

## 21.11 REFERENCE DOCUMENTS

Dd/mm/yyyy  
Proposed  
C191

### General

*Council Plan 2005-2009.*

*Inner Melbourne Action Plan (October 2005).*

*Yarra City Council Access and Inclusion Policy (November 2004)*

*City of Yarra Access and Inclusion strategy 2004-2009*

*Disability Action Plan 2001—2004*

### Land Use

*Yarra Residential Interface Study 2001 (City of Yarra, 2001)*

### Accommodation and housing

*Inner Regional Housing Statement (January 2006)*

*Retail, entertainment and the arts*

*Yarra City Council Arts and Cultural Plan, 2005-2009*

*Inner City Entertainment Precincts Taskforce “A Good Night for All”*

### Industry, office and commercial

*Yarra Economic Development Strategy 2001-2004*

*Yarra Industrial and Business Land Strategy Review (Hansen Partnerships & Charter, Keck, Cramer, September 2004).*

### Parks, gardens and public open space

*Yarra City Council Recreation Strategy Plan 2003/2008*

### Built Form

#### Heritage

*Swan Street Built Form Study Heritage Assessments & Analysis, October 2017 (GJM Heritage)*

*Yarra High Streets (Swan Street)- Statements of Significance: [Reference Document, October 2017](#) [December 2020](#) (GJM Heritage)*

*Heritage Citation: 112-124 Trenerry Crescent, Abbotsford. GJM Heritage, July 2016.*

*Heritage Citation: 20-60 Trenerry Crescent, Abbotsford. GJM Heritage, July 2016.*

*Heritage Gap Study: Review of Johnston Street East, Context Pty Ltd 2016.*

*Heritage Gap Study: Review of 17 Precincts Stage 2 Report, Context Pty Ltd 2014, revised 2016.*

*Heritage Review of Predefined Areas In Abbotsford & Collingwood Stage 2 Report, Context Pty Ltd 2015.*

*Heritage Gap Study: Review of Central Richmond, Stage 2 Final Report, Context Pty Ltd 2014.*

*City of Yarra Heritage Gaps Study – Smith Street South, Anthemion Consultancies 2014.*

*City of Yarra Heritage Gaps – 233-251 Victoria Street Abbotsford Anthemion Consultancies, 2012.*

*City of Yarra Heritage Gaps Stage Two, Graeme Butler and Associates 2009.*

- City of Yarra Heritage Gaps Stage One, Graeme Butler and Associates 2008.*
- City of Yarra Heritage Gaps Review One 2013 [Appendix A and B includes Statements of Significance] Incorporated Plan under the provisions of clause 43.01 Heritage Overlay - methodology report, Lovell Chen 2014.*
- City of Yarra Heritage Gaps Review Two 2013.*
- City of Yarra Heritage Gaps Study – 233-251 Victoria Street, Abbotsford, Anthemion Consultancies 2012.*
- World Heritage Environs Area Strategy Plan: Royal Exhibition Building and Carlton Gardens, Department of Planning and Community Development 2009.*
- City of Yarra Review of Heritage Overlay Areas [Appendix 7 includes Statements of Significance], Graeme Butler and Associates 2007 updated 2013.*
- Yarra Heritage Database 2007 including photos, Allom Lovell and Associates 1998.*
- Development Guidelines for Heritage Places (City of Yarra, 1999).*
- City of Yarra Heritage Review, Volumes 1-4, Allom Lovell and Associates 1998.*
- Protecting Archaeological Sites in Victoria, Heritage Victoria 1998.*
- The Burra Charter. Australian ICOMOS Charter for the Conservation of Places of Cultural Significance, as updated from time to time.*
- Fitzroy Urban Conservation Study Review, Allom Lovell and Associates 1992.*
- Collingwood Conservation Study, Andrew Ward and Associates 1989.*
- Richmond Conservation Study, J and T O'Connor and Coleman and Wright Architects 1985.*
- Carlton, North Carlton and Princes Hill Conservation Study, Nigel Lewis and Associates 1984.*
- City of Northcote Urban Conservation Study, Graeme Butler Architect 1982.*
- South Fitzroy Conservation Study, Jacob Lewis Vines Architects 1979.*
- North Fitzroy Conservation Study, Jacob Lewis Vines Architects 1978.*

### **Built form character**

- ~~*Swan Street Activity Centre Built Form Framework (Tract, September 2017)*~~
- Urban Design Guidelines for the Yarra River Corridor (City of Yarra, 1998), as amended April 2004*
- City of Yarra Built Form Review 2003*

### **Transport**

- Yarra Strategic Transport Statement City of Yarra 2006*
- Encouraging and increasing walking strategy, City of Yarra 2005*

### **Environmental Sustainability**

- The Yarra Environment Strategy: Our Sustainable Future (City of Yarra, November 2000).*
- Review of Policies and Controls for the Yarra River Corridor: Punt Road to Burke Road: Consultant Report (Planisphere and Jones & Whitehead, June 2005).*
- Middle Yarra Concept Plan (Dept. of Planning and Urban Growth, Dept. of Conservation and Environment, 1990)*
- Lower Yarra (Punt Road to Dights Falls) Concept Plan (Ministry for Planning and Environment, 1986)*
- Lower Darebin Creek Concept Plan (Darebin Creek Co-ordinating Committee, 1995)*
- Merri Creek Management Plan (Merri Creek Management Committee, 1997)*
- Merri Creek Concept Plan (Draft) (Merri Creek Management Committee, 1997)*

*Yarra River Corridor Strategy (City of Yarra, 1999)*

*Yarra Catchment Action Plan (YarraCare, 1996)*

*Port Phillip and Western Port Regional Catchment Strategy 2004 – 2009 (Port Phillip and Westernport Catchment Management Authority 2004)*

*Herring Island Enhancement Plan (Acer Wargon Chapman and EDAW AUST, 1995)*

*Environmental Guidelines for Major Construction Sites (Environment Protection Authority, 1996)*

*Yarra Bend Park Strategy Plan (Parks Victoria, 1998)*

*Yarra Bend Park Environmental Action Plan (Parks Victoria, April 2000)*

*Yarra Bend / Fairfield Area: Development Opportunities (Chris Dance Land Design and Fulcrum Town Planners, 1997)*

*City of Yarra Stormwater Management Plan (AWT, December 2000)*

### **Neighbourhood Plans**

*Smith / Wellington Streets Mixed Use Precinct Urban Design Framework, March 2005*

*Victoria Street Activity Precinct Urban Design Framework, July 2004;*

*Victoria Street East Precinct, Richmond, Urban Design Framework prepared for the City of Yarra 16 November 2005 (mgs in association with Jones and Whitehead Pty Ltd)*

### **Structure Plans and Local Area Plans**

*Johnston Street Local Area Plan, 2015*

## 21.12 LOCAL AREAS

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C191

This Clause focuses on the local area implementation of the objectives and strategies set out in the MSS. Each Section relates to a particular precinct within the municipality and should be read in conjunction with the rest of the Municipal Strategic Statement and not in isolation.

The sections are organised under the Local Area headings.

### 21.12-2 Swan Street Activity Centre

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Swan Street's diverse range of entertainment and retail activities, public transport options and close proximity to Melbourne's Central Business District and Sports and Entertainment Precinct have attracted residents, visitors and workers to the Activity Centre for well over a 100 years.

The character of the Activity Centre varies significantly along the length of Swan Street, moving from a cohesive and highly intact turn of the century 'High Street' with a prominent fine grain subdivision pattern, to a diverse and less historically intact retail and residential area towards the eastern end. The Activity Centre contains a number of landmark heritage commercial buildings, including the Dimmeys Clock Tower.

The preferred future character of each precinct reflects this diversity with moderate built form change planned for the highly intact heritage area west of Brighton Street and higher change at the eastern end of the Activity Centre where there is currently a more mixed built form character. This built form change will support the Activity Centre's role as a location for an extensive mix of retail, entertainment and commercial uses, and high quality apartment living.

The Framework Plan as shown in [Figure 4-Map 2](#) illustrates the boundary of the Swan Street Major Activity Centre and the precincts, which make up the centre:

- Precinct 1: Richmond Station
- Precinct 2: Swan Street Retail
- Precinct 3: Swan Street East
- Precinct 4: Burnley Station

#### Vision

Council's vision is to develop Swan Street Activity Centre into a vibrant and thriving mixed use centre that allows for well designed midrise commercial and residential development whilst preserving the prominence of the heritage streetscape and buildings and maintaining acceptable levels of amenity.

#### Local area implementation

Ensure that any proposed use or development within the Swan Street Activity Centre is generally consistent with the Swan Street Built Form Framework Plan as shown in [Figure 4-Map 2](#).

##### *Preferred Future Character*

- Ensure new development supports and contributes to the future preferred character of each precinct:

**Precinct 1** – is an exciting place anchored by the Richmond Train Station with shops, entertainment uses and alfresco dining areas that is a destination for visitors and the workers and students from the surrounding employment and education uses in Cremorne. The entry to the centre is more defined with high quality contemporary buildings that wrap the corner with Hoddle Street. Looking east the Precinct Hotel and other heritage buildings sit comfortably next to more contemporary buildings that reflect a similar scale at street level with new development above and behind a well defined street wall.

**Precinct 2** – is the major focus for retail and entertainment uses located within existing fine grain heritage shop fronts. The footpaths are the key public spaces providing opportunities for outdoor dining and public engagement. Activated linkages connect people to East Richmond Station and a potential new public park. Looking along Swan Street, the intact heritage buildings remain prominent defining a valued heritage streetscape where new development above and behind the street wall is visually recessive. The Dimmey’s Tower remains a clear focal point in the street standing out from surrounding buildings. Taller development along the railway line is visible and provides a greater sense of enclosure along side streets and laneways but is designed and spaced to contribute to a varied skyline that remains subservient to the heritage streetscape.

**Precinct 3** – is a true mixed use precinct providing for a range of retail, commercial and residential uses within new mid rise developments of varying heights, widths and character. The consistent front setback and continuation of the broad scale of the street wall of Precinct 2 provides a continuity with the balance of the centre and an improved pedestrian experience at street level. On the north side of Swan Street, the heritage buildings remain visually prominent in the streetscape and development is scaled and sited sensitively to the adjoining low scale residential area. On the south side of Swan Street high quality taller buildings gradually scale up to Burnley Street denoting the importance of the station. Breaks between these buildings maintain views to the sky from street level and create a varied skyline and backdrop to the shopping strip when viewed from surrounding residential and commercial areas. New and improved laneways connecting to Swan Street offer opportunities for interesting public spaces that complement improvements to the public realm along Swan Street.

**Precinct 4** – is anchored around Burnley Station and the Burnley Street Village. High quality new buildings adjoining Burnley Station with a range of retail, office, hospitality, residential and personal services activate new and improved pedestrian connections to the station. Along Burnley Street, the heritage buildings remain prominent with new infill development integrating as a cohesive part of the streetscape. Buildings behind Burnley Street and along the north side of Swan Street scale sensitively to the adjoining low scale residential area. Breaks between buildings at upper levels maintain views to the sky from street level and create a varied skyline when viewed from surrounding residential areas.

#### *Economic Development*

- Support sensitive reuse and adaptation of existing heritage buildings for a range of retail, entertainment and commercial uses.
- Support Precinct 2: Swan Street Retail as a compact, fine grain retail precinct.
- Support the area west of Church Street as a core entertainment precinct that contains a range of licensed premises that make a significant contribution to the Night Time Economy.
- Facilitate opportunities for office and residential uses throughout the centre, principally above the ground floor.
- Facilitate a range of uses that cater for the everyday needs of residents, visitors and workers.
- Maintain and increase land uses that support street level activation and passive surveillance of the public realm.

#### *Built Form and Heritage*

- Provide for midrise development (5 -12 storeys) that respects the heritage fabric and the adjoining low scale residential neighbourhoods.
- Ensure individually significant and contributory heritage buildings are retained to conserve the intactness of the original heritage streetscape.
- Maintain an intimate pedestrian scale at street level along Swan, Church, and Burnley Street.

- Ensure development respects a consistency and intactness of the heritage streetscapes and the unique architectural form and qualities of heritage buildings within the Activity Centre.
- Promote a gradual transition in height along Swan Street towards Burnley Train Station.
- Ensure tall buildings are well spaced and sited to avoid visual bulk and provide equitable access to an outlook and good daylight.
- Allow for taller buildings with highly articulated street walls in side streets and laneways on the southern side of Swan Street.
- Ensure development contributes to the evolution of Precinct 3: Swan Street East and Precinct 4: Burnley Station into vibrant mixed-use precincts with built form that enhances the streetscape character.
- Enhance and mark the entry to the centre from Hoddle Street.
- Protect key views lines on Swan Street to the Dimmey's Clock Tower.

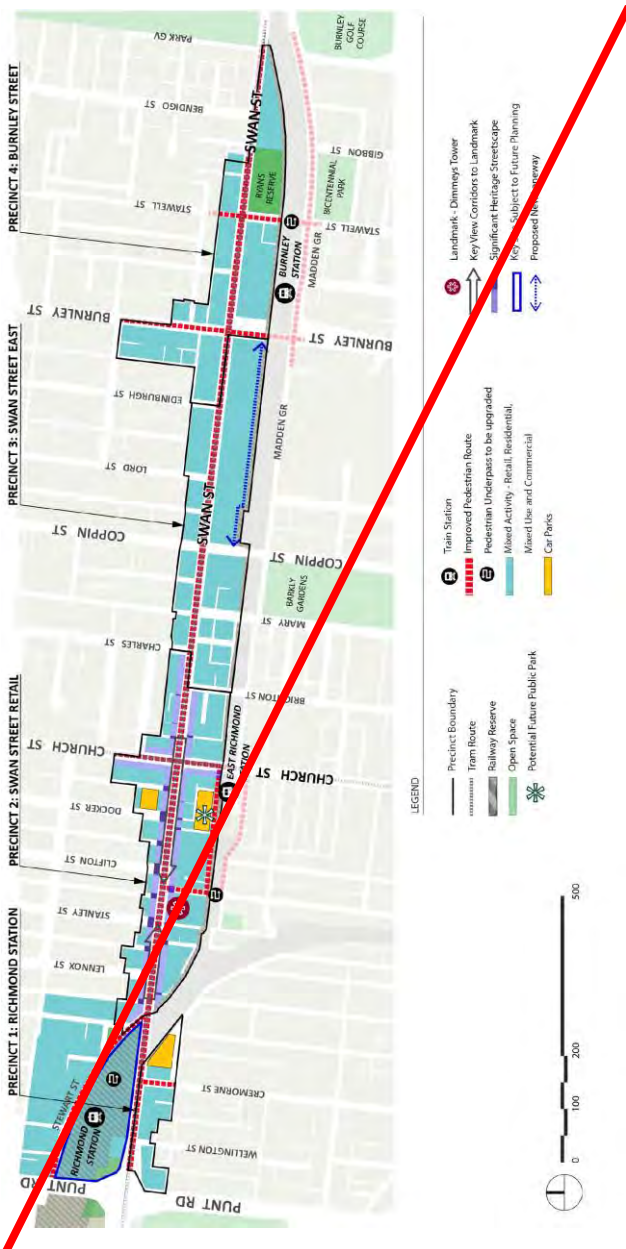
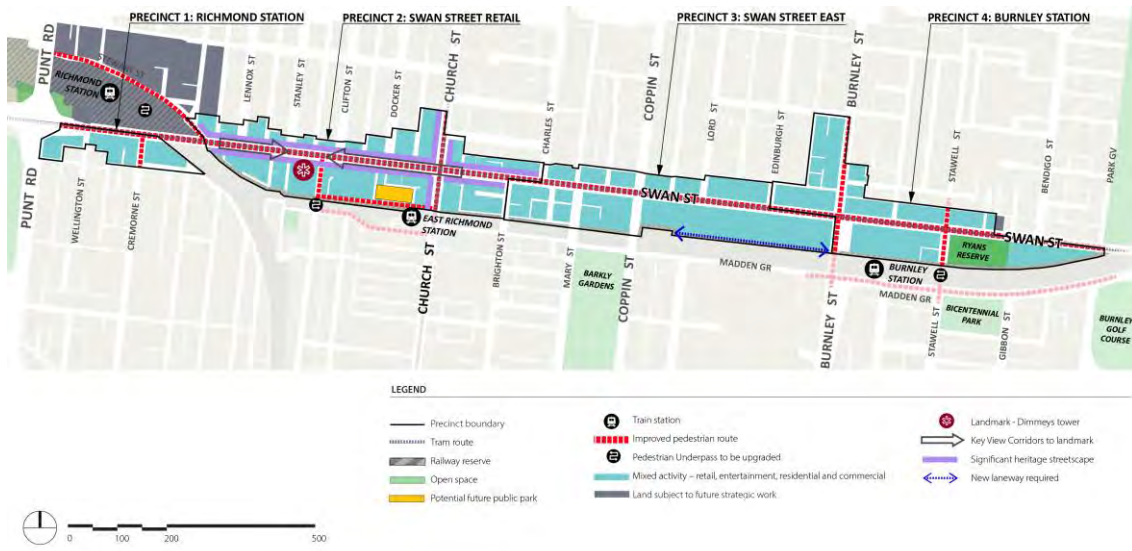
*Access and Movement*

- Facilitate widened laneways and new laneways to provide for safe vehicular access to and from new development.
- Ensure that pedestrian movement is given priority along Swan Street.
- Limit direct vehicular access on to Swan Street, Burnley Street and Church Street.
- Facilitate new footpaths and shared zones to provide safe pedestrian access to buildings.
- Encourage the provision of vehicular access from either the side or rear of buildings.

*Public Realm*

- Maintain daylight and sunlight to the key streets and public spaces.
- Encourage enhancement of the amenity and appearance of the public realm.
- Facilitate new and improved pedestrian connections to Swan Street and the railway stations.
- Safeguard the future amenity of the potential public park at the carpark adjoining East Richmond Train Station.
- Improve amenity and pedestrian safety under the railway bridge in Swan Street.
- Ensure that new development provides adequate weather protection for footpaths along Swan, Burnley, and Church Street.

**Map Figure 21: Swan Street Framework Plan**



## 22.02 DEVELOPMENT GUIDELINES FOR SITES SUBJECT TO THE HERITAGE OVERLAY

18/10/2018  
C232

This policy applies to all land within a Heritage Overlay.

### 22.02-1 Policy Basis

30/09/2010  
C85

The MSS highlights the importance of heritage to the identity and character of the municipality and one of its objectives is to protect and enhance the City's heritage places.

This policy provides guidance for the protection and enhancement of the City's identified places of cultural and natural heritage significance.

### 22.02-2 Definitions of Words used in this Policy

30/09/2010  
C85

- Adaptation: modifying a place to suit the existing use or a proposed use.
- Architectural integrity: the quality of closely reflecting the architecture of the period in which a building was created.
- Conservation: the process of looking after a place so as to retain its cultural significance.
- Cultural significance: aesthetic, historic, scientific, social or spiritual value for past, present or future generations.
- Fabric: all the physical material of the place including components and fixtures, and can include building interiors.
- Heritage place: anything subject to the Heritage Overlay and can include a site, area, land, landscape, tree, building or other work, or group of buildings of heritage significance, and may include components or spaces. When used in the context of a building graded individually significant, the heritage place is initially the individually significant building and then the broader heritage area. When used in the context of a contributory building, the heritage place is the broader heritage area.
- Maintenance: the continuous protective care of the fabric and setting of a place. It is distinguished from repair which involves restoration and reconstruction.
- Preservation: maintaining the fabric of a place in its existing state and retarding deterioration.
- Reconstruction: returning a place to a known earlier state and is distinguished from restoration by the introduction of new material into the fabric.
- Restoration: returning the existing fabric of a place to a known earlier state and is distinguished from reconstruction by no introduction of new material into the fabric (note a permit is only required for works, repairs and routine maintenance which change the appearance of a heritage place or which are not undertaken to the same details, specifications and materials).

### 22.02-3 Levels of Significance

19/10/2017  
C235

Every building of cultural significance has been assessed and graded according to its heritage contribution. The levels of significance used are:

- Individually significant: The place is a heritage place in its own right. Within a Heritage Overlay applying to an area each individually significant place is also Contributory.
- Contributory: The place is a contributory element within a larger heritage place. A contributory element could include a building, building groups and works, as well as building or landscape parts such as chimneys, verandahs, wall openings, rooflines and paving.



- Not contributory: The place is not individually significant and not contributory within the heritage place.

The level of significance of every building is identified in the incorporated document, *City of Yarra Review of Heritage Overlay Areas 2007 Appendix 8* (as updated from time to time). Details of methodology used to determine levels of significance can be found in *City of Yarra Review of Heritage Overlay Areas 2007 (Graeme Butler and Associates)*, *City of Yarra Heritage Gaps Review Two 2013*, *City of Yarra Heritage Gaps Study July, 2014 – Smith Street South (Anthemion)*, and *Heritage Gap Study: Review of Central Richmond, Stage 2 Final Report, November 2014*.

## **22.02-4 Objectives**

30/09/2010  
C85

To conserve Yarra’s natural and cultural heritage.

To conserve the historic fabric and maintain the integrity of places of cultural heritage significance.

To retain significant view lines to, and vistas of, heritage places.

To preserve the scale and pattern of streetscapes in heritage places.

To encourage the preservation, maintenance, restoration and where appropriate, reconstruction of heritage places.

To ensure the adaptation of heritage places is consistent with the principles of good conservation practice.

To ensure that additions and new works to a heritage place respect the significance of the place.

To encourage the retention of ‘individually significant’ and ‘contributory’ heritage places.

To protect archaeological sites of cultural heritage significance.

## **22.02-5 Policy**

19/10/2017  
C235

It is policy to:

### **22.02-5.1 Demolition**

19/10/2017  
C235

#### **Full Demolition or Removal of a Building**

Generally encourage the retention of a building in a heritage place, unless

- The building is identified as being not contributory.
- The building is identified as a contributory building, and
  - new evidence has become available to demonstrate that the building does not possess the level of heritage significance attributed to it in the incorporated document, *City of Yarra Review of Heritage Areas 2007 Appendix 8* (as updated from time to time) and
  - the building does not form part of a group of similar buildings.

*Note: The poor condition of a heritage place should not, in itself, be a reason for permitting demolition.*

Encourage the retention of original street furniture and bluestone road or laneway materials and details (where relevant).

An application for demolition is to be accompanied by an application for new development.

#### **Removal of Part of a Heritage Place or Contributory Elements**

Encourage the removal of inappropriate alterations, additions and works that detract from the cultural significance of the place.

Generally discourage the demolition of part of an individually significant or contributory building or removal of contributory elements unless:

- That part of the heritage place has been changed beyond recognition of its original or subsequent contributory character(s).
- For a contributory building:
  - that part is not visible from the street frontage (other than a laneway), abutting park or public open space, and the main building form including roof form is maintained; or
  - the removal of the part would not adversely affect the contribution of the building to the heritage place.
- For individually significant building or works, it can be demonstrated that the removal of part of the building or works does not negatively affect the significance of the place.

### **22.02-5.2 Original Location**

30/09/2010  
C85

Encourage the retention of a heritage place or a contributory element to a heritage place in its original location unless:

- The location is not an important component of the cultural significance of the heritage place.
- It can be shown that the relocation is the only reasonable means of ensuring the survival of the heritage place.

### **22.02-5.3 Reconstruction and Restoration**

30/09/2010  
C85

Encourage restoration of a heritage place or contributory element if evidence exists to support its accuracy.

Encourage the reconstruction of a building or works which previously existed in a heritage place if:

- The reconstruction will enhance the heritage significance of the heritage place
- Evidence exists to support the accuracy of the reconstruction.

Encourage the reconstruction of original or contributory elements where they have been removed. These elements include, but are not limited to, chimneys, fences, verandahs, roofs and roof elements, wall openings and fitting (including windows and doors), shopfronts and other architectural details and features.

### **22.02-5.4 Painting and Surface Treatments**

30/09/2010  
C85

Encourage the removal of paint from originally unpainted masonry surfaces.

Encourage the retention of historic painted signs.

Discourage the sand blasting of render, masonry or timber surfaces; and the painting of unpainted surfaces.

Encourage paint colours to be consistent with the period of the heritage place.

### **22.02-5.5 Culturally Significant Trees**

30/09/2010  
C85

Encourage the retention of culturally significant trees in a heritage place unless:

- The trees are to be removed as part of a maintenance program to manage loss of trees due to deterioration caused by old age or disease.
- The trees are causing structural damage to an existing structure and remedial measures (such as root barriers and pruning) cannot be implemented.

Ensure additions and new works respect culturally significant trees (and where possible, significant garden layouts) by siting proposed new development at a distance that ensures the ongoing health of the tree.

**22.02-5.6 Subdivision**30/09/2010  
C85

Support the subdivision of sites which do not detract from the heritage value of the place or contributory element.

Where appropriate, use a building envelope plan to protect the heritage values of the place. The building envelope plans should:

- Reflect the original rhythm of the streetscape.
- Allow sufficient space surrounding the heritage place or contributory element to a heritage place to retain its significance or contribution.

**22.02-5.7 New Development, Alterations or Additions**30/09/2010  
C85**22.02-5.7.1 General**30/09/2010  
C85

Encourage the design of new development and alterations and additions to a heritage place or a contributory element to a heritage place to:

- Respect the pattern, rhythm, orientation to the street, spatial characteristics, fenestration, roof form, materials and heritage character of the surrounding historic streetscape.
- Be articulated and massed to correspond with the prevailing building form of the heritage place or contributory elements to the heritage place.
- Be visually recessive and not dominate the heritage place.
- Be distinguishable from the original historic fabric.
- Not remove, cover, damage or change original historic fabric.
- Not obscure views of principle façades.
- Consider the architectural integrity and context of the heritage place or contributory element.

