. The external urban design advice obtained by Council notes that '*The tower expression has made the expression of scale more ambiguous and the visual interest achieved through the deep reveals and sliding screens provides considerable animation and visual interest*'.
250. The proposal is considered to be a highly resolved application, respecting the context of the buildings on the site and the industrial nature of those surrounding it.

Landscaping

251. The development includes limited landscaping in the:
- (a) neighbourhood square;
 - (b) ground floor planter boxes adjacent to Robert Street, the carriageway easement and the neighbourhood square;
 - (c) along the easement linking the site and Wellington Street; and
 - (d) roof terrace of the central building.
252. As the application address does not include 80 Wellington Street (the easement noted as being landscaped), the Applicant was approached to amend the application to include this parcel. The Applicant was not in a position to amend the application in this manner and as such, a condition should be imposed to require an amended landscape plan detailing no landscaping outside the title boundaries.
253. Council's Open Space Unit requested detailed landscape drawings be submitted prior to the commencement of the development. This should be imposed by way of permit conditions and specifically require details of planting, additional soft landscaping to the neighbourhood square (currently limited) and details of the planter beds above the basement to ensure species can function.
254. Council's Open Space Unit also did not support the planting of trees in Robert Street. This will be addressed later in the 'traffic' section of this assessment.

Service infrastructure

255. The plans detail service cupboards across both basement levels, in the ground floor and within each floor of the 17 and 10 storey buildings. Mail box locations however, have not been detailed and should be required by way of a permit condition.
256. Council's Engineering Services Unit have requested that Waterloo Road and Robert Street be re-constructed after the development is complete. However, the bluestone in Waterloo Road was recently re-laid. A condition should require the reconstruction of Robert Street between Wellington and Rokeby Streets, in bluestone), however the reconstruction of Waterloo Road may not be necessary. Should any damage occur to Waterloo Road during construction, Council's standard permit conditions would require road rectification works at the expense of the permit holder and to the satisfaction of the Responsible Authority. Additionally, consideration of a shared zone would be outlined in the traffic section of the report.

Urban design summary

257. Subject to the conditions contained in this report, the proposal responds appropriately to the physical and policy context of the subject site and would result in a development which positively responds to its site context.

On-site amenity

258. Relevant on-site amenity considerations will be addressed by way of themes, with the DSE Guidelines, clause 55 (not applicable, rather a decision guideline) and clause 22.05 (Interface Uses Policy) being the most relevant policies.

Overlooking

259. Views within 9m and within a 45 degree arc would be provided internally between the:
- (a) central row of south facing dwellings within the 14 storey Robert Street building to the northernmost units in the 10 storey building;
 - (b) easternmost dwellings in the cellar building and the westernmost dwellings in the stables building;
 - (c) balconies at the first floor of the 17 storey building;
 - (d) south apartments (closest exposed apartment) and the 10 storey building (closest apartment in the south-west corner);
 - (e) brew tower apartments and the adjacent Robert street building to the north (level 2 and above);
 - (f) brew tower apartments and the south apartments (level 2 and above);
 - (g) 14 and 17 storey Robert Street buildings (levels 3 and above);
260. The plans detail obscure glazing to some of these windows, however they do not confirm the windows are fixed or obscure to a minimum height of 1.7m. A condition should ensure that these overlooking opportunities are addressed, by way of 1.7m high, fixed obscure glazing or fixed, minimum 1.7m high, maximum 25 per cent transparent screens or the like to minimise overlooking opportunities.

Noise

261. The 'land use' section of this assessment has already addressed the amenity impacts of the café and deli (music, plant and equipment and patron noise). The following 'off-site amenity' assessment will also deal with noise associated with the residential plant and equipment.
262. This leaves the following relevant on-site amenity considerations in relation to noise:
- (a) noise associated with nearby industrial/commercial premises; and
 - (b) noise associated with the level 9 gym and lifts.
263. Council's external acoustic advice (Marshall Day) stated:
- (a) Wellington Street workshop – although the development can incorporate double glazing to achieve SEPP N-1 compliance inside the proposed dwellings, SEPP N-1 also requires compliance within balconies and courtyards. The onus is on the workshop to achieve compliance with SEPP N-1 requirements.
 - (b) Rooftop Plant – As with the Wellington Street workshop, the seafood processing plant in Robert Street must meet the requirements of SEPP N-1. Although the proposal has incorporated measures to achieve SEPP N-1 compliance inside the dwellings, the processing plant may need to attenuate roof top plant to ensure they meet the stringent SEPP N-1 requirements (including compliance in balcony and courtyard spaces).

264. In this instance, the use of the site as 'dwellings' does not require a planning permit. It is therefore not a question as to whether the use should be permitted, but rather, has the development reasonably protected itself from nearby commercial/industrial operations.
265. Council's Interface Uses Policy (clause 22.05) aims to both ensure a reasonable level of amenity for existing and new residents and protect the reasonable operations of nearby businesses.
266. In this instance, the appropriate design mechanisms to mitigate noise are dwelling layout and construction materials.
267. The development has incorporated double glazing to ensure that residents adjacent to the Wellington Street workshop (since identified as 79 Wellington Street) and the Robert Street seafood processing plant would achieve SEPP N-1 noise levels. However, a construction method or material cannot ensure that the balconies or open windows would still meet the SEPP N-1 limits (save acoustic screens which would be an onerous requirement in the MUZ).
268. In this instance, the Applicant has not placed the full obligation on these properties; rather the application includes reasonable measures to mitigate off-site amenity impacts.
269. The design has sited the 17 storey tower adjacent to the seafood processing plant, however the high rise heritage model (2001 Conservation Heritage Management Plan) required the built form to be sited as far away from the brew tower as possible. As has been highlighted in the 'urban design' and 'wind' assessments, this massing has evolved from the site analysis and balancing with heritage objectives.
270. Essentially, the development has done what it can to mitigate noise from nearby commercial/industrial properties. In many ways, the businesses in the surrounding B3Z have benefited from the MUZ land being vacant for many years. The Applicant's acoustic report has suggested that the adjacent balconies could be acoustically screened, however this is one approach where the plant and equipment themselves could be screened without impacting the amenity or outlook of the adjacent dwellings. A condition on any permit issued would require additional review of this interface issue.
271. The other issue raised in the Applicant's acoustic report was noise associated with the gym. As this is a matter that can be addressed through construction techniques (as opposed to restricting the use – which does not require a planning permit), there is considered to be a nexus with the permit triggers to require an amended acoustic report addressing the specific noise attenuation measures to protect residents from within the development from music noise and the dropping of weights. A condition should be imposed to this effect.
272. The Applicant's acoustic report has not addressed noise associated with the lifts and their impact on the proposed dwellings. This should be addressed by way of a permit condition and could require additional noise attenuation.

Private and communal open space

273. Most dwellings would be provided with 4.2m² to 91.6m² of private open space. Ten dwellings (within the former stables and cellar buildings) would not be allocated any private open space.
274. Although not all of the dwellings are provided with private open space (3 per cent have not) and some fall short of the recommended 8m² minimum area (Standard B28 of ResCode), the proposal is supported on the following grounds:
 - (a) a 493m² neighbourhood square is provided at the ground level; and

- (b) a community space is located at level 9 of the 10 storey Waterloo Road building (containing a gym, roof terrace, lounge and dining room).

275. There are viable alternatives for residents with no or limited private open space, ensuring their recreation needs can be met within the development.

Solar Amenity and daylight to windows

276. Objective 2.6 of the DSE Guidelines is 'To ensure areas can develop with an equitable access to outlook and sunlight'.

277. The ResCode standards are not equipped to deal with taller built form, as is proposed in this instance. However, VCAT decision *Cremorne Corporation Pty Ltd v Yarra CC* [2008] VCAT 1202 (2 July 2008) [**Ark decision**] established a reasonable tower separation principle:

63. *The council and Richmond Plaza argue that the east side setbacks proposed do not provide for an equitable sharing of development opportunities between the site and the Plaza's land. Essentially it was argued that a 4.5 metre setback to the edge of walls and balconies should be provided on this site, replicated on the Richmond Plaza site so as to achieve the desired 9 metre separation between dwellings, a concept that accords with some of the standards set out in clause 55.*

64. *The Guidelines for higher density residential development include the objective 2.6 which states: To ensure areas can develop with an equitable access to outlook and sunlight.*

65. *Design Suggestion 2.6.1 under this objective is: Consider the possible future development of adjoining sites and allow, as best as possible, or [sic] an equitable spread of development potential throughout the area.*

278. Although this decision considered the separation between towers on adjacent sites (or rather, potential towers), the methodology can be transferred. Generally, a 9m tower separation is favoured to achieve reasonable daylight, internal amenity levels and obviating the need for screening windows.

279. The proposal generally maintains a 9m separation between building elements and where it does not, the dwellings have generally been provided with alternative outlooks. The 9m encroachments are discussed further:

- (a) 14 storey Robert Street building and the 10 storey Waterloo Road building – minimum 7.2m setback. The original proposal detailed a 4.9m setback at this point. Council's external urban design advice raised issue with this and the on-site amenity implications. The plans have since been amended (Section 57A) and have increased the setback to 7.2m. This is acceptable because the adjacent dwellings in the Waterloo Road building enjoy outlooks to the west, north and east, with west facing balconies. Only 14 of the adjacent Robert Street dwellings would be affected by this 1.8m variation, without an alternative outlook. However this equates to only 4 per cent of the development and opposed to off-site amenity impacts, residents would have to choose whether or not they accept this configuration. Nevertheless, the dwellings would enjoy high levels of amenity by virtue of their location and presence of a neighbourhood square and access to the level 9 communal spaces in the 10 storey Waterloo Road building;
- (b) 14 storey Robert Street building and the 17 storey Robert Street building – minimum 6.6m setback. This is acceptable because the affected dwellings all enjoy dual outlook to Robert Street;
- (c) the three Waterloo Road podium segments – 6.5m setback. This variation is accepted because these dwellings also have an outlook to Waterloo Road.

280. Design suggestion 2.6.1 of the DSE Guidelines (referenced at paragraph 65 of the VCAT order above) requires consideration of equitable development, should sites near the site develop in the future. To ensure an appropriate level of daylight is afforded to the proposed dwellings, the side setbacks should also be considered.
- (a) West – minimum 4.3m setback (occurs at Robert Street podium). Although the minimum 4.5m is not achieved, a condition recommended in this report would see this setback increase to 4.75m;
 - (b) East – minimum 2.9m setback (occurs at Robert Street podium). The setback of the 17 storey tower than increases to 2.9m to 4.9m. Given the land to the east is zoned Business 3 and is portioned into 8 lots (presumably with an owners corporation managing any common property), it is unlikely that this site would be developed to the degree of this proposal. Further, if development did occur on the adjacent site, it would not be a residential proposal as dwellings are prohibited in the B3Z. In any event, should the site to the east be developed, the proposal would need to be considered accordingly. However, in this context, the proposal is considered to have reasonably ensured equitable development opportunities have not been hindered on adjoining sites.
281. Daylight to the communal space, or the neighbourhood square, must also be considered. Reviewing the Applicant's shadow diagrams, the central area would receive direct sunlight from approximately 12.00 pm to 3.00 pm (September equinox). Although this is not ideal, the built form has been derived from the heritage constraints of the site. Although this is not a relevant planning consideration, it does explain why there is no built form directly behind the historic brew tower. Nevertheless, the proposal incorporates a community gym, dining room, lounge and terrace in level 9 of the 10 storey Waterloo Road building. This offers residents a range of shaded and sunlit areas for their recreational needs.
282. Subject to the conditions contained in this report, the development would achieve a reasonable level of on-site amenity.

Storage

283. Standard B30 of ResCode states that each dwelling should have convenient access to at least 6m³ of externally accessible, secure storage space.
284. The development incorporates 200 storage cages with an average size of 4m³. Although a reduced storage size may be acceptable in this context (residents would not have a lawnmower, for example), all dwellings should be provided with externally accessible storage. This should be addressed by way of a permit condition, maintaining the minimum 4m³ storage cages and allowing over bonnet stores (some of which can be smaller).

Internal circulation/way finding

285. The development is clearly laid out and subject to a condition requiring way finding signage for visitors (e.g. 'brew tower apartments'), would also be clear for visitors.
286. The dwelling entries would be clearly visible and provide a sheltered, transitional space for residents (either within corridors or due to building overhangs).
287. Council's external urban design advice indicated that the corridor widths of 1.4m to 1.8m, and in some instances less than 1.2m were insufficient. A minimum 1.5m corridor width has been recommended to ensure that residents can move furniture in and out of dwellings and pass each other in the corridors. A review of the Section 57A amended drawings only revealed 1.4m to 1.8m wide corridors. A condition should therefore be imposed to require all internal corridors a minimum 1.5m wide. This is becoming a standard approach in major developments across the Municipality.

Off-site amenity

288. The DSE Guidelines, clause 55 (not applicable, rather a decision guideline) and clause 22.05 (Interface Uses Policy) will be used to guide this off-site amenity assessment.
289. The closest residential properties are along Wellington Street, a minimum 5m to the west of the proposed development. It has already been noted that some properties along Robert Street and Waterloo Road appear to be used as 'dwellings', however none have objected on this basis nor sought to establish existing use rights as part of this process. Nevertheless, even if these properties were able to demonstrate existing use rights (clause 63), they remain within the Business 3 Zone and could not expect the same level of amenity as dwellings in residential zones. This assessment will therefore focus on the residential uses to the west of the carriageway easement, fronting Wellington Street.
290. These Wellington Street dwellings present first and second floor habitable room windows and balconies towards the subject site.

Interface / visual bulk

291. The development proposes two, four storey building segments adjacent to these residential interfaces. The closest 'tower' form is the 14 storey Robert Street building, which is over 20.4m from 90 Wellington Street.
292. The four storey forms are 12.5m high and had ResCode been applicable (Standard B17 – Side and rear setbacks objective), would have necessitated a minimum 7.6m from the boundary (which is the western edge of the carriageway easement). The proposed setbacks are 5.3m to 6.4m.
293. The height and setback of these building segments adjacent to these residential properties are supported on the following:
- (a) these properties currently have an outlook to an approximately 32m high silos building and a dilapidated brew tower. On balance, this would provide an improved outlook for residents;
 - (b) the adjacent dwellings only have habitable room windows at level 1 (and 2 in the case of 82 Wellington Street). Taking the elevated nature of the dwellings into account, a 'level equivalent assessment' (i.e. taking 3m off the height of the proposed four storey forms) would see the height and setback meet ResCode requirements (12.5 – 3 = 9.5. A 9.5m high wall has a setback requirement of 4.6m).

Overlooking

294. Views within 9m and within a 45 degree arc would be provided externally between dwellings:
- (a) NW G.10 – NW G.13 and the 90 Wellington Street first floor habitable room windows; and
 - (b) HST G.09, HST 1.13, HST 2.12, HST 3.10 and the 82 Wellington Street first and second floor balconies and habitable room windows.
295. As per the land use assessment, only the level 1 to 3 windows and balconies would necessitate overlooking treatments as the ground floor street/carriageway easement activation are encouraged from a safety and passive surveillance perspective. This is also the case because the adjacent habitable room windows / balconies only commence at the first floor.
296. Should a permit issue the above overlooking matters can be addressed by way of a permit condition, requiring 1.7m high, fixed obscure glazing or 1.7m high, fixed maximum 25 per cent transparent screens (where views are provided within the 9m radius and 45 degree arc).

Daylight to windows

297. Although not applicable, Standard B19 (Daylight to existing windows objective) states that:
- (a) *Buildings opposite an existing habitable room window should provide for a light court to the existing window that has a minimum area of 3 square metres and minimum dimension of 1 metre clear to the sky. The calculation of the area may include land on the abutting lot.*
 - (b) *Walls or carports more than 3 metres in height opposite an existing habitable room window should be set back from the window at least 50 per cent of the height of the new wall if the wall is within a 55 degree arc from the centre of the existing window. The arc may be swung to within 35 degrees of the plane of the wall containing the existing window.*
 - (c) *Where the existing window is above ground floor level, the wall height is measured from the floor level of the room containing the window.*
298. The habitable room windows to the west would all continue to enjoy daylight in accordance with the minimum dimensions, being a minimum 1m by 3m.
299. Taking a standard floor level (3m), the development is 9.5m higher than the first floor of the dwellings to the west. Under Standard B19, this would necessitate a 4.75m minimum setback from these windows. Although the segment adjacent to 82 Wellington Street meets this Standard, the segment adjacent to 90 Wellington Street falls short by 450mm.
300. Level 2 of the segment adjacent to 90 Wellington Street is setback 4.3m from the habitable room windows, however the ground and first floors are setback 5m. The 700mm top level variation is due to an architectural projection. Should a permit issue, there is no reason the projection could not be reduced by 450mm (to 250mm) to ensure a reasonable level of daylight is afforded to the 90 Wellington Street dwellings. This should be imposed by way of a permit condition.
301. It is also noted that the carriageway easement also serves as a light and ventilation easement, further supporting a B19 compliant setback. It is noted that the condition recommended above would still see a 250mm projection into the carriageway/light/ventilation easement at level 2. As easements are not strictly planning matters, the Permit Holder's obligation to consider easements should be imposed in the form of a notation.

Noise

302. Noise associated with the café and deli has already been addressed in the 'land uses' section of this assessment.
303. Noise associated with the dwelling uses is not a relevant planning consideration as a dwelling use does not require a planning permit in the Mixed Use Zone. This is contrary to the advice received by Marshall Day Acoustics, which recommended a condition to restrict the hours of operation of the level 9, shared roof terrace. A condition cannot seek to control an 'as-of-right' use. Nevertheless, this terrace will be self-policed by the owner's corporation as the nearest dwellings are those within the development.
304. The Applicant's acoustic report also did not identify plant and equipment associated with either the commercial or residential land uses. Commercial plant must be demonstrated to meet SEPP N-1 (a condition has already been recommended to this effect). A condition should also ensure the proposed residential plant and equipment does not unreasonably impact the amenity of dwellings within the development and on nearby sites. The residential equipment should be shown to meet sleep disturbance criteria and any other relevant standard.

Light spill

305. A condition has already been recommended to ensure that external lighting is installed and baffled to ensure unreasonably glare is not directed to nearby residential properties.
306. Subject to the conditions contained in this report, the development would not unreasonably impact the amenity of the nearby dwellings.

Environmental sustainability

307. Clauses 15.02-1 and 21.07-1 of the Scheme aim to encourage energy efficient outcomes and the minimisation of greenhouse gas emissions.
308. The Applicant provided a SMP by Ark Resources, making the following commitments:
- (a) Green Star residential rating of 4 ('best practice');
 - (b) the provision of a rainwater tank of 75,000 L, providing water for toilets of 110 bedrooms (shown under basement level 1 on drawing A12.01, revision 3);
 - (c) a target-recycling rate of 80 per cent of construction and demolition waste will be achieved;
 - (d) a solar contribution of at least 40 per cent to the northern building's hot water demand will be met;
 - (e) provision of sliding shading louvers to northern, eastern and western facades (confirm full extent);
 - (f) common area corridors will have access to daylight and natural ventilation; and
 - (g) two car share spaces will be provided.
309. Council's ESD advisor has noted the following deficiencies:
- (a) *Provide a vertical facade articulation without the use of tinted glazing. Tinted glazing impacts on internal amenities through reduced access to daylight and reduced passive heat gains in winter. Tinted glazing therefore increases an apartment's energy demand for heating and lighting.*
310. Although tinted glazing restricts favourable daylight entering apartments during colder months, its use has been limited to elements of the podium and as single level bandings across the 14 and 17 storey buildings. The urban design benefits of articulating these forms are considered to outweigh any negative ESD implication.
311. Nevertheless, the development still achieves a 4 Green Star rating and residents would be able to enjoy many other positive ESD factors (ventilation, rainwater re-use, solar hot water and 2 car share facilities).
312. Additional recommendations include:
- (a) *Provide information about the provision of hot water for town houses and the central building.*
 - (b) *Clearly identify all sliding shading louvers on architectural drawings.*
313. The above items should be addressed by way of permit conditions.
314. Other considerations which will be discussed further later in the report include:
- (a) *Provide one bike rack per dwelling, encouraging all residents to use sustainable transport modes.*

315. For 350 dwellings, 88 resident bicycle parking spaces have been provided (plus 40 visitor bicycle parking spaces and 7 bicycle share spaces). Understanding that a rate of 1:1 bicycle parking space per dwelling would be a positive urban design feature, this matter is best addressed later in the ‘bicycle parking and facilities’ section of this assessment.
316. It is also worth noting that for a development containing 350 dwellings, only 1 bedroom would rely on borrowed light.
317. Subject to the conditions outlined in this report, the proposal would achieve a high level of energy efficiency and would reasonable minimize greenhouse gas emissions.

Car parking / traffic

Car parking requirements

318. Under clause 52.06-5, the following car parking rates are required:

| Use | Car parking requirement at clause 52.06-5 | Car parking proposed | Reduction sought |
|---|--|----------------------|------------------|
| Dwelling | 319. Studio, 1 and 2 bedroom dwellings = 347 = 347 spaces 320. 3 bedroom dwellings = 3 = 6 spaces 321. TOTAL = 353 | 323 | 30 |
| Dwelling visitor | 70 | 0 | 70 |
| Café (food and drinks premises - retail rate) | 5 | 1 | 4 |
| Deli (convenience shop - shop rate) | 2 | 1 | 1 |
| | TOTAL | 325 | 105 |

322. The car parking above does not take into account the 2 car share and 9 multi-use spaces, bringing the on-site car parking provision to 336. A total of 15 on-site motorcycle parking spaces are also provided.
323. The Applicant’s traffic report (GTA) included an on-street parking survey. The survey spanned a radius of approximately 200m and was undertaken on a Friday and Saturday (9.00 am to 11.00 pm) in April 2011. The survey took into account the loss of the 72 on-site car parking spaces and showed that parking demand is highest during business hours, with:
- (a) Friday parking availability being from 6 to approximately 66 spaces between 9.00 am and 4.00 pm;
 - (b) from 5.00 pm onwards on a Friday, on-street parking availability increased to 91 to 139 spaces;
 - (c) Saturday showed moderate on-street parking demand with 105 to 184 on-street spaces being available.
324. The Applicant’s traffic report also reiterated the available public transport modes in the area:
- (a) bus routes – within 500m of the site along Victoria Parade (including services to the Pines Shopping Centre, Warrandyte and Mitcham);
 - (b) tram – services extend along Smith Street and Victoria Parade;
 - (c) train – Collingwood Station is approximately 700m to the north-east of the site.
325. With the above in mind, an assessment against each category is offered below.

Dwelling

- 326. A reduction of 30 resident parking spaces is sought.
- 327. The Applicant's traffic report outlined the 2006 ABS data for car ownership in Collingwood. The report is based on the original development (349 dwellings), however the ABS rates can be easily extrapolated.

| | Number of dwellings | ABS 2006 parking rate (Collingwood) | ABS 2006 parking requirement |
|------------------|---------------------|-------------------------------------|------------------------------|
| 1 bedroom/studio | 146 | 0.68 | 99 |
| 2 bedroom | 201 | 1.04 | 209 |
| 3 bedroom | 3 | 1.14 | 3 |

- 328. The ABS data therefore indicates a demand for 311 on-site car parking spaces. With 323 being provided, the development exceeds this rate. The proposal is therefore likely to have adequate parking based on ABS statistics.

Dwelling visitor

- 329. A reduction of 70 visitor parking spaces is sought. None are provided on-site.
- 330. The Applicant's traffic report referenced the 'typical peak residential visitor car parking demand' as 0.12 spaces per dwelling. This equates to a peak visitor parking demand for 42 spaces. The report also stated that demand is typically highest in the evenings and weekends.
- 331. The times when visitor parking would be in demand complements the on-street parking surveys, which found medium demand for parking on weekdays from 5pm onwards and on weekends. Reviewing the Applicant's on-street parking surveys (again, which took into account the loss of 72 spaces from the subject site), the anticipated 42 visitor parking spaces could be accommodated in the surrounding streets (within a 200m radius).
- 332. The site is also well serviced by public transport, walking and cycling options, giving visitors viable public transport options in accordance with clauses 18 and 21.06 of the Scheme.
- 333. Nevertheless, it is noted that the car park includes 9 multi-use spaces which the owner's corporation could allocate/use as they see fit.

Café

- 334. A reduction of 4 car parking spaces is sought in association with the cafe. One space would be provided on-site. It is also noted that the café would cater for a maximum of 80 patrons and would be serviced by a maximum of 5 staff, although the car parking rate is derived from the floor area.
- 335. Given the size of the café, it is likely that patrons would already be in the area. This view is shared by the Applicant's traffic consultant and Council's Engineering Services Unit.
- 336. Nevertheless, the on-street parking survey found a minimum of 6 on-street spaces within survey times, indicating that these 4 spaces could be provided on-site in the event patrons drove the area.
- 337. Alternately, it is also a viable option for patrons and staff to walk, cycle or use public transport to access the site.

Deli

338. A reduction of 1 car parking space is sought in association with the deli. One space would be provided on-site.
339. Being a convenience shop, it is most likely that customers would be from the development itself and nearby businesses. Nevertheless, it is also a viable option for patrons and staff to walk, cycle or use public transport to access the site.

Layout and manoeuvrability

340. Internally the car park has generally been found to be functional and accessible. However, the following should be addressed by way of permit conditions:
- (a) all transitions from a 1:4 (25 per cent) grade increased to 2.5m;
 - (b) the access way to the south basement levels increased from 6m to 6.1m wide;
 - (c) the outer radial of the curve to the ramp at the north-east corner of the main basement be provided with an outside kerb/clearance of 500mm as required by the Australian/New Zealand Standard AS/NZS 2890.1:2004 Figure 2.9;
 - (d) the internal access ramp for the south car park modified in order to accommodate the turning requirements of a B99 design vehicle in a single manoeuvre, in accordance with AS2890.1-2004;
 - (e) the ramp grades along the access ramp to the south car park modified to accord with the requirements for grade changes under clause 52.06-8 (Design Standard 3) of the Yarra Planning Scheme;
 - (f) at least 25 per cent of all car stacker spaces provided with a height clearance of at least 1.8m, in accordance with clause 52.06-8 (Design Standard 4) of the Yarra Planning Scheme; and
 - (g) small car spaces are to be labelled as such on the plans and signed and line marked on-site. The transport management plan should also address the 31 small car parking spaces and their allocation.
341. Council's Strategic Transport and Engineering Services (Assets) Unit noted that there are no accessible disabled access parking spaces. One space is marked in basement level 1 as a disabled space, however it is to be allocated to a dwelling.
342. To address this, a condition should require one of the 9 'multi use' spaces to be a disabled access space. It appears that 'multi 9' in basement level 2 may comply with the disabled access requirements, however this should still be confirmed by way of a condition.
343. Council's Engineering Services (Assets) Unit also questioned how access to the service room could be provided from the level 1 basement entrance ramp. This should also be addressed by way of a permit condition.

Traffic

344. Traffic impact is a key objector concern given the narrow width of Waterloo Road (4.11m) and Robert Street (4.28m) and the 'blind corners' created by buildings built hard edge to boundaries. Waterloo Road and Robert Street are also both one way (east bound) and link to Rokeby Street (also one way, south bound).
345. The Applicant's traffic report has used empirical evidence to indicate that the development would generate 105 vehicle movements in a peak hour (residential) and that the café and deli would generate one movement each in the AM and PM.

346. The traffic report also included a SIDRA analysis, finding that the development would have a minor impact on the surrounding intersections, with an average delay of approximately 1 additional second at any intersection.
347. In response to the Objector and Council's safety concerns, the Applicant has undertaken a Road Safety Audit (again prepared by GTA consultants). In particular, residents of the Wellington Street dwellings to the west were concerned about increased traffic volumes and businesses along Robert Street, Waterloo Road and Rokeby Street were concerned about the impact of additional traffic on their business operations.
348. The Road Safety Audit reviewed the existing road networks, traffic volumes, accident statistics and sustainable transport infrastructure and made a number of recommendations in relation to street signs, road markings and parking restrictions. Many of the recommendations affect private land not part of this application (e.g. 'trimming a tree' at 82 Wellington Street) or are matters dealt with by Council's Engineering Services Unit (e.g. street signs and line marking). As a result, many of these recommendations cannot be imposed by way of permit conditions. Nevertheless, Council's Engineering Services Unit have been involved in this application and have reviewed the GTA recommendations.
349. Another recommendation was for the installation of a speed hump on Robert Street, preferably half way between Wellington Street and Rokeby Street. Although road works can be required by way of permit conditions (e.g. developer contributions), Council's Infrastructure and Special Projects Branch noted that it is not in a position to consider or give final approval for the road humps in Robert Street. The developer is required to obtain the necessary traffic data and a majority of support from the local community. This is a process outside the planning process and cannot be considered as part of this assessment. However, a condition on any permit granted can enable the cost of such infrastructure to be borne by the permit holder.
350. One recommendation that can be considered in the planning process however, is the installation of 'give way' signs be installed facing motorists on both legs of the north-south carriageway easement at the intersection with Waterloo Road. This would ensure that drivers traversing the carriageway easement do not assume 'right of way' and should be imposed by way of a permit condition.
351. The remaining consideration in relation to 'traffic' is whether there are any pedestrian/vehicular conflicts along Waterloo Road or Robert Street.

Waterloo Road

352. The Applicant has not proposed any changes to Waterloo Road. The development would include pedestrian access (western end) and one vehicular access to the site (eastern end). Given the site presents a 'heritage wall' to Waterloo Road, there is no opportunity to setback the development to provide a footpath on the northern side.
353. Council's external traffic advice recommended that, although not be expected to generate high pedestrian volumes, Waterloo Road should be signed as a shared zone with appropriate traffic management devices installed. A condition should require the installation of shared zone signage, with appropriate traffic management devices, ensuring the properties on the south side of Waterloo Road can maintain vehicular access. This could entail signage and line marking.

Robert Street

354. The Applicant has proposed a shared zone in Robert Street. Sketch plans provided by Tract consultants (dated 4 July 2012) show a raised footpath and a row of bollards and lights between the carriageway and footpath. This design approach is not considered to be a true shared zone. Additional objections were received during the re-advertising period (Section

57A plans) because of the proposed narrowing of the Robert Street carriageway to accommodate this 'shared zone' as shown in the Tract sketch plans. If Robert Street was narrower, trucks may not be able to access the loading bays on the north side of Robert Street. As these businesses are in the B3Z and are trying to provide on-site loading opportunities, access must be maintained.

