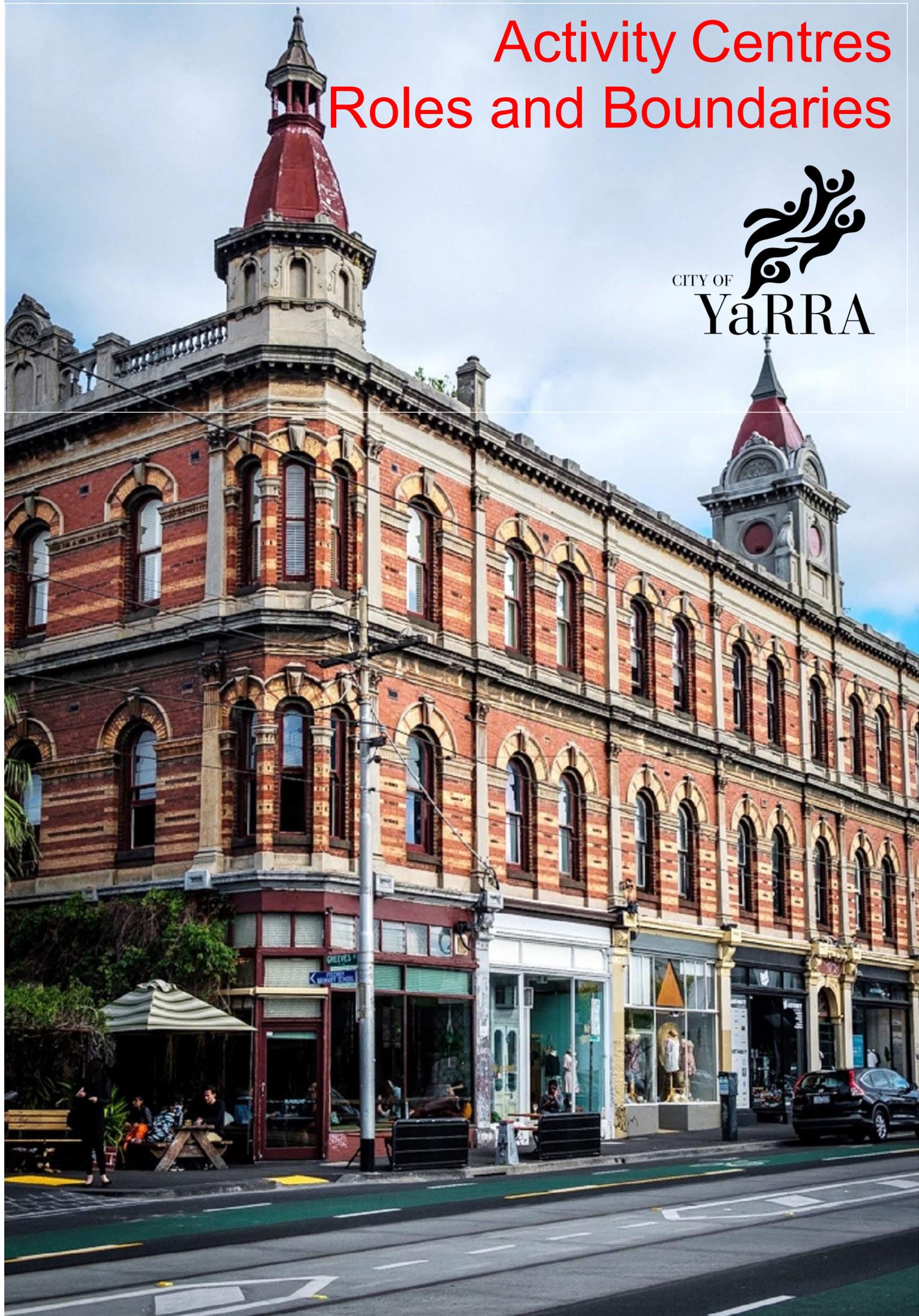
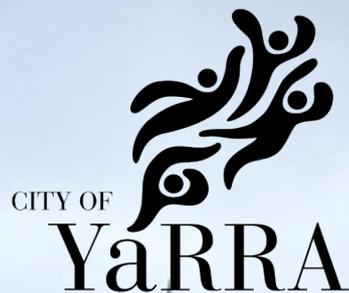


Activity Centres Roles and Boundaries



Abstract

This report assesses the policy context for the system of activity centres in the City of Yarra. It reviews the current planning scheme provisions and analyses the roles, extent and boundaries of all the activity centres. It provides the basis for revisions in the Planning Scheme re-write and adds explicit boundaries to local policy. The report also addresses the level of change and new development anticipated in the Major, Neighborhood and Local activity centres.

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1. Introduction

1.1 Purpose

Yarra has a network of activity centres, each with their own role and function, including:

- Major activity centres (MACs): these centres are identified in Plan Melbourne 2017-2050 and provide access to a wide range of goods and services, some serving larger subregional catchments;
- Neighbourhood activity centres (NACs): these centres are in the existing Yarra Planning Scheme and provide access to local goods, services and employment opportunities and serve the needs of the surrounding community.
- Local centre (LCs): these centres provide a limited range of goods, services and employment to adjoining communities. They are identified in the existing Yarra Planning Scheme (YPS).

Yarra's major and neighbourhood activity centres are predominantly along and around the main retail shopping streets (Yarra's high streets). They feature highly intact heritage streetscapes and heritage places, key landmark signs and buildings, larger commercial sites, and vibrant communities. Activity centres have been the focus of Yarra's growth in recent years, and have seen the addition of mid-rise commercial development and apartments. They will continue to accommodate most of the city's growth because their transport infrastructure, shops and services make them the most suitable locations for development.

Managing growth and change in activity centres aligns with the State Government's vision for a network of major activity centres that are critical to metropolitan economic performance and provide a focal point for services, employment, housing, public transport and social interaction.

The purpose of this report is to define the network of activity centres in the City of Yarra. In particular, it provides a rationale for the boundaries and role for Major and Neighbourhood activity centres and the smaller Local centres. This report also includes clearer mapping of the activity centre boundaries and identifies the categories of precincts in each centre.

This report is structured as follows:

- Section 2 lists Yarra's activity centres
- Section 3 sets out the method and approach used to define the activity centres
- Section 4 sets out the planning policy context
- Section 5 sets out the characteristics of the major activity centres
- Section 6 describes the role and function of the activity centres in delivering housing and employment growth
- Section 7 categorises the land that is included and excluded from the activity centres.

1.2 Yarra's Activity Centres

Yarra's activity centres host a vibrant and constantly evolving range of retail, entertainment, hospitality and commercial services.

Our major activity centres listed in Plan Melbourne are:

- Swan Street – Richmond
- Bridge Road – Richmond
- Victoria Street – Richmond
- Smith Street – Collingwood/Fitzroy
- Brunswick Street – Fitzroy

Our neighbourhood centres that provide access to local goods, services and employment opportunities and serve the needs of the surrounding community, include:

- Johnston Street – Collingwood/Abbotsford
- Queens Parade – Fitzroy North/ Clifton Hill
- Gertrude Street - Fitzroy
- Heidelberg Road – Alphington
- St Georges Road – Fitzroy North
- Nicholson Street – Carlton North and Fitzroy North

Yarra also has smaller, local centres which serve the adjoining local community. They are also appropriate locations for employment growth however the scope and opportunities for growth are more nuanced and constrained in these smaller centres.

Our local centres:

- Berry Street/Ramsden Street – Clifton Hill
- Lygon Street – Carlton North/Princes Hill
- Nicholson Street (south) – Carlton North
- Rathdowne Street – Carlton North
- Spensley Street – Clifton Hill

1.3 Approach and Method

Population and economic growth and housing diversity can be facilitated by establishing a boundary for each centre based on a consistent set of criteria. The capacity to accommodate growth is determined by land area, potential development densities and other factors including the amount of land available for redevelopment in each centre. The approach in establishing boundaries in Yarra is based on the *Activity Centres Boundary Criteria* in Planning Practice Note 58 (PN58). An assessment against each criteria is included for each activity centre in this report.

The method undertaken to define the boundaries is set out below.

1. Background research

- Desktop review of the Yarra Planning Scheme – zones; overlays; amendments
- Desktop review of land ownership; lot size; main road frontage; uses; permits; public transport routes
- Review of planning strategies – Housing Strategy; Spatial Economic and Employment Strategy

- Site visits to understand existing built form, land uses, pedestrian connections and open space.

2. Categories defined

Council defined categories to order land that is to be included in a boundary. It is based on Planning Practice Note 58 *Structure Planning for Activity Centres* (PPN58), which discusses the role and nature of activity centres in the opening paragraphs of the Practice Note:

Activity centres are a focus for housing, commercial, retailing, community, employment, transport, leisure, open space, entertainment and other services and are places where people shop, work, meet, relax and live.

State planning policy directs the further expansion of these services into activity centres, and recognises that activity centres are ideally placed to provide for growth in household numbers. As such, activity centres will be a major focus for change in metropolitan Melbourne.

PPN58 includes a set of criteria / issues to consider *in determining the potential location of an activity centre boundary*:

- *the location of existing commercial areas and land uses*
- *the location of existing government and institutional areas and land uses*
- *the location of existing areas of public open space*
- *commercial and residential needs*
- *environmental and flooding constraints*
- *heritage constraints*
- *availability of strategic redevelopment sites, both existing and potential*
- *the location of residential areas, including whether they provide significant redevelopment opportunities or constraints for the centre*
- *consideration of physical barriers and opportunities for their improvement*
- *proximity to public transport, especially fixed rail (train or tram)*
- *the location of existing and potential transport infrastructure including fixed rail, buses, bicycle paths, car parking areas and modal interchanges*
- *walkability – opportunities to provide for and improve walkability within 400 to 800 metres from the core of the centre (depending on topography and connectivity)*
- *consistency with State policy*
- *consistency with local policy and Municipal Strategic Statement (MSS)*
- *impacts of the boundary on other activity centre boundaries*

3. Analysis

Land along and around activity centres, included in Plan Melbourne 2017-2050 and the current YPS has been analysed to consider whether there is:

- sufficient land to provide for the commercial (retailing, office, fringe retailing and support activities such as entertainment) activities needed over a 15 to 20 year time frame and then into the 30-year horizon
- residential land that is integrated into the activity centre or surrounded by other uses that have a strong functional inter-relationship with the activity centre even where limited development opportunities exist
- key public land uses that have or are intended to have a strong functional inter-relationship with the activity centre even where there are no or limited redevelopment opportunities

- public open space areas that have or are intended to have a strong functional inter-relationship with the activity centre.

Residential land encumbered by significant constraints (such as a Heritage Overlay) was generally excluded from activity centres.

2. Policy Framework

2.1 Plan Melbourne 2017-2050

Plan Melbourne 2017-2050 maps the location of larger activity centres and includes the five major activity centres in the City of Yarra - listed in section 1.1 above. It also includes policy about neighbourhood activity centres as part of a wider network of centres. Planning policy makes these centres the best location for medium and higher density development. Activity centres provide potential to attract investment and will be a major focus for change in metropolitan Melbourne.

- Direction 1.2 *Improve access to jobs across Melbourne and closer to where people live* supports investment and job creation across the city
- Policy 1.2.1 *Support the development of a network of activity centres linked by transport* discusses activity centres which ‘fill diverse roles—including housing, retail, commercial and civic services—and are a focus for services, employment and social interaction. In recent times some activity centres have become a focus for higher-density development’.
- Direction 2.2 *Deliver more housing closer to jobs and public transport* aims to locate medium and higher density development near services, jobs and public transport supports the objectives of consolidation and housing choice.
- Policy 2.2.3 *Supports new housing in activity centres and other places that offer good access to jobs, services and public transport.*

Plan Melbourne 2017-2050 makes reference to specific major activity centres (Direction 2.2 *Deliver more housing closer to jobs and public transport – [list of activity centres](#)*) including five in Yarra and more general references to neighbourhood activity centres as locations for increased jobs and housing.

In Direction 5.1 - Create a city of 20-minute neighbourhoods Plan Melbourne 2017-2050 gives particular emphasis to neighbourhood activity centres.

Neighbourhood activity centres are an integral part of the city’s vibrant community life and critical to the creation of 20-minute neighbourhoods. These high streets and specialised strips of shops, cafes, small supermarkets, service businesses, community services and public spaces serve the needs of the surrounding community and provide a focus not only for local jobs but also for social interaction and community participation.

.....

Policy 5.1.2 *Support a network of vibrant neighbourhood activity centres*

The attributes of and opportunities for neighbourhood activity centres at the local level vary across Melbourne. That is why local communities should lead the planning of their own centres.

Where centres are well established or communities are seeking to protect the unique character of their centres (such as protecting heritage buildings or access to public land or open space to achieve community benefit), they should be assisted in determining the desired built form outcomes.

Local governments will be supported to prepare structure plans for their neighbourhood activity centres to help deliver 20-minute neighbourhoods.

In Yarra the major activity centres will play an important role as locations for additional housing and jobs. The level of change in the activity centres will be determined by the context of each centre. The neighbourhood centres will play a more nuanced role which will vary depending on the nature of each centre and the constraints or opportunities of their context. The local centres will play a limited role in providing for housing and local employment.

2.2 Yarra Planning Scheme

The Yarra Planning Scheme (YPS) sets out a number of activity centres (refer to Table 1 and Figure 1). The centres are along main road or in smaller pockets in North Carlton, Clifton Hill and Alphington. The current YPS does not distinguish between the larger neighbourhood centres which serve significant local catchments and the small local centres discussed in this report.

FIGURE 1. STRATEGIC FRAMEWORK PLAN



Source: Yarra Planning Scheme

Clause 21.04-2 of the YPS states that *Activity centres in Yarra have a retail, hospitality, and service focus. Because of the pattern of development in the municipality, almost all residents are within 400m of an activity centre. The provision of local services is therefore important for maintaining the character of activity centres as local destinations.*

Table 1: Activity Centres in the Yarra Planning Scheme

Major Activity Centre (Neighbourhood Map in YPS)	Neighbourhood Activity Centre (Neighbourhood Map in YPS)
<ul style="list-style-type: none"> • Richmond – Swan Street (Figures 7 and 23: Burnley, Cremorne, South Richmond + Central Richmond) • Richmond – Bridge Road (Figures 21 and 23: North and Central Richmond) • Richmond – Victoria Street (Figure 5: Abbotsford) • Fitzroy – Brunswick Street (Figure 17: Fitzroy) • Fitzroy – Smith Street (Figure 13: Collingwood) 	<ul style="list-style-type: none"> • Alphington – Heidelberg Road b/n Parkview Road and Como Street (Figure 15: Fairfield / Alphington) • Carlton North – Rathdowne Street, at the Richardson Street Junction (Figure 9: Carlton/ Princes Hill) • Carlton North – Lygon St, south of Pigdon Street (Figure 9: Carlton/ Princes Hill) • Carlton North / Fitzroy North – Nicholson St, north of Richardson Street, north of Curtain Street and at the junction of Lee St (Figure 9: Carlton/ Princes Hill) • Collingwood / Abbotsford – Johnston Street (Figures 5: Abbotsford and 13: Collingwood) • Fitzroy North – St Georges Road (Figure 19: North Fitzroy) • Fitzroy North / Clifton Hill – Queens Parade (Figure 11: Clifton Hill) • Fitzroy – Gertrude Street (shown as part of Brunswick MAC in Figure 13, however referred to a neighbourhood centre in policy, clause 21.08-7) • Clifton Hill – Spensely Street at the intersection with Berry Street (Figure 11: Clifton Hill) • Clifton Hill - Berry Street at the junction with Ramsden Street (Figure 11: Clifton Hill)

As part of the process to assess the boundaries and roles of the centres in the current Yarra Planning Scheme a number of the small centres designated as Neighbourhood centres were considered to play a lower order and more local role in the network. These have been recognised as local centres and include:

- Carlton North – Rathdowne Street
- Carlton North – Lygon St, south of Pigdon Street
- Carlton North / Fitzroy North – Nicholson St, , north of Curtain Street and at the junction of Lee St

- Clifton Hill – Spensely Street at the intersection with Berry Street
- Clifton Hill - Berry Street at the junction with Ramsden Street

2.3 Review of Yarra Planning Scheme 2014

A review of the Yarra Planning Scheme in 2014 suggested that the current YPS lacked an overarching, evidence-based narrative for projected housing and employment growth in the municipality. As growth of both housing and employment continues, it is expected that there will be competition for land and other assets in Yarra. The review noted that it is important to carefully manage and protect certain assets and land, and facilitate growth in locations that will provide benefit for current generations but also be sustainable into the future.

The review identified a lack of spatial plans for major activity and neighbourhood centres and further suggested that the roles and strengths of activity centres in particular need to be better defined, and strategies to meet the projected demand for retail and other facilities need to be identified.

Council has been implementing spatial plans (Swan Street Structure Plan, Johnston Street Local Area Plan) through separate amendments that introduce Design and Development Overlays. In addition Council has been implementing built form frameworks for Bridge Road, Queens Parade, Victoria Street, Brunswick Street, Smith and Gertrude Streets Activity Centres to guide decision making on built form outcomes. These have been pursued through separate amendments.

The review also identified the need for clear maps. This report will assist to clearly illustrate draft policy in the Yarra Planning Scheme.

2.4 Current Council Structure Plans and Local Area Plans

Council has previously prepared and adopted a series of structure plans and local areas plans to guide and manage growth within Yarra's key activity centres. These plans were developed to manage the future of the centres and provide direction on preferred land use, built form, movement (vehicle and pedestrian), and identify zones and other planning controls required.

There are currently five structure and local area plans in Yarra with varying status levels:

- The Smith Street Structure Plan was adopted by Council 2008 but the subsequent Design and Development Overlay was abandoned June 2012;
- The Swan Street Structure Plan was adopted by Council in December 2013 and is being implemented through Planning Scheme Amendments C191 and C236;
- The Victoria Street Structure Plan was adopted by Council in 2010; and
- The Johnston Street Local Area Plan was adopted in December 2015 and is being implemented through a Planning Scheme Amendment C220.

2.5 Built form Frameworks

As outline earlier, Plan Melbourne and State policy seek directs the growth of Melbourne in sustainable manner in locations that has strong access to existing public transport and services. This however, needs to be at a scale appropriate to the role and capacity of each centre and support each centre's unique character. To ensure the values of Yarra's retail strips and adjoining mixed use / commercial areas are protected, Council has commenced a programme of preparing built form frameworks. The frameworks guide the future form and

design of development and have (or will) inform planning controls in the Yarra Planning Scheme through the Design and Development Overlay.

Built form Frameworks have been prepared for:

- Smith and Brunswick Streets
- Gertrude St and Johnston St (east)
- Queens Parade
- Heidelberg Road
- Bridge Road
- Victoria Streets

3. . Yarra's Activity Centre Network

3.1 Major Activity centres

The Major Activity Centres have their own distinct character and mix of activity. Future growth of the centres would have regard for these characteristics.

The Major Activity Centres are:

- Bridge Road – Richmond
- Victoria Street – Richmond
- Smith Street – Collingwood/Fitzroy
- Brunswick Street – Fitzroy
- Swan Street – Richmond

Brunswick and Smith Streets

Brunswick and Smith Streets (along with the neighbourhood centres of Gertrude and Johnston Streets) function as a single highly accessible network. This network includes the major north-south retail strip centres connected by the neighbourhood centres of Gertrude Street and Johnston Street which is categorised, east of Smith Street.

Local streets play a significant role as walking and cycle routes or connections in this network. This area has a strong hospitality focus and night-time economy, drawing from a metropolitan catchment. It has historically been a place that attracts artists and alternative lifestyle creative types.

The urban characteristics vary throughout these Activity centres, which includes:

- Retail spines with fine grain shop fronts with distinct heritage character
- More moderate grain shop fronts with a mix of contemporary and heritage character along the northern and southern ends of Smith Street, the northern end of Brunswick Street, Johnston Street between Brunswick and Smith Streets and a portion along Nicholson Street
- Older industrial sites, such as the MacRobertson industrial buildings in Fitzroy and Foy and Gibson in Collingwood, which are defined by the original industrial forms and factories and were the cornerstone for the development of Collingwood and Fitzroy at that time
- Small pockets of traditional residential precincts with heritage terrace housing behind the retail strips.
- Opulent heritage homes

- A variety of industrial and contemporary built form with pockets of traditional residential scattered within mixed use areas behind the retail strips.

These retail strips are associated with substantial areas of Mixed Use Zone land which covers former mixed industrial / residential areas behind the retail strips. They include large former industrial buildings and warehouses and nineteenth century housing.

These centres also include the health and education precincts around St Vincent's Hospital and Australian Catholic University on Victoria Parade. This is likely to see growth as part of a wider inner city trend for growth in health and education services.

These centres also connect to major cultural nodes and open space at the Exhibition Building / Melbourne Museum at the west end of Gertrude Street and Collingwood Arts Precinct on Johnston Street.

Victoria Street

Victoria Street includes contrasting components. The western end between Hoddle Street and Church Street is characterised by Chinese and Vietnamese goods and food with a wide range of cafes and restaurants, specialty shops and a more recent supermarket. This serves a significant local Vietnamese / Chinese community and adjoining public housing. The main shopping strip is predominantly fine grained low rise shops.

The eastern end of Victoria Street is dominated by the Victoria Gardens enclosed shopping centre and recent higher density housing along the northern end of Burnley Street and along Victoria Street east of Grosvenor Street. Victoria Gardens serves a substantial local and regional catchment. This end of Victoria Street also connects to the Yarra River with recent major redevelopment along the south bank of the River. Redevelopment of former industrial land has been on larger scale lots and in the range of four to eight storeys. On some larger sites apartment buildings have approached or exceeded ten storeys.

The central section of Victoria Street includes a mix of retail, service business and industry. It adjoins the Carlton and United Breweries complex and associated industrial area. The scale of development is generally low rise but buildings in the CUB complex are bulkier and taller. Redevelopment of this area constrained by and linked to the future of the CUB complex. If CUB was redeveloped it could become a major node for activity, as has happened with the Alphington paper-mill site.

The Skipping Girl sign at 651 Victoria Street is a valued Yarra landmark.

A large proportion of floor space in Victoria Street is accounted for within the Victoria Gardens and Hive shopping centres, which accommodate 52,000 and 8,000 square metres of floor space respectively. Victoria Gardens is more like a traditional suburban shopping centre with a large car park and minimal activation at ground level to the main road frontages. Conversely, the Hive, which was developed more recently, provides opportunities for centre retailers to engage at the street level, providing a focus for local retail activity on this part of Victoria Street. The remainder of the precinct hosts a mix of retail types with a focus on food and hospitality, and is home to one of Melbourne's most renowned Vietnamese food precincts. There is significant capacity for the precinct to further develop and provide additional employment opportunities.

Bridge Road

Bridge Road is changing from an outlet, fashion oriented retailing strip, particular west of Church St. Recently, the south side of the strip between Church and Burnley Streets, has begun to develop a vibrant hospitality offer.

Epworth Hospital is a regional health facility with an associated precinct of medical specialists and other health services around Erin Street. The hospital and associated health sector businesses is a dominant feature of the Bridge Road employment market. Epworth Hospital is designated as a health precinct in Plan Melbourne 2017-2050. Future expansion of the hospital and related health uses is constrained by a lack of capacity, and it is unclear how expansion of medical activities can be accommodated in the precinct. One option may be to identify opportunities to provide capacity at the western end of Bridge Road as part of broader work to define the health precinct. Expansion of health-related activities on Bridge Road would also generate demand for associated commercial services and support the local retail sector.

Buildings in Bridge Road are predominantly low rise but recent development at Epworth Hospital and apartment buildings on the north side of Bridge Road, between Lennox Street and Church Street, have been in the range of four to six storeys.

The Pelaco sign as 21 Goodwood Street, Richmond is a Yarra landmark visible from Bridge Road.

The eastern end of Bridge Road has developed as a mix of discount furniture, homewares, office supplies, other larger floor-space outlets with café and other food outlets in finer grained shops. The centre adjoins the Yarra River but has very limited orientation to the River corridor and the Main Yarra Trail.

The centre section of Bridge Road has a civic, recreational and educational focus near Gleadell Street with the Richmond Town Hall, a new high school and Citizens Park adjoining Highett Street.

Swan Street

Swan Street's diverse range of entertainment and retail activities, public transport options and close proximity to Melbourne's Central Business District and Sports and Entertainment Precinct have attracted residents, visitors and workers to the Activity Centre for well over a 100 years.

The character of the Swan Street varies significantly along the length of Swan Street, moving from a cohesive and highly intact, turn of the century 'High Street' with a prominent fine grain subdivision pattern, to larger bulky goods retailing (furniture and high end vehicles), to a diverse and less historically intact retail and residential area towards the eastern end. It contains a number of landmark heritage commercial buildings, including the Dimmeys Clock Tower and prominent corner hotels.

The western end of Swan Street is home to a vibrant mix of retail activities including hospitality, entertainment, and a new full line supermarket. Redevelopment of the former Dimmey's site, which includes an expanded Coles supermarket, has helped to revitalise the convenience retail offer. The eastern end of the strip is partly zoned C2Z and supports restricted retail and commercial functions. Based on capacity analysis, Swan Street was identified as having a latent capacity under the current zoning provisions. Amendment C191 implements the directions in the Swan Street Structure Plan to re zone land to allow for residential and commercial development. This allows for intensification of development around Burnley Station, potentially leading to further growth in capacity at the street's eastern end.

3.2 Neighbourhood Centres

The neighbourhood centres vary considerably in the range of shopping and services provided and in the capacity for growth. Johnston Street for example has some characteristics similar to a Major Activity Centre, including major road frontages and capacity for growth and redevelopment. Gertrude Street, unlike Johnston Street however has limited capacity for growth based on the heritage significance of buildings and the streetscape. Johnston Street and Gertrude Street centres are connected to Brunswick Street and Smith Street forming a network of retail precincts.

Most of Yarra's neighbourhood centres are constrained by their context, including: small land parcels close to low scale residential areas; landscape character; zoning restrictions; and intact heritage streetscapes and places covered by the heritage overlay. Rathdowne Street, North Carlton centre is a good example of these limitations with parts of the centre in the Neighbourhood Residential Zone and most of it covered by the heritage overlay. It is surrounded by low scale residential area. The character of this centre is also influenced by the strong landscape character in Rathdowne Street defined by mature deciduous street trees.

Queens Parade includes a retail node east of Wellington Street / Delbridge Street, which is a highly valued high street with heritage significance. There is potential for a new node of activity and redevelopment associated with the former gasworks site and other former industrial and commercial areas along the south side of Queens Parade.

Queens Parade (west of Grant and Smith Streets) is characterised by the double avenue street trees including the heritage listed Dutch Elms, framing views along Queens Parade between Brunswick Street and Smith Street. Key heritage places along Queens Parade includes St John's Church, former ANZ Bank at 370 Queens Parade, and the former United Kingdom Hotel located at the junction of Queens Parade and Dummett Crescent / Heidelberg Road. The full width of Queens Parade (60m) and views to the St John's Church spire can be appreciated west of Grant and Smith Streets.

The redevelopment of the Alphington paper mill will see the existing Heidelberg Road retail centre play a somewhat different role in the future. The paper mill redevelopment will see substantial growth in commercial, retail and community facility floor-space, west of the existing small centre, thereby moving its focal point. There will also be significant higher and medium density housing adjoining this new centre.

4. The Role of the Activity Centre Network

Yarra's activity centres operate as a significant element of the economic and social functioning of the municipality. The activity centre network plays an important role as a focus for retail activity, employment and increasingly for growth in higher density housing. Projected growth in the number of jobs and population means the activity centres must have the capacity to accommodate new commercial and apartment development at a scale which meets projections.

4.1 Employment Land Capacity

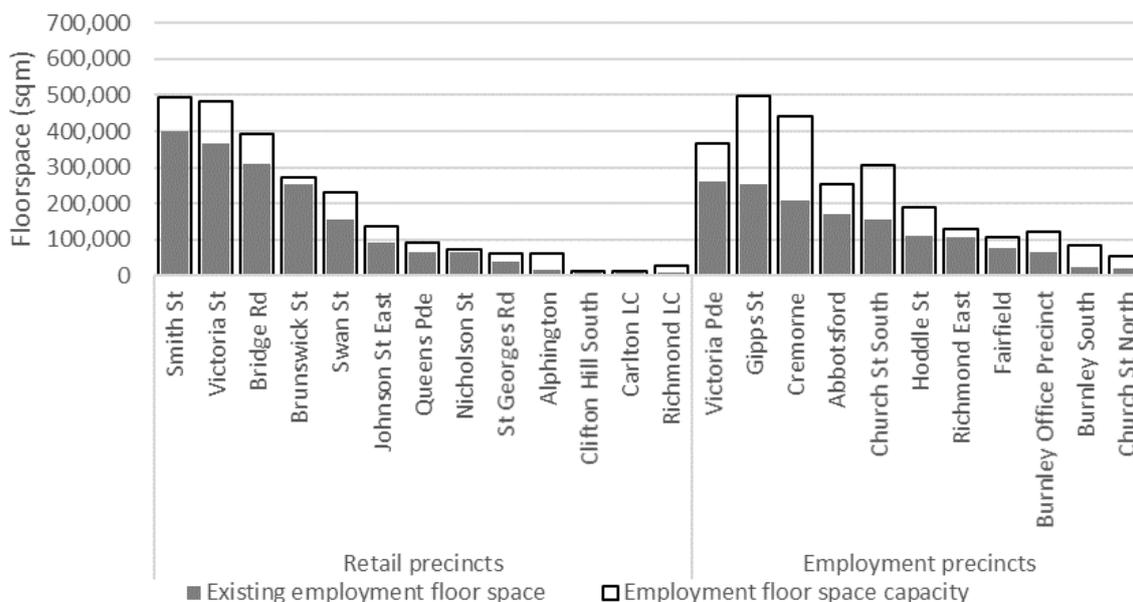
Council's adopted Yarra Spatial Economic and Employment Strategy (SEES), 2018, provides guidance on managing growth and change in employment and economic activity.

In Yarra, economic activity is arranged spatially in different types of employment precincts: activity centres, specialised medical facilities, industrial precincts, mixed employment precincts, educational institutions, and areas where the Comprehensive Development and Priority Development zones have been applied.

Across Yarra's employment precincts, there is sufficient aggregate opportunity to accommodate projected growth to 2031. Employment in Yarra is projected to increase by 50,000 jobs to 2031. Analysis in the SEES indicates that there is around 3.6 million square metres of employment floor space in Yarra. To meet forecasts, Yarra will require an additional 270,000 square metres of employment floor space by 2031. Based on a series of assumptions on development potential under current planning zones there is theoretical capacity for about 4.9 million square metres of employment floor space. This suggests the current commercial / industrial zoned land in Yarra has capacity for additional floor space of around 1.31 million square metres, exceeding the forecast demand for employment floor space.

Figure 2 illustrates employment floor space estimates. It suggests that of the retail precincts, Smith Street, Victoria Street, Bridge Road and Swan Street have the most potential for growth. Brunswick Street appears to have limited capacity, which suggests there might be constraints to realising additional floor space. Alphington on the other hand has significant capacity which will be realised through the redevelopment of the former AMCOR Papermill site.

Figure 2: Employment Floor Space Estimates; Existing vs Capacity



Source: SEES, 2018

4.2 Retail floor-space

Yarra currently supports 910,000sqm of retail floor space. The SEES estimates demand for a further 89,000sqm of additional retail floor space within Yarra to 2031. The majority of retail floor space growth in Yarra can be seen to come from small scale retail developments with 95% of new retail developments between 50sqm and 300sqm.

4.3 Supporting employment growth in activity centres

The SEES includes 6 directions to manage Yarra’s employment land over the next 10-15 years, which includes:

Support employment growth in Yarra’s Activity Centres: Activity Centres are areas that provide a focus for services, employment, housing, transport and social interaction. In Yarra, major activity centres set out in Plan Melbourne include: Bridge Road, Brunswick, Smith, Swan and Victoria Streets. Neighbourhood activity centres include: Johnston Street (east), Queens Parade, Nicholson Street St Georges Road and Alphington. Yarra’s local centres, such as Rathdowne Village and Lygon Street will support modest growth

Yarra’s activity centres are likely to accommodate significant growth in retail, commercial and institutional floor space. It is anticipated that an estimated 134,000 sqm of additional floor space will be added to Yarra’s larger activity centres between 2016 and 2031 for commercial, retail and institutional uses (an increase of around 10%). Based on the capacity assessment set out earlier, the extent of Yarra’s activity centres currently zoned for employment is likely to be sufficient to accommodate this growth. A diverse cross section of employment activities should be supported in centres, including health related businesses in centres that overlap with and adjoin the designated health precincts (Plan Melbourne 2017 – 2050).

Yarra’s smaller neighbourhood and local centres are likely to accommodate modest growth in retail and commercial floor-space largely linked to local population growth. The land

currently zoned for employment within these smaller centres is considered sufficient to accommodate projected growth to 2031.

The SEES also proposes to accommodate projected residential growth in activity centres and mixed use precincts to help relieve pressure for residential redevelopment in employment precincts.

4.4 Housing capacity and population change

Analysis for the Yarra Housing Strategy indicated that in 2016 the resident population of Yarra was approximately 91,300. Yarra has experienced rapid population and housing growth in the past 5 years with an increase of 17,208 people between 2011 and 2016. This represents a 3.7% yearly increase and corresponds with a peak in housing development completions. This is an unprecedented growth rate for Yarra but is forecast to reduce to a yearly growth rate of approximately 2.1%. Over the next 15 years Yarra will need to accommodate a growing and diverse population.

The Victoria in Future 2019 (VIF2019) population and housing projections indicate there will be 32,970 new residents and 16,540 new dwellings required in Yarra, by 2031.

The additional 32,970 new residents will likely form households that are:

- 31% Family households (including single parent families): This proportion translates to 5,093 new households of this type
- 25% Couples households: This proportion translates to 4,177 new households of this type
- 38% Lone person households: This proportion translates to 6,242 new households of this type

Source Victoria in the Future 2019

The Yarra Housing Strategy proposes that residential growth should be directed to the Major activity centres and parts of the Neighbourhood activity centres which can accommodate growth. This approach, directing growth to activity centres; is consistent with state policy, provides housing close to employment, transport and services and is generally consistent with concepts such as the 20 minute city proposed in Plan Melbourne 2017 – 2050. This approach also helps to protect the neighbourhood character of established residential areas in the municipality, in particular those areas with heritage significance.

Recent capacity monitoring for Yarra by SGS Economics as part of expert evidence for Amendment C231 (August 2019) found there is ample dwelling capacity in Yarra's activity centres. Based on planning controls proposed in current amendments there is a total potential capacity of 32,730 dwellings across Yarra's activity centres. This far exceeds the predicted dwelling demand for 16,540 dwellings in all of Yarra to 2031 (VIF2019).

The level of housing growth in these areas will need to vary depending on the individual context and suitability for accommodating housing growth. Directions for guiding future residential growth in these areas will be informed by the current heritage significance, detailed heritage reviews and urban design analysis being undertaken for Yarra's activity centres and will provide guidance on the levels of growth appropriate in each precinct. (Yarra Housing Strategy, 2018).

6.5 Implications of employment and population growth for activity centres

The analysis in the Housing Strategy and the SEES confirms the role of activity centres as a location for commercial and residential development to accommodate jobs and population

growth. The combined analysis as well as capacity monitoring by SGS Economics indicates that the centres have sufficient capacity to accommodate growth within the areas of currently zoned land.

The role played by each centre and by the different types of centres can be summarised as follows:

Employment

- The Major activity centres will play an important role in accommodating employment floor-space growth.
- The retail precincts of, Smith Street, Victoria Street, Bridge Road and Swan Street all have potential for growth in commercial space with the largest employment floor space capacities.
- Brunswick Street appears to have limited capacity and less scope for growth.
- The former Alphington Paper Mill redevelopment will accommodate substantial new commercial and retail floor space.
- The neighbourhood and local centres are likely to accommodate only modest growth in retail and commercial floor-space.
- The activity centres will be part of a mix of locations and precincts which accommodate growth in employment, with other locations including employment precincts at Gipps Street precinct and in Cremorne and other former industrial areas.