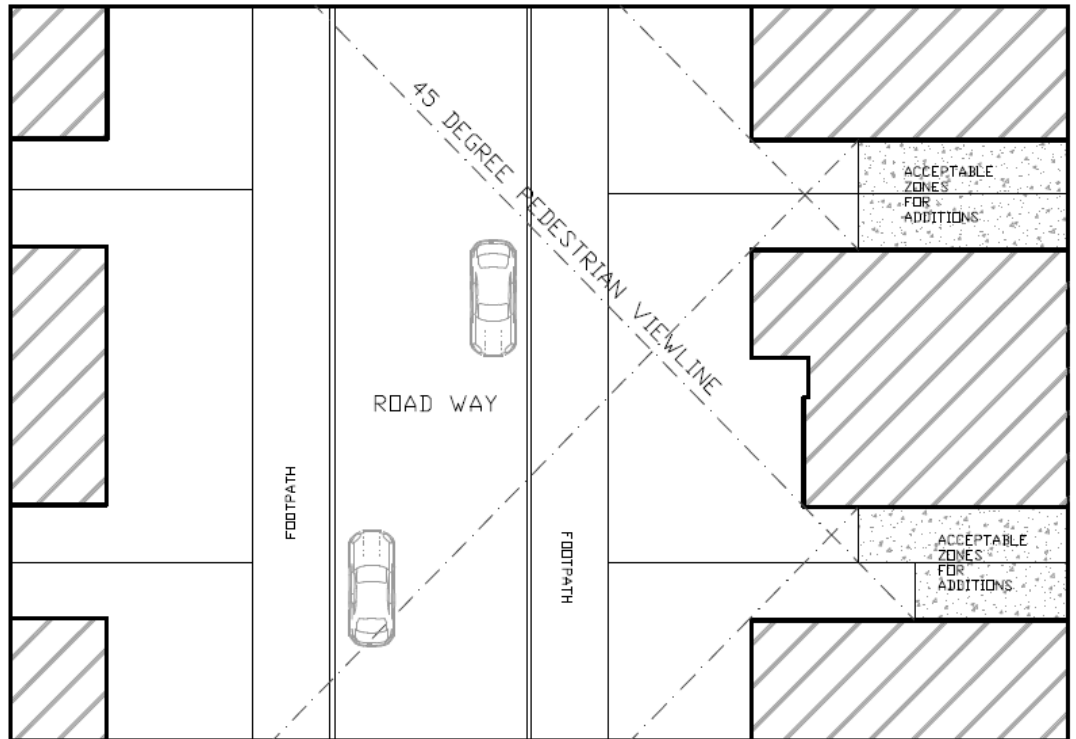
Encourage setbacks from the principal street frontage to be similar to those of adjoining contributory buildings; where there are differing adjoining setbacks, the greater setback will apply.

Encourage similar façade heights to the adjoining contributory elements in the street. Where there are differing façade heights, the design should adopt the lesser height.

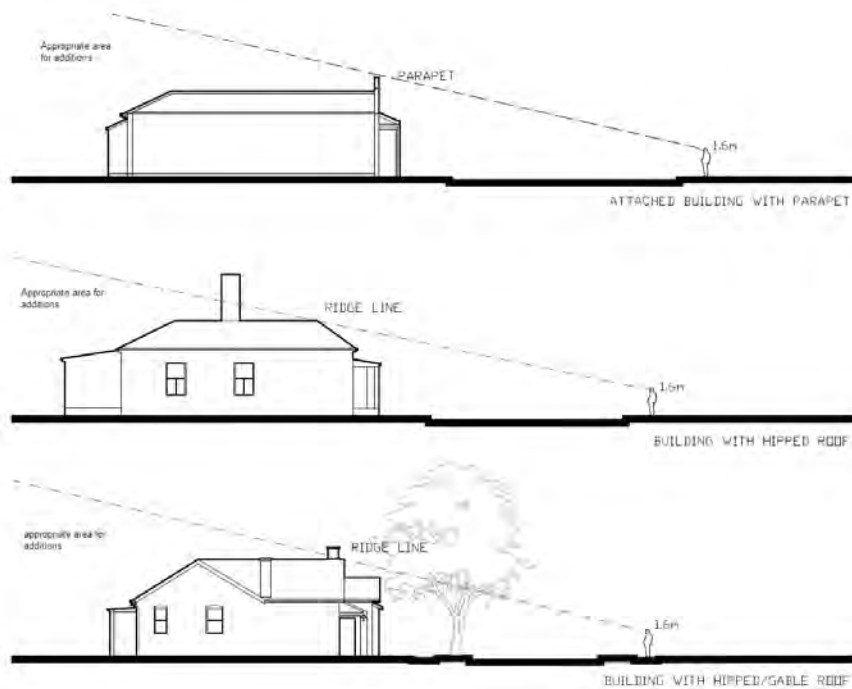
Minimise the visibility of new additions by:

- Locating ground level additions and any higher elements towards the rear of the site.
- Encouraging ground level additions to contributory buildings to be sited within the 'envelope' created by projected sight lines (see Figure 1)
- Encouraging upper level additions to heritage places to be sited within the 'envelope' created by projected sight lines (for Contributory buildings refer to Figure 2 and for Individually significant buildings refer to Figure 3).
- Encouraging additions to individually significant places to, as far as possible, be concealed by existing heritage fabric when viewed from the front street and to read as secondary elements when viewed from any other adjoining street.

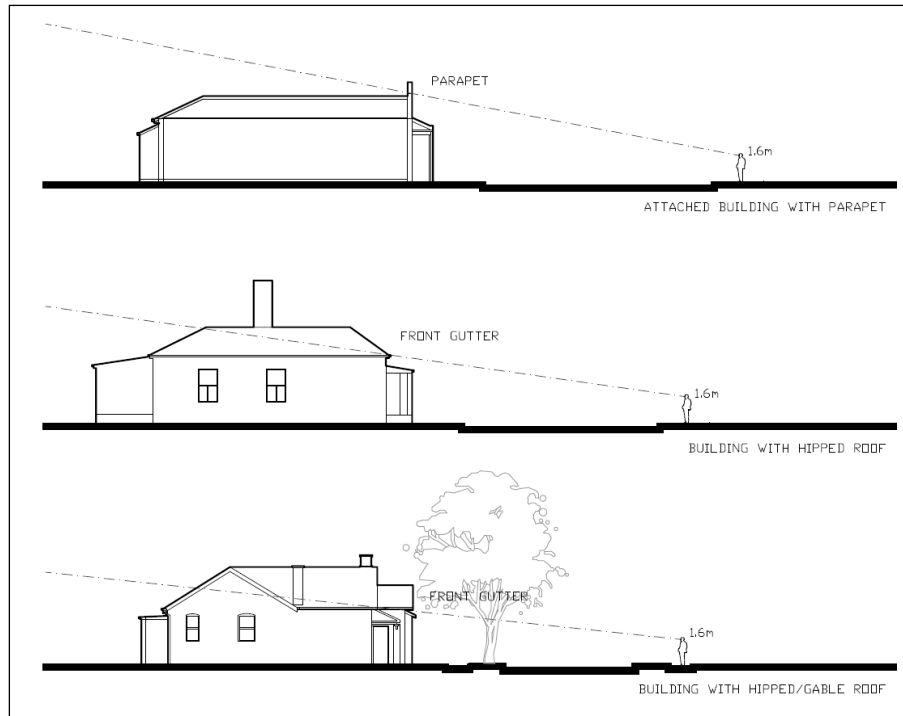
Discourage elements which detract from the heritage fabric or are not contemporary with the era of the building such as unroofed or open upper level decks or balconies, reflective glass, glass balustrades and pedestrian entrance canopies.



**Figure 1** acceptable areas for ground level additions are sited within the area created by drawing a 45 degree view line from the opposite footpath through the front corner of the subject building and the corners of adjacent buildings.



**Figure 2** – appropriate areas for upper level additions to contributory buildings are sited within the ‘envelope’ created by projecting a sight line from 1.6 metres above ground level (eye level of average adult person) from the footpath on the opposite side of the street through the top of the front parapet or the ridge line of the principal roof form.



**Figure 3** – appropriate areas for upper level additions to individually significant buildings are sited within the ‘envelope’ created by projecting a sight line from 1.6 metres above ground level (eye level of average adult person) from the footpath on the opposite side of the street through the top of the front parapet or the gutter line of the principal roof form.

**22.02-5.7.2 Specific Requirements (where there is a conflict or inconsistency between the general and specific requirements, the specific requirements prevail)**

30/09/2010  
C85

**Corner Sites and Sites with Dual Frontages**

Encourage new building and additions on a site with frontages to two streets, being either a corner site or a site with dual street frontages, to respect the built form and character of the heritage place and adjoining or adjacent contributory elements to the heritage place.

Encourage new buildings on corner sites to reflect the setbacks of buildings that occupy other corners of the intersection.

**Residential Upper Storey Additions**

Encourage new upper storey additions to residential heritage places or contributory elements to heritage places to:

- Preserve the existing roof line, chimney(s) and contributory architectural features that are essential components of the architectural character of the heritage place or contributory elements to the heritage place.
- Respect the scale and form of the heritage place or contributory elements in the heritage place by stepping down in height and setting back from the lower built forms.

Sightlines should be provided to indicate the ‘envelope’ from the street of proposed upper storey additions (refer to the sightline diagrams in 22.02-5.7.1).

**Industrial, Commercial and Retail Heritage Place or Contributory Elements**

Encourage new upper level additions and works to:

- Respect the scale and form of the existing heritage place or contributory elements to the heritage place by being set back from the lower built form

elements. Each higher element should be set further back from lower heritage built forms.

- Incorporate treatments which make them less apparent.

### **Carports, Car Spaces, Garages, and Outbuildings**

Encourage carports, car spaces, garages and outbuildings to be set back behind the front building line (excluding verandahs, porches, bay windows or similar projecting features) of the heritage place or contributory element or to be reasonably obscured. New works should be sited within the 'envelope' shown in Figure 1 of 22.02-5.7.1.

Discourage:

- new vehicle crossovers in streets with few or no crossovers
- high fencing, doors and boundary treatments associated with car parking that are unrelated to the historic character of the area
- new vehicle crossovers in excess of 3 metres wide in residential streets.

### **Front Fences and Gates**

Encourage front fences and gates to be designed to

- allow views to heritage places or contributory elements from surrounding streets
- be a maximum of 1.2 metres high if solid or 1.5 metres high if more than 50% transparent (excluding fence posts)
- be consistent with the architectural period of the heritage place or contributory element to the heritage place.

### **Ancillaries and Services**

Encourage ancillaries or services such as satellite dishes, shade canopies and sails, access ladders, air conditioning plants, wall and roof top mounted lighting, roof top gardens and their associated planting, water meters, and as far as practical aerials, to contributory or significant buildings, to be concealed when viewed from street frontage.

Where there is no reasonable alternative location, ancillaries and services which will reduce green house gas emissions or reduce water consumption, such as solar panels or water storage tanks, or provide universal access (such as wheel chair ramps), may be visible but should be sensitively designed.

Encourage ancillaries or services in new development to be concealed or incorporated into the design of the building.

Encourage ancillaries or services to be installed in a manner whereby they can be removed without damaging heritage fabric.

#### **22.02-6 Archaeological Sites**

30/09/2010  
C85

Encourage applicants to consult with Heritage Victoria where any proposed buildings or works may affect archaeological relics to facilitate compliance with Part 6 of the *Heritage Act 1995* (Protection of Archaeological Places).

#### **22.02-7 Decision Guidelines**

30/09/2010  
C85

Before deciding on an application the responsible authority will consider:

- Whether there should be an archival recording of the original building or fabric on the site.
- The heritage significance of the place or element as cited in the relevant Statement of Significance or Building Citation.

**22.02-8****References**18/10/2018  
C232

- Swan Street Built Form Study Heritage Assessments & Analysis, ~~GJM Heritage~~, October 2017 (~~GJM Heritage~~).
- Yarra High Streets (~~Swan Street~~): Statements of Significance: Reference Document, December 2020 (GJM Heritage); ~~GJM Heritage, October 2017~~.
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- Collingwood Conservation Study, Andrew Ward and Associates 1989.
- Richmond Conservation Study, J and T O'Connor and Coleman and Wright Architects 1985.
- Carlton, North Carlton and Princes Hill Conservation Study, Nigel Lewis and Associates 1984.
- City of Northcote Urban Conservation Study, Graeme Butler Architect 1982.
- South Fitzroy Conservation Study, Jacob Lewis Vines Architects 1979.
- North Fitzroy Conservation Study, Jacob Lewis Vines Architects 1978.

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## ~~SCHEDULE 17 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY~~

Shown on the planning scheme map as ~~DDO17~~.

### ~~SWAN STREET ACTIVITY CENTRE~~

#### ~~1.0 General design objectives~~

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- ~~■ To recognise and respond to the distinct character and varying development opportunities defined by the four precincts along Swan Street.~~
- ~~■ To support a new mid-rise scale built form character with lower built form at the interfaces with streets and the adjoining low-rise residential areas that maintains an active, high-quality and pedestrian-friendly environment.~~
- ~~■ To ensure development maintains the prominence of the heritage street wall and respects the architectural form and qualities of heritage buildings and the heritage streetscapes.~~
- ~~■ To minimise the amenity impacts on residential properties adjoining the Swan Street Activity Centre including overlooking, overshadowing and visual bulk impacts.~~
- ~~■ To ensure that vehicular access to development does not adversely impact the level of service, efficiency, and safety of the arterial and tram network.~~

#### ~~2.0 Buildings and works~~

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##### ~~2.1 Definitions~~

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~~**Street wall** is the facade of a building. Street wall height is measured at the vertical distance between the footpath at the centre of the frontage and the highest point of the building at the street edge, with the exception of architectural features and building services.~~

~~**Building height** does not include non-structural elements and service equipment including plant rooms, lift overruns, structures associated with green roof areas and other such equipment provided that all of the following criteria are met:~~

- ~~■ The total roof area occupied by the equipment (other than solar panels) is minimised;~~
- ~~■ The service equipment is located in a position on the roof so as to minimise its visibility;~~
- ~~■ The non-structural elements and service equipment minimise additional overshadowing of neighbouring properties and public spaces;~~
- ~~■ The equipment does not extend higher than 3.6 metres above the maximum building height; and~~
- ~~■ The non-structural elements and service equipment are integrated into the design of the building to the satisfaction of the responsible authority.~~

~~**Setback** is the shortest horizontal distance from a building façade, including projections such as balconies, building services and architectural features, to the boundary.~~

~~**Parapet height** does not include features such as brackets, pediments, urns, finials or other decorative elements.~~

~~**Road boundary** is the boundary between the public road and the private property.~~

~~**Shared zone** is a road or network of roads where pedestrians, cyclists, and vehicles share the roadway.~~



~~Upper level is development above the height of the street wall.~~

## ~~2.2 General design requirements~~

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~~The following general design requirements apply to an application to construct a building or construct or carry out works and must be read in conjunction with the relevant precinct design requirements.~~

### ~~Building heights~~

~~A permit cannot be granted to construct a building or construct or carry out works which exceeds the mandatory maximum building height shown in the Height and Interface Plans (Plan 1, 3, 5 and 7) of this schedule.~~

~~A permit may be granted to construct a building or construct or carry out works which exceeds the preferred building height shown in the Height and Interface Plans (Plan 1, 3, 5 and 7) of this schedule where all the following requirements are met to the satisfaction of the responsible authority:~~

- ~~▪ the built form outcome as a result of the proposed variation satisfies the general design objectives in Clause 1.0 of this schedule, the relevant precinct design requirements specified in this schedule;~~
- ~~▪ the proposed building height achieves the preferred future mid-rise character for Swan Street of generally 5 to 12 storeys; and~~
- ~~▪ the proposal will achieve each of the following:~~
  - ~~• greater building separation than the minimum requirement in this schedule~~
  - ~~• housing for diverse households types, including people with disability, older persons, and families, through the inclusion of varying dwelling sizes and configurations~~
  - ~~• universal access, and communal and/or private open space provision that exceeds the minimum standards in Clauses 55.07 and 58~~
  - ~~• excellence for environmentally sustainable design measured as a minimum BESS project score of 70%~~
  - ~~• no additional amenity impacts to residentially zoned properties, beyond that which would be generated by a proposal that complies with the preferred building height.~~

### ~~Street wall and setbacks~~

~~A permit cannot be granted to construct a building or construct or carry out works which exceed the mandatory maximum street wall height and/or reduces the mandatory minimum setback requirements specified in the Precinct Tables in this schedule.~~

~~A permit cannot be granted which exceeds the relevant preferred maximum street wall height and/or reduces the relevant preferred mandatory minimum setback requirements specified in this schedule unless the following are met, to the satisfaction of the responsible authority:~~

- ~~▪ The built form outcome as a result of the proposed variation satisfies the general design objectives in Clause 1.0 of this schedule;~~
- ~~▪ The built form outcome as a result of the proposed variation satisfies the relevant requirements specified in this schedule~~
- ~~▪ The following requirements also apply to the design of the street wall and upper levels:~~
  - ~~• Frontages at ground floor and within the street wall must be designed with floor to floor ceiling heights suitable to accommodate commercial activity.~~

- Frontages at ground floor must incorporate verandahs, consistent with the form and scale of adjoining verandahs, into the façade design.
- Development must be designed to adopt the same street setback from all interfaces for a minimum of 65% of the height of upper levels to avoid repetitive stepped form.
- Upper level development must be designed to ensure buildings are expressed in the round and provide detail on facades when viewed from all directions.
- Where development shares a common boundary and no interface treatment is shown in Plan 1, upper level development must:
  - be setback a minimum of 4.5m from the common boundary, where a habitable window or balcony is proposed
  - be setback a minimum of 3.0m from the common boundary where a commercial or non-habitable window is proposed.
- Where the common boundary is a laneway, the setback is measured from the centre of the laneway.

**Heritage design requirements**

The following design requirements apply on land within a heritage overlay or immediately adjoining a heritage overlay.

Design Element	Design Requirement
<p>Building facades and street frontages</p>	<p><b>Infill Buildings and Development Adjoining a Heritage Building</b>                      Façade treatments and the articulation of infill buildings on land affected by a heritage overlay and of new buildings on land immediately adjoining a heritage building must:</p> <ul style="list-style-type: none"> <li>▪ ensure the façade treatments and the articulation of new development are simple and do not compete with the more elaborate detailing of the adjoining heritage building(s)</li> <li>▪ respect the vertical proportions of the nineteenth and early twentieth century facades of the heritage streetscape and/or adjoining heritage building(s)</li> <li>▪ avoid large expanses of glazing with a horizontal emphasis except to ground floor shopfronts</li> <li>▪ maintain the existing canopy/verandah height of the heritage streetscape and/or adjoining heritage building.</li> </ul> <p><b>Contributory or Individually Significant Buildings</b>                      Adaptation of contributory or individually significant buildings must:</p> <ul style="list-style-type: none"> <li>▪ avoid highly reflective glazing in historic openings</li> <li>▪ encourage the retention of solid built form behind retained facades and avoid balconies behind existing openings</li> <li>▪ maintain the inter floor height of the existing building and avoid new floor plates and walls cutting through historic openings.</li> </ul>
<p>Upper Levels (above street wall height)</p>	<p><b>Upper level development on land within a heritage overlay and on land immediately adjoining a heritage building must:</b></p> <ul style="list-style-type: none"> <li>▪ be visually recessive and not visually dominate the heritage building and the heritage streetscape</li> <li>▪ retain the primacy of the three dimensional form of the heritage building as viewed from the public realm to avoid ‘facadism’</li> <li>▪ utilise visually lightweight materials and finishes that are recessive in texture and colour and provide a juxtaposition with the heavier masonry of the heritage facades</li> </ul>

Design Element	Design Requirement
	<ul style="list-style-type: none"> <li>▪ incorporate simple architectural detailing that does not detract from significant elements of the heritage building and the heritage streetscape</li> <li>▪ be articulated to reflect the fine grained character of the streetscape.</li> </ul>

### Overshadowing

A permit cannot be granted to construct a building or construct or carry out works that would overshadow any of the following spaces between 10 am and 2 pm at 22nd September:

- any part of the southern footpath of Swan Street, measured as 4.0m from the southern road boundary of Swan Street,
- any part of the opposite footpath of Church Street, measured as 4.0 metres from the road boundary of Church Street
- any part of the opposite footpath of Burnley Street, measured as 4.0 metres from the road boundary of Burnley Street

A permit cannot be granted to construct a building or construct or carry out works which are not in accordance with the overshadowing requirements specified in Clause 2.3 of this schedule unless the resultant overshadowing would not unreasonably prejudice the amenity of the public space, to the satisfaction of the responsible authority.

### Vehicle and pedestrian access

Development must provide vehicular access from rear lanes or from side streets in the preferred locations in the Access and Movement Plans (Plan 2, 4, 6 and 8) of this schedule except in locations identified as “Left in – Left Out Access Permitted” in the Access and Movement Plans (Plan 2, 4, 6 and 8) of this schedule.

Development identified as “Left in – Left Out Access Permitted” in the Access and Movement Plan must limit the width of vehicle crossovers and incorporate ‘Left in’ and ‘Left out’ only vehicle access.

Development with redundant vehicle access points to Swan Street, Church Street and Burnley Street must reinstate the kerb, linemark parking bays, and relocate any parking signs.

Vehicle ingress and egress into development, including loading facilities and building servicing, must be designed to ensure a high quality pedestrian amenity and limit potential conflict between vehicle movements and pedestrian activity.

Pedestrian access to buildings, including upper level apartments, must be from a street or a shared zone shown on the Access and Movement Plans (Plan 2, 4, 6 and 8) of this schedule. Where pedestrian access can only be provided from a laneway at the rear of buildings the pedestrian entrance must be setback from the rear laneway and well lit to enable safe access.

## 2.3 Precinct design requirements

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The following specific Precinct Design Requirements apply in addition to the general design requirements.

### Precinct 1 – Richmond Station

Shown on the planning scheme map as **DDO17-1**

The Precinct Design Requirements for Precinct 1 are as follows:

- Development must provide a consistent built form edge to the street.

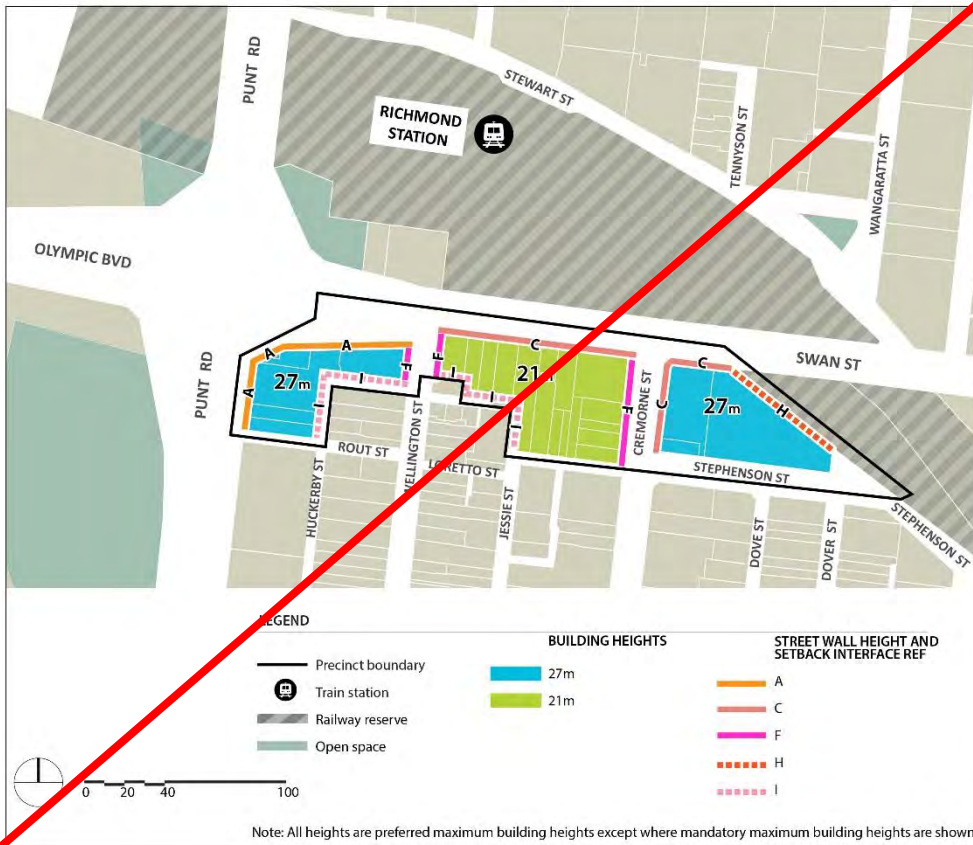
- The street wall of development on the corner of Swan Street and Punt Road must mark the entry into the Swan Street Activity Centre.
- The street wall of new development east of Wellington Street must reflect the height and fine grain articulation of the heritage buildings.
- Development must improve the pedestrian environment and amenity of the streetscape along Swan Street and the streets leading to the Cremorne Employment Precinct through high quality architectural design and improvements to the public realm.
- Development must not overshadow any part of the opposite footpath of Cremorne Street (measured as 2.0 metres from the boundary of Cremorne Street between 10 am and 2 pm at 22nd September).

**Table 1 – Street Wall Heights and Setbacks for Precinct 1**

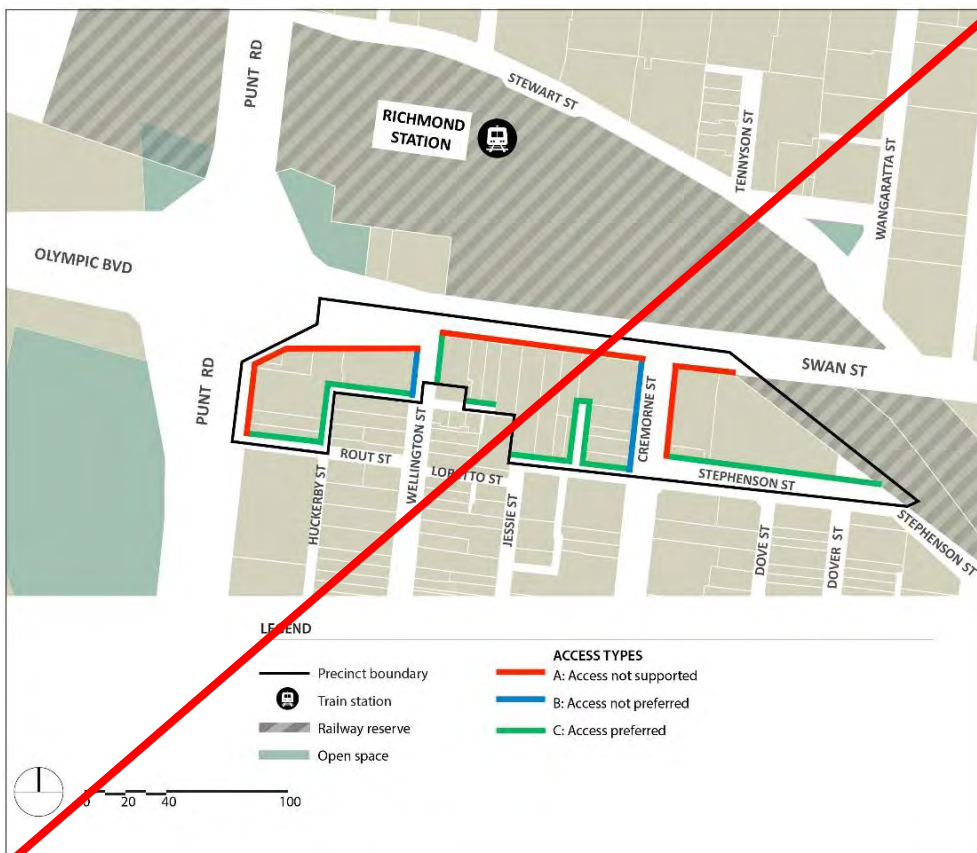
Interface Ref	Design Element	Mandatory Requirements	Preferred Requirements
A	Street wall height		21m maximum
	Street wall setback		0m
	Upper level setback		5m minimum
C	Street wall Height		11m maximum or the parapet height of the adjoining individually significant or contributory building if higher than 11m. 8m minimum. Match the parapet height of the taller adjoining heritage building.
	Street wall setback	-0m	
	Upper level setback	Minimum 5m for individually significant heritage buildings.	Minimum 5m elsewhere. Any part of a building above the street wall must be designed to ensure that it occupies no more than one third of the vertical angle defined by the whole building in the view from a sight line of 1.7 metres (on the opposite side of the street).
E	Street wall height		11m minimum.
	Street wall setback		0m.
	Upper level setback		3m minimum.
H	Street wall height		N/A.
	Street wall setback		0m minimum unless setback is identified on Plan 2.
	Upper level setback		0m.
†	Side and rear wall height		8m maximum on a common boundary with a property in a residential zone. 11.5m maximum if boundary abuts a laneway.