355. Little Buckingham Street in Richmond has recently been converted into a shared zone, its construction formed part of a planning permit development for a multi unit development which flanks the shared on either side.

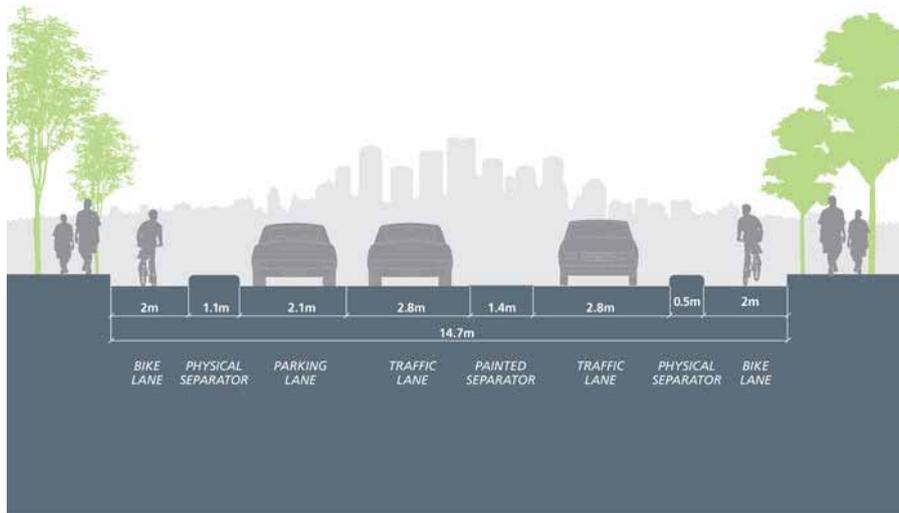


Figure 37: Little Buckingham, Richmond Shared Zone

356. It is noted that a shared zone does not specifically delineate between vehicular and pedestrian areas.
357. Objectors/submitters have been commenting on the heritage value of the bluestone laneways. Although neither Robert Street nor Waterloo Road are affected by a Heritage Overlay, they should be retained as bluestone as they positively contribute to the public realm (Waterloo Road was recently re-laid too).
358. A shared zone could therefore be achieved in Robert Street if the pedestrian path were lowered to the bluestone level, with no bollards or trees between. The pedestrian path should be constructed in asphalt to complement the bluestone, but not in bluestone to assist access for people with limited mobility. This will require detailed drawings and a further review by Council's Engineering Services Unit (considering drainage, Australian Standards, Council requirements, etc.) and can be addressed by way of a permit condition.

Other matters

359. This assessment has not yet addressed the proposed 'Copenhagen style' bicycle lanes on Wellington Street.



- 360. Council is proposing these lanes to create greater separation between cyclists and motorists and is expected to result in a loss of up to 117 parking spaces on Wellington Street.
- 361. A report on this issue will be presented to the 18 September Council meeting.

Bicycle parking and facilities

- 362. The application does not require a bicycle parking reduction under clause 52.34 (135 spaces are provided where the statutory rate is 105). The Council’s Strategic Transport Unit and ESD Advisor have requested additional parking be provided. However, the car parking assessment has outlined that the residential car parking reduction can be supported on the basis of current on-street car parking, cycling and public transport alternatives and the visitor parking reduction can be supported by public transport, walking and cycling options. Although walking and cycling were also referenced in the justification of the deli (1), café (4) and dwelling visitor (70) reductions, the development includes 40 on-site bicycle parking spaces in the neighbourhood square. Additional bicycle parking is therefore not considered necessary in this instance.
- 363. Council’s Strategic Transport Unit raised issue with the location of the 88 resident bicycle parking spaces (at ground floor under the 17 storey building) and the 40 visitor bicycle parking spaces (in the neighbourhood square). However, there appears to have been some confusion, because Council’s Strategic Transport Unit noted that cyclists would have to lift bicycles up stairs to access these spaces. This is not the case. Cyclists have two options for cycling to these spaces; from Robert Street across the carriageway easement, or from Wellington Street across the easement of ‘way’ within 80 Wellington Street (to which the subject site enjoys favour).
- 364. The application seeks the waiver of the one change room requirement of clause 52.34 of the Scheme. Given the bicycle parking requirement under clause 52.34 is associated with the dwellings, a change room is not considered necessary.
- 365. The application does not confirm whether the design of the bicycle parking spaces meets clause 52.34-4 and whether bicycle signage is provided in accordance with clause 52.34-5. Conditions can be imposed to this effect, however some common bicycle parking systems do not meet the dimensions specified in the Scheme (Ned Kelly systems, for example). The condition should be tailored to require dimensions, with reasonable flexibility.

Loading facilities

- 366. The deli and café do not have a designated on-site loading bay. A waiver under clause 52.07 is therefore required.

367. A permit may be granted under clause 52.07 to reduce or waive these requirements if either:
- (a) the land area is insufficient; or
 - (b) adequate provision is made for loading and unloading vehicles to the satisfaction of the Responsible Authority.
368. The Applicant's Traffic Report (GTA consultants) has stated that a waiver is appropriate because:
- (a) loading events would be minimal;
 - (b) a limited demand loading bay would be an inappropriate constraint on the development; and
 - (c) on-street parking are available to accommodate vehicle demands.
369. The external traffic advice obtained by Council (Traffix), also supported the waiver of a loading bay in this instance.
370. It is also noted that the carriageway easement could accommodate a small van for loading purposes, particularly given it adjoins the café and deli at their western edge. However, this easement is also used for vehicular access by the two adjacent Wellington Street buildings. To mitigate any off-site amenity impacts, should the carriageway easement be used for loading/unloading, a condition should require deliveries to occur between 8.00am and 4.00pm.
371. Subject to a condition to this effect, the presence of no on-site loading bay would not unreasonably impact the amenity of the area.
372. Some objectors have raised issue with residential loading i.e. moving vans. While the planning scheme does not require a designated loading area for this, a condition on any permit should require the transport plan to address where residential loading could occur.

Waste management

373. The Applicant provided a waste management plan (prepared by Leigh Design).
374. Bin stores are located at the ground floor (within each commercial tenancy) and in basement 2. A bin holding room is located adjacent to the Robert Street vehicular entrance ramp. Waste collection would occur as follows:
- (a) residential waste collection – on-site waste collection by a private contractor (small vehicle) within the:
 - (i) Robert Street driveway, collecting rubbish from the bin holding room; and
 - (ii) Waterloo Road driveway, collecting rubbish from the bin holding room.
 - (b) deli and café waste collection – on-site waste collection in the carriageway easement.
375. Council's Services Contracts Unit have found the submitted Waste Management Plan to be satisfactory, however they note that this is due to '*severe restrictions, such as heritage restrictions, town planning requirements, etc. the WMP is far from best practice from a waste management perspective. However, it can be also said that it is a reasonable outcome, all things considered*'.

Commercial waste

376. Commercial bin stores are provided within each commercial tenancy and would not unreasonably impact the amenity of the area.

377. The collection of commercial waste from the carriageway easement is considered acceptable and will only block the (private) lane. This arrangement ensures that the public roads (Waterloo Road and Robert Street) would remain unobstructed.

Residential waste

378. Residential bin stores are located in basement level 2 and can be easily accessed via lifts or stairs. Prior to collection, the owner's corporation would be responsible for moving the bins to the ground floor bin holding rooms.
379. This arrangement has been selected because Robert Street and Waterloo Road are too narrow for 'typical' waste management vehicles to park and remove waste from the kerb side.
380. The collection of residential waste from the Robert Street car park entry ramp and Waterloo Road driveway requires careful consideration however, as it would block access into the car park for 3 and 7 minutes at a time. However, vehicles would still be able to exit the driveways. Because the owner's corporation would manage rubbish collection, they could manage this internally and inform residents as to when the ramp would be obstructed from incoming vehicles.
381. In many ways, the Applicant has elected to deal with this site constraint internally as opposed to pushing the issue onto the street. Appropriate management is considered to be the best way to deal with this matter. This approach has been supported in the external traffic advice obtained by Council (Traffix). Should a permit issue, a condition should require a transport management plan, with a requirement to inform all residents as to when the driveways would be obstructed due to waste collection.
382. The waste management plan states that for improved safety, waste collections and bin transfers would be carried-out during off-peak traffic periods. This should also form a permit condition, with 'off peak' being defined as within the hours of 10.00 am to 2.00 pm in the Applicant's planning submission (letter dated 3 July 2012).
383. Subject to the conditions contained in this report, the waste management methods would not unreasonably impact the amenity of the area and would be efficient within the context and constraints of the site.

Cancellation of section 173 agreement

384. The Section 173 Agreement was registered on 3 August 1999 and essentially obliged the owner to develop the land in accordance with the Heritage Victoria Permit P4379 and undertake conservation works. The agreement also outlined a financial open space contribution applies to the site prior to any further subdivision, or if the subject site (lot S2) is not further subdivided, the financial contribution was required to be paid within two years of the statement of compliance.
385. As Heritage Victoria are a signatory to the agreement, comment from them was sought and their response is outlined below:
- (a) *Whilst the S173 does refer to a now lapsed permit for a previous development proposal which did not proceed, it also binds the owner to ongoing obligations including a prohibition on undertaking works which will in the opinion of Council or Heritage Victoria adversely affect the cultural heritage significance of the land, requires the owner to keep up the maintenance of the heritage buildings and not apply for any subdivision of the place until the conservation works specified in the Conservation Management Plan are completed.*
 - (b) *Heritage Victoria believes these are matters worth retaining and suggests that rather than remove the S173 it could be varied to retain these matters but updated to reflect*

the current permit status.

386. While Heritage Victoria did not impose a Section 173 Agreement, it is agreed that a new or modified agreement should be imposed on the site to ensure that the works are undertaken in accordance with the recent Heritage Victoria permit (including conservation works) and the financial open space contribution is paid on Lot S2 (the subject site).
387. However, given a number of matters are not resolved at this stage, including the specific requirements Heritage Victoria would be seeking, it is not considered appropriate to agree to remove the agreement at this time. As this process is outside the standard planning permit application process, the Applicant could contact Council and Heritage Victoria at a later date to resolve this matter.
388. It is also not considered that this application necessarily contradicts the intent of the existing Section 173 Agreement, although permit references and dates are now outdated.

Objector concerns

389. Most of the objector concerns have been addressed throughout this assessment.

neighbourhood character, overdevelopment and heritage (including views to the heritage tower)

390. Whilst neighbourhood character and overdevelopment have been addressed at paragraphs 173 -255 heritage is not a relevant planning consideration in this instance (see paragraph 57).

height

391. This objector concern has been addressed at paragraphs 193 - 233.

urban design

392. This objector concern has been addressed at paragraphs 179 – 261.

zoning does not reflect character/history of the area

393. This objector concern has been addressed at paragraphs 54 - 62.

off-site amenity (overlooking, reduced daylight, overshadowing)

394. This objector concern has been addressed at paragraphs 292 - 310.

on-site amenity (insufficient open space)

395. This objector concern has been addressed at paragraphs 262 - 291.

interface with B3Z and commercial/industrial properties

396. This objector concern has been addressed at paragraphs 292 - 310.

safety (traffic and pedestrian along Waterloo Road and Robert Street, moss along laneway) and anti-social behaviour

397. This objector concern has been addressed at paragraphs 237 – 245 and 348 - 362.

increased traffic / safety

398. This objector concern has been addressed at paragraphs 237 – 245 and 348 - 362.
narrowing of Robert Street will restrict access into commercial properties to the north
399. This objector concern has been addressed at paragraphs 358 – 362.
impact on the environment
400. This objector concern has been addressed at paragraphs 311 – 321.
insufficient car parking (including loss of parking on-site and loss of parking should the Wellington Street bicycle lane be approved)
401. This objector concern has been addressed at paragraphs 322 – 365.
construction phase (noise, impact on access, pressure on on-street car parking and potential damage to surrounding properties)
402. This objector concern has been addressed by condition 52 of the recommendation.
waste removal
403. This objector concern has been addressed at paragraphs 377 – 387.
no on-site loading bay
404. This objector concern has been addressed at paragraphs 370 - 376.
insufficient infrastructure (public transport, car parking, childcare and education facilities)
405. This context of the area has been addressed at paragraphs 34 - 51.
application inaccuracies
406. This objector did not outline what was inaccurate with the application, therefore this matter has not been assessed.
property devaluation
407. Property devaluation is not a relevant planning consideration.

Conclusion

408. With the changes outlined in the recommendation section of this report, it is considered that the redevelopment of the site would provide an appropriate built form outcome.
409. The proposed development incorporates a series of towers and open space areas that take into account the unique interfaces and long range views. On balance, the proposal is an acceptable planning outcome and is considered to meet the relevant elements of the Yarra Planning Scheme.
410. It is also recommended that pursuant to Section 177 of the *Planning and Environment Act 1987*, Council resolve not to consent to the cancellation of the existing Section 173 Agreement (Instrument No. U463301Y) registered on the Certificate of Title for 677 and 679 Victoria Street, Abbotsford.

RECOMMENDATION

It is recommended to Council that having considered all objections and relevant planning documents, the Council resolves to issue a Notice of Decision to Grant a Permit (PLN11/0750) for the development of the land for the construction of buildings and works; use of part of the land as a food and drinks premises (café) and a convenience shop (deli); reduction in the car parking requirements associated with dwellings, a food and drinks premises (café) and a convenience shop (deli); waiver of the loading bay requirement; waiver of the bicycle change room requirement at 1-21 Robert Street, Collingwood VIC 3066 subject to the following conditions:

1. Before the development starts, amended plans to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. When approved, the plans will be endorsed and will then form part of the permit. The plans must be drawn to scale with dimensions and three copies must be provided. The plans must be generally in accordance with the advertised plans (received by Council on 4 July 2012) but modified to show:

Built form

- (a) level 2 of the Robert Street building (dwellings NW 2.20 to NW 2.23) setback an additional 450mm from the west title boundary;
- (b) where habitable room windows and private open space areas of the following could provide internal overlooking opportunities (within a 9m radius and 45 degree arc), they are to be treated with 1.7m high, fixed obscure glazing or fixed, minimum 1.7m high, maximum 25 per cent transparent screens or the like:
 - (i) central row of south facing dwellings within the 14 storey Robert Street building to the northernmost unit in the 10 storey building;
 - (ii) easternmost dwellings in the cellar building and the westernmost dwellings in the stables building;
 - (iii) balconies at the first floor of the 17 storey building;
 - (iv) south apartments (closest exposed apartment) and the 10 storey building (closest apartment in the south-west corner);
 - (v) brew tower apartments and the adjacent Robert street building to the north (level 2 and above);
 - (vi) brew tower apartments and the south apartments (level 2 and above); and
 - (vii) 14 and 17 storey Robert Street buildings (levels 3 and above).
- (c) where habitable room windows and private open space areas of the following could provide external overlooking opportunities (within a 9m radius and 45 degree arc), they are to be treated with 1.7m high, fixed obscure glazing or fixed, minimum 1.7m high, maximum 25 per cent transparent screens:
 - (i) NW G.10 – NW G.13;
 - (ii) HST G.09;
 - (iii) HST 1.13;
 - (iv) HST 2.12; and
 - (v) HST 3.10.
- (d) details of all screening types proposed;
- (e) identify all sliding shading louvers;
- (f) the fences adjacent to Robert Street be a maximum 1.5m high and a minimum 75 per cent transparent;
- (g) all elevations (internal and external) of the development, in colour;
- (h) an updated schedule of all external materials and finishes (including materials samples,

colours and coloured elevations/perspectives). The schedule must show the materials, colour, finish and application methods (where relevant) of all external walls, roof, fascias, window frames, glazing types, cladding, doors and fences.

- (i) a set of highly detailed 'typical' elevations to demonstrate 'craftsmanship' in the quality and application of façade finishes and details. Including illustrating the design logic supporting the selection of the various components;
- (j) the location of integrated art work but not limited to location(s) within the neighbourhood square;

General

- (k) all internal corridors in the apartment buildings to have a minimum width of 1.5m with indentations to dwelling entrances;
- (l) plans to label the 'food and drinks premises' as a 'food and drinks premises (café)' and 'deli' as a 'convenience shop (deli)';
- (m) provision of an externally accessible storage cage (minimum 4m³) or above bonnet store for each dwelling;
- (n) mail box locations;

Traffic, car parking and bicycle parking

- (o) all transitions grades from a 1:4 (25 per cent) grade increased to 2.5m;
- (p) the access way to the south basement levels increased from 6m to 6.1m wide;
- (q) the outer radial of the curve to the ramp at the north-east corner of the main basement be provided with an outside kerb/clearance of 500mm in accordance with the Australian/New Zealand Standard AS/NZS 2890.1:2004 Figure 2.9;
- (r) the internal access ramp for the south car park modified in order to accommodate the turning requirements of a B99 design vehicle in a single manoeuvre, in accordance with AS2890.1-2004;
- (s) the ramp grades along the access ramp to the south car park modified to accord with the requirements for grade changes under clause 52.06-8 (Design Standard 3) of the Yarra Planning Scheme;
- (t) at least 25 per cent of all car stacker spaces provided with a height clearance of at least 1.8m, in accordance with clause 52.06-8 (Design Standard 4) of the Yarra Planning Scheme;
- (u) all car stackers (except those designated as 'small car spaces' to accommodate 85th percentile vehicles);
- (v) small car spaces are to be labelled on the plans;
- (w) shared zone signage along Robert Street and Waterloo Road, including details and location of signage;
- (x) at least 1 of the 'multi use' car parking spaces to be shown as a disabled access space and to be designed in accordance with the Australian/New Zealand Standard AS/NZS 2890.6:2006;
- (y) detailed drawings showing how the floor level of the level 1 basement services room can be accessed from the ramp;
- (z) clear signage to show entry and exit points to all car parking areas;
- (aa) bicycle parking dimensions in accordance with clause 52.34-4 of the Yarra Planning Scheme, or otherwise to the satisfaction of the Responsible Authority;
- (bb) bicycle signage in accordance with clause 52.34-5 of the Yarra Planning Scheme;
- (cc) notations confirming Robert Street and Waterloo Road will become 'shared zones';

- (dd) notations confirming 'give way' signs will be installed facing motorists on both legs of the north-south carriageway easement at the intersection with Waterloo Road;

Landscape plan

- (ee) updated landscape plans generally in accordance with the Landscape Concept Plans (August and October 2011) prepared by Tract Consultants Pty Ltd to the satisfaction of the Responsible Authority must be prepared by a suitably qualified profession. The plan must be drawn to scale with dimensions and three copies must be provided. The plan must show:
- (i) re-design of the neighbourhood square to include additional soft landscaping;
 - (ii) deletion of landscaping proposed within the 'way' easement located outside the title boundaries;
 - (iii) details of landscaping to level 9 (central building) roof terrace;
 - (iv) detailed planting plan with plant schedule (common and botanical plant names, density, quantities, size at planting and maturity height);
 - (v) cross sections demonstrating how landscaping will function above the basement;
 - (vi) the location of all areas to be covered by lawn or other surface materials;
 - (vii) the location of any permanent structures, furniture, public seating or other facilities;
 - (viii) the location of all underground and overhead services;
 - (ix) all public area footpaths, steps, ramps and crossings to comply with DDA – Tactile indicators, nosing's and handrails where necessary;
 - (x) grades of all ramps shown on plan;
 - (xi) grades of all footpaths shown on plan;
 - (xii) details of street furniture, including bench seats and street tree planters;
 - (xiii) existing and proposed levels including top wall and fence heights (where relevant);
 - (xiv) ongoing maintenance and management details of all publically accessible spaces; and
 - (xv) a schedule of 'way finding' signage;

Lighting plan

- (ff) a detailed lighting plan, confirming:
- (i) the car parking spaces, storage, rubbish bin, recycling bin, pedestrian walkways, stairwells, lift, dwelling entrances and entry foyer pedestrian and vehicular entries to the site from Robert Street, Waterloo Road and the carriageway easement will be illuminated (including the neighbourhood square, deli and café);
 - (ii) street lighting for pedestrian areas will be designed in accordance to the requirements of AS1158.3.1;
 - (iii) standard street lighting luminaries are as approved by Citi Power;
 - (iv) street lighting luminaire are from the Energy Efficient list (Council's Engineering Services Unit);
 - (v) lighting must be located, directed, shielded and of limited intensity so that no nuisance or loss of amenity is caused to any person within and beyond the site, to the satisfaction of the Responsible Authority; and
 - (vi) power supply to the proposed lighting poles will be underground.

Acoustic

- (gg) all works recommended in the report of the professional acoustic engineer referred to in condition 7;

ESD

(hh) all works recommended in the ESD report referred to in condition 11;

Waste management

(ii) all works recommended in the waste management plan referred to in condition 13;

Endorsed Plans

2. All development and use must accord with the endorsed plans. Any alterations must be approved in writing by the Responsible Authority.
3. Floor levels shown on the endorsed plans must not be altered or modified. Any alterations must be approved in writing by the Responsible Authority.

Ongoing involvement of the architect

4. As part of the ongoing consultant team Hayball Pty Ltd must be retained to complete the design and provide architectural oversight of the delivery of the detailed design as shown in the endorsed plans during the construction unless with the prior written approval of the Responsible Authority.

Road works

5. Within 6 months of the development commencing, the design of the Shared Zones in Robert Street (between Wellington and Rokeby Streets) and Waterloo Road (between Wellington and Rokeby Streets) must be prepared by an independent and suitably qualified professional to the satisfaction of the Responsible Authority and in consultation with the Responsible Authority. The design must be submitted to and approved by the Responsible Authority. The design of the Shared Zones must confirm:
 - (a) the roads (carriageways) will remain in bluestone;
 - (b) the full reconstruction of Robert Street between Wellington and Rokeby Streets to include the relaying of the bluestone;
 - (b) any extension to the south side of Robert Street ('footpath') is to be constructed in asphalt or a material to complement the bluestone;
 - (c) vehicular access to be maintained to the properties on the north side of Robert Street and south side of Waterloo Road;
 - (d) location and details of 'shared zone' signage;
 - (e) traffic calming devices, or otherwise to the satisfaction of the Responsible Authority;
 - (f) the re-setting of stones within the existing bluestone threshold treatment, demonstrating the threshold treatment is robust to accommodate vehicle loadings and repetitive movements, ensuring that stones are not dislodged;
 - (g) removal of parking restrictions and signs (as necessary); and
 - (h) and any other works required including traffic calming measures as outlined in the Road Safety Review prepared by GTA Consultants 24 July 2012.

Details must include, but are not to be limited to, all dimensions, existing reduced levels, proposed kerb levels, surface treatments, line marking, kerb, channel and footpath works, positions and details of all signs (existing and proposed), accurate locations of any stormwater pits and underground services.

6. Before any use commences on site and prior to the issue of a certificate of occupancy, all works associated with provision of Shared Zones in Robert Street (between Wellington and Rokeby Streets) and Waterloo Road (between Wellington and Rokeby Streets) as set out in condition 5 must be undertaken and completed at the cost of the permit holder to the

satisfaction of the Responsible Authority. Unless with the prior written consent of the Responsible Authority.

Acoustic report

7. Before the plans are endorsed, an updated acoustic report generally in accordance with the report dated November 2011 must be submitted to and approved by the Responsible Authority. Once approved, the acoustic report will be endorsed and will then form part of the permit. The report must be prepared by a suitably qualified acoustic engineer and include an assessment of how the requirements of State Environment Protection Policy N-1, N-2 and relevant Australian Standards will be met and must prescribe the form of acoustic treatment to:
 - (a) protect all dwelling occupants and nearby occupants from noise generated from the mechanical plant equipment and ventilation mechanisms installed or constructed as part of the development (including the lift, residential air conditioner units and commercial plant and equipment);
 - (b) protect all dwelling occupants and nearby occupants from music noise generated by the café and deli;
 - (c) protect all dwelling occupants from music noise, vibrations and 'weight dropping' within the level 9 gymnasium;
 - (d) protect all dwellings within the development from noise generated from use of the bin chutes; and
 - (e) protect all dwelling occupants within the development from noise generated by the Wellington Street Workshop and Robert Street seafood processing plant and surrounding commercial activities
8. If the development has not started within 12 months of the date of the approved acoustic report referred to in condition 7, a further acoustic report prepared by a suitably qualified acoustic consultant to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. The further acoustic report must demonstrate that the State Environment Protection Policy N-1, N-2 amenity levels as assessed in the acoustic report submitted and approved under condition 6 will still be achieved.

In the event that non-compliance with the State Environment Protection Policy N-1, N-2 levels is demonstrated, the further report must recommend additional measures necessary to achieve compliance to the satisfaction of the Responsible Authority.

9. On the completion of any works required by the endorsed Acoustic report (condition 7 or 8) above and before the residential use commences, an updated acoustic report prepared by a suitably qualified acoustic consultant to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority demonstrating that the required level of noise attenuation has been achieved. The report must:
 - (a) confirm compliance with condition 7 or 8 of the permit; and
 - (b) provide measurement data taken from inside the dwellings of the development, demonstrating compliance with State Environment Protection Policy N-1 and N-2.
10. The recommendations and any works contained in the approved acoustic reports pursuant to conditions 6, 7 and 8 (as applicable) must be implemented and completed and where they are recommendations of an ongoing nature must be maintained all to the satisfaction of the Responsible Authority.

ESD report

11. Before the plans are endorsed, an amended Environmentally Sustainable Report to the

satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. When approved, the report will then form part of this permit. The Report must be generally in accordance with the report prepared by Ark Resources (dated 22 August 2011) but modified to include:

- (a) information about the provision of hot water for town houses and the central building; and
- (b) details regarding sliding shading louvers.

Integrated Art Plan

- 12. Within 6 months of the commencement of the development, details of the public artwork to be provided in the locations shown on the endorsed plans must be submitted to the Responsible Authority for approval. The cost of the public artwork is to be borne by the owner or occupier of the land. Except with the consent of the Responsible Authority, the artwork must be installed prior to occupation of any part of the development and then maintained to the satisfaction of the Responsible Authority.

Waste management plan

- 13. Before the plans are endorsed, a Waste Management Plan to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. When approved, the report will then form part of this permit. The Report must be generally in accordance with the report prepared by Leigh (dated 2 July 2012).
- 14. Residential waste collections and bin transfers must be carried-out between the hours of 10.00 am and 2.00 pm, unless with the prior written consent of the Responsible Authority.
- 15. The collection of all waste must be in accordance with the approved Waste Management Plan. Rubbish, including bottles and packaging material, must at all times be stored within the building and screened from external view and be managed to the satisfaction of the Responsible Authority in accordance with the approved Waste Management Plan.
- 16. No emptying of bottles into garbage bins (associated with the café and deli uses) is permitted after 10.00 pm on any night, or before 7.00 am on any day.
- 17. All waste must be collected by private collection service to the satisfaction of the Responsible Authority.

Car and bicycle share

- 18. Before the development starts, the owner must enter into an agreement with the Responsible Authority under section 173 of the Planning and Environment Act 1987 and apply to the Registrar of Titles to have the agreement registered on each of the titles to the land under Section 181 of the Act. Under the agreement, the owner must covenant with the Responsible Authority that provision will be made for a minimum 2 car share vehicles and 7 bicycle share spaces on-site by way of arrangement with car/bicycle share operator(s) and/or the future Owners Corporation for a minimum period of 10 years.

The agreement will reflect that any costs associated with ensuring ongoing availability of at least 2 car share vehicles and 7 bicycle share spaces will be borne by the owner. All costs of preparation, execution and registration of the agreement must be borne by the owner of the land, or the future Owners Corporation, including those costs incurred by the Responsible Authority.

Transport Management Plan

- 19. Prior to the commencement of the development, a Transport Management Plan prepared by

a suitably qualified professional to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. When approved, the Transport Management Plan will be endorsed and will then form part of the permit. The Transport Management Plan must address the following:

General

- (a) an internal signage/line marking plan:
 - (i) distinguishing resident, retail and multi-use parking spaces, with directional arrows and signage;
 - (ii) detailing information signs indicating the location of disabled bays, small parking bays, bicycle parking and exits;
 - (iii) detailing line marking and signage to the entry and exit lanes to all car parking areas and exits (including at the Robert Street frontage);
 - (iv) confirming 'give way' signs will be installed facing motorists on both legs of the north-south carriageway easement at the intersection with Waterloo Road.
- (b) security arrangements;

Car Parking

- (c) a minimum 323 residential car parking spaces;
- (d) residential car parking must be allocated such that all two and three bedroom dwellings are provided with at least one on-site car parking space;
- (e) other car parking must be allocated as follows:
 - (i) deli/café – 2
 - (ii) multi-use – 9 (inclusive of one disabled bay)
- (f) a minimum of 13 motorcycle parking bays;
- (g) a minimum of 2 car spaces for a car share programme;
- (h) management details of the car share programme provided;
- (i) allocation, signage and management of the 31 small car spaces shown within the car parking areas;
- (j) how all residents would be informed of the waste collection times (and associated restricted access to the car park);

Bicycle spaces

- (k) a minimum of 135 bicycle parking spaces as follows:
 - (i) dwelling – 88;
 - (ii) visitor – 40; and
 - (iii) bicycle share – 7.

Loading and Deliveries

- (l) details regarding management of loading and unloading of goods in compliance with any other condition(s) of this permit.
- (m) management arrangements for resident loading (i.e. moving vans).

20. Except with the further consent of the Responsible Authority, the management of the car park must, at all times, comply with the approved Transport Management Plan.

Car parking, crossovers and footpaths

21. The area set aside for the parking of vehicles, together with the associated access lanes as delineated on the endorsed plan must:
 - (a) be provided and completed to the satisfaction of the Responsible Authority prior
 - (b) to the commencement of the development hereby permitted;
 - (c) thereafter be maintained to the satisfaction of the Responsible Authority;
 - (d) be made available for such use at all times and not used for any other purpose;
 - (e) be properly formed to such levels that it can be used in accordance with the
 - (f) endorsed plan; and
 - (g) be drained and sealed with an all weather seal coatall to the satisfaction on the Responsible Authority.
22. Any redundant vehicle crossings must be broken out and reinstated with paving, kerb and channel of the surrounding area. The cost of these reinstatement works must be borne by the Permit Holder.
23. Any damaged road(s) and footpath(s) adjacent to the development site as a result of the development (including as a result of trenching and excavation for underground utility service connections) must be reinstated to the satisfaction of the Responsible Authority and at the expense of the Permit Holder.
24. All vehicle crossings must be constructed in accordance with City of Yarra Standard Drawings and Specifications.
25. The development's finished floor levels relative to the existing footpath and road levels must be such that pedestrian and vehicular access accord with the Australian/New Zealand Standard AS/NZS 2890.1:2004.
26. All existing kerb and channel, and road pavement surface levels must not be altered, unless with the prior written approval of the Responsible Authority. Council may permit the adjustment of Building Line levels to provide access in accordance with AS/NZS 2890.1:2004.