Housing

- The majority of housing growth will be accommodated in activity centres or mixed use areas adjoining the activity centres.
- The Major activity centres will see the most significant growth and major change, particularly in those precincts less constrained by heritage or other design constraints.
- Some of the Neighbourhood activity centres have capacity to accommodate growth in identified precincts.
- The activity centre network will play an important role by accommodating housing development relieving pressure for encroachment into other employment areas.

Building form and scale

The location, design and scale of development in each activity centre will be influenced by and in some cases constrained by heritage and local character considerations. This will be addressed by existing structure plans or local area plans and proposed building form analysis and associated Planning Scheme provisions such as the Design and Development Overlay.

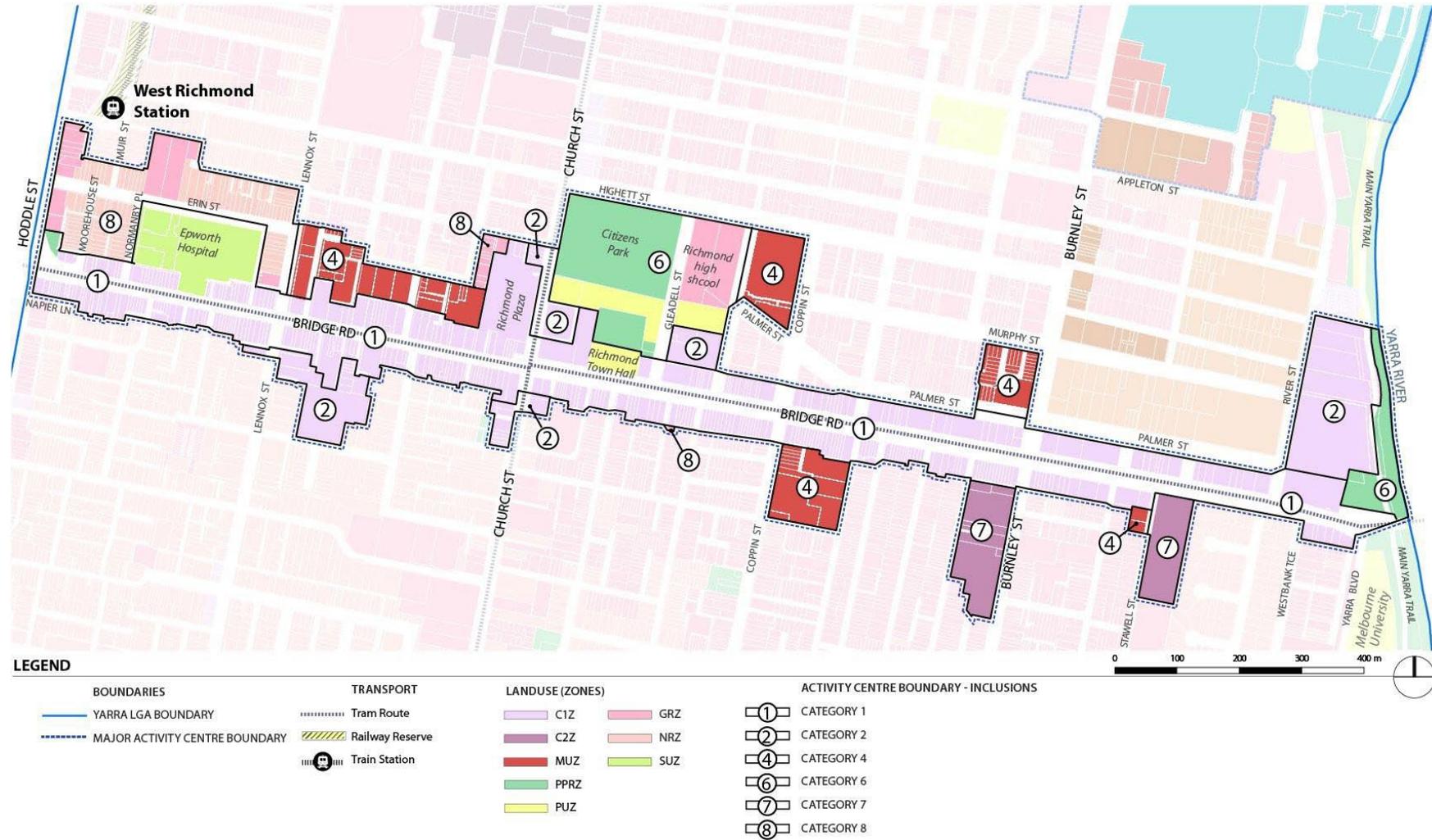
7 Major activity centres –defining boundaries

- Bridge Road – Richmond
- Victoria Street – Richmond
- Smith Street – Collingwood/Fitzroy and Brunswick Street – Fitzroy
- Swan Street – Richmond

The boundary for each centre is mapped and then analysed, based on Planning Practice Note 58 *Structure Planning for Activity Centres*. The tables in this section set out categories defined by Council as a basis on which to assess whether land is included or excluded from the activity centres. The reasons for excluding some land are also discussed.

7.1 Bridge Road

Map 1 Bridge Road



Category	Descriptions	Reasoning
1	<p>Commercial 1, Commercial 2, Mixed Use, Comprehensive Development Zones and civic uses fronting the main street of the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Location of existing government and institutional areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> • <i>Key public land uses that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • All properties front Bridge Road • Properties close to jobs and the tram line along Bridge Road – <i>walkability; proximity to fixed transport; location of existing commercial areas and land uses</i> • C1 zones allow for higher density than residential zones – contribute to providing for <i>sufficient land use for commercial and residential activities</i> • Epworth Hospital – <i>a key public land use</i>. It is an identified Health Precinct in Plan Melbourne, and forms a sub-precinct to the Activity Centre • Richmond town hall – <i>a key public land use</i> • Open space at the west and eastern ends of Bridge Road – <i>Key public open space</i> • Officeworks – SEES recommends site to remain for commercial use – <i>contribute to commercial activities over a 15-20 year timeframe</i> • No impact on the boundaries of other activity centres.
2	<p>Commercial 1 areas not fronting the retail street, but contiguous (generally behind) with Category 1 that do or could have a strong physical or land use connection to the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> 	<ul style="list-style-type: none"> • Properties close to jobs and the tram line along Bridge Road – <i>walkability; proximity to fixed transport; location of existing commercial areas and land uses</i> • C1 Zone allows for higher density than residential zones - contribute to providing for <i>sufficient land use for commercial and residential activities</i> • Pelaco site; Church St – north and south of Bridge Road, Gleadell St – recognition of <i>location of existing commercial areas and land uses; proximity to fixed transport; walkability</i>

	<ul style="list-style-type: none"> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	<ul style="list-style-type: none"> • C1 - River Street sites have been developed - <i>contribute to providing for commercial activities needed over a 15 – 20 year timeframe + then 30 year horizon.</i> • The NHP Electrical site (River St) includes a large car park - potential for redevelopment – <i>recognise the location of existing commercial areas and land uses; proximity to fixed transport; walkability</i> • No impact on the boundaries of other activity centres • Gleadell St – recognises the non-residential use and zoning in proximity to the civic precinct
3	<p>Commercial 2 areas not fronting the retail street, but contiguous (generally behind) with Category 1 that do or could have a strong physical or land use connection to the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	<ul style="list-style-type: none"> • No land in this category.
4	<p>Mixed Use areas that are contiguous with Category 1 and 2 areas and do or could have a strong physical connection to the retail core</p>	<ul style="list-style-type: none"> • Jacques site; north side Bridge Road; Burnley St – north side of Bridge Road; and Stawell St recognises that the zone allows for higher density than NRZ / GRZ. Recognises the higher density that has been

	<p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	<p><i>developed – contribute to providing for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon</i></p>
5	<p>Health</p> <p><i>Relevant PPN58 criteria: Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> 	<p>Epworth Hospital – see Category 1</p>
6	<p>Public open space, community facilities and schools not fronting the main street of the retail core, with a strong functional inter relationship with the activity centre</p> <p><i>Relevant PPN58 criteria: Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> • <i>Location of existing areas of public open space</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Public open space areas that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • Civic Precinct around the Richmond Town Hall – town hall; bowls club; Citizen Park; future Richmond High School site; Council leisure centre. • Public open space at the eastern end connects Bridge Road to the Yarra River. • Main Yarra Trail – potential to develop a stronger link between the Activity Centre and River corridor.

<p>7</p>	<p>Commercial 1 and Commercial 2 fronting a main road that intersects the main street of the retail core, and contiguous with land in another category.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • Location of existing commercial areas and land uses • Commercial and residential needs • Proximity to fixed transport esp fixed rail (train or tram) • Location of transport infrastructure • Walkability • Impacts of boundary on other activity centres • Strategic Redevelopment Sites <p><i>Include:</i> Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</p>	<ul style="list-style-type: none"> • C2 - Burnley Street (south of Bridge Road) the SEES has indicated that this land be retained as C2 zone. • C1 – Church Street (north and south of Bridge Road). Reflects existing uses
<p>8</p>	<p>Residential areas that due to the surrounding uses, built form and road network are logically included</p> <p><i>Relevant PPN58 criteria: Consider</i></p> <ul style="list-style-type: none"> • Location of existing government and institutional areas and land uses • Location of existing areas of public open space <p><i>Include:</i></p> <ul style="list-style-type: none"> • Residential areas that are integrated into the activity centre or surrounded by other uses that have a strong relationship with the activity centre, even where limited development opportunities exist 	<ul style="list-style-type: none"> • Land around Epworth Hospital which is an identified health precinct in Plan Melbourne • Reflects existing land use pattern along Erin Street, which is predominantly consulting rooms or medical related. • Includes the Elms Rehabilitation Centre • Land fronting Hoddle Street includes a mix of accommodation types. • This land is covered by HO338 however, including this land in the activity centre is based on promoting the health related land uses rather than intensification of development. • Muir Street, Normanby Place and Moorehouse Street provide direct pedestrian access to west Richmond Station and the tram along Bridge Road.

9	<p>Large sites or areas with potential to be part of or contribute to an activity centre, if upgraded or redeveloped</p> <p><i>Relevant PPN58 criteria:</i></p> <p><i>Consider:</i> <i>Strategic Redevelopment Sites</i></p>	<ul style="list-style-type: none"> • N/A
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Excluded land

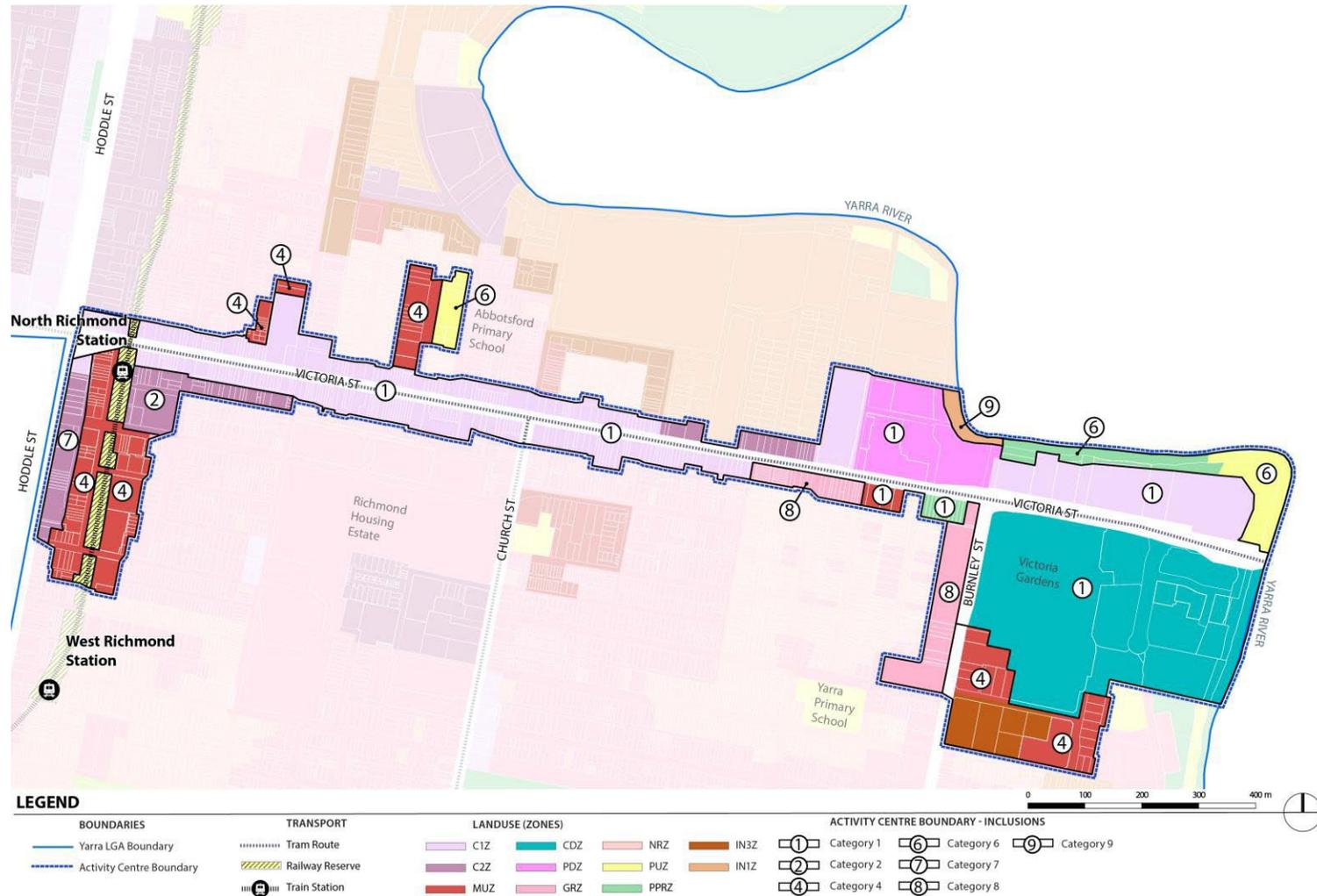
The following table sets out reasons for excluding land that is proximate to the proposed activity centre boundaries. The reasons consider the criteria of PPN58, in particular: environmental and flooding constraints; and residential land encumbered by significant constraints (eg Heritage Overlay_ located at the edge of the activity centre.

Type	Reason
All NRZ and GRZ	This land covers residential areas which are covered by an HO or developed for established lower density housing
Richmond East industrial precinct (adjacent to Burnley / Murphy Streets)	<p>The SEES includes this area of almost 10 hectares generally between Palmer and Appleton Streets and adjacent to land zoned Residential, Mixed Use and Commercial 1. The precinct has access to Bridge Road, Victoria Gardens and the Yarra River. Compared to the Burnley South precinct the existing industrial uses are more local population serving in their orientation, providing an important service role. Lot sizes in this precincts are generally much smaller than those in the Burnley and Abbotsford precincts and would benefit from amalgamation to maximise development potential.</p> <p>The earlier capacity analysis suggests there is no immediate need for industrial zoned land to be converted to accommodate demand for alternative employment uses,</p> <p>The industrial precincts will remain, and not be considered for zone change, until further strategic work is completed.</p>

Land adjacent to West Richmond Station	This land is predominantly low scale residential development and use. It is not related to the health land uses occurring along Erin Street. It is covered by HO 338.
Carpark in Napier Lane (south of Bridge Road)	This land is subject to cluster plan. Redevelopment of the land may be constrained legal agreements about the car park use.
NRZ land between Stawell Street and West Bank Terrace	<p>Buildings are part of the Racecourse, Heritage Overlay Area (HO 331). This Area is significant as:</p> <ul style="list-style-type: none"> • the first public housing estate to be built in Richmond and as an estate developed by Richmond Council. • a symbol of the site of John Wren’s popular trotting track, Richmond Racecourse • Visual homogenous collection of dwellings in a simplified English cottage style • Cul-de-sac planning
Church St (north of Highett Street)	<ul style="list-style-type: none"> • This may form part of a local centre with the Commercial 2 Zone land further north. This area also relates to the Victoria Street activity centre. The Victoria Street Structure Plan 2010 identifies some of this land as employment land.
Melbourne Girls College	<ul style="list-style-type: none"> • The connection between the College and the activity centre is very limited, and its exclusion, does not impact on the potential for the activity centre to accommodate residential or employment capacity.

7.2 Victoria Street

Map 2 Victoria Street



Category	Descriptions	Reasoning
1	<p>Commercial 1, Commercial 2, Mixed Use, Comprehensive Development Zones and civic uses fronting the main street of the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Location of existing government and institutional areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> • <i>Key public land uses that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • Properties front Victoria Street and include Comprehensive Development Zone and Priority Development Zone which allow for higher density development • Properties close to jobs and the tram line along Victoria Street • C1 zone allows for higher density than residential zones • C2 supports commercial growth / opportunities • Williams Reserve – public parkland with a strong relationship with the activity centre
2	<p>Commercial 1 areas not fronting the retail street, but contiguous (generally behind) with Category 1 that do or could have a strong physical or land use connection to the retail core.</p> <p><i>Relevant PPN58 criteria:</i></p>	<ul style="list-style-type: none"> • N/A

	<p><i>consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	
3	<p>Commercial 2 areas not fronting the retail street, but contiguous (generally behind) with Category 1 that do or could have a strong physical or land use connection to the retail core.</p> <p><i>Relevant PPN58 criteria:</i></p> <p><i>consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p>	<ul style="list-style-type: none"> • C2 – Adjacent to North Richmond Station –proximity to fixed transport; walkability

	<ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	
4	<p>Mixed Use areas that are contiguous with Category 1 and 2 areas and do or could have a strong physical connection to the retail core</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	<ul style="list-style-type: none"> • Burnley Street – Appleton / Doonside Streets. Remaining Industrial land (81-95 Burnley Street and 25 Doonside Street) is currently the subject of a rezoning to MUZ. This land is in Precinct 11 of the <i>Victoria Street Structure Plan</i>, adopted 2010, which is identified as a substantial change area and having the potential for mixed use development. • Adjacent to the North Richmond Train station. Reflects opportunities for sustainable land use and development close to public transport – proximity to fixed transport • Properties close to jobs and the tram line along Victoria Street • MUZ – Church Street north of Victoria St; Lithgow Street; Little Charles Street recognises that the zone allows for higher density than NRZ / GRZ
5	<p>Health Precinct</p> <p><i>Relevant PPN58 criteria: Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> 	N/A
6	<p>Public open space, community facilities and schools not fronting the main street of the retail core, with a strong functional inter relationship with the activity centre</p> <p><i>Relevant PPN58 criteria:</i></p>	<ul style="list-style-type: none"> • Abbotsford Primary School Lithgow Street • Open space along the Yarra - forms part of the Main Yarra Trail with the potential for a strong link to activity centre • PUZ land adjoining the Yarra River immediately north of Victoria Street / Barkers Road bridge

	<p><i>Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> • <i>Location of existing areas of public open space</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Public open space areas that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	
7	<p>Commercial 1 and Commercial 2 fronting a main road that intersects the main street of the retail core, and contiguous with land in another category.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i> Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</p>	<ul style="list-style-type: none"> • Land along Hoddle Street which is contiguous to the mixed use land around the north Richmond Station - <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • Contributing to economic and commercial activities in the centre
8	<p>Residential areas that due to the surrounding uses, built form and road network are logically included</p> <p><i>Relevant PPN58 criteria: Consider</i></p>	<ul style="list-style-type: none"> • GRZ2 land fronting Victoria Street and is not covered by a Heritage Overlay (408 – 496 Victoria St). 408-422 Victoria Street – existing townhouse development. Number of properties occupied by a mix of uses (retail, hairdresser, residential) • Burnley Street – from 522 Victoria Street to 34 Burnley Street to reflect the existing higher density built form (part of Victoria St East Urban Design)

	<ul style="list-style-type: none"> • Location of existing government and institutional areas and land uses • Location of existing areas of public open space <p><i>Include:</i></p> <ul style="list-style-type: none"> • Residential areas that are integrated into the activity centre or surrounded by other uses that have a strong relationship with the activity centre, even where limited development opportunities exist 	Framework which identifies the land as <i>potential for higher intensity housing to consolidate the Burnley Street edge of the Residential 1 Zone</i>)
9	<p>Large sites or areas with potential to be part of or contribute to an activity centre, if upgraded or redeveloped</p> <p><i>Relevant PPN58 criteria:</i></p> <p><i>Consider:</i> <i>Strategic Redevelopment Sites</i></p>	<ul style="list-style-type: none"> • N/A

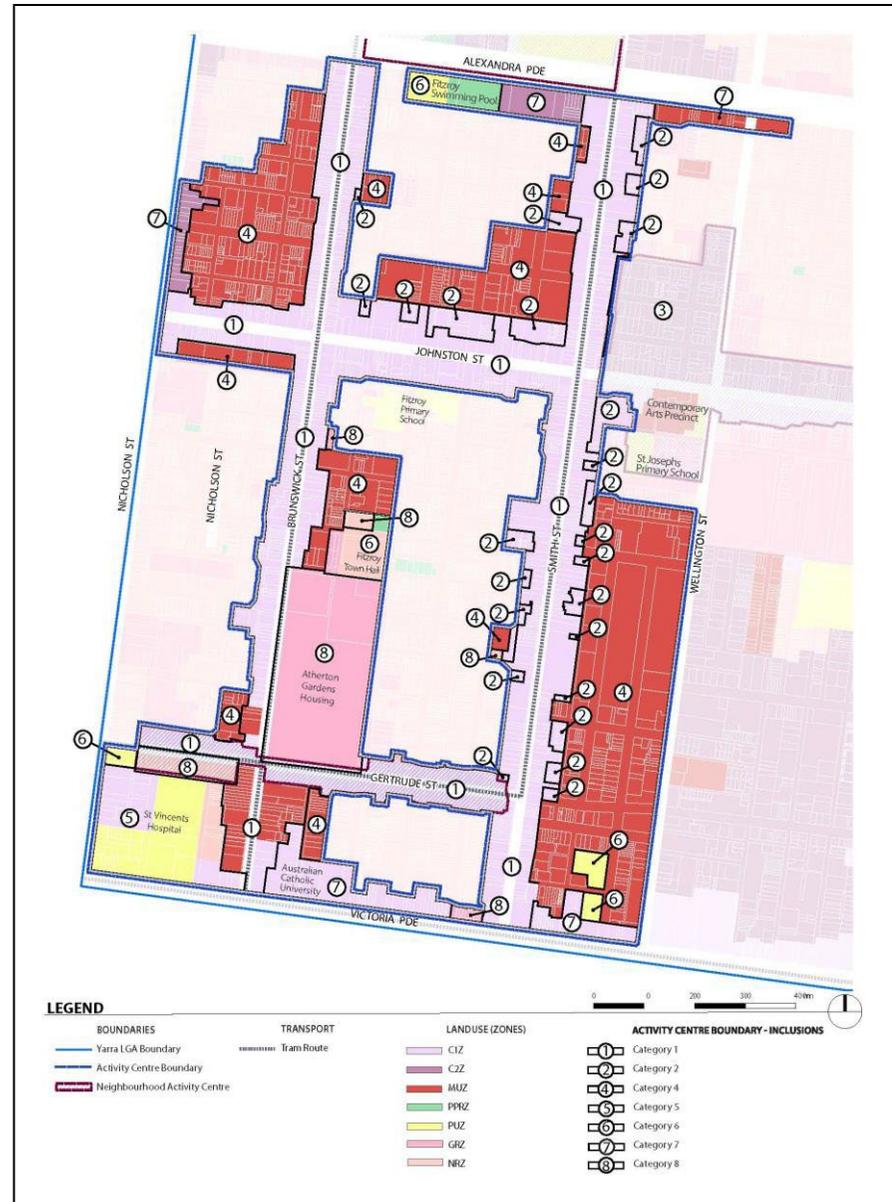
Excluded land

The following table sets out reasons for excluding land that is proximate to the proposed activity centre boundaries. The reasons consider the criteria of PPN58, in particular: environmental and flooding constraints; and residential land encumbered by significant constraints (eg Heritage Overlay_ located at the edge of the activity centre.

Type	Reason
2 – 8 Davison St (corner of Victoria St and Davison)	<ul style="list-style-type: none"> • Land is within HO 363 and zoned NRZ.
CUB and surrounding industrial land	<ul style="list-style-type: none"> • The SEES identified that the future of this precinct is largely dependent on the presence of the CUB which occupies about half the precinct. • If CUB moved it would be subject to a separate strategic planning process to accommodate employment, housing and community uses. Integration with Victoria Street Activity Centre would be a further consideration.
River Street – land south of the CDZ / open space / housing along River Street	<ul style="list-style-type: none"> • Acts as a residential enclave with tree lined streets • Currently only pedestrian access from CDZ to this area

7.3 Smith Street and Brunswick Street

Map 3 Smith Street and Brunswick Street



Category	Descriptions	Reasoning
1	<p>Commercial 1, Commercial 2, Mixed Use, Comprehensive Development Zones and civic uses fronting the main street of the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Location of existing government and institutional areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> • <i>Key public land uses that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • Properties front Smith Street– All C1Z • MUZ, C1Z fronting Brunswick Street • Reflects existing commercial uses, recognises walkability and proximity to tram
2	<p>Commercial 1 areas not fronting the retail street, but contiguous (generally behind) with Category 1 that do or could have a strong physical or land use connection to the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p>	<ul style="list-style-type: none"> • Warehouses, former industrial and commercial properties along Bedford Street – location of existing commercial use, proximity to transport.

	<ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	
3	<p>Commercial 2 areas not fronting the retail street, but contiguous (generally behind) with Category 1 that do or could have a strong physical or land use connection to the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p>	<ul style="list-style-type: none"> • This land is likely to remain C2Z and provide opportunities for redevelopment for office land uses and growth in employment floor-space

	<ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	
4	<p>Mixed Use areas that are contiguous with Category 1 and 2 areas and do or could have a strong physical connection to the retail core</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	<ul style="list-style-type: none"> • MUZ – between Smith and Wellington Streets • MUZ between Brunswick and Smith, reflecting mixed built form • MUZ – between Nicholson and Brunswick, • Provides for higher density development than residential areas in the NRZ and GRZ. The heritage overlay does constrain opportunities in parts of this area. • Recognises walkability and proximity to trams
5	<p>Health Precinct not fronting the main street of the retail core, but contiguous with Category 1 or 2</p> <p><i>Relevant PPN58 criteria: Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> 	<ul style="list-style-type: none"> • St Vincent Hospital is identified in Plan Melbourne 2017-2050 as a Health and Education Precinct. It forms a sub precinct within the AC - – <i>recognising location of institutional areas</i>

<p>6</p>	<p>Public open space, community facilities and schools not fronting the main street of the retail core, with a strong functional inter relationship with the activity centre</p> <p><i>Relevant PPN58 criteria:</i> <i>Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> • <i>Location of existing areas of public open space</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Public open space areas that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • Fitzroy Town Hall – <i>recognising location of institutional areas</i> • Fitzroy Pool and adjoining open space • Local primary school in Cambridge Street • Former Tram engine house Gertrude Street at the corner of Nicholson Street • Public land with a range of institutional, educational and civic land uses
<p>7</p>	<p>Commercial 1 and Commercial 2 fronting a main road that intersects the main street of the retail core, and contiguous with land in another category.</p> <p><i>Relevant PPN58 criteria:</i> <i>consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i> <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i></p>	<ul style="list-style-type: none"> • Land along Alexandra Parade • Victoria Parade between Napier St and Wellington Street • Nicholson St adjoining C1Z and MUZ

8	<p>Residential areas that due to the surrounding uses, built form and road network are logically included</p> <p><i>Relevant PPN58 criteria:</i> <i>Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> • <i>Location of existing areas of public open space</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Residential areas that are integrated into the activity centre or surrounded by other uses that have a strong relationship with the activity centre, even where limited development opportunities exist</i> 	<ul style="list-style-type: none"> • Atherton Gardens public housing estate to recognise the density and importance of the site if it is re-developed in the future
9	<p>Large sites or areas with potential to be part of or contribute to an activity centre, if upgraded or redeveloped</p> <p><i>Relevant PPN58 criteria:</i> <i>Consider:</i> <i>Strategic Redevelopment Sites</i></p>	<ul style="list-style-type: none"> • N/A

Excluded land

The following table sets out reasons for excluding land that is proximate to the proposed activity centre boundaries. The reasons consider the criteria of PPN58, in particular: environmental and flooding constraints; and residential land encumbered by significant constraints (eg Heritage Overlay_ located at the edge of the activity centre.

Type	Reason
Former industrial sites along Rose St (inc McRobertson)	<ul style="list-style-type: none"> • Zoned NRZ1 • Former industrial sites surrounded by finer grain residential development
Fitzroy Primary School	<ul style="list-style-type: none"> • It is separated by a laneway from the C1Z land

	<ul style="list-style-type: none">• It extends into the NRZ1 land – which is surrounded by lower density dwellings
MUZ land Mater Street	<ul style="list-style-type: none">• Not contiguous with AC• Already developed as 3 storey townhouses

7.4 Swan Street

Map 4 Swan Street



LEGEND

BOUNDARIES	TRANSPORT	LANDUSE (ZONES)	ACTIVITY CENTRE BOUNDARY - INCLUSIONS
Yarra LGA Boundary	Tram Route	C1Z	Category 1
Activity Centre Boundary	Railway Reserve	C2Z	Category 2
	Train Station	MUZ	Category 3
		PPRZ	Category 4
		PUZ	Category 5
		GRZ	Category 6
		NRZ	Category 7
			Category 8

Category	Descriptions	Reasoning
1	<p>Commercial 1, Commercial 2, Mixed Use, Zones and civic uses fronting the main street of the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Location of existing government and institutional areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> • <i>Key public land uses that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • Properties front Swan St recognising existing commercial areas and land uses, proximity to fixed public transport, walkability and access to services • C1Z zone allows for higher density than MUZ, NRZ and GRZ
2	<p>Commercial 1 areas not fronting the retail street, but contiguous (generally behind) with Category 1 that do or could have a strong physical or land use connection to the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> 	<ul style="list-style-type: none"> • N/A

	<ul style="list-style-type: none"> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	
3	<p>Commercial 2 areas not fronting the retail street, but contiguous (generally behind) with Category 1 that do or could have a strong physical or land use connection to the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	<ul style="list-style-type: none"> • Recognising commercial uses between Swan St and railway (east of Church St) near Mary and Coppin Street • Include C2Z adjacent north of Stephenson St / Jesse St as it is logical to include the block north of Jesse St and west of Cremorne Street. • Recognises different land uses, proximity to Richmond Station, walkability • Opportunity for higher development adjacent to a non-sensitive interface – railway line, east of Church St • Provides for a broader range of commercial activity

<p>4</p>	<p>Mixed Use areas that are contiguous with Category 1 and 2 areas and do or could have a strong physical connection to the retail core</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	<ul style="list-style-type: none"> • Recognises proximity to Richmond Station and East Richmond Station • Recognises existing apartment development • Recognises a mix of uses • Opportunity for higher development adjacent to a non-sensitive interface – railway line
<p>5</p>	<p>Health Precinct</p> <p><i>Relevant PPN58 criteria: Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> 	<p>N/A</p>
<p>6</p>	<p>Public open space, community facilities and schools not fronting the main street of the retail core, with a strong functional inter relationship with the activity centre</p> <p><i>Relevant PPN58 criteria: Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> 	<ul style="list-style-type: none"> • Ryans Reserve – public land with a strong relationship with the activity centre

	<ul style="list-style-type: none"> • <i>Location of existing areas of public open space</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Public open space areas that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	
7	<p>Commercial 1 and Commercial 2 fronting a main road that intersects the main street of the retail core, and contiguous with land in another category.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i> <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i></p>	<ul style="list-style-type: none"> • CIZ land along Burnley Street north from Swan Street • Relates to the node of retail and other activity around the junction of Burnley and Swan Streets
8	<p>Residential areas that due to the surrounding uses, built form and road network are logically included</p> <p><i>Relevant PPN58 criteria: Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> 	<ul style="list-style-type: none"> • GRZ1 land - Inclusion in the activity centre recognises: <ul style="list-style-type: none"> - the opportunities as a gateway to Swan Street - 6 storey building at 381 Punt Road • Series of smaller zones are anomalies and will be corrected through a rezoning as part of Amendment C191.

	<ul style="list-style-type: none"> • <i>Location of existing areas of public open space</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Residential areas that are integrated into the activity centre or surrounded by other uses that have a strong relationship with the activity centre, even where limited development opportunities exist</i> 	
9	<p>Large sites or areas with potential to be part of or contribute to an activity centre, if upgraded or redeveloped</p> <p><i>Relevant PPN58 criteria:</i></p> <p><i>Consider:</i> <i>Strategic Redevelopment Sites</i></p>	<ul style="list-style-type: none"> • N/A

8 Neighbourhood activity centres – boundaries and categories

- Johnston Street – Collingwood/Abbotsford
- Queens Parade – North Fitzroy/ Clifton Hill
- Nicholson Street – North Fitzroy/North Carlton
- Rathdowne Street – North Carlton
- St Georges Road – North Fitzroy
- Heidelberg Road – Alphington
- Gertrude Street - Fitzroy

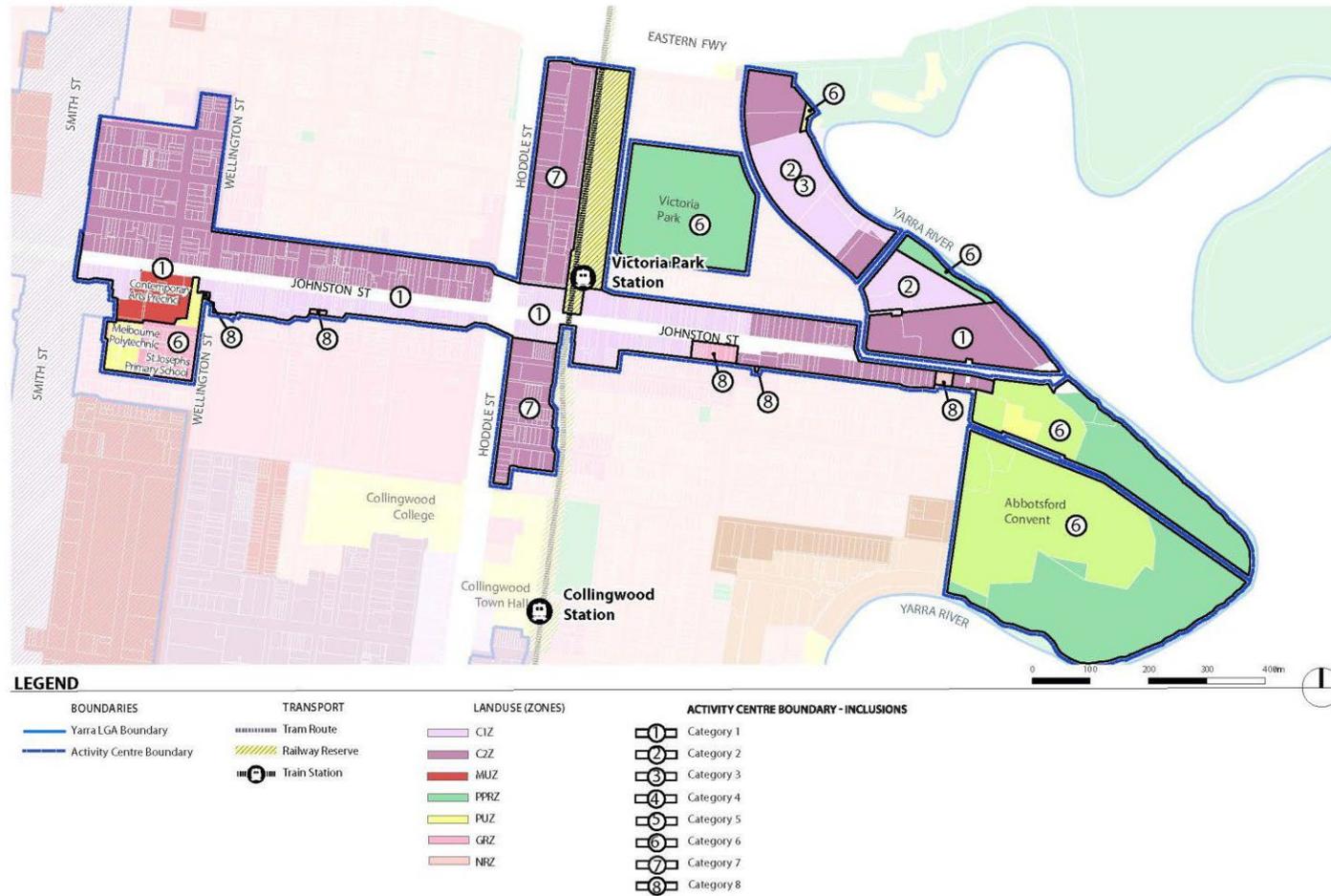
The boundary for each centre is mapped and then analysed, based on the criteria and categories discussed earlier in this report. The reasons for excluding some land are also discussed.