Interface Ref	Design Element	Mandatory Requirements	Preferred Requirements
	Upper-level setback		Development must be setback in accordance with Figure 1 and Figure 2. Development must minimise repetitive stepped form.

**Plan 1: Height and Interface Plan – Precinct 1 – Richmond Station**



**Plan 2: Access and Movement Plan – Precinct 1 – Richmond Station**



**Precinct 2—Swan Street Activity Centre**

Shown on the planning scheme map as **DDO17-2**

The Precinct Design Requirements for Precinct 2 are as follows:

- ~~Development must respect the consistent scale, grain and architectural quality of the highly intact heritage streetscapes and the individually significant buildings in the precinct.~~
- ~~Development along the rail corridor must avoid a continuous wall of taller development when viewed from local streets south of the rail corridor.~~
- ~~Development on the south side of Swan Street must maintain Dimmey's Tower as the prominent landmark in the streetscape when viewed from the northern footpath of Swan Street east of the rail bridge and west of Church Street.~~
- ~~Taller built form adjoining the rail corridor must not compete with the architectural form and complexity of the Dimmey's Clock Tower.~~
- ~~Development must improve the pedestrian environment and amenity of streets and laneways that provide a pedestrian connection to Swan Street, Church Street, East Richmond Train Station, and entrances to buildings.~~
- ~~Development adjoining Milton Place must be designed to address the potential future public park adjoining East Richmond Railway Station.~~
- ~~Development must not overshadow any part of the opposite footpath of Lennox Street, Stanley Street, Clifton Street, and Docker Street, (measured as 2.0 metres from the road boundary of the street between 10 am and 2 pm at 22nd September).~~
- ~~Development must not overshadow any part of the potential future open space adjacent to the East Richmond Station (measured as beyond 7.0m from the eastern road boundary of Milton Place and beyond 10.0m from the southern road boundary of Milton Place between 10 am and 2 pm at 22nd September).~~
- ~~Development at 108-120 Swan Street and 2 Kipling Street, as shown in Plan 4, should include a rear setback, at ground floor, to facilitate the ongoing function of the laneway and allow for building services and car park access. The setback and laneway should be a minimum width of 6m in total.~~
- ~~Development of 94-98 Swan Street, as shown in Plan 4, should include a rear setback, at ground floor, to allow for building services and car park access. The setback and laneway should be a minimum width of 3m in total.~~
- ~~Development should facilitate the creation of a shared zone where properties abut a future shared zone as shown on Plan 4.~~

**Table 2—Street Wall Heights and Setbacks for Precinct 2**

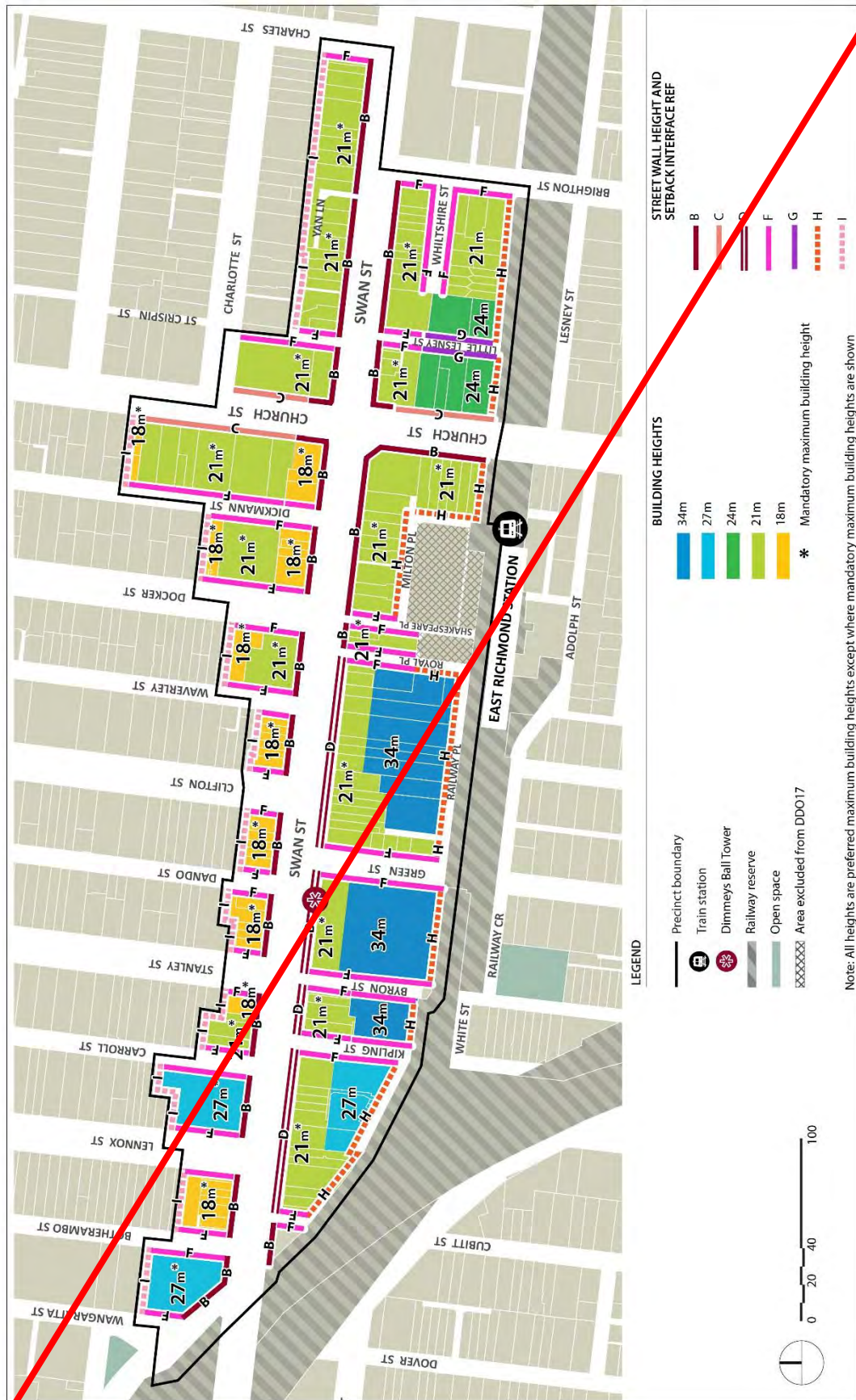
Interface Ref	Design Element	Mandatory Requirements	Preferred Requirements
B	Street wall height	11m maximum or the parapet height of the adjoining individually significant or contributory building if higher than 11m. 8m minimum.	Match the parapet height of the adjoining heritage building.
	Street wall setback	0m	
	Upper level setback	5m minimum setback	For all development in a heritage overlay, any part of the building above the heritage street wall should

Interface Ref	Design Element	Mandatory Requirements	Preferred Requirements
			be designed to ensure that it occupies no more than one third of the vertical angle defined by the whole building in the view from a sight line of 1.7 metres (on the opposite side of the street).
C	Street wall Height		11m maximum or the parapet height of the adjoining individually significant or contributory building if higher than 11m. 8m minimum. Match the parapet height of the taller adjoining heritage building.
	Street wall setback	-0m	
	Upper level setback	5m minimum for individually significant heritage buildings.	Minimum 5m elsewhere. Any part of a building above the street wall should be designed to ensure that it occupies no more than one third of the vertical angle defined by the whole building in the view from a sight line of 1.7 metres (on the opposite side of the street).
D	Street wall height	11m maximum or the parapet height of the adjoining individually significant or contributory building if higher than 11m. 8m minimum.	Match the parapet height of the taller adjoining heritage building.
	Street wall setback	0m.	
	Upper level setback	10m minimum for development up to 24m. 20m minimum for any height above 24m.	
F	Street wall height		11m maximum
	Street wall setback		0m.
	Upper level setback		5m minimum for land affected by HO335 and individually significant buildings. 3m minimum elsewhere.
G	Street wall height		14.5m maximum.
	Street wall setback		0m.
	Upper level setback		3m minimum.

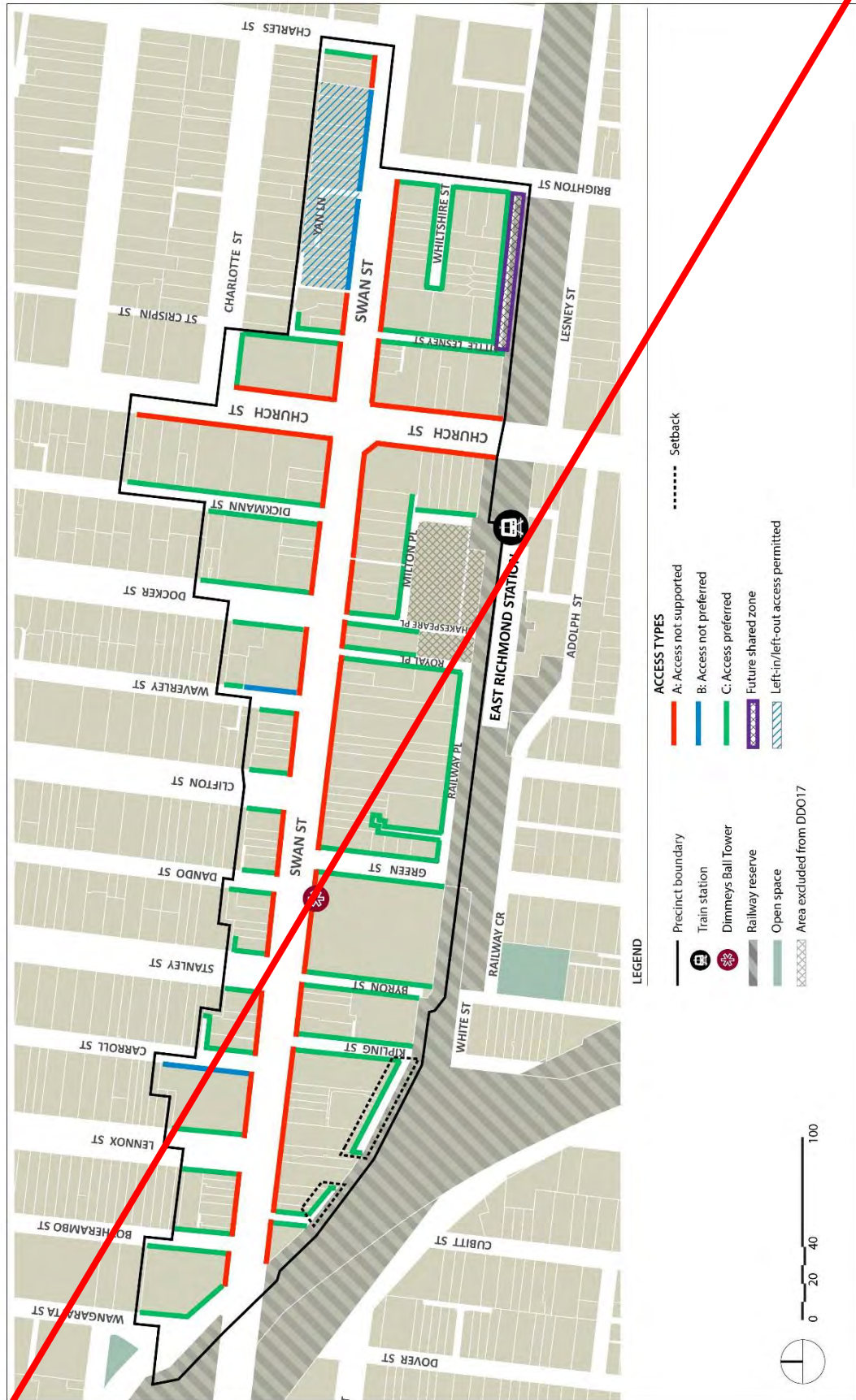


Interface Ref	Design Element	Mandatory Requirements	Preferred Requirements
H	Street wall height		N/A.
	Street wall setback		0m minimum unless setback is identified on the Plan 4.
	Upper level setback		0m.
I	Side and rear wall height		8m maximum on a common boundary with a property in a residential zone. 11.5m maximum if boundary abuts a laneway.
	Upper level setback		Development must be setback in accordance with Figure 1 and Figure 2. Development should minimise stepped form.

**Plan 3: Height and Interface Plan – Precinct 2 – Swan Street Retail Centre**



**Plan 4: Access and Movement Plan – Precinct 2 – Swan Street Retail Centre**



**Precinct 3 – Swan Street East**

Shown on the planning scheme map as **DDO17-3**

The design requirements for Precinct 3 are as follows:

- Development along Swan Street must continue the street level experience of the Swan Street and Burnley Street heritage precincts by maintaining a consistent and prominent street wall.
- Development must improve the pedestrian environment and amenity of Swan Street, and easements and laneways that provide a pedestrian connection to Swan Street and to the entrances to new developments.
- Development of properties in the locations shown as “Upper Level Building Breaks” on Plan 5 must incorporate side setbacks greater than the standards in Clause 2 and enable clear views to the sky between buildings along Swan Street when viewed from the opposite side of Swan Street and along Lord Street and Edinburgh Street.
- Development must not overshadow any part of the opposite footpath of Mary Street and Coppin Street (measured as 2.0m from the road boundary between 10 am and 2 pm at 22nd September).
- Development should be designed to enhance, activate, and provide passive surveillance to the pedestrian connection between Glass Street and Swan Street as shown on Plan 6.
- Development should include north-south access in the locations shown on the Access and Movement Plan 6 to allow for building services and car park access.

**Table 3 – Street Wall Heights and Setbacks for Precinct 3**

Interface Ref	Design Element	Mandatory Requirements	Preferred Requirements
G	Street wall Height		11m maximum or the parapet height of the adjoining individually significant or contributory building if higher than 11m. 8m minimum. Match the parapet height of the taller adjoining heritage building
	Street wall setback	-0m	
	Upper level setback	Minimum 5m for individually significant heritage buildings.	Minimum 5m elsewhere. Any part of a building above the street wall should be designed to ensure that it occupies no more than one third of the vertical angle defined by the whole building in the view from a sight line of 1.7 metres (on the opposite side of the street).
E	Street wall height		14.5m maximum.
	Street wall setback		0m.
	Upper level setback		5m minimum.
F	Street wall height		11m minimum.

Interface Ref	Design Element	Mandatory Requirements	Preferred Requirements
	Street wall setback		0m.
	Upper level setback		5m minimum for individually significant buildings. 3m minimum elsewhere.
G	Street wall height		14.5m maximum.
	Street wall setback		0m.
	Upper level setback		3m minimum.
H	Street wall height		N/A.
	Street wall setback		0m minimum unless setback is identified on the Plan 6.
	Upper level setback		0m.
I	Side and rear wall height		8m maximum on a common boundary with a property in a residential zone. 11.5m maximum if boundary abuts a laneway.
	Upper level setback		Development should be setback in accordance with Figure 1 and Figure 2. Development should minimise stepped form.

**Plan 5: Height and Interface Plan – Precinct 3 – Swan Street East**



**Plan 6: Access and Movement Plan – Precinct 3 – Swan Street East**



**Precinct 4 – Burnley Station**

Shown on the planning map as **DDO17-4**

The Precinct Design Requirements for Precinct 4 are as follows:

- Development must respect the scale, rhythm, and architectural detail of the streetscape and the heritage buildings in the Burnley Street Heritage Precinct.
- Development along Swan Street must achieve a prominent street wall and provide an appropriate transition in the street wall height to the Burnley Street and Bendigo Street heritage precincts.
- Development must maintain the amenity of Ryan’s Reserve.
- Development of properties in the locations shown as “Upper Level Building Breaks” on Plan 7 must incorporate side setbacks greater than the standards in Clause 2 to enable clear views to the sky between buildings along Swan Street when viewed from the opposite side of Swan Street and along Cutter Street and Bendigo Street.
- Development must not overshadow the opposite footpath of Stawell Street (measured as 2.0m from the road boundary between 10 am and 2 pm at 22nd September).
- Development must maintain solar access to Ryan’s Reserve as follows:
  - Beyond 16m of the eastern boundary of the reserve from 10 am onwards on 22 September
  - At the western boundary of the reserve until 2 pm on 22 September.
- Development of 500 to 506 Swan Street, as shown in Plan 8, should include a rear setback, at ground floor, to facilitate the ongoing function of the laneway and allow for building services and car park access. The setback and laneway should be a minimum width of 6m in total.
- Development of 130 to 136 Stawell, as shown in Plan 8, should include a rear setback, at ground floor, to facilitate the ongoing function of the laneway and allow for building services and car park access. The setback and laneway should be a minimum width of 3m in total.

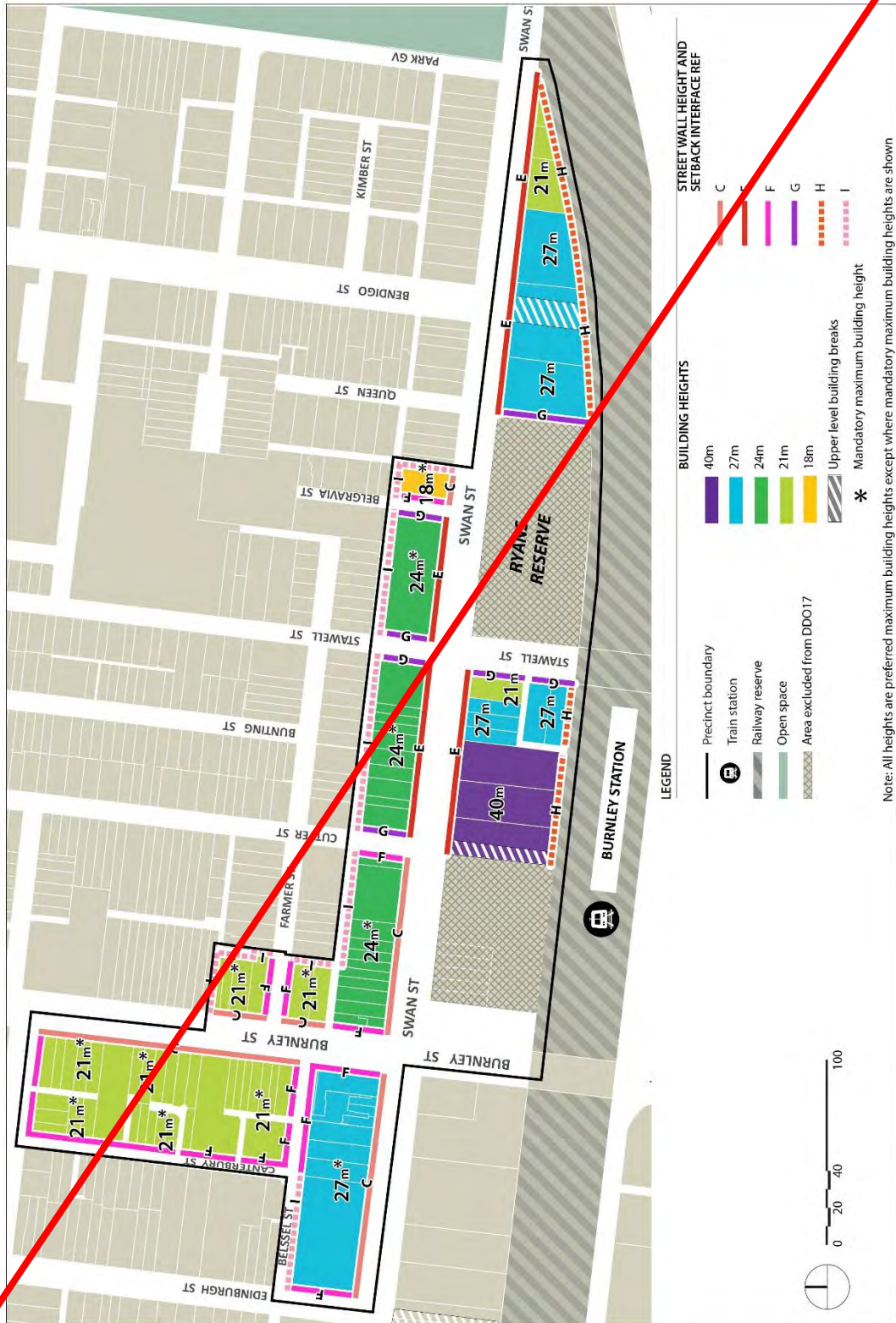
**Table 4 – Street Wall Heights and Setbacks for Precinct 4**

Interface Ref	Design Element	Mandatory Requirements	Preferred Requirements
G	Street wall Height		11m maximum or the parapet height of the adjoining individually significant or contributory building if higher than 11m. 8m minimum. Match the parapet height of the taller adjoining heritage building.
	Street wall setback	-0m.	
	Upper level setback	10m minimum from Swan Street setback for land affected by HO 286 (365 Swan Street). Minimum 5m for other individually significant heritage buildings.	Minimum 5m elsewhere. Any part of a building above the street wall should be designed to ensure that it occupies no more than one third of the vertical angle defined by the whole building in the view from a sight line of

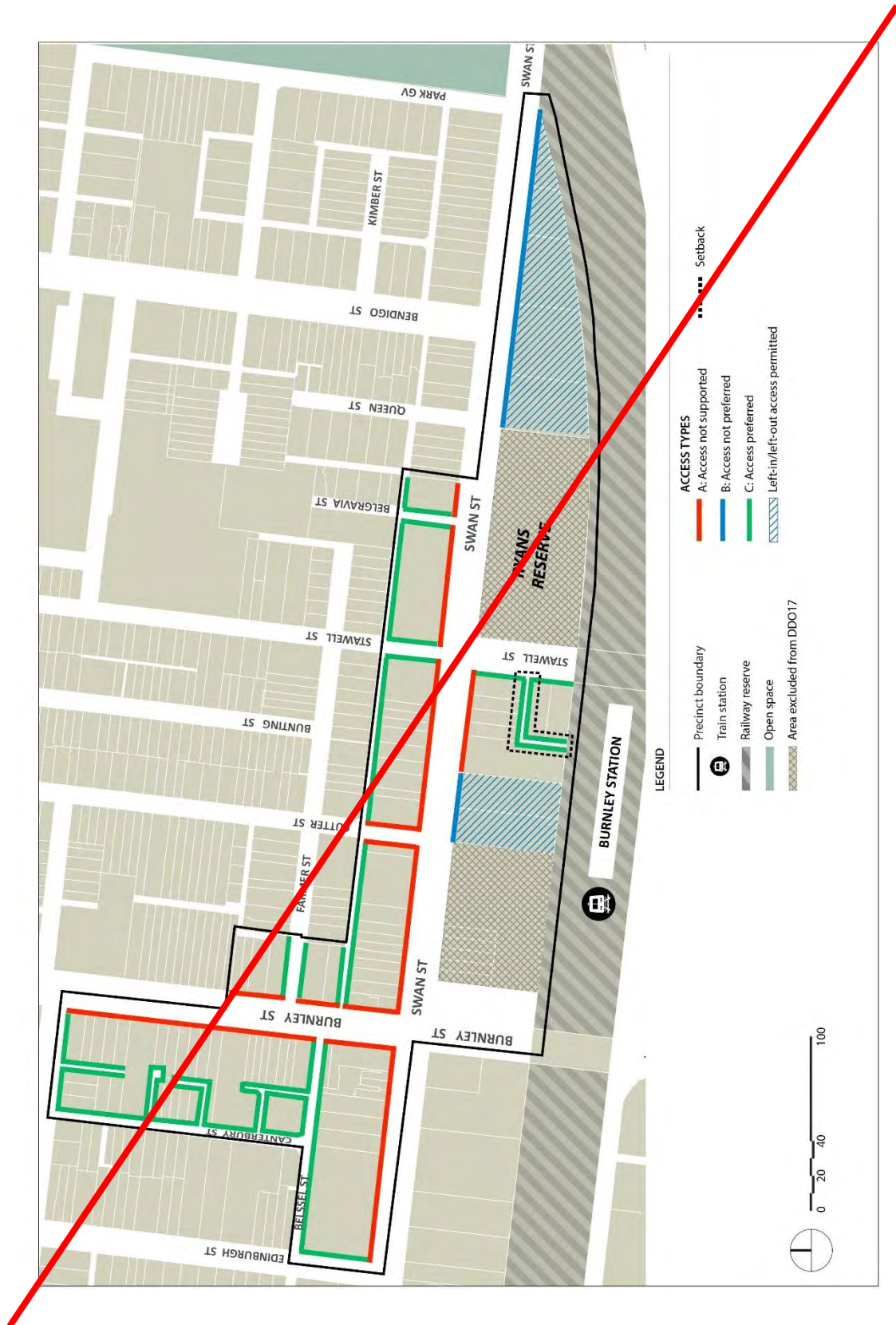


Interface Ref	Design Element	Mandatory Requirements	Preferred Requirements
			1.7 metres (on the opposite side of the street).
E	Street wall height		14.5m maximum
	Street wall setback		0m.
	Upper level setback		5m minimum.
F	Street wall height		11m minimum.
	Street wall setback		0m.
	Upper level setback		5m minimum for individually significant buildings. 3m minimum elsewhere.
G	Street wall height		14.5m maximum.
	Street wall setback		0m.
	Upper level setback		3m minimum.
H	Street wall height		N/A.
	Street wall setback		0m minimum unless setback is identified on the Plan 8.
	Upper level setback		0m.
I	Side and rear wall height		8m maximum on a common boundary with a property in a residential zone. 11.5m maximum if boundary abuts a laneway.
	Upper level setback		Development must be setback in accordance with Figure 1 and Figure 2. Development must minimise stepped form.