Amenity

27. Alarms associated with the commercial premises must be directly connected to a security service and must not produce noise beyond the premises.
28. The amenity of the area associated with the café and deli uses must not be detrimentally affected, through:
 - (a) the transport of materials, goods or commodities to or from land;
 - (b) the appearance of any buildings, works or materials; and
 - (c) the emission of noise, artificial light, vibration, smell, fumes, smoke, vapour, steam, soot, ash, dust, waste water, waste products, grit or oil, or the presence of vermin.
29. All buildings must be maintained in good order and appearance to the satisfaction of the Responsible Authority.
30. Noise emanating from the development, including plant and other equipment, must comply with the State Environment Protection Policy N-1 to the satisfaction of the Responsible Authority.

31. Noise emanating from the commercial uses within the development must comply with the permissible noise levels for entertainment noise as specified in the State Environment Protection Policy (Control of Music Noise from Public Premises) No. N2.

Landscaping

32. Before the occupation of the development, the landscaping works shown on the endorsed plans must be carried out and completed to the satisfaction of the Responsible Authority. Unless with the further written consent of the Responsible Authority.
33. The landscaping shown on the endorsed plans must be maintained to the satisfaction of the Responsible Authority including that any dead or diseased or damaged plants are to be replaced.
34. Before construction works start, a landscaping bond or bank guarantee of no less than 5% of the total landscaping value must be paid to the Responsible Authority. This bond will be returned in full no later than six month after the satisfactory completion of these works and subject to an inspection by the Responsible Authority.

Lighting

35. External lighting must be located, directed, shielded and of limited intensity so that no nuisance or loss of amenity is caused to any person within and beyond the site, to the satisfaction of the Responsible Authority.
36. Within 6 months of the development commencing, the permit holder must provide to the Responsible Authority a public lighting plan for Waterloo Road. The plan must detail:
 - (a) The level of lighting complying with level "P4" (or higher) as per Australian Standard AS1158.3.1.
 - (b) use of standard Citi Power energy efficient luminaries.
 - (c) no light spillage occurring into windows of the proposed residential properties and should comply with the requirements of Australian Standard AS 4282.
 - (d) A consultation plan detailing consultation with the existing occupiers regarding the proposed location for the new street lighting poles.
 - (e) The process of the Implementation of new scheme must not cause any disruption to the existing public lighting - ensure continuity of public lighting during the installation process.

The cost of the design/consultation and installation of lighting scheme will be borne by the permit holder.

General

37. Privacy screens as required in accordance with the endorsed plans must be installed prior to occupation of any stage of the development to the satisfaction of the Responsible Authority and maintained to the satisfaction of the Responsible Authority thereafter.
38. All new on boundary walls must be cleaned and finished to the satisfaction of the Responsible Authority.
39. All piping and ducting, other than for drainage above the ground floor storey of the building must be concealed.

Food and drinks premises (café)

40. The food and drinks premise (café) may only operate between the hours of 7.00 am and 11.00 pm, seven days, unless with the prior written consent of the Responsible Authority.
41. Notwithstanding condition 40, above, the outdoor seating area (designated area within the 'neighbourhood square') must not be used past 8.00 pm, unless with the prior written consent of the Responsible Authority.
42. No more than 5 staff members may be on the site at any one time, unless with the prior written consent of the Responsible Authority.
43. No more than 40 patrons may be inside the food and drinks premises (café) at any one time, with an additional 40 patrons permitted within the designated area within the 'neighbourhood square', unless with the prior written consent of the Responsible Authority.
44. Speakers must not be located outside the building.
45. Deliveries may only occur between 8.00 am and 4.00 pm, unless with the prior written consent of the Responsible Authority.

Convenience shop (deli)

46. The convenience shop (deli) may only operate between the hours of 7.00 am and 11.00 pm, seven days, unless with the prior written consent of the Responsible Authority.
47. No more than 1 staff member may be on the site at any one time, unless with the prior written consent of the Responsible Authority.
48. Deliveries may only occur between 8.00 am and 4.00 pm, unless with the prior written consent of the Responsible Authority.

Public Transport Victoria Conditions (49 to 51)

49. Before the use of the land commences, a Green Travel Plan must be prepared to the satisfaction of the Responsible Authority following consultation with the Department of Transport (Public Transport Division). The Plan must be prepared by a suitably qualified person and must encourage the use of non-private vehicle transport modes by the occupiers of the land. The Plan must include the following:
 - (a) a description of the location in the context of alternate modes of transport and objectives for the Green Travel Plan;
 - (b) outline Green Travel Plan measures for the development including, but not limited to:
 - (i) household welcome packs – tram, train and bus timetables relevant to the local area must be included in the pack of information provided to purchaser's upon a purchaser's occupation of an apartment;
 - (ii) include a minimum to 10 by 2 hour Metcard (zone one / two) within the household welcome pack or a Myki pass (of equivalent value) and registration information;
 - (iii) bicycle parking and facilities available on the land;
 - (iv) pedestrian routes to key destinations.
50. The Green Travel Plan must not be amended without written consent of the Responsible Authority following consultation with the Department of Transport (Public Transport Division).
51. Once approved the Green Travel Plan will form part of the planning permit and any ongoing Management Plan for the land to ensure the Green Travel Plan continues to be implemented by residents / owners to the satisfaction of the Responsible Authority.

Construction Management Plan

52. Before any development commences, a Construction Management Plan to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. When approved, the plan will be endorsed as evidence of its approval. The plan must provide for or include the following:
- (a) a pre-conditions survey (dilapidation report) of the subject site and all adjacent Council roads frontages and nearby road infrastructure;
 - (b) protection works necessary to road and other infrastructure (limited to an area reasonably proximate to the site);
 - (c) remediation of any damage to road and other infrastructure (limited to an area reasonably proximate to the site);
 - (d) containment of dust, dirt and mud within the site and method and frequency of clean up procedures in the event of build up of matter outside the site;
 - (e) on site facilities for vehicle washing;
 - (f) the location of loading zones, site sheds, materials, cranes and crane/hoisting zones, gantries and any other construction related items or equipment to be located in any street;
 - (g) site security;
 - (h) management of any environmental hazards that the activities on-site pose including but not limited to: contaminated soil, materials and waste, dust, stormwater contamination from run-off and wash-waters, sediment from the site on roads, washing of concrete trucks and other vehicles and machinery, spillage from refuelling cranes and other vehicles and machinery;
 - (i) construction program;
 - (j) preferred arrangements for trucks delivering to the site including delivery and unloading points and expected frequency;
 - (k) parking facilities for construction workers;
 - (l) measures to ensure that sub-contractors/tradespersons operate in accordance with the Construction Management Plan;
 - (m) an outline of requests to occupy public footpaths or roads, or anticipated disruptions to local services;
 - (n) an emergency contact that is available for 24 hours per day for residents and the Responsible Authority in the event of relevant queries or problems experienced;
 - (o) the provision of a traffic management plan to comply with provisions of AS 1742.3-2002 Manual of uniform traffic control devices - Part 3: Traffic control devices for works on roads;
 - (p) a noise and vibration management plan showing methods to minimise noise and vibration impacts on nearby properties and to demonstrate compliance with Noise Control Guideline 12 for Construction (Publication 1254) as issued by the Environment Protection Authority in October 2008, to the satisfaction of the Responsible Authority. In this regard, consideration (amongst other matters) may be given to:
 - (i) using lower noise work practice and equipment
 - (ii) the suitability of the site for the use of an electric crane
 - (iii) silencing all mechanical plant by the best practical means using current technology; and
 - (iv) fitting all pneumatic tools operated near a residential area with an effective silencer on their air exhaust port.

53. During the construction, the following must occur:
- (a) any stormwater discharged into the stormwater drainage system to comply with EPA guidelines;
 - (b) stormwater drainage system protection measures must be installed as required to ensure that no solid waste, sediment, sand, soil, clay or stones from the premises enters the stormwater drainage system;
 - (c) vehicle borne material must not accumulate on the roads abutting the site;
 - (d) the cleaning of machinery and equipment must take place on site and not on adjacent footpaths or roads;
 - (e) all litter (including items such as cement bags, food packaging and plastic strapping) must be disposed of responsibly; and
 - (f) all site operations must comply with the EPA Publication TG302/92.
54. The development once commenced, must be completed to the satisfaction of the Responsible Authority.

Expiry

55. This permit will expire if one of the following circumstances applies:
- (a) the development is not started within two years of the date of this permit;
 - (b) the development is not completed within four years of the date of this permit; and
 - (c) the café and deli uses are not commenced within five years of the date of this permit.

The Responsible Authority may approve extensions to these time limits if requests are made before the permit expires or within 3 months afterwards.

NOTE: The site is located within an Environmental Audit Overlay. Pursuant to clause 45.03 of the Yarra Planning Scheme, the requirements of the Environmental Audit Overlay must be met prior to the commencement of works permitted under the permit.

NOTE: The site is listed on the Victorian Heritage Register. Please contact Heritage Victoria to ascertain permit requirements.

NOTE: The proposal includes construction over a carriageway easement. As easements are not strictly planning matters in this regard, the Permit Holder is required to consider this obligation.

NOTE: A building permit maybe required before development is commenced. Please contact Council's Building Department on PH 9205 5351 to confirm.

NOTE: In accordance with the Yarra Planning Scheme, a 4.5 per cent public open space contribution may apply in the event of the subdivision of the land.

NOTE: The design and construction of the vehicle crossing must also satisfy the requirements of Council's Community Amenity unit's Vehicular Access into Properties (Info Sheet and Application Form) before a vehicle crossing permit can be issued.

NOTE: The Permit Holder must apply for a Legal Point of Discharge under Regulation 610 – Stormwater Drainage of the Building Regulations 2006 from Yarra Building Services Unit.

NOTE: Any storm water drainage within the property must be provided and be connected to the nearest Council pit of adequate depth and capacity (legal point of discharge), to Council's satisfaction under Section 200 of the Local Government Act 1989 and Regulation 610.

NOTE: Areas must be provided inside the property line and adjacent to the footpath to accommodate pits and meters. No private pits, valves or meters on Council property will be accepted.

NOTE: The Permit Holder is required to identify any overflow paths surrounding the site and ensure that they are not obstructed. Please contact Council's Engineering Services Unit on 9205 5555 to discuss.

NOTE: The site must be drained to the legal point of discharge to the satisfaction of the Responsible Authority.

NOTE: All future residents, employees and occupiers residing within the development approved under this permit will not be permitted to obtain resident, employee or visitor parking permits.

NOTE: A copy of this permit and the endorsed plan (and any subsequent variation) must form part of the document to any Contract of Sale or Lease for any part of the site after the date of the permit.

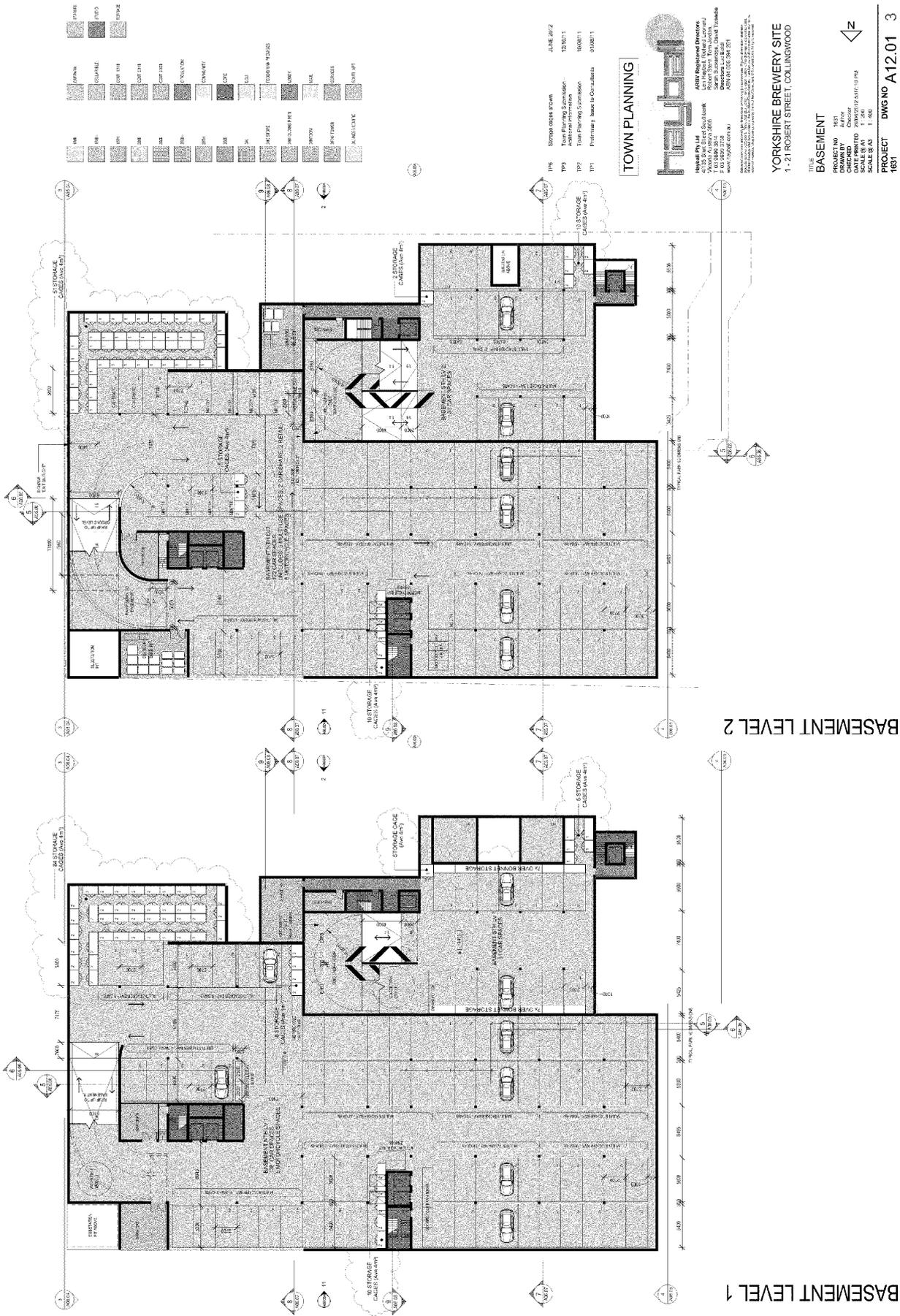
NOTE: The food premises are required to comply with health and food safety regulations before commencing operation. Please contact Council's Public Health Unit on 9205 5166.

CONTACT OFFICER: Sarah Thomas
TITLE: Principal Planner
TEL: 92055005

Attachments

- 1 PLN11/0750 - 1 - 21 Robert Street Collingwood - Yorkshire Brewery Amended Plans (Part One)
- 2 PLN11/0750 - 1 - 21 Robert Street Collingwood - Yorkshire Brewery Amended Plans (Part Two)
- 3 PLN11/0750 - 1 - 21 Robert Street Collingwood - Yorkshire Brewery Amended Plans (Part Three)
- 4 PLN11/0750 - 1 - 21 Robert Street Collingwood - Yorkshire Brewery Amended Plans (Part Four)
- 5 PLN11/0750 - 1 - 21 Robert Street Collingwood - Amended Plans - Perspective
- 6 PLN11/0750 - 1 - 21 Robert Street Collingwood - Shadow Diagrams
- 7 Amendment / Former Yorkshire Brewery - 88 Wellington Street and 1 - 21 Robert Street Collingwood - Victoria Heritage Register Number H0807 & Permit P16751 [All Scanned]

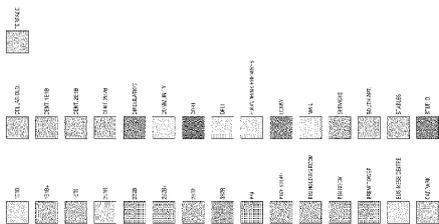
Attachment 1 - PLN11/0750 - 1 - 21 Robert Street Collingwood - Yorkshire Brewery Amended Plans (Part One)



BASEMENT LEVEL 2

BASEMENT LEVEL 1

Attachment 1 - PLN11/0750 - 1 - 21 Robert Street Collingwood - Yorkshire Brewery Amended Plans (Part One)



TP6 Erection & Schedule amended - JUNE 2012
 TP5 Heritage Victoria additional info - 09/02/12
 TP4 Heritage Victoria additional info - 09/02/12
 TP3 Town Planning Amendment - 20/01/12
 TP2 Town Planning Amendment - 12/01/11
 TP1 Heritage Victoria additional info - 09/02/12

TOWN PLANNING



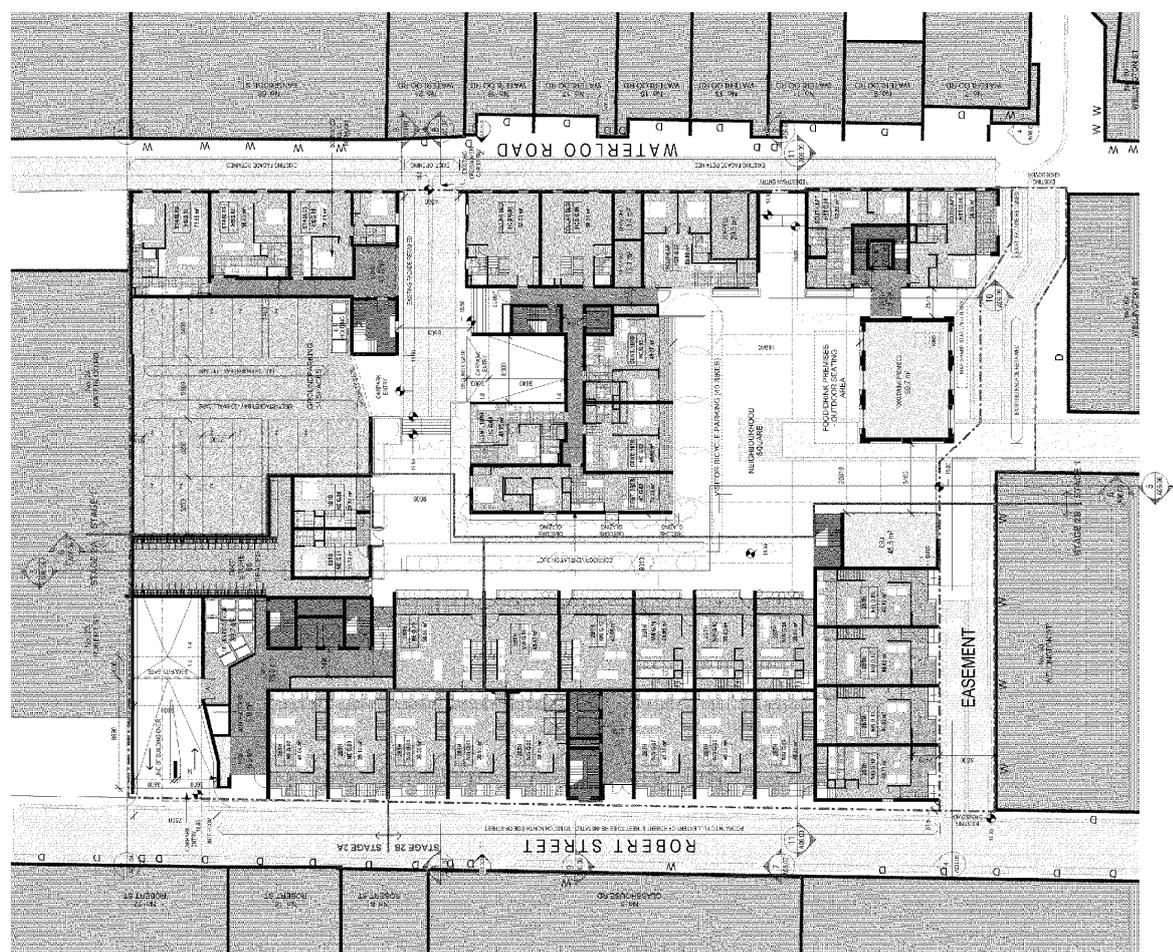
Atkins Pty Ltd
 100 Collins Street
 Melbourne VIC 3000
 P.O. Box 2708
 Melbourne VIC 3000
 www.atkins.com.au
 ABN 88 006 384 281

YORKSHIRE BREWERY SITE
 1-21 ROBERT STREET, COLLINGWOOD

TITLE
 GROUND - WITH CONTEXT

PROJECT NO
 11831
 CHECKED
 SCALE 1:1000
 SCALE 1:500

PROJECT DWG NO
 A12.02A 4
 1831



11831
 WELLINGTON ST

GROUND - WITH CONTEXT

Attachment 1 - PLN11/0750 - 1 - 21 Robert Street Collingwood - Yorkshire Brewery Amended Plans (Part One)

| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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TPN Enriched & Sketches Approved: JUNE 2012
 TPN No. 1000/1000 - Additional information on planning conditions to be provided to the Council.
 TPN Town Planning Department - 127/1011
 Address: Collingwood

TOWN PLANNING

ASBY ENGINEERING DIRECTORS
 ASBY ENGINEERING DIRECTORS
 2/100 ROBERT STREET, COLLINGWOOD
 VIC 3068
 P.O. BOX 2708
 COLLINGWOOD VIC 3068
 www.asby.com.au
 ASBY ENGINEERING DIRECTORS
 2/100 ROBERT STREET, COLLINGWOOD
 VIC 3068
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 www.asby.com.au

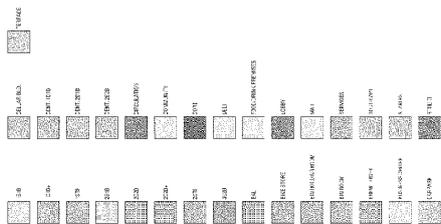
YORKSHIRE BREWERY SITE
 1-21 ROBERT STREET, COLLINGWOOD

TITLE: LEVEL 1 - WITH CONTEXT
 PROJECT NO: 1031
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 CHECKED: [Signature]
 SCALE: 1:500
 PROJECT: DWG NO A12.02B 3
 1031



LEVEL 1 - WITH CONTEXT

Attachment 1 - PLN11/0750 - 1 - 21 Robert Street Collingwood - Yorkshire Brewery Amended Plans (Part One)



TP8 Emerald & Schrecks Engineers JUN 2012
 TP9 Heritage Value additional info - 06/02/12
 TP10 Heritage Value additional info - 06/02/12
 TP11 Heritage Value additional info - 12/10/11
 TP12 Heritage Value additional info - 12/10/11

TOWN PLANNING



ARBY Registered Director
 25-27 WILKINSON ST
 COLLINGWOOD VIC 3061
 P 03 9888 2718
 F 03 9888 2719
 www.townplanning.com.au
 ASB 88 006 384 251

YORKSHIRE BREWERY SITE
 1-21 ROBERT STREET, COLLINGWOOD

TITLE
 LEVEL 5 - WITH CONTEXT

PROJECT No 1631
 DRAWN BY J. HARRIS
 CHECKED BY J. HARRIS
 SCALE 1:100
 DATE 11/06/12

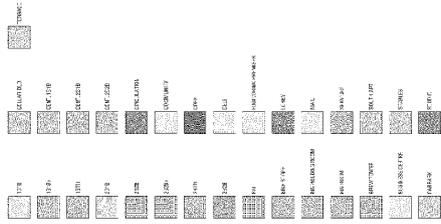


PROJECT DWG NO A12.04B 3
 1631



LEVEL 5 - WITH CONTEXT

Attachment 2 - PLN11/0750 - 1 - 21 Robert Street Collingwood - Yorkshire Brewery Amended Plans (Part Two)



TP6 Erskine & Strainville amended JUNE 2012
 TP5 Heritage Victoria additional info - 09/09/12
 TP4 City of Melbourne additional info - 12/08/11
 TP3 Town Planning Submission - 12/01/11
 TP2 Initial Submission

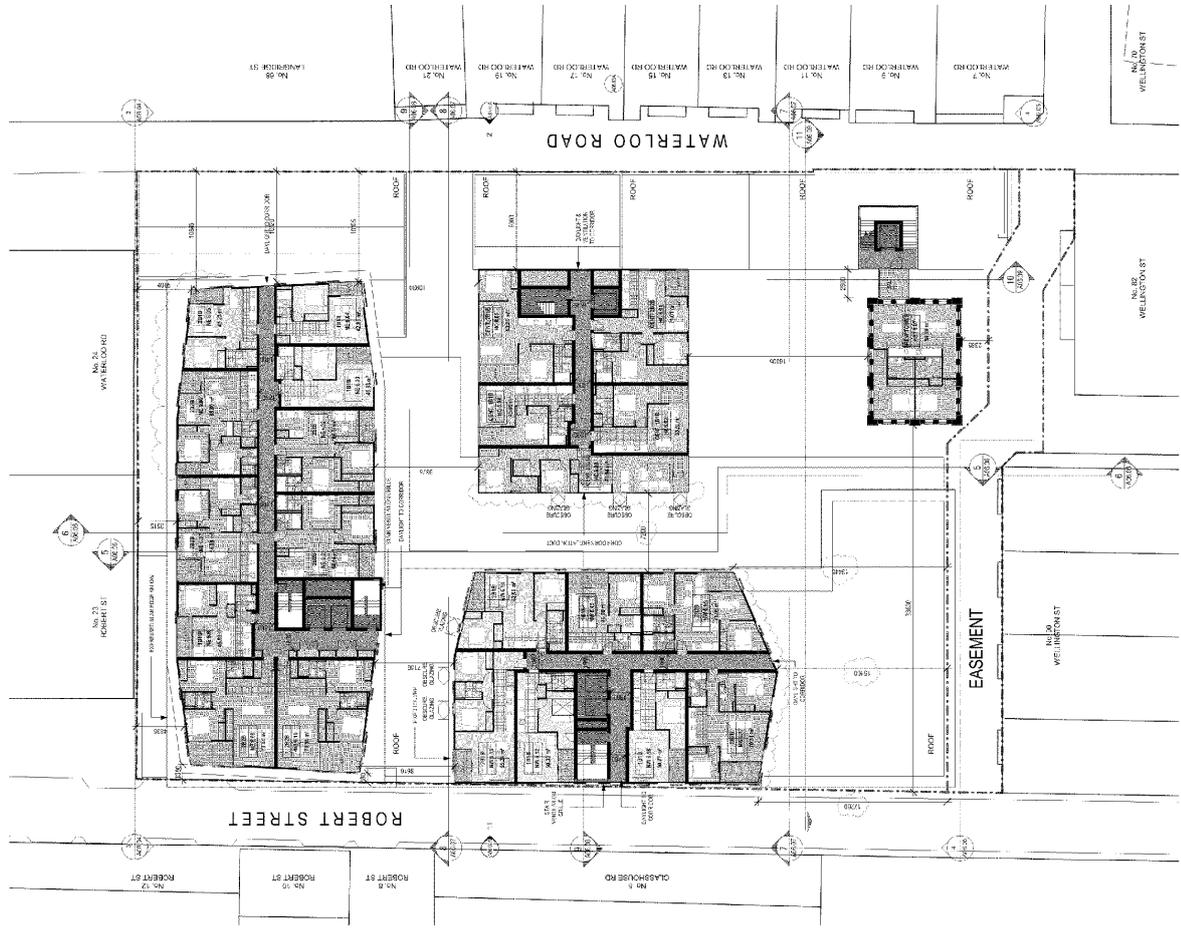
TOWN PLANNING



YORKSHIRE BREWERY SITE
 1 - 21 ROBERT STREET, COLLINGWOOD

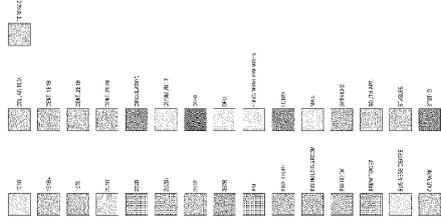
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 1831



LEVEL 6 - WITH CONTEXT

Attachment 2 - PLN11/0750 - 1 - 21 Robert Street Collingwood - Yorkshire Brewery Amended Plans (Part Two)



TPS Erection & Submits amended
 Town Planning Submission -
 address: 11/11/11
 JUNE 2012

TOWN PLANNING



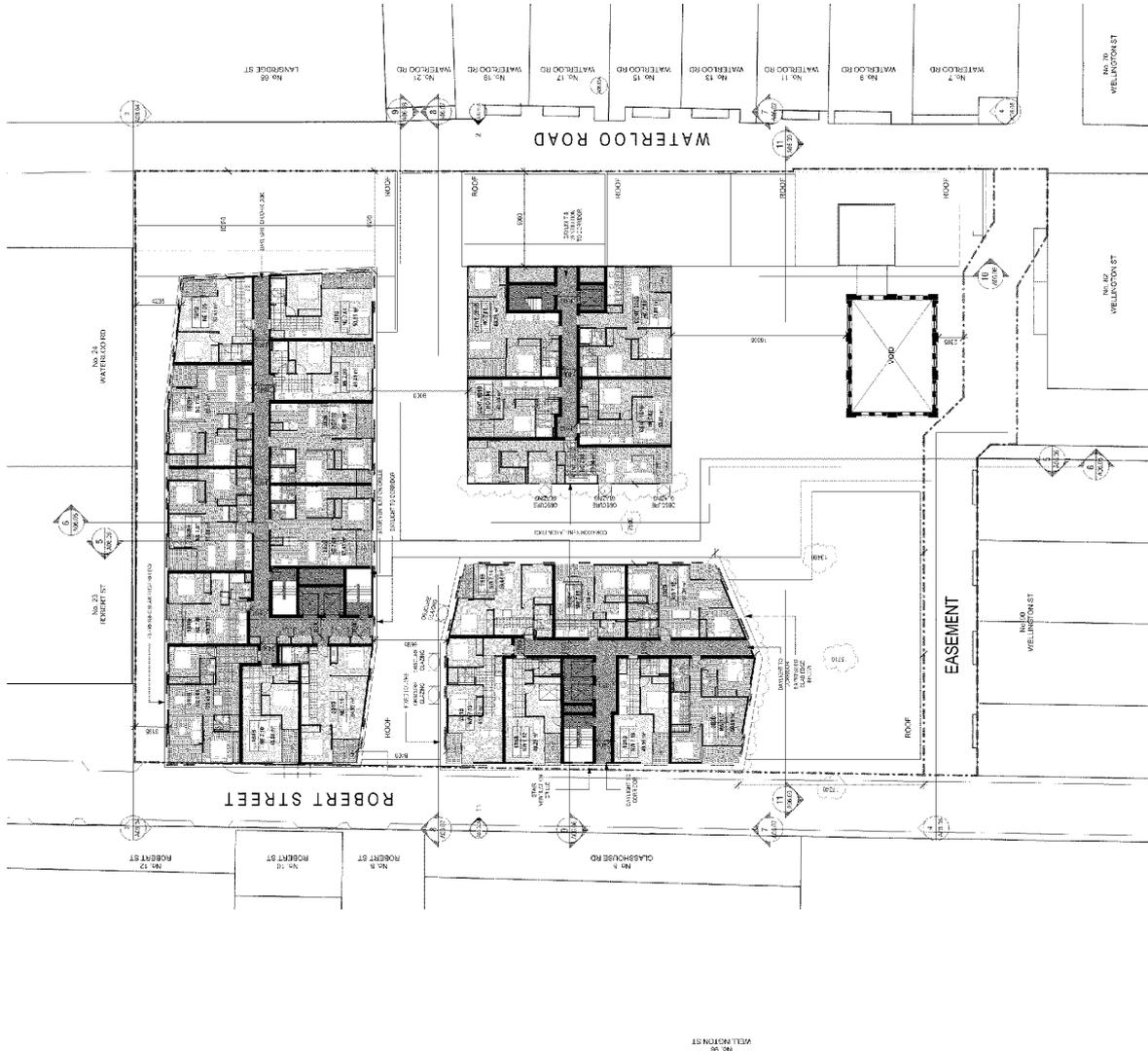
ASBN Registered Director
 Victoria's Accredited
 F. 03 9896 3743
 www.townplanning.com.au