Activity centres – roles and boundaries – April 2022

8.1 Johnston Street – Collingwood/Abbotsford

Map 5 Johnston Street



Category	Descriptions	Reasoning
1	<p>Commercial 1, Commercial 2, Mixed Use, Comprehensive Development Zones and civic uses fronting the main street of the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Location of existing government and institutional areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> • <i>Key public land uses that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • The land identified as the main strip shopping centre precincts in the recent Johnston Street Local Area Plan <ul style="list-style-type: none"> ○ Properties fronting Johnston Street– a mix of C1Z and C2Z from the Yarra River in the east to Smith Street ○ Properties on the south side of Sackville Street between Hoddle Street and Wellington Street • This includes a mix of industrial, office and retail activity • Provides for future growth in retail, residential and office space along or adjoining the main street • Includes C2Z land around Easey Street to provide opportunities for redevelopment for office land uses and growth in employment floor-space
2	<p>Commercial 1 areas not fronting the retail street, but contiguous (generally behind) with Category 1 that do or could have a strong physical or land use connection to the retail core.</p> <p><i>Relevant PPN58 criteria:</i></p>	<ul style="list-style-type: none"> • Land between Trenerry Crescent and the Yarra River has been mostly converted from former industrial uses to a mix of office and higher density residential use. It has and will in the future provide for growth in these sectors. This area also provides a connection to and potentially stronger orientation to the River corridor and landscape. • This includes the sites subject to rezoning (either MUZ or C1) • This area has been included in the JSLAP

	<p><i>consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	
3	<p>Commercial 2 areas not fronting the retail street, but contiguous (generally behind) with Category 1 that do or could have a strong physical or land use connection to the retail core.</p> <p><i>Relevant PPN58 criteria:</i></p> <p><i>consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p>	<ul style="list-style-type: none"> • Includes land along Sackville Street in Category 1 • Trennery Crescent included in the JSLAP

	<ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	
4	<p>Mixed Use areas that are contiguous with Category 1 and 2 areas and do or could have a strong physical connection to the retail core</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	<ul style="list-style-type: none"> • N/A
5	<p>Health Precinct not fronting the main street of the retail core, but contiguous with Category 1 or 2</p> <p><i>Relevant PPN58 criteria: Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> 	<ul style="list-style-type: none"> • N/A

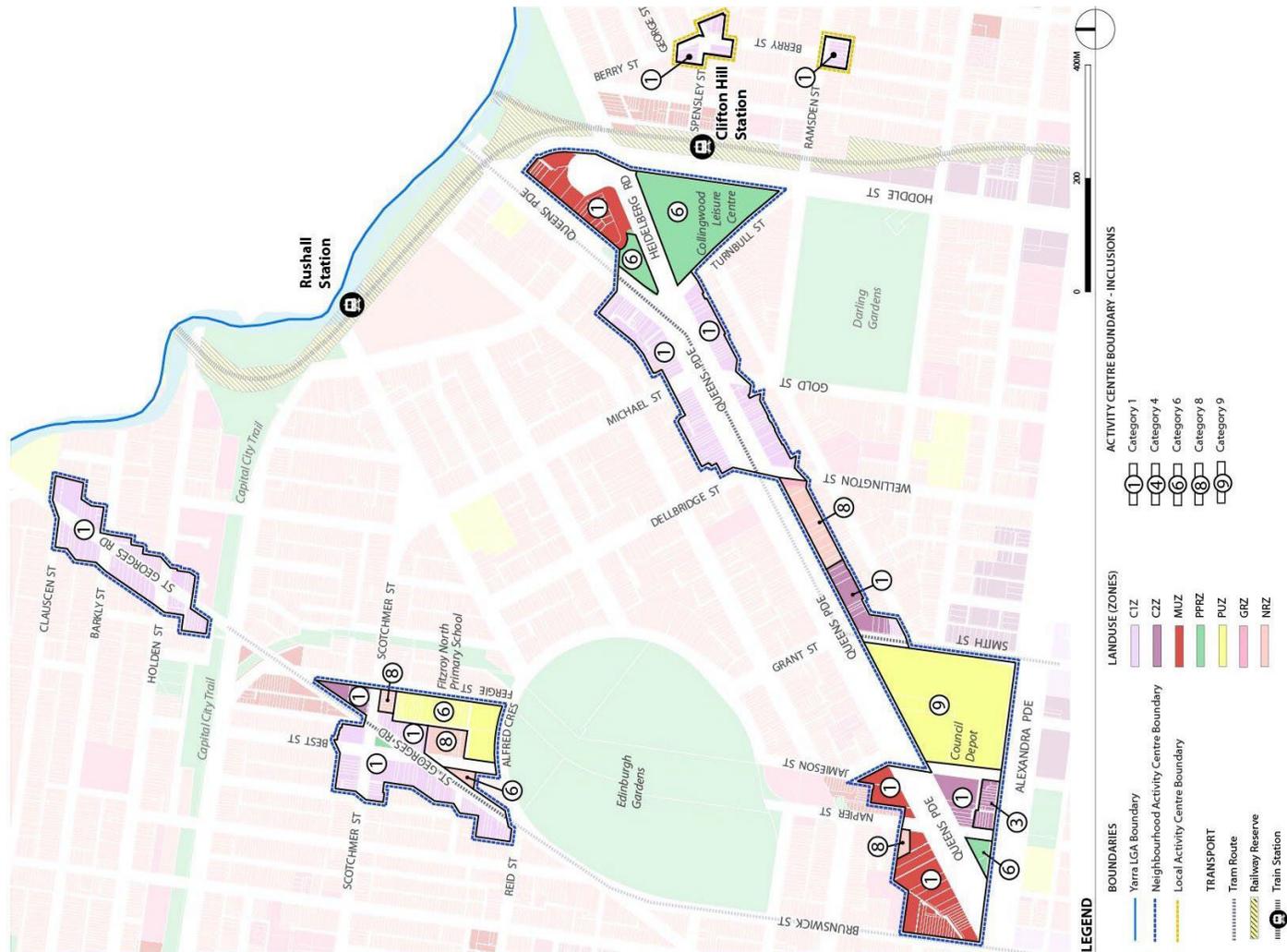
<p>6</p>	<p>Public open space, community facilities and schools not fronting the main street of the retail core, with a strong functional inter relationship with the activity centre</p> <p><i>Relevant PPN58 criteria:</i> <i>Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> • <i>Location of existing areas of public open space</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Public open space areas that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • Victoria Park football ground provides an important open space with a cultural and community focus. It is located between Victoria Park Station, Trenerry Crescent and Turner Street. • Melbourne Polytechnic (Collingwood Campus), Otter St Collingwood • St Joseph’s Primary School, Otter St Collingwood • The Collingwood Arts Precinct between Perry Street, Otter Street, Wellington Street and Smith Street is identified in the JSLAP. It includes Collingwood TAFE and Circus Oz. It will provide a focus for arts, community and cultural activities. • Abbotsford Convent and Collingwood Children’s Farm are east of Clarke Street and have a long frontage to the Yarra River. They provide a local and regional attraction for cultural and recreational activity
<p>7</p>	<p>Commercial 1 and Commercial 2 fronting a main road that intersects the main street of the retail core, and contiguous with land in another category.</p> <p><i>Relevant PPN58 criteria:</i> <i>consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i> <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i></p>	<ul style="list-style-type: none"> • 2 precincts in the JSLAP are along the east side of Hoddle Street in the C2Z these areas provide opportunity for uses that complement the commercial activity along Johnston St: <ul style="list-style-type: none"> - Land north of Johnston Street to the Eastern Freeway and including land on both the west and east side of the railway corridor through to Lulie Street - Land south of Johnston Street between Hoddle Street and the railway to Vere Street

<p>8</p>	<p>Residential areas that due to the surrounding uses, built form and road network are logically included</p> <p><i>Relevant PPN58 criteria:</i></p> <p><i>Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> • <i>Location of existing areas of public open space</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Residential areas that are integrated into the activity centre or surrounded by other uses that have a strong relationship with the activity centre, even where limited development opportunities exist</i> 	<ul style="list-style-type: none"> • Two parcels fronting the south side of Johnston Street immediately east of Paterson Street in the NRZ • An area west of Nicholson Street fronting the south side of Johnston Street on the GRZ • A number of minor boundary anomalies where land in the GRZ or NRZ is within the AC boundary
<p>9</p>	<p>Large sites or areas with potential to be part of or contribute to an activity centre, if upgraded or redeveloped</p> <p><i>Relevant PPN58 criteria:</i></p> <p><i>Consider:</i></p> <p><i>Strategic Redevelopment Sites</i></p>	<ul style="list-style-type: none"> • No areas in this category

Activity centres – roles and boundaries – April 2022

8.2 Queens Parade – North Fitzroy/ Clifton Hill

Map 6 Queens Parade - St Georges Road (Local Activity Centres: Spensley Street – Berry Street – Ramsden Street)



Category	Descriptions	Reasoning
1	<p>Commercial 1, Commercial 2, Mixed Use, Comprehensive Development Zones and civic uses fronting the main street of the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Location of existing government and institutional areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> • <i>Key public land uses that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • The main retail centre is located in C1Z land on both the north and south sides of Queens Parade between Wellington Street / Delbridge Street and the junction of Queens Parade and Heidelberg Road. • Convenience retail and food offerings are stronger on the north side. • Significant residential redevelopment either exists or is proposed for areas of MUZ between Jamieson Street and Brunswick Street on the north side of Queens Parade • These areas are remote from the retail centre but close to the redevelopment opportunities of the former gasworks land on the opposite (south) side of Queens Parade. This area is likely to see educational, commercial and civic activities. • Land between Queens Parade, Heidelberg Road and Hoddle Street in the MUZ will be redeveloped for a mix of activities including higher density housing
2	<p>Commercial 1 areas not fronting the retail street, but contiguous (generally behind) with Category 1 that do or could have a strong physical or land use connection to the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p>	<ul style="list-style-type: none"> • No areas in this category

	<ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	
3	<p>Commercial 2 areas not fronting the retail street, but contiguous (generally behind) with Category 1 that do or could have a strong physical or land use connection to the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p>	<ul style="list-style-type: none"> • Land fronting Alexandra Parade – opportunities associate with land to the north and to amalgamate with other land in the block of C2Z land

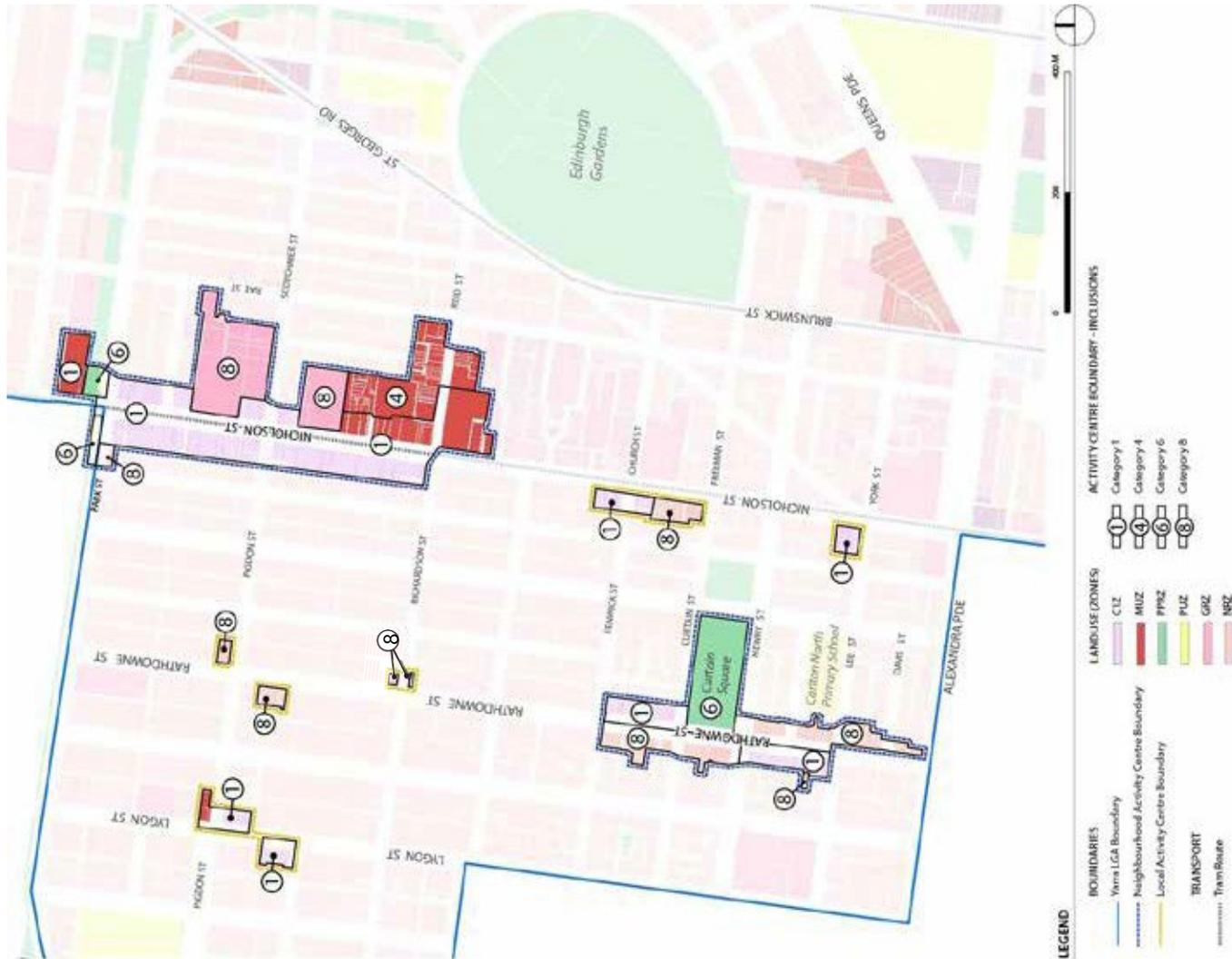
	<ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	
4	<p>Mixed Use areas that are contiguous with Category 1 and 2 areas and do or could have a strong physical connection to the retail core</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	<ul style="list-style-type: none"> • No areas in this category
5	<p>Health Precinct not fronting the main street of the retail core, but contiguous with Category 1 or 2</p> <p><i>Relevant PPN58 criteria: Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> 	<ul style="list-style-type: none"> • No areas in this category

<p>6</p>	<p>Public open space, community facilities and schools not fronting the main street of the retail core, with a strong functional inter relationship with the activity centre</p> <p><i>Relevant PPN58 criteria:</i> <i>Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> • <i>Location of existing areas of public open space</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Public open space areas that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • Land between Turnbull Street, Heidelberg Road and Hoddle Street is public open space with tennis courts and the Collingwood Leisure Centre. It provides a significant focus for recreational activities. • A small pocket of open space at the west end of the Queens Parade has a direct link with the Activity Centre.
<p>7</p>	<p>Commercial 1 and Commercial 2 fronting a main road that intersects the main street of the retail core, and contiguous with land in another category.</p> <p><i>Relevant PPN58 criteria:</i> <i>consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i> <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i></p>	<ul style="list-style-type: none"> • No areas in this category

<p>8</p>	<p>Residential areas that due to the surrounding uses, built form and road network are logically included</p> <p><i>Relevant PPN58 criteria:</i> <i>Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> • <i>Location of existing areas of public open space</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Residential areas that are integrated into the activity centre or surrounded by other uses that have a strong relationship with the activity centre, even where limited development opportunities exist</i> 	<ul style="list-style-type: none"> • A small area in the NRZ on the south side of Queens Parade between Wellington Street and Smith Street provides a link between the retail centre east of Wellington Street and the future commercial and educational redevelopment around the former gasworks land. • Includes land occupied by the Church and school • 497 Napier Street - a site inspection shows that this site has been developed as part of land at 58 Queens Parade. The AC boundary makes a logical line along the northern boundary of this property with fine grained housing to the north.
<p>9</p>	<p>Large sites or areas with potential to be part of or contribute to an activity centre, if upgraded or redeveloped</p> <p><i>Relevant PPN58 criteria:</i></p> <p><i>Consider:</i> <i>Strategic Redevelopment Sites</i></p>	<ul style="list-style-type: none"> • The former gasworks (PUZ1 and PUZ6) land between Smith Street and George Street with frontages to both Queens Parade and Alexandra Parade is likely to provide opportunities for educational, cultural and leisure activities. Subject to a Government Standing Land Advisory Committee (GSLAC) process.

8.3 Nicholson Street – North Fitzroy/North Carlton

Map 7 Nicholson Street - Rathdowne Street and Lygon Street



Category	Descriptions	Reasoning
1	<p>Commercial 1, Commercial 2, Mixed Use, Comprehensive Development Zones and civic uses fronting the main street of the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Location of existing government and institutional areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> • <i>Key public land uses that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • Properties fronting Nicholson Street– all C1Z and MUZ • Primary retail core at and north of Pigdon Street on the west side of Nicholson Street • C1Z south of Pigdon Street includes a mix of retail, office and residential use • MUZ on the east side of Nicholson Street, immediately north of Park Street and Inner Circle parklands. This area includes a designated Strategic Development site in the MSS.
2	<p>Commercial 1 areas not fronting the retail street, but contiguous (generally behind) with Category 1 that do or could have a strong physical or land use connection to the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p>	<ul style="list-style-type: none"> • No areas in this category

	<ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	
3	<p>Commercial 2 areas not fronting the retail street, but contiguous (generally behind) with Category 1 that do or could have a strong physical or land use connection to the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p>	<ul style="list-style-type: none"> • No areas in this category

	<ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	
4	<p>Mixed Use areas that are contiguous with Category 1 and 2 areas and do or could have a strong physical connection to the retail core</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	<ul style="list-style-type: none"> • MUZ – east Nicholson Street, north and south of Reid Street and extending to Rae Street, with a mix of parcel sizes and building forms. Provides some scope for more intensive mixed use development but is constrained by proximity to GRZ and NRZ land and interface considerations.
5	<p>Health Precinct not fronting the main street of the retail core, but contiguous with Category 1 or 2</p> <p><i>Relevant PPN58 criteria: Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> 	<ul style="list-style-type: none"> • No areas in this category

<p>6</p>	<p>Public open space, community facilities and schools not fronting the main street of the retail core, with a strong functional inter relationship with the activity centre</p> <p><i>Relevant PPN58 criteria:</i> <i>Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> • <i>Location of existing areas of public open space</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Public open space areas that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • Public open space adjoining mixed use and commercial zones. This includes land adjoining the north side of Park Street which is part of the Inner Circle railway parklands. • This land has a strong inter-relationship with the centre and includes a café and cycle shop adjoining the parkland immediately west of Nicholson Street. The linear parkland serves a range of recreational purposes and links to other local and neighbourhood centres.
<p>7</p>	<p>Commercial 1 and Commercial 2 fronting a main road that intersects the main street of the retail core, and contiguous with land in another category.</p> <p><i>Relevant PPN58 criteria:</i> <i>consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i> <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i></p>	<ul style="list-style-type: none"> • No areas in this category

<p>8</p>	<p>Residential areas that due to the surrounding uses, built form and road network are logically included</p> <p><i>Relevant PPN58 criteria:</i></p> <p><i>Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> • <i>Location of existing areas of public open space</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Residential areas that are integrated into the activity centre or surrounded by other uses that have a strong relationship with the activity centre, even where limited development opportunities exist</i> 	<ul style="list-style-type: none"> • GRZ2 area on the east side of Nicholson Street – just south of Scotchmer Street, adjoins and is opposite Category 1 and retail core. This land has been developed for medium density housing and is unlikely to be redeveloped in the foreseeable future. • NRZ1 land on the south side of Park Street, between Nicholson Street and Station Street. This land is occupied by small shops which face the parkland, a café and playground on the north side of Park Street. The shops complement the activities along the street and parkland. • GRZ3 area on the east side of Nicholson Street and immediately north of Scotchmer Street. This area is a designated Strategic Development site in the MSS. This land faces the retail core and could be redeveloped to strengthen the retail function and character of the centre. It is occupied by a bus depot.
<p>9</p>	<p>Large sites or areas with potential to be part of or contribute to an activity centre, if upgraded or redeveloped</p> <p><i>Relevant PPN58 criteria:</i></p> <p><i>Consider:</i></p> <p><i>Strategic Redevelopment Sites</i></p>	<ul style="list-style-type: none"> • No areas in this category

8.4 Rathdowne Street – North Carlton

See Map 7

Category	Descriptions	Reasoning
1	<p>Commercial 1, Commercial 2, Mixed Use, Comprehensive Development Zones and civic uses fronting the main street of the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Location of existing government and institutional areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> • <i>Key public land uses that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • Properties front Rathdowne Street– all C1Z • Primary retail core areas are in two small blocks <ul style="list-style-type: none"> ○ East side of Rathdowne between Fenwick Street and Curtain Street ○ West side of Rathdowne between Newry Street and Lee Street
2	<p>Commercial 1 areas not fronting the retail street, but contiguous (generally behind) with Category 1 that do or could have a strong</p>	<ul style="list-style-type: none"> • No areas in this category

	<p>physical or land use connection to the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	
3	<p>Commercial 2 areas not fronting the retail street, but contiguous (generally behind) with Category 1 that do or could have a strong physical or land use connection to the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> 	<ul style="list-style-type: none"> • No areas in this category

	<ul style="list-style-type: none"> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	
4	<p>Mixed Use areas that are contiguous with Category 1 and 2 areas and do or could have a strong physical connection to the retail core</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	<ul style="list-style-type: none"> • No areas in this category
5	<p>Health Precinct not fronting the main street of the retail core, but contiguous with Category 1 or 2</p> <p><i>Relevant PPN58 criteria: Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> 	<ul style="list-style-type: none"> • No areas in this category

6	<p>Public open space, community facilities and schools not fronting the main street of the retail core, with a strong functional inter relationship with the activity centre</p> <p><i>Relevant PPN58 criteria:</i> <i>Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> • <i>Location of existing areas of public open space</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Public open space areas that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • Curtain Square is parkland which adjoins the retail core of this centre and adds to the attractiveness, function and character of the centre
7	<p>Commercial 1 and Commercial 2 fronting a main road that intersects the main street of the retail core, and contiguous with land in another category.</p> <p><i>Relevant PPN58 criteria:</i> <i>consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p>	<ul style="list-style-type: none"> • No areas in this category

	<p><i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i></p>	
8	<p>Residential areas that due to the surrounding uses, built form and road network are logically included</p> <p><i>Relevant PPN58 criteria:</i></p> <p><i>Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> • <i>Location of existing areas of public open space</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Residential areas that are integrated into the activity centre or surrounded by other uses that have a strong relationship with the activity centre, even where limited development opportunities exist</i> 	<ul style="list-style-type: none"> • Two areas in the NRZ1 adjoin or are opposite the retail areas in Category 1. <ul style="list-style-type: none"> ○ Land on the west side of Rathdowne Street between Fenwick Street and Newry Street includes a mix of cafes, shops, dwellings and the North Carlton Library. The café and shop activities complement the more intense retail and Curtain Square on the opposite frontage ○ Land on the east side of Rathdowne Street between Newry Street and Princes Street includes a mix of cafes, shops and dwellings. The café and shop activities complement the more intense retail and Curtain Square on the opposite frontage, north of Lee Street • One small section of NRZ1 is located immediately west of the C1Z just north of Lee Street. This land is part of two lots which extend across the zone boundary and front Rathdowne Street in the C1Z.
9	<p>Large sites or areas with potential to be part of or contribute to an activity centre, if upgraded or redeveloped</p> <p><i>Relevant PPN58 criteria:</i></p> <p><i>Consider:</i></p> <p><i>Strategic Redevelopment Sites</i></p>	<ul style="list-style-type: none"> • N/A

8.5 St Georges Road – North Fitzroy

See Map 6

Category	Descriptions	Reasoning
1	<p>Commercial 1, Commercial 2, Mixed Use, Comprehensive Development Zones and civic uses fronting the main street of the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Location of existing government and institutional areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> • <i>Key public land uses that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • There are two main retail nodes <ul style="list-style-type: none"> ○ The southern area extends from Reid Street to Fergie Street along St Georges Road and extends east and west along Scotchmer Street. Most of this land is in the C1Z but a small section north of Scotchmer Street is C2Z. The focus of activity is near the junction of Scotchmer Street with St Georges Road and the Piedimontes supermarket ○ The northern area extends from the Inner Circle parkland north to Clausen Street. This area is separated from the other area around Scotchmer Street by about 600m
2	<p>Commercial 1 areas not fronting the retail street, but contiguous (generally behind) with Category 1 that do or could have a strong</p>	<ul style="list-style-type: none"> • No areas in this category

	<p>physical or land use connection to the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	
3	<p>Commercial 2 areas not fronting the retail street, but contiguous (generally behind) with Category 1 that do or could have a strong physical or land use connection to the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> 	<ul style="list-style-type: none"> • No areas in this category

	<ul style="list-style-type: none"> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	
4	<p>Mixed Use areas that are contiguous with Category 1 and 2 areas and do or could have a strong physical connection to the retail core</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	<ul style="list-style-type: none"> • No areas in this category
5	<p>Health Precinct not fronting the main street of the retail core, but contiguous with Category 1 or 2</p> <p><i>Relevant PPN58 criteria: Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> 	<ul style="list-style-type: none"> • No areas in this category

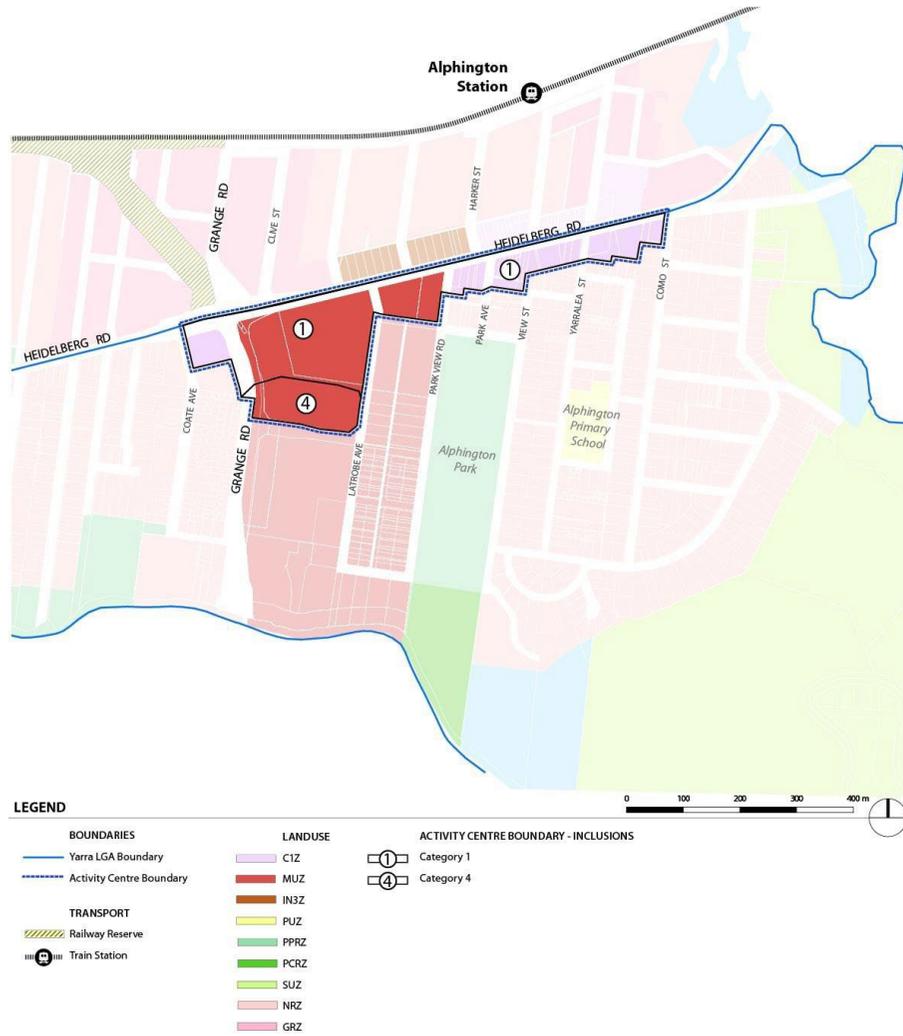
6	<p>Public open space, community facilities and schools not fronting the main street of the retail core, with a strong functional inter relationship with the activity centre</p> <p><i>Relevant PPN58 criteria:</i> <i>Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> • <i>Location of existing areas of public open space</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Public open space areas that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • The Government Fitzroy North primary school is located on land bounded by Fergie Street, Alfred Crescent and Best Street. This school adjoins the new community centre and library at the junction of St Georges Road and Best Street and Edinburgh Gardens to the south. • The site of the new Bargoonga Nganjin, North Fitzroy Library, at the corner of Best Street and St Georges Road is in the NRZ1. It is an important addition to the community facilities in the centre.
7	<p>Commercial 1 and Commercial 2 fronting a main road that intersects the main street of the retail core, and contiguous with land in another category.</p> <p><i>Relevant PPN58 criteria:</i> <i>consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p>	<ul style="list-style-type: none"> • No areas in this category

	<p><i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i></p>	
8	<p>Residential areas that due to the surrounding uses, built form and road network are logically included</p> <p><i>Relevant PPN58 criteria:</i></p> <p><i>Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> • <i>Location of existing areas of public open space</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Residential areas that are integrated into the activity centre or surrounded by other uses that have a strong relationship with the activity centre, even where limited development opportunities exist</i> 	<ul style="list-style-type: none"> • One small area in the NRZ1 on the south side of Scotchmer Street between St Georges Road and Fergie Street is a mix of shops and dwellings. This land faces commercial and retail activity in the C2Z. It complements the adjoining retail activity and is close to the retail core. • NRZ in Bent St is between the school, library and commercial land and is occupied by converted warehouses.
9	<p>Large sites or areas with potential to be part of or contribute to an activity centre, if upgraded or redeveloped</p> <p><i>Relevant PPN58 criteria:</i></p> <p><i>Consider:</i></p> <p><i>Strategic Redevelopment Sites</i></p>	<ul style="list-style-type: none"> • N/A

Activity centres – roles and boundaries – April 2022

8.6 Heidelberg Road – Alphington

Map 8 Heidelberg Road



Category	Descriptions	Reasoning
1	<p>Commercial 1, Commercial 2, Mixed Use, Comprehensive Development Zones and civic uses fronting the main street of the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Location of existing government and institutional areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> • <i>Key public land uses that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • This centre is very small with few retail businesses and very little weekly shopping. The existing C1Z land is in two parts: <ul style="list-style-type: none"> ○ Land on the south side of Heidelberg Road between Parkview Road and Como Street ○ Land on the north side of Heidelberg Road between Harker Street and Yarralea Street. This is in the City of Darebin and outside the scope of the YPS re-write. • Future development in this centre is likely to be strongly influenced by the Alphington Paper Mill redevelopment (former AMCOR site) • A parcel of land between Coate Avenue and Grange Road on the south side of Heidelberg Road is zoned C1Z and will become part of the larger centre as Alphington Paper Mill develops.
2	<p>Commercial 1 areas not fronting the retail street, but contiguous (generally behind) with Category 1 that do or could have a strong physical or land use connection to the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p>	<ul style="list-style-type: none"> • No areas in this category

	<ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	
3	<p>Commercial 2 areas not fronting the retail street, but contiguous (generally behind) with Category 1 that do or could have a strong physical or land use connection to the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p>	<ul style="list-style-type: none"> • No areas in this category

	<ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	
4	<p>Mixed Use areas that are contiguous with Category 1 and 2 areas and do or could have a strong physical connection to the retail core</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	<ul style="list-style-type: none"> • Parts of the Alphington Paper Mill redevelopment will in the future become a new activity centre or extension of the current small centre. The Alphington Paper Mill redevelopment will be staged over 7-12 years with the retail and commercial components potentially built from around 2024 to 2030. This will see a new neighbourhood centre established with non-residential activities including office, community and retail which will total 24,050m² – 33,450m² with retail as a sub-set of about 13 – 15,000m². • The extent of the land in this new centre is defined in the approved Development Plan and is generally bounded by Parkview Road, La Trobe Avenue and future internal roads within the redevelopment.
5	<p>Health Precinct not fronting the main street of the retail core, but contiguous with Category 1 or 2</p> <p><i>Relevant PPN58 criteria: Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> 	<ul style="list-style-type: none"> • No areas in this category

<p>6</p>	<p>Public open space, community facilities and schools not fronting the main street of the retail core, with a strong functional inter relationship with the activity centre</p> <p><i>Relevant PPN58 criteria:</i> <i>Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> • <i>Location of existing areas of public open space</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Public open space areas that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • No areas in this category
<p>7</p>	<p>Commercial 1 and Commercial 2 fronting a main road that intersects the main street of the retail core, and contiguous with land in another category.</p> <p><i>Relevant PPN58 criteria:</i> <i>consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i> <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i></p>	<ul style="list-style-type: none"> • No areas in this category

<p>8</p>	<p>Residential areas that due to the surrounding uses, built form and road network are logically included</p> <p><i>Relevant PPN58 criteria:</i> <i>Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> • <i>Location of existing areas of public open space</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Residential areas that are integrated into the activity centre or surrounded by other uses that have a strong relationship with the activity centre, even where limited development opportunities exist</i> 	<ul style="list-style-type: none"> • No areas in this category
<p>9</p>	<p>Large sites or areas with potential to be part of or contribute to an activity centre, if upgraded or redeveloped</p> <p><i>Relevant PPN58 criteria:</i> <i>Consider:</i> <i>Strategic Redevelopment Sites</i></p>	<ul style="list-style-type: none"> • N/A

8.7 Gertrude Street – Fitzroy

See Map 3

Category	Descriptions	Reasoning
1	<p>Commercial 1, Commercial 2, Mixed Use, Comprehensive Development Zones and civic uses fronting the main street of the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Location of existing government and institutional areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> • <i>Key public land uses that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • C1Z land fronting Gertrude Street between Smith Street and Nicholson Street
2	<p>Commercial 1 areas not fronting the retail street, but contiguous (generally behind) with Category 1 that do or could have a strong physical or land use connection to the retail core.</p>	<ul style="list-style-type: none"> • No areas in this category

	<p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	
3	<p>Commercial 2 areas not fronting the retail street, but contiguous (generally behind) with Category 1 that do or could have a strong physical or land use connection to the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> 	<ul style="list-style-type: none"> • No land in this category

	<p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	
4	<p>Mixed Use areas that are contiguous with Category 1 and 2 areas and do or could have a strong physical connection to the retail core</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> 	<ul style="list-style-type: none"> • No land in this category
5	<p>Health, civic and cultural precincts not fronting the main street of the retail core, but contiguous with Category 1 or 2</p> <p><i>Relevant PPN58 criteria: Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> 	<ul style="list-style-type: none"> • No land in this category

<p>6</p>	<p>Public open space, community facilities and schools not fronting the main street of the retail core, with a strong functional inter relationship with the activity centre</p> <p><i>Relevant PPN58 criteria:</i> <i>Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> • <i>Location of existing areas of public open space</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Public open space areas that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • No land in this category
<p>7</p>	<p>Commercial 1 and Commercial 2 fronting a main road that intersects the main street of the retail core, and contiguous with land in another category.</p> <p><i>Relevant PPN58 criteria:</i> <i>consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i> <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i></p>	<ul style="list-style-type: none"> • No areas in this category

<p>8</p>	<p>Residential areas that due to the surrounding uses, built form and road network are logically included</p> <p><i>Relevant PPN58 criteria:</i> <i>Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> • <i>Location of existing areas of public open space</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Residential areas that are integrated into the activity centre or surrounded by other uses that have a strong relationship with the activity centre, even where limited development opportunities exist</i> 	<ul style="list-style-type: none"> • NRZ1 land on the south side of Gertrude Street between Nicholson and Brunswick Streets
<p>9</p>	<p>Large sites or areas with potential to be part of or contribute to an activity centre, if upgraded or redeveloped</p> <p><i>Relevant PPN58 criteria:</i></p> <p><i>Consider:</i> <i>Strategic Redevelopment Sites</i></p>	<ul style="list-style-type: none"> • N/A

9 Local centres – boundaries and categories

These small local centres offer a limited range of local services such as cafes, restaurants, milk bar or newsagent. In many cases the amount of local weekly shopping has declined in these centres as this type of shopping has shifted to supermarkets in larger centres.