**Plan 7: Height and Interface Plan – Precinct 4 – Burnley Station**



**Plan 8: Access and Movement Plan – Precinct 4 – Burnley Station**



**3.0 Subdivision**

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~~None specified.~~

**4.0 Advertising**

~~dd/mm/yyyy  
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~~None specified.~~

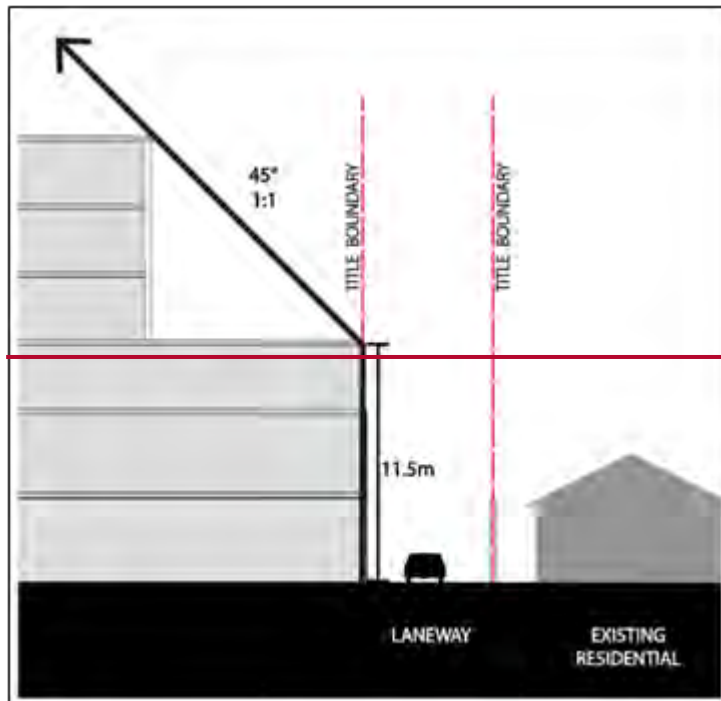
**5.0 Decision guidelines**

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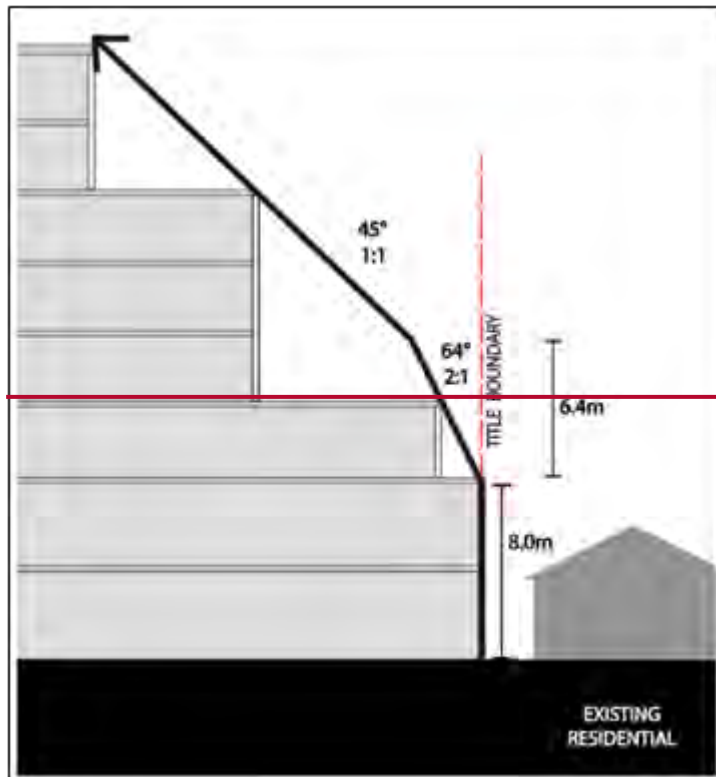
~~The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specified in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:~~

- ~~▪ Whether the General Design Requirements and the Precinct Design Requirements in Clause 2.0 are met.~~
- ~~▪ The design of the streetscape interface and its contribution to an active street environment.~~
- ~~▪ Whether the proposal contributes to and improves the pedestrian connectivity and amenity of the public realm.~~
- ~~▪ The shadowing impacts of the development on footpaths and public spaces.~~
- ~~▪ The wind effects created by the development.~~
- ~~▪ The separation between buildings at upper levels when viewed from the opposite side of Swan Street and from local streets.~~
- ~~▪ The prominence of the heritage street wall in the vistas along Swan Street, Church Street, Burnley Street, and local streets.~~
- ~~▪ Whether heritage buildings on street corners retain their prominence when viewed on both streets.~~
- ~~▪ Whether heritage buildings retain their three-dimensional form as viewed from the public realm.~~
- ~~▪ Whether upper level development above the heritage street wall is visually recessive and does not overwhelm the heritage buildings.~~
- ~~▪ The impact of development on view lines to the Dimmey's Clock Tower.~~
- ~~▪ The impact of development on the operation of the tram routes along Swan Street and Church Street.~~

**Figure 1 to schedule 17 – Residential interface with an existing laneway**



**Figure 2 to schedule 17 – Residential interface direct abuttal**



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Proposed C191yara

## SCHEDULE 25 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY

Shown on the planning scheme map as **DDO25**.

### SWAN STREET ACTIVITY CENTRE - PRECINCT 1 RICHMOND STATION

#### 1.0

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Proposed C191yara

#### Design objectives

- To create an entry to the activity centre, anchored by Richmond Station, that is defined by high quality, contemporary buildings that wrap around the corner from Hoddle Street into Swan Street.
- To provide taller street walls on Swan Street, west of Wellington Street to define the entry to the Activity Centre.
- To maintain the prominence of the Precinct Hotel on the corner of Cremorne Street and Swan Street within the streetscape.
- To improve the pedestrian environment and amenity of the streetscape along Swan Street and the streets leading to Cremorne through activated ground floor frontages and improvements to the public realm.
- To support a new mid rise scale built form character with lower built form at the interfaces with streets and the adjoining low rise residential areas.

#### 2.0

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Proposed C191yara

#### Buildings and works

A permit is required to construct a building or construct or carry out works.

##### 2.1 Definitions

**Street-wall** means the facade of a building at the street boundary, or, if the existing heritage building is set back from the street boundary, the front of the existing building. Street wall height is measured at the vertical distance between the footpath at the centre of the frontage and the highest point of the building at the street edge, with the exception of architectural features and building services.

**Heritage building** means any building subject to a Heritage Overlay, graded as either Contributory or Individually Significant (including properties on the Victorian Heritage Register).

**Laneway** means a road reserve, public highway or right of way 9 metres or less wide.

**Building height** means the vertical distance from natural ground level to the roof or parapet at any point.

**Parapet height** does not include features such as brackets, pediments, urns, finial or other decorative elements.

**Road boundary** means the boundary between the public road and the private property.

**Shared zone** means a road or network of roads where pedestrians, cyclists, and vehicles share the roadway.

**Upper level** means development above the height of the street wall.

##### 2.2 Requirements

A permit cannot be granted to vary a requirement expressed with the term ‘must’ or listed in a ‘Mandatory’ column of a table.

A permit cannot be granted to construct a building or construct or carry out works, which:

- exceeds the mandatory maximum building height and street wall height requirements shown in the Height and Interface Plan 1 of this schedule.
- reduces the mandatory minimum street wall height and upper level setback requirements shown in the Height and Interface Plan 1 of this schedule.

### **2.3 Design requirements**

The following design requirements apply to an application to construct a building or construct or carry out works.

#### **Design quality requirements**

Development should achieve urban design and architectural excellence.

Development should be well spaced and sited to avoid visual bulk and provide equitable access to an outlook and good daylight.

Development should provide for street activation at ground levels.

Development should be designed to avoid repetitive stepped form at upper levels.

Upper level development should be designed to ensure buildings provide detail on all facades when viewed from all directions.

Frontages at ground floor should incorporate verandahs, consistent with the form and scale of adjoining verandahs, into the façade design.

Protect the amenity of existing residential properties in terms of visual bulk, overshadowing of private open space, overlooking and vehicle access.

Development should maintain the prominence of the heritage street wall and respect the architectural form and qualities of heritage buildings and the heritage streetscape within land affected by HO524.

Upper level development on land within a Heritage Overlay or on land immediately adjoining a heritage building should:

- be visually recessive and not visually dominate the heritage building and the heritage streetscape; and
- avoid large expanses of glazing with a horizontal emphasis in the upper levels of development.

#### **Building height requirements**

A permit should only be granted to construct a building or construct or carry out works, which exceeds the preferred building height shown in the Height and Interface Plan 1 of this schedule where all the following requirements are met to the satisfaction of the responsible authority:

- the built form outcome that results from the proposed variation satisfies the design objectives in Clause 1.0 of this schedule and the relevant design requirements specified in this schedule; and
- the proposal achieves each of the following:
  - greater building separation than the minimum requirement in this schedule;
  - excellence for environmentally sustainable design measured as a minimum BESS project score of 70%;
  - no additional amenity impacts to residentially zoned properties, beyond that which would be generated by a proposal that complies with the preferred building height;
  - provision of end-of-trip facilities, including secure bicycle parking, locker and shower facilities and change rooms; and
- where the proposal includes dwellings, it also achieves each of the following:
  - housing for diverse households types;

## YARRA PLANNING SCHEME

- accessibility provision objective that exceeds the minimum standards in Clauses 55.07 and 58;
- communal and/or private open space provision that exceeds the minimum standards in Clauses 55.07 and 58; and
- where the proposal is on land within a Heritage Overlay, it also achieves the following:
  - Upper level development above the preferred building height on land within a Heritage Overlay should have increased setbacks to minimise its visibility above the heritage building and from the heritage streetscape.

Architectural features may exceed the preferred or mandatory height.

Service equipment/structures including plant rooms, lift overruns, stairs, structures associated with green roof areas and other such equipment may exceed the preferred or mandatory height provided that each of the following criteria are met for the equipment or structure:

- Less than 50 per cent of the roof area is occupied by the equipment /structures (other than solar panels);
- The equipment/structures does not cause additional overshadowing of adjoining properties; and
- The equipment/structures does not extend higher than 3.6 metres above the maximum building height.

### Street wall and setback requirements

A permit should not be granted to construct a building or construct or carry out works, which exceeds the relevant preferred maximum street wall height and/or reduces the relevant preferred minimum setback requirements specified in this schedule unless the following are met, to the satisfaction of the responsible authority:

- The built form outcome that results from the proposed variation satisfies the design objectives in Clause 1.0 of this schedule.
- The built form outcome that results from the proposed variation satisfies the relevant design requirements specified in this schedule.
- The street wall at ground floor level is designed to allow floor to floor ceiling heights suitable to accommodate commercial activity.

Projections such as balconies, building services and architectural features must not intrude into a setback.

On corner sites where two different street wall heights are nominated, buildings should 'turn the corner' and apply the Swan Street wall height. If the Swan Street wall is higher it should transition to the lower nominated street wall height on the side street.

### Building separation requirements

Where development shares a common boundary and no interface treatment is shown in Plan 1, upper level development should:

- For buildings up to 28 metres, be setback a minimum of 4.5m from the common boundary, where a habitable window or balcony is proposed.
- For buildings up to 28 metres, be setback a minimum of 3.0m from the common boundary, where a commercial or non-habitable window is proposed.
- For buildings taller than 28 metres, be setback a minimum of 6 metres above 28 metres.

Where the common boundary is a laneway, the setback is measured from the centre of the laneway.



**Overshadowing requirements**

A permit should not be granted to construct a building or construct or carry out works that would overshadow part of the opposite footpath of Cremorne Street (measured from the property boundary to the existing kerb of Cremorne Street between 10 am and 2 pm at 22nd September), unless the overshadowing would not unreasonably prejudice the amenity of the public space.

**Vehicle and pedestrian access requirements**

Development should provide vehicular access from rear lanes or from side streets in the preferred locations in the Access and Movement Plan 2 of this schedule.

Development with redundant vehicle access points to Swan Street should reinstate the kerb, linemarked parking bays, and relocate any parking signs.

Vehicle ingress and egress into development, including loading facilities and building servicing, should be designed to ensure a high quality pedestrian amenity and limit potential conflict between vehicle movements and pedestrian activity.

Pedestrian access to buildings, including upper level apartments, should be from a street or a shared zone shown on the Access and Movement Plan 2 of this schedule. Where pedestrian access can only be provided from a laneway at the rear of buildings the pedestrian entrance should be setback from the rear laneway and well-lit to enable safe access.

**Table 1 – Street Wall Heights and Setbacks for Precinct 1 Richmond Station**

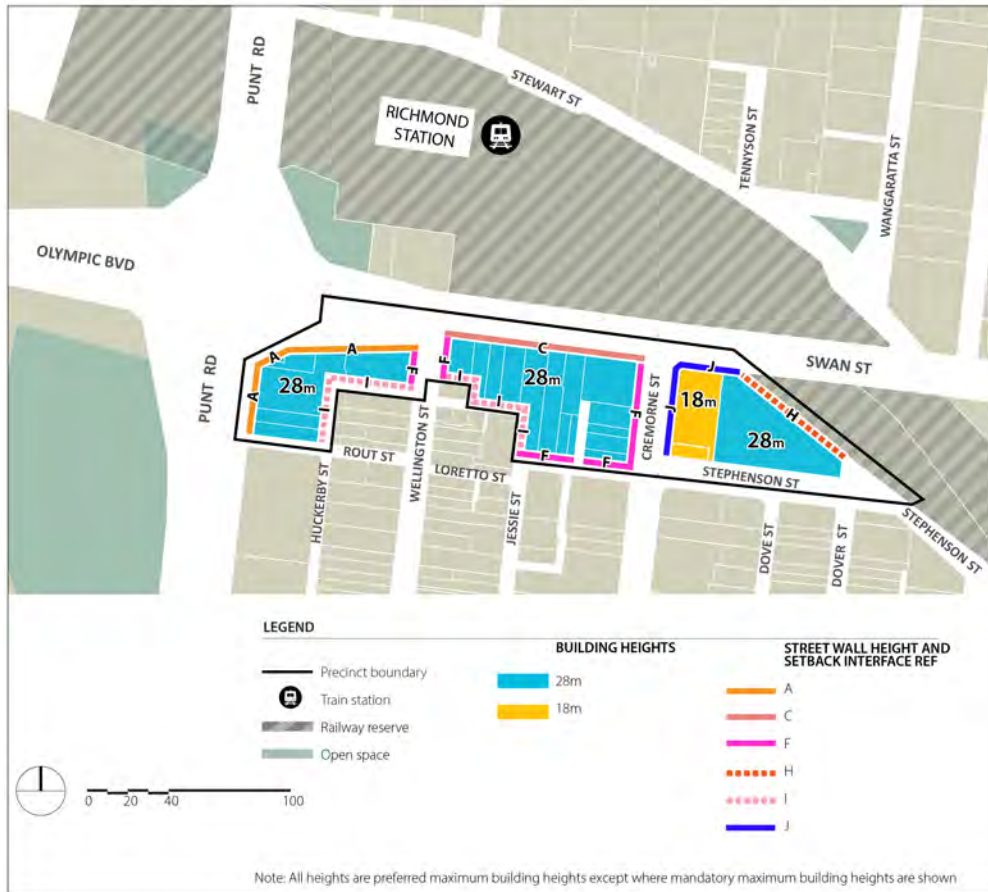
Interface Ref	Design Element	Mandatory Requirements	Preferred Requirements
A	Street wall height		21m maximum.
	Street wall setback		0m.
	Upper level setback		6m minimum.
C	Street wall height		11m maximum. 8m minimum. Match the parapet height of the taller adjoining heritage building, for a minimum length of 6m from the heritage building.
	Street wall setback	0m.	
	Upper level setback	Minimum 6m for heritage buildings.	Minimum 6m elsewhere.
F	Street wall height		11m maximum.
	Street wall setback		0m.
	Upper level setback		3m minimum.
H	Street wall height		N/A.
	Street wall setback		0m minimum unless setback is identified on Plan 2.
	Upper level setback		0m.
I	Side and rear wall height		8m maximum on a common boundary with a property in a residential zone. 11m maximum if boundary abuts a laneway.
	Side and rear setback		Development should be setback in accordance with Figure 1 and Figure 2. Development should minimise repetitive stepped form.

**YARRA PLANNING SCHEME**

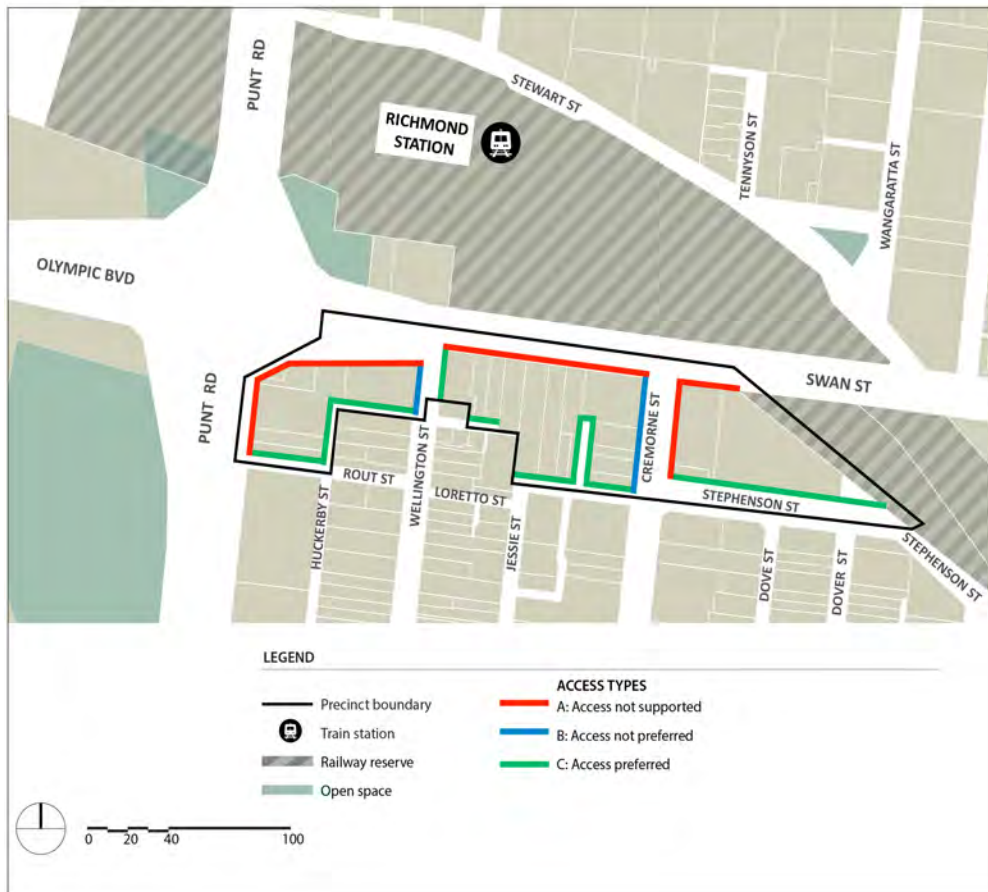
<b>Interface Ref</b>	<b>Design Element</b>	<b>Mandatory Requirements</b>	<b>Preferred Requirements</b>
	Boundary wall setback		At 40 Swan Street only, development not built on a common boundary should provide a minimum setback of 4.5 metres to the common boundary.
J	Street wall height		11m maximum. 8m minimum. Match the parapet height of the taller adjoining heritage building, for a minimum length of 6m from the heritage building.
	Street wall setback	0m.	
	Upper level setback	Minimum 10m from Swan Street setback. Minimum 8.5m from Cremorne Street setback.	

# YARRA PLANNING SCHEME

## Plan 1: Height and Interface Plan – Precinct 1 Richmond Station



## Plan 2: Access and Movement Plan – Precinct 1 Richmond Station



**3.0 Subdivision**

--/--  
Proposed C191yara

None specified

**4.0 Advertising**

--/--  
Proposed C191yara

None specified

**5.0 Application requirements**

--/--  
Proposed C191yara

An application must be accompanied by:

- A desktop wind effects assessment for the proposed development when the building height is 15m or higher from natural ground level.

**6.0 Decision guidelines**

--/--  
Proposed C191yara

The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specific in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

- Whether the Design Requirements in Clause 2.3 are met.
- Whether design excellence is achieved (in terms of building siting, scale, massing, articulation and materials).
- The design of the streetscape interface and its contribution to an active street environment.
- Whether the proposal contributes to and improves the pedestrian connectivity and amenity of the public realm.
- The shadowing impacts of the development on footpaths and public spaces.
- The wind effects created by the development.
- The separation between buildings at upper levels when viewed from the opposite side of Swan Street and from local streets.
- The prominence of the heritage street wall in the vistas along Swan Street and local streets.
- Whether heritage buildings on street corners retain their prominence when viewed on both streets.
- Whether heritage buildings retain their three-dimensional form as viewed from the public realm.
- Whether upper level development above the heritage street wall is visually recessive and does not overwhelm the heritage buildings.
- The impact of development on the operation of the tram routes along Swan Street.

Figure 1 to schedule 25 – Interface with an existing laneway

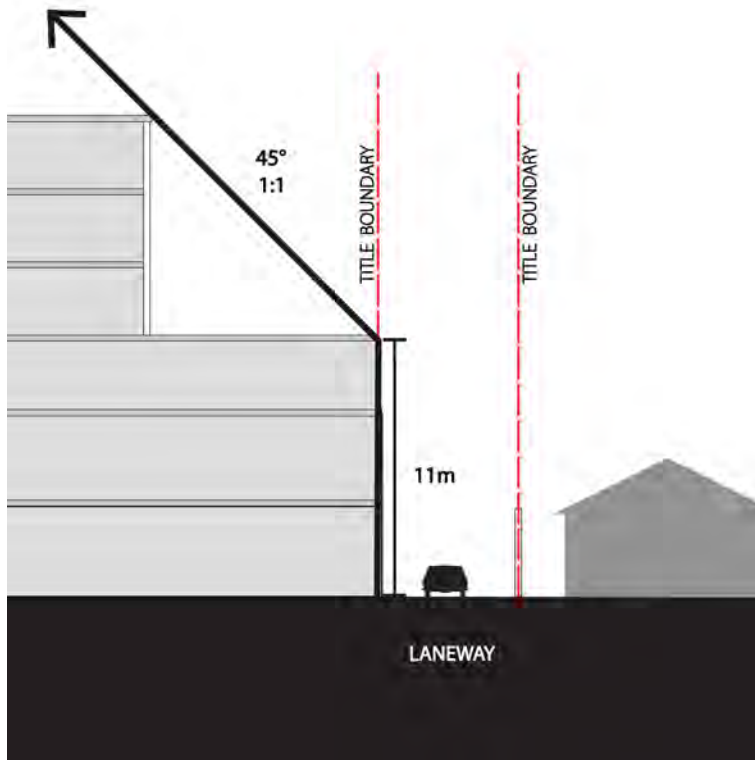
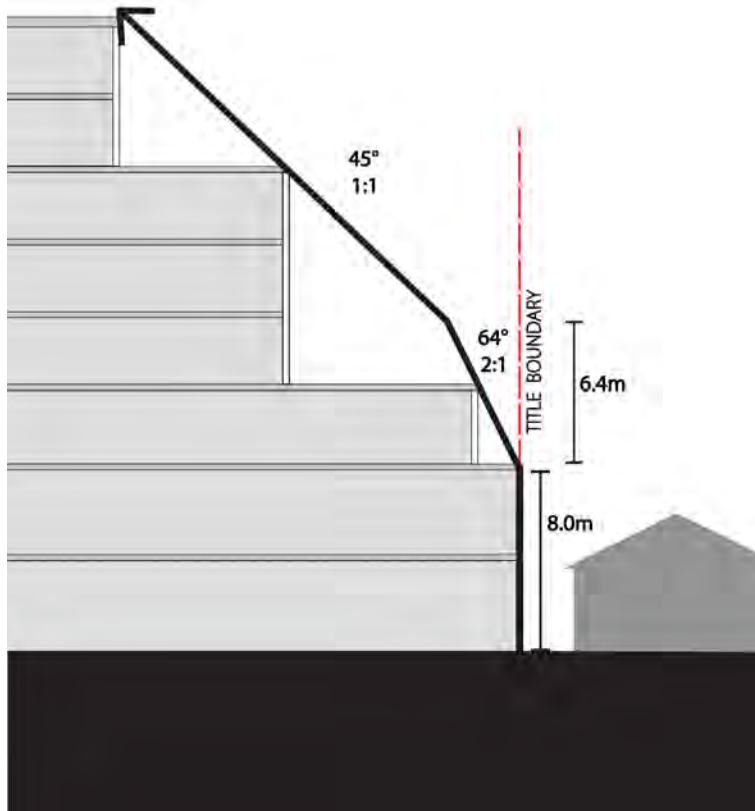


Figure 2 to schedule 25 – Interface direct abuttal



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Proposed C191yara

## SCHEDULE 26 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY

Shown on the planning scheme map as **DDO26**.

### SWAN STREET ACTIVITY CENTRE - PRECINCT 2 SWAN STREET RETAIL CENTRE

#### 1.0

--/--/---  
Proposed C191yara

#### Design objectives

- To ensure development maintains the prominence of the heritage street wall and respects the architectural form and qualities of heritage buildings and the heritage streetscapes.
- To ensure development, on the south side of Swan Street, maintains the Dimmeys Tower as the prominent landmark in the streetscape when viewed from the northern footpath of Swan Street east of the rail bridge and west of Church Street.
- To support a new mid-rise scale built form character with lower built form at the interfaces with streets and the adjoining low-rise residential areas that supports the fine grain, compact retail and entertainment focus of the precinct.
- To ensure development enhances the pedestrian experience through improved activation at ground floor and promoting a sense of enclosure and continuity in built form along Swan Street, Church Street, side streets and laneways.
- To ensure taller development on the south side of Swan Street, close to the railway line, is designed and spaced to contribute to a varied skyline and provides street walls that create a stronger sense of enclosure to the streets.