YORKSHIRE BREWERY SITE
 1 - 21 ROBERT STREET, COLLINGWOOD

LEVEL 7 - WITH CONTEXT

PROJECT NO: 1181
 CHECKED: [Signature]
 SCALE: 1:100
 SCALE REF: A3

PROJECT DWG NO: A12.05B 2
 1/81



LEVEL 7 - WITH CONTEXT

Attachment 2 - PLN11/0750 - 1 - 21 Robert Street Collingwood - Yorkshire Brewery Amended Plans (Part Two)

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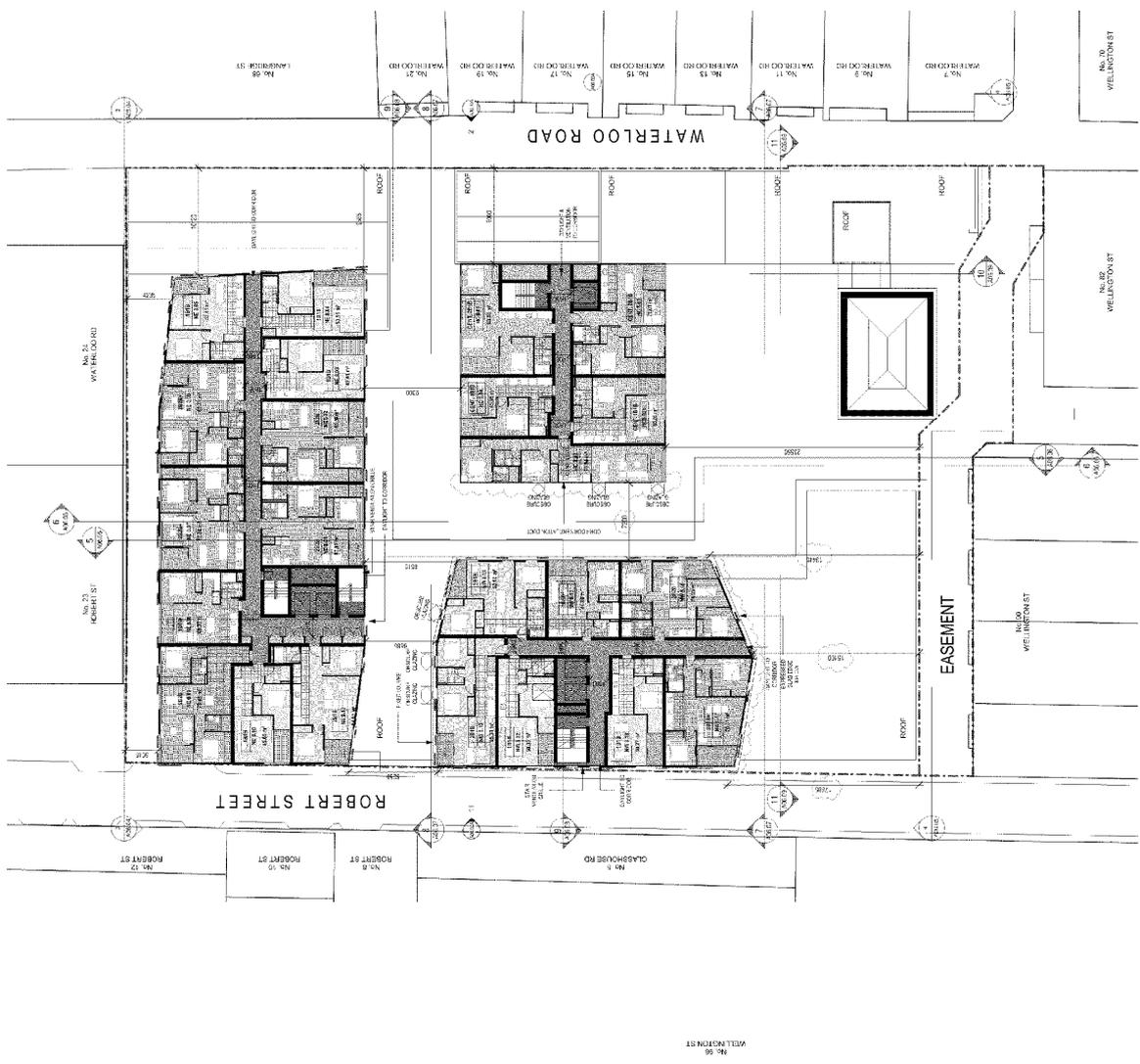
TPS Erection & Submits amended
Town Planning Submission -
addressed: 12/01/11
JUNE 2012

TOWN PLANNING

YORKSHIRE BREWERY SITE
1 - 21 ROBERT STREET, COLLINGWOOD

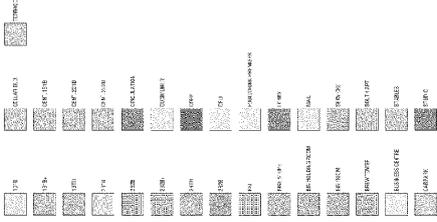
LEVEL 8 - WITH CONTEXT

PROJECT NO: 1831
DRAWN BY: JAMES WILSON
CHECKED BY: JAMES WILSON
SCALE: 1:100
SCALE REF: A3
PROJECT: DWG NO A12.06A 2
1831



LEVEL 8 - WITH CONTEXT

Attachment 2 - PLN11/0750 - 1 - 21 Robert Street Collingwood - Yorkshire Brewery Amended Plans (Part Two)



TPS Erection & Submits amended
Town Planning Submission -
addressed circulation
JUNE 2012
12/01/11

TOWN PLANNING

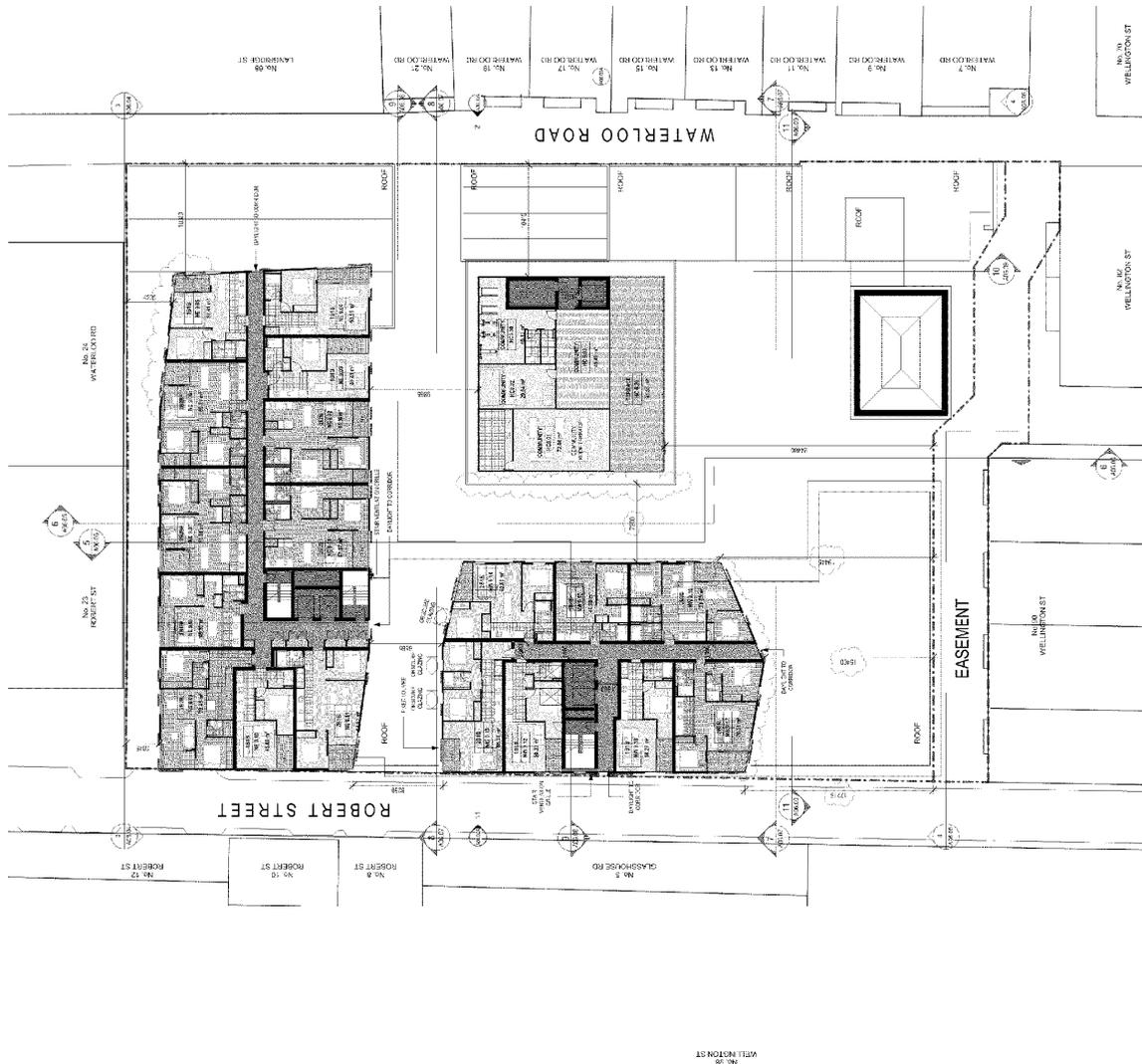
Hayball Pty Ltd
ASBN Registered Director
Hayball Pty Ltd
1/200 Victoria Street, Collingwood
VIC 3068
P 03 9886 3743
www.hayball.com.au
ASBN 66 000 396 251

YORKSHIRE BREWERY SITE
1 - 21 ROBERT STREET, COLLINGWOOD

LEVEL 9 - WITH CONTEXT

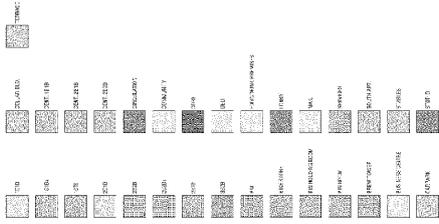
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PROJECT: DWG NO: A12.06B.2
1831



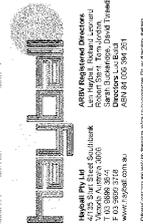
LEVEL 1 - WITH CONTEXT

Attachment 2 - PLN11/0750 - 1 - 21 Robert Street Collingwood - Yorkshire Brewery Amended Plans (Part Two)



TPS Erection & Submits completed
 Town Planning Submission -
 address: 12/01/11
 JUNE 2012

TOWN PLANNING

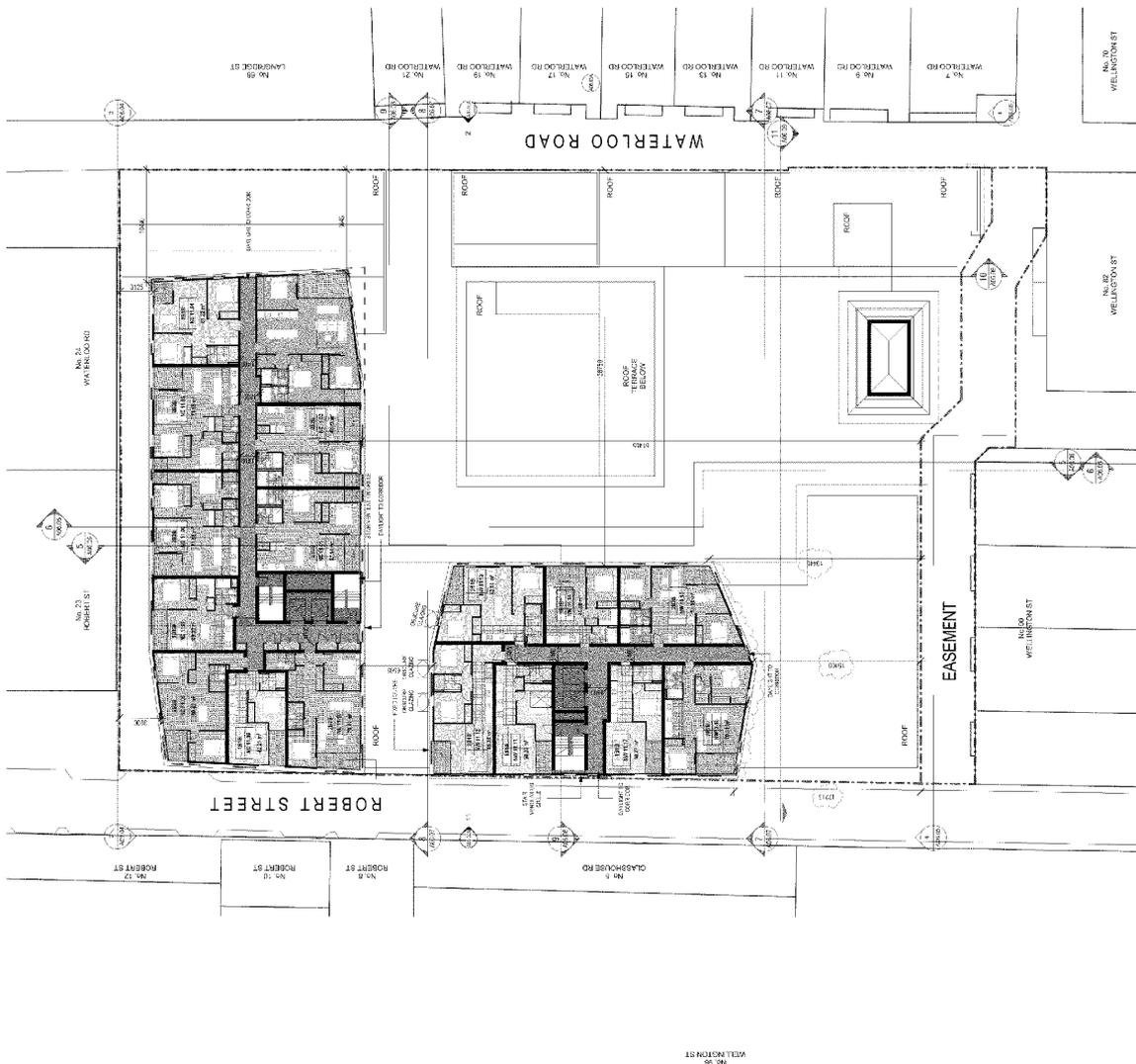


YORKSHIRE BREWERY SITE
 1 - 21 ROBERT STREET, COLLINGWOOD

TITLE
 LEVEL 11 - WITH CONTEXT

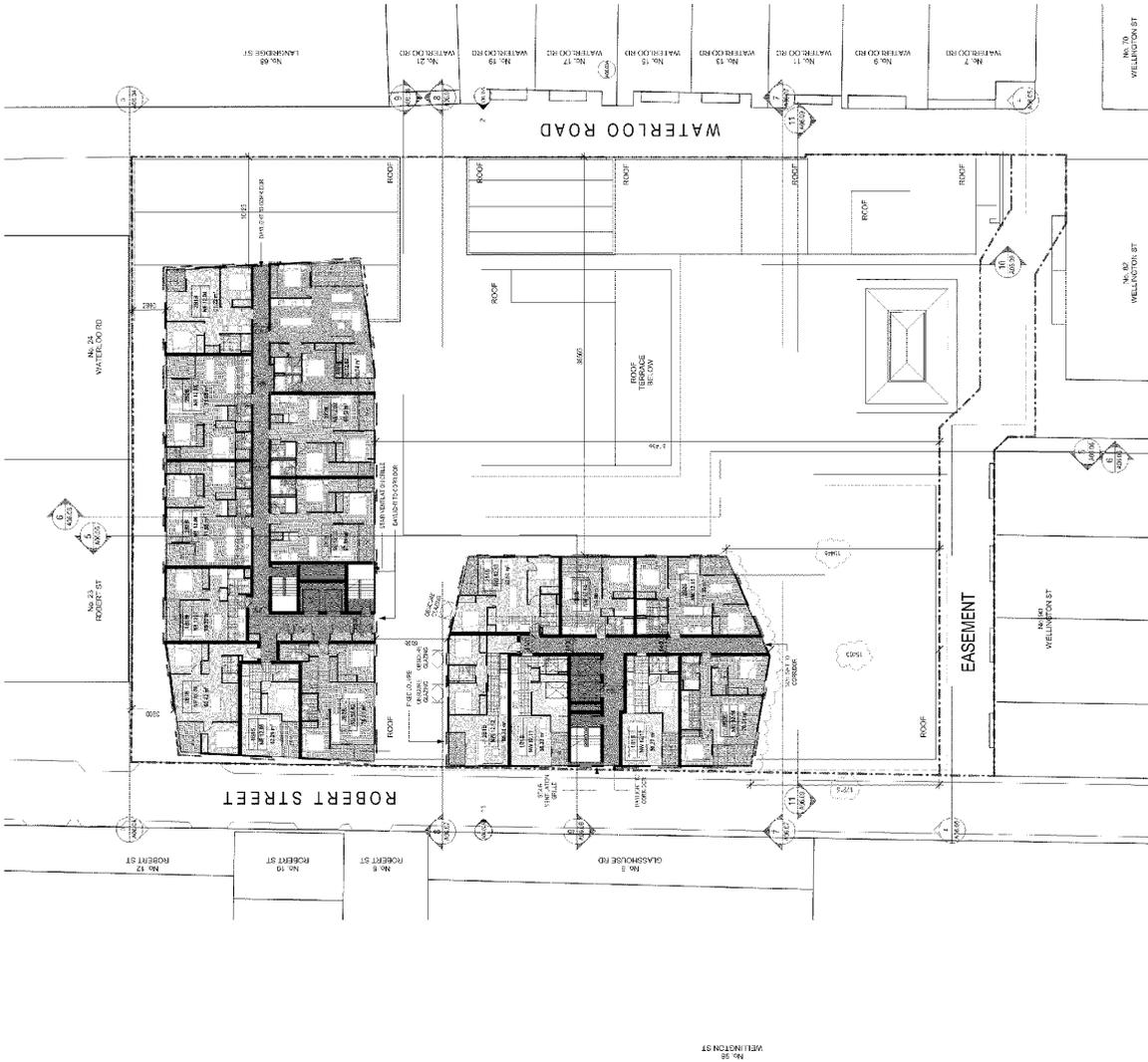
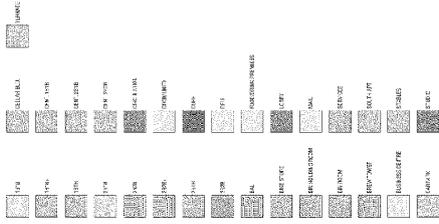
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 CHECKED: [Signature]
 SCALE: 1:100
 SCALE REF: A3

PROJECT DWG NO: A12.07B 2
 1831



LEVEL 11 - WITH CONTEXT

Attachment 2 - PLN11/0750 - 1 - 21 Robert Street Collingwood - Yorkshire Brewery Amended Plans (Part Two)



TPS Erection & Submits amended
Town Planning Submission -
addressed consultation
12/10/11
JUNE 2012

TOWN PLANNING

Yorkshire Brewery Site
1-21 ROBERT STREET, COLLINGWOOD

Hayball Pty Ltd
ASBN Registered Director
Victoria's Architectural
Practising Architect
100/101 Wellington Street, Collingwood
VIC 3068
P 03 9886 3743
www.hayball.com.au
ASBN 84 000 384 251

Yorkshire Brewery Site
1-21 ROBERT STREET, COLLINGWOOD

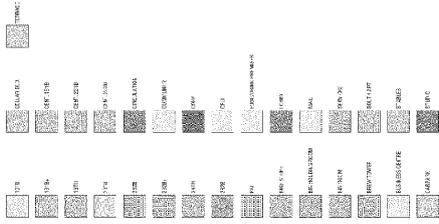
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SCALE REF: A3
DATE: 11/10/11

PROJECT: DWG NO A12.08A 2
1831

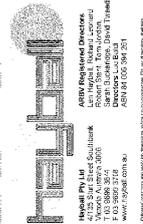
LEVEL 12 - WITH CONTEXT

Attachment 3 - PLN11/0750 - 1 - 21 Robert Street Collingwood - Yorkshire Brewery Amended Plans (Part Three)



TPS Erection & Submits amended
 Town Planning Submission -
 address: 1/21 Robert Street
 JUNE 2012
 12/01/11

TOWN PLANNING

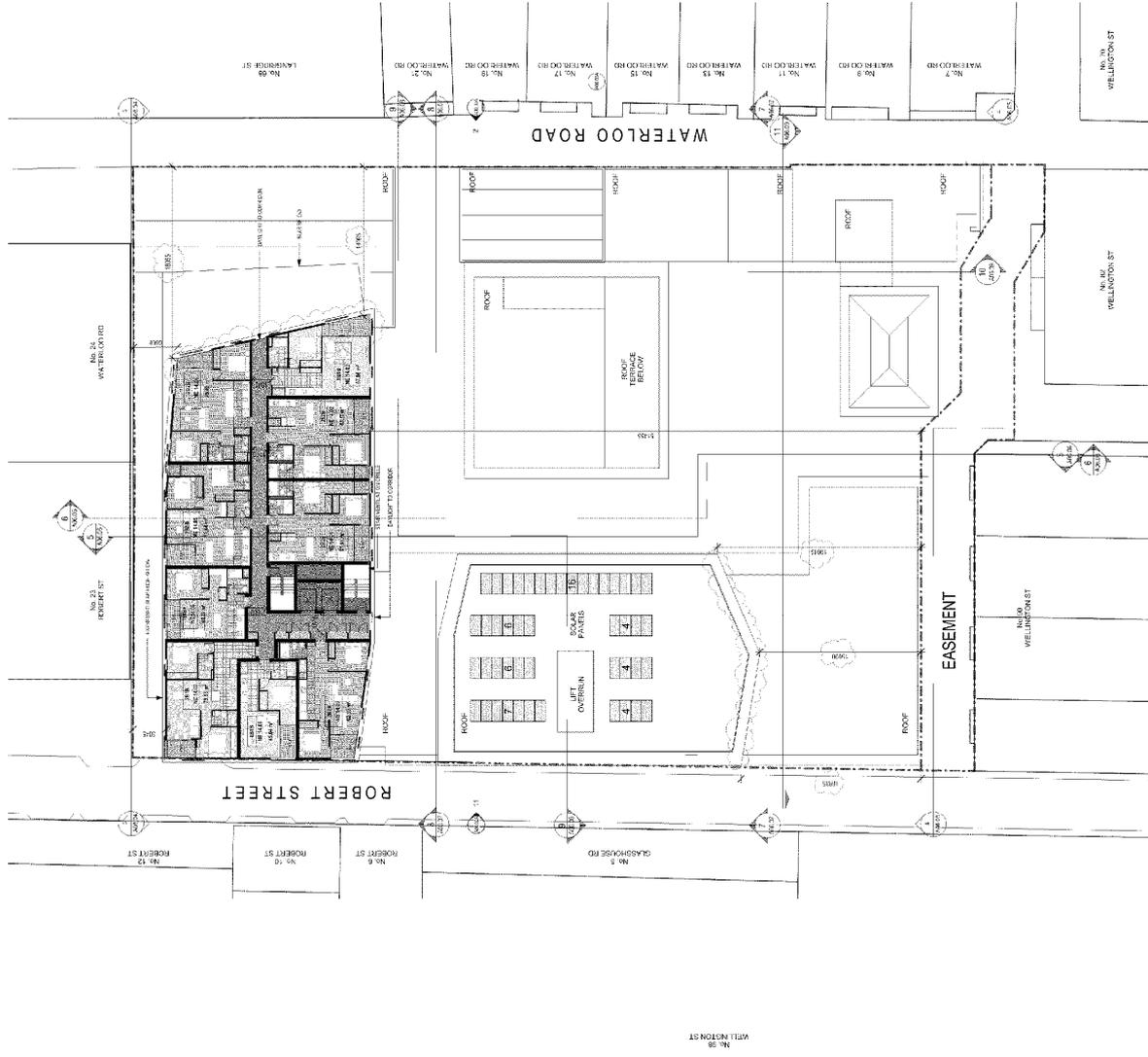


YORKSHIRE BREWERY SITE
 1 - 21 ROBERT STREET, COLLINGWOOD

LEVEL 14 - WITH CONTEXT

PROJECT NO: 1181
 CHECKED: [Signature]
 SCALE: 1:100
 DATE: 12/01/11

PROJECT DWG NO: A12.09A 2
 1/81



LEVEL 14 - WITH CONTEXT

Attachment 3 - PLN11/0750 - 1 - 21 Robert Street Collingwood - Yorkshire Brewery Amended Plans (Part Three)

| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|
| 7/1 | 7/2 | 7/3 | 7/4 | 7/5 | 7/6 | 7/7 | 7/8 | 7/9 | 7/10 | 7/11 | 7/12 | 7/13 | 7/14 | 7/15 | 7/16 | 7/17 | 7/18 | 7/19 | 7/20 | 7/21 | 7/22 | 7/23 | 7/24 | 7/25 | 7/26 | 7/27 | 7/28 | 7/29 | 7/30 | 7/31 | |
| CONCRETE |

TPS Erection & Submits amended
Town Planning Submission -
addressed: 12/01/11
JUNE 2012

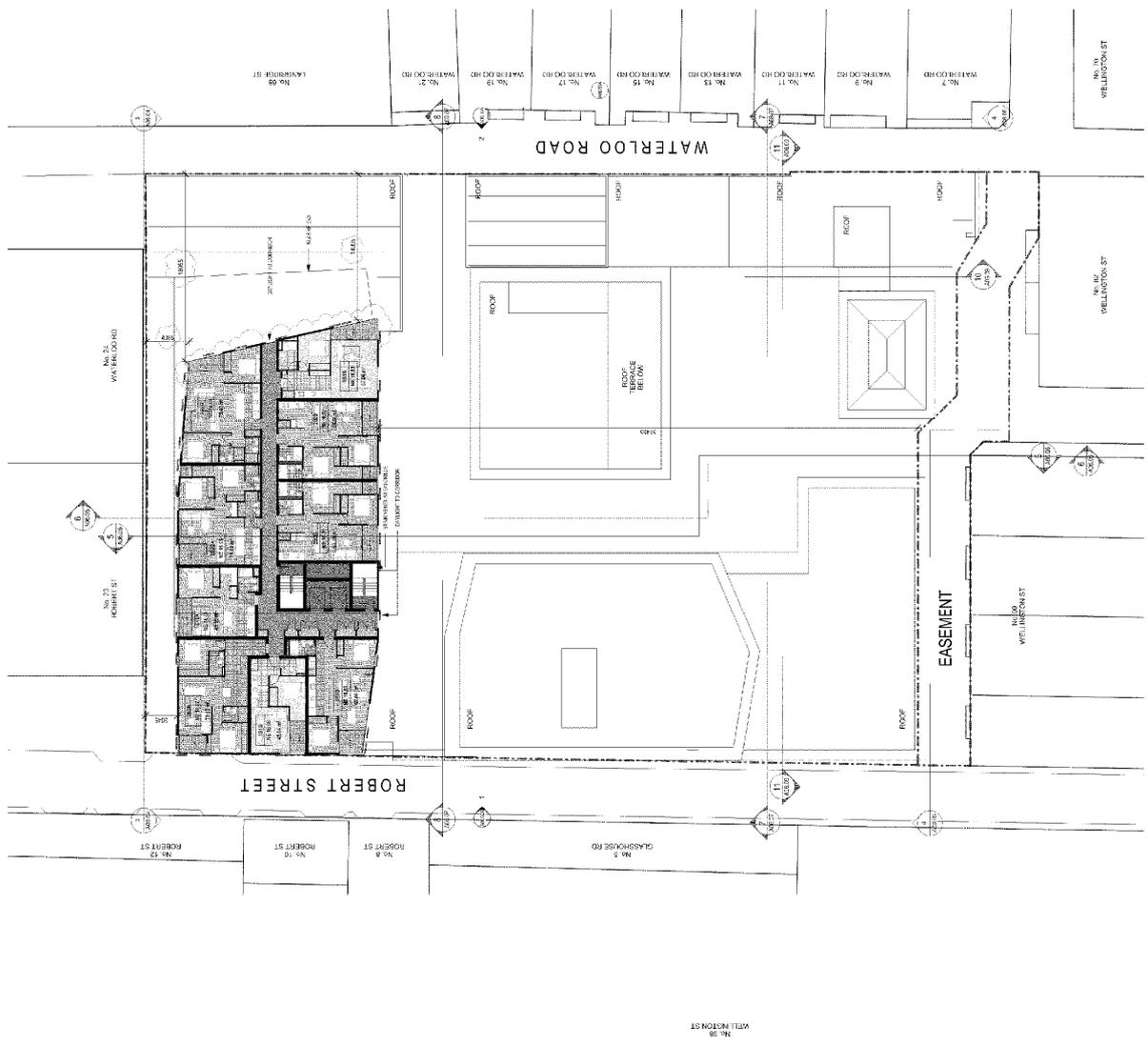
TOWN PLANNING

Yorkshire Brewery Site
1 - 21 ROBERT STREET, COLLINGWOOD

LEVEL 16 - WITH CONTEXT

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SCALE REF: A12.10A.2

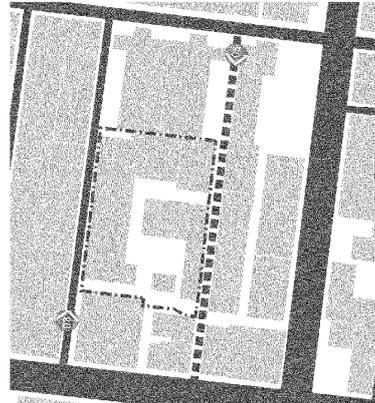
PROJECT: 1631
DWG NO: A12.10A.2



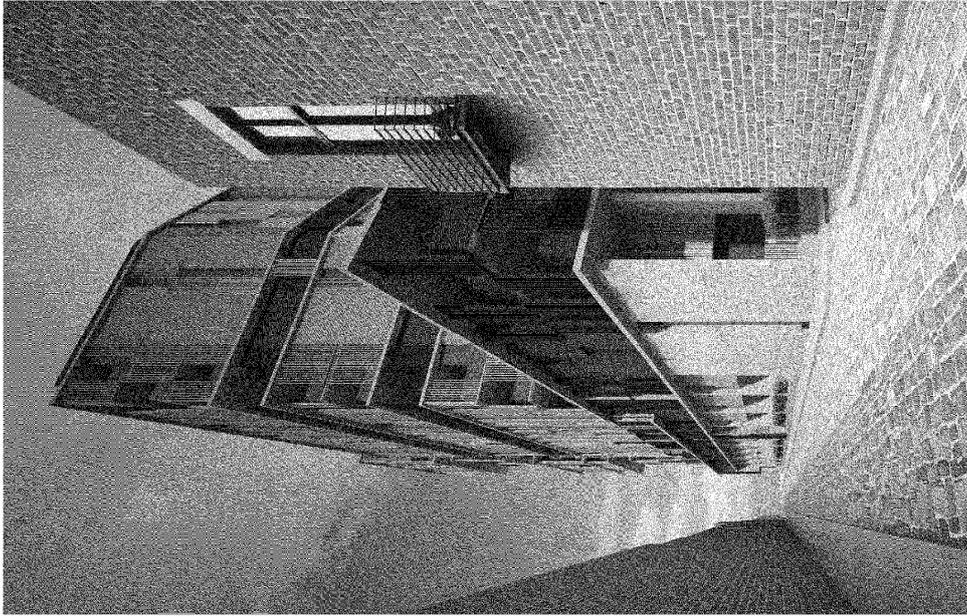
LEVEL 16 - WITH CONTEXT

Attachment 5 - PLN11/0750 - 1 - 21 Robert Street Collingwood - Amended Plans - Perspective