- Lygon Street – North Carlton / Princes Hill
- Rathdowne Street – North Carlton
- Nicholson Street – North Carlton
- Spensely Street - Clifton Hill
- Berry Street - Clifton Hill

Analysis and categories in tables

The Local Centres include a much more limited mix of the categories set out in earlier parts of this report. The tables below apply only category 1 and 8.



9.1 Lygon Street – North Carlton / Princes Hill

See Map 7

This centre is located at and south of the junction of Pigdon Street and Lygon Street. It includes two groups of properties in the C1Z and land north of Pigdon Street in the MUZ. The extent is shown on the map below.

Category	Descriptions	Reasoning
1	<p>Commercial 1, Commercial 2, Mixed Use, Comprehensive Development Zones and civic uses fronting the main street of the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Location of existing government and institutional areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> • <i>Key public land uses that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • C1Z land fronting Lygon Street at the junction with Pigdon Street and extending south to Paterson Street, Princes Hill • A parcel of MUZ land immediately north of Pigdon Street

9.2 Rathdowne Street – North Carlton

See Map 7

This 'centre' is in three parts, located between Pigdon Street and Richardson Street. It includes two groups of shops or former shops and a hotel north of Pigdon Street. All these properties are in the NRZ1. The areas between the groups of shops or former shops are consistently residential. The extent of these areas is shown on the map below.

Category	Descriptions	Reasoning
8	<p>Residential areas that due to the surrounding uses, built form and road network are logically included</p> <p><i>Relevant PPN58 criteria:</i></p> <p><i>Consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing government and institutional areas and land uses</i> • <i>Location of existing areas of public open space</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Residential areas that are integrated into the activity centre or surrounded by other uses that have a strong relationship with the activity centre, even where limited development opportunities exist</i> 	<ul style="list-style-type: none"> • NRZ1 land on both the east and west side of Rathdowne Street between Pigdon and Richardson Streets. The properties include the Great Northern Hotel at the NE corner of Pigdon Street and clusters of shops and former shops. The activities include cafes and other food outlets and small businesses providing a range of services. • There are few if any shops providing for local weekly shopping. • The clusters of shops are separated by consistent housing development.

9.3 Nicholson Street (south) – North Carlton

See Map 7

Category	Descriptions	Reasoning
1	<p>Commercial 1, Commercial 2, Mixed Use, Comprehensive Development Zones and civic uses fronting the main street of the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Location of existing government and institutional areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> • <i>Key public land uses that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • C1Z land fronting Nicholson Street in two areas: <ul style="list-style-type: none"> ○ North and south of Fenwick Street ○ North of Lee Street • The land north of Lee Street is occupied by the San Remo Ballroom and associated activities including a music store. • The land north and south of Fenwick Street is occupied by a mix of activities including restaurants, laundromat and service businesses. • There are no local weekly shopping retail businesses (apart from the 24hr convenience shop to the south in the NRZ1 NW cnr Curtain Street)
8	<p>Residential areas that due to the surrounding uses, built form and road network are logically included</p> <p><i>Relevant PPN58 criteria: Consider</i></p>	<ul style="list-style-type: none"> • NRZ1 land extends south of the C1Z land at Fenwick Street to Curtain Street and includes a 24hr convenience shop at the NW corner of Curtain Street and a few shops or former shops mixed in with dwellings • The 24hr convenience shop at the NW corner of Curtain Street is the only significant weekly convenience shopping in the centre and serves the high volume traffic in Nicholson Street. It provides off street parking.

	<ul style="list-style-type: none">• <i>Location of existing government and institutional areas and land uses</i>• <i>Location of existing areas of public open space</i> <p><i>Include:</i></p> <ul style="list-style-type: none">• <i>Residential areas that are integrated into the activity centre or surrounded by other uses that have a strong relationship with the activity centre, even where limited development opportunities exist</i>	
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9.4 Spensley Street – Clifton Hill

See Map 6

Category	Descriptions	Reasoning
1	<p>Commercial 1, Commercial 2, Mixed Use, Comprehensive Development Zones and civic uses fronting the main street of the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Location of existing government and institutional areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> • <i>Key public land uses that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • C1Z land fronting Spensley Street and Berry Street around the junction generally coincides with the extent of this local centre • The C1Z includes the large three storey Royal Hotel at the SW corner. • The activities include the hotel, cafes, fish and chip shop and a medical centre. • Local weekly convenience shopping is minimal.

9.5 Berry Street / Ramsden Street – Clifton Hill

See Map 6

This centre is located at the junction of Berry Street and Ramsden Street. It includes shops, apartments and commercial properties in the C1Z around this junction.

Category	Descriptions	Reasoning
1	<p>Commercial 1, Commercial 2, Mixed Use, Comprehensive Development Zones and civic uses fronting the main street of the retail core.</p> <p><i>Relevant PPN58 criteria: consider</i></p> <ul style="list-style-type: none"> • <i>Location of existing commercial areas and land uses</i> • <i>Location of existing government and institutional areas and land uses</i> • <i>Commercial and residential needs</i> • <i>Proximity to fixed transport esp fixed rail (train or tram)</i> • <i>Location of transport infrastructure</i> • <i>Walkability</i> • <i>Impacts of boundary on other activity centres</i> • <i>Strategic Redevelopment Sites</i> <p><i>Include:</i></p> <ul style="list-style-type: none"> • <i>Sufficient land to provide for commercial activities needed over a 15 – 20 years timeframe + then 30 year horizon.</i> • <i>Key public land uses that have or are intended to have a strong functional inter-relationship with the activity centre</i> 	<ul style="list-style-type: none"> • C1Z land fronting the south side of Ramsden Street at the intersection with Berry Street generally coincides with the extent of this local centre • The C1Z includes five or six shops on the south side of Ramsden Street. • The activities include a cafe and service businesses. • There is no local weekly convenience shopping. • One site at the east end of the centre has been redeveloped for two level apartments and some of the shops may be used as dwellings.

NOISE AND VIBRATION CONSIDERATIONS DISCUSSION REPORT

City of Yarra

Prepared for:

City of Yarra
PO BOX 168
Richmond VIC 3121

SLR Ref: 640.10090.99990-R01
Version No: -v0.6
[8269117: 32230927_1]March 2022



PREPARED BY

SLR Consulting Australia Pty Ltd
ABN 29 001 584 612
Level 11, 176 Wellington Parade
East Melbourne VIC 3002 Australia

T: +61 3 9249 9400
E: melbourne@slrconsulting.com www.slrconsulting.com

BASIS OF REPORT

This report has been prepared by SLR Consulting Australia Pty Ltd (SLR) with all reasonable skill, care and diligence, and taking account of the timescale and resources allocated to it by agreement with City of Yarra (the Client). Information reported herein is based on the interpretation of data collected, which has been accepted in good faith as being accurate and valid.

This report is for the exclusive use of the Client. No warranties or guarantees are expressed or should be inferred by any third parties. This report may not be relied upon by other parties without written consent from SLR.

SLR disclaims any responsibility to the Client and others in respect of any matters outside the agreed scope of the work.

DOCUMENT CONTROL

Reference	Date	Prepared	Checked	Authorised
640.10090.99990 Planning Review -R01-v0.6	24 March 2022	Dianne Williams	Jim Antonopoulos	Jim Antonopoulos

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1 Introduction

This report provides a technical discussion and summary of previously provided advice to City of Yarra on planning related noise and vibration issues and forms the basis of the *Guidelines – managing noise impacts in urban development, Guidance for planning permit and related decisions under the Yarra Planning Scheme 2021*.

1 Road Traffic Noise

1.1 Background Information

Road traffic is a significant and major source of noise impact to dwellings on main roads. The issue has been acknowledged and addressed in recently introduced Apartment Developments provisions at Clause 58 and specifically at 58.04-3 Noise impacts objectives and Standard D16. Planning Practice Note PPN83 *Assessing external noise impacts for apartments* provides further guidance to these Clauses. The provisions nominate decibel targets for day and night average road traffic noise levels, and applies to apartment developments on roads carrying more than 40,000 vehicles, or within 300 m from a freeway. The design targets are 40 dBA Leq,16h for all habitable rooms and 35 dBA Leq,8h for bedrooms.

It is of note that the time classifications used in *Better Apartments* document place the 6 am to 7 am period in the 'day' rather than the 'night' category. This classification is not consistent with those we have been applying to City of Yarra projects, and is not consistent with the classifications usually used in Victoria (e.g. the Vic EPA 'Noise limit and assessment protocol for the control of noise from commercial, industrial and trade premises and entertainment venues' (Publication 1826) and the Vic EPA 'Noise control guidelines' (Publication 1254)).

In our opinion the *Better Apartments* document should be modified as follows:

- the 6 am to 7 am period should be included in the 'night' period rather than the 'day' period;
 - application of the targets should be extended to all developments affected by road traffic noise (not just those near freeways or on roads carrying more than 40,000 vehicles), and
 - the targets should be applied to all residential developments, not just apartments.

1.2 Mandatory Requirements

The Victorian Planning Provisions were amended in March 2017 with Clause 58, Apartment Developments, objectives and Standard D16 which adopted the *Better Apartments Design Standards*. This introduced the design targets of 40 dBA Leq,16h for living rooms and 35 dBA Leq,8h for bedrooms in developments within 300 m of a freeway on roads carrying more than 40,000 vehicles.

1.3 Other Potentially Useful Standards and Guidelines

AS/NZS2107:2016 Acoustics – Recommended design sound levels and reverberation times for building interiors, provides recommended noise level ranges for dwellings near major and minor roads. This Standard has traditionally been called up in planning permits to address road traffic noise impacts. However the provision in the Standard of a decibel range instead of a specific design target has led to uncertainty with regard to actual design targets (most consultants design to the upper end of the range). Furthermore the assessment methodology is not defined (it is unclear whether traffic noise should be quantified as an average or worst case level, e.g. the loudest hour of traffic noise). In our reviews of acoustic reports for the City of Yarra, and in our own planning noise assessments, we have interpreted the AS/NZS2107 design levels to be as follows: the day and night average noise levels are assessed to the lower end of the AS/NZS2107 range (35 dBA bedrooms and 40 dBA living rooms), and the loudest hour of road traffic noise during the day and night periods are assessed to the upper end of the AS/NZS2107 range (40 dBA bedrooms and 45 dBA living rooms).

The NSW *Road Noise Policy, 2011* provides internal targets for road traffic noise of 35 dBA in bedrooms at night and 40 dBA in all habitable rooms during the day period (NSW Road Noise Policy, 2011, C10). These targets are elaborated in the NSW guideline document *Development Near Rail Corridors and Busy Roads - Interim Guideline, 2008*. The NSW targets are generally consistent with the *Better Apartments Design Standards* (with the exception that the 6 am to 7 am period is classified as 'night' in the NSW Road Noise Policy – as per our preference and consistent with the time intervals used in the assessment of commercial noise).

Development Near Rail Corridors and Busy Roads - Interim Guideline also provides clear and practical guidance for measuring and reporting. This information is missing from the Victorian policy and guidelines documents for road and rail noise.

1.4 SLR Recommendations

Our office recommends designing to the following levels for road traffic noise:

- 40 dBA Leq,16h (6 am to 10 pm) to all habitable rooms and 35 dBA Leq,8h (10 pm 6 am) in bedrooms, and
- Loudest hour of road traffic noise is not to exceed 45 dBA Leq,1h in habitable rooms from 7 am to 10 pm, and 40 dBA Leq,1h in bedrooms from 10 pm to 7 am the following morning. The basis for the loudest hour targets is AS/NZS2107:2016, with the day and night periods defined in accordance with Victorian EPA legislation and guidelines rather than in accordance with the Better Apartment Design Standards.
- These recommended maximum design noise levels should apply to all residential development where there is a reasonable expectation that traffic noise may impact the land (i.e. not just those formally triggered by Clause 58 of the planning scheme).

Reporting should generally be in accordance with NSW guideline document *Development Near Rail Corridors and Busy Roads - Interim Guideline* Appendix D – Acoustic Consultant Reports, Methodology for Testing and Compliance Reporting.

2 Rail Noise

2.1 Background Information

The *Victorian Government Passenger Rail Infrastructure Noise Policy, 2013* provides screening levels for rail noise. The Policy is a high level document and it is designed for transport bodies and planning authorities. Where existing rail noise exceeds the threshold levels specific consideration of rail noise is required before the project proceeds. However if the threshold levels are not exceeded, rail noise impacts may still need to be considered as a 'secondary matter'. In the context of a proposed residential development, we understand this to mean that the issue of rail noise should be dealt with by the individual developer and local government.

Until recently there was no government guidance about how to assess rail noise impacts on apartment developments. The Clause 58, Apartment Developments, Standard D16 (and associated Planning Practice Note PPN83) address both rail and road traffic noise. Standard D16 requires that all apartment developments within 80 m of a passenger line or 135 m of a freight rail line, are designed to internal targets. The design targets to be met are the same as those for road traffic noise: 40 dBA Leq,16h and 35 dBA Leq,8h.

Prior to the release of the Apartment Developments, Standard D16, a common approach has been to assess rail noise to L_{max} targets of 60 dBA in living rooms and either 50 or 55 dBA L_{max} in bedrooms. These design levels broadly align with:

- The minimum acoustic standard (2-3 star) for external noise intrusion provided in the Association of Australasian Acoustical Consultants (AAAC) Guideline for Apartment and Townhouse Acoustic Ratings (2017)
- Aircraft noise design levels provided in Australian Standard AS 2021:2015 Acoustics – Aircraft noise intrusion – Building siting and construction
- General sleep disturbance criteria provided in the NSW Road Traffic Policy and sleep disturbance studies.

The levels of 60 dBA (living rooms) and 55 dBA (bedrooms) have been accepted at VCAT on some projects, and are cited by acoustical consultants on those grounds.

The use of long term day and night average targets for rail noise, in the Apartment Developments Standard D16 provides a clear standard. There is, however, benefit in also incorporating L_{max} criteria for rail noise for the following reasons:

- Leq is primarily a noise descriptor used to quantify steady or quasi-steady state noise. So this is appropriate for sources such as mechanical plant noise, and reasonably applied to traffic noise which has a fairly regular and consistent noise level. Train noise is not as constant / regular as traffic noise. While there are no trains passing by, the occupant experiences little or no noise, yet while the train passes, there is a short term high noise event which can only be quantified via an L_{max} descriptor; to clarify, the L_{max} is the actual highest level that someone experiences as the train goes past. In contrast, the long term Leq's are not easily related to the actual objective experience of an occupant when the train passes by.
- L_{max} levels are often used to address sleep disturbance.

- On suburban rail corridors where there may only be one line in each direction, relatively infrequent trains and no trains during some of the night period, the L_{max} criteria become more important and are likely to drive the assessment. If these targets are not in place it is possible for the L_{eq} targets to be met, and rail noise to exceed sleep disturbance L_{max} targets by appreciable amounts.
- Very short term noise events, such as train horns, are not well quantified using long term L_{eq} criteria.

It may be of consideration that in the City of Yarra, where all rail lines carry significant numbers of trains, it is less likely that the assessment will be driven by the L_{max} criteria.

2.2 Mandatory Requirements

Clause 58, Apartment Developments, objectives and Standard D16 has been incorporated into the planning scheme.

2.3 Other Potentially Useful Standards and Guidelines

The NSW guideline document *Development Near Rail Corridors and Busy Roads - Interim Guideline, 2008* provides average day and night targets for road and rail noise that are similar to the levels included in Apartment Developments, Standard D16.

2.4 SLR Recommendations

Our office recommends designing to the following levels for road traffic noise:

- Clause 58, Apartment Developments, Standard D16 - 40 dBA L_{eq} ,16h (6 am to 10 pm) in habitable rooms and 35 dBA L_{eq} ,8h (10 pm to 6 am) in bedrooms, and
- Train and Tram generated L_{max} levels, including horn noise, should not exceed 60 dBA L_{max} in living rooms or 55 dBA L_{max} in bedrooms. L_{max} levels to be achieved for 95% of train pass-by events (a minimum of 20 trains to be measured).

Reporting should generally be in accordance with NSW guideline document *Development Near Rail Corridors and Busy Roads - Interim Guideline* Appendix D – Acoustic consultant Reports, Methodology for Testing and Compliance Reporting.

3 Rail Vibration – Trains and Trams

3.1 Background Information

In Victoria, there are no guidelines, standards or policies that address transportation vibration impacts.

The only time that vibration assessments are typically undertaken is when a local council perceives there may be a vibration issue and calls up a vibration assessment in a permit condition, or larger scale projects where a Planning Panel or VCAT may require consideration of the issue.

The lack of guidelines and policy leads to enormous variability and inconsistency in addressing vibration in Victoria.

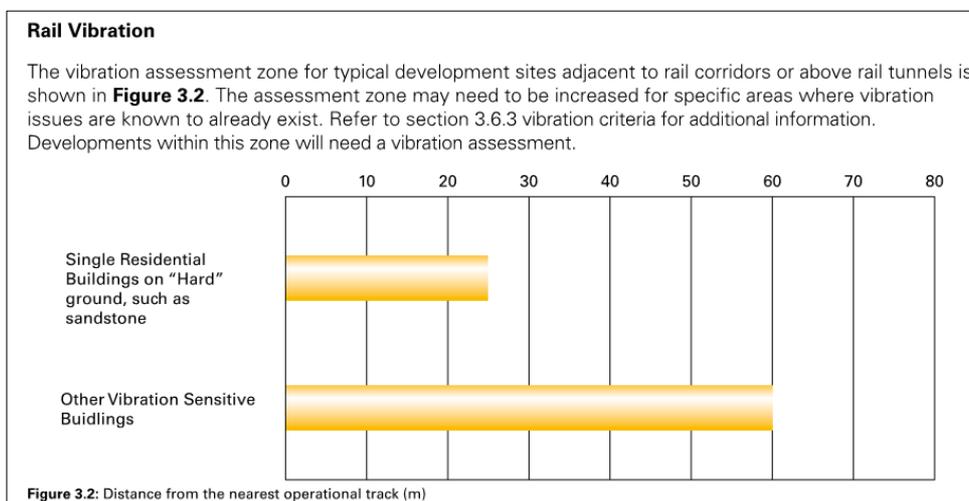
3.2 Other Potentially Useful Standards and Guidelines

NSW has significantly more noise and vibration related planning guidance documents than Victoria does.

Their main planning guideline document relating to new developments is the *Development Near Rail Corridors and Busy Roads – Interim Guideline* (Department of Planning, State Government of NSW, 2008) and provides some guidance on vibration assessment methodology, including a basic preliminary screening process.

Section 3.5.1 of the NSW Interim Guideline provides a basic buffer distance within which a rail vibration assessment should be undertaken, and is reproduced below:

Figure 1 NSW Interim Guideline Rail Vibration Assessment Zones



So from the above any multi-level development within 60 m of a railway line will require a vibration assessment. This is a particularly large assessment zone. It should be noted that this relates to railway lines, not trams (which we discuss further below).

The NSW Interim Guideline does not provide the technical requirements of the assessment; it instead refers to another NSW Technical Guideline; *Assessing Vibration: a technical guideline* (DECC 2006).

The Technical Guideline includes all necessary details of the testing / or prediction of vibration and also provides the assessment targets. The assessment methodology and targets are based on British Standard BS6472 which uses the 'Vibration Dose Value' measurement (VDV) for intermittent vibration assessment.

The VDV is a long term averaged 'dose' based parameter (a little like a long term Leq), and is a relatively new measurement parameter. The equipment used to measure VDV is more advanced than traditional vibration measuring equipment, however, is readily available and most of the larger acoustical consulting firms have the necessary equipment.

The VDV is assessed for the day (16 h) and night (9 hour) with different criteria applicable for each period and for different uses. The following excerpt from the NSW Technical Guideline shows the criteria:

Table 2.4 Acceptable vibration dose values for intermittent vibration (m/s^{1.75})

Location	Daytime ¹		Night-time ¹	
	Preferred value	Maximum value	Preferred value	Maximum value
Critical areas ²	0.10	0.20	0.10	0.20
Residences	0.20	0.40	0.13	0.26
Offices, schools, educational institutions and places of worship	0.40	0.80	0.40	0.80
Workshops	0.80	1.60	0.80	1.60

1 Daytime is 7.00 am to 10.00 pm and night-time is 10.00 pm to 7.00 am.
2 Examples include hospital operating theatres and precision laboratories where sensitive operations are occurring. These criteria are only indicative, and there may be a need to assess intermittent values against the continuous or impulsive criteria for critical areas.
Source: BS 6472-1992

It is normally necessary to monitor for at least a 24 hour period to obtain the VDV value (although it is possible to estimate using shorter measurements). There are also some further complicated calculations necessary when the vibration transmits to the upper floors of a building.

One of the most significant issues relating to the assessment is what to do if the criteria are exceeded. There are really only two options – do not build the building in that location, or design in complicated building vibration isolation into the footings. Both are of major concern to any application / developer.

Trams

There is a large gap in knowledge and information on tram vibration impacts, primarily because it has not been considered historically in any assessments.

We have minimal reference data upon which to draw indicative buffer distance triggers, however, from our experience in CoY, it is clear that trams operate in very close proximity to existing and proposed residential / office buildings. In addition, there are many additional variables such as track condition, joint locations in the track, and the speed of pass-bys, that would affect the vibration level in the building.

3.3 SLR Summary and Recommendations

Rail and tram vibration presents one of the most difficult challenges in relation to planning assessments.

There are no Victorian policy or guideline documents, and no precedent for assessing vibration (with many historical and new developments constructed in close proximity to rail with no formal assessment undertaken).

The further complicating issue is that if vibration impacts are found to exceed the British Standard BS 6472 criteria at a particular building, it has drastic implications; either further setback is required, or the building is required to be designed with vibration isolation within the footings – potentially making the development financially not-viable.

Overall, due to the lack of formal guidance in any Victorian policy or guideline and the large extent of variables that can affect a vibration assessment, it is not considered appropriate for these issues to be addressed in the Yarra Planning Scheme at this time.

4 Commercial and Industrial Plant and Equipment Noise

Commercial and industrial noise assessable to the Victorian EPA Noise limit and assessment protocol for the control of noise from commercial, industrial and trade premises and entertainment venues, Part I: Commercial, industrial and trade premises (Noise Protocol, Part I) is considered in the following section.

4.1 Background Information

This is a critical 'tier one' planning issue from our perspective. Noise from existing commercial and industrial premises to residential developments needs to be addressed to ensure that:

- The compliance status of the business with the Noise Protocol is not changed by the residential development. If the business is forced into non-compliance by introduction of new noise receptors, it may be required to undertake significant and costly noise control works.
- The amenity of future occupants is addressed.

The main issue with commercial and industrial noise pertains to whether or not the noise should be assessed at the façade of the proposed residential development, or within the residence with windows and doors closed. The latter approach is not strictly in accordance with the Noise Protocol, which requires noise to be assessed externally unless noise enters the dwelling via a non-openable section of the façade (solid wall, fixed window etc.).

Whereas the Noise Protocol requires commercial noise to be assessed externally, the City of Yarra (and other Councils) has often allowed for an internal assessment location. This is seen as a reasonable amenity compromise in an inner city environment, where insisting on an external assessment location would effectively make many sites impossible to develop.

Our approach has been largely consistent with the City of Yarra's, however we have encouraged the following exceptions / modifications:

- Where commercial noise is to be assessed internally, we recommend that the targets are equal to the lower of:
 - The effective Noise Protocol internal noise limit, taking into consideration any relevant corrections for noise character (corrections for character are required under the Noise Protocol), and
 - The lower end of the original AS/NZS2107:2000 ranges, which was 35 dBA in living rooms and 30 dBA in bedrooms. However, in the 2016 release of AS/NZS2107, the lower end of the allowable ranges for apartments near major roads has increased to 35 dBA in all rooms

4.2 Mandatory Requirements

Compliance with the Noise Protocol is required, however strictly speaking the onus of compliance is on the business, rather than the developer.

Clause 58, Apartment Developments, Standard D16 (and associated Planning Practice Note PPN83) also applies to commercial noise however the targets provided in this document are generally likely to be less stringent than the Noise Protocol indoor limits.

4.3 Other Potentially Useful Standards and Guidelines

The Association of Australian Acoustical Consultants (AAAC) *Guideline for Acoustical Star Ratings for Apartments and Townhouses* provides indoor design targets for commercial noise. Assuming 'three star' (i.e. average) apartments the targets would be:

- Bedrooms: 35 dBA Leq and 50 dBA Lmax
- Living rooms: 40 dBA Leq and 55 dBA Lmax

4.4 SLR Recommendations

All residential developments should be designed to ensure that existing commercial uses formally comply with the EPA Noise Protocol, Part I external to the development wherever possible. Where it is demonstrated that external compliance cannot practically be achieved, including treatment of the source of noise in consultation with the commercial operator, an internal assessment may be considered. The following internal targets for commercial / industrial noise (windows closed) are to be met if noise is assessed internally:

- Noise Protocol, Part I indoor limits, being the outdoor limits less 20 dB, and
- Not more than 30 dBA Leq in bedrooms and 35 dBA Leq in living rooms, and
- Not more than 45 dBA Lmax in bedrooms and 50 dBA Lmax in living rooms.

In addition to the above, commercial plant and equipment noise levels should not exceed the following levels externally:

- Not more than 10 dBA above any Noise Protocol, Part I period noise limits, outside any openable windows or doors,
AND
- For balconies and other private open spaces:
 - Not more than 65 dBA during the day
 - Not more than 55 dBA during the evening and night

All assessment methodologies should apply corrections for character in accordance with the Noise Protocol, Part I procedure.

The above is aimed at providing a reduced risk of complaint from new sensitive receivers in the City of Yarra. The above does not represent a formal compliance outcome to the requirements of the Noise Protocol.

5 Music Noise

The following considers the issue of music noise impacts from existing venues to new dwellings only. The issue of music from proposed new venues to existing dwellings is straightforward one, and need not revisited here. Consistent with the Noise Protocol, Part II, all new venues need to comply with external noise limits at existing dwellings.

5.1 Background Information

This is a critical 'tier one' planning issue for acoustics, with existing music venues being at risk of non-compliance with the Noise Protocol, Part II due to residential encroachment. Yarra City Council has, in recent times, adopted an 'Agent of Change' approach to new dwellings in the vicinity of existing music venues. This approach is both supported and complicated by the planning scheme amendment VC183 28/09/2020 Clause 53.06 Live Music and Entertainment Venues (previously VC120 4 September 2014 Clause 52.43). Planning Practice Note PPN81 *Live Music and Entertainment Noise* provides further technical guidance on the planning scheme clause.

The clause provides support for the assessment of music to indoor locations. Prior to the amendment, there was no formal recognition that building upgrades to control music noise would protect existing venues from residential encroachment (i.e. upgrades could be adopted, but the assessment location was still formally outdoors).

The issue of music noise impacts to new residential development is complicated by the following

- Clause 53.06 applies to live music venues only. Many existing music venues are not live music venues, and these are explicitly excluded from consideration under Clause 53.06.
- Clause 53.06 only applies to venues within 50 m from a proposed residential development.

The amendment requires new dwellings at which a music noise excess is established, and which cannot be managed in any other way, to be constructed such that noise limits are met indoors. However the means by which compliance is to be achieved in doors has not, in our opinion, been thought through. A brief outline of the issues is provided below.

5.1.1 Music Noise Compliance Indoors

Compliance with the Noise Protocol, Part II is achieved by either designing for a 'background + margin' target (as defined in the Policy), or the 'base noise limits'. The issues are that the 'background + margin' target can rarely be reached when a dwelling's façade is upgraded – the same upgrades that control music noise ingress also work to reduce the ambient noise and effectively lower the noise targets.

By contrast, the 'base noise limits' are fixed targets. However they are very low, and can in practice be extremely difficult to achieve. Most high performance acoustic glazing, for example, performs poorly in the region of the acoustic spectrum where music noise generally exceeds the base noise limits the most (i.e. in the 63 Hz and 125 Hz octave bands).

The planning practice note provides options for upgrading a noise sensitive dwelling on page 3 of the May 2016 revision, however the options are inadequate in the context of controlling significant levels of bass music noise.

In practice, to achieve compliance indoors when music levels are 10 dB or more above the external limit, the following methods or combinations of them can be required:

- Design the dwelling so that there are no windows or lightweight walls or roof exposed to high levels of music noise. This can represent significant architectural constraints on a site.
- Include wintergardens to all balconies and windows – this is effectively very large cavity double glazing (e.g. 10 mm glass, 1.2 m airgap, 12 mm glass).
- Incorporate controlled noise masking into the design (relying on air conditioning systems to provide masking is not sufficient as the masking noise provided in this way will vary with the weather). The masking should preferably not be controlled or varied by the user.

5.1.2 What Needs to be Done

In our opinion there is still a lot of work to do before Clause 53.06 is workable.

Review of indoor limits

- Are the base noise limits appropriate for all environments? It may be reasonable to apply higher limits in an inner urban environment and particularly in one that is acknowledged as an entertainment district or on a very busy road.

And specifically with regard to noise making:

- What level of noise masking is acceptable? The masking should not cause occupants discomfort (ideally it should not even be noticed).
- What level of music is acceptable above the noise masking? i.e. should the masking be equal in level to the music, or should the masking be treated as the background noise level, and higher levels of music allowed in accordance with the 'background +' targets.
- Could 'user controlled' masking systems be used? Clause 53.06 allows user control of the acoustic environment by permitting noise limits to be met in dwellings with openable windows closed. It could be argued that the same use control could be extended to noise masking

The above questions cannot be simply answered, and should ideally be explored in the context of a wider review.

5.2 Mandatory Requirements

Compliance with the Noise Protocol, Part II is mandatory, however strictly speaking the onus of compliance is on the venue rather than the developer.

Compliance with Clause 53.06 is also mandatory, but only for developments within 50 m of a live music venue.

5.3 Other Potentially Useful Standards and Guidelines

Acoustic rating curves (NR, RC or NC) are provided in acoustic literature for quantifying noise intrusion. The curves define acceptable levels of noise in octave measurement bands, not dissimilar to the Noise Protocol octave band night noise limits.

From recent experience on projects incorporating masking for music noise, use of 'Noise Criteria' or NC curves is considered the most appropriate for domestic use.

5.4 SLR Recommendations

We recommend that formal compliance with the Noise Protocol, Part II noise limits is demonstrated, using any of the methods described in **Section 5.1.1** of this review. Effectively this means achieving:

- Base noise limits within apartment habitable rooms with doors and windows closed, OR
- The Noise Protocol 'background + margin' noise limits with continuous noise masking installed in habitable rooms of all apartments. The masking system should be set to no more than NC20 L₉₀ and the Leq of the masking to no greater than NC20 L₉₀ + 5 dB.
- Where noise masking is used to achieve compliance:
 - The masking system should be designed to enable masking levels in all rooms to be individually controlled.
 - Future occupants are to be informed that compliance with the relevant music noise limits relies on the masking system operating at the pre-determined level.
 - Commissioning testing should be conducted to demonstrate that the masking system meets the above requirements and is deemed acceptable for domestic use by the acoustical consultant.
- Note: Noise masking should not be relied on as the sole measure to address music noise exceedances. If implemented on a project, it should be used in conjunction with other reasonable and practical façade upgrades.
- These indoor targets for music noise should be applied to all existing sources of music, not just live music.
- The requirements should be met at dwellings where any significant music noise impacts are identified, not just due to venues within 50 m of the proposed residential use.

6 Patron Noise – New Outdoor Patron Areas

6.1 Background

The Noise Protocol Parts I and II specifically exclude patron noise, but with the significant increase in outdoor patron area applications, there have been major noise issues associated with this source.

While there are still no mandatory requirements for patron noise, there is general acknowledgement that this source of noise needs to be considered in the context of proposed dwellings near existing outdoor patron areas, and in the context of proposed new outdoor patron areas near existing dwellings. New outdoor patron areas are considered in this section.

6.1.1 Noise Targets

As part of any planning application for an outdoor patron area we expect an assessment of patron noise to 'Leq' and Lmax targets. The Leq descriptor quantifies the average level of patron noise over an interval, and is particularly important for outdoor areas that are proposed to accommodate appreciable numbers of people (say more than 10). The Lmax targets are useful for quantifying impacts from smaller outdoor areas, where the steady state noise emissions may be low, or variable, and the most intrusive impacts are due to isolated loud voices.

The following noise criteria are typically used in assessments:

- The Noise Protocol, Part I – while the Noise Protocol does not strictly apply to patron noise, it can nevertheless provide a useful assessment methodology and we find it valuable for quantifying patron noise impacts.
- Background + 5 dB – this is a standard basis for quantifying the intrusiveness of noise. It is a useful assessment tool for patron noise although we have found that the day and evening limits can be impractically low.
- Sleep disturbance targets of 55 dBA L_{max} in bedrooms with windows open (usually assessed as 65 dBA L_{max} externally, outside openable windows).
- Marshall Day in-house targets for patron noise. MDA have developed patron noise targets based on background noise levels plus a variable margin, being:
 - Background + 10 dB during the day and evening period (including weekends)
 - Background + 5 dB at night (after 10 pm)

The MDA approach is generally supported however the following is noted:

- The evening noise target of 'background + 10 dB' can be too high in some circumstances where there is little other ambient noise (for example for dwellings that back onto an outdoor patron area, but are not exposed to general street noise).
- MDA are careful to emphasise that these levels are not 'noise limits', and that modelling or predictions that show targets will be exceeded represent a risk of nuisance rather than grounds for stopping a project from going ahead.
- The MDA approach appears to allow for long term averaging of background levels. This can lead to a misrepresentation of the impact during, for example, the last hour of operation of a venue when background levels in an area are at their minimum.

6.1.2 Predicting Patron Noise Levels

Unless the application is for the expansion of an existing outdoor patron area, patron noise levels need to be predicted to the nearest receivers. The prediction involves two steps:

- Quantification of the amount of noise produced in the outdoor area. This should take into consideration both the level of noise in the outdoor area, the size of the outdoor area and the number of patrons. Ideally the overall noise level should be expressed as a sound power level.
- Prediction of acoustic attenuation or losses, between the outdoor patron area and the receiver location due to distance, shielding and the like. For complicated built environments it can be appropriate to use a 3D computer noise modelling program to predict noise to receiver locations.

There is enormous variability in how acoustical consultants predict patron noise and we have been particularly disturbed by the recent use of patron sound power data derived from restaurants and non-drinking venues to beer garden environments. Most patron noise assessment we review are delayed during the review process due to differences in opinion with regard to the amount of noise produced in outdoor patron areas.