#### 2.0

--/--/---  
Proposed C191yara

#### Buildings and works

A permit is required to construct a building or construct or carry out works.

##### 2.1 Definitions

**Street-wall** means the facade of a building at the street boundary, or, if the existing heritage building is set back from the street boundary, the front of the existing building. Street wall height is measured at the vertical distance between the footpath at the centre of the frontage and the highest point of the building at the street edge, with the exception of architectural features and building services.

**Heritage building** means any building subject to a Heritage Overlay, graded as either Contributory or Individually Significant (including properties on the Victorian Heritage Register).

**Laneway** means a road reserve, public highway or right of way 9 metres or less wide.

**Building height** means the vertical distance from natural ground level to the roof or parapet at any point.

**Parapet height** does not include features such as brackets, pediments, urns, finial or other decorative elements.

**Road boundary** means the boundary between the public road and the private property.

**Shared zone** means a road or network of roads where pedestrians, cyclists, and vehicles share the roadway.

**Upper level** means development above the height of the street wall.

##### 2.2 Requirements

A permit cannot be granted to vary a requirement expressed with the term ‘must’ or listed in a ‘Mandatory’ column of a table.

A permit cannot be granted to construct a building or construct or carry out works, which:

- exceeds the mandatory maximum building height and street wall height requirements shown in the Height and Interface Plan 1 of this schedule.
- reduces the mandatory minimum street wall height and upper level setback requirements shown in the Height and Interface Plan 1 of this schedule.

### **2.3 Design requirements**

The following design requirements apply to an application to construct a building or construct or carry out works.

#### **Design quality requirements**

Development should achieve urban design and architectural excellence.

Development should be well spaced and sited to avoid visual bulk and provide equitable access to an outlook and good daylight.

Development should provide for street activation at ground levels.

Development should be designed to avoid repetitive stepped form at upper levels.

Upper level development should be designed to ensure buildings provide detail on all facades when viewed from all directions.

Frontages at ground floor should incorporate verandahs, consistent with the form and scale of adjoining verandahs, into the façade design.

Protect the amenity of existing residential properties in terms of visual bulk, overshadowing of private open space, overlooking and vehicle access.

Development adjoining Milton Place should be designed to address the potential future public park adjoining East Richmond Railway Station.

Upper level development on land within a Heritage Overlay or on land immediately adjoining a heritage building should:

- be visually recessive and not visually dominate the heritage building and the heritage streetscape; and
- avoid large expanses of glazing with a horizontal emphasis in the upper levels of development.

#### **Building height requirements**

A permit should only be granted to construct a building or construct or carry out works, which exceeds the preferred building height shown in the Height and Interface Plan 1 of this schedule where all the following requirements are met to the satisfaction of the responsible authority:

- the building elements permitted by the proposed variation satisfy the general design objectives in Clause 1.0 of this schedule, the relevant design requirements specified in this schedule; and
- the proposal achieves each of the following:
  - greater building separation than the minimum requirement in this schedule;
  - excellence for environmentally sustainable design measured as a minimum BESS project score of 70%;
  - no additional amenity impacts to residentially zoned properties, beyond that which would be generated by a proposal that complies with the preferred building height;
  - provision of end-of-trip facilities, including secure bicycle parking, locker and shower facilities and change rooms; and
- where the proposal includes dwellings, it also achieves each of the following:
  - housing for diverse households types;

## YARRA PLANNING SCHEME

- accessibility provision objective that exceeds the minimum standards in Clauses 55.07 and 58; and
- communal and/or private open space provision that exceeds the minimum standards in Clauses 55.07 and 58.

Architectural features may exceed the preferred or mandatory height.

Service equipment / structures including plant rooms, lift overruns, structures associated with green roof areas and other such equipment may exceed the preferred or mandatory height provided that each of the following criteria are met for the equipment or structure:

- Less than 50 per cent of the roof area is occupied by the equipment (other than solar panels);
- The equipment does not cause additional overshadowing of adjoining properties; and
- The equipment does not extend higher than 3.6 metres above the maximum building height.

### **Street wall and setback requirements**

A permit should not be granted to construct a building or construct or carry out works, which exceeds the relevant preferred maximum street wall height and/or reduces the relevant preferred minimum setback requirements specified in this schedule unless the following are met, to the satisfaction of the responsible authority:

- The built form outcome that results from the proposed variation satisfies the design objectives in Clause 1.0 of this schedule;
- The built form outcome that results from the proposed variation satisfies the relevant design requirements specified in this schedule; and
- The street wall at ground floor level is designed to allow floor to floor ceiling heights suitable to accommodate commercial activity.

Projections such as balconies, building services and architectural features must not intrude into a setback.

On corner sites where two different street wall heights are nominated, buildings should 'turn the corner' and apply the Swan Street wall height. If the Swan Street wall is higher it should transition to the lower nominated street wall height on the side street.

### **Building separation requirements**

Where development shares a common boundary and no interface treatment is shown in Plan 1, upper level development should:

- For buildings up to 28 metres, be setback a minimum of 4.5m from the common boundary, where a habitable window or balcony is proposed.
- For buildings up to 28 metres, be setback a minimum of 3.0m from the common boundary, where a commercial or non-habitable window is proposed.
- For buildings taller than 28 metres, be setback a minimum of 6 metres above 28 metres.

Where the common boundary is a laneway, the setback is measured from the centre of the laneway.

Development along the rail corridor must avoid a continuous wall of taller development when viewed from local streets south of the rail corridor.

### **Overshadowing requirements**

A permit must not be granted to construct a building or construct or carry out works that would overshadow any of the following spaces between 10 am and 2 pm at 22nd September:

- any part of the southern footpath of Swan Street, measured from the property boundary to the existing kerb.



## YARRA PLANNING SCHEME

A permit should not be granted to construct a building or construct or carry out works that would overshadow any of the following spaces between 10 am and 2 pm at 22nd September, unless the overshadowing would not unreasonably prejudice the amenity of the public space, to the satisfaction of the responsible authority:

- any part of the opposite footpath of Church Street, measured from the property boundary to the existing kerb.
- any part of the opposite footpath of Lennox Street, Stanley Street, Clifton Street, and Docker Street, measured from the property boundary to the existing kerb.
- any part of the potential future open space adjacent to the East Richmond Station, measured as beyond 7.0m from the eastern kerb of Milton Place and beyond 10.0m from the southern kerb of Milton Place.

### Vehicle and pedestrian access requirements

Development should provide vehicular access from rear lanes or from side streets in the preferred locations in the Access and Movement Plan 2 of this schedule except in locations identified as “Left in - Left Out Access Permitted” in the Access and Movement Plan 2 of this schedule.

Development identified as “Left in - Left Out Access Permitted” in the Access and Movement Plan should limit the width of vehicle crossovers and incorporate ‘Left in’ and ‘Left out’ only vehicle access.

Development with redundant vehicle access points to Swan Street and Church Street should reinstate the kerb, linemarked parking bays, and relocate any parking signs.

Vehicle ingress and egress into development, including loading facilities and building servicing, should be designed to ensure a high quality pedestrian amenity and limit potential conflict between vehicle movements and pedestrian activity.

Pedestrian access to buildings, including upper level apartments, should be from a street or a shared zone shown on the Access and Movement Plan 2 of this schedule. Where pedestrian access can only be provided from a laneway at the rear of buildings the pedestrian entrance should be setback from the rear laneway and well-lit to enable safe access.

Development at 108-120 Swan Street and 2 Kipling Street, as shown in Plan 2, should include a rear setback, at ground floor, to facilitate the ongoing function of the laneway and allow for building services and car park access. The setback and laneway should be a minimum width of 6m in total.

Development of 94-98 Swan Street, as shown in Plan 2, should include a rear setback, at ground floor, to allow for building services and car park access. The setback and laneway should be a minimum width of 3m in total.

Development should facilitate the creation of a shared zone where properties abut a future shared zone as shown on Plan 2.

Development should improve the pedestrian environment and amenity of streets and laneways that provide a pedestrian connection to Swan Street, Church Street, East Richmond Train Station, and entrances to buildings.

**Table 1 – Street Wall Heights and Setbacks for Precinct 2**

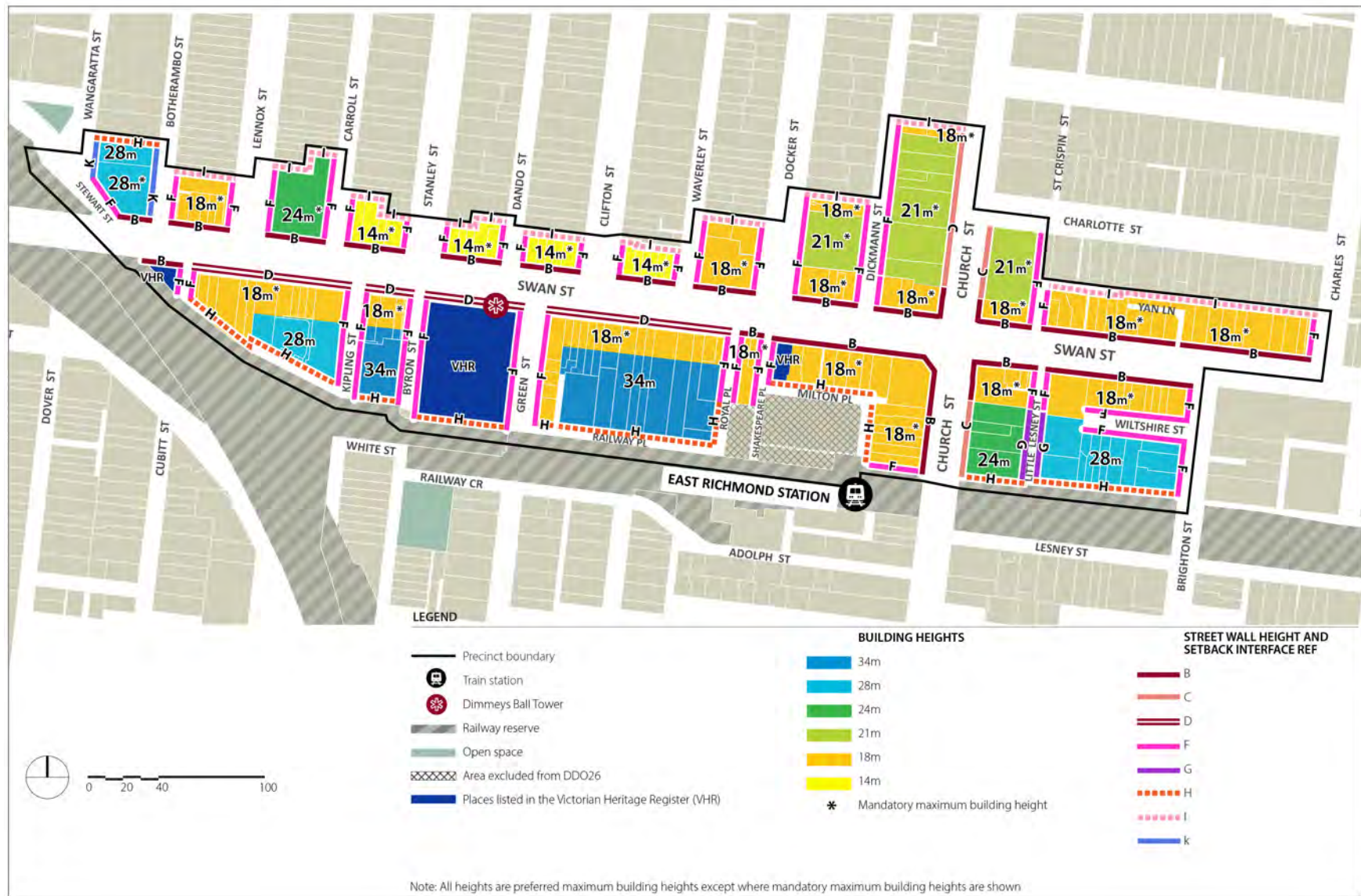
Interface Ref	Design Element	Mandatory Requirements	Preferred Requirements
B	Street wall height	11m maximum. 8m minimum.	Match the parapet height of the adjoining heritage building, for a minimum length of 6m from the heritage building.
	Street wall setback	0m.	
	Upper level setback	6m minimum setback.	

**YARRA PLANNING SCHEME**

<b>Interface Ref</b>	<b>Design Element</b>	<b>Mandatory Requirements</b>	<b>Preferred Requirements</b>
C	Street wall height		11m maximum. 8m minimum. Match the parapet height of the taller adjoining heritage building, for a minimum length of 6m from the heritage building.
	Street wall setback	0m.	
	Upper level setback	6m minimum for heritage buildings.	Minimum 6m elsewhere.
D	Street wall height	11m maximum. 8m minimum.	Match the parapet height of the taller adjoining heritage building, for a minimum length of 6m from the heritage building.
	Street wall setback	0m.	
	Upper level setback	10m minimum for development up to 21m. 20m minimum for any height above 21m.	
F	Street wall height		11m maximum.
	Street wall setback		0m.
	Upper level setback		6m minimum for land affected by HO335 or heritage buildings. 3m minimum elsewhere.
G	Street wall height		14m maximum.
	Street wall setback		0m.
	Upper level setback		3m minimum.
H	Street wall height		N/A.
	Street wall setback		0m minimum unless setback is identified on the Plan 2.
	Upper level setback		0m.
I	Side and rear wall height		8m maximum on a common boundary with a property in a residential zone. 11m maximum if boundary abuts a laneway.
	Side and rear setback		Development should be setback in accordance with Figure 1 and Figure 2. Development should minimise stepped form.
K	Street wall height		11m maximum.
	Street wall setback		0m.
	Upper level setback		3m minimum.

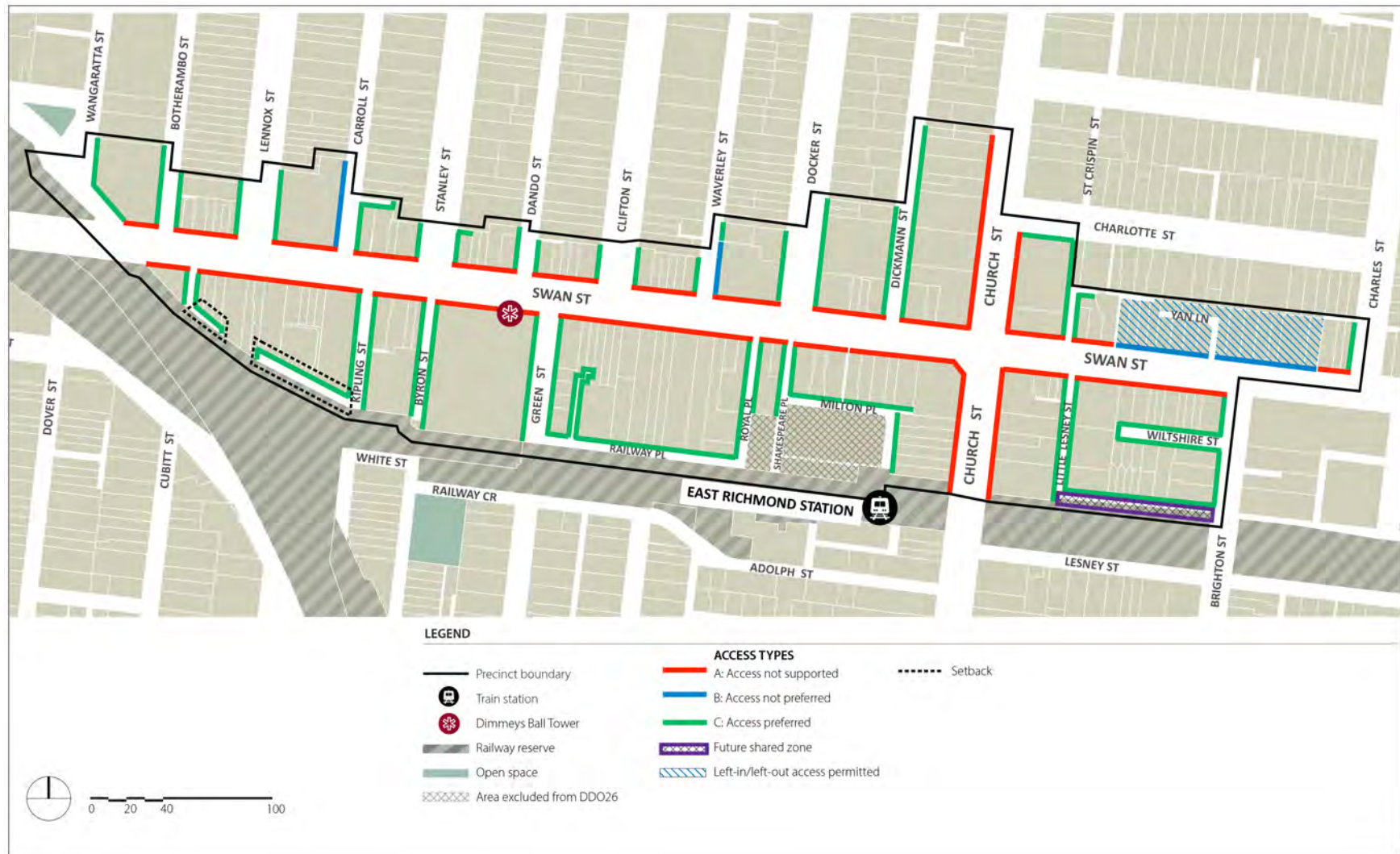
# YARRA PLANNING SCHEME

## Plan 1: Height and Interface Plan – Precinct 2 Swan Street Retail Centre



# YARRA PLANNING SCHEME

## Plan 2: Access and Movement Plan – Precinct 2 Swan Street Retail Centre



**3.0 Subdivision**

--/--/---  
Proposed C191yara None specified

**4.0 Advertising**

--/--/---  
Proposed C191yara None specified

**5.0 Application requirements**

--/--/---  
Proposed C191yara An application must be accompanied by:

- A desktop wind effects assessment for the proposed development when the building height is 15m or higher from natural ground level.

**6.0 Decision guidelines**

--/--/---  
Proposed C191yara The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specific in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

- Whether the Design Requirements in Clause 2.3 are met.
- Whether design excellence is achieved (in terms of building siting, scale, massing, articulation and materials).
- The design of the streetscape interface and its contribution to an active street environment.
- Whether the proposal contributes to and improves the pedestrian connectivity and amenity of the public realm.
- The shadowing impacts of the development on footpaths and public spaces.
- The wind effects created by the development.
- The separation between buildings at upper levels when viewed from the opposite side of Swan Street and from local streets.
- The prominence of the heritage street wall in the vistas along Swan Street, Church Street, and local streets.
- Whether heritage buildings on street corners retain their prominence when viewed on both streets.
- Whether heritage buildings retain their three-dimensional form as viewed from the public realm.
- Whether upper level development above the heritage street wall is visually recessive and does not overwhelm the heritage buildings.
- The impact of development on view lines to the Dimmeys Clock Tower.
- The impact of development on the operation of the tram routes along Swan Street and Church Street.

YARRA PLANNING SCHEME

Figure 1 to schedule 26 – Interface with an existing laneway

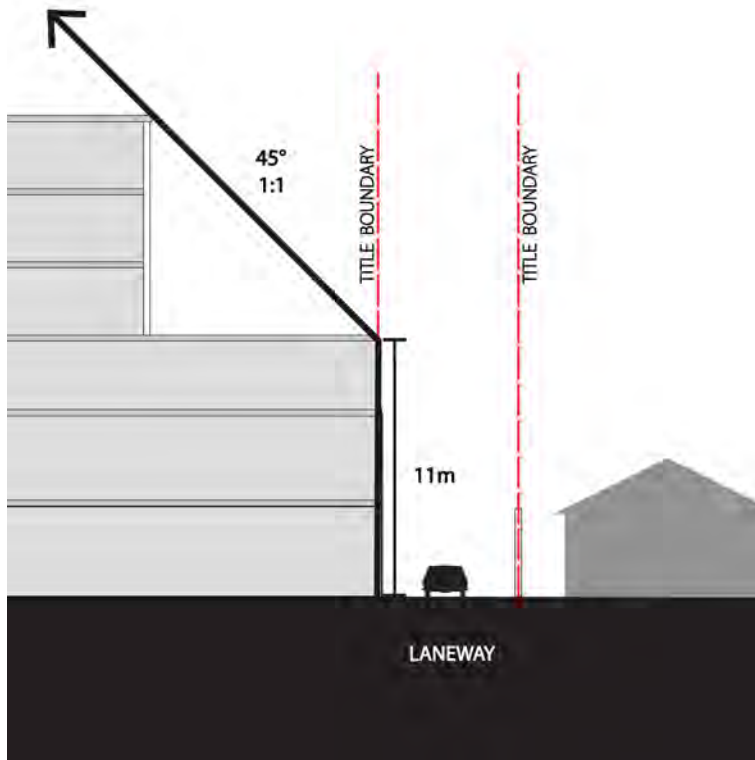
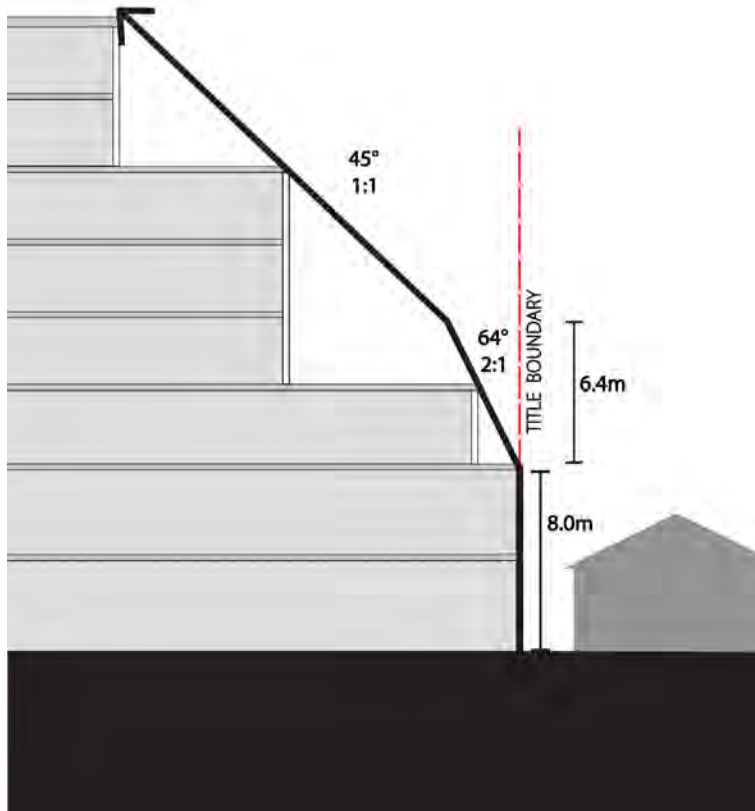


Figure 2 to schedule 26 – Interface direct abuttal



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Proposed C191yara

## SCHEDULE 27 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY

Shown on the planning scheme map as **DDO27**.

### SWAN STREET ACTIVITY CENTRE - PRECINCT 3 SWAN STREET EAST

#### 1.0

--/--/---  
Proposed C191yara

#### Design objectives

- To support a new mid rise scale built form character with lower built form at the interfaces with streets and the adjoining low rise residential areas that maintains an active, high quality and pedestrian friendly environment.
- To support taller development on the south side of Swan Street that has regard to the north side of Swan Street and gradually scales up to Burnley Street, denoting the importance of the station.
- To create a safe and attractive pedestrian environment that protects solar access, enhances ground floor activation along Swan Street and side streets and limits vehicle access from Swan Street to new development.
- To reinforce a consistent built form edge to Swan Street while supporting physical and visual permeability through breaks in built form on the south side of Swan Street.
- To ensure that, along the north side of Swan Street, the heritage buildings remain visually prominent in the streetscape.

#### 2.0

--/--/---  
Proposed C191yara

#### Buildings and works

A permit is required to construct a building or construct or carry out works.

##### 2.1 Definitions

**Street-wall** means the facade of a building at the street boundary, or, if the existing heritage building is set back from the street boundary, the front of the existing building. Street wall height is measured at the vertical distance between the footpath at the centre of the frontage and the highest point of the building at the street edge, with the exception of architectural features and building services.

**Heritage building** means any building subject to a Heritage Overlay, graded as either Contributory or Individually Significant (including properties on the Victorian Heritage Register).

**Laneway** means a road reserve, public highway or right of way 9 metres or less wide.

**Building height** means the vertical distance from natural ground level to the roof or parapet at any point.

**Parapet height** does not include features such as brackets, pediments, urns, finial or other decorative elements.

**Road boundary** means the boundary between the public road and the private property.

**Shared zone** means a road or network of roads where pedestrians, cyclists, and vehicles share the roadway.

**Upper level** means development above the height of the street wall.

##### 2.2 Requirements

A permit cannot be granted to vary a requirement expressed with the term ‘must’ or listed in a ‘Mandatory’ column of a table.

A permit cannot be granted to construct a building or construct or carry out works, which:

- exceeds the mandatory maximum building height and street wall height requirements shown in the Height and Interface Plan 1 of this schedule.
- reduces the mandatory minimum street wall height and upper level setback requirements shown in the Height and Interface Plan 1 of this schedule.

### 2.3 Design Requirements

The following design requirements apply to an application to construct a building or construct or carry out works.

#### Design quality requirements

Development should achieve urban design and architectural excellence, demonstrating improved streetscape outcomes.

Development should be well spaced and sited to avoid visual bulk and provide equitable access to an outlook and good daylight.

Development should provide for street activation at ground levels.

Development should be designed to avoid repetitive stepped form at upper levels.

Upper level development should be designed to ensure buildings provide detail on all facades when viewed from all directions.

Frontages at ground floor should incorporate verandahs, consistent with the form and scale of adjoining verandahs, into the façade design.

Protect the amenity of existing residential properties in terms of visual bulk, overshadowing of private open space, overlooking and vehicle access.

Upper level development on land within a Heritage Overlay or on land immediately adjoining a heritage building should:

- be visually recessive and not visually dominate the heritage building and the heritage streetscape; and
- avoid large expanses of glazing with a horizontal emphasis in the upper levels of development.