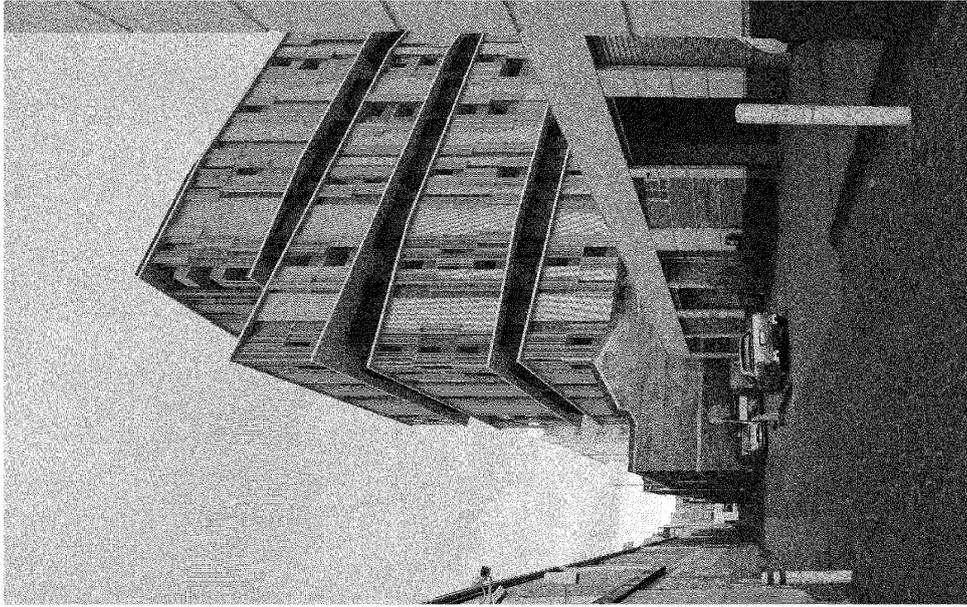
ARCHITECTURAL DESIGN RESPONSE
URBAN CHARACTER



KEY PLAN



03/ LOOKING EAST ON ROBERT STREET



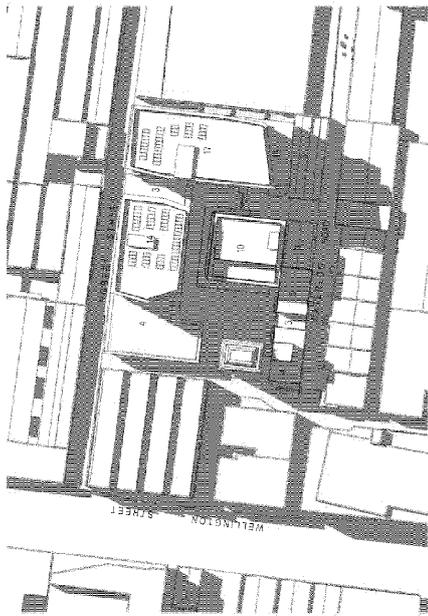
05/ LOOKING WEST ON WATERLOO ROAD NEAR THE CORNER OF PICKETT STREET

Former Yorkshire Brewery 1-21 Robert Street, Collingwood

Attachment 6 - PLN11/0750 - 1 - 21 Robert Street Collingwood - Shadow Diagrams

SHADOW DIAGRAMS

PROPOSED SHADOW DIAGRAMS



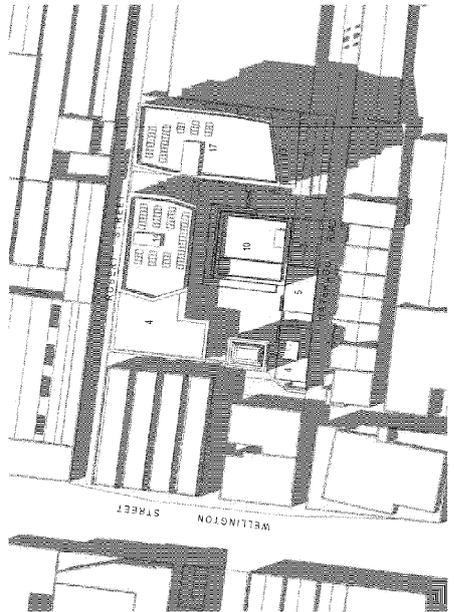
EQUINOX 11AM SEPTEMBER 22
(NTS)



EQUINOX 9AM SEPTEMBER 22
(NTS)



EQUINOX 3PM SEPTEMBER 22
(NTS)



EQUINOX 1PM SEPTEMBER 22
(NTS)

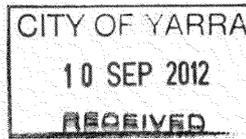
Former Yorkshire Brewery 1-21 Robert Street, Collingwood

Project No. 1031 A

Attachment 7 - Amendment / Former Yorkshire Brewery - 88 Wellington Street and 1 - 21 Robert Street Collingwood - Victoria Heritage Register Number H0807 & Permit P16751 [All Scanned]



**Department of Planning
and Community Development**



File Nos. 11/011226
Permit Nos. P16751

1 Spring Street
Melbourne Victoria
3000
GPO Box 2392
Melbourne Victoria 3001
Telephone: (03) 9208 3333
Facsimile: (03) 9208 3680
www.dpcd.vic.gov.au

3 September 2012

Ms Anita Brady
Lovell Chen Architects & Heritage Consultants
Level 5, 176 Wellington Parade
EAST MELBOURNE VIC 3002

Dear Ms Brady

RE: FORMER YORKSHIRE BREWERY, 88 WELLINGTON STREET AND 1-21 ROBERT STREET COLLINGWOOD, VICTORIAN HERITAGE REGISTER NUMBER H0807, PERMIT P16751

I refer to your letter of 13 August 2012 seeking an amendment to the above permit. You advised the changes are sought in order to align the permit with that issued by the City of Yarra.

I have assessed your request and have determined to amend the permit as follows:

- Amends the permit by endorsing a revised set of 31 drawings prepared by Hayball Pty Ltd dated 29 June 2012 marked A12.01, A12.02A, A12.02B, A12.03A, A12.03B, A12.04A, A12.04B, A12.05A, A12.05B, A12.06A, A12.06B, A12.07A, A12.07B, A12.08A, A12.08B, A12.09A, A12.09B, A12.10A, A06.02, A06.02A, A06.03, A06.03A, A06.04, A06.04A, A06.05, A06.05A, A06.06, A06.06A, A06.07, A06.07A and A06.08.

The amended permit is enclosed together with a set of endorsed drawings.

I also refer to your letter dated 16 August 2012 regarding conditions 2 and 3 of the permit relating to conservation works required to be undertaken and the scheduling of these works. Condition 2 of the permit required the conservation works to be completed prior to the commencement of building work on any of the new structures excluding

Any personal information about you or a third party in your correspondence will be collected, held, managed, used, disclosed or transferred in accordance with the provisions of the Information Privacy Act 2000 (Vic) and applicable laws. Enquiries about access to information about you held by the Department should be directed to the Privacy Officer, Department of Planning and Community Development, PO Box 2392, Melbourne, VIC 3001.

Notwithstanding the above, please note that information provided to enable the administration of the Heritage Act 1995 may be disclosed to persons with an interest in the heritage place or object particularly, and information provided in a permit application may be made available on-line where the application has been publicly advertised under section 106 of the Heritage Act 1995.



Attachment 7 - Amendment / Former Yorkshire Brewery - 88 Wellington Street and 1 - 21 Robert Street Collingwood - Victoria Heritage Register Number H0807 & Permit P16751 [All Scanned]

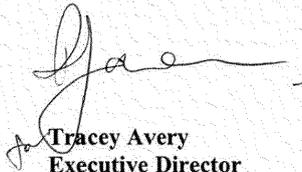
excavation. However, condition 3 provided for an agreed variation to the scheduling of works provided financial security was lodged with the Executive Director.

Your client is now proposing to lodge a bank guarantee to the value of \$1,000,000 (one million dollars) once excavation works and the demolition of the 1950s silos are complete. Once the bank guarantee is lodged, the conservation works would then proceed concurrently with the new works. I consider this arrangement to be consistent with the intent of conditions 2 and 3 provided the conservations works are completed prior to the expiration of the permit on 20 April 2016.

The bank guarantee will be unlimited in terms of its period of validity and the **Heritage Council Of Victoria** (ABN: 87 967 501 331), as the appropriate incorporated body, shall be named in the document as sole beneficiary. The bank guarantee shall be returned to the provider at the written request of the permit holder on the satisfactory completion of the permitted works as determined by the Executive Director. A staged release of the bank guarantee will be possible subject to the required level of documentation being provided to and endorsed by the Executive Director.

Please contact Janet Sullivan, Acting Permits Manager Heritage Victoria on (03) 9208 3193 or write to PO Box 2392 Melbourne, Vic 3001 if you have any queries.

Yours sincerely



Tracey Avery
Executive Director
HERITAGE VICTORIA
Cc Statutory Planner, City of Yarra

Any personal information about you or a third party in your correspondence will be collected, held, managed, used, disclosed or transferred in accordance with the provisions of the Information Privacy Act 2000 (Vic) and applicable laws. Enquiries about access to information about you held by the Department should be directed to the Privacy Officer, Department of Planning and Community Development, PO Box 2392, Melbourne, VIC 3001.

Notwithstanding the above, please note that information provided to enable the administration of the Heritage Act 1995 may be disclosed to persons with an interest in the heritage place or object particularly, and information provided as part of a permit application may be made available on-line where the application has been publicly advertised under section 68 of the Heritage Act 1995.

Attachment 7 - Amendment / Former Yorkshire Brewery - 88 Wellington Street and 1 - 21 Robert Street Collingwood - Victoria Heritage Register Number H0807 & Permit P16751 [All Scanned]

PERMIT

HERITAGE ACT 1995

PERMIT NO: P16751 (As amended)

OWNER/S: Mr Martin Strode
SMA Projects Australia Pty Ltd
ADDRESS: 31 Regent Street
PRAHRAN VIC 3181



| | | | |
|---------------------------------|---|----------|-----------|
| HERITAGE REGISTER NO: | H0807 | FILE NO: | 11/011226 |
| REGISTRATION CATEGORY: | Heritage Place | | |
| NAME OF PLACE /OBJECT (IF ANY): | FORMER YORKSHIRE BREWERY | | |
| LOCATION: | 88 WELLINGTON STREET and 1-21 ROBERT STREET | | |
| COLLINGWOOD | | | |

Pursuant to Section 74 of the Heritage Act (1995) and in respect to the above-mentioned place / object, the Executive Director, Heritage Victoria hereby grants a PERMIT, subject to conditions as prescribed hereunder to carry out the following:

Redevelopment of the site as a residential 'urban village' including partial demolition and adaptation of heritage buildings and construction of new buildings within the registered land area as shown on the following documentation:

'Statement of Heritage Impact for Proposed Works and Development, former Yorkshire Brewery' Plan prepared by Lovell Chen for SMA Projects and dated August 2011 – and including a Schedule of Conservation works in Appendix A

'Design and Context Report, former Yorkshire Brewery' prepared by Hayball Pty Ltd for SMA Projects and dated August 2011

Former Yorkshire Brewery, Existing Conditions EX01 -03, prepared by Lovell Chen and dated March 2011

Yorkshire Brewery Town Planning Landscape Design Report, prepared by Tract, and dated August 2011

Urban design Statement, prepared by David Locke Associates and dated August 2011

'Yorkshire Brewery site' prepared by Hayball Pty Ltd, Project 1631, numbered A01.01-02, A02.01, A06.02-08, A12.01 -10 and dated August 2011

as endorsed by the Executive Director and forming part of this permit.

Note: see foot of permit for details of amendment

CONDITIONS:

Time limit

1. This permit shall expire if the permitted works have not commenced within two (2) years of the date of issue of this permit, or are not completed within four (4) years of the date of issue of this permit unless otherwise agreed in writing by the Executive Director, Heritage Victoria.

Conservation works

2. To ensure that the conservation works are completed as part of the redevelopment project all conservation works as set out in the Schedule of Works which forms Appendix 1 included in the 'Statement of Heritage Impact for proposed works and development', prepared by Lovell Chen, dated August 2011 are to be completed prior to the commencement of building work on any of the new structures, excluding excavation.
3. Any proposed change in project scheduling that may delay commencement of conservation works outlined in Condition 2, must be approved in writing by the Executive Director, and may result in the requirement for a bank guarantee to be lodged to the value of the delayed conservation works.

Attachment 7 - Amendment / Former Yorkshire Brewery - 88 Wellington Street and 1 - 21 Robert Street Collingwood - Victoria Heritage Register Number H0807 & Permit P16751 [All Scanned]

Conservation works detailed documentation

4. Two copies of the detailed drawings and specification for all conservation works as set out in the Schedule of Works which forms Appendix 1 included in the 'Statement of Heritage Impact for proposed works and development', prepared by Lovell Chen, dated August 2011 are to be submitted to the Executive Director for approval prior to the commencement of work.

Detailed drawings of the registered buildings

5. Two full sets of detailed architectural plans for the conversion of the registered buildings including plans, elevations, sections showing conservation works and locations of services are to be submitted to the Executive Director for approval prior to the commencement of work.
6. Detailed axonometric drawings at a scale of minimum 1:100 of the typical junctions between the tower and the connecting glazed element on the south elevation, and the junction between the north west corner of the stables building clearly marking the extent of any demolition are to be submitted to the Executive Director for approval prior to the commencement of work.

Exterior materials and finishes to new work

7. Details of the exterior materials and finishes of all new buildings on the site are to be submitted to the Executive Director for approval prior to the commencement of work.

Material and finishes to registered buildings

8. Details of all new surface treatments, finishes and materials to registered buildings (both exterior and interior) are to be submitted to the Executive Director for approval prior to the commencement of work.

Landscaping and remnants of earlier paving

9. The landscaping is to reflect the earlier bluestone paving shown on the Extent of Registration diagram and all remaining original bluestone pavers on site are to be incorporated into the landscaping. Two full sets of landscaping plans are to be submitted to the Executive Director for approval prior to the commencement of work.

Protection of registered fabric

10. To ensure no structural damage takes place to the existing registered buildings as a consequence of the excavation works, all remaining existing registered building fabric that is proposed for retention must be adequately protected during the course of the works to ensure it does not suffer any damage. A structural Engineering report shall be submitted to the Executive Director in relation to proposed excavation basement car parking detailing the work methods, including monitoring, to be employed for the excavation and post excavation

Attachment 7 - Amendment / Former Yorkshire Brewery - 88 Wellington Street and 1 - 21 Robert Street Collingwood - Victoria Heritage Register Number H0807 & Permit P16751 [All Scanned]

Archaeology

11. To ensure any archaeologically significant items within the site are recorded, in order to add to the understanding of the historical development of the site, during the course of the excavation works, a suitably qualified and experienced archaeologist must assess and record any significant deposits, including historical artefacts, that are exposed. The archaeologist is required to submit two copies of the report (one bound and one unbound) to Heritage Victoria at the completion of the project.
12. **Artefact Management:** All portable relics recovered from the site are to be catalogued, stored and conserved to the satisfaction of the Executive Director of Heritage Victoria. The applicant is liable for all expenses arising from the conservation, storage and management of any significant historical archaeological artefacts that are recovered during the works.

Interpretation

13. To aid the understanding of the continued evolution of the site, and the interpretation of its cultural heritage significance for future generations, high quality, professionally designed interpretation signage is to be installed at each public entry to the site, and in the main public open space which explains the significance of the site. A design proposal including proposed images, text, materials and locations and fixings, is to be submitted to the Executive Director for approval. The signage is to be installed on site prior to the practical completion of the redevelopment project.

Recording

14. To ensure an accurate archival record is created of the current condition of the Former Yorkshire Brewery prior to its redevelopment prior to the undertaking of any works to the registered place, a comprehensive archival quality photographic record, in accordance with Heritage Victoria's Technical Note – 2007, Photographic Recording for Heritage Places and Objects (copy attached) shall be undertaken. Two copies of the photographic record are to be submitted to the Executive Director, for approval in writing.

Updated Conservation Management Plan

15. To ensure there is a relevant Conservation Management Plan for the site, an updated Conservation Management Plan is to be submitted to the Executive Director within 6 (six) months of practical completion of the project.

General conditions

16. The Executive Director is to be given five working days notice of the intention to commence the approved works.
17. Approved works or activities are to be planned and carried out in a manner which prevents damage to the registered place / object. However, if other previously hidden original or inaccessible details of the object or place are uncovered, any works that may affect such items shall immediately cease. The Executive Director shall be notified of the details immediately to enable Heritage Victoria representatives to inspect and record the items, and for discussion to take place on the possible retention of the items, or the issue of a modified approval.
18. Should any munitions or other potentially explosive artefacts be discovered, Victoria Police is to be immediately alerted whilst the site is cleared of all personnel.
19. The Executive Director is to be informed when the approved works have been completed.
20. The development approved by this permit is to be carried out in accordance with the endorsed drawings, unless otherwise agreed in writing by the Executive Director, Heritage Victoria.

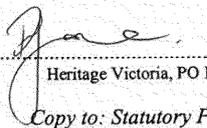
Attachment 7 - Amendment / Former Yorkshire Brewery - 88 Wellington Street and 1 - 21 Robert Street Collingwood - Victoria Heritage Register Number H0807 & Permit P16751 [All Scanned]

NOTE THAT PERMISSION HAS BEEN GIVEN FOR INSPECTIONS OF THE PLACE OR OBJECT TO BE UNDERTAKEN DURING THE CARRYING OUT OF WORKS, AND WITHIN SIX (6) MONTHS OF NOTIFICATION OF THEIR COMPLETION.

TAKE NOTICE THAT ANY NATURAL PERSON WHO CARRIES OUT WORKS OR ACTIVITIES NOT IN ACCORDANCE WITH THE PERMIT OR CONDITIONS IS GUILTY OF AN OFFENCE AND LIABLE TO A PENALTY OF UP TO 2,400 PENALTY UNITS (\$286,680) OR 5 YEARS IMPRISONMENT OR BOTH, OR IN THE CASE OF A BODY CORPORATE 4800 PENALTY UNITS (\$573,360).

THE ATTENTION OF THE OWNER AND/OR APPLICANT IS DRAWN TO THE NEED TO OBTAIN ALL OTHER RELEVANT PERMITS PRIOR TO THE COMMENCEMENT OF WORKS.

Date issued: 20 April 2012
Date Amended: 3 September 2012

Signed  Executive Director
 Heritage Victoria, PO Box 2392 Melbourne VIC 3001

Note: Please check with Heritage Victoria that this Permit is the current permit and can be acted upon.

Copy to: Statutory Planner, Local Government Authority

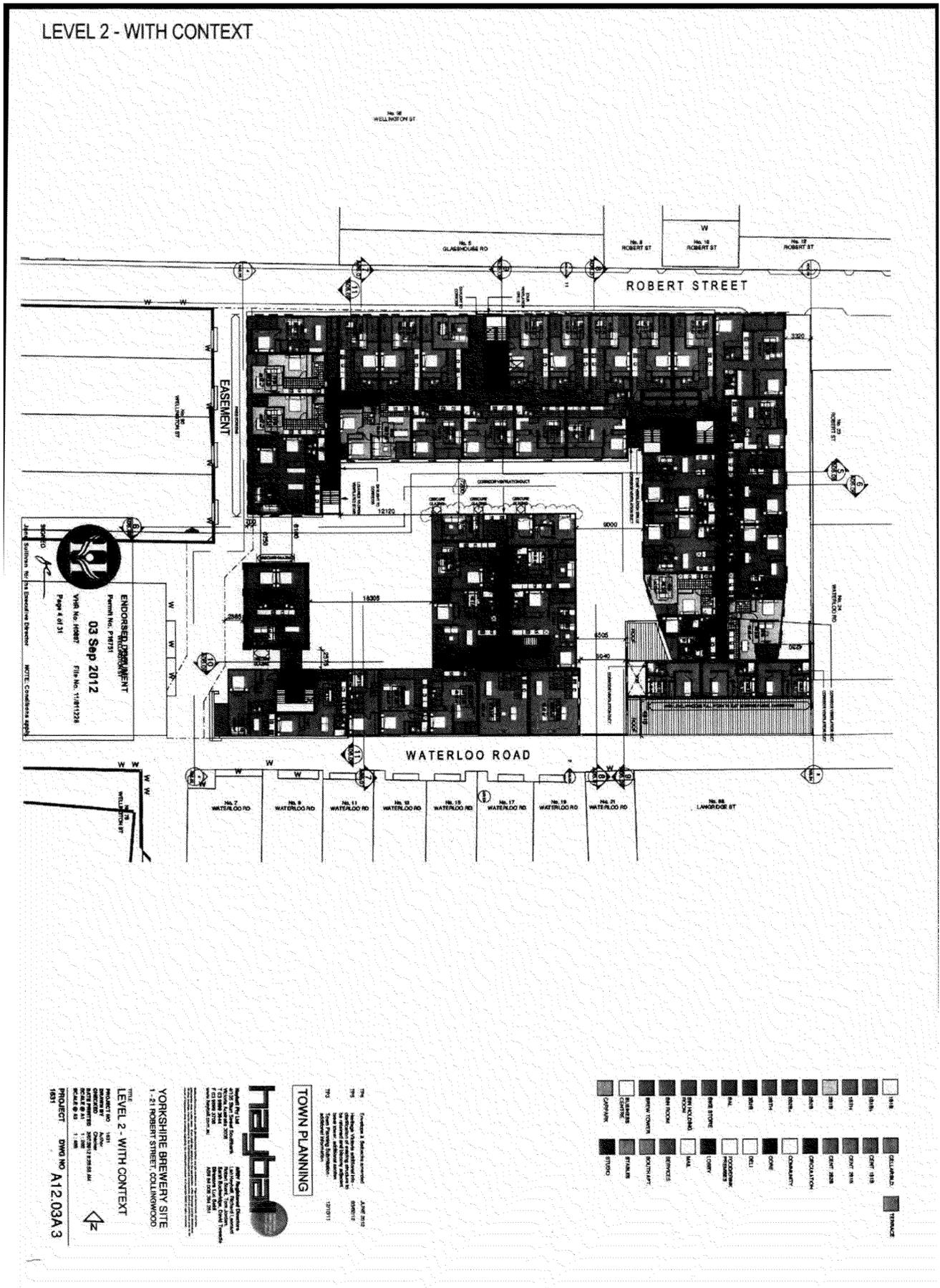
THIS PERMIT HAS BEEN AMENDED AS FOLLOWS:

| Date of amendment | Description of amendment |
|--------------------------|---|
| 3 September 2012 | Amends the permit by endorsing a revised set of 31 drawings prepared by Hayball Pty Ltd dated 29 June 2012 marked A12.01, A12.02A, A12.02B, A12.03A, A12.03B, A12.04A, A12.04B, A12.05A, A12.05B, A12.06A, A12.06B, A12.07A, A12.07B, A12.08A, A12.08B, A12.09A, A12.09B, A12.10A, A06.02, A06.02A, A06.03, A06.03A, A06.04, A06.04A, A06.05, A06.05A, A06.06, A06.06A, A06.07, A06.07A and A06.08. |

Notes:

- (a) The amendments are required to align the heritage permit with the planning permit issued by the City of Yarra. The majority of changes relate to interiors of the new buildings and increasing setbacks to the new apartment buildings.

Attachment 7 - Amendment / Former Yorkshire Brewery - 88 Wellington Street and 1 - 21 Robert Street Collingwood - Victoria Heritage Register Number H0807 & Permit P16751 [All Scanned]

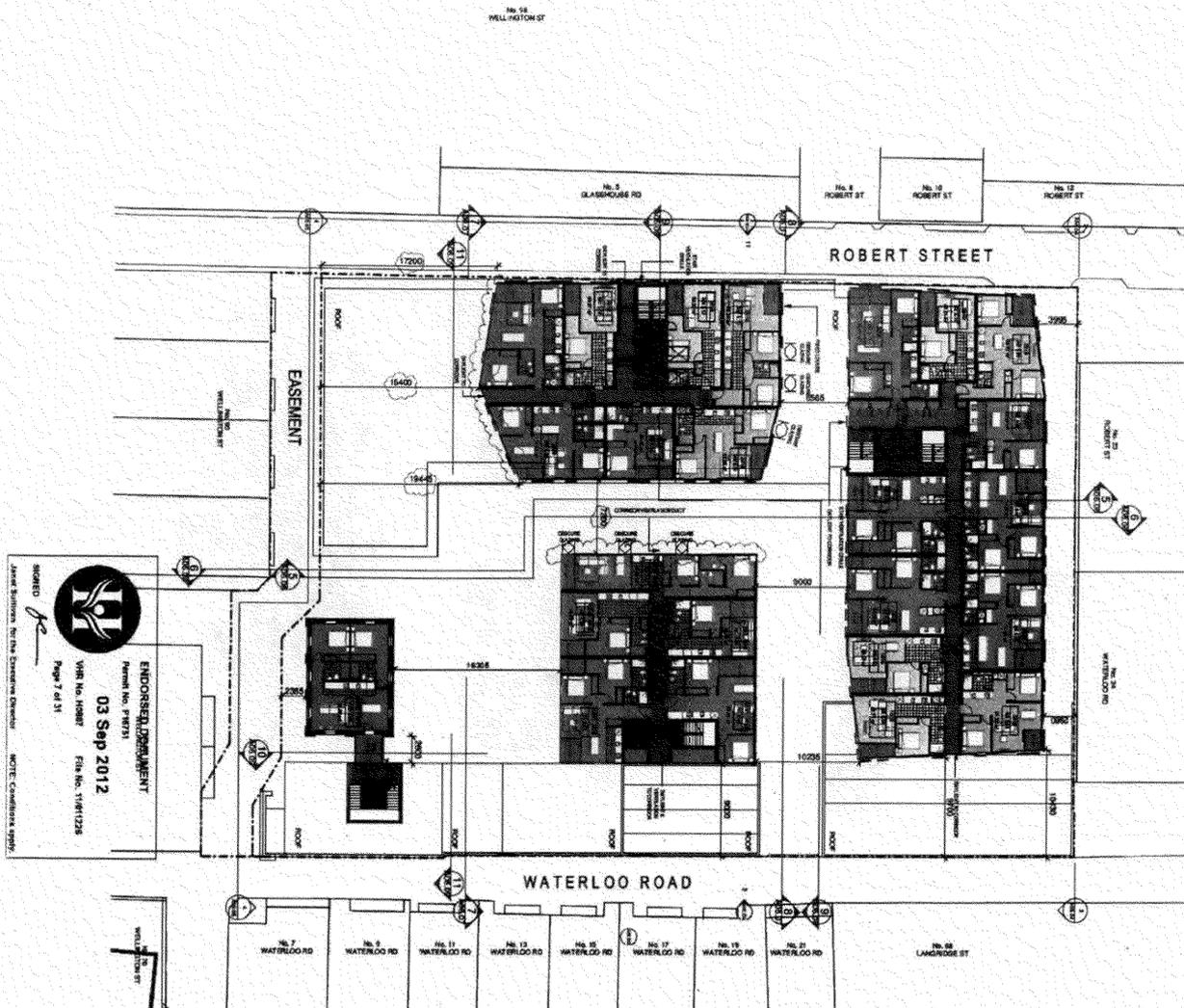


Attachment 7 - Amendment / Former Yorkshire Brewery - 88 Wellington Street and 1 - 21 Robert Street Collingwood - Victoria Heritage Register Number H0807 & Permit P16751 [All Scanned]

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| 1819 | CENT 2018 | CENT 2018 |
| 1820 | CENT 2019 | CENT 2019 |
| 1821 | CENT 2020 | CENT 2020 |
| 1822 | CENT 2021 | CENT 2021 |
| 1823 | CENT 2022 | CENT 2022 |
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| 1826 | CENT 2025 | CENT 2025 |
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| 1830 | CENT 2029 | CENT 2029 |
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| 1832 | CENT 2031 | CENT 2031 |
| 1833 | CENT 2032 | CENT 2032 |
| 1834 | CENT 2033 | CENT 2033 |
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| 1867 | CENT 2066 | CENT 2066 |
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| 1869 | CENT 2068 | CENT 2068 |
| 1870 | CENT 2069 | CENT 2069 |
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| 2015 | CENT 2214 | CENT 2214 |
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| 2017 | CENT 2216 | CENT 2216 |
| 2018 | CENT 2217 | CENT 2217 |
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Attachment 7 - Amendment / Former Yorkshire Brewery - 88 Wellington Street and 1 - 21 Robert Street Collingwood - Victoria Heritage Register Number H0807 & Permit P16751 [All Scanned]

LEVEL 5 - WITH CONTEXT



ENDORSEMENT
 Permit No. P16751
03 Sep 2012
 VHS No. H0807
 File No. V161238
 Page 7 of 31

SHARED
 James Simmons, Project Coordinator
 NOTE: Conditions apply.