6.2 Mandatory Requirements

There are no mandatory requirements for patron noise.

6.3 Other Potentially Useful Standards and Guidelines

None.

6.4 SLR Recommendations

Regarding noise limits, or targets for patron noise, we support the following:

- Noise Protocol, Part I
- OR
- Background based assessment of:
 - 'night' targets (background + 5 dB)
 - 'evening' and 'day' targets (background + 10 dB) where they can be demonstrated to be reasonable, and where they align with the Noise Protocol definition of evening (that is including all day Sunday).
 - Background levels to be based on the minimum 15 minute to 1 hour interval and conducted during a time that is representative of potential worst case noise impacts (long term averaging of background levels is not appropriate).

AND

- Sleep disturbance targets of 55 dBA L_{max} in bedrooms with windows open (65 dBA L_{max} externally, outside openable windows).

7 Patron Noise – New Residential Development Near Existing Outdoor Patron Areas

7.1 Background

The issue of existing noise from outdoor patrons areas to new developments should be assessed in any planning application to protect future residents from noise. Due to the fact that there are no mandatory requirements for patron noise, we are generally comfortable with the developer designing to meet appropriate patron noise targets indoors with windows closed. Some consideration should also be given to patron noise to balconies.

7.1.1 Noise Targets

Our approach to date has been to require patron noise to be designed to meet the ‘satisfactory’ levels provided in AS/NZS2107:2000. These were 35 dBA in living rooms and 30 dBA in bedrooms near major roads. The recently reissued version of the Standard proposes higher minimum noise levels in bedrooms (35 dBA). In our opinion these are not appropriate for voice noise and we recommend adoption of the 30 dBA target in bedrooms which also aligns with the WHO recommendations for sleep disturbance during the night (WHO 1996). Patron noise is a very distinctive, potentially annoying and variable noise source. Patron noise levels equal to 35 dBA Leq will include frequent levels of over 40 dBA Lmax, which we believe are unacceptable in sleeping areas.

With regard to acceptable patron noise levels to balconies, this is a complicated issue given that:

- By adopting indoor targets we are effectively accepting high levels of noise externally, and in many cases these will occur on balconies
- It can be difficult to avoid having balconies overlooking the noise source (many apartment developments only have one external façade).
- Unavoidably high levels of noise on balconies can occur in the context of road traffic noise.
- Patron noise levels on balconies above 60-65 dBA Leq would, in our opinion make the outdoor space unusable for most residents
- Options for controlling noise to balconies are limited to:
 - Wintergardens (high level of control but effectively an enclosed space), OR
 - Solid balcony balustrades in combination with sound absorption to the underside of the balcony ceiling (small reduction in noise level to seated position on balcony).

7.1.2 Predicting Patron Noise Levels

Patron noise from existing venues should ideally be measured at a location representative of the most exposed new dwelling. Where this is not practical, for example where a proposed multi-level residential development will overlook an existing outdoor area, it may be necessary to predict patron noise to the new façade. The predictions should, however, still take into consideration the actual patron noise levels at the venue during worst case operating conditions. This may involve monitoring noise over a busy weekend period, with a logger located above the outdoor patron area. In our opinion it is not appropriate to use theoretical patron noise data to predict patron levels from an existing outdoor area

7.2 Mandatory Requirements

None.

7.3 Other Potentially Useful Standards and Guidelines

The *Better Apartments Design Standards, 2016* provides indoor targets of 40 LAeq,16h and 35 LAeq,8h. As indicated above, we believe these are too high for patron noise. The long term averaging component (16h and 8h) is also not relevant. To provide for a reasonable level of amenity we would expect patron noise to meet the nominated targets at all times, not just over a long averaged period.

The Noise Protocol effective indoor limits (external noise limit less 20 dB) can be used as indoor targets for patron noise however there is a risk that the resultant limits will be unreasonably high in some instances.

7.4 SLR Recommendations

We recommend that new residential developments exposed to noise from outdoor patron areas be designed to achieve the following internal targets:

- 35 LAeq,15mins in habitable rooms
- 30 LAeq,15 mins in bedrooms at night
- 45 dBA, Lmax in bedrooms at night
- 65 LAeq,15 mins to balconies, 1.2 m high

Noise from existing outdoor patron areas should be measured in order to quantify the worst case impacts to the subject site. Where measurements cannot be undertaken at a location representative of the proposed new receptors, they should be made closer to the venue. The measured levels should be adjusted for the size of the outdoor patron area, and for the distance to the development façade.

8 Noise from Apartment Developments to Existing Dwellings

8.1 Noise Protocol, Part I Assessable Noise

Communal mechanical plant, car stackers, carpark entrance gates and the like are required to comply with the Noise Protocol, Part I at existing and proposed dwellings.

In our opinion as much advice as practical should be provided by the consultant at the planning stage, particularly with regard to items that have structural implications for the project, such as carpark entrance doors and car stackers. Effective control of noise and vibration can for example, require full enclosure of the carpark and/or setdowns to accommodate vibration isolation mounts.

Noise from mechanical plant cannot usually be fully addressed during the planning stage because equipment is rarely specified at this time, and equipment location may not be finalised. On larger projects, where an acoustical consultant is retained during the detailed design, it is reasonable for the consultant to state that these issues will be addressed during the detailed design.

On smaller projects, where it is unlikely that a consultant will be retained after the planning phase, we recommend that more guidance be provided for achieving compliance. This could entail providing a maximum overall sound power level for any mechanical plant proposed to be installed on a roof top plant deck, and / or maximum ratings for air conditioning condenser units.

8.2 Sleep Disturbance

Noise from operation of carpark equipment should also be designed to comply with sleep disturbance targets outside openable windows of nearby dwellings. Noise levels should not be in excess of 65 dBA L_{max}.

8.3 Apartment Common Areas

There are no mandatory limits for voice noise from apartment common areas such as communal decks, gardens, pools and spas. In our opinion these should be assessed similarly to any other patron noise (see **Section 6** of this document).

9 Noise from Apartment Common Areas to Apartments within the Development

Noise to apartments from common areas within and outside the development buildings has the potential to cause nuisance. We consider this a 'second tier' planning issue because, theoretically, any issues due to noise within the development can be addressed in the future by the Body Corporate. However, it is preferable to address these items during the planning stage particularly as they can be costly and difficult to rectify post construction.

The following is a summary of the relevant potential impacts. City of Yarra should consider if their planning documents should require assessment of these '2nd tier' issues.

9.1 Communal outdoor areas, including decks, outdoor pools, gardens, carpark entrance etc.

Facade upgrades should be provided to apartments within the development that are potentially impacted by noise from voice and vehicles in communal outdoor areas. From our perspective moderate glazing upgrades, as opposed to a full patron noise assessment, are appropriate in the context of outdoor communal areas. If, after reasonable upgrades, occupants are still annoyed by voice noise, the Body Corporate should manage impacts through restricted access to the communal facilities.

Appropriate moderate upgrades may include, for example, calling up double glazing comprising 10.38 mm thick laminated glass, 12 mm airgap, 6 mm glass to the most affected windows.

9.2 Communal enclosed areas, including cinemas, gyms, indoor pools etc.

The main issue with regard to gyms and pools is vibration and structure-borne noise due to running machines, free weights, and weight machines.

The degree of vibration control appropriate for a development will depend on the size of the gym, the proximity of the closest apartments, and the equipment proposed for use. For small gyms, it may be sufficient to install a 50 mm thick dense rubber matt throughout the gym and to restrict the use of free weights and running machines. For more elaborate gyms a full acoustic floating floor may be required.

9.3 Noise transfer between apartments via lightwells

Noise transfer between apartments that share a lightwell should be addressed in the acoustic report as this issue is not covered under the National Construction Code (NCC).

Noise transfer can be a particular problem in instances where the lightwell is enclosed on all sides, as the ambient noise within the lightwell is low, and sound attenuation within the lightwell is minimal. Sound emanating from one apartment (which may have their windows open) will reflect off the walls and windows of the lightwell, potentially causing nuisance to other occupants.

There are two basic scenarios:

- a. Lightwells that contain non-openable windows to habitable rooms, and openable windows to bathrooms / toilets only
- b. Lightwells that have openable window from habitable room.

Scenario A is not a major concern, because, provided reasonable well sealed glazing is fitted to all windows of habitable rooms, noise between apartments will typically travel through two widely spaced panes of glass. We recommend that glazing to habitable rooms in this situation be not less than $R_w = 30$ dB (eg. 6 mm thick glass to windows of all habitable rooms onto the lightwell).

Scenario B is the greater concern because the noise generating apartment may have their window open. In that instance, there is only one window separating affected apartments from the noise source. We recommend glazing to habitable rooms be rated not less than $R_w = 38$ dB in this situation (e.g. double glazing comprising 10.38 mm thick laminated glass, 12 mm airgap, 6 mm glass).

The above advice will not be optimum for all situations – very large lightwells or light courts, for example, may be less critical because more sound attenuation will take place between apartments. A lesser upgrade would be reasonable in these areas.

9.4 Carpark entrance gates and car stackers

These items are potential sources of airborne noise, structure-borne noise and vibration.

Noise

Carpark entrance gates and carstackers need to comply with the Noise Protocol, Part I effective indoor limits within apartments and should also be designed to achieve appropriate L_{max} levels indoors for sleep disturbance and general annoyance.

The Noise Protocol assessment should take into consideration typical frequency of use during various times of day and night; the duration of the event, and any relevant corrections for impulse, tonality and intermittency. In our measurements of car stackers we have found that a 5 dB impulsive correction always applies; a 2 dB correction for tonality is often appropriate and intermittency corrections apply to the day and evening periods.

Regarding sleep disturbance, as a minimum, we recommend that the AAAC internal targets for L_{max} levels in three star apartments are met with windows closed (40 dBA L_{max} in living rooms and 35 dBA L_{max} in bedrooms). Lower noise levels should be targeted by the developer if they classify the apartment as moderately high to high quality.

Controls to apartments potentially affected by noise from the carpark typically include glazing upgrades and / or floor ceiling upgrades (particularly for lightweight/non-masonry floor construction).

Structure-borne Sound and Vibration

Car stackers and carpark entrance gates should be vibration isolated to ensure that the noise targets are met in potentially affected apartments.

10 AAAC Guideline for Acoustical Star Ratings for Apartments and Townhouses

SLR have often advised on the use of the AAAC Acoustic Star Rating design targets because these address many sources of noise in apartments that are not always captured or assessable under existing guidelines, standards and policies.

The AAAC recommended indoor targets for internal and external noise, and for discrete events (quantified using the 'Lmax' acoustical descriptor) and steady state noise are provided in the sections below. We have generally advised targeting for not less than 3 stars in City of Yarra reports. If a development is advertised as moderately high to high quality, a higher star rating should be targeted by the developer.

These targets are a useful fall-back for many sources of noise, however we do not recommend using them for:

- Noise from existing outdoor patron areas - the Guideline would result in targets of 35 dBA Leq in bedrooms and 40 dBA Leq in living rooms, which is too high for patron noise.

10.1 External noise intrusion

Examples of external Lmax sources of noise include: individual truck pass-bys, crashing and banging due to deliveries or rubbish collection.

Table 1 External Noise Intrusion Design Levels (AAAC Star Rating Guide), Lmax levels

External noise intrusion	2 star	3 Star	4 Star	5 Star	6 Star
Bedrooms	50	50	45	40	35
Other habitable rooms	55	55	50	45	40

Table 2 External Noise Intrusion Design Levels (AAAC Star Rating Guide), Leq day and night levels

External noise intrusion	2 star	3 Star	4 Star	5 Star	6 Star
Bedrooms	36	35	32	30	27
Other habitable rooms	41	40	35	30	27

10.2 Internal Noise Intrusion

Examples of internal Lmax sources of noise include: mechanical plant and equipment serving the building or commercial tenancies and hydraulic noise.

Table 3 Internal Noise Intrusion Design Targets (AAAC Star Rating Guide), Lmax day and night levels

Internal noise intrusion	2 star	3 Star	4 Star	5 Star	6 Star
Bedrooms	45	40	35	30	27

Internal noise intrusion	2 star	3 Star	4 Star	5 Star	6 Star
Other habitable rooms	55	45	40	35	32

Examples of internal Leq sources of noise include: mechanical plant and equipment serving the building or commercial tenancies and hydraulic noise.

A decibel penalty should be added to the measured noise level where the source is deemed to include annoying characteristics. Penalties are typically equal to +2 dB for just audible characteristics, and +5 dB for clearly audible characteristics

Table 4 Internal Noise Intrusion Design Targets (AAAC Star Rating Guide), Leq day and night levels

Internal noise intrusion	2 star	3 Star	4 Star	5 Star	6 Star
Bedrooms	36	35	32	30	27
Other habitable rooms	41	40	35	30	27

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ASIA PACIFIC OFFICES

BRISBANE

Level 2, 15 Astor Terrace
Spring Hill QLD 4000
Australia
T: +61 7 3858 4800
F: +61 7 3858 4801

CANBERRA

GPO 410
Canberra ACT 2600
Australia
T: +61 2 6287 0800
F: +61 2 9427 8200

DARWIN

Unit 5, 21 Parap Road
Parap NT 0820
Australia
T: +61 8 8998 0100
F: +61 8 9370 0101

GOLD COAST

Level 2, 194 Varsity Parade
Varsity Lakes QLD 4227
Australia
M: +61 438 763 516

MACKAY

21 River Street
Mackay QLD 4740
Australia
T: +61 7 3181 3300

MELBOURNE

Suite 2, 2 Domville Avenue
Hawthorn VIC 3122
Australia
T: +61 3 9249 9400
F: +61 3 9249 9499

NEWCASTLE

10 Kings Road
New Lambton NSW 2305
Australia
T: +61 2 4037 3200
F: +61 2 4037 3201

PERTH

Ground Floor, 503 Murray Street
Perth WA 6000
Australia
T: +61 8 9422 5900
F: +61 8 9422 5901

SYDNEY

2 Lincoln Street
Lane Cove NSW 2066
Australia
T: +61 2 9427 8100
F: +61 2 9427 8200

TOWNSVILLE

Level 1, 514 Sturt Street
Townsville QLD 4810
Australia
T: +61 7 4722 8000
F: +61 7 4722 8001

TOWNSVILLE SOUTH

12 Cannan Street
Townsville South QLD 4810
Australia
T: +61 7 4772 6500

WOLLONGONG

Level 1, The Central Building
UoW Innovation Campus
North Wollongong NSW 2500
Australia
T: +61 404 939 922

AUCKLAND

68 Beach Road
Auckland 1010
New Zealand
T: +64 27 441 7849

NELSON

6/A Cambridge Street
Richmond, Nelson 7020
New Zealand
T: +64 274 898 628

people place heritage

CONTEXT

City of Yarra
Residential Heritage Policy Review

REVISED REPORT
31 October 2019

Prepared for City of Yarra



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Project Team:

Louise Honman

Vicki McLean

Claire Scott, Claire Scott Planning

Revised by:

Louise Honman

Report Register

This report register documents the development and issue of the report entitled *City of Yarra Input to Residential Heritage Policy* undertaken by Context in accordance with our internal quality management system.

Project No.	Issue No.	Notes/description	Issue Date	Issued to
2220	1	Draft Stage 1 report	9 February 2018	Amanda Haycox
2220	2	Draft Stage 2 report	21 February 2018	Amanda Haycox
2220	3	Draft final report	28 February 2018	Amanda Haycox
2491	4	Revised report	31 October 2019	Amanda Haycox

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1.0 INTRODUCTION

This report has been revised in October 2019 following key changes in Victorian Planning Schemes. Further detail is in Section 1.2.

1.1 Scope

The residential areas of the City of Yarra are diverse, ranging from inner urban terraces in Richmond and Collingwood to suburban development in Alphington. They are also experiencing rapid change as the whole city grows and the desirability of inner city living is reflected in the market for denser living. These changes are being expressed in a new densification of inner city neighbourhoods, many of which are in Heritage Overlays.

In response to a major planning scheme review of the Yarra Planning Scheme, the Local Planning Policy Clause 22.02 (Development Guidelines for Sites Subject to the Heritage Overlay) was identified as requiring updating. The brief for this project provided for the development of improved policy content to address the identified shortcomings of that policy. The brief included:

- Analysing a sample of recent developments in the City of Yarra, both ‘good’ and ‘bad’.
- Identifying different residential typologies that may benefit from particular policies.
- Assessing the common heritage policy tests for visual impact assessment and how they apply to different typologies.
- Identifying specific heritage and design considerations for additions and infill development.

1.2 Method

Considerable background material was provided for this project and comprised preliminary work undertaken by planning officers, references to recent VCAT cases, and identified issues with the current policy illustrated through photographic examples.

The project in 2017-18 was undertaken in two stages.

Stage 1 included a policy critique and analysis of Clause 22.02 before the draft rewrite, with the outcome as a letter of advice (included in the February 2018 report as Appendix 1). As this critique is now out of date it has been deleted from this version of the *Yarra Residential Heritage Policy Review*. The exploration of different residential typologies is now Appendix 1.

Stage 2 in the 2018 report included policy intentions and key content including recommendations for an improved residential heritage policy.

This 2019 report retains the same content but has been revised to be consistent with subsequent work by the City of Yarra and DELWP on a new heritage policy, to meet the requirements of Amendment VC148.

Subsequent to the 2018 report Amendment VC148 has amended all Victorian Planning Schemes. Arising from the Victorian Government’s Smart Planning program, Amendment VC148, gazetted on 31 July 2018, introduced significant changes to all Victorian Planning Schemes. Amongst many other changes, it has amended Clause 43.01 (Heritage Overlay) to require the schedule to the overlay to specify a statement of significance for each new heritage place added to the overlay after July 2018. Also as a result of VC148 a new format heritage policy has been prepared in line with the Practitioner’s Guide to Victorian Planning Schemes prepared by the Department of Environment, Land, Water and Planning (DELWP) (Clause 15.03-1L) for all land within a Heritage Overlay.

Project brief

The brief called for building typology to be the basis for the development of policy. Initial work indicated that keeping it simple through the adoption of broad typological groupings was likely to be of most benefit. Consequently, the following typologies were identified.

- Terrace house in a row of similar houses (single and double storey examples).
- Pair/ duplex (single and double storey examples).
- House on corner of laneway/street- compact block (single and double storey examples).
- House on corner of laneway/street – medium or large block (single and double storey examples).
- Freestanding house not on a corner – compact block.
- Freestanding house not on a corner – medium/ large block.
- House with a parapet concealing the roof form (single and double storey).
- House with eaves line and visible roof (gable or hip, single and double storey).

Through examining ‘good’ and ‘bad’ examples and selected VCAT cases, it became evident that common considerations of lot size (compact, medium and large), siting (midblock or corner), scale (one or two storeys) and roof form can be most useful in informing heritage policy. Consequently, the project has concentrated on providing policy content differentiated by lot size, siting, scale and roof form.

This report includes draft text that may form the basis of a new heritage policy for the City of Yarra.. It is understood that heritage is to be addressed as a comprehensive policy rather than as policy with additional heritage guidelines. Guidelines are not proposed to supplement the policy.

An examination of a range of local government heritage policies and guidelines has informed the project. Illustrations throughout the report have been selected from several heritage guidelines and policies, and were not specially prepared for the City of Yarra. The use of illustrations as part of heritage policy has been discontinued as a result of the rewrite, however they have been retained in this report of the purposes of explanation. All guidelines and policies consulted are in the References section.

Limitations

The project brief did not require fieldwork to identify further examples, however additional examples of houses with additions and alterations were reviewed to inform this review.

The following items within the current heritage policy are not addressed in this report. They are not considered to need substantial revision.

- Painting and surface treatments to buildings. Requiring specific colours or colour schemes is not considered necessary in heritage policy.
- Culturally significant trees.
- Carports, car spaces, garages and outbuildings.
- Front fences and gates.
- Ancillaries and services.

1.3 Stage 1 report

Summary

The purpose of the heritage policy is to provide guidance on decisions relating to development on sites covered by the Heritage Overlay. A local policy helps the responsible authority and other users of the planning scheme, such as VCAT members, to decide on a particular matter, and assist applicants and the local community to understand how a proposed development will be considered and what will influence whether or not a planning permit is issued.

The Stage 1 report included the following issues for consideration in a new residential heritage policy:

- Appropriate design responses for different architectural typologies.
- Facadism and integrity i.e. the preference to retain the three-dimensional form of buildings, particularly when visible from the public domain needs retention and strengthening in policy.
- The siting and visibility of roof decks, and their contribution to facadism.
- Issues of increased visibility on corner sites, and appropriate design responses where additional policy for corner sites is recommended.
- Scenarios when the sightline envelope is not appropriate or helpful e.g. small sites or sites of minimal depth, corner sites etc.
- Guidance on what level of visibility, or what type of design response is appropriate (should also be included in the typologies work)
- As above for corner sites.
- Improved guidance on demolition, including what extent of demolition is appropriate.
- Demolition policy linked to whether a place is Significant or Contributory. We would also recommend that policy on Non—contributory places be addressed
- Identification of significant building elements to assist in determining an acceptable extent of demolition.
- Discouraging the use of atypical examples as precedents where the surrounding context and character should be privileged over atypical examples.
- Consideration of the surrounding context and its importance relative to the host site and related to that the impact of development when a dwelling forms part of a ‘group’ e.g. terrace row.
- The surrounding context and character is a first order consideration, leading to responses where scale and form are of elevated importance.
- Roof decks have been identified as a particular issue in development approvals and require specific mention in heritage policy.

How can ‘visibility’ be determined?

The visibility of alterations and additions to heritage places is one of the key issues identified for addressing in the new heritage policy.

Most policies use one of several tests to determine visibility for additions. Guidelines or policies generally use diagrams to illustrate the tests.

- Retention of the primary roof form. This usually includes mention of chimneys as significant elements.

- Retention of the two front rooms.
- Absolute setback distances, often given as 5-8m for small and compact lots and 8-10m for larger lots. Absolute distances are more appropriate for heritage guidelines rather than heritage policy.
- Building envelopes established through sightlines. This is either done from the viewing eye level across the street, or through a given viewing angle in degrees originating from the same point. Viewing angles can be specified as achieving either full, substantive or partial concealment.
- Preferred ceiling heights of two storey additions. This shows how fitting two storeys into approximately 1.5 x height of a building with high ceilings may be achieved.
- Establishing viewing lines from the corners of the site to the corners of the building. This shows an area for rear extensions that is concealed behind the dwelling.

How does building typology affect 'visibility'?

Buildings of different typologies may partially conceal upper storey alterations to different degrees. Typologies can be usefully distinguished by size of lot (large, small, compact) or roof form (hipped, gabled or with parapet).

In examining various building typologies, the following issues are found:

- Common roof forms are gabled and hipped with an eaves line.
- Roof ridges are generally either parallel to or perpendicular to the street.
- Many Victorian era houses have a parapet that conceals the front eaves line. The parapet may be small or large, simple or elaborate, solid or balustraded, and this affects visibility.
- The type of parapet results in different levels of concealment when viewed directly from the front.
- Oblique views of the roof with, or without a parapet are similar and the parapet is not necessarily helpful in concealing additions.
- Street corner sites provide a more three-dimensional view of the house.
- Laneway corner sites also provide a three-dimensional view, but the urban form may be more compact and laneway edge buildings may be present.
- Consideration of only the front façade is not sufficient for either street or laneway corner sites.

2.0 INPUT TO HERITAGE POLICY

2.1 Demolition

What is the issue?

- Demolition or removal of buildings can result in a loss of heritage value to the place and its surrounding context.
- Buildings occupy three-dimensional space and the street facade is not the only important significant element.
- Poor condition is often cited as a reason for demolition.
- There may be different outcomes for demolition depending whether a place is scheduled as individually significant (IS), contributory (C) or non-contributory (NC).

Learnings from VCAT

Demolition and visibility

261-265 Fitzroy Street Fitzroy

The key issue in this case is the extent of demolition permitted. Whilst the Burra Charter's general principal of "*changing as much as necessary but as little as possible*" is a useful one, it does require further interrogation for individual places. For individually significant places, the statement of significance should define the attributes that contribute to its heritage value. That all parties in this case agreed that a rear wall was a significant element should trigger caution about its alteration/partial demolition. Whether the alteration is visible or not is an additional consideration. This is addressed in Section 2.3 Alterations and additions.

Reflecting upon this case and the policy intent, it may be that for demolition or partial demolition, consideration of the significance of an element should carry more weight than visibility.

It is therefore recommended that the application requirements for demolition are accompanied by appropriate material that identifies the significance of the place and the key elements within. An improved checklist for application requirements could support this approach (an example of a check list is provided at Appendix 3).

Policy intention

A heritage policy for demolition should provide the following:

- Retention of heritage places.
- Retention of the elements of heritage places that form part of their significance.
- Minimising the adverse effect on significance of a heritage place by partial demolition.
- Demolition as a result of poor condition is based on solid evidence.
- Appropriate policies are provided for individually significant, contributory and non-contributory places. These terms will need to be defined and all places within precincts scheduled.
- It is recommended that IS and C places have demolition controls but that this is not required for NC places.
- A comprehensive checklist and documentation accompanies a permit application for full or partial demolition. This should indicate significant attributes of the place that contribute to its heritage value.

Individually significant and contributory places

Demolition should be limited to non-significant features.

Demolition of all but the façade is strongly discouraged.

Non- contributory places

Non -contributory places may be demolished provided that a replacement plan for new development accompanies the application.

A cross reference to the policy for new buildings would be useful.

Key content

Partial demolition

This applies to Individually Significant and Contributory places.

The extent to which significant and contributory elements are retained including visible elevations (front and sides), verandahs, chimneys, original door and window openings, at least the front two rooms. The statement of significance now required to accompany all places added to the Heritage Overlay after 31 July 2018 may assist in the interpretation of significant elements.

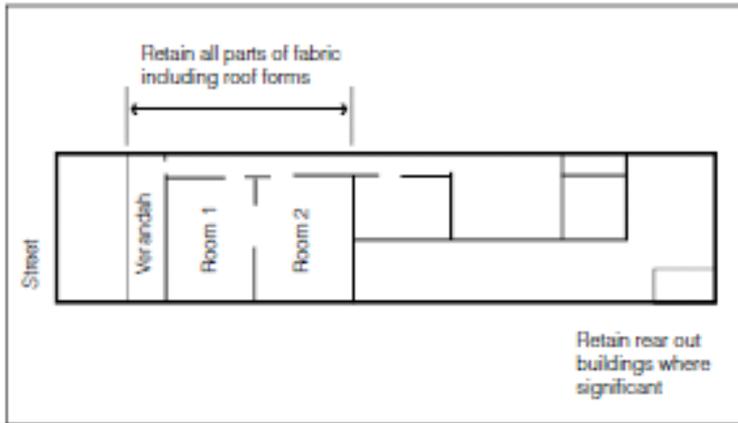


Figure 1 Small and compact houses. Source: Heritage Council Guidelines, 2007 – Demolition.

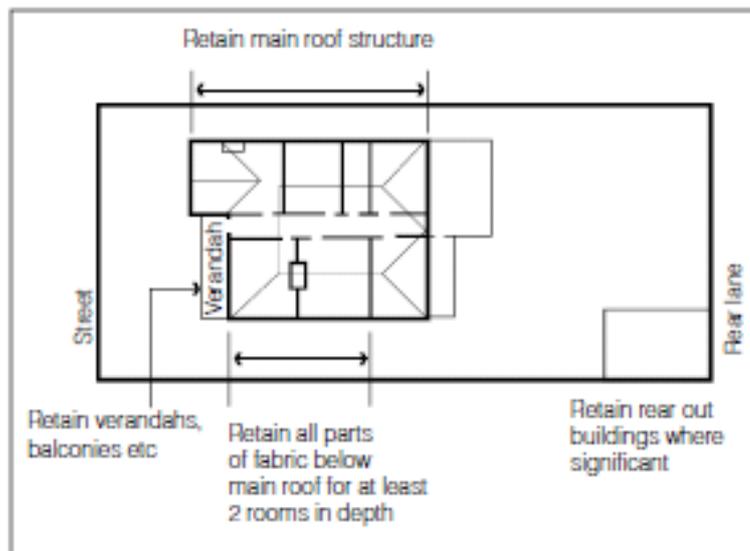


Figure 2 Freestanding larger houses Source: Heritage Council guidelines – Demolition

Full demolition

The policy should require all applications for full demolition to be accompanied by an application for new development and a report from a suitably qualified structural engineer with demonstrated experience in the conservation of heritage buildings. The report should outline the structural condition of the building and the various options available for its management.

2.2 Subdivision**What is the issue?**

Subdivision can impact on the cultural significance of a place and its setting.

Policy intention

- Subdivision of a heritage place should not adversely affect the significance of the heritage place.
- A subdivision layout should maintain an appropriate setting for the heritage place, including the retention of any significant garden areas and elements.

Key content

- Subdivision should retain the significant elements of a heritage place on the one lot. This may include residential buildings, outbuildings, trees or garden features. The statement of significance now required to accompany all places added to the Heritage Overlay after July 2018 may assist in the interpretation of significant elements.
- Ensure that the subdivision of a heritage place will support development that is sympathetic to the scale and setbacks of surrounding heritage places.

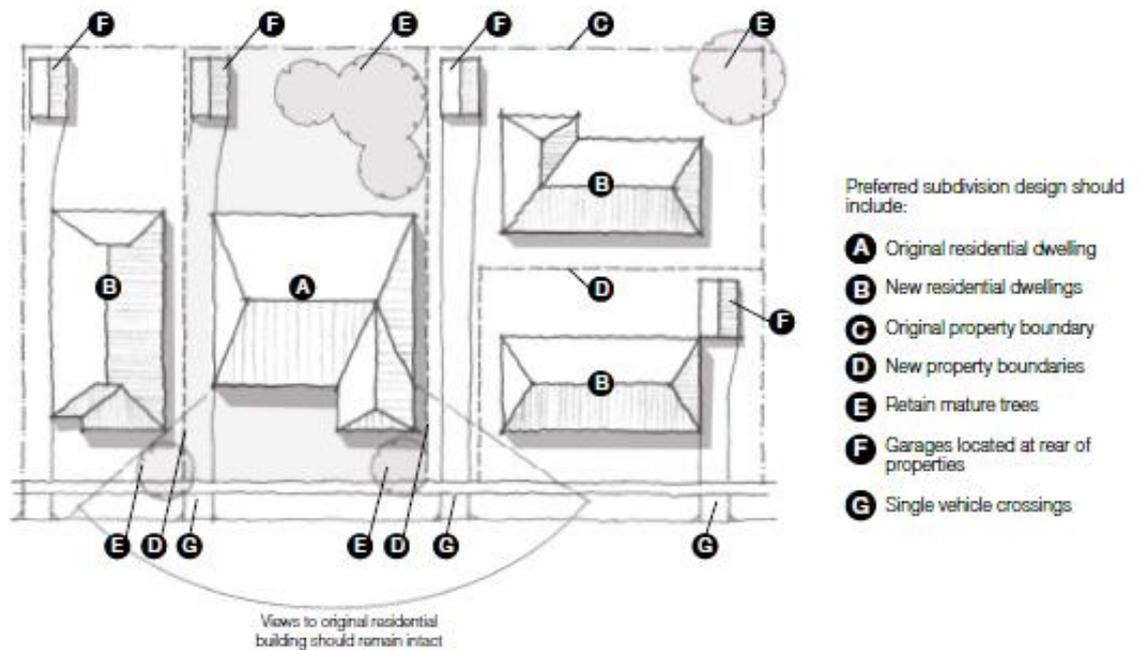


Figure 3 Preferred outcomes for subdivision – larger freestanding houses. Source: Stonnington Design Guidelines

2.3 Alterations and additions

What is the issue?

A heritage place may be impacted by additions and alterations that are unsympathetically designed. The design of alterations and additions is often dominant and highly visible.

Particular issues include:

- Being ‘readily identifiable’ as an addition does not automatically make it sympathetic.
- Policy needs to be made clearer that alterations and additions includes roof decks.
- The visibility of additions varies according to many factors including lot size, location, siting and roof form.
- The demonstration of viewing sightlines. This is not considered an effective measure in controlling the visibility of upper floor additions as it is limited to viewing from the centre front of a house and not when viewed at an oblique angle. The extent of visibility is also dependent on the width of the street with wider streets allowing more visibility through a lower viewing angle.
- There was inadequate consideration of corner sites in the heritage policy.
- There was no policy effectively addressing corner sites.

Learnings from VCAT

Visibility and contemporary design

39-41 Oxford Street Collingwood

Visibility of additions and alterations is not the issue, but the degree to which excessive visibility detracts from the heritage values of the place is. The question to be asked is – which heritage values are impacted and to what degree?

It is not reasonable for a heritage policy to require additions to have no visibility. What is clear is that scale and form is of primary importance in the maintenance of heritage values, and that mitigation of poor outcomes for scale and form is very difficult to achieve through materials or other building details.

The degree to which visibility is acceptable is highly subjective, but visibility tests can be useful as a performance standard. Using ‘design quality’ to justify high visibility additions is unlikely to add clarity or transparency to decision making. This is supported by the Burra Charter Practice Note on New Work. The intention of this Practice Note has not yet been taken up in heritage policies but should be.

The outcome of this VCAT hearing may have been improved by heritage policy that is not so reliant on contemporary design, material palette and being ‘distinguishable’. The key wording is:

*Contemporary design is encouraged **provided that** the cultural significance of the heritage place is also respected.*

261-265 Fitzroy Street Fitzroy

This case addressed the issues of the visibility of alterations to significant elements. The proposed heritage policy provides the following two points regarding significance and visibility. It is intended to require both the consideration of significance and of visibility.

Additions and alterations should be designed not to obscure or alter an element that contributes to the significance of the heritage place.

Additions and alterations should be designed to be either not visible or have limited visibility from key viewing points.

*Corner sites***383 Scotchmer Street North Fitzroy**

The requirements for corner sites need to be specifically addressed in heritage policy and this is best done in a site diagram such as Fig.5. The important issue is one of setbacks that reflect not only the primary frontage but also the secondary frontage. Heritage policy that addressed corner site setbacks may have resulted in a different outcome in this case.

It is appropriate that heritage policy allows for corner sites on streets and on lanes to be considered differently. What is important is consideration of the context and surrounding buildings and their setbacks.

*Roof decks***200 Park Street Fitzroy North**

The roof deck in this case required the removal of part of the front two rooms of the house. Under a 'depth of two rooms' test this would have discouraged this proposal at the assessment stage. Both the use of this test and the inclusion of roof decks specifically as part of additions and alterations, increases the chances that this type of proposal would not gain approval.

246 Canning Street Carlton North

A similar scenario is enacted in the hearing for 246 Canning Street that also involved a roof deck in a prominent location. Canning Street is a cohesive Victorian streetscape and this case may also have benefited from heritage policy that required defining the extent of the heritage place, and its cultural significance. For this site it might reasonably have included the characteristics of the street that are shared by many houses.

Policy intention

The policy for alterations and additions should apply to the following circumstances:

- Ground level additions and alterations.
- Upper level additions and alterations including roof decks.
- Reconstruction.

The intent of the policy should include reference to the following:

- Alterations and additions should provide an appropriate design response to the setting, immediate context and host building.
- A respectful design response is **primarily** demonstrated through appropriate siting, scale and massing in relation to the host building and its surrounding context.
- Additions to the side of a house may be considered providing their scale, design, and distance from the street frontage do not significantly compromise views of the building and site from the public domain or from an adjacent heritage place.
- Where additions are taller than the host building, additions and alterations (including roof decks) are required to be set back from the front and side façades.
- The height of upper level additions and alterations should be proportional to the host building and its context and be either wholly or substantially concealed.
- Reference to atypical buildings as indicators of scale and form is not an appropriate design response.
- Additions and alterations should be designed not to change the original or early elements of a principal facade(s) or primary roof form and to retain building fabric to the depth of at least the front two rooms.