#### Building height requirements

A permit should only be granted to construct a building or construct or carry out works, which exceeds the preferred building height shown in the Height and Interface Plan 1 of this schedule where all the following requirements are met to the satisfaction of the responsible authority:

- the building elements permitted by the proposed variation satisfy the general design objectives in Clause 1.0 of this schedule, the relevant precinct design requirements specified in this schedule; and
- the proposal will achieve each of the following:
  - greater building separation than the minimum requirement in this schedule;
  - excellence for environmentally sustainable design measured as a minimum BESS project score of 70%;
  - no additional amenity impacts to residentially zoned properties, beyond that which would be generated by a proposal that complies with the preferred building height;
  - provision of end-of-trip facilities, including secure bicycle parking, locker and shower facilities and change rooms; and
- where the proposal includes dwellings, it also achieves each of the following:
  - housing for diverse household types;



## YARRA PLANNING SCHEME

- accessibility provision objective that exceeds the minimum standards in Clauses 55.07 and 58; and
- communal and/or private open space provision that exceeds the minimum standards in Clauses 55.07 and 58.

Architectural features may exceed the preferred or mandatory height.

Service equipment / structures including plant rooms, lift overruns, structures associated with green roof areas and other such equipment may exceed the preferred or mandatory height provided that each of the following criteria are met for the equipment or structure:

- Less than 50 per cent of the roof area is occupied by the equipment (other than solar panels);
- The equipment does not cause additional overshadowing; and
- The equipment does not extend higher than 3.6 metres above the maximum building height.

### **Street wall and setback requirements**

A permit should not be granted to construct a building or construct or carry out works, which exceeds the relevant preferred maximum street wall height and/or reduces the relevant preferred minimum setback requirements specific in this schedule unless the following are met, to the satisfaction of the responsible authority:

- The built form outcomes that results from the proposed variation satisfy the design objectives in Clause 1.0 of this schedule.
- The built form outcome that results from the proposed variation satisfy the relevant design requirements specific in this schedule.
- The street wall at ground floor level is designed to allow floor to floor ceiling heights suitable to accommodate commercial activity.

Projections such as balconies, building services and architectural features must not intrude into a setback.

On corner sites where two different street wall heights are nominated, buildings should 'turn the corner' and apply the Swan Street wall height. If the Swan Street wall is higher it should transition to the lower nominated street wall height on the side street.

Development should create a sense of enclosure to Swan Street through the height and setback of its street wall.

### **Building separation requirements**

Where development shares a common boundary and no interface treatment is shown in Plan 1, upper level development should:

- For buildings up to 28 metres, be setback a minimum of 4.5m from the common boundary, where a habitable window or balcony is proposed.
- For buildings up to 28 metres, be setback a minimum of 3.0m from the common boundary, where a commercial or non-habitable window is proposed.
- For buildings taller than 28 metres, be setback a minimum of 6 metres above 28 metres.

Where the common boundary is a laneway, the setback is measured from the centre of the laneway.

Development of properties in the locations shown as "Upper Level Building Breaks" on Plan 1 should incorporate side setbacks greater than the setback distances required for upper level development on common boundaries set out above.

Development of properties in the locations shown as "Upper Level Building Breaks" on Plan 1 should enable clear views to the sky between buildings along Swan Street when viewed from the opposite side of Swan Street and along Lord Street and Edinburgh Street.

**Overshadowing requirements**

A permit must not be granted to construct a building or construct or carry out works that would overshadow any of the following spaces between 10 am and 2 pm at 22nd September:

- any part of the southern footpath of Swan Street, measured from the property boundary to the existing kerb.

A permit should not be granted to construct a building or construct or carry out works that would overshadow part of the opposite footpath of Burnley Street, Mary Street and Coppin Street (measured from the property boundary to the existing kerb between 10 am and 2 pm at 22nd September) unless the overshadowing would not unreasonably prejudice the amenity of the public space, to the satisfaction of the responsible authority.

**Vehicle and pedestrian access requirements**

Development should provide vehicular access from rear lanes or from side streets in the preferred locations in the Access and Movement Plan 2 of this schedule except in locations identified as “Left in - Left Out Access Permitted” in the Access and Movement Plan 2 of this schedule.

Development identified as “Left in - Left Out Access Permitted” in the Access and Movement Plan should limit the width of vehicle crossovers and incorporate ‘Left in’ and ‘Left out’ only vehicle access.

Development with redundant vehicle access points to Swan Street and Burnley Street should reinstate the kerb, linemarked parking bays, and relocate any parking signs.

Vehicle ingress and egress into development, including loading facilities and building servicing, should be designed to ensure a high quality pedestrian amenity and limit potential conflict between vehicle movements and pedestrian activity.

Pedestrian access to buildings, including upper level apartments, should be from a street or a shared zone shown on the Access and Movement Plan 2 of this schedule. Where pedestrian access can only be provided from a laneway at the rear of buildings the pedestrian entrance should be setback from the rear laneway and well-lit to enable safe access.

Development should be designed to enhance, activate, and provide passive surveillance to the pedestrian connection between Glass Street and Swan Street as shown on Plan 2.

Development should include north-south access in the locations shown on the Access and Movement Plan 2 to allow for building services and car park access.

**Table 1 – Street Wall Heights and Setbacks for Precinct 3**

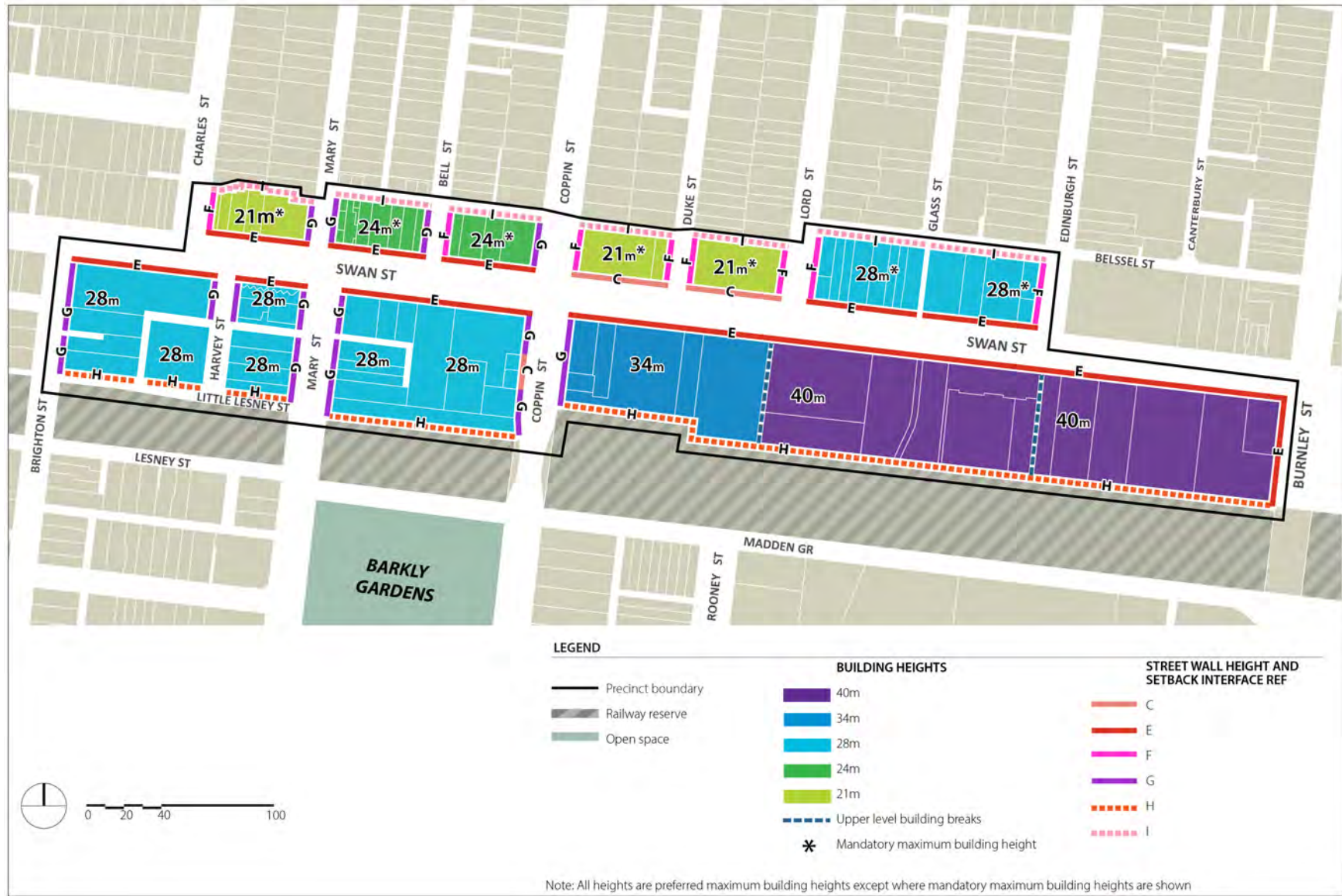
Interface Ref	Design Element	Mandatory Requirements	Preferred Requirements
C	Street wall height		11m maximum. 8m minimum. Match the parapet height of the taller adjoining heritage building, for a minimum length of 6m from the heritage building.
	Street wall setback	0m.	
	Upper level setback	Minimum 6m for heritage buildings.	Minimum 6m elsewhere.
E	Street wall height		14m maximum. Match the parapet height of the taller adjoining heritage building, for a minimum length of 6m from the heritage building.
	Street wall setback		0m.

**YARRA PLANNING SCHEME**

<b>Interface Ref</b>	<b>Design Element</b>	<b>Mandatory Requirements</b>	<b>Preferred Requirements</b>
	Upper level setback	Minimum 6m for heritage buildings.	6m minimum.
F	Street wall height		11m maximum.
	Street wall setback		0m.
	Upper level setback		6m minimum for heritage buildings. 3m minimum elsewhere.
G	Street wall height		14m maximum.
	Street wall setback		0m.
	Upper level setback		3m minimum.
H	Street wall height		N/A.
	Street wall setback		0m minimum unless setback is identified on the Plan 2.
	Upper level setback		0m.
I	Side and rear wall height		8m maximum on a common boundary with a property in a residential zone. 11m maximum if boundary abuts a laneway.
	Side and rear setback		Development should be setback in accordance with Figure 1 and Figure 2. Development should minimise stepped form.

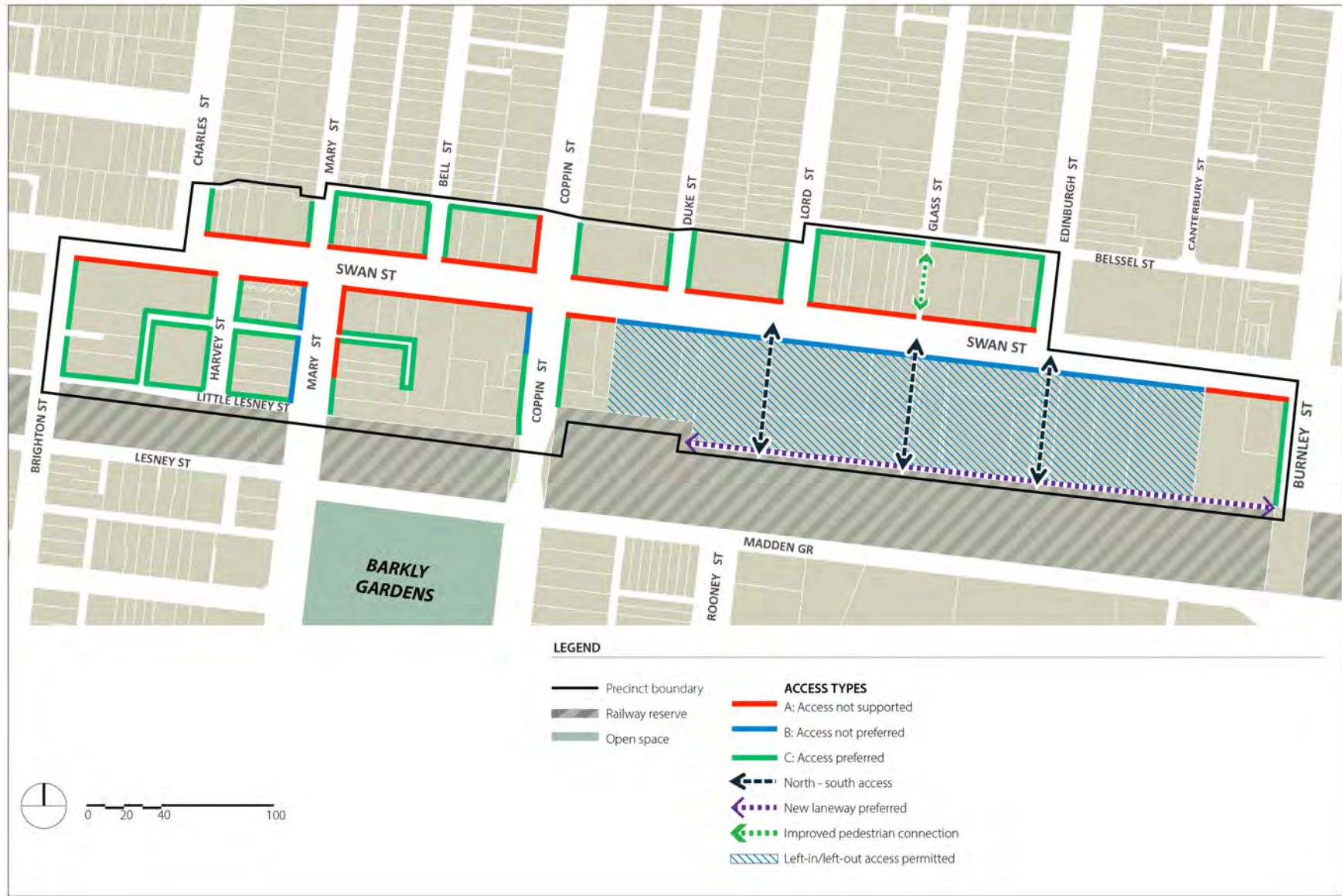
# YARRA PLANNING SCHEME

## Plan 1: Height and Interface Plan – Precinct 3 Swan Street East



# YARRA PLANNING SCHEME

## Plan 2: Access and Movement Plan – Precinct 3 Swan Street East



**3.0 Subdivision**

--/--  
Proposed C191yara None specified

**4.0 Advertising**

--/--  
Proposed C191yara None specified

**5.0 Application requirements**

--/--  
Proposed C191yara An application must be accompanied by:

- A desktop wind effects assessment for the proposed development when the building height is 15m or higher from natural ground level.

**6.0 Decision guidelines**

--/--  
Proposed C191yara The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specific in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

- Whether the Design Requirements in Clause 2.3 are met.
- Whether design excellence is achieved (in terms of building siting, scale, massing, articulation and materials).
- The design of the streetscape interface and its contribution to an active street environment.
- Whether the proposal contributes to and improves the pedestrian connectivity and amenity of the public realm.
- The shadowing impacts of the development on footpaths and public spaces.
- The wind effects created by the development.
- The separation between buildings at upper levels when viewed from the opposite side of Swan Street and from local streets.
- The prominence of the heritage street wall in the vistas along Swan Street, Burnley Street, and local streets.
- Whether heritage buildings on street corners retain their prominence when viewed on both streets.
- Whether heritage buildings retain their three-dimensional form as viewed from the public realm.
- Whether upper level development above the heritage street wall is visually recessive and does not overwhelm the heritage buildings.
- The impact of development on the operation of the tram routes along Swan Street.

YARRA PLANNING SCHEME

Figure 1 to schedule 27 – Interface with an existing laneway

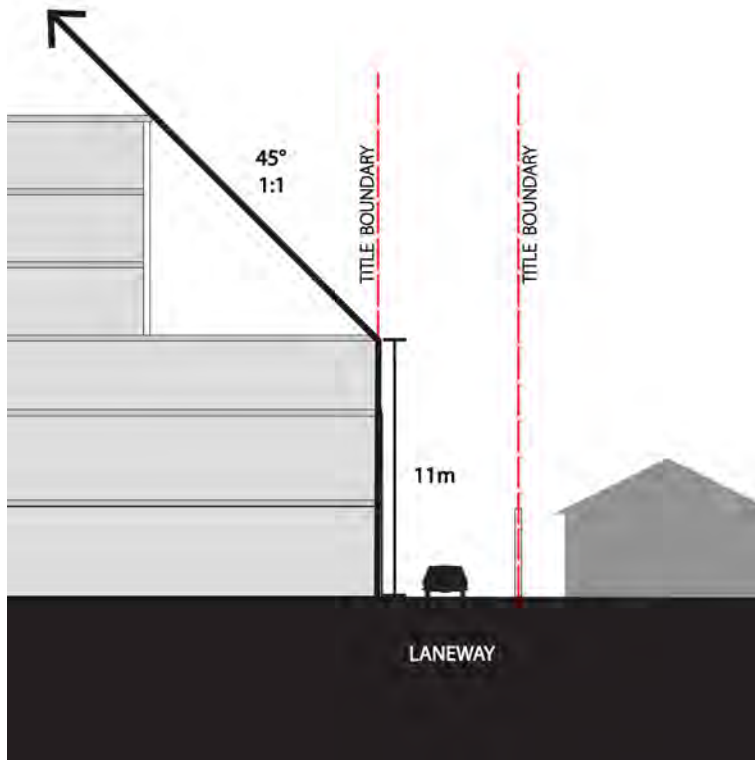
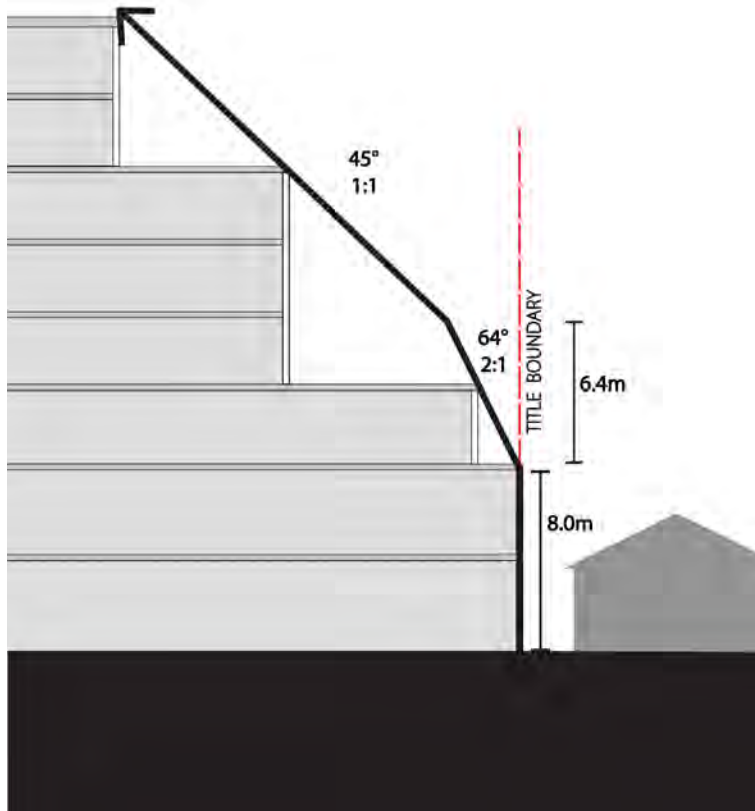


Figure 2 to schedule 27 – Interface direct abuttal



--/--/---  
Proposed C191yara

## SCHEDULE 28 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY

Shown on the planning scheme map as **DDO28**.

### SWAN STREET ACTIVITY CENTRE - PRECINCT 4 BURNLEY STATION

#### 1.0

--/--/---  
Proposed C191yara

#### Design objectives

- To ensure development respects the scale, rhythm, and architectural detail of the streetscape and the heritage buildings in the Burnley Street Heritage Precinct.
- To support a new mid rise scale built form character with lower built form at the interfaces with streets and the adjoining low rise residential areas that maintains an active, high quality and pedestrian friendly environment.
- To reinforce the corners of Swan Street and Burnley Street as a vibrant commercial, retail and residential location.
- To provide for taller development on the south side of Swan Street that delivers significant public realm outcomes and ensure development on both side of Swan Street maintains the amenity of Ryan's Reserve.
- To support high quality new buildings adjoining Burnley Station with mixed uses that activate and improve pedestrian connections to the station.

#### 2.0

--/--/---  
Proposed C191yara

#### Buildings and works

A permit is required to construct a building or construct or carry out works.

##### 2.1 Definitions

**Street-wall** means the facade of a building at the street boundary, or, if the existing heritage building is set back from the street boundary, the front of the existing building. Street wall height is measured at the vertical distance between the footpath at the centre of the frontage and the highest point of the building at the street edge, with the exception of architectural features and building services.

**Heritage building** means any building subject to a Heritage Overlay, graded as either Contributory or Individually Significant (including properties on the Victorian Heritage Register).

**Laneway** means a road reserve, public highway or right of way 9 metres or less wide.

**Building height** means the vertical distance from natural ground level to the roof or parapet at any point.

**Parapet height** does not include features such as brackets, pediments, urns, finial or other decorative elements.

**Road boundary** means the boundary between the public road and the private property.

**Shared zone** means a road or network of roads where pedestrians, cyclists, and vehicles share the roadway.

**Upper level** means development above the height of the street wall.

##### 2.2 Requirements

A permit cannot be granted to vary a requirement expressed with the term 'must' or listed in a 'Mandatory' column of a table.

A permit cannot be granted to construct a building or construct or carry out works, which:



- exceeds the mandatory maximum building height and street wall height requirements shown in the Height and Interface Plan 1 of this schedule.
- reduces the mandatory minimum street wall height and upper level setback requirements shown in the Height and Interface Plan 1 of this schedule.

### **2.3 Design requirements**

The following design requirements apply to an application to construct a building or construct or carry out works.

#### **Design quality requirements**

Development should achieve urban design and architectural excellence.

Development should be well spaced and sited to avoid visual bulk and provide equitable access to an outlook and good daylight.

Development should provide for street activation at ground levels.

Development should be designed to avoid repetitive stepped form at upper levels.

Upper level development should be designed to ensure buildings provide detail on all facades when viewed from all directions.

Frontages at ground floor should incorporate verandahs, consistent with the form and scale of adjoining verandahs, into the façade design.

Protect the amenity of existing residential properties in terms of visual bulk, overshadowing of private open space, overlooking and vehicle access.

Development should maintain the prominence of the heritage street wall and respects the architectural form and qualities of heritage buildings and the heritage streetscape within land affected by HO474.

Upper level development on land within a Heritage Overlay or on land immediately adjoining a heritage building should:

- be visually recessive and not visually dominate the heritage building and the heritage streetscape; and
- avoid large expanses of glazing with a horizontal emphasis in the upper levels of development.

#### **Building height requirements**

A permit should only be granted to construct a building or construct or carry out works, which exceeds the preferred building height shown in the Height and Interface Plan 1 of this schedule where all the following requirements are met to the satisfaction of the responsible authority:

- the built form outcome that results from the proposed variation satisfies the design objectives in Clause 1.0 of this schedule, and the relevant design requirements specified in this schedule; and
- the proposal achieves each of the following:
  - greater building separation than the minimum requirement in this schedule;
  - excellence for environmentally sustainable design measured as a minimum BESS project score of 70%;
  - no additional amenity impacts to residentially zoned properties, beyond that which would be generated by a proposal that complies with the preferred building height;
  - provision of end-of-trip facilities, including secure bicycle parking, locker and shower facilities and change rooms; and
- where the proposal includes dwellings, it also achieves each of the following:

## YARRA PLANNING SCHEME

- housing for diverse households types;
- accessibility provision objective that exceeds the minimum standards in Clauses 55.07 and 58; and
- communal and/or private open space provision that exceeds the minimum standards in Clauses 55.07 and 58.

Architectural features may exceed the preferred or mandatory height.

Service equipment / structures including plant rooms, lift overruns, structures associated with green roof areas and other such equipment may exceed the preferred or mandatory height provided that each of the following criteria are met for the equipment or structure:

- Less than 50 per cent of the roof area is occupied by the equipment (other than solar panels);
- The equipment does not cause additional overshadowing; and
- The equipment does not extend higher than 3.6 metres above the maximum building height.

### **Street wall and setback requirements**

Development along Swan Street must achieve a prominent street wall and provide an appropriate transition in the street wall height to the Burnley Street and Bendigo Street heritage precincts.

A permit should not be granted to construct a building or construct or carry out works, which exceeds the relevant preferred maximum street wall height and/or reduces the relevant preferred minimum setback requirements specified in this schedule unless the following are met, to the satisfaction of the responsible authority:

- The built form outcome that results from the proposed variation satisfies the design objectives in Clause 1.0 of this schedule.
- The built form outcome that results from the proposed variation satisfies the relevant design requirements specified in this schedule.
- The street wall at ground floor level is designed to allow floor to floor ceiling heights suitable to accommodate commercial activity.

Projections such as balconies, building services and architectural features must not intrude into a setback.

On corner sites where two different street wall heights are nominated, buildings should 'turn the corner' and apply the Swan Street wall height. If the Swan Street wall is higher it should transition to the lower nominated street wall height on the side street.

### **Building separation requirements**

Where development shares a common boundary and no interface treatment is shown in Plan 1, upper level development should:

- For buildings up to 28 metres, be setback a minimum of 4.5m from the common boundary, where a habitable window or balcony is proposed.
- For buildings up to 28 metres, be setback a minimum of 3.0m from the common boundary, where a commercial or non-habitable window is proposed.
- For buildings taller than 28 metres, be setback a minimum of 6 metres above 28 metres.

Where the common boundary is a laneway, the setback is measured from the centre of the laneway.

Development of properties in the locations shown as "Upper Level Building Breaks" on Plan 1 should incorporate side setbacks greater than the setback distances required for upper level development on common boundaries set out above.

## YARRA PLANNING SCHEME

Development of properties in the locations shown as “Upper Level Building Breaks” on Plan 1 should enable clear views to the sky between buildings along Swan Street when viewed from the opposite side of Swan Street and along Cutter Street and Bendigo Street.

### Overshadowing requirements

A permit must not be granted to construct a building or construct or carry out works that would overshadow any of the following spaces between 10 am and 2 pm at 22nd September.

- any part of the southern footpath of Swan Street, measured from the property boundary to the existing kerb.

A permit should not be granted to construct a building or construct or carry out works that would overshadow part of the opposite footpath of Burnley Street or Stawell Street (measured from the property boundary to the existing the kerb between 10 am and 2 pm at 22nd September), unless the overshadowing would not unreasonably prejudice the amenity of the public space, to the satisfaction of the responsible authority.

Development should maintain solar access to Ryan’s Reserve as follows:

- Beyond 16m of the eastern boundary of the reserve from 10 am onwards on 22 September.
- At the western boundary of the reserve until 2 pm on 22 September.