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TOWN PLANNING

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YORKSHIRE BREWERY SITE
 1 - 21 ROBERT STREET, COLLINGWOOD

LEVEL 5 - WITH CONTEXT

PROJECT: DWG NO. A12.04B.3

DATE: 03 SEP 2012

PROJECT NO: P16751

FILE NO: V161238

PERMIT NO: P16751

ENDORSEMENT

03 Sep 2012

VHS No. H0807

File No. V161238

Page 7 of 31

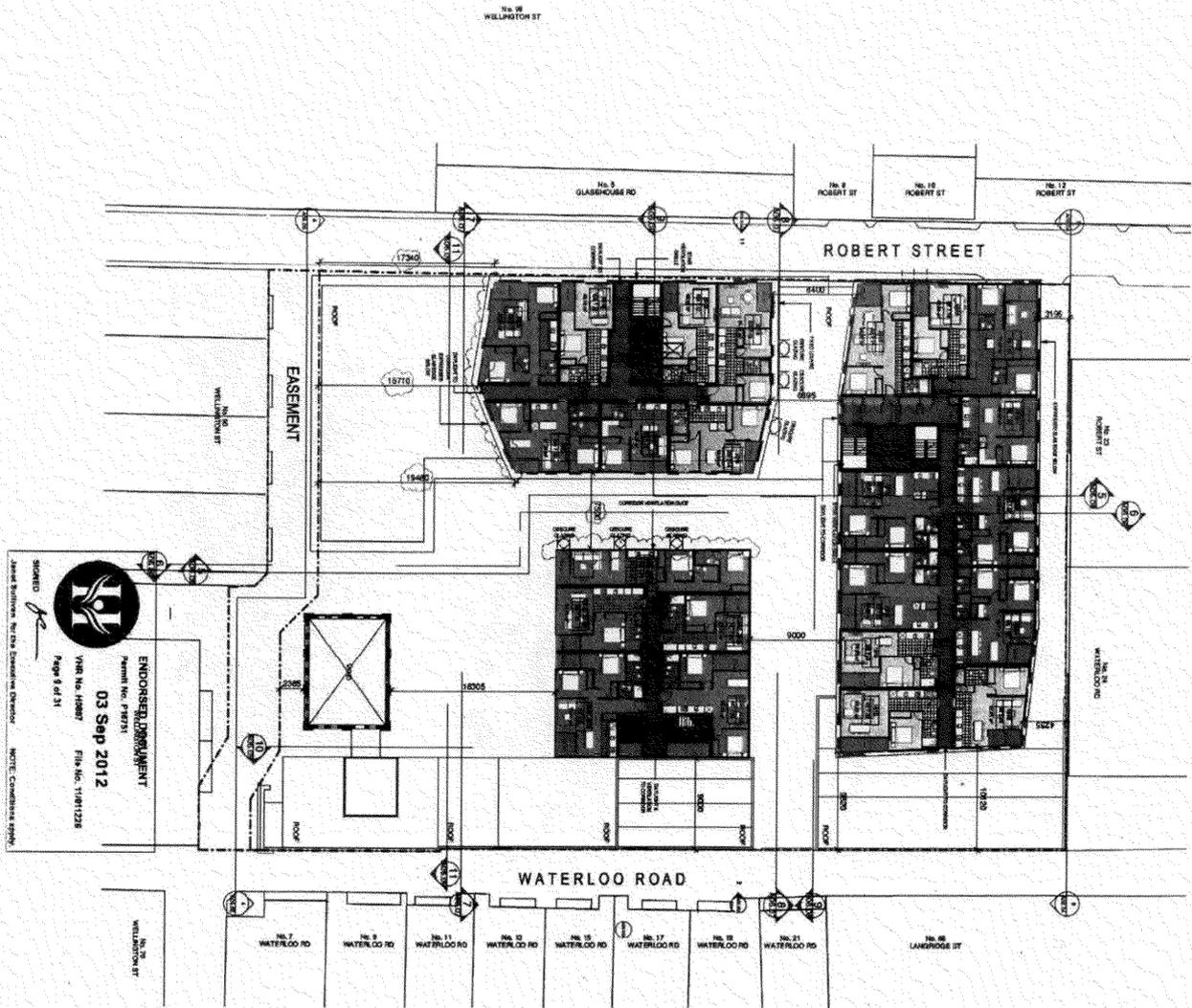
NOTE: Conditions apply.

SHARED

James Simmons, Project Coordinator

Attachment 7 - Amendment / Former Yorkshire Brewery - 88 Wellington Street and 1 - 21 Robert Street Collingwood - Victoria Heritage Register Number H0807 & Permit P16751 [All Scanned]

LEVEL 7 - WITH CONTEXT





 ENDORSED DOCUMENT
 Permit No. P16751
03 Sep 2012
 VNR No. A12052 File No. 1191228
 Page 8 of 21
 SHARED
 JAMES BATHURST, Victorian Planning Inspector NOTE: Commission only

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 Town Planning
 1-21 ROBERT STREET, COLLINGWOOD
 PROJECT: A12 05B 2
 DWG NO: 1651
 SCALE: 1:100
 DATE: 03/09/12
 DRAWN BY: J.B.
 CHECKED BY: J.B.
 PROJECT NO: A12 05B 2
 TITLE: LEVEL 7 - WITH CONTEXT
 YORKSHIRE BREWERY SITE
 1 - 21 ROBERT STREET, COLLINGWOOD
 PROJECT NO: A12 05B 2
 DWG NO: 1651
 SCALE: 1:100
 DATE: 03/09/12
 DRAWN BY: J.B.
 CHECKED BY: J.B.

Attachment 7 - Amendment / Former Yorkshire Brewery - 88 Wellington Street and 1 - 21 Robert Street Collingwood - Victoria Heritage Register Number H0807 & Permit P16751 [All Scanned]

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TYS Engineering & Builders Pty Ltd
 Town Planning
 127/1271

TOWN PLANNING



Hayball
 100/100 Collins Street
 Melbourne, Victoria 3000
 T: 03 9246 3444
 F: 03 9246 3444
 www.hayball.com.au

YORKSHIRE BREWERY SITE
 1 - 21 ROBERT STREET, COLLINGWOOD

TITLE
 LEVEL 9 - WITH CONTEXT

PROJECT NO
 1631
 CHECKED
 JAMES BROWN
 DATE
 20/09/2012
 SCALE
 1:100

PROJECT
 DWG NO

Attachment 7 - Amendment / Former Yorkshire Brewery - 88 Wellington Street and 1 - 21 Robert Street Collingwood - Victoria Heritage Register Number H0807 & Permit P16751 [All Scanned]

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7/16 Evolution & Research completed
 7/17 Town Planning Submission
 12/10/11 additional information

TOWN PLANNING

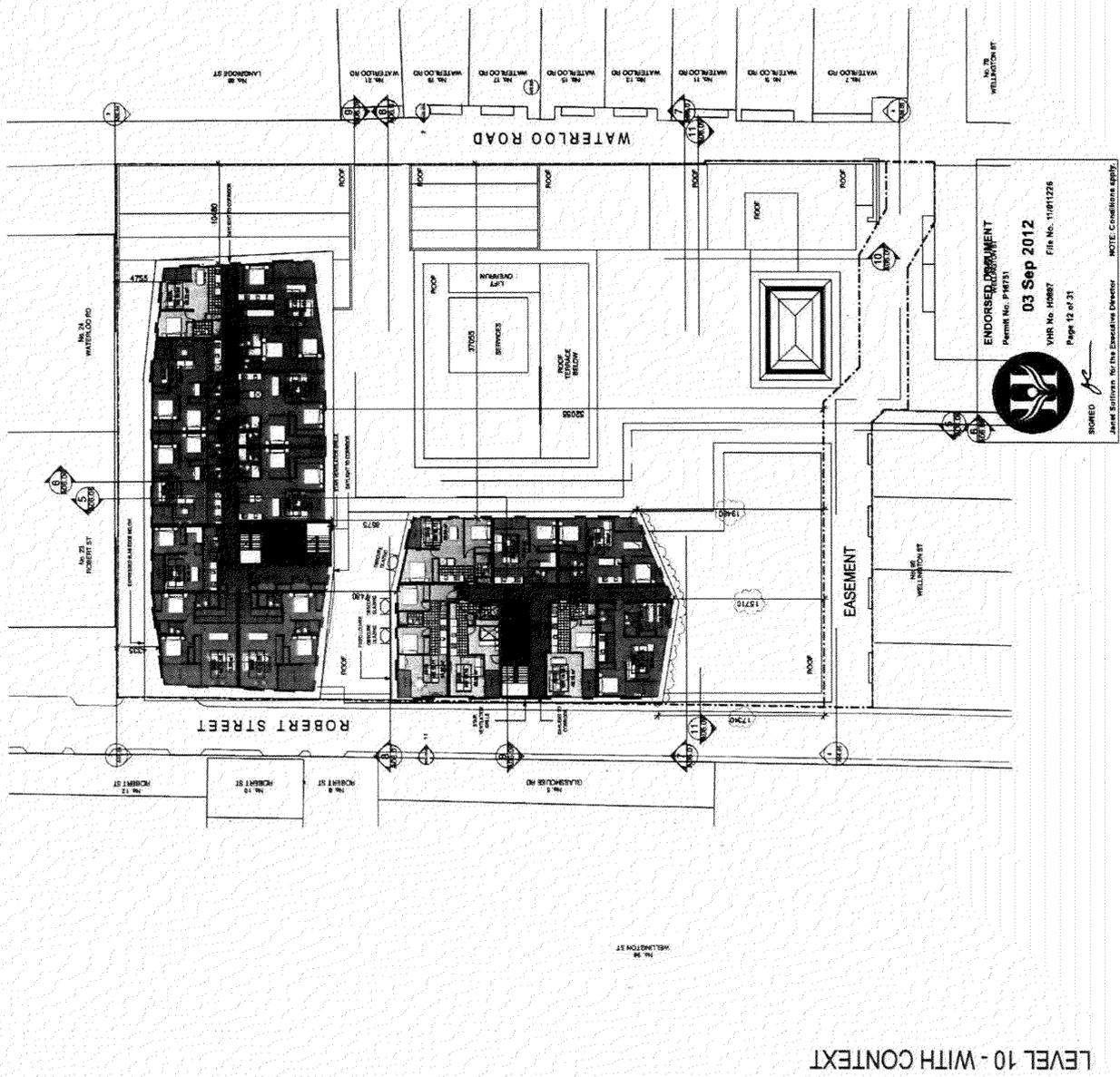


YORKSHIRE BREWERY SITE
 1-21 ROBERT STREET, COLLINGWOOD

THIS LEVEL 10 - WITH CONTEXT

PROJECT NO. 101
 DRAWN BY: A/Arch
 DATE PUBLISHED: 03/09/12 9:19 AM
 SCALE: 1:100

PROJECT: DWG NO. A12.07A.2
 181



ENDORSED/AMENDMENT
 Permit No. P16751
03 Sep 2012
 YHR No. H0807
 File No. 11/01226
 Page 2 of 31

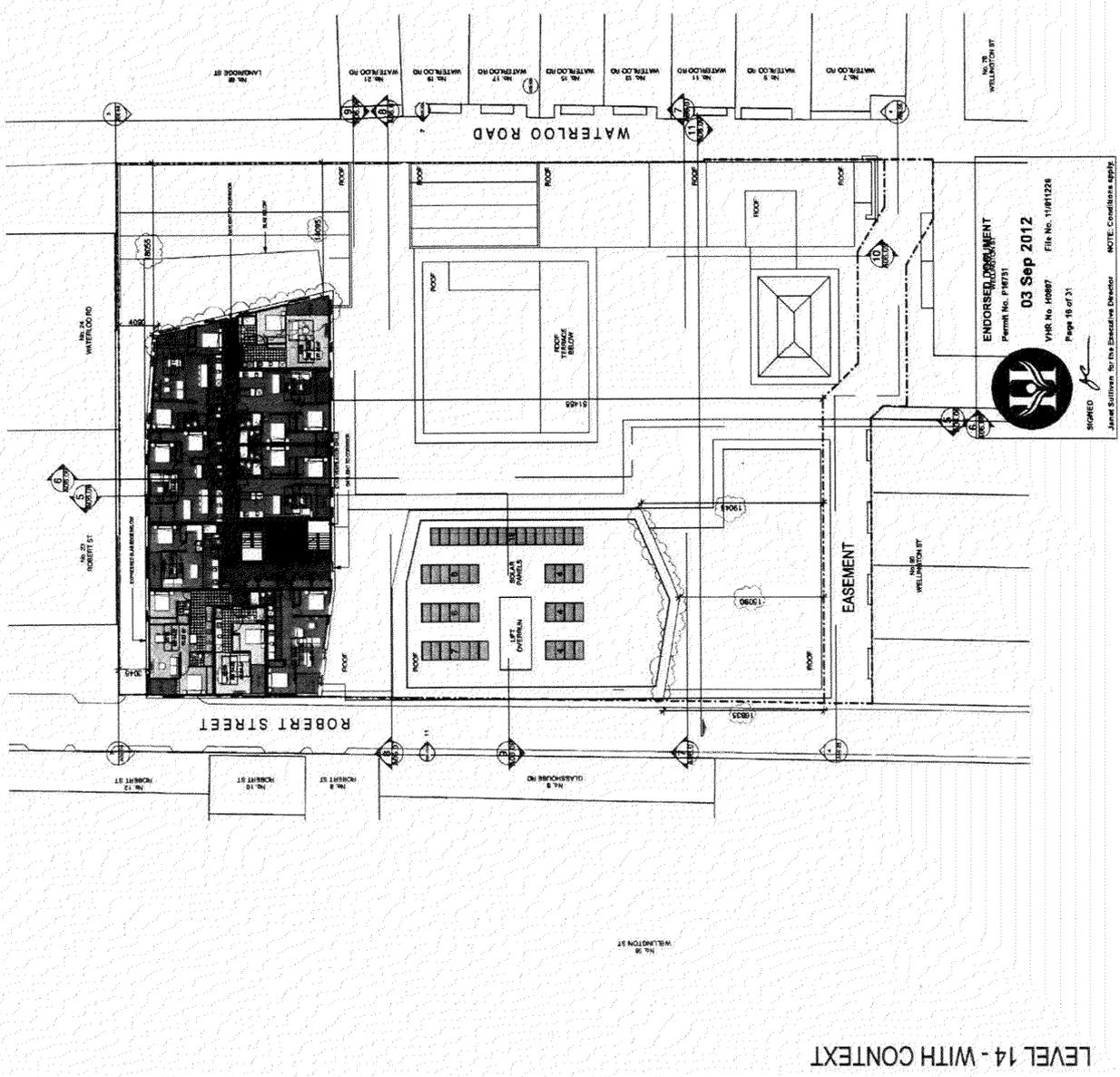
James Sullivan, No the Executive Director
 NOTE: Conditions apply.

LEVEL 10 - WITH CONTEXT

Attachment 7 - Amendment / Former Yorkshire Brewery - 88 Wellington Street and 1 - 21 Robert Street Collingwood - Victoria Heritage Register Number H0807 & Permit P16751 [All Scanned]

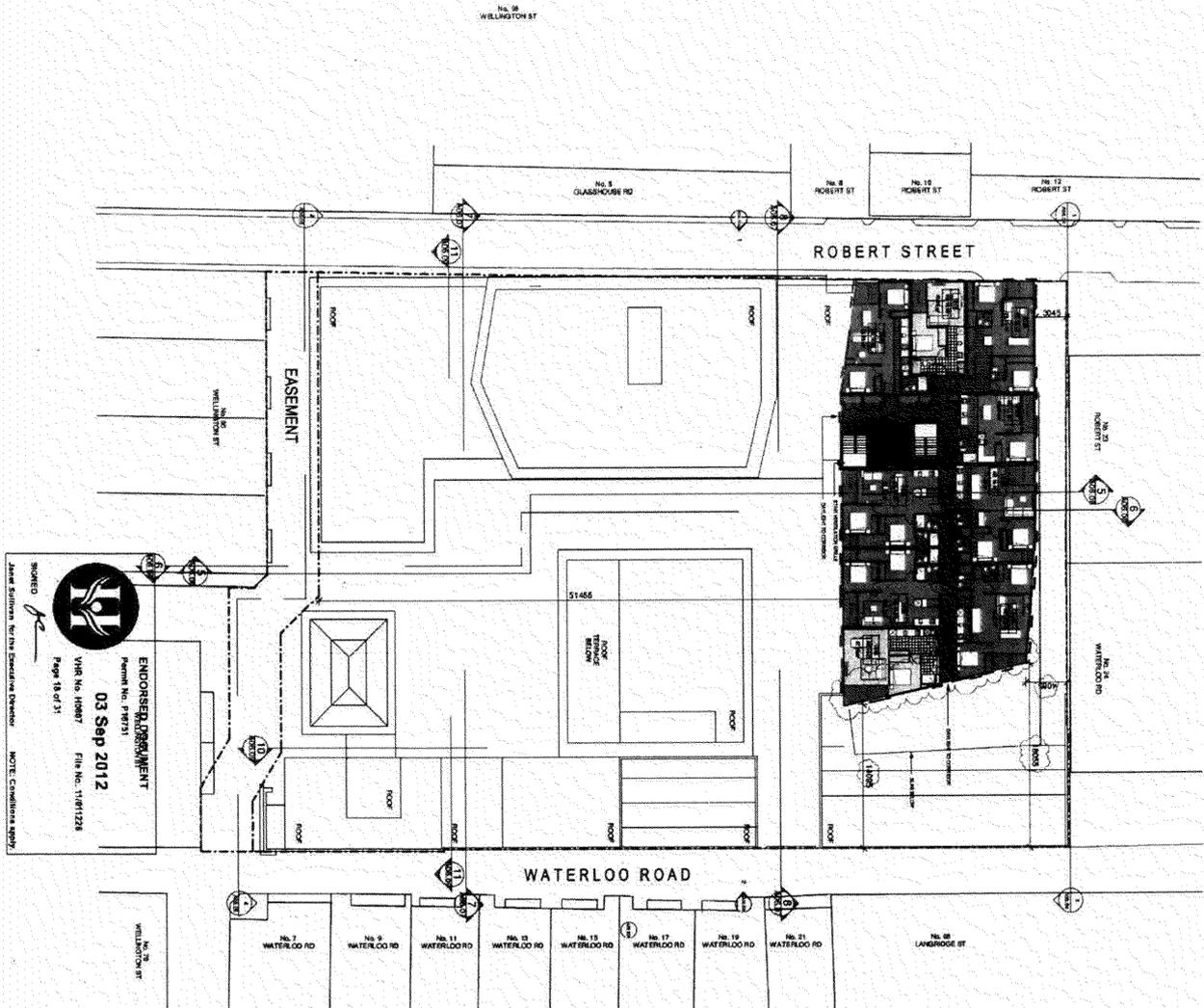
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TOWN PLANNING
 YORKSHIRE BREWERY SITE
 1-21 ROBERT STREET, COLLINGWOOD
 TITLE: LEVEL 14 - WITH CONTEXT
 PROJECT NO: 1031
 DRAWN BY: [Name]
 DATE PLOTTED: 2012/09/11 1:54:09 PM
 SCALE: 1:1000
 PROJECT: DWG NO A12.09A 2
 1631



Attachment 7 - Amendment / Former Yorkshire Brewery - 88 Wellington Street and 1 - 21 Robert Street Collingwood - Victoria Heritage Register Number H0807 & Permit P16751 [All Scanned]

LEVEL 16 - WITH CONTEXT





 ENDORSED DOCUMENT
 Permit No. P16751
03 Sep 2012
 VHR No. H0807 File No. 11611228
 Page 18 of 21
 SHOWN
 James Simmons, Victorian Planning Authority
 NOTE: Conditions apply

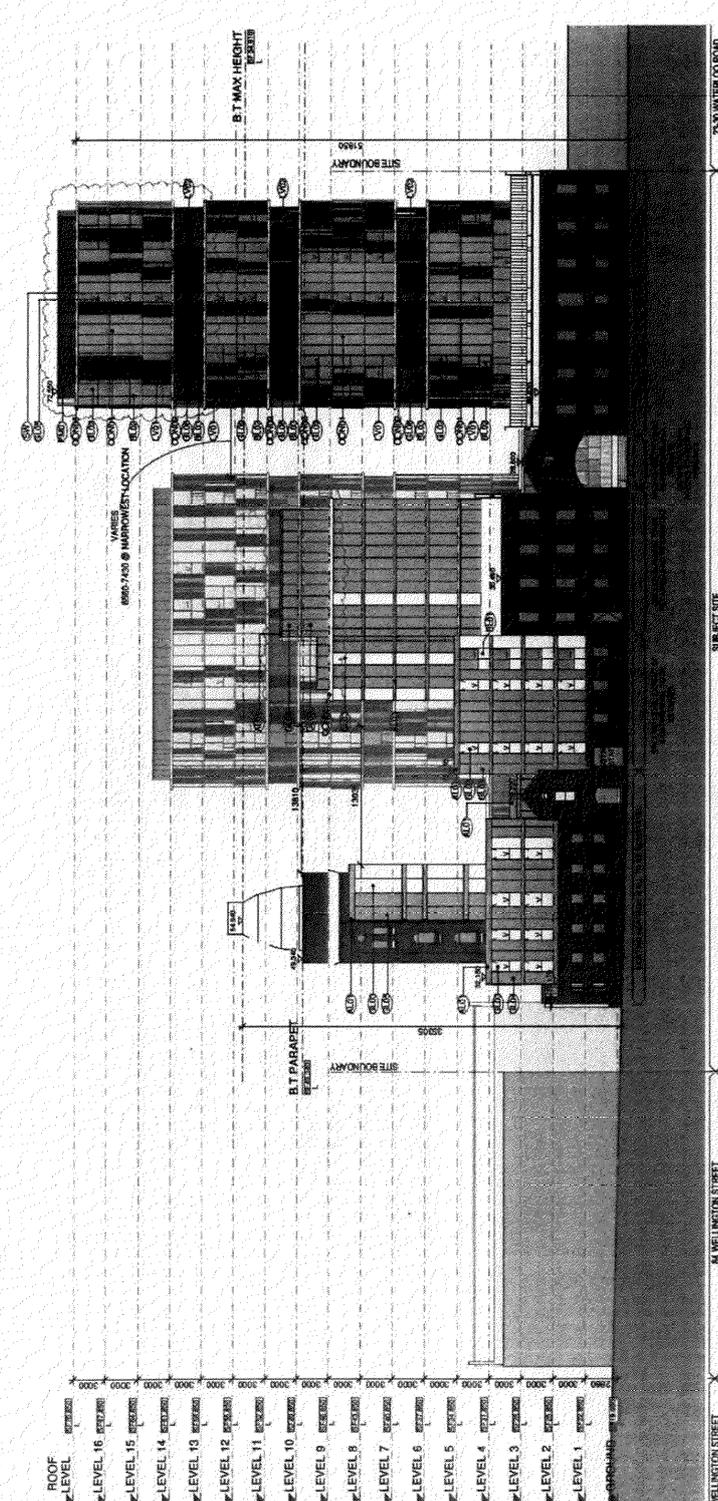
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Haydon
 Architectural & Planning Services
 1-21 Robert Street Collingwood
 Victoria 3068
 Tel: 03 9594 1111
 Fax: 03 9594 1112
 Email: info@haydon.com.au
 www.haydon.com.au

TOWN PLANNING
 THE Victorian Planning Authority
 120 Queen Street
 Melbourne VIC 3000
 Tel: 03 9636 9000
 Fax: 03 9636 9001
 Email: info@vpa.vic.gov.au
 www.vpa.vic.gov.au

PROJECT DWG NO A12.10A.2
 18/11

Attachment 7 - Amendment / Former Yorkshire Brewery - 88 Wellington Street and 1 - 21 Robert Street Collingwood - Victoria Heritage Register Number H0807 & Permit P16751 [All Scanned]



WELLINGTON STREET
 84 WELLINGTON STREET
 SOUTH ELEVATION - WATERLOO ROAD
 1:200

NOTE: REFER TO LOWELL CHEN DRAWINGS FOR INFORMATION DETAILS TO HERITAGE FACADE

EXTERNAL FINISHES LEGEND

- ROOF:
 - 1. ROOF FINISH: CONCRETE
 - 2. ROOF FINISH: ASPHALT
 - 3. ROOF FINISH: TERRAZZO
 - 4. ROOF FINISH: POLISHED CONCRETE
 - 5. ROOF FINISH: POLISHED TERRAZZO
 - 6. ROOF FINISH: POLISHED MARBLE
 - 7. ROOF FINISH: POLISHED GRANITE
 - 8. ROOF FINISH: POLISHED SLATE
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- 174: Enquiries & Submissions Received 04/06/2012
- 175: Enquiries & Submissions Received 12/07/11
- 176: Town Planning Submissions 09/09/11
- 177: Preliminary Issue to Council 01/09/11

TOWN PLANNING



YORKSHIRE BREWERY SITE
 1-21 ROBERT STREET, COLLINGWOOD

TITLE: ELEVATIONS
 PROJECT NO: 1671
 CHECKED: [Signature]
 DRAWN BY: [Signature]
 SCALE: A1
 DATE: 11/09/11
 PROJECT: DWG NO. A06.03 4
 1671

DATE/TIME: 11/09/11 11:00 AM

PROJECT NO: 1671

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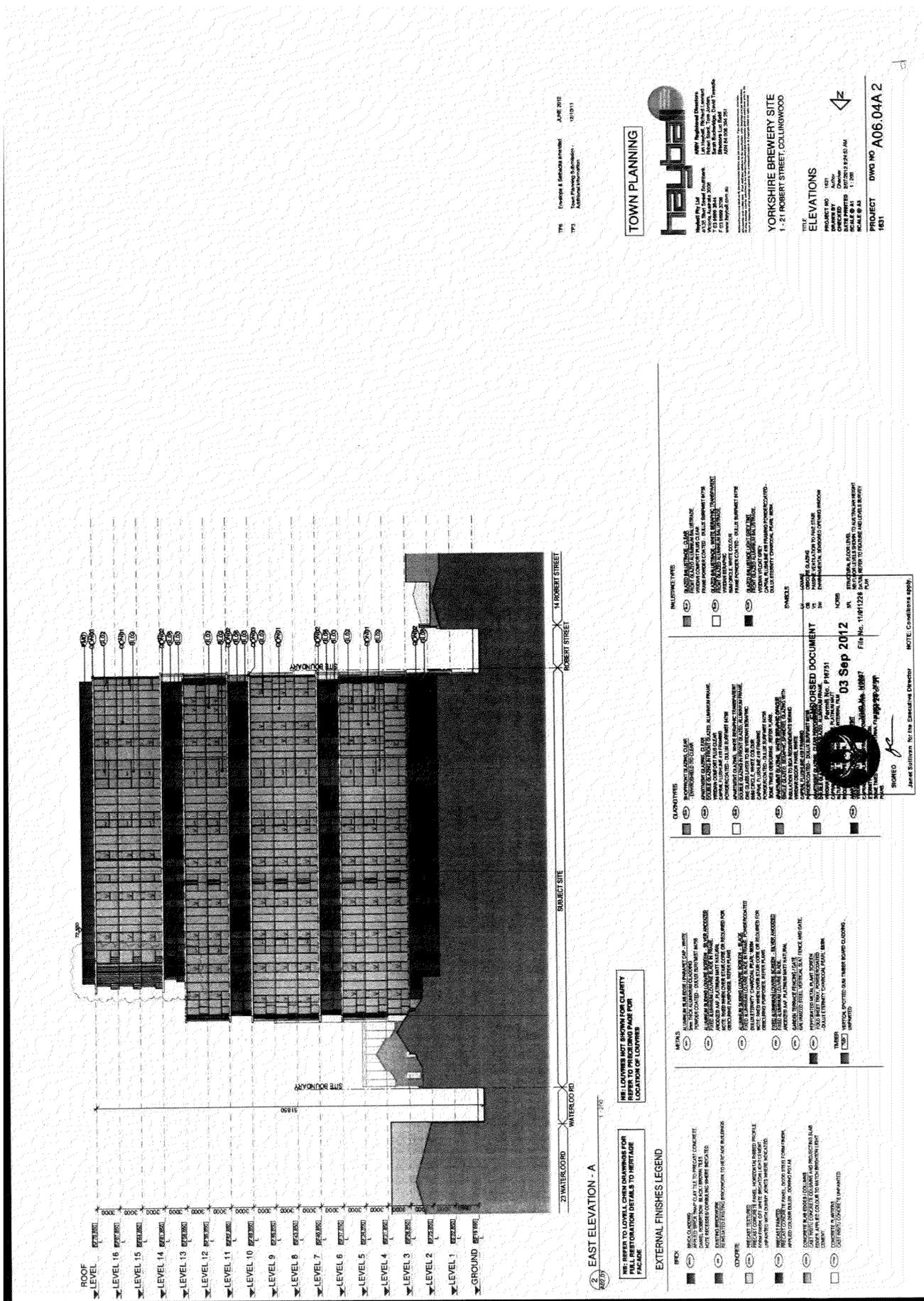
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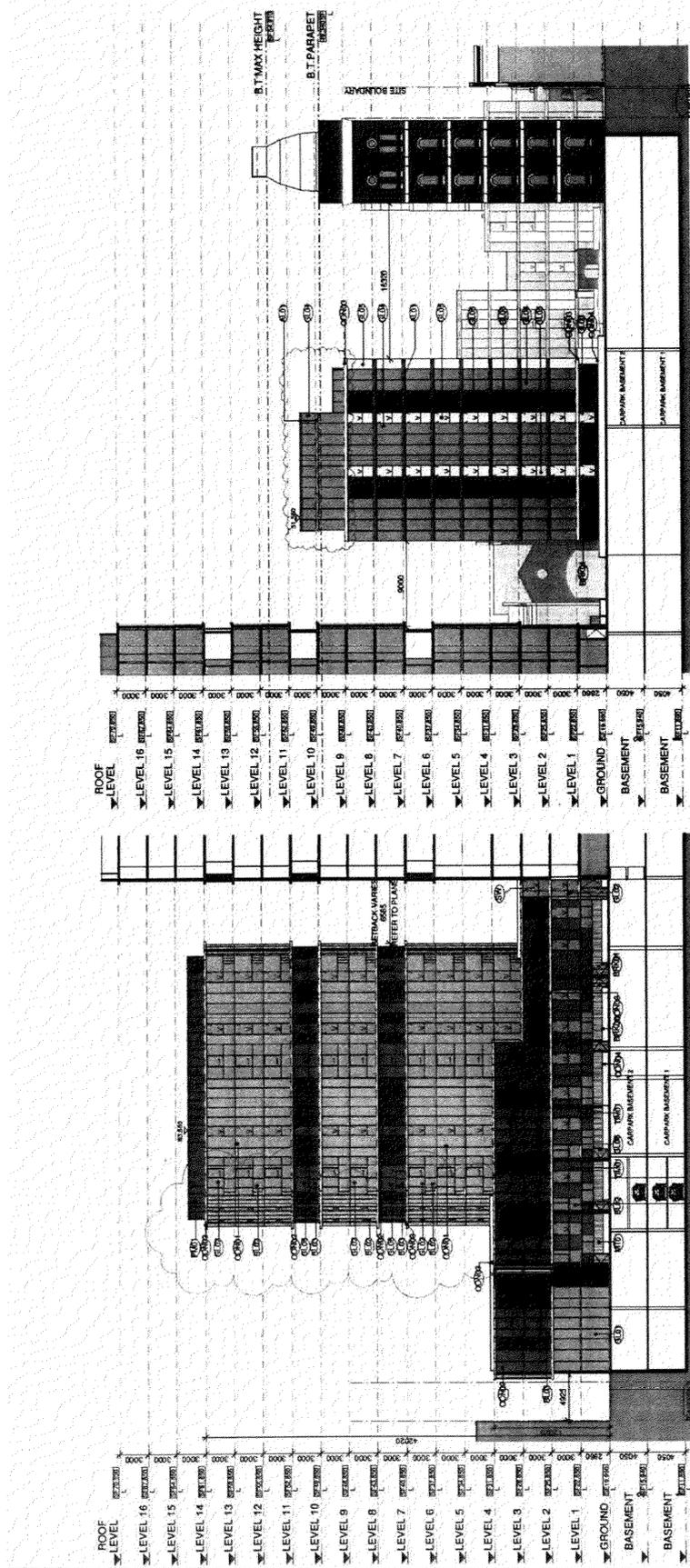
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NOTE: CONSULT WITH THE DESIGNER FOR THE DESIGNER'S COMMENTS.