- Additions and alterations should be designed not to obscure or alter an element that contributes to the significance of the heritage place. to accompany all places on the Heritage Overlay may assist in the interpretation of significant elements.
- Additions and alterations should be designed to be either substantially concealed from key viewing points.
- Vistas or view lines to the principal facade(s) of a heritage place should be maintained.

The methods by which these key outcomes may be achieved is through ‘visibility tests’.

Visibility tests

This section provides some analysis of the visibility tests that are commonly used in heritage policy and guidelines. These are essentially performance measures with some analysis and recommendations. The illustrations have been those gathered from other sources and are commonly used as performance measures for how to achieve certain outcomes.

Alterations and additions, if over one storey in height will rarely be able to be totally concealed, however large sites may provide more opportunity to do so. Small and compact sites generally have fewer opportunities for substantial concealment.

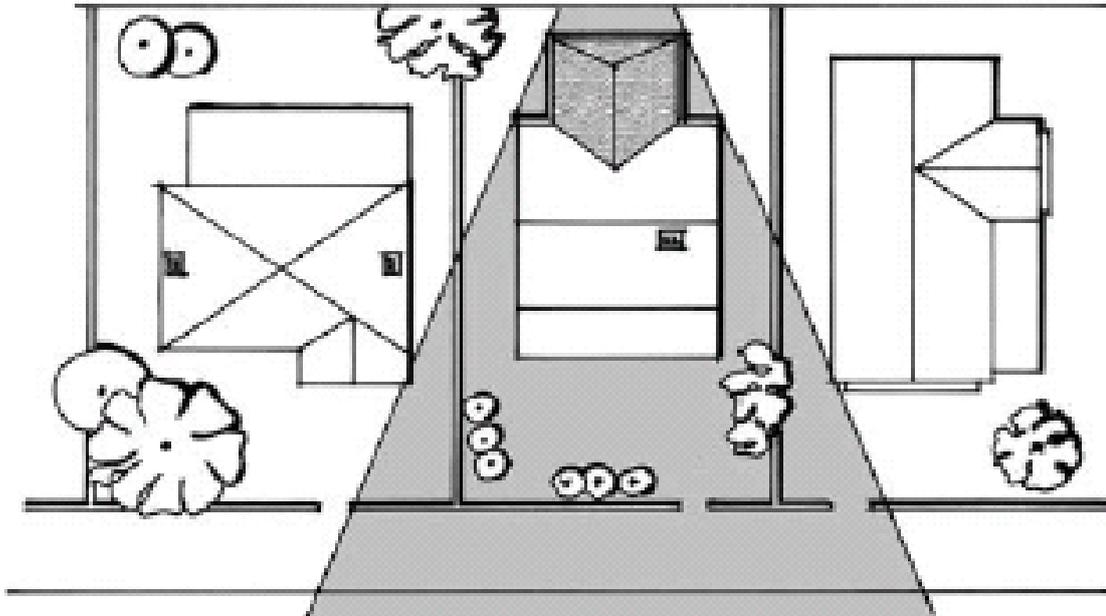


Figure 4 Visibility to the side of a building.

Larger sites may provide opportunity to wholly conceal extensions using a sightline test to determine building envelope. The viewing point is directly at the front of the building. Source: Whitehorse Heritage Guidelines.

This diagram is clearer than the one in the current heritage policy however it may be overly restrictive for small and compact sites. It is recommended that it be applied to lots over 500m² in area. Whilst it does not limit visibility entirely, it provides a performance measure of a greater setback for wider side additions and encourages additions to the rear of the site. The angle is established from the two front points of the building and is not dependent on where the viewer is standing. The depth of two front rooms test results has a similar resultant effect on the visibility of extensions to the side of a house.

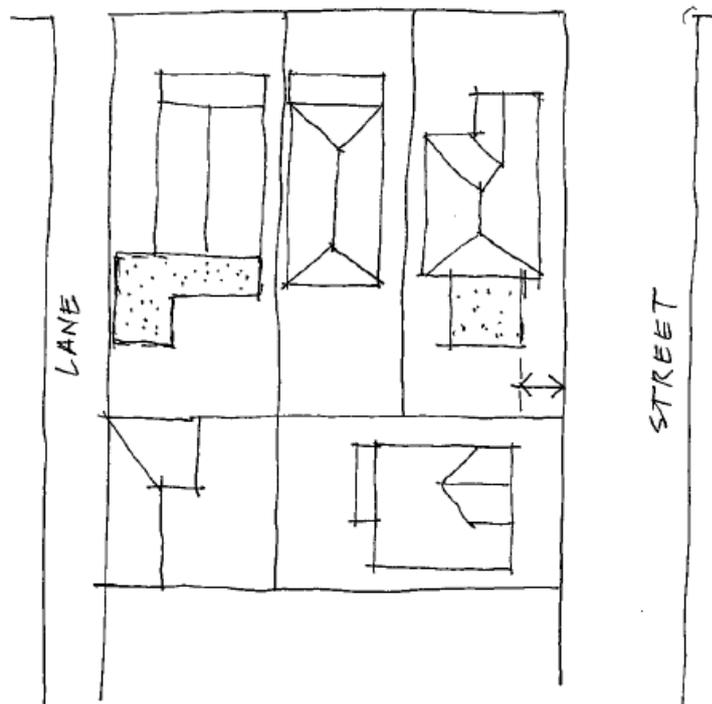


Figure 5 Corner siting on streets and laneways.

Setbacks for additions to houses on corner sites, both for two streets and for street and laneway corners.

Source: Context preliminary sketch

Streets and laneways have evolved historically as very different places. Laneways do allow the opportunity to build to zero setback if that is part of the character of the lane. Setbacks on laneways may be reduced to reflect the predominant setbacks along the laneway. Setbacks to side streets should reflect the setbacks of the houses fronting the side street. It is recommended that a heritage policy allows for differing setbacks depending on whether the corner is a street or a laneway. On corner sites concealment of additions may not be possible, making scale and form an important consideration.

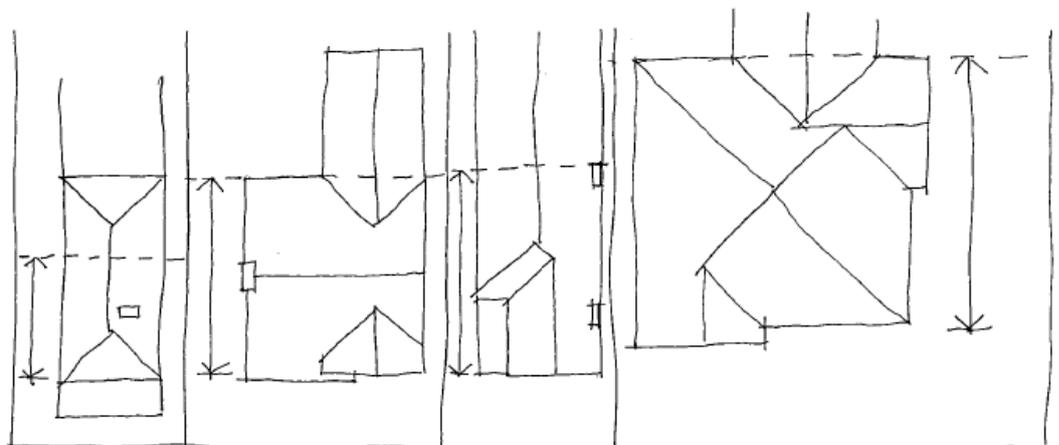


Figure 6 Depth of two rooms test.

Setbacks showing the depth of two rooms for different lot sizes and building typologies as defined by roof form.

Source: Context preliminary sketch adaptation of Stonnington Guidelines, 2000

A review of good and bad examples shows that all types of residential buildings benefit from this test. It is useful in promoting the conservation of the primary roof form and the chimneys. It is recommended that a performance measure based on the depth of two rooms test be included.

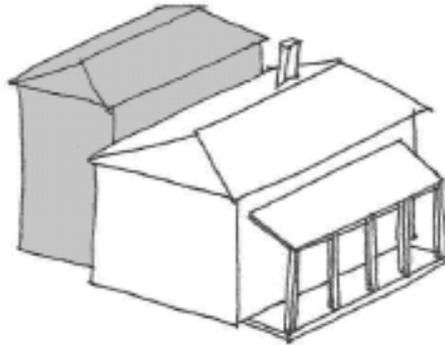


Figure 7 Additions sited behind the primary roof form – small lots

Small lots may create the opportunity for additions to be concealed behind the primary roof form.

Source Manningham Design Guidelines



Figure 8 Additions sited behind the primary roof form – large lots

For larger freestanding houses extensions can be concealed behind the primary roof form.

Source Moreland Design Guidelines

It is recommended that a performance measure on siting additions behind the primary roof form be used.



Figure 9 Using ceiling heights as a guide to height of two storey additions

Source: Moreland Heritage Guidelines

This is less a visibility test than a tool for achieving reasonable height in a two-storey extension. This diagram is a measurement of the ceiling height of the early building – nominally 3.0 metres.

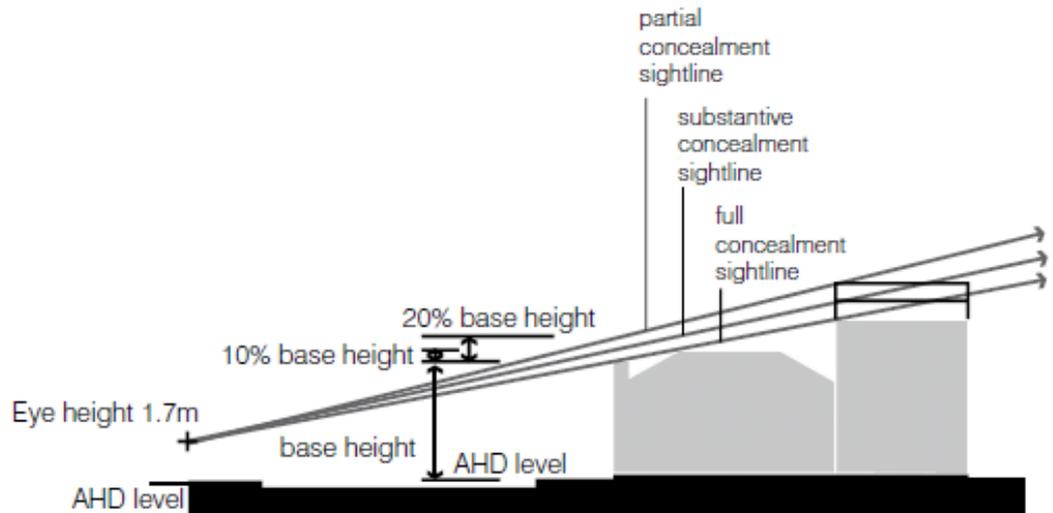


Figure 10 Vertical sightline test with three variables

A sightline test for full, substantive or partial concealment of additions that are taller than the host building.

Source: Heritage Council Guidelines

This vertical sightline test introduces a finer grain of detail to visibility testing and introduces added complexity for no great benefit..

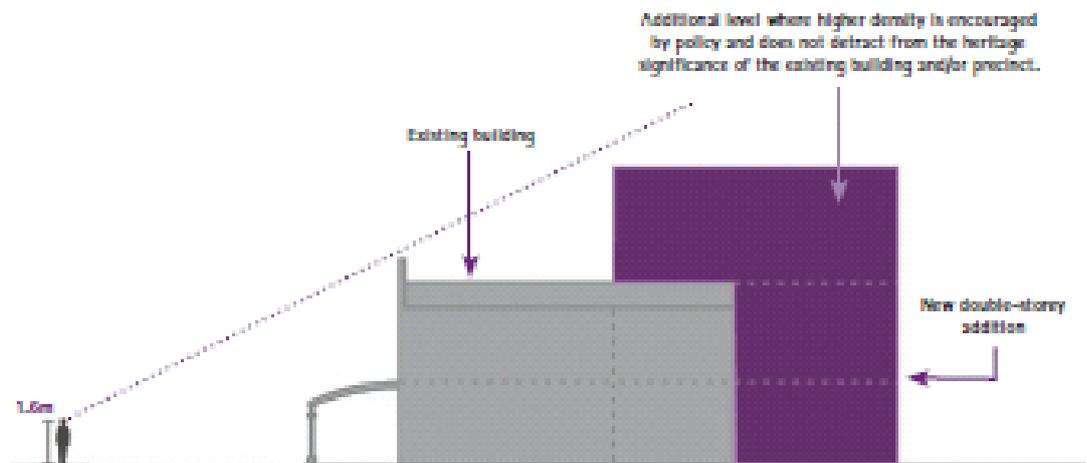


Figure 11 A two storey building and higher additions

Establishing a building envelope by sightline for a two-storey terrace. Source: Moonee Valley Heritage Guidelines.

Summary – visibility tests

Vertical sightline tests have limited success in limiting the visibility of upper floor additions. The sightline established through a fixed viewing point is limited in its effectiveness because the extension can be seen from more than one fixed point.

The sightline test for the ‘depth of two rooms’ or ‘behind the primary roof form’ allows for a setback that is related to the particular roof form and an easily understood performance measure that is less open to conjecture.

Key content

- Limit the visibility of additions to achieve substantial concealment.
- Require proposals for additions and alterations to be sited appropriately, and be of an appropriate scale relative to the host building.
- Show the extent to which the addition is visible to either side of a house (Fig.4).
- Show how additions address corner sites through appropriate setbacks to the street or laneway (Fig.5).
- Retention of the depth of the front two rooms (Fig.6). This addresses the situation where the ridgeline is perpendicular to the street i.e. gable fronted, hip fronted, or with a parapet. It covers all lot sizes, small, compact and large.
- Retention of the primary roof form (Figs 7 & 8) This addresses the situation where the ridgeline is parallel to the street or where is a dominant roof form that should be retained. It covers all lot sizes, small, compact and large.

Reconstruction of significant elements

Encourage the accurate reconstruction of elements where they have previously been removed.

Elements may include, but are not limited to:

- Verandahs
- Roofing
- Wall claddings
- Windows
- Front fences
- Other architectural details and features.

2.4 New buildings**What is the issue?**

- The design of new buildings should have close regard to the urban context in which they are located.
- Reference to atypical buildings as indicators of scale and form is not an appropriate design response.
- There may be different design approaches that are appropriate, depending on the context.
- New buildings should provide an appropriate design response to the setting, context and host building.
- Alterations and additions should not result in a loss of heritage value to individually significant and contributory places.

Learnings from VCAT***Atypical examples as precedents*****396 Canning Street Carlton North |**

Two related issues are of interest in this case. The use of atypical examples as precedents and the definition of the heritage place as the surrounding context rather than the individual place. The key content in the heritage policy includes:

A respectful design response is primarily demonstrated through appropriate siting, scale and massing in relation to the host building and its surrounding context

This would suggest that both the individual place and its context are considered. More weight might reasonably be given to contextual surroundings for a contributory place. For an individually significant place more weight may be given to significant attributes of the host place. The policy might benefit from differentiation in this regard, with specific reference to whether the place is contributory or individually significant.

The heritage policy intent includes reference to atypical examples in the following way:

Reference to atypical buildings as indicators of scale and form is not an appropriate design response.

This lends weight to the argument that atypical buildings should not be considered as the new benchmark for height or dominance. This is equally important whether the atypical building is a contemporary one or a heritage place, for example a church or other public building in a street of houses will be atypical but is not a reference point for additions to a house.

Policy intention

- Demonstrate in the design response, an understanding of the extent and values inherent in the cultural significance of the place. The use of site analysis diagrams is beneficial.
- Cultural significance, and the response to it should be demonstrated in the application requirements.
- A definition of the heritage place that is being responded to needs to be included. The boundary of the heritage place may be the individual site, the street or part of it, or the whole precinct. Where precincts are either very large and/or diverse in character the boundary of the area being considered is likely to be smaller to be meaningful.
- Reference to the Burra Charter Practice Note on Article 22 (Appendix 2) that encourages new buildings that support the cultural significance of the place.
- Reference to atypical buildings as indicators of scale and form is not an appropriate design response.

The policy should encourage new buildings that:

- Reinforce the spatial characteristics and relationships between buildings that form part of cultural significance
- Do not dominate adjoining heritage buildings
- Respect the scale, form, siting, setting and front and side setbacks of nearby heritage places (Fig. 12).
- Do not obscure views of the principal façade(s) or identified significant features of surrounding heritage places.

Key content

Where appropriate, such as in a street of uniform buildings, including a row or terrace, encourage a sympathetic approach to new buildings. (Fig. 15)

Contemporary design is encouraged provided that the cultural significance of the heritage place is also respected.

Contemporary designs for new buildings should also have regard to:

- Prevailing front and side setbacks
- Appropriate scale - height relative to the surrounding context
- Appropriate massing - roof form and pitch relative to the surrounding context

- Complementary materials and colours

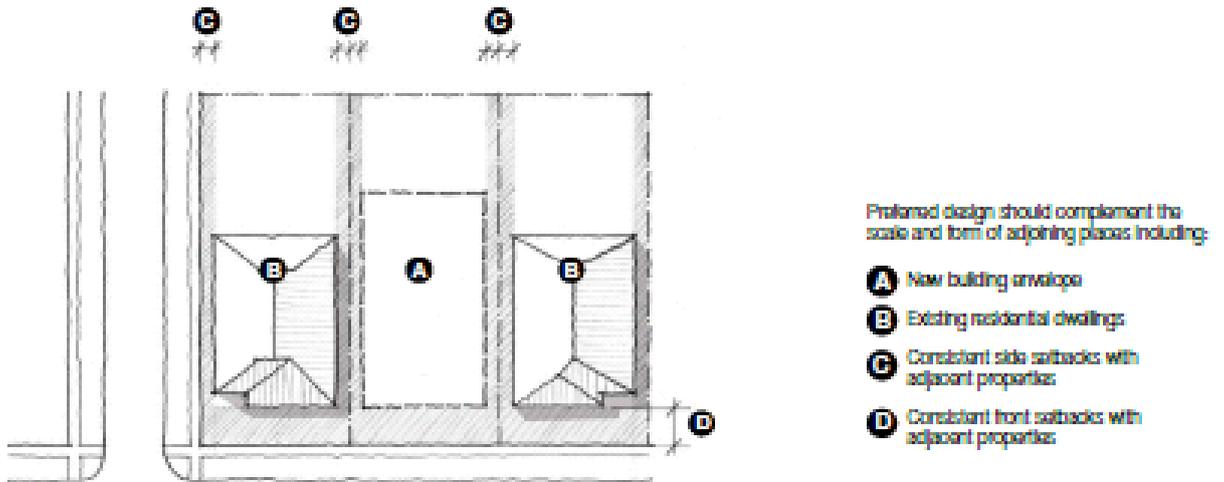


Figure 12 Residential setbacks, orientation and siting

This diagram could be made more useful with a greater diversity of setbacks shown.

Source: Stonnington Guidelines

Reconstruction – new buildings

Reconstruction of entire buildings is not generally recommended and is not recommended as an alternative to demolition or removal.

Reconstruction may be appropriate for a missing house in an otherwise identical row or terrace. A reconstructed building should be distinguishable **at close quarters**, but its scale and form may be identical.

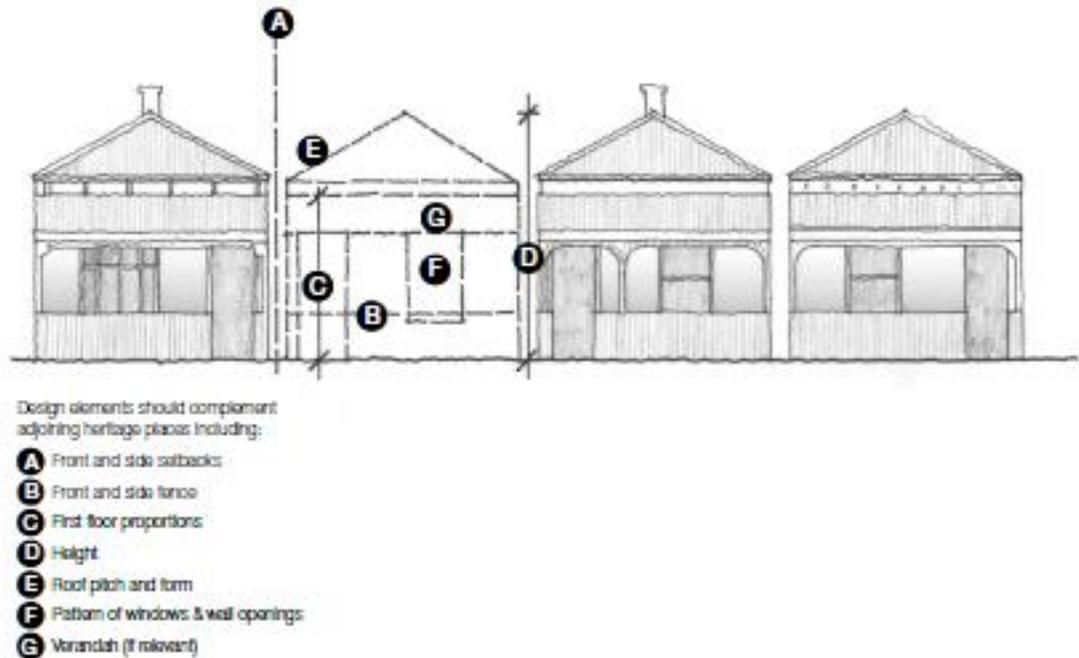


Figure 13 Modelling the form and scale of neighbouring buildings

Source: Stonnington Design Guidelines

REFERENCES

Australia ICOMOS Charter for Places of Cultural Significance, *Burra Charter*, 2013

Australia ICOMOS, Burra Charter Practice Note, 2013, Article 22, New Work

Heritage policies

City of Yarra

City of Brimbank

City of Port Phillip

City of Moonee Valley

City of Moreland

Heritage guidelines

City of Manningham – Warrandyte Township Guidelines, 2007

City of Greater Bendigo, Heritage Design Guidelines, 2015

Victorian Heritage Council Heritage Guidelines, 2007

City of Moonee Valley Heritage Guidelines (n.d.)

City of Greater Geelong Heritage and Design Guidelines, 1997

Darebin Heritage Guide, Alterations and Additions 1998

Darebin Heritage Guide, Residential Infill, 1998

City of Moreland, Heritage Guideline 2, alterations and additions to houses in heritage areas (n.d.)

City of Stonnington Heritage Design Guidelines, 2016

City of Stonnington Heritage Guidelines, 2000

City of Whitehorse, Guidelines for Alterations and Additions to individually listed dwellings in the Heritage Overlay, (n.d.)

NSW Heritage Office and RAI NSW Chapter, Design in Context, Guidelines for Infill Development in the Historic Environment, 2005

VCAT decisions

383 Scotchmer Street, Fitzroy North VCAT Ref. No. P2115/2014

39-41 Oxford Street, Collingwood VCAT Ref. No. 1527/2013

200 Park Street, Fitzroy North VCAT Ref. No. P2255/2016

246 Canning Street, Carlton North VCAT Ref. No. P125/2017

261-265 Fitzroy Street, Fitzroy VCAT Ref. No. P1231/2014

396 Canning Street, Carlton North VCAT Ref. No. P2605/2013

APPENDICES

- 1 Residential typologies in Yarra
- 2 Burra Charter Practice Note Article 22 New Work
- 3 Design in Context - application requirement checklist (example)

Residential types in the City of Yarra

Introduction

The City of Yarra is made up of a diverse group of suburbs with varied residential building types and streetscapes. Residential character varies considerably throughout the municipality. Defining what makes the special qualities of residential character is important in structuring an effective heritage policy.

Residential areas are defined by characteristics that include, but are not limited to, scale, form, siting, materials and colour and detailing. These characteristics underpin residential building typologies and are keys to structuring policy.

- Scale
 - Height
 - Density
 - Grain
- Form
 - Volume
 - Roofline and slope
 - Repetition
- Siting
 - Orientation to street frontage
 - Setback patterns
 - Alignment
 - Views and vistas
- Materials and colour
 - Predominant building materials
 - Texture
 - Pattern
 - Colour
- Detail elements
 - Predominant patterns
 - Repetition
 - Particular distinctive detail

Examples

The following broad groupings identifying compact, medium and large character areas provide some instructive information for the further identification of residential typologies using the characteristics defined above.

Compact character

- Narrow frontage
- No or minimal front setback
- Single storey (or small double storey)
- Hipped roof or parapet
- Terraced house or small freestanding cottage
- Located on narrow street with minimal landscaping and dense development.

Typically, houses in this category would be from the mid-late nineteenth century, and built as small dwellings in the suburbs of Richmond, Collingwood, Abbotsford and parts of Fitzroy and Carlton.



Figure 1. Richmond (typical terrace row with narrow street frontages and minimal setbacks).



Figure 2. Richmond (small detached dwelling).



Figure 3. Richmond (terrace row with single roof form, narrow frontages, minimal setbacks on crowded street setting).



Figure 4. North Carlton (row of small terraces with minimal street setbacks).

Medium sized character

- Wider frontages than above
- Houses set back behind small or medium sized gardens
- Single or double storey
- Hipped roof or parapet
- Large terraced houses or terraced styled houses detached or semi-detached,
- Detached houses with narrow side set backs
- On wider streets with some landscaping

Typically, these houses date from the early 20th century, or are double fronted examples from the late twentieth century. They represent a better class of housing in the working suburbs of Richmond and Fitzroy, and the lower end of development in the more prestigious parts of the municipality such as North Fitzroy, North Carlton and Alphington.



Figure 5. Richmond (wide frontages, minimal setback, semi detached, double fronted houses, wide street).



Figure 6. Clifton Hill (wide frontages, medium set back and front garden, semidetached, wide open street).



Figure 7. Clifton Hill (mixed row with generally wide frontages, attached or narrow side setbacks, mix of double fronted and single fronted, narrow front setback, wide open street).



Figure 8. North Carlton (row of mixed terraces, single fronted but with more generous frontage and front setbacks, wide open street setting).



Figure 9. Richmond (generous terraces and double fronted, narrow front setbacks, corner and laneway boundaries, medium width street setting).

Large character

- Free standing houses located on large garden blocks, with deep front and side setbacks.
- Single or double storey
- Located on wide tree lined streets



Figure 10. Clifton Hill (wide generous garden block, open wide street, large detached house).



Figure 11. North Fitzroy (large double storey detached house on large garden block, wide open street setting).



Figure 12. Alpbington (generous house on large garden site).

Burra Charter Article 22 — New Work

1 Purpose

This Practice Note provides guidance on the application of Article 22 of the *Australia ICOMOS Burra Charter, 2013* (hereafter *Burra Charter*). It is not a substitute for the *Burra Charter*.

The 1999 version of Article 22.2 of the Burra Charter (*'New work should be readily identifiable as such'*) has sometimes been used to support new design which does not respect the cultural significance of the place. Consequently, in the 2013 *Burra Charter* Article 22.2 has been revised to read: *New work should be readily identifiable as such, but must respect and have minimal impact on the cultural significance of the place.*

Scope

This Practice Note covers:

- 1 Purpose
- 2 What is New Work
- 3 Common issues in relation to New Work
- 4 Resources

2 What is New Work?

New work means additions or changes to a place and is commonly undertaken as part of *adaptation* (Articles 1.9, 21 of the *Burra Charter*) where a place is modified to suit an existing use or a proposed new use. New work may include additional buildings or structures at a place, as well as alterations to an existing building, to introduce new services, or to comply with legal or code requirements.

Terminology

Repair	<i>Repair involves restoration or reconstruction. (Article 1.5)</i>
Reconstruction	<i>Reconstruction means returning a place to a known earlier state and is distinguished from restoration by the introduction of new material. (Article 1.8)</i>
Adaptation	<i>Adaptation means changing a place to suit the existing use or a proposed use. (Article 1.9)</i>

3 Common issues in relation to New Work

Issue: Being readily identifiable does not automatically make new work sympathetic to the place.

The Burra Charter should always be read as a whole.

Guidance: As the preamble to the *Burra Charter* explains, *the Charter should be read as a whole and many articles are interdependent.* It is not appropriate to quote a single article as justification for

proposed works. All work should comply with the Charter as a whole, and this means that while new work should be readily identifiable, it should also:

- Not *adversely affect the setting* of the place (Article 8)
- Have *minimal impact on the cultural significance of the place* (Article 21.1)
- *Not distort or obscure the cultural significance of the place, or detract from its interpretation and appreciation* (Article 22.1)
- *Respect and have minimal impact on the cultural significance of the place* (Article 22.2).

An important factor in the success of new work is the quality and sensitivity of the design response. New work should respect the context, strength, scale and character of the original, and should not overpower it. The key to success is carefully considered design that respects and supports the significance of the place. Imitative solutions should generally be avoided: they can mislead the onlooker and may diminish the strength and visual integrity of the original. Well-designed new work can have a positive role in the interpretation of a place.

The cultural significance of a place and its particular circumstances will determine any constraints on the design of new work. If, for example, the issue is replacement of a removed building (producing a 'missing tooth') in a row of buildings that have a degree of uniformity, then the new work should closely follow the existing buildings in bulk, form, character, complexity of detail, set back, etc. Detailing of joinery or masonry should be modified to indicate the new work.

There will be other places where there are less contextual constraints on the design of new work. These will be where there is a greater diversity in the setting, or where the siting, form and scale of the new work will not adversely impact on significance. As Article 15.1 says: *The amount of change to a place and its use should be guided by the cultural significance of the place and its appropriate interpretation.*

Issue: The importance of getting the study boundary right

There may be different outcomes depending on the choice of study boundary.

Guidance: The boundary of the area being considered may need to be changed to properly understand a particular situation. Using the example of the missing tooth in a row of buildings: if an important aspect of their significance is the uniformity of the group, then the scale of reference should be changed to include the whole row and replacement of the missing one should be treated as reconstruction (Article 20), rather than new work. This would be the case where the buildings in the row are essentially identical, and where they are significant for that uniformity — a rigorous understanding of significance is critical.

Issue: Recognising that there is a distinction between New Work and Reconstruction

Fabric is treated differently, depending on whether it is New Work or Reconstruction.

Guidance: There is often confusion about the distinction between new work (Article 22) and the work done as part of reconstruction (Articles 1.8 and 20). While reconstruction makes use of new materials, it is not new work in the *Burra Charter* sense. Whereas *New work should be readily identifiable as such* (Article 22.2), *Reconstruction should be identifiable on close inspection or through additional interpretation* (Article 20.2). Thus repairs (of which reconstruction is the most common) should **not** be readily apparent, so that the cultural significance of the place is not distorted or obscured, and can be clearly understood. At the same time there is the need to be honest about

the repairs, so they should be identifiable, by subtly modifying materials or details, or by incorporating the date and/or marking devices that indicate the extent of the work.

4 Resources

Primary resources

Australia ICOMOS, 2013 *The Burra Charter: the Australia ICOMOS charter for places of cultural significance 2013*. Available from <http://australia.icomos.org/publications/charters/>

Walker, M. & Marquis-Kyle, P., 2004 *The Illustrated Burra Charter: good practice for heritage places*. Australia ICOMOS. Available from <http://australia.icomos.org/publications/charters/>

Other key guidelines

NSW Heritage Office and RAIA NSW Chapter, 2008 *New uses for heritage places: guidelines for the adaptation of historic buildings and sites*. Available from <http://www.environment.nsw.gov.au/resources/heritagebranch/heritage/NewUsesforHeritagePlaces.pdf>

NSW Heritage Office and RAIA NSW Chapter, 2005 *Design in context: guidelines for infill development in the historic environment*. Available from <http://www.environment.nsw.gov.au/resources/heritagebranch/heritage/DesignInContext.pdf>

ASSESSING NEW DEVELOPMENT IN A HISTORIC CONTEXT

THIS SECTION PROVIDES INFORMATION ON HOW STATUTORY BODIES ASSESS DEVELOPMENT APPLICATIONS FOR NEW BUILDINGS AFFECTING A HERITAGE LISTED CONTEXT.

Assessors take a number of aspects into account when appraising development applications. When assessing heritage impact the assessor will check that the development meets the design criteria outlined in these guidelines. Design quality will be an important element. Given that most heritage buildings and conservation areas have been listed for their historic and/or architectural (aesthetic)

significance, it is almost impossible to ignore design quality in the assessment process. Heritage assessment is often accused of being subjective, and to some extent there will be a subjective element to the process. It is possible, however, to define objectives for assessing development within an established and valued historic context. The design criteria put forward in these guidelines attempt to do this.

This checklist assists designers, applicants and assessors to determine whether the proposal is appropriate. It encompasses both the qualitative aspects of the design of the building and the quality of its contribution to its historic context.

ASSESSING NEW DEVELOPMENT IN AN HISTORIC CONTEXT: CHECKLIST FOR APPLICANTS AND ASSESSORS

PART A: DOCUMENTS TO BE INCLUDED	APPLICANT'S CONFIRMATION	ASSESSOR'S COMMENTS
Date of submission		
Statement of heritage impact statement (SOHI) Include a statement of significance for any heritage item, precinct or conservation area affected by the new development Respond to the design criteria described in Design in Context in graphic and written point form (see Part B of this checklist)		
Site plan Showing setting including adjacent properties, buildings, trees and structures such as fences 1:200 scale min.		
Landscape plan 1:100 scale		
Floor plans 1:100 scale		
Sections and details 1:100 scale minimum		
Elevations 1:100 scale minimum		
Fencing details 1:50 scale minimum		
External materials and colours Provide schedule and, where required, a sample board		
Working model 1:200 scale minimum		

ASSESSING NEW DEVELOPMENT IN AN HISTORIC CONTEXT: CHECKLIST FOR APPLICANTS AND ASSESSORS

PART B: CHECKLIST FOR INCLUSION IN HERITAGE IMPACT STATEMENT: RESPONSE TO DESIGN CRITERIA	APPLICANT'S CONFIRMATION	ASSESSOR'S COMMENTS
<p>01. CHARACTER</p> <p>Use annotated diagrams, photographs and/or sketches to describe the factors which contribute to the character of the historic context, including:</p> <ul style="list-style-type: none"> • topography of site and its surroundings; • distinctive landscape elements and quality; • street and subdivision patterns; • date and style of built form; • figure/ground and figure/landscape qualities; • views, vistas and skylines; • local culture and traditions; • uses; • consistency or repetition of above factors. 		
<p>02. SCALE</p> <p>Annotate drawings, photographs of model or photomontages to describe the relationship between the proposed new development and the context, in terms of the following design criteria:</p> <ul style="list-style-type: none"> • scale of buildings; • building and wall heights; • massing; • density — pattern of arrangement of buildings and size of buildings; • proportions; • rhythm of buildings and landscape; • floor-to-floor heights and relationship to ground or street plane; • modulation of walls, openings and roof planes in response to the scale of neighbouring buildings; • transition between different heights (for example, through the use of setbacks). 		
<p>03. FORM</p> <p>Annotate drawings, photographs of model or photomontages to describe the relationship between the proposed new development and the context, in terms of the following design criteria:</p> <ul style="list-style-type: none"> • predominant form of neighbours; • roof form and skyline — ridge and parapet lines, roof slopes, punctuation by party walls, chimneys and lanterns or skylights; • proportion and number of openings; • solid to void ratios; • relationship between internal and external spaces. 		