### Vehicle and pedestrian access requirements

Development should provide vehicular access from rear lanes or from side streets in the preferred locations in the Access and Movement Plan 2 of this schedule except in locations identified as “Left in - Left Out Access Permitted”.

Development identified as “Left in - Left Out Access Permitted” in the Access and Movement Plan must limit the width of vehicle crossovers and incorporate ‘Left in’ and ‘Left out’ only vehicle access.

Development with redundant vehicle access points to Swan Street and Burnley Street should reinstate the kerb, linemarked parking bays, and relocate any parking signs.

Vehicle ingress and egress into development, including loading facilities and building servicing, should be designed to ensure a high quality pedestrian amenity and limit potential conflict between vehicle movements and pedestrian activity.

Pedestrian access to buildings, including upper level apartments, should be from a street or a shared zone shown on the Access and Movement Plan 2 of this schedule. Where pedestrian access can only be provided from a laneway at the rear of buildings the pedestrian entrance should be setback from the rear laneway and well-lit to enable safe access.

Development of 500 to 506 Swan Street, as shown in Plan 2, should include a rear setback, at ground floor, to facilitate the ongoing function of the laneway and allow for building services and car park access. The setback and laneway should be a minimum width of 6m in total.

Development of 130 to 136 Stawell Street, as shown in Plan 2, should include a rear setback, at ground floor, to facilitate the ongoing function of the laneway and allow for building services and car park access. The setback and laneway should be a minimum width of 3m in total.

**Table 1 – Street Wall Heights and Setbacks for Precinct 4**

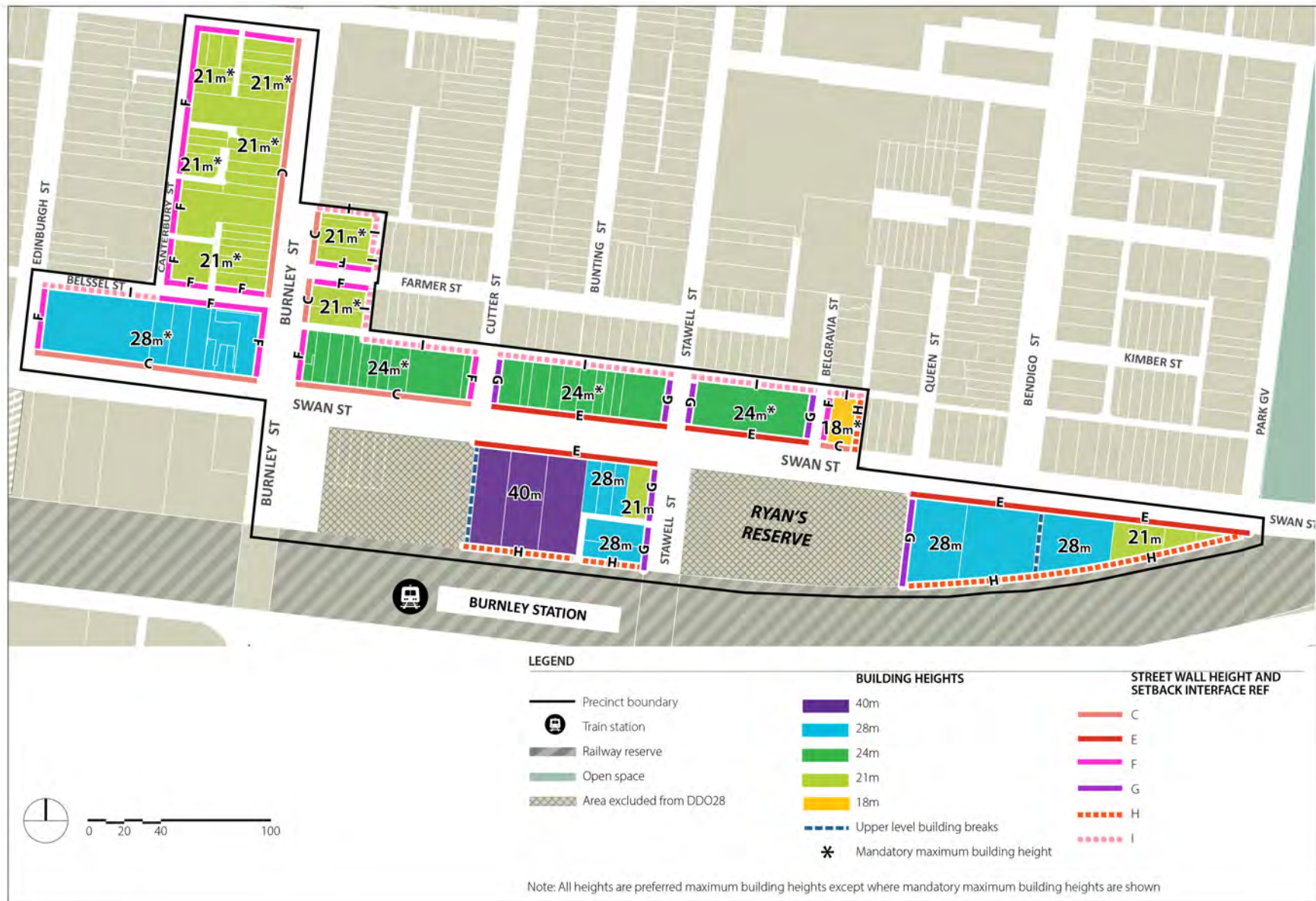
Interface Ref	Design Element	Mandatory Requirements	Preferred Requirements
C	Street wall height		11m maximum. 8m minimum. Match the parapet height of the taller adjoining heritage building, for a minimum length of 6m from the heritage building.

**YARRA PLANNING SCHEME**

<b>Interface Ref</b>	<b>Design Element</b>	<b>Mandatory Requirements</b>	<b>Preferred Requirements</b>
	Street wall setback	0m.	
	Upper level setback	10m minimum from Swan Street setback for land affected by HO286 (365 Swan Street).  Minimum 6m for other heritage buildings.	Minimum 6m elsewhere.
E	Street wall height		14m maximum.
	Street wall setback		0m.
	Upper level setback		6m minimum.
F	Street wall height		11m maximum.
	Street wall setback		0m.
	Upper level setback		6m minimum for heritage buildings. 3m minimum elsewhere.
G	Street wall height		14m maximum.
	Street wall setback		0m.
	Upper level setback		3m minimum.
H	Street wall height		N/A.
	Street wall setback		0m minimum unless setback is identified on the Plan 2.
	Upper level setback		0m.
I	Side and rear wall height		8m maximum on a common boundary with a property in a residential zone.  11m maximum if boundary abuts a laneway.
	Side and rear setback		Development should be setback in accordance with Figure 1 and Figure 2.  Development should minimise stepped form.

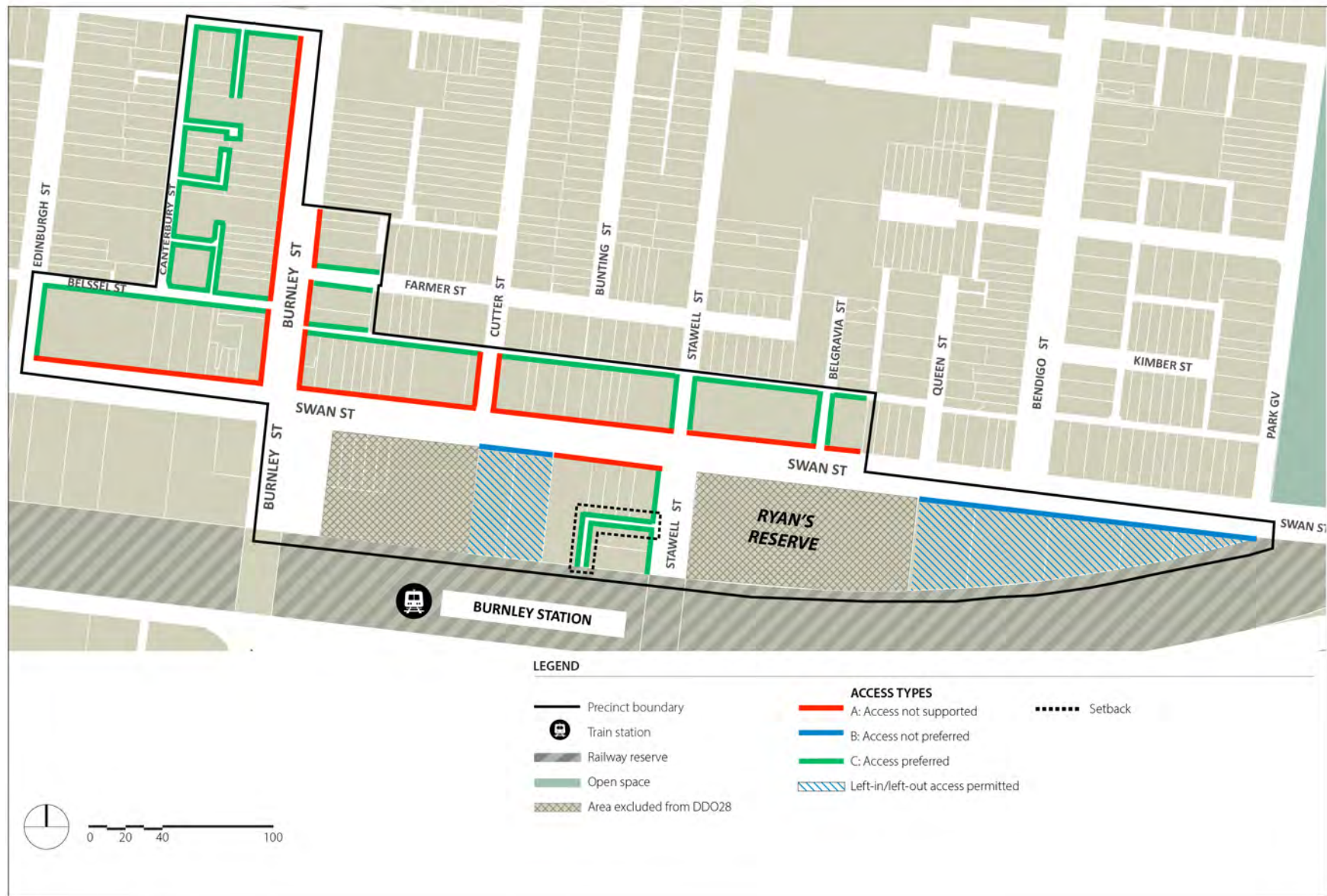
# YARRA PLANNING SCHEME

## Plan 1: Height and Interface Plan – Precinct 4 Burnley Station



# YARRA PLANNING SCHEME

## Plan 2: Access and Movement Plan – Precinct 4 Burnley Station



**3.0 Subdivision**

--/--  
Proposed C191yara None specified

**4.0 Advertising**

--/--  
Proposed C191yara None specified

**5.0 Application requirements**

--/--  
Proposed C191yara An application must be accompanied by:

- A desktop wind effects assessment for the proposed development when the building height is 15m or higher from natural ground level.

**6.0 Decision guidelines**

--/--  
Proposed C191yara The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specific in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

- Whether the Design Requirements in Clause 2.3 are met.
- Whether design excellence is achieved (in terms of building siting, scale, massing, articulation and materials).
- The design of the streetscape interface and its contribution to an active street environment.
- Whether the proposal contributes to and improves the pedestrian connectivity and amenity of the public realm.
- The shadowing impacts of the development on footpaths and public spaces.
- The wind effects created by the development.
- The separation between buildings at upper levels when viewed from the opposite side of Swan Street and from local streets.
- The prominence of the heritage street wall in the vistas along Swan Street, Burnley Street, and local streets.
- Whether heritage buildings on street corners retain their prominence when viewed on both streets.
- Whether heritage buildings retain their three-dimensional form as viewed from the public realm.
- Whether upper level development above the heritage street wall is visually recessive and does not overwhelm the heritage buildings.
- The impact of development on the operation of the tram routes along Swan Street.

YARRA PLANNING SCHEME

Figure 1 to schedule 28 – Interface with an existing laneway

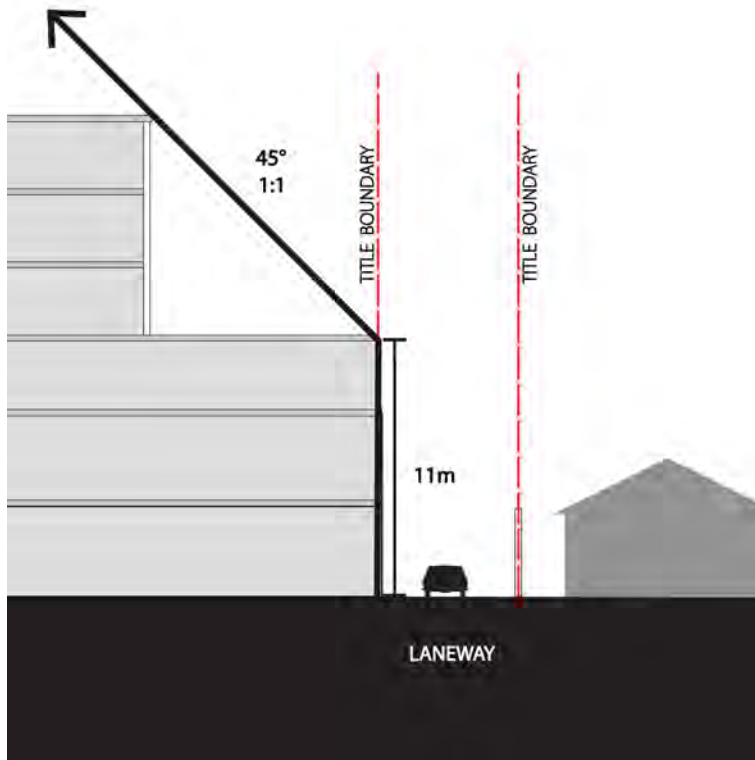
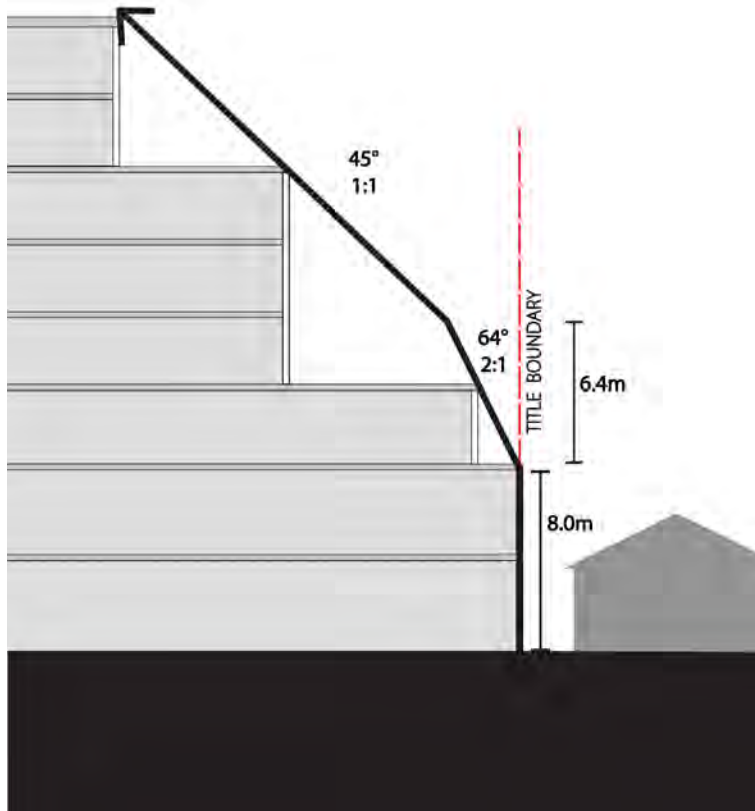


Figure 2 to schedule 28 – Interface direct abuttal



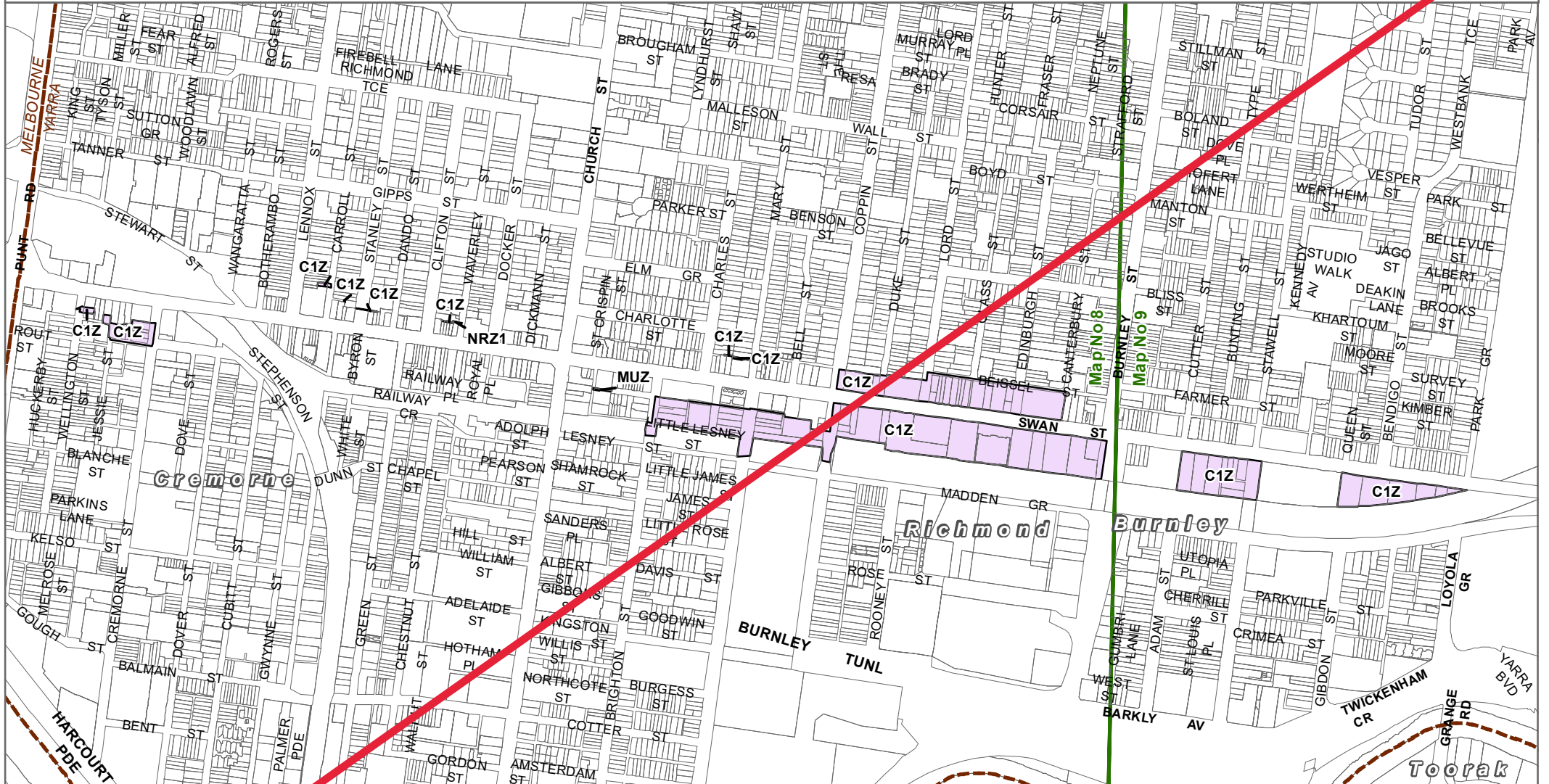


dd/yy/yyyy  
C191**SCHEDULE TO CLAUSE 72.04 DOCUMENTS INCORPORATED IN THIS PLANNING SCHEME****1.0****Incorporated documents**dd/yy/yyyy  
C191

<b>Name of document</b>	<b>Introduced by:</b>
5-15 Mayfield Street, Abbotsford, Incorporated Document, October, 2018	C188
10 Bromham Place, Richmond Incorporated Document, February 2013	C171
18-62 Trenerry Crescent, Abbotsford (Incorporated Plan, May 2018)	C218
32-68 Mollison Street and 61-69 William Street, Abbotsford July 2013	C170
520 Victoria Street, 2A Burnley Street, and 2 – 30 Burnley Street, Richmond, Burnley Street West Precinct - Incorporated Plan, 2012	C150
Amcors Alphington Paper Mill Site Preparation – Incorporated Document, September 2012	C161
Atherton Gardens – Fitzroy, September 2010	C136
Caulfield Dandenong Rail Upgrade Project, Incorporated Document, April 2016	GC37
Chandler Highway Upgrade Incorporated Document, March 2016 (Amended December 2017)	GC80
City of Yarra Database of Heritage Significant Areas, <del>January 2019</del> <u>December 2020</u>	C191 <u>yara</u>
Cremorne Balmain Dover Street Project	NPS1
Crown Land Car Park Works, Burnley, August 2005	C92
Fitzroy Former Gasworks Site, Incorporated Document, February 2018	C242
Flying Fox Campsite, Yarra Bend Park, December 2004	C90
Hurstbridge Rail Line Upgrade 2017 Incorporated Document, January 2017	GC60
Incorporated Plan under the provisions of clause 43.01 Heritage Overlay, Planning permit exemptions, July 2014	C178
Local Policy “Protection of Biodiversity” Sites of Remnant Vegetation (Biosis 2001)	C49
M1 Redevelopment Project, October 2006	C86
Melbourne City Link Project – Advertising Sign Locations, November 2003	VC20
Melbourne Metro Rail Project: Upgrades to the Rail Network Incorporated Document, May 2018	GC96
Planning and Design Principles for the Richmond Maltings Site, Cremorne – November 2007	C101
Richmond Walk Up Estate Redevelopment, September 2010	C136
Social housing redevelopment; Atherton Gardens Estate, Fitzroy, and Richmond Public Housing Estate, Richmond, for which the Minister for Planning is the Responsible Authority, May 2010	C135
Specific Site and Exclusion – Lot 2 on PS433628L (452 Johnston Street, Abbotsford)	C56
Swan Street Works, Burnley, June 2005	C91
Tramway Infrastructure Upgrades Incorporated Document, May 2017	GC68
Victoria Gardens Building Envelope and Precinct Plan and Precinct 3 Plan – Warehouse Area	C7

Name of document	Introduced by:
Victoria Gardens Urban Design Guidelines	NPS1
Victorian Institute of Forensic Psychiatry Concept Plan (January 1997)	NPS1
Yarra Gardens Precinct Plan, December 2009	C128

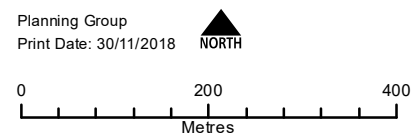
# YARRA PLANNING SCHEME - LOCAL PROVISION AMENDMENT C191



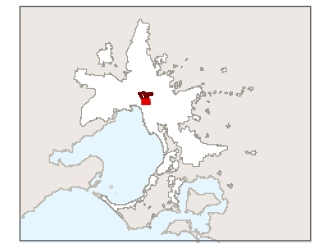
- LEGEND**
- C1Z - Commercial 1 Zone
  - MUZ - Mixed Use Zone
  - NRZ - Neighbourhood Residential Zone
  - Local Government Area

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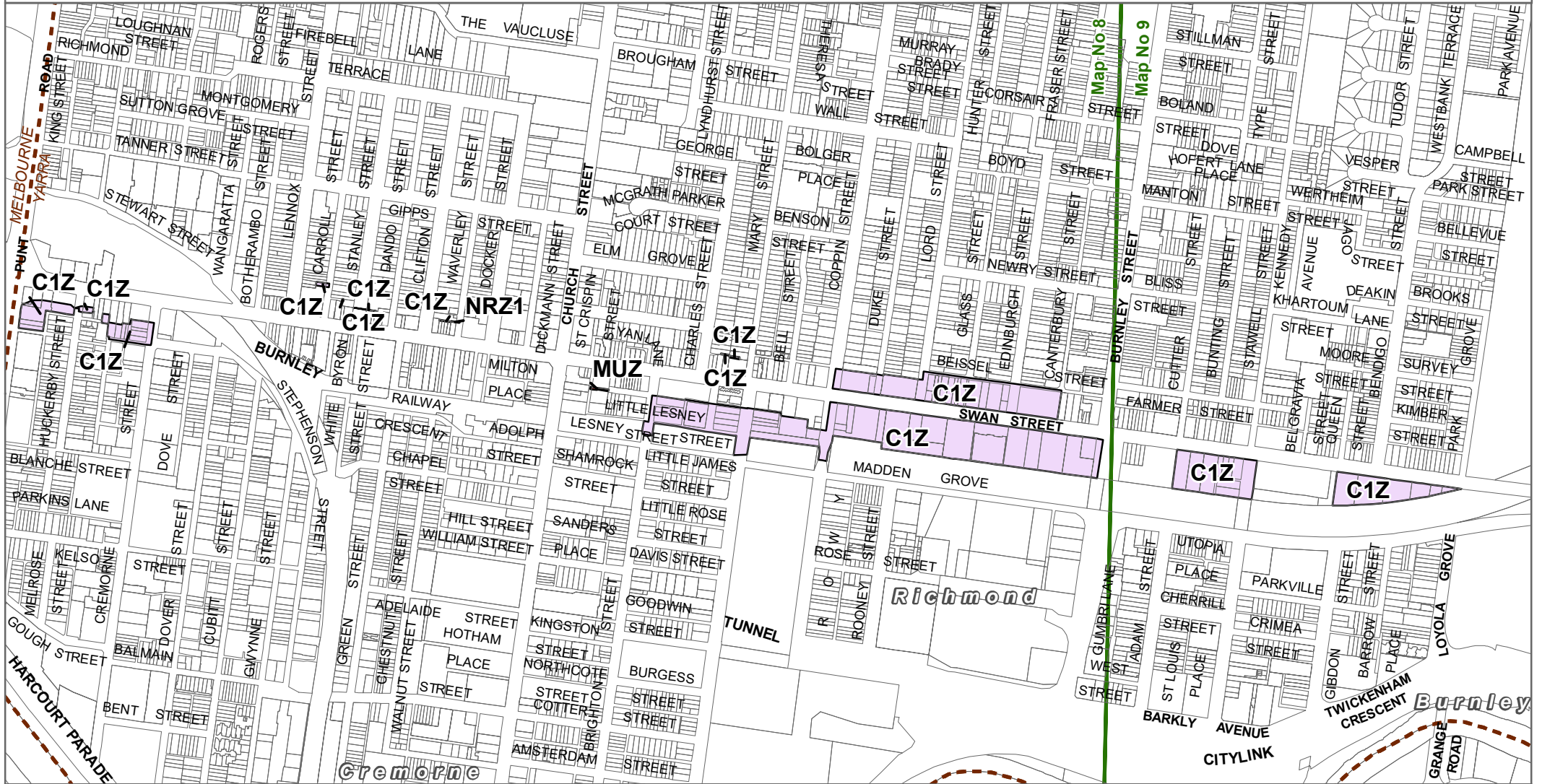
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Part of Planning Scheme Maps 8 & 9