Attachment 7 - Amendment / Former Yorkshire Brewery - 88 Wellington Street and 1 - 21 Robert Street Collingwood - Victoria Heritage Register Number H0807 & Permit P16751 [All Scanned]



Attachment 7 - Amendment / Former Yorkshire Brewery - 88 Wellington Street and 1 - 21 Robert Street Collingwood - Victoria Heritage Register Number H0807 & Permit P16751 [All Scanned]



TW
 TW3
 Twinn & Seckles Architects
 Town Planning & Architecture
 Adelaide, South Australia
 08 8333 1011

TOWN PLANNING



YORKSHIRE BREWERY SITE
 1 - 21 ROBERT STREET, COLLINGWOOD

TITLE
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 PROJECT NO 1631
 DRAWN BY J. Seckles
 CHECKED BY J. Seckles
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 SCALE @ A3
 PROJECT DWG NO A06-06A 2
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3 INTERNAL NORTH ELEVATION - A

1 INTERNAL SOUTH ELEVATION - A

SEE ELEVATIONS AND DRAWINGS FOR CLARITY
 REFER TO PLOTTING TABLE FOR
 LOCATION OF LOBBIES

SEE ELEVATIONS AND DRAWINGS FOR CLARITY
 REFER TO PLOTTING TABLE FOR
 LOCATION OF LOBBIES

EXTERNAL FINISHES LEGEND

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FOR MORE INFORMATION CONTACT
 JAMES SULLIVAN
 03 Sep 2012
 File No. 11811226



James Sullivan for the Executive Director

NOTE: Conditions apply.

10.2 Economic Development Strategy 2009 - 2014 Annual Progress Report Year 3

Trim Record Number: D12/61331

Responsible Officer: Director City Development

“The recommended decision is not a “Major Policy Decision”, as defined in section 93A of the Local Government Act 1989, or a “Significant Decision” within the meaning of the Code of Conduct”.

Purpose

1. To report back to Council on the progress of the implementation of the Yarra Economic Development Strategy 2009-2014 and the progress of the Year 2 implementation of the Tourism Strategy for the City of Yarra 2011-2016.

Background

2. Yarra's Economic Development Strategy 2009-2014 was adopted by Council in October 2009 with annual reports to be provided to Council on the progress of its implementation.
3. The Tourism Strategy 2011-2016 was adopted by Council in October 2011 with progress reports to be included in the annual report on the Economic Development Strategy 2009-2014.
4. The actions identified in the key strategic directions respond to the challenges and opportunities identified through the development of the Strategy.

Progress of Implementation

5. The Strategy outlines 57 actions for implementation over its five year term.
6. Attachment 1 provides the status and summary of the implementation of the actions nominated in the Strategy. It also provides a summary of the progress of the Year 2 implementation of the Tourism Strategy for the City of Yarra 2011-2016. In summary:
 - (a) 43 actions are ongoing and all are in progress;
 - (b) 11 actions were due for completion in 2010. Eight have been completed and the remaining 3 have commenced or are considered ongoing; and
 - (c) 3 actions are annual and are in progress.
7. The implementation of the Strategy is progressing well. The following lists some key achievements:
 - (a) strong and robust relationships formed with trader groups;
 - (b) active engagement of Business Advisory Group members in projects;
 - (c) high attendance rates in BRAINfoodforBusiness events program;
 - (d) positive feedback from local business in relation to support received from the Economic Development team;
 - (e) partnership established with the Textile and Fashion Industries Australia and Kangan Institute in the promotion of the Kangan Fashion Hub and Textile and Fashion Precinct;
 - (f) establishment of a Women's Business Network;
 - (g) introduction of Expression of Interest process seeking Yarra nominations for the Northern Business Achievement Awards;
 - (h) IMAP student placement project that aims to build on the business survey introduced in 2011;
 - (i) extension of Gertrude projection festival into Smith Street;

- (j) broad representation of Licensees at Yarra's Licensees Forums;
- (k) Yarra's Urban Design Strategy and Business Industrial Land Strategy endorsed by Council;
- (l) \$1.5 million pledged from Federal and State Governments for the Victoria Street Gateway Project;
- (m) record number of entries for the Yarra Sustainability Awards Sustainable Business Category;
- (n) collaborative procurement of on line demographic, forecast and economic tools;
- (o) an additional 450 businesses subscribed to Yarra's online business directory;
- (p) establishment of a comprehensive marketing and communications plan;
- (q) positive survey results received from participants attending the BRAINfoodforBusiness events;
- (r) development of a five year Marketing Campaign for Bridge Road;
- (s) development of a Yarra Education Youth Commitment (YEYC) Strategic Plan;
- (t) 47 representatives from local business and secondary schools at the inaugural Yarra's Business/School Breakfast Roundtable;
- (u) engagement of business in broad range of tourism initiatives;
- (v) YEYC Spirit of Cooperation achieving over 90 signatories from community sector and school;
- (w) international recognition of Yarra's Tourism Strategy as a best practice case study - Kendall/Hunt Publishing is preparing a new Tourism Textbook "The Tourism System" for distribution to college students across USA and Canada.

8. It is also noted that significant demands on Council officer resourcing have been directed towards responding to issues raised by traders and general business enquiries.

Consultation

- 9. There has been a considerable level of consultation with Council officers across different units of Council in delivering actions from the Strategy.
- 10. Council's Business Advisory Group, Business Associations, the business community, government bodies, industry associations and education providers have been consulted in delivering actions from the Strategy.

Financial Implications

- 11. Implementation of the nominated actions in the Strategy requires a commitment of monetary and human resources for the duration of the Strategy.

Economic Implications

- 12. While there are no economic implications directly associated with this report, the Strategy provides Council with a clear vision and strategic principles to advance the City's economy with actions outlined for:
 - (a) transport infrastructure that reduce traffic congestion and improve access to employment, goods and services;
 - (b) the development of the City's key employment precincts in a proactive and coordinated way to provide an integrated and diverse economic base;
 - (c) achieving a high awareness amongst the business community, both of Council services and Yarra as a place to invest;
 - (d) developing a greater match between the skills of the local workforce and the skills required by the local community;
 - (e) promotion of Yarra as a visitor destination;

- (f) development of the City's key employment precincts in a proactive and coordinated way to provide an integrated and diverse economic base; and
 - (g) engagement at a regional level in the delivery of economic development issues.
13. Yarra's Business Industrial Land Strategy sets a land use planning direction for Yarra's business and industrial areas and gives effect to a precinct typology as the basis for exploring development and investment opportunities. The focus of this Strategy is on employment and economic activity. It is noted that the proposed zone changes announced midyear by the Minister for Planning, however, raises uncertainties that need to be worked through.

Sustainability Implications

14. The Economic Development and Environment Teams have regular contact with the Yarra Energy Foundation (YEF) to identify opportunities for promoting programs that help local businesses. The CitySwitch program was initiated by Council before being handed over to YEF.

Social Implications

15. There are many benefits for Yarra's broader community associated with the implementation of actions outlined in the Strategy.
16. The Strategy nominates actions which will:
- (a) provide high quality, vibrant, safe and clean streetscapes that reflect the unique character of each economic precinct;
 - (b) improve access of businesses to information and assistance to facilitate growth;
 - (c) develop and maintain a supportive investment culture through the provision and facilitation of a comprehensive training and networking program; and
 - (d) achieve a high awareness amongst the business community both of Council's services and Yarra as a place to invest.

Human Rights Implications

17. There are no known restrictions or infringements within the draft Tourism Strategy on the substantive rights outlined in the *Charter of Human Rights and Responsibilities Act 2006*.

Council Plan, Strategy and Policy Implications

18. The Council Plan 2009-2013 commits to the implementation of actions from the Strategy, with the aim of enhancing the economy and meeting the needs of the business community, institutions and residents.

Legal Implications

19. There are no known legal implications of this report.

Other matters

20. There have been significant additional unplanned demands placed on Council officer resourcing in responding to the legal process associated with the Queens Parade Special Charge Scheme, VCAT appeal and its subsequent discontinuance.

Conclusion

21. The implementation of the Economic Development Strategy is progressing well with strong support from Yarra's business community, government bodies, industry associations and education providers.

RECOMMENDATION

1. That Council:
 - (a) notes the second implementation progress report for the Economic Development Strategy 2009-2014; and the Tourism Strategy 2011-2016; and
 - (b) continues to support the delivery of actions from the Economic Development Strategy 2009-2014.
2. That officers report back to Council on the progress of the implementation of the Yarra Economic Development Strategy 2009-2014 in November 2013.

CONTACT OFFICER: Kim Swinson
TITLE: Coordinator Economic Development
TEL: 9205 5303

Attachments

- 1 Economic Development Strategy Council Report Year 3 Report August 2012
- 2 Tourism Strategy 2011-2016 Year 2 Implementation

Attachment 1 - Economic Development Strategy Council Report Year 3 Report August 2012

Economic Development 2009-2014 Year 3 Implementation

1.1 DECISION MAKING & REGULATIONS

| Objectives & Actions | Responsibility | Timeline | Status | Measure/Outcome Sought |
|--|----------------|----------|----------|--|
| Action 1 Improve the web-based single reference point for regulation services for business that also provides continuous improvements to the local regulatory framework for business. | SEP, BR&S, SP | Mid 2010 | Complete | Implemented in 2010 |
| Action 2 Implement a system for efficient and timely consideration of applications for planning permits, including a fast track system for minor applications. | SEP, SP | 2009 | Complete | Implemented in 2010 |
| Action 3 Clearly identify key (re)development sites and establish a proactive approach to their future development in partnership with landowners. | SEP | Mid 2010 | Complete | Implemented in 2010 |
| Action 4 Ensure all future strategic planning relating to economic precincts around the City embrace the principles of the Economic Development Strategy. | SEP | Ongoing | Ongoing | Council's Strategic Planning Unit is familiar with the Economic Development Strategy 2009-2014. Economic implications are identified and considered as part of reporting to Council. |
| Action 5 Implement the recommended findings of the Inner City Entertainment Precinct Taskforce 2006 on residential and business interface issues. | DCD | Ongoing | Ongoing | <p>IMAP's final Report delivered to the IMAP Implementation Committee concluded that the work undertaken out of the IMAP Action 6.3 has developed a good base for Council's further thinking and progression of the aspects which have been important to their local circumstances. A meeting was held between IMAP committee members Bruce Phillips, G Lawler, E McElroy and VCGLR Commissioner K Hamond, E Sanders, L Bertolini, S Admington on 3 May 2012 at the VCGLR offices Richmond, to discuss matters regarding liquor licences and planning provisions – improved coordination amongst the agencies including Police is being sought.</p> <p>Specifically, the City of Yarra has undertaken the following initiatives:</p> <ul style="list-style-type: none"> • Conducting a second Review in 2011 of its Local Law (No8) relating to restriction of drinking in public places; • Supporting the Yarra Licensees Forum with clear Terms of Reference being established to enable a closer working relationship with traders to better understand the local issues with alcohol and the role Licensees can play in addressing these; • Continued collaboration between local services, Council and State Government agencies to engage people with long-term alcohol addiction; • Planning for and supporting vibrant, diverse and safe night time activities in the City of Yarra through the development of an evidence-based strategy. |
| Action 6 Continue to develop and coordinate a range of strategic land use, environmental and issue specific strategic plans to clearly guide the City's physical and economic growth and inform the Council Plan. | All of Council | Ongoing | Ongoing | <p>Council has adopted:</p> <ul style="list-style-type: none"> • Business and Industrial Strategy 2012 • Urban Design Strategy 2012 <p>Council is in the process of developing:</p> <ul style="list-style-type: none"> • Draft Swan Street Structure Plan • Draft Johnston Street Local Area Plan • East Richmond Station – Proposed Precinct Improvements |

Attachment 1 - Economic Development Strategy Council Report Year 3 Report August 2012

1.2 STREETS CAPES

| Objectives & Actions | Responsibility | Timeline | Measure/Outcome Sought |
|--|------------------------|-----------------|---|
| <p>Action 1</p> <p>Design, develop, maintain and manage the hard infrastructure supporting economic precincts to best practice standards for footpaths and street furniture, landscaping, and lighting</p> | <p>ES, SEP, I, EO</p> | <p>Ongoing</p> | <p>The following improvements have been undertaken in year 2011-12:</p> <ul style="list-style-type: none"> • Refurbished the streetscape for the junction of Webb/Smith Streets - including new seating paving, street furniture and improved lighting • New seats installed along Johnston Street • Design concepts prepared for Victoria Street Gateway • Detailed designs prepared for East Richmond Station Precinct • Way finding plans and designs prepared for Spensley Village • Detailed design for Post Office Lane off Victoria Street • Design concept for St Heliers St Abbotsford |
| <p>Action 2</p> <p>Ongoing cleaning of retail shopping strips.</p> | <p>EO</p> | <p>End 2011</p> | <p>Council maintains a cleaning program for retail shopping strips. Council officers have consulted with Yarra's Business Association's throughout the year.</p> |
| <p>Action 3</p> <p>Ongoing management of the adopted Yarra Footpath Trading policy that activates the appropriate use of public spaces.</p> | | | |
| <p>Action 4</p> <p>Actively implement Council's graffiti and bill posting management policy and partner with the business community to better manage the private domain</p> | <p>BRS EO, BRS</p> | <p>Ongoing</p> | <p>Council's graffiti and bill posting management policy has been implemented through the provision of the following services:</p> <ul style="list-style-type: none"> • Graffiti Removal Kits - free kits make it easy for residents and business owners to remove tags or smaller pieces of graffiti from their own property. • Paint Out - in partnership with local paint suppliers, offer a discount on exterior paint for the purposes of removing graffiti • Rapid Removal - remove graffiti from any private property or business within Yarra, subject to the owner's consent • Programmed Cleaning - continuously undertaking clean-ups of high profile locations, such as retail centres and community hubs • Graffiti removal training • Supplying high pressure water cleaners • Extension of the bill poster pole program with installations in Otter and Kerr Streets. |
| <p>Action 5</p> <p>Review and implement coordinated employment precinct public identification, gateway, and precinct branding and continue to support and ensure that public advertising signage reflects the character of economic precincts</p> | <p>SEP, EO</p> | <p>End 2010</p> | <p>The Victoria Street Gateway Project has received joint funding from the Federal and State governments of approximately \$1.5 million towards the gateway's construction, while Council has committed \$500,000 in funding and in-kind support towards the gateway project.</p> |

Attachment 1 - Economic Development Strategy Council Report Year 3 Report August 2012

1.3 TRANSPORT

| Objectives & Actions | Responsibility | Timeline | Ongoing | Measure/Outcome Sought |
|---|----------------|----------|----------|---|
| <p>Action 1</p> <p>Design, develop, maintain and manage the hard infrastructure supporting economic precincts to best practice standards to maintain safe, convenient transport access</p> | I, ST, PS | Ongoing | Ongoing | <p>Council has undertaken the following initiatives to improve hard infrastructure:</p> <ul style="list-style-type: none"> • 40km/h area wide speed limit for North Carlton precinct that includes Rathdowne Street • 40km/h area wide speed limit for Smith Street between Gertrude St and Victoria Parade • Kerb extensions for improved pedestrian accessibility at Rathdowne/Newry Streets • New bus shelters along Johnston St near Tenery Cr, Hoddle St and Smith Street • 20km/h shared zone along Berry and Eucalyptus Streets • Bridge Road Tram Stop Improvements - community consultation • Victoria Street, Richmond – concept designs for Gateway project • East Richmond Station precinct - design plan • Brunswick Street bicycle parking facility improvements . |
| <p>Action 2</p> <p>Link transport routes to provide accessibility throughout Yarra by walking, cycling and public transport as the preferred means for the movement of people</p> | ST, SEP | Ongoing | Ongoing | <p>Bicycles - Constructed the Elizabeth Street route between Hoddle street and Church Street. In mid-2011, Yarra Council constructed a new bicycle route connecting Heidelberg Road in Clifton Hill with Cljps Street in Abbotsford.</p> |
| <p>Action 3</p> <p>Lobby for improved frequencies to the public transport network</p> | ST, SEP | Ongoing | Ongoing | <p>Council working with Department of Transport and VicRoads to improve the existing tram facilities along the Bridge Road, Richmond.</p> |
| <p>Action 4</p> <p>Implement improved way-finding signage and information regarding the availability of motor vehicle and bicycle parking facilities</p> | I, ST, SEP | Ongoing | Ongoing | <p>Council participated in consultation process facilitated by DOT regarding route 96 along Nicholson Street, and Metro rail service linking the Melbourne, Doncaster and Ringwood activity centres and the proposed new metro station at Carlton/Parkville.</p> |
| <p>Action 5</p> <p>Explore opportunities to promote greater use of alternative non car based transportation for the movement of people, goods and services</p> | ST, SEP | 2010 | complete | <p>As member of the Metropolitan Transport Forum (MTF), promoting effective, efficient and equitable transport in metropolitan Melbourne by providing a forum for debate, research and policy development, and by disseminating information to improve transport choices. An audit of existing IMAP signs was undertaken. Damaged signs were repaired. One new sign has been manufactured which improves on the earlier design and takes care of regular maintenance issues. Completed 2011</p> |

Attachment 1 - Economic Development Strategy Council Report Year 3 Report August 2012

1.4 ENVIRONMENT SUSTAINABILITY

| Objectives & Actions | Responsibility | Timeline | Measure/Outcome Sought |
|---|---------------------|-----------------|--|
| <p>Action 1</p> <p>Review and improve waste management services supporting businesses to improve access to and use of waste recycling practices</p> | <p>EO, ES, SEP</p> | <p>End 2011</p> | <p>Implementation of Yarra's Waste Management Plan 2009-2014 including:</p> <ul style="list-style-type: none"> • cleaning every major shopping strip daily and other residential streets, on average, once every 4 weeks. • all street litter bins in shopping strips emptied daily. • working to reduce the amount of waste collected in Yarra being sent to landfill. • investigation of reports of litter and dumped waste for prosecution within four hours of it being reported. • Exploring a waste/litter reduction program with Bridge Road Main Street Association and Swan Street traders • worked with Cultivating Communities to implement the Compost Mates program into Queens Parade • 12 monthly calendar for a mobile waste paper and cardboard collection in commercial areas <p>Use the established Litter Task Force to integrate education, infrastructure, and enforcement, and target litter hot-spots in a strategic manner, within the framework of Lower Yarra Litter Strategy. (Environment Strategy Action)</p> |
| <p>Action 2</p> <p>Encourage environmentally sustainable building designs.</p> | <p>SP, ES, SEP,</p> | <p>Ongoing</p> | <p>Implementation of Yarra's Waste Management Plan 2009-2014 including:</p> <ul style="list-style-type: none"> • cleaning every major shopping strip daily and other residential streets, on average, once every 4 weeks. • all street litter bins in shopping strips emptied daily. <p>Council regularly promotes environmental sustainability to businesses through the following avenues:</p> <ul style="list-style-type: none"> • "Sustainable Business" on Council's website • Yarra Business e-bulletin • Yarra Environment e-bulletin • Information stand at the Richmond Town Hall • Information at BRAI/InfoforBusiness seminars, events and workshops • Ralph Parre director of Ferguson Parre bakeries was a keynote speaker at the Yarra Business Networking Breakfast in March 2012 sharing his story on how he turned his business into a sustainability success story. • Yarra Sustainability Awards include a Sustainable Business category. There was a record number of entries for this category in 2012. Economic Development participated on the 2012 selection panel for the Yarra Sustainability Awards 2012 Business Category. • The Yarra Energy Foundation (YEF) was established in 2010 with a goal of supporting the municipality to achieve zero emissions by 2020. YEF has responsibility for running programs and supporting business in Yarra to reduce emissions and help meet the target. • The Economic Development and Environment Teams have regular contact with Yarra Energy Foundation to identify opportunities for promoting programs that help local businesses reduce their carbon emissions. • Supporting the Yarra Energy Foundation to coordinate the CitySwitch program with Yarra businesses. The CitySwitch program was initiated by Council before being handed over to YEF |
| <p>Action 3</p> <p>Work with businesses and trader groups to develop, promote and deliver sustainability programs for the small to medium enterprise sector including the Lower Yarra Litter Strategy program and facilitate a reduction in greenhouse emissions from the commercial and manufacturing sector. Support the programs with materials to guide sustainable business practices and ensure materials are available to all new and existing traders.</p> | <p>ES, SEP, EO</p> | <p>Ongoing</p> | <p>Council regularly promotes environmental sustainability to businesses through the following avenues:</p> <ul style="list-style-type: none"> • "Sustainable Business" on Council's website • Yarra Business e-bulletin • Yarra Environment e-bulletin • Information stand at the Richmond Town Hall • Information at BRAI/InfoforBusiness seminars, events and workshops • Ralph Parre director of Ferguson Parre bakeries was a keynote speaker at the Yarra Business Networking Breakfast in March 2012 sharing his story on how he turned his business into a sustainability success story. • Yarra Sustainability Awards include a Sustainable Business category. There was a record number of entries for this category in 2012. Economic Development participated on the 2012 selection panel for the Yarra Sustainability Awards 2012 Business Category. • The Yarra Energy Foundation (YEF) was established in 2010 with a goal of supporting the municipality to achieve zero emissions by 2020. YEF has responsibility for running programs and supporting business in Yarra to reduce emissions and help meet the target. • The Economic Development and Environment Teams have regular contact with Yarra Energy Foundation to identify opportunities for promoting programs that help local businesses reduce their carbon emissions. • Supporting the Yarra Energy Foundation to coordinate the CitySwitch program with Yarra businesses. The CitySwitch program was initiated by Council before being handed over to YEF |

Attachment 1 - Economic Development Strategy Council Report Year 3 Report August 2012

2.1 INFORMATION & ASSISTANCE

| Objectives & Actions | Responsibility | Timeline | Measure/Outcome Sought |
|--|-----------------|----------------|---|
| <p>Action 1</p> <p>Provide access to information services on local catchment profiles (demographics) and economic trends</p> | <p>CPA, SEP</p> | <p>Ongoing</p> | <p>Yarra has subscribed to id@ consulting demographic products for close to a decade, these include:</p> <ul style="list-style-type: none"> • Community Profiles: ABS Census of Population and Housing data (CData) packaged and presented for the Yarra catchment through an externally hosted website, and • Forecast: Population projections using a suite of assumptions (births, deaths, net migration and residential land supply) with id@ use Council planning data to inform the projections. Council participated in a working group at the MAV to undertake collaborative procurement of online demographic/forecast/economic tools – Provision of Demographic Resources (RFT No. DT9314) <p>The benefits of the collaborative procurement were:</p> <ul style="list-style-type: none"> • Reduced administrative burden • Potential to attract additional and new suppliers into the market due to scale • Enhanced bargaining power <p>Following submissions and a day of interviews, four suppliers were appointed to the panel: Compelling Economics (REMPLAN), id Consulting, Geogratica and Pitney Bowes.</p> <p>A follow up presentation has been scheduled to further investigate the most suitable program for economic data.</p> |
| <p>Action 2</p> <p>Provide and maintain a reliable database of local businesses</p> | <p>SEP</p> | <p>Ongoing</p> | <p>An extensive validation program was undertaken, linking the ABR source data against the Yarra property register to update Council Business Directory. There are now over 2500 businesses registered in Council's Business Directory.</p> |
| <p>Action 3</p> <p>Work in partnership with State and Federal government business service agencies to provide up to date information on available business services, funding and grants, and information.</p> | <p>SEP</p> | <p>Ongoing</p> | <p>Council continues to refer business to the Victorian Business Centre who provide advice and links into State and Federal government business service agencies such as Aus Trade. Links on Councils Business webpage have been made to the Small Business Victoria.</p> |
| <p>Action 4</p> <p>Provide referral assistance to State and Federal government business service agencies such as the Victorian Business Centre and Aus Trade</p> | <p>SEP</p> | <p>Ongoing</p> | <p>Council continues to refer business to the Victorian Business Centre who provide advice and links into State and Federal government business service agencies such as Aus Trade.</p> |
| <p>Action 5</p> <p>Regularly inform the local business community of available business support services through a range of communication mediums (newsletter, website, presentations, brochures, displays)</p> | <p>SEP, CLS</p> | <p>Ongoing</p> | <p>Council provides regular updates to business via the following mediums:</p> <ul style="list-style-type: none"> • Yarra Business monthly e-bulletins • Yarra News • Information stands at all BRAINWOODforBusiness events • Council's Business Website • Through partners (Small Business Victoria, Business Enterprise Centre, Destination Melbourne) • Meetings with businesses and trader groups • Networking events |
| <p>Action 6</p> <p>Continue to monitor the impact of legislation and regulations on the success of local businesses.</p> | <p>SEP</p> | <p>Ongoing</p> | <p>Council continues to monitor legislation and regulations which impact on local business, particularly through its active engagement with VECCI.</p> |

Attachment 1 - Economic Development Strategy Council Report Year 3 Report August 2012