ASSESSING NEW DEVELOPMENT IN A HISTORIC CONTEXT

ASSESSING NEW DEVELOPMENT IN AN HISTORIC CONTEXT: CHECKLIST FOR APPLICANTS AND ASSESSORS

PART B: CHECKLIST FOR INCLUSION IN HERITAGE IMPACT STATEMENT: RESPONSE TO DESIGN CRITERIA	APPLICANT'S CONFIRMATION	ASSESSOR'S COMMENTS
<p>04. SITING</p> <p>Annotate drawings, photographs of model or photomontages to describe the relationship between the proposed new development and the context, in terms of the following design criteria:</p> <ul style="list-style-type: none"> • predominant setbacks — front, side and rear; • boundary walls and fences; • orientation and address of buildings; • location and dimensions of driveways and garages and design strategies to reduce their visual and physical impact on the streetscape; • retention of views and vistas to and from the new development, across townscape or landscape; • retention of natural features of significance; • retention of significant archaeological remains; • quality of spaces created between existing and new. 		
<p>05. MATERIALS AND COLOUR</p> <p>Annotate drawings, photographs of model or photomontages to describe the relationship between the proposed new development and the context, in terms of the following design criteria:</p> <ul style="list-style-type: none"> • response to predominant materials, textures and colour palette — harmonious, complementary, contrasting; • commensurate quality of new materials; • qualities of light and shadow; • hierarchy of material use (for example, solid masonry base and lightweight upper levels); • relationship between skeleton or structure and skin. 		
<p>06. DETAILING</p> <p>Annotate drawings, photographs of model or photomontages to describe the relationship between the proposed new development and the context, in terms of the following design criteria:</p> <ul style="list-style-type: none"> • response to distinctive details of neighbouring existing buildings — reinterpretation in contemporary materials, contrast; • relationship of new fences, garden walls, planting and landscape elements to important existing details; • unobtrusive design of new service elements, such as solar panels and water tanks. 		



Yarra Industrial Heritage Policy

15 October 2019



Prepared for City of Yarra

GJM Heritage

a: Level 3, 124 Exhibition Street [GPO Box 2634], Melbourne, VIC 3001

t: 0481 284 130

e: enquiries@gjmheritage.com

w: www.gjmheritage.com

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Project Team

Jim Gard'ner Director
Rena Jarman Director
Jessica Hogg Heritage Consultant

Document versions

Project no.	Version	Issued to	Date issued
2017-001	Memorandum of Advice	Amanda Haycox	15 February 2018
2019-046	Draft Report	Amanda Haycox	1 October 2019
	Final Report	Amanda Haycox	15 October 2019

Cover Image

Former W. Saunders & Son Factory/Warehouse Complex (c.1911-1920s), 18-62 Trenerry Crescent, Abbotsford with later additions by Darryl Jackson AO for Esprit de Corps (1984) ©GJM Heritage, March 2017.

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Executive Summary

This report updates and builds on earlier advice prepared by GJM Heritage in early 2018, which was intended to inform revised heritage policy in relation to industrial places within the Yarra Planning Scheme.

The methodology adopted for completing this project was as follows:

1. The *Northern Suburbs Factory Study* (Vines & Churchward, 1992) was reviewed, particularly as it relates to suburbs within the City of Yarra. The categories of industrial building types relevant to Yarra were identified and extant examples of the various building typologies were identified in Abbotsford, Collingwood, Clifton Hill, Fitzroy North and Fitzroy. Citations were reviewed.
2. Site visits to identified typological examples was undertaken. Where the buildings or complexes had been redeveloped, photographs were taken and positive and negative features of the redevelopment were identified.
3. Existing policy guidance relating to the management of industrial heritage sites from other local planning authorities was identified and reviewed.
4. Policy relevant to the Yarra context has been drafted, drawing on the above material.

Whilst the *Northern Suburbs Factory Study* identifies 11 industrial building ‘types,’ for the purposes of the new planning policy we have described four key typologies: Residential scale industry / workshops; Multi-storey factories; Low-rise (one and two storey) factories; and Specialist processing plants. Each of these four typologies requires different management approaches when considering redevelopment.

Following the analysis, it is evident that some industrial sites in the City of Yarra are included within broad precinct-based Heritage Overlays that are largely residential and in character and may not share heritage values while others, particularly large-scale industrial complexes are located within dedicated industrial precincts. Each of these industrial building forms and contexts required nuanced and specific design responses to enable their development in a way that protects both the values of the individual industrial place and, in the case of those within precinct overlays, their wider context.

Drawing on a number of case studies and the analysis of existing policies from other municipalities, a number of additional heritage-related policies are recommended for inclusion within the Yarra Planning Scheme.

The introduction of specific provisions within the Yarra Planning Scheme to encourage the retention and appropriate redevelopment of industrial heritage sites should include new clauses within both the Local Planning Policy at Clause 22.02 – ‘Development Guidelines For Sites Subject To The Heritage Overlay’ noting that this clause will be translated into Clause 15.03-1S as required by VC148.

Many of the policies – recommended in this advisory report at 5. *Recommended Industrial Heritage Policy* – will apply to all heritage places, while some are specific to industrial sites. Consideration will need to be given to the format of the policy to avoid unnecessary repetition, but to ensure that all relevant policies are applied to a consideration of applications for development of industrial heritage buildings and sites.

1.0 Introduction

GJM Heritage has been engaged to provide input into planning policy to manage change within industrial heritage sites in the City of Yarra. The policy is intended to form part of a revised Heritage Policy within the Yarra Planning Scheme.

The methodology adopted for completing this project was as follows:

1. The *Northern Suburbs Factory Study* (Vines & Churchward, 1992) was reviewed, particularly as it relates to suburbs within the City of Yarra. The categories of industrial building types relevant to Yarra were identified and extant examples of the various building typologies were identified in Abbotsford, Collingwood, Clifton Hill, Fitzroy North and Fitzroy. Citations were reviewed.
2. Site visits to identified typological examples was undertaken. These site visits also considered the built heritage context within which they are located. Where the buildings or complexes had been redeveloped, photographs were taken and positive and negative features of the redevelopment were identified.
3. Existing policy guidance relating to the management of industrial heritage sites was sourced and reviewed.
4. Policy relevant to the Yarra context has been drafted, drawing on the above material

This advice originally prepared in February 2018 in the form of a memorandum, and has been updated and expanded to consider additional properties and changes in policy since that time.

2.0 Northern Suburbs Factory Study

The *Northern Suburbs Factory Study* identifies 11 industrial building types. Within Yarra a number of these types overlap; for example, there are a number of multi-storey factories (Type 5) with imposing classical façades (Type 4), as well as many large low-rise (single- and two-storey) complexes with saw-tooth roofs (Type 6) that have Moderne façades (Type 7) (see Appendix A for the extract from *Northern Suburbs Factory Study*) The change in industrial building form and design relates to technological changes in manufacturing from the nineteenth to the twentieth centuries, and the move away from the multi-storey mill to the efficiencies of the single-level production line, which required lower-scaled buildings with a larger footprint.

The typologies identified in the *Northern Suburbs Factory Study* span form, type, style and period and are not considered useful for informing the future development of these buildings. Therefore, when considering industrial heritage sites in Yarra with redevelopment potential, we have identified four key typologies:

1. **Residential scale industry / workshops** (Types 1 and 3 in the *Northern Suburbs Factory Study*) – this type of industrial building is found scattered throughout the City of Yarra. These buildings contained small-scale manufacturing or trade services such as blacksmithing, motor mechanics or artisans and are normally single-storey and have a domestic scale and appearance. This type of industrial building is often found within established low-scale residential areas within the City of Yarra.
2. **Multi-storey factories** (Type 5 in the *Northern Suburbs Factory Study*) – industrial buildings where manufacturing occurred on multiple levels of the same complex. These factories have their antecedents in the eighteenth and nineteenth century mills of the Industrial Revolution in England and were constructed for manufacturing textiles, clothing and footwear. This type of industrial building is commonly located within or immediately adjacent to low-scale residential areas within the City of Yarra.
3. **Large low-rise factories** (Type 6 in the *Northern Suburbs Factory Study*) – larger sites for single- and two-level production-line operation reflecting the changing manufacturing techniques pioneered by Henry Ford and others in the United States at the beginning of the twentieth century. This typology often has saw-tooth roof forms and a decorative (often Moderne) one or two-storey principal façade. These large-scale factories are more commonly found within dedicated industrial precincts rather than within established residential areas.
4. **Specialist processing plants** (Type 2 in the *Northern Suburbs Factory Study*) – industrial complexes comprising numerous buildings or structures in which specialised manufacturing processes occurred. Such sites include breweries, distilleries, tanneries, gasworks, freezing works, rope works and flour mills. These often have specific building forms such as grain silos, shot towers, distillery and brewing towers, gasometers etc. Again, this industrial heritage place type is typically located within dedicated industrial precincts rather than within established residential areas.

Each of these four typologies requires different management approaches when considering redevelopment.

3.0 Examples of redeveloped industrial sites

A number of examples of the four different typologies were visited and photographed from the public realm. Where redevelopment of the site had occurred, both positive and negative features of the redevelopment were documented to build up a body of principles that can be applied in a policy sense.

Regardless of the typology under consideration, the following principles were found to be relevant when considering the redevelopment of all four industrial typologies in order to encourage a positive heritage outcome:

- Significant fabric, such as principal elevations and industrial roof forms (where the roof is an important feature of place, for example, due to the presence of lanterns etc), should be retained and conserved.
- Chimneys, silos and towers within industrial sites are often local landmarks and communicate the function and industrial nature of these heritage places. These features should be retained and conserved. Their visual prominence within the site should be retained.
- The original form and scale of the industrial site should remain legible in any redevelopment.
- Remnant painted signage should be retained and conserved.
- The inter floor height of new development should consider the impact on the existing fenestration pattern to avoid new floor plates cutting through window and openings etc.
- Recessive colours and materials for additions and new built form should be used and dramatically contrasting colours or patterns within additions should be avoided.
- Additions should reflect or subtly interpret historic façade patterning, including fenestration patterns and proportions, the relationship between solid and void, and the module of structural bays. Unarticulated curtain glazing should be avoided
- Highly reflective glazing in both historic openings and new built form should be avoided.
- Visually lightweight additions and/or linking elements should be encouraged to transition between historic and new built form, especially where the heritage building is of masonry (brick or concrete) construction.

The above principles are not replicated in the following examples.

3.1 Residential scale industry / workshops

This typology is scattered throughout both the residential and commercial areas of Yarra. It is generally associated with small-scale 'light industry' such as clothing and footwear manufacture, blacksmithing, motor garages and maintenance workshops. Other examples of this type include local utilities such as water pumping stations and electrical substations. These buildings often have a domestic scale and appearance.

When this typology occurs in residential areas, the residential provisions of the heritage policy should be applied. Given that the building can often be atypical within a residential heritage precinct context, the heritage policy needs to ensure that it does not preclude or compromise the appropriate management of these places. When occurring in a mixed use or commercially zoned context these buildings are often susceptible to inappropriate redevelopment due to their small scale and perceived lack of adaptability.



Pattern Maker – Rae St, Fitzroy North (HO327 – North Fitzroy Precinct)

Positive features:

- Conversion to residential retains original form and scale.
- Original signage retained and conserved.
- Alterations confined to the rear.

Negative features:

- Domestic-style landscaping obscures the building.

Policy considerations:

- Encourage the use of 'industrial' rather than 'domestic' type fences, walls and landscaping, where appropriate.
- In a residential precinct, policy relating to the siting and form of alterations and additions should generally apply to this typology.



Footwear Manufacturers – Noone St, Clifton Hill (HO316 – Clifton Hill Eastern Precinct)

Positive features:

- Façade and western elevation retained intact.
- Three-storey townhouses concealed behind façade when viewed from the street.
- Subtly contrasting brickwork applied to increase height of the western façade.
- Materials used reference the brickwork and rendered parapet.
- A similar ratio of solid to void is used in the new walls to that of the historic façade.

Negative features:

- Only a small return of the brickwork on the eastern elevation retained.



Policy considerations:

- Side elevations visible from the public realm should normally be retained. Where a side elevation has been substantially altered or makes a minor contribution to the significance of the place, the depth of one or more structural bays should normally be retained to ensure facadism is avoided.
- Minimise new openings in intact secondary elevations and reuse existing openings wherever possible.



160 Argyle St, Fitzroy (HO334 – South Fitzroy Precinct)

Positive features:

- Retention and conservation of the historic openings and security bars on the heritage building.
- Retention of historic painted signage on the primary facade.
- Visual contrast of the rectilinear form of the heritage building and the curved form of the new building.

Negative features:

- Complete loss of the roof form.
- Inadequate setbacks resulting in the new building engulfing the single-storey heritage building.
- Poorly considered junction between the new and heritage fabric, including truncation of side elevation signage.
- Floor to ceiling heights within the heritage shell do not reflect original volumes, resulting in a floor level running across window openings in the heritage fabric.
- The loss of the majority of the external fabric results in facadism.

Policy considerations:

- Side elevations visible from the public realm should normally be retained. Where a side elevation has been substantially altered or makes a minor contribution to the significance of the place, the depth of one or more structural bays should normally be retained to ensure facadism is avoided.
- Require retention of part of the roof form where it is visible from the public realm. Generally, a minimum depth of a structural bay should be retained.



- Avoid cantilevering or building over the significant elements of the historic building including facades and parapets.
- Require new built form to adopt a subtly contrasting approach that respects the industrial character of the heritage place and distinguishes new built form from the historic fabric.

3.2 Multi-storey factories

These are industrial buildings where manufacturing occurred on multiple levels of the same complex. For larger buildings (three or more storeys), redevelopment can often be accommodated within the existing volume of the factory. A one or two-storey visually lightweight rooftop addition may also be appropriate for these buildings.

This industrial typology is normally characterised by decorative (often Classically inspired) facades and brick or rendered masonry construction. Chimney stacks may also be present. Examples in Yarra include the Denton Hat Mills, parts of both the Foy & Gibson Complex, and parts of the MacRobertson's Confectionary Complex. These buildings are often located within or immediately about low-rise residential areas that are subject to the Heritage Overlay.



Denton Hat Mills – 46-60 Nicholson St, Abbotsford (VHR H0815)

Positive features:

- Volume and scale of the original factory retained.
- Principal façades and chimney retained and conserved.
- New development concealed behind the principal façades.
- Internal redevelopment retains original floor to ceiling heights.

Negative features:

- No obvious negative features – a good heritage outcome.

Policy considerations:

- Avoid new openings in highly intact facades, and minimise the need for large-scale openings particularly for vehicle access.





Foy & Gibson Complex – Collingwood (VHR H0755)

Positive features:

- Volume and scale of the original factory retained.
- Principal façades and chimneys retained and conserved.
- Internal redevelopment retains original floor to ceiling heights.
- Upper level additions are set back from the principal façades and read as a clearly separate intervention.
- The vertical proportions of the two-storey addition (to the left) reflect the bay widths of the façade.
- The two-storey addition (to the left) is set back from the chimneys, allowing them to continue to be read as freestanding elements.
- The colour of the roof top addition (below) is recessive.

Negative features:

- The contrasting striped appearance of the two-storey addition (above) is visually distracting and draws the eye away from the historic façade.
- The balcony treatments of the addition (above) are incongruous with the materiality of the heritage place.
- The windows in the roof top addition (to the left) do not reflect the fenestration pattern or proportions of the windows in the façade. The high void to solid wall proportions of the addition visually distracts the eye from the façade.

Policy considerations:

- One to two-storey visually lightweight rooftop additions may be acceptable for multi-storey factories of three or more storeys.
- Avoid dramatically contrasting colours or patterns within additions.
- Upper level additions on multi-storey buildings (three or more storeys) should be set back at least the depth of one structural bay from facades to enable the three-dimensional form of the building to remain legible.
- Where the roof is an important feature of place (for example, due to the presence of lanterns etc), the historic form of the roof should be retained and conserved.



21-23 Stewart Street, Richmond (HO332 – Richmond Hill Precinct)

Positive features:

- Volume and scale of the original factory retained.
- Principal façades retained and conserved.
- Internal redevelopment retains original floor to ceiling heights.
- Upper level additions are set back from the principal façade and read as a clearly separate intervention.
- Secondary elevations are retained.
- The new additions utilise an appropriate 'industrial' design aesthetic.
- The extent of the heritage building and the new development is clearly differentiated with the change of colour and materials; in this case a yellow 'fin' is used to delineate the new development.

Negative features:

- Balconies created behind the windows of the principal façade break up the rhythm of the façade and can result in it being read as thin-skin façadism.
- Setbacks of new built form on the secondary elevations should be greater to enable the form of the heritage building to be more clearly read.

Policy considerations:

- Discourage the removal of windows from existing openings to create balconies.
- Require adequate setbacks from secondary elevations to ensure the three dimensional form of the heritage building is maintained.
- Require new built form to adopt a subtly contrasting approach that respects the industrial character of the heritage place and distinguishes new built form from the historic fabric.



80-88 Trenerry Crescent, Abbotsford (HO337 – Victoria Park Precinct)

Positive features:

- The Trenerry Crescent elevation has been retained.

Negative features:

- Complete loss of the roof form and side elevations resulting in facadism.
- Lack of side and upper-level setbacks means the new building engulfs and overwhelms the heritage building.
- Strong horizontal emphasis of the new building bears no relationship to the vertical emphasis of the heritage building.
- Overly reflective glazing inserted into historic openings.

Policy considerations:

- Side elevations visible from the public realm should normally be retained. Where a side elevation has been substantially altered or makes a minor contribution to the significance of the place the depth of one or more structural bays should normally be retained to ensure facadism is avoided.
- Require retention of part of the roof form where it is visible from the public realm. Generally, a minimum depth of a structural bay should be retained.
- Avoid cantilevering or building over the significant elements of the historic building including facades and parapets.
- Require new built form to adopt a subtly contrasting approach that respects the industrial character of the heritage place and distinguishes new built form from the historic fabric.



3.3 Large low-rise factories

This typology is subject to the most pressure from a redevelopment perspective but are often located within dedicated industrial precincts rather than in established low-rise residential areas. These factories generally occupied larger sites for single- or two-level production-line operation, resulting in large footprints with redevelopment potential. This typology often has saw-tooth roof forms delineating the factory floor (refer to Type 6 ‘The Shed Principle’ in the *Northern Suburbs Factory Study*) and a one or two-storey administration/office component behind the principal façade.



Image Source: VHD

Harry the Hirer (former Repco Factory) 85-91 Burnley Street, Richmond (HO252)

The former Repco Factory and office complex is subject to Yarra Amendment C223 which seeks to apply a Development Plan Overlay (DPO15). The front part of this site is subject to the Heritage Overlay upon which it is proposed to erect two towers, one of 12 storeys and one of 7 storeys set back between 8m and 13m from the street frontages. Note: the comments below are based on the DPO not a approved or built development.

Positive features:

- The street elevations of HO252 have been retained.
- The proposed development will reinstate the original brick and render finishes.
- Lower (7 storey) built form is proposed behind the single storey factory building facing Appleton Street.
- The setbacks (8m, 10m and 13m) proposed in DPO15 help ensure that the addition reads as a clearly separate element to the historic form.
- The breaking up of the new built form into separate towers avoids a monolithic upper level form.

Policy considerations:

- Require retention of part of the roof form where it is visible from the public realm. Generally, a minimum depth of a structural bay should be retained.
- Set back taller elements from the retained form of the heritage building.
- Encourage higher built form to be broken up into separate elements to avoid a monolithic new built form.
- Recognise that large industrial complexes outside low-rise residential areas that are subject to the Heritage Overlay provide the opportunity for taller build form than might be achieved within a primarily residential precinct.



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Former Builders' Steel Form Supply Co., 9-11 David Street, Richmond (H0250)

Positive features:

- The street elevations of H0250 have been retained.
- The development has enabled the restoration of elements of the David Street façade.
- Window openings have generally been retained.

Negative features:

- Complete loss of the roof form.
- The new 11 storey tower is a large stepped mass that visually dominates the retained façade.
- The minimal (2m) setback of new upper level form from the retained façade does not enable the three dimensional form of the former Builders' Steel Form Supply Co. building to be understood.
- The heavily articulated façade to the upper level development including recessed balconies, 'winter garden' balconies and moveable perforated metal screens is not recessive and visually dominates the heritage façade.
- Height of the new towers is not keeping with the character and appearance of adjacent buildings and the heritage place.

Policy considerations:

- Require adequate setbacks from street elevations to ensure the three-dimensional form of the heritage building is maintained and it reads as the primary element of the site.
- Encourage higher built form to be broken up into separate elements to avoid a monolithic new built form.
- The architectural language and materiality of the new addition bears little or no relationship to the heritage form.
- Avoid overly articulated façade treatments that visually compete with the simple Moderne façade of the existing building.



Cnr Gipps & Nicholson Street, Abbotsford (HO313 – Charles Street Precinct)

Positive features:

- The volume and exterior walls of the original factory building is maintained.
- Door and window openings in the original factory building are maintained and the facades have been conserved.
- The new addition reads as a clearly separate element to the historic form.
- The use of a darker brick for the addition references the materials and solidity of the original form, but provides an appropriate contrast. The darker colour is recessive against the original brickwork.
- A recessed, dark glazed level allows the new addition to rise above the historic parapet, allowing the new form to appear as a clearly separate element to the historic form.
- The fenestration pattern of the new addition references the patterning and solid to void proportions of the historic form.
- The roof form of the new addition subtly references the sawtooth roof form that originally existed on the historic form.

Negative features:

- Front and side setbacks of the new development could have been increased to enhance the prominence of the historic fabric.

Policy considerations:

- New built form (as visible from the street) should not exceed approximately the same volume of the historic form.
- Require adequate setbacks from street elevations to ensure the three-dimensional form of the heritage building is maintained and it reads as the primary element of the site.
- Encourage the use of visually lightweight 'linking elements' (for both upper level and side additions) to clearly differentiate historic built form from new.



Footwear Manufacturers – 98-100 Roseneath St, Clifton Hill (HO316 – Clifton Hill Eastern Precinct)

Positive features:

- The volume and exterior walls of the original factory building is retained.
- Door and window openings in the original factory building are maintained and the facades have been conserved.
- The new addition reads as a clearly separate element to the historic form.

Negative features:

- Front and side setbacks of the new development could have been increased to enhance the prominence of the historic fabric.
- The architectural language and materiality of the new addition bears little or no relationship to the heritage form.
- Large areas of curtain glazing are used and the window proportions of the new development bear no relationship to the heritage building.
- The glazed walls are set on the parapet line of the existing façade.
- The new balustrades are not recessive and disturb the line of the brick parapet.

Policy considerations:

- New built form (as visible from the street) should not exceed approximately the same volume of the historic form.
- Require adequate setbacks from street elevations to ensure the three-dimensional form of the heritage building is maintained and it reads as the primary element of the site.
- Avoid building directly above existing parapets and require that new balustrades are setback from the face of the existing parapet.



Bootmakers – 28-42 Reid Street, Fitzroy North (HO327 – North Fitzroy Precinct)

Positive features:

- The volume and exterior walls of the original factory building are maintained.
- Door and window openings in the original factory building are maintained and the facades have been conserved.
- The new addition reads as a clearly separate element to the historic form, and is substantially concealed from views directly opposite due to its stepped form
- The fenestration pattern of the new addition references the patterning and solid to void proportions of the historic form.



- Retention of historic painted signage.

Negative features:

- None of the saw-tooth roof form that was previously visible from oblique views has been retained.
- No side setbacks of the new development with a corrugated steel clad wall build directly off the saw-tooth profile parapet.
- The architectural language of the new addition bears little or no relationship to the heritage form, particularly in terms of the spacing of horizontal features.

Policy considerations:

- Require adequate setbacks from side elevations to ensure the three dimensional form of the heritage building is maintained.
- Retain one or more bays of the industrial roof form, such as the saw-tooth roof, where visible from the public realm.
- Avoid building directly above existing parapets.



Clothing Manufacturers – Cnr Wellington & Keele St, Collingwood (HO321 – Gold Street Precinct)

Positive features:

- The volume and exterior walls of the original factory building are retained.
- Door and window openings in the original factory building are maintained and the facades have been conserved.
- The new addition reads as a clearly separate element to the historic form.

Negative features:

- The awkwardly composed box-like addition does not adequately differentiate itself in materials or colours from the heritage building and overwhelms the heritage fabric.
- The fenestration pattern of the addition is ad hoc and bears little or no relationship to the heritage form.
- Ground floor windows have been obscured.
- Front and side setbacks of the new development could have been increased to enhance the prominence of the historic fabric.

Policy considerations:

- Require adequate setbacks from street elevations to ensure the three-dimensional form of the heritage building is maintained and it reads as the primary element of the site.

- Encourage the addition to be more clearly read as a new element, utilising materials and colours that are recessive against the heritage fabric.



Engineering Factory – 50 Rose St, Fitzroy (HO334 – South Fitzroy Precinct)

Positive features:

- The principal elevation of the heritage building is retained.
- Door and window openings in the original factory facade have been sensitively adapted with appropriate new industrial-style doors and windows to enable new uses.
- The new addition reads as a clearly separate element to the historic form.
- The top levels are treated differently from the main building to create a separate ‘capping’ element.
- Retention of remnant historic painted signage.
- The floor to ceiling heights of the factory component are maintained.

Negative features:

- The complete loss of roof form and inadequate setback of the new development behind the principal façade results in façadism.
- The addition is visually bulky and overwhelms the heritage fabric.
- The architectural language and structural module of the new addition bears little or no relationship to the heritage form.
- Poorly considered junction between new and heritage fabric at the western return of the factory building.

Policy considerations:

- Where new doors, windows and other features or services are required, they should subtly reference the industrial language of the place.
- The new built form (as visible from the street) should not exceed approximately the same volume of the historic form.
- Require adequate setbacks from principal elevations to ensure the three-dimensional form of the heritage building is maintained.
- Avoid cantilevering or building over the significant elements of the historic building including parapets and facades.
- Require new built form to adopt a subtly contrasting approach that respects the industrial character of the heritage place and distinguishes new built form from the historic fabric.



Part of the MacRobertson's Confectionary Complex – 420-428 Gore St, Fitzroy (HO334 – South Fitzroy Precinct)

Positive features:

- The volume and exterior walls of the original factory building are retained.
- Door and window openings in the original factory building have been sensitively adapted with appropriate new industrial-style doors and windows to enable new uses.
- The new addition reads as a clearly separate element to the historic form.
- The relatively plain wall treatment for the addition references the materials and solidity of the original form, but does not attempt to compete with the fine-grained texture of the historic brick fabric. The colouring is recessive against the heritage fabric.
- The new addition is setback from the parapets of the heritage building, allowing the heritage fabric to retain prominence in the streetscape.
- The heavily planted parapet softens the juxtaposition between the heritage façade and the new development, although it is noted that it is inconsistent with the industrial aesthetic of the former factory. The residential context probably supports this approach in this context.

Negative features:

- The visual bulk of the addition, particularly as viewed obliquely, could have been improved by breaking the up the form through increased articulation.
- Many existing openings have been modified and a number of new openings introduced.

Policy considerations:

- The new built form (as visible from the street) should not exceed approximately the same volume of the historic form.
- Encourage the massing of new additions to avoid large unarticulated lengths of wall.
- Maintain existing openings within historic facades and discourage the introduction of new openings in intact elevations.
- Where new doors, windows and other features or services are required, they should subtly reference the industrial language of the place.
- Encourage the use of 'industrial' rather than 'domestic' landscaping treatments, where appropriate – noting that the approach taken here helps soften the impact of the development within its residential context.

3.4 Specialist processing plants

These multi-faceted sites contain a series of industrial buildings or structures in which specialist manufacturing or processing occurred. They are often comprised of a range of separate buildings or structures of different forms (silos and towers as well as more typical factory buildings) on the one site. Such sites include breweries, distilleries, tanneries, gasworks, freezing works, rope works and flour mills. The nature of these industries often meant they were located outside residential areas or were near main roads and/or the Yarra River. They also commonly included tall multi-storey structures as part of their function e.g. silos, chimney stacks and brew towers etc.

Due to their complexity, and the often-unique nature of the manufacturing equipment and process involved it is recommended that a Conservation Management Plan be prepared prior to their redevelopment to ensure the important heritage components are retained and managed appropriately. Where this typology has been successfully redeveloped, the different elements of the site remain legible and a variety of new built form responses have been applied to the different historic components.



Byfas Ltd Factory – 8-16 Trenerry Crescent, Abbotsford (HO314 – Yarra Falls Precinct) *Note: land surrounding the art deco building is not within the HO

Positive features:

- The volume of the original factory building is maintained.
- Door and window openings in the original factory building are retained and the facades have been conserved.
- The chimney has been retained and conserved.
- Redevelopment of the site has occurred in discrete built form envelopes rather than a single large structure. This reflects the history and nature of the site as a complex of individual buildings, rather than a single form.
- New built form reads as a clearly separate element to the historic form.
- The new buildings within the complex are sensitive in terms of scale and massing to the heritage building.
- The fenestration pattern of the new roof top addition and new residential component fronting Turner Street references the patterning and solid to void proportions of the historic form.
- The roof top addition is subservient in scale to the heritage building and is visually lightweight.
- New elements such as the balconies continue the industrial aesthetic of the heritage building.

Negative features:

- The profile of the brick chimney has been lost against the sky by the construction of the apartment building.

Policy considerations:

- Redevelopment of large industrial complexes should be guided by a Conservation Management Plan.



- Development within larger complexes should be broken down into smaller building envelopes to reflect the historical arrangement of the site.
- Discrete heritage structures within the complex should be retained, conserved and reused where their reuse is appropriate.
- Remnant industrial features such as chimney, towers, silos and remnant machinery should be retained, incorporated into the redevelopment, and interpreted.
- Historically prominent features such as chimneys and silos should remain prominent within the redeveloped site and within the wider streetscape or precinct.
- New built form should respect and reflect the scale and architectural language of the heritage buildings and should reflect the industrial aesthetic of the place.



Malt House Complex – Abinger Street, Richmond (HO424)

Positive features:

- The volume of the original factory building and to a lesser degree the silos has been maintained.
- Door and window openings in the original factory building are maintained and the facades have been conserved.
- The roof form of the factory building has been maintained within simple roof lights added.
- The new additions read as clearly separate elements to the historic form.
- The new buildings within the complex and additions to existing buildings are sensitive in terms of scale and massing with the heritage building.
- The fenestration pattern of the new addition references the patterning and solid to void proportions of the historic form.
- The adaptive reuse of the silos is incorporated into an innovative contemporary design.
- New elements such as the balconies reference the industrial aesthetic of the complex through the use of materials such as off-form concrete and Corten™ steel.

Negative features:

- The nautical imagery of the 'Ark' apartments in the silos (ships bow and porthole-like windows) is incongruous both in an industrial and inland suburban context.

Policy considerations:

- See Byfas Ltd Factory discussed above.



Former Victoria (Aitken's) Distillery – Northumberland Street, Collingwood (HO116)

Positive features:

- The original distillery buildings and silos within the complex have been retained, conserved and reused.
- The original industrial complex remains legible and the site has been subtly interpreted through the naming/identification of the different buildings within the complex.
- Door and window openings in the original factory buildings are maintained.
- The roof form of the buildings has been maintained within simple roof lights added.
- The new buildings and additions read as clearly separate elements to the historic form, but strongly reflect the industrial aesthetic of the site.
- The new buildings within the complex and additions to existing buildings are sensitive in terms of scale, massing and materials.
- Remnant distillery plant and equipment has been incorporated into the new public spaces created.

Negative features:

- A good heritage outcome although some new windows have been insensitively located and detailed.

Policy considerations:

- See Ltd Factory discussed above
-



4. Existing and proposed policy guidance

Industrial heritage places are specifically addressed within the Kingston and Maribyrnong heritage policy of their respective planning schemes. The City of Melbourne has exhibited updated heritage policies as part of Planning Scheme Amendment C258, the Planning Panel Report into which has been released. While not specifically referencing industrial heritage, the updated policies provide useful guidance for this class of place. The Heritage Council of Victoria has also produced guidance on the adaptive reuse of industrial heritage places. These policies are discussed in detail below.

4.1 City of Kingston

Kingston's Heritage Policy at Clause 22.16 of the Kingston Planning Scheme addresses, albeit briefly, new building and additions to industrial and commercial heritage places. The Kingston policy provides guidance aimed at retaining contributory fabric and ensuring that new development protects key views of the heritage building and respects its scale through the sensitive massing of form.

The policy states:

New buildings and works and additions: industrial and commercial heritage places

- *To promote sensitively designed new development to industrial and commercial heritage places that:*
 - *Maintain and respect the unique character of significant industrial and commercial heritage places;*
 - *Preserve the existing roof line, chimney and contributory architectural features that are essential components to the architectural character of the heritage place;*
 - *Are articulated and massed to correspond with the prevailing building form;*
 - *Respect the scale of the existing heritage place by graduating building form of adjacent new development in a stepped effect away from the place to minimise the visual dominance of adjacent new works;*
 - *Do not obscure principal viewlines to existing significant signage;*
 - *Do not negatively impact upon the significance or architectural character of the place.*

4.2 City of Maribyrnong

Maribyrnong's Heritage Policy at Clause 22.01-16 of the Maribyrnong Planning Scheme includes specific guidance on industrial heritage. The guidance on the redevelopment of industrial places sets out a sound policy basis based on the municipality's industrial past. It also provides decision guidelines that recognise the reality of dealing with heritage places that may have issues that make the retention of heritage fabric difficult or impossible, such as high levels of contamination. The policy recognises the value of preparing a Conservation Management Plan for major industrial sites or complexes, which are common in the City of Maribyrnong. The interpretation of industrial sites is also encouraged as is the retention of redundant equipment where this can aid the understanding of the heritage place. However, the emphasis of the policy is on retaining places in an industrial usage, which is likely to be less relevant in the context of the City of Yarra, and does not apply to sites that have already been fully converted to non-industrial uses.

The policy states:

22.01-16 Industrial Heritage Policy

Where the policy applies

This policy applies to scheduled Heritage Overlay sites within an Industrial 1, Industrial 3, Business 3 or various other zones, which have been developed and are being used for industrial purposes or for a utility installation.

This policy does not apply to former industrial sites which have been fully converted to non-industrial uses.

Policy basis

A dominant theme in the development of Maribyrnong since the mid-nineteenth century has been the development of industry and some of Victoria's most significant industrial heritage is now found within the city. Meat and meat product processing, engineering, munitions and armaments, have been dominant industries in Maribyrnong over long periods of time.

The Maribyrnong Heritage Review 2001 has identified a wealth of industrial heritage in the area, which includes places that are no longer used for their original purpose and have been redeveloped such as the former Barnett Glass Rubber Factory, to complexes such as Sugar Australia which continue processes which originally commenced more than a century ago. Conservation policy and practice is needed to protect and promote industrial heritage in a context of rapid change occurring at industrial sites throughout the municipality.

The conservation of industrial heritage places presents specific management issues. It is often the use of the site for the same purpose over a long period that is of the primary historical significance. While fabric such as buildings or plant may contribute to the significance of industrial heritage places by illustrating the development of the place over time, the ongoing replacement and upgrading of this fabric is often an integral part of the operation of the use. 'Conservation by use' is an important heritage principle and on this basis, there may be circumstances where it may be appropriate to permit the removal or alteration of fabric if it will facilitate the historic use of a site and ensure future viability.