2.2 TRAINING & NETWORKING

| Objectives & Actions | Responsibility | Timeline | Measure/Outcome Sought |
|--|----------------|-----------|---|
| <p>Action 1 Prepare an annual business plan with the BAG that encourages the development of a supportive municipal wide business network</p> | SEP | Annual | <p>Council sought expressions of interest for 7 new members to join BAG in 2012. A program for inducting new BAG members has been introduced. A twelve month business plan has been developed to identify key priorities for implementation of Councils Economic Development Strategy in 2012. An exit interview process has commenced for members ending their term. BAG members receive a bimonthly updates on projects and key emerging issues.</p> <p>Councils Economic Development Unit has regular meetings with members of the Trader Associations and the Council of Textile & Fashion Industries of Australia (TFIA). Council has partnered with the following State government agencies and local education institutions to deliver business development programs and seminars and events:</p> <ul style="list-style-type: none"> • Australian Tax Office • Small Business Victoria • DBI • NBAA (4 breakfast events); • ACCC • Kangan Institute <p>Council has continued to partner with the Kangan Institute BEC to provide Small Business Counsellor Services and the Small Business Mentoring Service to provide free mentoring sessions to local businesses.</p> |
| <p>Action 2 Actively participate in local business association meetings and establish industry and business clusters (such as tourism and fashion).</p> | SEP | Ongoing | <p>Council promotes an annual calendar of events, seminars and training opportunities under the brand BRAINfoodforBusiness through:</p> <ul style="list-style-type: none"> • mail out of BRAINfoodforBusiness calendar to businesses registered on the online Business Directory • distribution to Business Associations • Monthly Yarra Business e-bulletin • New Business Kills • Information stand at the Richmond Town Hall • Information stand at BRAINfoodforBusiness seminars and events • face to face meetings • through our partners such as Small Business Victoria, and Destination Melbourne. |
| <p>Action 3 Partner with State government agencies and local education institutions in the delivery of business development programs, business seminars and events that are aimed at growing businesses</p> | SEP | Ongoing | <p>Produced 5000 copies of a desktop BRAINfoodforBusiness events calendar for 2012 that was widely distributed to local businesses.</p> <p>Whilst no formal program has been established, large business enterprises are represented on Councils Business Advisory Group which meets quarterly and provides a forum for priorities, strategies, initiatives and issues affecting the business community to be discussed.</p> <p>The Economic Development unit in consultation with the Department of Business Innovation - Melbourne Region is establishing a visitation program that will target businesses with more than 100 employees. The Economic Development Unit responds to numerous requests from SME's to meet with them on a broad range of business related issues. The feedback received from business in relation to these visits have been very positive.</p> <p>Hi Kim, Thank you also. As I said to Angela we both took a lot out of the meeting, certainly worth the time for any new business to do. I will make contact with David today. Kind Regards National Sales Manager</p> <p>Hi Kim and Angela, Thank you for your email Kim on behalf of yourself and Angela. I was delighted to meet with you both yesterday and felt encouraged by your visit. I have commenced the process of generating a brochure so will ensure you receive them when they are finalised. Thank you also for providing the link to the resources discussed yesterday. This is really useful information that I have already forwarded to various women in business in our group. I look forward to keeping in touch and to an ongoing positive relationship working with the Economic Development department. I appreciate your support and interest. Best Regards, - M</p> |
| <p>Action 4 Facilitate and promote a calendar of events, seminars and training opportunities through a range of communication mediums</p> | SEP | Quarterly | <p>Council has continued to partner with the Kangan Institute BEC to provide Small Business Counsellor Services and the Small Business Mentoring Service to provide free mentoring sessions to local businesses.</p> |
| <p>Action 5 Establish a regular forum or visitation program to connect with key landowners and large business enterprises to monitor and respond to their needs</p> | SEP | Annual | <p>Produced 5000 copies of a desktop BRAINfoodforBusiness events calendar for 2012 that was widely distributed to local businesses.</p> <p>Whilst no formal program has been established, large business enterprises are represented on Councils Business Advisory Group which meets quarterly and provides a forum for priorities, strategies, initiatives and issues affecting the business community to be discussed.</p> <p>The Economic Development unit in consultation with the Department of Business Innovation - Melbourne Region is establishing a visitation program that will target businesses with more than 100 employees. The Economic Development Unit responds to numerous requests from SME's to meet with them on a broad range of business related issues. The feedback received from business in relation to these visits have been very positive.</p> <p>Hi Kim, Thank you also. As I said to Angela we both took a lot out of the meeting, certainly worth the time for any new business to do. I will make contact with David today. Kind Regards National Sales Manager</p> <p>Hi Kim and Angela, Thank you for your email Kim on behalf of yourself and Angela. I was delighted to meet with you both yesterday and felt encouraged by your visit. I have commenced the process of generating a brochure so will ensure you receive them when they are finalised. Thank you also for providing the link to the resources discussed yesterday. This is really useful information that I have already forwarded to various women in business in our group. I look forward to keeping in touch and to an ongoing positive relationship working with the Economic Development department. I appreciate your support and interest. Best Regards, - M</p> |

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2.3. MARKETING & PROMOTION

| Objectives & Actions | Responsibility | Timeline | Measure/Outcome Sought |
|---|----------------------|---------------------------|---|
| <p>Action 1</p> <p>Develop a marketing strategy to promote Council's business services and establish regular communications with the local business community using a variety of mediums such as business kits, events, a newsletter, website and e-communications</p> | <p>SEP, CCS</p> | <p>2010 & Ongoing</p> | <p>A comprehensive Marketing and Communications Plan for promoting Council's Services has been developed. Strategies include promoting Council's business support services through the following:</p> <ul style="list-style-type: none"> • Yarra Business monthly e-bulletins • Yarra News • Information stands at the Richmond Town Hall and Libraries • Regularly updating Council's Business Website • Through partners (Small Business Victoria, Business Enterprise Centre, Destination Melbourne) • Regular meetings with businesses and trader groups • Networking events • Sponsorship opportunities • Attendance at business promotion events |
| <p>Action 2</p> <p>Undertake an annual survey of local businesses to find ways to improve Council's services, business confidence, and identify inhibitors to investment. Include these findings with the annual State of the Economic Audit monitoring and evaluation process for the Strategy</p> | <p>SEP</p> | <p>Annual</p> | <p>The Economic Development Unit hosted four post-graduate students from Melbourne University through the IMAP Student Placement Program. The students were required to develop a framework for capturing feedback from businesses in the City of Yarra that builds on the business survey introduced in 2011. The outcomes of this project will inform the approach taken for seeking feedback from businesses in future surveys. A survey has been conducted over a six month period in 2012 to participants attending Council's BRAINWOOD/BUSINESS program. 90% of survey respondents indicated that they are either likely or very likely to recommend the event they attended to others. 97% of the respondents indicated that they believe that it is either important or very important that the City of Yarra Organise small business events.</p> |
| <p>Action 3</p> <p>Develop a marketing strategy to promote Yarra as a place to invest using a variety of mediums (such as a prospectus, website, events) and targeted to key industry sectors of:</p> <ul style="list-style-type: none"> Health and Community Services Property and Business Services Finance and Insurance Retail Trade Arts and Culture | <p>SEP, CCS, CLS</p> | <p>2011 & Ongoing</p> | <p>Yarra has established a partnership with TFIA to assist in the promotion of the Kangaroo Fashion Hub and Textile and Fashion Precinct. The Precinct concept was the creation of the Council of Textile & Fashion Industries of Australia (TFIA) in collaboration with the City of Yarra and Kangaroo Institute.</p> <p>The Precinct will embrace places of innovation, design, production, distribution, sales and learning and will ultimately form the heart of a State-wide textile and fashion network. This initiative and the hive of activity that will flow from it will strengthen Victoria's position as the centre for Australia's TCF sector. Meetings have been conducted with a representatives from the Department of Business Innovation - Investment Facilitation to discuss marketing strategies adopted by State Government and to identify opportunities for Yarra to leverage of these initiatives.</p> |
| <p>Action 4</p> <p>Promote the City's position as a highly desirable residential location for highly skilled people (focusing on those with skills that are being imported) and using a variety of media</p> | <p>CCS, SEP</p> | <p>2011 & Ongoing</p> | <p>The recently endorsed Yarra Business and Industrial Land Strategy (BILS) identifies development and investment opportunities. The information from BILS will be used as the basis for developing a marketing strategy to promote Yarra as a place to invest.</p> |
| <p>Action 5</p> <p>Recognise and reward business excellence by communicating local success in marketing materials and establishing a partnership with the BAG to deliver a business excellence award program</p> | <p>SEP,</p> | <p>2011 & Ongoing</p> | <p>Council participates in the Northern Business Achievement Awards, partnership of industry, education, local and state government and decision makers across Melbourne's north to recognise the business achievements of SMEs in the northern region of Melbourne. Council in consultation with BAG, introduced an expression of interest program in March 2012, inviting nominations from local businesses to become Yarra's representative in the 2012 NBAA.</p> |

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3.1 WORKFORCE SUPPLY

| Objectives & Actions | Responsibility | Timeline | Measure/Outcome Sought |
|--|----------------|----------|--|
| Action 1 Undertake a skills audit for the local business community to identify gaps and capture data on future skill requirements | SEP | End 2010 | The 2011 inaugural Business Survey results indicate that the availability of suitably skilled staff and employee stability/retention are considered advantages of doing business in Yarra. Resourcing constraints have not allowed for the scoped skills audit to be undertaken and partnership opportunities are currently being explored. |
| Action 2 Continue to work with community partners on the development and implementation of an employment strategy targeting long term unemployed members of the Yarra community. This will include: (a) exploration of "social procurement" arrangements for Council services delivered under major contracts; (b) encouraging the development of sustainable social enterprises that employ locally for the delivery of Council services and works; (c) developing and supporting pathways into local employment opportunities for people from high disadvantage areas of Yarra; and (d) reviewing Council's recruitment practices to encourage local employment in all relevant service areas. | SEP, CPA, FCS | End 2011 | Council's Procurement Policy November 2010 outlines its commitment to supporting social procurement because of the positive social impacts this provides to both Council and the community. Council has developed processes to ensure that social enterprises become part of a diverse and dynamic supplier market. • A designated Youth Development – Education, Training and Employment Officer was recruited in February 2012. This new position for Youth Services works directly with primary and secondary schools, training and employment providers across the municipality to provide referral and support to young people at risk of disengagement from school and pathways to education, training and employment. This position has now started to form good working relationships with the schools and in Year 4 will look at developing partnership programs with identified schools in relation to identified issues for young people such as bullying, school disengagement, employment opportunities and work-ready skills etc. • Yarra Youth Services has been working with the Yarra Aboriginal Partnerships Officer to provide a placement opportunity for an Indigenous young person to undertake an Administration, Traineeship with the Unit for three years. This project is run in partnership with AFL Sportsready. • In Independent Living Skills Program is run every Thursday night from the Yarra Youth Community Centre for older young people (18-25years) and provides them with information regarding independent living, CV writing, work-ready skills etc. |
| Action 3 Continue to create local employment pathways, work experience initiatives, and targeted mentoring programs through the Inner Northern LLEN | FCS, SEP | Ongoing | The Yarra Education Youth Commitment has continued to be co-facilitated by Yarra Youth Services and the Inner Northern Local Learning and Employment Network (INLLEN) with 0.2 FTE committed to the initiative. In 2011/2012, key deliverables have included: - The development of the YEYC Strategic Plan; - The establishment of three working groups that are facilitated on a regular basis; - The delivery of the Schools/Sector Bus Tour in February 2012; - Business/School Breakfast Roundtable held on 17 May 2012 which was co-facilitated by City of Yarra and the INLLEN and had over fifty businesses and schools attend the roundtable. A number of key partnerships have now formed from the breakfast roundtable. - The YEYC Split of Cooperation Signing and Launch held on 21 June. Over ninety sector representatives and principals from primary and secondary schools attended the event held at the Richmond Town Hall and showed their commitment to the YEYC by signing a banner to be displayed at the Town Hall. |
| Action 4 Facilitate joint initiatives with State government agencies, local employment services, and local education institutions in the delivery of skills programs that are aimed at increasing local workforce skills | SEP | Ongoing | Council has partnered with ATO, Small Business Victoria, DBI, Destination Melbourne Ltd, Kangan Institute (small business counselling), Small Business Mentoring Services/mentoring sessions). |
| Action 5 Reinforce the merits of life-long learning to business operators and the local workforce | SEP, CLS | Ongoing | Council provides an extensive and affordable training program (BRAINfoodforbusiness) for its community. This program and those provided by State Government and other service providers are actively promoted via an extensive communications program. |

4.1 IDENTIFICATION & PROMOTION OF PRODUCT

| Objectives & Actions | Responsibility | Timeline | Measure/Outcome Sought |
|--|----------------|----------|---|
| Action 1 Continue to participate and partner in the delivery of the IMAP Regional Tourism Program | SEP | Ongoing | Refer attachment 2 Tourism Strategy 2011-2016 Year 2 Implementation Plan |
| Action 2 Develop and implement a Tourism Strategy and Action Plan that continues to offer a range of information, maps and events to support the key themes and products that are the tourism strengths of Yarra | SEP, CCS | Ongoing | An implementation plan for Year 2 (2012) has been prepared and commenced. Key initiatives for 2012 include: • Collaborative marketing opportunities • Leveraging from Melbourne's hallmark events • Wayfinding signage/map review • Implementation of the IMAP Tourism Program (Year 2 Plan) • "Discover your own Backyard" campaign • Industry Education • Tourism Victoria Product Briefings |
| Action 3 Ensure the Tourism Strategy and Action Plan is integrated with the IMAP Regional Tourism Program | SEP | Ongoing | Refer attachment 2 for status of implementation of Year 2 actions. |
| Action 4 Work with the BAG to assist in delivering the Tourism Strategy and Action Plan and IMAP Regional Tourism Program | SEP | Ongoing | The Tourism Strategy for the City of Yarra 2011-2016 has been aligned and integrated with the IMAP Regional Tourism Program. BAG members have been actively engaged in the tourism program. An overview of the Tourism Strategy Year 2 Implementation Plan was presented at the May BAG meeting. |

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| 5.1 MANAGING EMPLOYMENT PRECINCTS | | | | |
|---|----------------------------|----------|------------------------|--|
| Objectives & Actions | Responsibility SEP, CLS | Timeline | Measure/Outcome Sought | |
| Action 1 Establish the delivery of services and improvements in a coordinated way that includes evaluation and monitoring, for the City's Activity Centre network and apply this to other Employment Precincts once established | SEP, CLS | Mid 2010 | Completed | |
| Action 2 Develop guidelines to assist business and retail groups in the establishment of sustainable business associations for the purposes of business development and promotions activities. | SEP | Ongoing | Completed | The Guidelines for developing, promoting and marketing sustainable Business Associations include templates that will assist Business Associations in the preparation of business and marketing plans. The Guidelines have been distributed to Trader Associations and are available on the Business Website. |
| Action 3 Provide assistance with the preparation of business and marketing plans that support the implementation of Activity Centre Structure Plans and build on the unique character of each Centre | SEP | Ongoing | Ongoing | Council has been working with Mainstreet Australia to develop a template of rules for Associations that could be easily adapted to suit any incorporated business or traders association. Rules of Associations provide a solid framework to govern the rights and responsibilities of business/trader associations and their members. |
| Action 4 Support the development and implementation of special rate/charge schemes for marketing, promotion and business development | SEP, F | Ongoing | Ongoing | Council's Economic Development Unit has facilitated briefings from Smith Street, Gertrude Street and Bridge Road to Tourism Victoria to promote Yarra's Activity Centres as key visitor destinations. Council received notice from the Queens Parade Traders' Association that there is no longer any support for the Scheme from the Queens Parade Traders' Association or from property owners, traders and business operators in the Precinct. |
| Action 5 Work in partnership with the Business Associations to provide regular communications to landowners and business operators in Activity Centres on the progress of the implementation of Structure Plans, and Business and Marketing Plans | SEP | Ongoing | Ongoing | Council sought feedback from the community about the proposed discontinuance of the Queens Parade Special Charge Scheme in May and June 2012. No submissions were received. VCAT has been advised of Council's decision to discontinue the Scheme. Council manages the payment of funds collected for the Bridge Road Special Levy to the Bridge Road Main Street Inc. under a Funding Agreement. |
| Action 6 Develop ongoing relationships with leasing managers and major property owners in Activity Centres and Employment Precincts to achieve lower vacancy rates, a better business mix and ensure high level presentation standards | SEP | End 2010 | In progress | Council's Economic Development Unit meets with Business Associations regularly and provides information on the implementation of Structure Plans. Council also provides regular communication to land owners and business operators through communication noted in action 2.1.5 on Structure Plans and Business and Marketing Plans e.g. Swan Street Structure Plan. A workshop was conducted to identify the issues affecting the economic sustainability of the Bridge Road activity centre and opportunities to address these issues. The workshop was facilitated by the Centre of Entrepreneurship, Innovation and Community from Deakin University at Richmond Town Hall on 8 September 2011. Approximately sixty people attended the workshop, including business owners, property owners, leasing managers, tenants and councillors. A report was prepared, outlining key issues, impacts and possible strategies as expressed by the participants at the workshop. The report summarised the four top concerns raised by participants as being: 1. The image of Bridge Road; 2. Financial issues such as rising rents, land tax, which together are tending to cause high turnover and vacant shops; 3. Relationships between stakeholders (Council, landlords, tenants, residents); and 4. Physical amenities, particularly footpaths, toilets and carparks. The report on the workshop included the following possible strategies suggested by participants: • Rebuild the image of Bridge Road; • Provide financial assistance to address issues such as rising rents and land tax; • Improve relationships between stakeholders; and • Improve physical amenities, particularly footpaths, signage, toilets and car parks. The report was discussed in detail at a meeting with Bridge Road Main Street (BRMS) and various strategies were considered for implementation through Council's partnership arrangement with BRMS. A key focus of the work this year has been around rebuilding the image of Bridge Road. This has entailed hosting three workshops with traders to assist in developing a comprehensive and strategic marketing campaign for the next five years. |
| Action 7 Identify priority Activity Centre and Employment Precinct redevelopment sites, prepare guidelines for their future use and development in partnership with owners, and promote these opportunities | SEP | Ongoing | Ongoing | Opportunities for future use and development of priority redevelopment sites are being identified through Yarra's Business and Industrial Land Strategy, Structure and Local Area Plans and other strategic projects. |
| Action 8 Work collaboratively with Business Associations to implement infrastructure improvements (tenely streetscape, transport and environment actions) as outlined in Key Strategic Direction 1 | SEP, ES, I, ST | Ongoing | Ongoing | Council's Economic Development Unit meets with Business Associations and the Business Advisory Group regularly and provides information on the implementation of Structure Plans. Council also provides regular communication to land owners and business operators through communication noted in action 2.1.5 on Structure Plans and Business and Marketing Plans e.g. Swan Street Structure Plan, Peel Street Park and Other Street Streetscape improvements. |

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6.0 REGIONAL COLLABORATION

| Objectives & Actions | Responsibility | Timeline | Measure/Outcome Sought |
|---|-----------------|----------------|--|
| <p>Action 1 Continue to work with and support the IMAP Group</p> | <p>SEP</p> | <p>Ongoing</p> | <p>Council is currently participating in the following IMAP initiatives: • Action 2.4 - Walking Links and Pedestrian Priority Areas (Regional Greenlight Project) Completed. No further action required • Action 3.3 - Regional Approach to Parking Management. Largely completed. Waiting research being undertaken by DOT. • Action 4.7 - Improvement to Public Transport Services. MCC reports on Melbourne Transport Strategy on this action. • Action 6.3 - Managing Conflict in Activity Centres - Completed - final report went to IMAP in 2011. • Action 7.7 - Universities and Regional Development Completed May 2012 • Action 11- Regional Tourism Program ongoing. • Action 1.4- Boulevards and Major roads. On Hold • Improvements to public transport services-AS ABOVE • Action 2.5- Bicycle Network. Current and ongoing Led by MCC. • Action 3.2 - Affordable Housing - completed final report November 2011 • Action 9.4 - Green demonstration projects Current Action 9.4(Green roofs) and Yarra Energy Foundation and COY • Action 10.1 -Regional Open Space and Trail Network Current • Action 9.2 - Environmentally sustainable design - commercial buildings. Current and ongoing. Action 3.2 - Roads as places -Current - will start soon Action 3.5 - Reduced through traffic -Current - Action 5.3 -Housing - Current Action 5.4/5.5 -Housing - current - tender stage Action 8.1 - Freight - on hold</p> |
| <p>Action 2 Continue to work with and support the LLEN Group</p> | <p>FCS, SEP</p> | <p>Ongoing</p> | <p>Yarra has a position on the Inner Northern LLEN Committee of Management (COM) which is responsible for establishing the strategic direction of INLLEN. Council is a partner in the Yarra Education Support Network which is currently working on developing the Yarra Youth Commitment Network. The network is committed to developing strategic responses to issues facing young people.</p> |
| <p>Action 3 Continue to work with the Northern ACC</p> | <p>SEP</p> | <p>Ongoing</p> | <p>Council continues to work with the Northern ACC on regional economic matters including the Northern Business Achievement Awards program.</p> |
| <p>Action 4 Continue to strengthen relationships with all adjoining municipalities</p> | <p>SEP</p> | <p>Ongoing</p> | <p>Council continues to strengthen relationships with adjoining municipalities through: • regular attendance as a member of Regional Economic Development Forums; • being a signatory and participant with Cities of Melbourne and Port Phillip on the Cityswitch Program; • NORTH Link a regional partnership of industry, education and government and the seven LGAs in Melbourne's North; and • IMAP working groups.</p> |

Attachment 2 - Tourism Strategy 2011-2016 Year 2 Implementation

Attachment 2

Tourism Strategy 2011 - 2016 Year 2 Implementation

| Goal 1 - Marketing & Communications | | Action | Status | Comments |
|-------------------------------------|--|--|--------------|--|
| Strategy 1.1 | Support research opportunities which inform marketing direction and assist the tourism industry's market intelligence | Investigate models for obtaining data that will provide an understanding of tourism's contribution to the local economy. Investigate opportunities for partnering with Universities to deliver on research projects. | In progress | This action has been combined with the implementation of the Economic Development Strategy. Refer to 2.1 Information & Assistance Action 1. |
| Strategy 1.2 | Develop and implement an annual integrated strategic marketing plan that provides direction and focus for industry to collaborate, participate and contribute | Support the local tourism industry to develop an integrated and strategic approach to promoting Yarra. | Completed | Meeting held with key tourism stakeholders and Council's Business Advisory Group. Outcome was stakeholders would work with Council when specific projects or opportunities arise. |
| Strategy 1.3 | Leverage City of Yarra's attributes (product strengths and diverse visitor experiences) with Tourism Victoria's brand campaigns and Destination Melbourne Limited's (DML) marketing activities | Review Council's partnership with Destination Melbourne to determine how Council can work more effectively with Destination Melbourne to achieve the desired outcomes of the Tourism Strategy. Continue to inform Tourism Victoria about new City of Yarra products and experiences | In progress | Partnership has been reviewed. A meeting with Destination Melbourne is scheduled for August 2012 to discuss a partnership arrangement that better responds to local government needs. |
| Strategy 1.4 | Identify and leverage from Melbourne's major events | Support the local tourism industry to identify strategic opportunities for leveraging from Melbourne's major events. | Ongoing | New products and experiences are communicated regularly with Tourism Victoria's PR and Marketing teams. |
| Strategy 1.5 | Continue to identify opportunities for products/experiences to ensure an appropriate range and diversity of products/experiences is provided | Establish a process for ensuring that Council's Business Database is regularly updated and maintained (Stage 1). Develop a process for identifying and increasing the number of tourism related businesses on Council's Business Database. | In progress | Hosted a Melbourne Events Showcase for local tourism businesses to hear from Tennis Australia, Melbourne Food & Wine Festival (MFWF), Melbourne Fringe and L'Oréal Melbourne Fashion Festival (LMFF). Two projects with the 2013 MFWF and 2013 LMFF are currently in development. |
| Strategy 1.7 | Identify and utilise existing communication opportunities to raise awareness of the benefits and value of tourism and inform stakeholders of the progress of the Strategy | Showcase inner Melbourne product at key regional Visitor Information Centres and at significant Melbourne entry points and transport hubs. Look for opportunities to use Council's website to communicate the value of tourism and progress of the Strategy in a more dynamic and interactive manner. | Completed | This action has been combined with the implementation of the Economic Development Strategy. Refer to 2.1 Information & Assistance Action 2. |
| | | | Discontinued | IMAP Tourism Group project. Project was investigated and it was determined that there is insufficient space available for distributing inner Melbourne product at regional Visitor Information Centres. The inner Melbourne Map is distributed at significant Melbourne entry points and transport hubs. |
| | | | In progress | Review of tourism information on Council's website is currently being investigated. |

Attachment 2 - Tourism Strategy 2011-2016 Year 2 Implementation

| Goal 2 - Transport Accessibility | | | |
|---|---|---------------|---|
| Strategy | Action | Status | Comments |
| Strategy 2.1 Advocate on transport issues that impact the visitor experience | Seek active engagement with Metlink on the transition to MYKI. In conjunction with Metlink, develop key messages for communicating the MYKI system to visitors. Explore opportunities for integrating MYKI cards with existing Council systems e.g. library cards. | Completed | IMAP Tourism Group project. Considerable lobbying by key industry representatives resulted in the development of a MYKI Visitor Pass. In addition the Inner Melbourne Map brochure has been updated with information to reflect the transition to Myki. |
| Strategy 2.2 Continue to communicate the availability of the different sustainable transport options for visitors. | Continue to work with the IMAP Tourism Group on the licensing and distribution of the Inner Melbourne Map. Review Council's partnership with Voyage Publishing to identify opportunities for customising the Grid Maps to provide more detailed, localised maps that support visitors with travelling into and around Yarra. Investigate opportunities for installing Yarra Trams Tram Tracker Screens at City of Yarra Town Halls and Libraries. | Ongoing | IMAP Tourism Group project. There are two versions of the map that are available for license to interested parties. |
| Strategy 2.3 Support and enhance existing sustainable transport modes and identify new opportunities for visitors to travel to and within the City of Yarra | Undertake a review/audit of the existing Wayfinding Signage in Yarra and identify opportunities for rolling out further signage across the municipality to assist visitors with travelling around. Audit the City of Yarra as a visitor destination to determine gaps in public transport access, barriers to using current transport modes and identify opportunities for new modes. | Completed | The partnership was reviewed and subsequent to the review, Voyages Publishing have discontinued the production of the Grid Maps. Partnership has been cancelled. |
| | | In progress | Currently in discussion with Yarra Trams regarding the Tram Tracker Screens. |
| | | In progress | Review/audit in progress. Project managed by Urban Design. |
| | | In progress | Project scheduled for investigation in last quarter of 2012. |
| Goal 3 - Character and Amenity | | | |
| Strategy | Action | Status | Comments |
| Strategy 3.2 Determine measures for protecting and enhancing what visitors value about the city of Yarra's character and amenity. | Consider what visitors value about Yarra's character and amenity when developing the Swan Street Structure Plan. | Completed | This action has been combined with the implementation of the Economic Development Strategy. Refer to 1.1 Decision Making and Regulations Action 6. |
| Strategy 3.4 Continue to provide a safe experience for our visitors | Support City of Yarra licensees to develop an agenda, strategy and action plan to address minimising the impact of negative alcohol issues for the Yarra Liquor Licence Forum. | Ongoing | This action has been combined with the implementation of the Economic Development Strategy. Refer to 1.1 Decision Making and Regulations Action 5. |
| Strategy 3.5 Engage with Council, industry and community on ways to improve infrastructure and facilities to support the visitor experience | Facilitate meetings with local business associations to identify opportunities for improving infrastructure. Continue to explore opportunities for enhancing the streetscape of Victoria Street through the Gateway Project. | Ongoing | This action has been combined with the implementation of the Economic Development Strategy. Refer to 5.1 Managing Employment Precincts Action 5. |
| | | Ongoing | This action has been combined with the implementation of the Economic Development Strategy. Refer to 1.2 Streetscapes Action 5. |

Attachment 2 - Tourism Strategy 2011-2016 Year 2 Implementation

| Goal 4 - Industry Development and Partnerships | | Action | Status | Comments |
|---|--|---|--|--|
| Strategy 4.1 | Investigate and determine the most appropriate model for a local tourism body in the City of Yarra | Present the findings from the investigation undertaken in Year 1 to local tourism businesses. | Completed | In 2011 Destination Melbourne were engaged to develop a model for a local tourism body in Yarra. The model that Destination Melbourne presented was the formation of a Yarra Visitor Partnership (YVP). A report outlining the process for establishing the YVP was presented to Executive in January 2012. A key recommendation of that report was to present the partnership model to local tourism businesses to determine their level of support. This model was presented to key tourism businesses and Council's Business Advisory Group (BAG) and it was agreed that rather than establish an independent body, the BAG would assist Council with the implementation of the Tourism Strategy. |
| Strategy 4.2 | Review the Tourism Strategy annually in collaboration with the tourism industry | Establish a process for seeking feedback from local tourism businesses on the Tourism Strategy. | Completed | Strategy will be reviewed in consultation with key tourism stakeholders and Council's Business Advisory Group. |
| Strategy 4.3 | Educate and embed an understanding of the value and role of tourism with all Council staff and Councillors | Link with action outlined in Strategy 1.7 and ensure Council staff and Councillors are informed of the value of tourism | Ongoing | Refer to Strategy 1.7 comments. |
| Strategy 4.5 | Continue to raise awareness of the value and role of tourism to business | Link with Strategy 1.7 & 4.3 and ensure that businesses receive regular communication regarding the value of tourism to their business | Ongoing | Refer to Strategy 1.7 comments. |
| Strategy 4.6 | Facilitate opportunities to connect tourism industry businesses and stakeholders | Continue to promote opportunities for local tourism businesses to connect with key stakeholders. | Ongoing | Monthly briefings to Tourism Victoria's PR & Marketing Teams have been established and proving to be very successful. |
| Strategy 4.7 | Facilitate educational opportunities to build industry's capability | Partner with the local tourism industry and Destination Melbourne to ensure that the Tourism Excellence program is responding to industry needs. Support 10 local tourism businesses to participate in the Australia Tourism Accreditation Program (ATAP). Facilitate training opportunities for City of Yarra Liquor Licensees. Seek opportunities to align with the 2012 Tourism & Events Excellence (TEE) Conference to shape the agenda. | Ongoing Discontinued Completed | Regular communications takes place with Destination Melbourne to ensure that the Tourism Excellence program is relevant for small to medium tourism businesses. Following several attempts to engage local businesses in this opportunity, the project was discontinued due to lack of interest. Information and training opportunities are extended to licensees through the regular Liquor License Forums. IMAP Tourism Group project. The IMAP Tourism Group sponsored the 2012 TEE Conference - Strategic Partnerships theme. The conference was a great success and feedback to date has the Partnerships theme the most popular program of the conference. |
| Strategy 4.8 | Support ways to acknowledge and recognise our tourism industry's achievements | Continue to promote the innovation and achievements demonstrated by Yarra's tourism businesses through the Business Bulletin and other opportunities as they arise. | Ongoing | This action has been combined with the implementation of the Economic Development Strategy. Refer to 2.3 Marketing and Promotion Action 5. |

Attachment 2 - Tourism Strategy 2011-2016 Year 2 Implementation

| Goal 5 - Community Engagement and Participation | | | |
|--|---|--|---|
| Strategy | Action | Status | Comments |
| Strategy 5.1 | Leverage from Destination Melbourne Limited's Visiting Friends and Relatives campaign that builds host/resident awareness of things to see and do in their municipality Empower Council's front line staff to become visitor information sources | Participate in Destination Melbourne's Visiting Friends and Relatives initiatives | In progress Launch of the City of Yarra Discover Your Own Backyard campaign is scheduled for end of November 2012. |
| Strategy 5.4 | | Explore the opportunity of partnering with Destination Melbourne, (through Council's 2012/13 partnership agreement) to deliver famils for City of Yarra staff. | In progress Currently in discussion with Destination Melbourne. |

13. Confidential business reports

The following items were deemed by the Chief Executive Officer to be suitable for consideration in closed session in accordance with section 89 (2) of the *Local Government Act 1989*. In accordance with that Act, Council may resolve to consider these issues in open or closed session.

RECOMMENDATION

1. That the meeting be closed to members of the public, in accordance with section 89 (2) of the *Local Government Act 1989*, to allow consideration of:
 - (a) Matters prejudicial to Council and/or any person

2. That all information contained within the Confidential Business Reports section of this agenda and reproduced as Council Minutes be treated as being and remaining strictly confidential in accordance with the provisions of sections 77 and 89 of the *Local Government Act 1989* until Council resolves otherwise.