Objectives

The objectives of this policy are:

- *To conserve industrial heritage places as an integral part of the City's heritage fabric and in a manner which is supportive of ongoing industrial activity.*
- *To encourage development to be undertaken in accordance with the accepted conservation standards of the ICOMOS Burra Charter.*
- *To encourage conservation and other works including maintenance, restoration, reconstruction and adaptation.*
- *To ensure that industrial heritage places continue to be used and conserved, and that replacement and renewal occur where necessary for the continuation of an historic industrial use.*
- *To ensure that the conservation of industrial places is balanced against other policies and objectives which may have a bearing upon the ongoing use and development of land.*
- *To conserve infrastructure associated with industrial sites including railways, pipelines, roads, wharves and the like where this is practically achievable and will not unreasonably impact, physically or economically, upon the re-development of the site for a continued or new industrial use.*

Policy

It is policy that:

- *Preference will be given to applications for development that:

 - *Support the continued viability of the historic industrial use of the site.*
 - *Seek to achieve a balance between the achievement of conservation objectives and other relevant planning scheme policies, and objectives in relation to economic viability and occupational health and safety.*
 - *Where possible, conserve fabric considered to be of primary significance unless the fabric has been made redundant and must be replaced by new buildings, plant or equipment associated with an ongoing or new industrial use, or its retention is outweighed by other more important policy outcomes.*
 - *Retains redundant fabric of primary significance in-situ if there is no immediate need to remove or relocate it.**
- *A Conservation Management Plan (CMP) be prepared for major industrial sites prior to works being undertaken to inform the development application.*

- *An appropriate record be made of any significant fabric that is proposed to be removed or demolished. This may include a photographic survey and measured floor plans and elevations. All recordings must be of archival quality.*
- *When a use is discontinued and a site redeveloped, preference will be given to applications for new development that:*
 - *Retains as much significant fabric as possible;*
 - *Creatively interprets the history of the heritage place through the design or layout of new development including buildings, subdivision, landscaping, movement systems and public art; and*
 - *Incorporates interpretation of the heritage place, and the place within its context. This may include interpretative signage that outlines the history of the site and may include text, images or maps.*

Policy

It is policy that before deciding on an application, in addition to the decision guidelines elsewhere in this policy and the Scheme, the responsible authority should also consider when relevant:

- *Whether the proposed buildings, works or demolition will advance policies and objectives of the planning scheme in terms of the ongoing use and development of land for industrial or port-related purposes.*
- *Whether the proposed buildings, works or demolition will support the continuation of the historic industrial use carried out on the site.*
- *Whether the proposed buildings, works or demolition will support the viability of a new industrial use on the site.*
- *Whether demolition will, when balanced against other policies and objectives, achieve a net community benefit.*
- *Whether the demonstrated level of contamination prevents, or makes prohibitive, the repair, adaptation or re-use of a significant building or place.*
- *Whether there is an opportunity for redundant equipment to remain in-situ as historic evidence or for interpretation.*
- *Whether, when balanced against other planning policies and objectives, the historic use of the site is able to be properly recognised by interpretation.*
- *Whether any proposed interpretation will provide adequate information about the historic use and development of the site, including any significant links with other related industrial, commercial or residential places.*
- *Any framework or strategy relating to the land that is included as a reference or incorporated document in the Scheme.*

4.3 City of Melbourne

The Melbourne Planning Scheme Amendment C258 – Heritage Policies Review proposes, amongst other things, new heritage policies within and outside the Capital City Zone. Melbourne City Council’s expert witness, Ms Jordan, recommended that ‘Clause 22.04 Heritage Places within the Capital City Zone’ and ‘Clause 22.05 Heritage Places outside the Capital City Zone’ be combined into one policy, ‘Clause 22.05 Heritage Policy’. In contrast, the Panel Report (dated 15 May 2019) recommended that the separate policies be retained. The new policy (which still undergoing review) includes the following clauses that are of relevance to the matters considered in this review.

The clauses of particular relevance are bolded:

22.05-7 Demolition

It is policy that:

- *The demolition of a non-contributory place will generally be permitted.*
- ***Full demolition of significant or contributory buildings will not generally be permitted.***

- ***Partial demolition in the case of significant buildings, and of significant elements or the front or principal part of contributory buildings will not generally be permitted.***
- ***Retention of the three dimensional form is encouraged; facadism is discouraged.***
- ***The adaptive reuse of a heritage place is encouraged as an alternative to demolition.***
- ***The poor structural or aesthetic condition of a significant or contributory building will not be considered justification for permitting demolition.***
- *A demolition permit should not be granted until the proposed replacement building or works have been approved.*
- *The demolition of fences and outbuildings which contribute to the cultural significance of the heritage place is discouraged.*

Before deciding on an application for full or partial demolition, the responsible authority will consider, as appropriate:

- ***The assessed significance of the heritage place or building.***
- ***The character and appearance of the building or works and its contribution to the historic, social and architectural values, character and appearance of the heritage place, and the streetscape.***
- ***The significance of the fabric or part of the building, and the degree to which it contributes to the three-dimensional form of the building, regardless of whether it is visible.***
- ***Whether the demolition or removal of any part of the building contributes to the long-term conservation of the significant fabric of the building.***
- ***Whether the demolition is detrimental to the conservation of the heritage place***
- *Whether there are any exceptional circumstances.*

Where approval is granted for full demolition of a significant building, a recording program including, but not limited to, archival photographic recording and/or measured drawings may be required prior to demolition, to the satisfaction of the Responsible Authority.

22.05-8 Alterations

It is policy that:

- ***External fabric which contributes to the cultural significance of the heritage place, on any part of a significant building, and on any visible part of a contributory building, should be preserved.***
- *Alterations to non-contributory buildings and fabric are respectful of, and do not detract from the assessed cultural significance of the heritage precinct.*
- *Sandblasting of render, masonry or timber surfaces and painting of previously unpainted surfaces will not generally be permitted.*

Before deciding on an application to alter the fabric of a significant or contributory building, the responsible authority will consider, as appropriate:

- ***The assessed cultural significance of the building and heritage place.***
- ***The degree to which the works would detract from the significance, character and appearance of the building and heritage place.***
- ***Its structural condition.***
- ***The character and appearance of the proposed replacement materials.***
- ***Whether the works can be reversed without loss of fabric which contributes to significance.***

Removal of paint from originally unpainted masonry or other surfaces is encouraged providing this can be undertaken without damage to the heritage fabric.

The introduction of awnings and verandahs to ground floor façades and shopfronts may be permitted where:

- *The works reconstruct an original awning or verandah, based on evidence of the original form, detailing and materials; or*
- *The awning is an appropriate contextual design response, compatibly placed in relation to the building, and can be removed without loss of fabric which contributes to cultural significance.*

22.05-9 Additions

It is policy that additions to buildings in a heritage precinct are respectful of and in keeping with:

- ***Identified ‘key attributes’ of the heritage precinct.***
- ***Precinct characteristics including building height, massing and form; style and architectural expression; details; materials; front and side setbacks; and orientation.***
- ***Character and appearance of nearby significant and contributory buildings.***

Where abutting a lane, additions are to be respectful of the scale and form of heritage fabric to the lane.

Additions to significant or contributory buildings:

- ***are respectful of the building’s character and appearance, scale, materials, style and architectural expression.***
- ***do not visually dominate or visually disrupt the appreciation of the building as it presents to the streetscape(s).***
- ***maintain the prominence of the building by setting back the addition behind the front or principal part of the building, and from other visible parts and moderating height.***
- ***do not build over or extend into the air space directly above the front or principal part of the significant or contributory building.***
- ***retain significant roof form within the setback from the building façade together with any chimneys or similar roof elements of original fabric. Not obscure views of façades or elevations associated with the front or principal part of the building.***
- ***be distinguishable from the original fabric of the building.***

The design of additions is to:

- *Adopt high quality and respectful contextual design.*
- *Avoid direct reproduction of the form of historic fabric.*
- *Adopt an interpretive design approach to other details such as verandahs, fences, and shopfronts.*

22.05-10 New Buildings

It is policy that new buildings are respectful of and do not detract from the assessed cultural significance of the heritage place.

New buildings:

- ***Are to be in keeping with:***
 - ***‘Key attributes’ of the heritage precinct such as:***
 - ***Building height, massing and form; style and architectural expression; details; materials; front and side setbacks; and orientation and fencing.***
 - ***Prevailing streetscape height and scale.***
- ***Do not obscure views from the street(s) and public parks of the front or principal part of adjoining significant or contributory places or buildings.***
- ***Do not visually dominate or visually disrupt the appreciation of the heritage place by:***

- ***maintaining a façade height which is consistent with that of adjoining significant or contributory buildings, whichever is the lesser, and***
- ***setting back higher rear building components.***
- *Do not adopt a façade height which is significantly lower than prevailing heights in the streetscape.*
- *Are neither positioned forward of the façade of adjoining significant or contributory heritage places or buildings, or set back significantly behind the prevailing building line in the streetscape. For land within the CCZ, new buildings should be positioned in line with the prevailing building line in the streetscape.*
- ***Do not build over or extend into the air space directly above the front or principal part of an adjoining significant or contributory building or place.***
- *Where abutting a lane, are respectful of the scale and form of historic fabric of heritage places abutting the lane.*
- *Do not impact adversely on the aboriginal cultural heritage values, as indicated in an archaeologist's report, for any site known to contain aboriginal archaeological relics.*

The design of new buildings are to:

- ***Adopt high quality and respectful contextual design.***
- *Adopt an interpretive design approach to other details such as verandahs, fences and shopfronts.*

...

22.05-12 Subdivision

It is policy that subdivision of a heritage place:

- *Reflect the pattern of development in the streetscape or precinct, whichever is most relevant to the place.*
- *Ensure that appropriate settings and contexts for significant and contributory heritage buildings and places are maintained including the retention or any original garden areas, large trees and other features which contribute to the significance of the heritage place.*
- *Not provide for future development which will visually disrupt the setting and impact on the presentation of the significant or contributory building.*
- *Provide for three dimensional building envelopes for future built form to each lot proposed.*

Subdivision of airspace above heritage buildings, to provide for future development, is discouraged.

...

22.05-18 Signage

It is policy that new signage associated with heritage places meet the following standards:

- ***Minimise visual clutter.***
- ***Not conceal architectural features or details which contribute to the significance of the heritage place.***
- ***Not damage the fabric of the heritage place.***
- ***Be in keeping with historical signage in terms of size and proportion in relation to the heritage place.***
- ***Be readily removable.***
- *Address all relevant performance standards of Clause 22.07 – Advertising Signage*

Advertising signs may be placed in locations where they were traditionally placed.

The historical use of signage may be justification for new or replacement signage.

Existing signage that is deemed to have heritage value should be retained, and not altered or obscured, including historic painted signage.

This aim of the policy appears primarily to inform development behind the front or principal form of domestic dwellings (of which terraced houses are a common type within the City of Melbourne as they are in the City of Yarra). Its application to industrial buildings (beyond residential-scaled industry/workshops in residential areas) is potentially limited.

While not specifically addressing industrial places, many of the above policies could reasonably be applied to this class of place as they seek to retain important heritage fabric including significant roof forms and signage, and require that new development be setback from facades to ensure that new development does not dominate the heritage form. Of most relevance to current issues within the City of Yarra is the performance standard that seeks to retain the "...perception of the three-dimensional form and depth of the building" including from secondary elevations. Planning Scheme Amendment C258 also seeks to establish policy discouraging building over the front or principal part of a heritage place or its air space above.

4.4 Heritage Council of Victoria

In 2013 the Heritage Council Victoria produced *Adaptive Reuse of Industrial Heritage: Opportunities and Challenges* along with a number of case studies in Victoria and elsewhere in Australia. These examples demonstrate a range of appropriate design responses and development outcomes across different industrial place types. While the examples provided all involve adaption and reuse they do not involve the development of larger scale new built form. The accompanying issues paper sets out a range of issues and establishes a policy basis for the retention of heritage places but it does not provide examples of policy or decision guidelines for assessing proposals for the redevelopment of industrial places.

4.5 Industrial places within large precinct Heritage Overlays

In Yarra, industrial sites are often included within broad precinct-based Heritage Overlays and individual Statements of Significance haven't been prepared and tailored provisions (such as internal alteration controls) have not been applied. These large-scale precincts are commonly residential in character and it is these values, rather than their industrial heritage that are recognised within the Statement of Significance. Where these industrial buildings are located within a predominantly residential precinct, their scale, form and architectural expression is often contrary to the character of that context. In these cases, consideration should be given to introducing individual overlays within the residential precinct to recognise the heritage values of the industrial building or complex that may be inconsistent with those of the surrounding Heritage Overlay precinct or alternatively preparing a Statement of Significance that recognises the specific heritage values of the industrial site.

Larger and more complex industrial sites, such as mills, breweries and distilleries, would benefit from the preparation of Conservation Management Plans to inform future adaptive reuse and redevelopment prior to the development of proposals.

4.6 Application of policy for places graded 'Individually Significant' or 'Contributory'

The grading of individual buildings within heritage studies has been a practice applied by heritage consultants since the earliest heritage studies of the late 1970s and early 1980s. Over this 40-year period inconsistencies in the application of letter gradings (A, B, C and D or A1, A2, B etc.) or 'Individually Significant' / 'Contributory' have become apparent across a number of inner-urban municipalities including the City of Yarra. Planning Practice Note 1: *Applying the Heritage Overlay* (August 2018) (PPN1) provides the following guidance:

The thresholds to be applied in the assessment of significance shall be 'State Significance' and 'Local Significance'. 'Local Significance' includes those places that are important to a particular community or locality. Letter gradings (for example, 'A', 'B', 'C') should not be used.

PPN1 does not suggest any further hierarchy be applied within the threshold of 'local significance,' such as 'individually significant' or 'contributory'. The practice note identifies that those elements that contribute to the heritage significance of a precinct Heritage Overlay be graded 'contributory' within the Statement of Significance.

Melbourne Amendment C258 sought to translate the existing letter gradings applied within the Heritage Places Inventory (an Incorporated Document to the Melbourne Planning Scheme) into a 'significant' / 'contributory' / 'non-contributory' hierarchy. This approach closely aligns to that used in the City of Yarra's Appendix 8 (of the *City of Yarra Review of Heritage Overlay Areas 2007* (Graeme Butler & Associates)), which is a frequently-updated Incorporated Document that grades buildings 'individually significant', 'contributory' or 'not-contributory'.

The Panel considering Melbourne C258 concluded that 'significant' and 'contributory' gradings should not be used within that municipality's heritage inventory, but that all buildings that make a contribution to the values of the precinct should be graded 'contributory'. It recommended that "...where the property is located outside a precinct or where it is a place of heritage value situated amongst precinct properties, but it does not share the values of the precinct" it should be identified in the Heritage Places Inventory as an 'Individual Heritage Place'. This definition would apply to a number of industrial buildings that have heritage values but are located within precincts that have not been identified for their industrial characteristics.

The Panel, in considering the Clauses 22.04 and 22.05 of the Melbourne Planning Scheme, recommended removal of any differentiation between the application of heritage policy for 'significant' and 'contributory', which only occurred three times within each policy (i.e. 22.0X-6 'Demolition', 22.0X-7 'Alterations' and 22.0X-10 'Restoration and Reconstruction'). This Panel recommendation, if implemented, effectively removes the implicit hierarchy of importance that a 'significant' / 'contributory' grading system creates.

The Panel Report into Yarra Amendment C220 also recommended that levels of significance (and therefore grading) do not in themselves justify the use of different built form controls. In particular, the Panel stated:

The Panel does not agree that less significant sections [of Johnston Street] warrant a different treatment. Less significant areas equally deserve to exhibit the overall urban design outcome: a strong street wall with a distinct setback to the mid level form.

Both the Panel reports into Yarra C220 and Melbourne C258 concluded that different controls should not be applied based on the level of significance or grading of the heritage fabric, and therefore it is recommended that the same policy objectives and built form controls should apply for places subject to the Heritage Overlay irrespective of their grading in Appendix 8.

In the absence of any material difference between the controls or policies applied to places based on their grading there appears to be little value in retaining the differentiation in Appendix 8 between 'individually significant' and 'contributory' buildings within Heritage Overlay precincts.

5. Recommended Industrial Heritage Policy

Drawing on the above case studies and the analysis of existing policies, a number of additional heritage-related policies are recommended for inclusion within the Yarra Planning Scheme.

The introduction of specific provisions within the Yarra Planning Scheme to encourage the retention and appropriate redevelopment of industrial heritage sites should include new clauses within both the Local Planning Policy Framework at Clause 21.05 – ‘Built Form’ and Local Planning Policy at Clause 22.02 – ‘Development Guidelines For Sites Subject To The Heritage Overlay’. These will need to be translated into the new Planning Policy Framework format when the new heritage policy is introduced at Clause 15.03-1L.

Many of the policies recommended below will apply to all heritage places, while some are specific to industrial sites. Consideration will need to be given to the format of the policy to avoid unnecessary repetition, but to ensure that all relevant policies are applied to a consideration of applications for development of industrial heritage buildings and sites.

5.1 Local Planning Policy Framework

It is recommended that additional commentary be added to Clause 21.05-1 ‘Heritage’ to recognise the importance of the municipality’s industrial past. The suggested new text is identified below in **bold** typeface.

With over half of the municipality having heritage protection, heritage features, including buildings, subdivision patterns, open spaces, and streetscapes, underpin Yarra's valued character.

*Factors that contribute to the heritage character of particular neighbourhoods include the period of development and pattern of subdivision, predominant land uses, and the original socio-economic structure of the population. This has created unique neighbourhoods, retail activity centres and **industrial complexes** which are identified in Yarra’s Heritage Overlays. These factors must be considered when understanding a site’s significance. The cultural significance of heritage places must not be compromised by new development.*

*In conserving areas of heritage significance there is also a need to provide for adaptive reuse and change of buildings **especially those that are no longer in their original use such as former industrial buildings and sites.***

5.2 Local Planning Policy

It is recommended that a number of policies be added under Clause 22.02 – ‘Development Guidelines for Sites subject to the Heritage Overlay’ or its successor policy at Clause 15.01-3L.

The recommended policies are made within the context of the existing Clause 22.02. The recommended new policy is identified in bold. Where the new policy has been drawn directly from another policy this is noted in brackets. Where a sub-clause has not been identified it is considered appropriate in the context of managing industrial heritage places or is not relevant. The policy in relation to residential buildings has not been reviewed as part of this project and therefore no changes have been identified.

These recommendations will need to be translated into the new format required by VC148 which introduced Clause 15.03-1S and 15.03-1L.

5.2.1 Clause 22.02 -5.1 ‘Demolition – Full Demolition or Removal of a Building’

Add the following policy:

- a) **Where approval is granted for full demolition of a heritage place, a recording program including, but not limited to, archival photographic recording and/or measured drawings may be required prior to demolition, to the satisfaction of the Responsible Authority** (modified from Melbourne C258 – replaced ‘significant building’ with ‘heritage place’).

5.2.2 Clause 22.02 -5.1 'Demolition - Removal of Part of a Heritage Place or Contributory Elements'

- a) Encourage the removal of inappropriate alterations, additions and works that detract from the cultural significance of the place.
- b) **Encourage the retention and conservation of significant fabric, such as principal elevations, chimneys, visible roof forms and architectural features.**
- c) Generally discourage the demolition of part of an individually significant or contributory building or removal of contributory elements unless:
 - (i) That part of the heritage place has been changed beyond recognition of its original or subsequent contributory character(s).
 - (ii) For a contributory building:
 - o that part is not visible from the street frontage (other than a laneway), abutting park or public open space, and the main building form including roof form is maintained; or
 - o the removal of the part would not adversely affect the contribution of the building to the heritage place.
 - (iii) For individually significant building or works, it can be demonstrated that the removal of part of the building or works does not negatively affect the significance of the place.
- d) **Before deciding on an application for partial demolition or removal, the responsible authority will consider, as appropriate:**
 - (i) **The significance of the place and the degree to which the fabric or part of the building proposed to be removed contributes to the significance of the place.**
 - (ii) **The significance of the fabric or part of the building, and the degree to which it contributes to the perception of the three-dimensional form and depth of the building (Melbourne C258).**

5.2.3 Clause 22.02 -5.4 'Painting and Surface Treatments'

Add the following policy:

- a) **For additions and new built form, recessive colours and materials should be used and dramatically contrasting patterns should be avoided.**

5.2.4 Clause 22.02-5.6 'Subdivision'

- a) Support the subdivision of sites which do not detract from the heritage value of the place or contributory element.
- b) Where appropriate, use a building envelope plan to protect the heritage values of the place. The building envelope plans should:
 - (i) Reflect the original rhythm of the streetscape.
 - (ii) **Maintain an appropriate setting to the heritage place (Melbourne C258).**
 - (iii) **Not provide for future development which will visually disrupt the setting and impact on the presentation of the heritage place (Melbourne C258).**
 - (iv) **For former industrial complexes, reflect or interpret the historical arrangement of heritage buildings and processes within the complex.**

5.2.5 Clause 22.02-5.7.1 'New Development, Alterations or Additions - General'

- a) Encourage the design of new development and alterations and additions to a heritage place or a contributory element to a heritage place to:
 - (i) Respect or interpret the pattern, rhythm, orientation to the street, spatial characteristics, fenestration, roof form, materials and heritage character of the surrounding historic streetscape.
 - (ii) Be articulated and massed to correspond with the prevailing building form of the heritage place or contributory elements to the heritage place.
 - (iii) Be visually recessive and not dominate the heritage place.
 - (iv) Be distinguishable from the original historic fabric **and adopt a high quality and respectful contextual design response** (Melbourne C258).
 - (v) Not remove, cover, damage or change original historic fabric.
 - (vi) **Not build over or extend into the air space above the front or principal part of a heritage place** (Melbourne C258).
 - (vii) Not obscure views of principle façades.
 - (viii) **Not employ external column/structural supports through the front or principal part of the building** (Melbourne C258).
 - (ix) Consider the architectural integrity and context of the heritage place or contributory element.
 - (x) **Maintain the perception of the three-dimensional form and depth of the building by setting back the addition behind the front or principal part of the building, and from visible secondary elevation(s)** (Melbourne C258).
 - (xi) **Retain significant roof forms within the setback from the building façade particularly where this is visible from the public realm** (Melbourne C258).
 - (xii) **Maintain the inter-floor height of existing buildings and avoid new floor plates and walls cutting through window openings.**
 - (xiii) **For additions, interpret historic façade patterning, including fenestration patterns and proportions, the relationship between solid and void and the module of structural bays.**
 - (xiv) **Discourage the use of highly reflective glazing in both historic openings and new built form. Unarticulated curtain glazing should be avoided.**
- b) Encourage setbacks from the principal street frontage to be similar to those of adjoining contributory buildings; where there are differing adjoining setbacks, the greater setback will apply.
- c) Encourage similar façade heights to the adjoining **significant or** contributory elements in the street. Where there are differing façade heights, the design should adopt the lesser height.
- d) **Encourage visually lightweight additions and linking elements to transition between historic and new built form.**
- e) Minimise the visibility of new additions **in residential areas** by:
 - (i) Locating ground level additions and any higher elements towards the rear of the site.
 - (ii) Encouraging ground level additions to contributory buildings to be sited within the 'envelope' created by projected sight lines (see Figure 1)

- (iii) Encouraging upper level additions to heritage places to be sited within the 'envelope' created by projected sight lines (for Contributory buildings refer to Figure 2 and for Individually significant buildings refer to Figure 3).
- (iv) Encouraging additions to individually significant places to, as far as possible, be concealed by existing heritage fabric when viewed from the front street and to read as secondary elements when viewed from any other adjoining street.
- f) Discourage elements which detract from the heritage fabric or are not contemporary with the era of the building such as unroofed or open upper level decks or balconies, reflective glass, glass balustrades and pedestrian entrance canopies.

5.2.6 Clause 22.02-5.7.2 'New Development, Alterations or Additions – Specific Requirements: Industrial, Commercial and Retail Heritage Place or Contributory Elements'

- a) Encourage new upper level additions and works to:
 - ~~Respect the scale and form of the existing heritage place or contributory elements to the heritage place by being set back from the lower built form elements. Each higher element should be set further back from lower heritage built forms.~~
 - Incorporate treatments which make them less apparent.
- b) **Require the preparation of a Conservation Management Plan to guide the redevelopment of industrial complexes containing a number of heritage buildings and other elements** (adapted from Kingston 22.01).
- c) **Encourage the retention of features such as chimneys, silos and towers within industrial sites that serve as local landmarks and communicate the historic function of these heritage places. Ensure these features remain visually prominent in any redevelopment proposal.**
- d) **Encourage the retention of remnant historic signage where it can help interpret the previous uses and history of the place.**
- e) **Encourage the interpretation of significant industrial sites and complexes including the retention of redundant equipment where this can aid the understanding of the heritage place** (Kingston 22.01).
- f) **Roof forms that contribute to the significance of the building should be retained, particularly where these are visible from the public realm or incorporate features such as lanterns, skylights, vents or chimneys.**
- g) **Side elevations visible from the public realm should normally be retained. Where a side elevation has been substantially altered or makes a minor contribution to the significance of the building, the depth of one or more structural bays should normally be retained to ensure the three-dimensional form of the building is retained.**
- h) **Encourage new built form to adopt a subtly contrasting approach that respects the scale and industrial character of the place but is recessive against the heritage fabric.**
- i) **Encourage visually lightweight one to two-storey rooftop additions on medium-rise (three or more storey) industrial buildings where the additions are set back a minimum of one structural bay from the principal façade/s.**
- j) **Encourage new upper-level development behind one and two-storey industrial facades to be setback a minimum depth of two structural bays. New built form – as visible from the street – should not generally exceed the same volume of the historic form.**
- k) **Encourage taller new built form on large industrial site to be broken up into separate tower forms rather than a single mass.**

- l) Encourage upper-level development to be setback from secondary elevations to ensure the three-dimensional form of the heritage building is retained.**
- m) Avoid new openings or the widening of existing openings in highly intact facades.**
- n) Encourage fencing and landscape treatments that reflect the industrial character of the heritage place.**

APPENDIX A – EXTRACT FROM *NORTHERN SUBURBS FACTORY STUDY*

Northern Suburbs Factory Study (1992), Gary Vines and Matthew Churchward, Pages 133 - 136

Categories of building types

1. Residential scale industry

This type of factory is characterised by its domestic scale and appearance. In some cases it could be mistaken for a large but plain terrace house or parish hall. The examples are only one or two storeys with narrow frontages dictated by the size of the residential blocks and with window and door proportions similar to the surrounding houses. They can usually be distinguished by their almost complete coverage of the block and lack of out buildings. They generally date from the 1860s to 1890s and were intended for light manufacturing such as clothing and footwear. Examples in the study area include, Spicer's boot factory in Bell St. Lewis & Whitty's blacking factory in Charles St. the Phoenix Chemical Co. Napier St. and Ferguson's Mantle factory, Wood St. all in Fitzroy, Yates boot factory in Page St. Clifton Hill and Peatt's boot factory in Langridge St. Collingwood.

Many inner suburban factories which date from the mid to late nineteenth century are built in a form which mirrors the domestic architecture in which they are situated. Before the 1920s there was little concerted effort to segregate industrial uses from the commercial and residential districts except where the trade was so offensive as to cause an immediate public outcry. In fact, the reverse was often the case, where industry amidst housing was seen as desirable in an age when the workforce had to be within walking distance from their place of work. The style of building reflected the domestic architecture probably for two reasons. This style was in keeping with the environment and so reduced opposition to industry. It was also the predominant form of building, familiar to the carpenters, bricklayers and stonemasons who designed and erected the bulk of buildings in Melbourne, before the trade of architect became common.

2. Specialist processing plants

A number of industries which turned traditional craft skills into mass production required purpose built processing plants to take the large scale equipment and handle bulk raw materials. Breweries, distilleries, tanneries, gasworks, freezing works, rope works, flour mills, etc. were dictated in their form by the actual process. The plant and equipment of these works, such as malting floors, brewing vats, distillation tanks, tan pits, gas retorts, freezing chambers, rope walks, etc., comprised the major part of the factory, often with the building simply forming a skin around equipment suspended in a framework to which the walls were attached. Because of the level of capitalisation required to get some of these industries off the ground, the buildings were often elaborately finished. The Yorkshire Brewery is the most prominent example.

This type of building is often the most exciting in terms of industrial heritage, because even when the equipment has been replaced or removed, the building itself can clearly demonstrate how the process was carried out. Other good examples of this factory type in the study area are the Vauxhall and Victoria distilleries, Victoria brewery, Thompson's tannery in Rokeby St. Collingwood, Miller's and Sampson's ropeworks in Brunswick, the Brunswick gas works in Hope St. Dight's flour mill on the Yarra, Reilly's flour mills in Brunswick Street Fitzroy and Benalla, Hoffman's brickworks, Coop's shot tower, the Leeds Dyeworks and the Fitzroy Freezing Works, sadly under demolition as this study was being prepared. Within this group are a few examples of bluestone industrial architecture more often seen in the Footscray-Williamstown area. They include Nettleton's woolworks and the Vauxhall Distillery. Early bluestone industrial buildings utilised locally obtained stone to solve practical construction problems of strength and cost. Their early date and substantial

nature were the deciding factors in choice of the appropriate building material since brickmaking had not reached the standard required for such massive building works.

3. Workshops

A minor type of building both in its role in industry and the form of the building is the once ubiquitous small, single storey workshop, employed for a variety of manufacturing purposes including carriage building, blacksmiths, foundries, engineers, joineries, furniture makers, etc. These buildings are almost universally single storey with gable roofs and in the case of the blacksmiths, farriers, carriage builders, motor engineers, and other transport related works, they have a central vehicle entrance flanked by windows. Because these were often the result of a single tradesman or partner's efforts who may employ between one and a dozen hands, their size is remarkably consistent. Examples include Lawson & Paterson Gore St., Bull's coachbuilding works, Fitzroy St., The Fitzroy ironworks, Greeves St., Federal Truck and Trolley Co. in George St., Lancaster's Farrier in Napier St. and Abrahams' Jute works in Young St.

4. Classical commercial

A far more elaborate form of factory developed as part of the boom of the 1880s and reflected the architectural fashions of the period as well as the wealth and status of the companies which built them. Moderate sized firms such as R.J. Henderson, Henry Hooper & Co. The United Shoe Machinery Co. and Moran & Cato employed simple forms of neo-classical architecture for their facades. Even some smaller boot factories such as Spry's and Burston and Treleaven had elaborate classical detailing echoing the styles of the inner city terrace housing. Some larger firms such as Denton's Hats and the Foster Lager Brewing Co. applied very imposing classical facades to their very large works. The fashion was taken to extremes by some manufacturers, particularly the brewers, with the Yorkshire Brewery being the most elaborate example. The style for flamboyant factory facades appears to have faded by the early twentieth century.

5. Multi-storey factories

By the early twentieth century, the cost of land and shortage of space for factories close to the city sent many manufacturers upward in their quest for factory expansion. Coupled with these considerations, the improvements in building technology allowed higher buildings to be erected without excessive cost, while certain industries benefited from the speedy transfer of part processed items between operators and levels in a compact factory. The earliest multi-storey factories (ie. those with three or more floors) appeared in the footwear industry around 1910. Earlier examples of multi-storey factories are found in a few specific industries where the process demanded 3 or more storeys such as flour mills, distilleries and brew towers. A very few particularly large general manufacturers reached three storeys in the nineteenth century. The Phoenix Clothing Factory in King St. Melbourne and Denton's Hats in Abbotsford are rare examples of such factories.

By the 1930s multi-storey factories had become commonplace, particularly in textile and footwear industries. Shoe factories such as Trescowthick's Clifton Hill factory, Llewellyn's and William's boot factories changed the face of that industry in Collingwood. MacRobertson's built progressively higher as it expanded its Fitzroy confectionery works, culminating in the seven storey 'Old Gold' factory. Clothing factories such as Staley and Staley in Brunswick and Austral Hats in Abbotsford demonstrated the scale of manufacture being carried out by the 1930s, while Rawleigh's Brunswick factory demonstrates another use for the building form.

Initially built in brick, these works were some of the earliest to adopt new materials such as reinforced concrete for window and door lintels, and for reinforcing bands in the brickwork as well as asbestos cement sheeting, steel framing and total reinforced concrete construction.

6. The shed principal

The 'shed principle', was a term used in England, and promoted by William Fairbairn, which referred to a single storey factory illuminated by sawtooth roof lights facing north in the northern hemisphere, but turned around to the south in Australia to ensure indirect light flooded the entire workplace. The building type was first applied to the woollen mills and had become common in England by the 1870s. Its use in Australia was also connected with woollen mills in Geelong and Melbourne's western suburbs in the 1860s and 1870s and the top showroom floor of many woolstores.

The single storey sawtooth roof factories of the outer industrial areas can be seen as the antithesis of the multi-storey factories in the inner suburbs. The principle requirement of these new factories was a large, unobstructed, single level for production-line operations at a mass-production scale. Cheap land on the fringes of Melbourne allowed the factories to sprawl. The best examples are Lincoln Mills, and Hilton in Coburg. An unusual example of a very large sawtooth roof factory in the inner suburbs is Yarra Falls in Abbotsford, which was able to take advantage of a large, unoccupied site near the Yarra River in Abbotsford.

These factories have timber or steel frames and are clad in corrugated iron, the more substantial having brick walls to the street frontages and sometimes all external walls. The sawtooth roof factory had become the standard form of large industrial building by the 1940s and has only recently been displaced in the last decade, by steel clearspan and prefabricated, prestressed concrete construction.

7. 'Moderne' facades

Within the range of sawtooth factories built in the 1930s are those smaller buildings which gave the utilitarian sawtooth roof shed slightly greater prestige by adding a facade in the then fashionable 'Streamlined Moderne' style. This was executed in brick, often rendered and can be instantly distinguished by the horizontal lines set into the brick or stucco and the vertical motif of turrets or fins over the main office entrance and sometimes other entrances. Curved walls and curved sections of parapet are very common. The architecture is usually pedestrian using standard features rather than demonstrating any architectural excellence. Lygon Street Brunswick, seems to have a disproportionate number of such factories which usually started life as textile mills. These include Red Robin at numbers 162,236 & 240, G. Burgin at 260, and Castle Knitwear at 326. Latoof & Callil in Brunswick Road and the Union Knitting Mills in Coburg are other typical examples. The Streamlined Moderne style was also used by more proficient industrial architects, but this is covered in the next style.

8. Elaborate and distinctive

Several large factories were executed in a very self-consciously stylish manner to designs by prominent and fashionable architects in particularly avant-garde styles. The Avon Butter Factory in Nicholson St. has a Moorish or Spanish Mission character with unusual detail in coloured glazed bricks and wrought iron. The old Brunswick Market Henderson Boxes in Ballarat St. is the only other example of such an elaborate Spanish Mission factory. William Pitt reinterpreted the Edwardian decorative treatment for his designs for Foy & Gibson's factories in Oxford and Cambridge Streets, Collingwood.

As noted above, the Streamlined Moderne was favoured for factories of the 30s as it expressed the up-to-date and progressive attitude of the new firms which had been established following the depression. Birmacley margarine in Scotchmer St., Handley & Tilley in Abbotsford and Truemould Tyres in Clifton Hill are all fine examples of the style. The unique style of Walter Burley Griffin's Melbourne architectural office is expressed in the Joseph Lyddy building in Fitzroy St, Fitzroy. while the Art Deco which inspired Griffin can also be seen in the severe Byfas/Yarra Falls building in Trennery Crescent, Abbotsford. Another popular style of the later part of our study period is the International or Dutch Modernist seen in such large and imposing

factories as the Oakley & Parkes designed Spicer's Paper mills, A.R. Butler's additions to Lincoln Mills and the Tip Top bakery in Brunswick.

9. Edwardian and Inter-war

The distinctive style of the period from about 1910 to the late 30s warrants a classification of its own as this appears to have been a time of considerable expansion in manufacturing and the adoption of new construction techniques in a remarkably consistent group of factories. Brick pilastered facades often gable ended with stuccoed decoration to the sills, lintels and bands in the brickwork, large steel hopper sash windows, and the beginnings of the extensive use of concrete and steel for window and door lintels, characterise this style. The single storey gable ended factory of this type is ubiquitous, some examples being the Ideal Box Factory in Rokeby St., Davis pickle factory in Rupert St. and Trescowthick's Hoddle St. boot factory.

10. Functional severe

The 1930s produced a particularly plain and functional group of factories which are clearly expressive of their period. Basic materials of brick, concrete and steel are used without any embellishment, G.N. Raymond's last factory in Easy St. Collingwood. The Gordon Slipper Co., Anderson & Ritchie, and Romar Knitwear in Fitzroy are a few of the many small factories of the period to eschew decoration altogether, while the bulk and repetitive pattern of the British United Shoe Machinery Co. in Alexandra Parade demonstrates that this sort of treatment can still result in an impressive structure.

11. Uncharacteristic

A small number of factories do not fit easily in any of the above categories because they were built outside of any industrial tradition. Sometimes, as in the case of Bates Cocoa Mills which began life as a Common School, the building was erected for another purpose, and adapted to manufacture with minimal alteration. Others like the Lane Shirt Factory in Union St. Brunswick, were factory extensions to shopfront retail outlets, and so are more indicative of the trends in commercial building architecture.