# YARRA PLANNING SCHEME - LOCAL PROVISION AMENDMENT C191yara



- LEGEND**
- C1Z - Commercial 1 Zone
  - MUZ - Mixed Use Zone
  - NRZ - Neighbourhood Residential Zone
  - Local Government Area

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Print Date: 29/10/2020  
Amendment Version: 6

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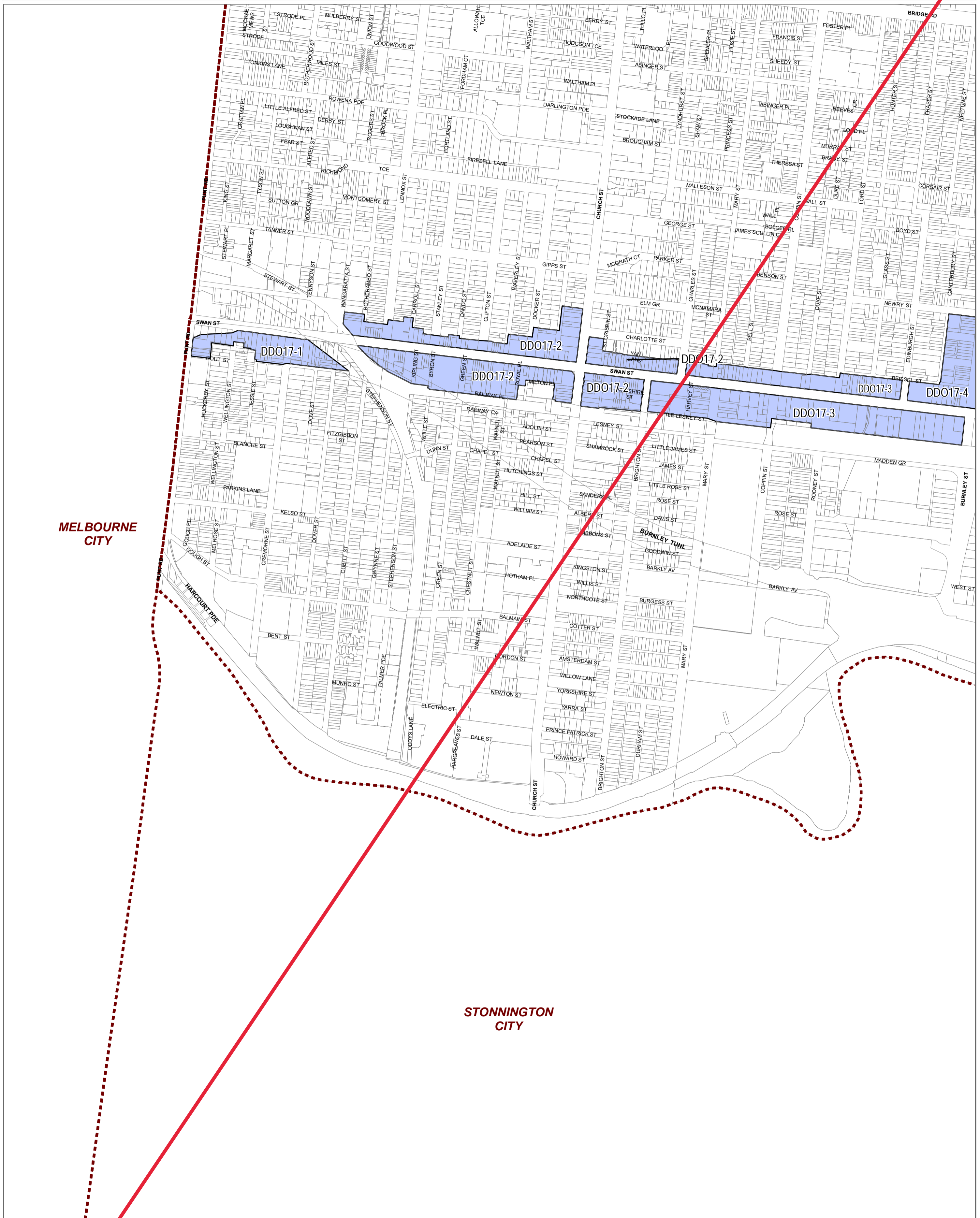
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Part of Planning Scheme Maps 8 & 9



# YARRA PLANNING SCHEME - LOCAL PROVISION AMENDMENT C191



MELBOURNE CITY

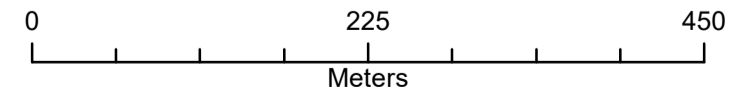
STONNINGTON CITY

**LEGEND**  
 DDO - Design and Development Overlay

Municipal Boundary

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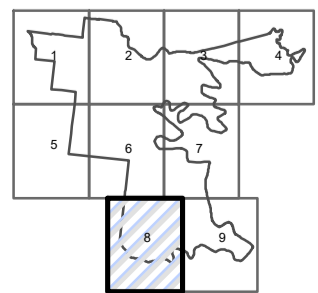
Australian Map Grid Zone 55



Planning Group  
 Printed: 7/01/2019



INDEX TO ADJOINING SCHEME MAPS



# YARRA PLANNING SCHEME - LOCAL PROVISION AMENDMENT C191yara



MELBOURNE CITY

STONNINGTON CITY

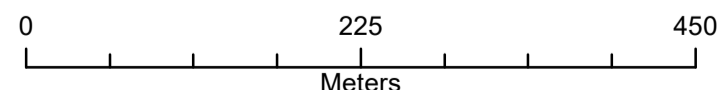
## LEGEND

- DDO25 - Design and Development Overlay - Schedule 25
- DDO26 - Design and Development Overlay - Schedule 26
- DDO27 - Design and Development Overlay - Schedule 27
- DDO28 - Design and Development Overlay - Schedule 28

Municipal Boundary

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Australian Map Grid Zone 55

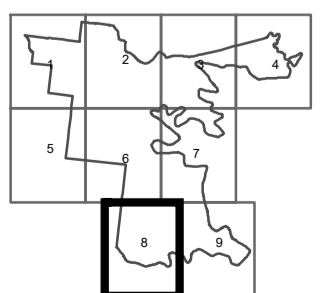


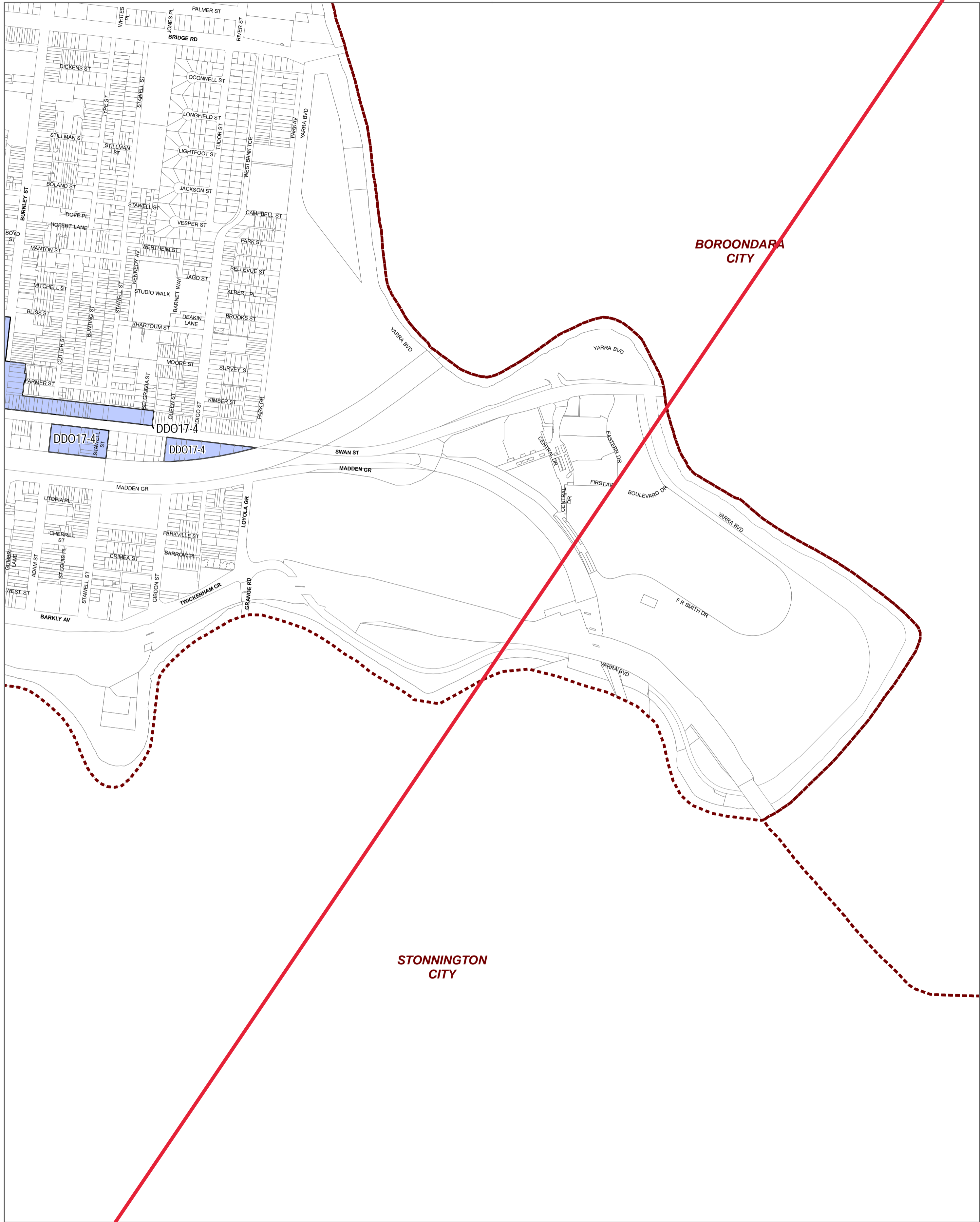
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## INDEX TO ADJOINING SCHEME MAPS



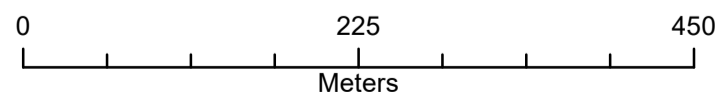


**LEGEND**  
 DDO - Design and Development Overlay

Municipal Boundary

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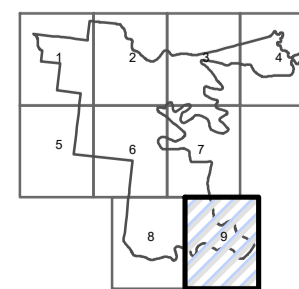
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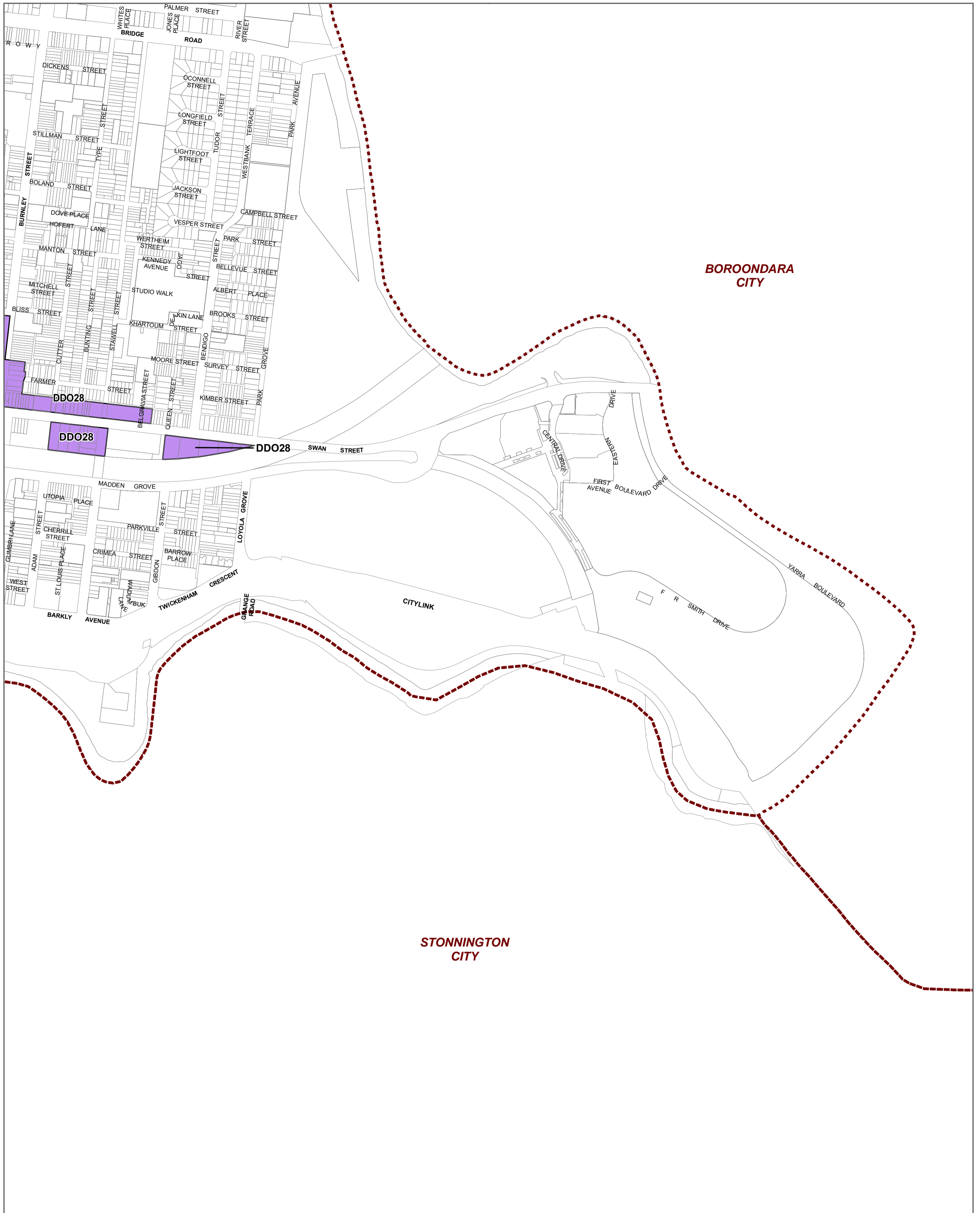
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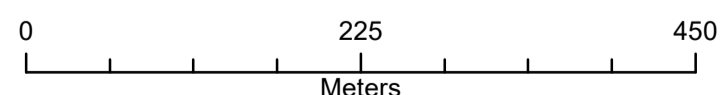


**LEGEND**  
 DDO28 - Design and Development Overlay - Schedule 28

Municipal Boundary

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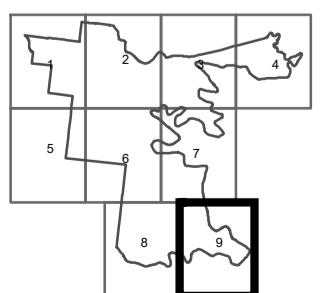


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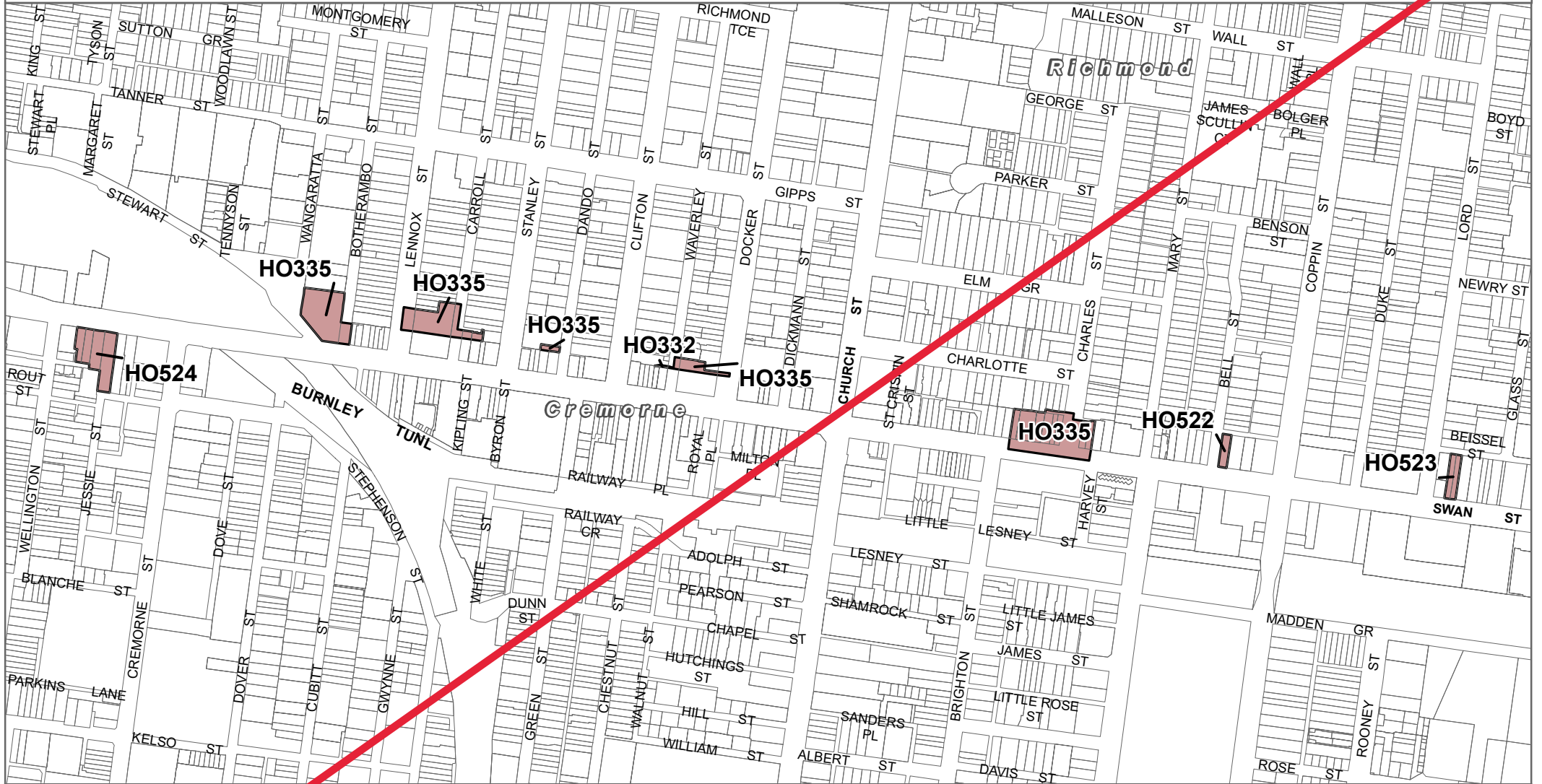
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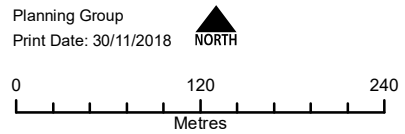
# YARRA PLANNING SCHEME - LOCAL PROVISION AMENDMENT C191



- LEGEND**
- HO - Heritage Overlay
  - Local Government Area

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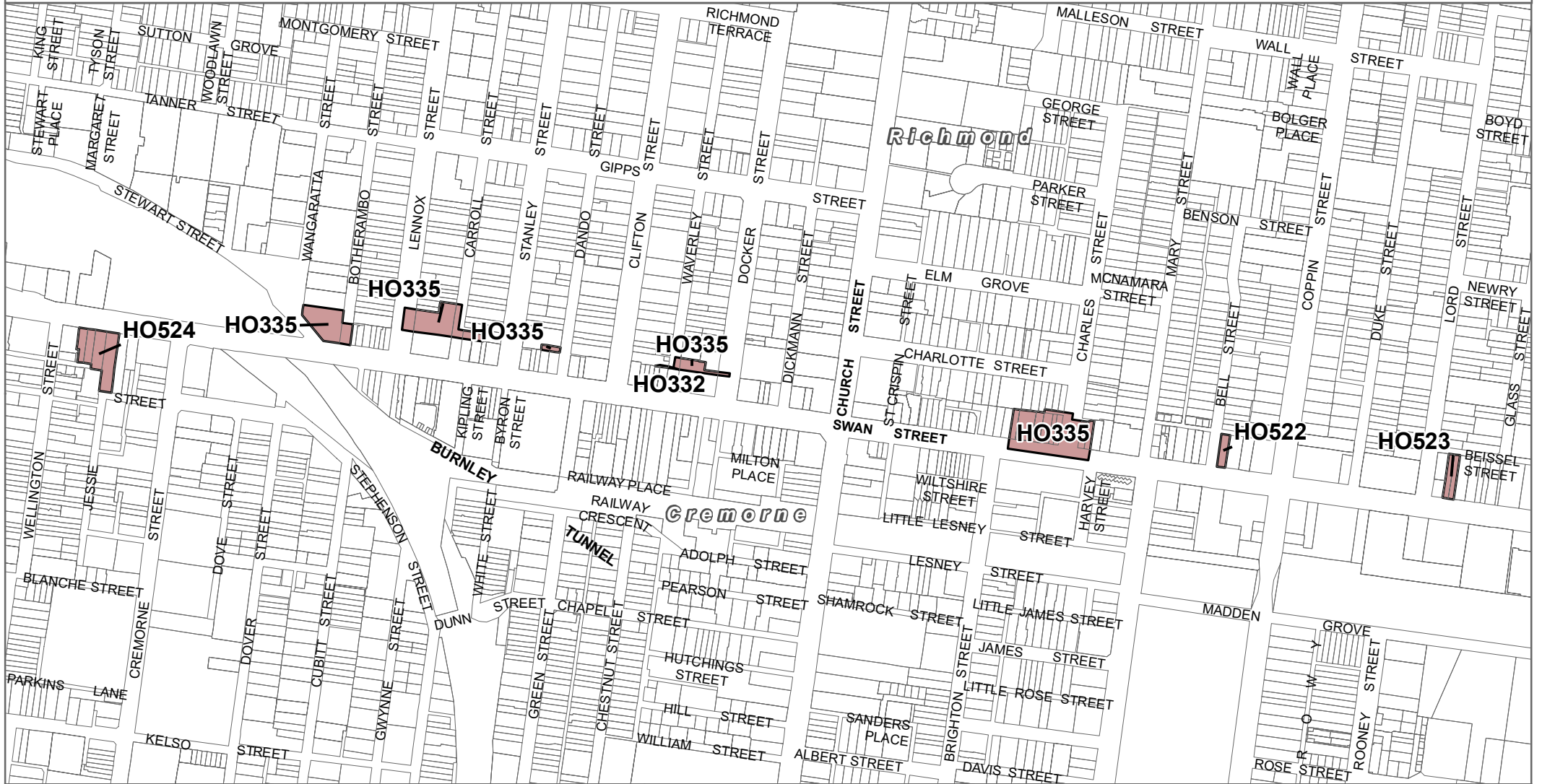
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Part of Planning Scheme Map 8HO



# YARRA PLANNING SCHEME - LOCAL PROVISION AMENDMENT C191yara



## LEGEND

- HO - Heritage Overlay
- Local Government Area

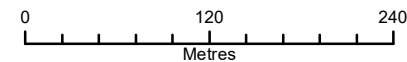
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Part of Planning Scheme Map 8HO



*Planning and Environment Act 1987*

**YARRA PLANNING SCHEME**

**AMENDMENT C191~~yara~~**

**INSTRUCTION SHEET**

The planning authority for this amendment is the Yarra City Council.

The Yarra Planning Scheme is amended as follows:

**Planning Scheme Maps**

The Planning Scheme Maps are amended by a total of ~~eight~~ ten attached maps sheets.

**Zoning Maps**

1. Amend Planning Scheme Map Nos. 8ZN and 9ZN affected in the manner shown on the ~~two~~ three attached maps marked “Yarra Planning Scheme, Amendment C191~~yara~~.”

**Overlay Maps**

2. Amend Planning Scheme Map Nos. 8DDO, 9DDO, ~~8D-DDO, 9D-DDO,~~ 8EAO, 9EAO, 8HO, 8D-HO, ~~8ZN and 9ZN~~ in the manner shown on the ~~eight~~ seven attached maps marked “Yarra Planning Scheme, Amendment C191~~yara~~”.

**Planning Scheme Ordinance**

The Planning Scheme Ordinance is amended as follows:

3. In Local Planning Policy Framework – replace Clause 21.08 with a new Clause 21.08 in the form of the attached document.
4. In Local Planning Policy Framework – replace Clause 21.11 with a new Clause 21.11 in the form of the attached document.
5. In Local Planning Policy Framework – ~~insert new~~ replace Clause 21.12 in the form of the attached document.
6. In Local Planning Policy Framework – replace new Clause 22.02-8 in the form of the attached document
7. In Overlays – Clause 43.01, replace the Schedule with a new Schedule in the form of the attached document.
8. In Overlays – Clause 43.02, delete Schedule 17.
9. In Overlays – Clause 43.02, insert a new Schedule 25 in the form of the attached document.
10. In Overlays – Clause 43.02, insert a new Schedule 26 in the form of the attached document.
11. In Overlays – Clause 43.02, insert a new Schedule 27 in the form of the attached document.
- 7-12. In Overlays – Clause 43.02, insert a new Schedule 28 in the form of the attached document.

~~8. In Overlays – Clause 43.02, insert a new Schedule 17 in the form of the attached document.~~

~~9.13.~~ In Operational Provisions – Clause 72.04, replace the Schedule with a new Schedule in the form of the attached document.

End of document

## YARRA PLANNING SCHEME

### AMENDMENT C191 yara

#### EXPLANATORY REPORT

##### Who is the planning authority?

This Amendment ~~has been~~was prepared by the Yarra City Council, which is the planning authority for this amendment.

##### Land affected by the amendment

The Amendment applies to land within the Swan Street Activity Centre as shown in Figure 1 below.

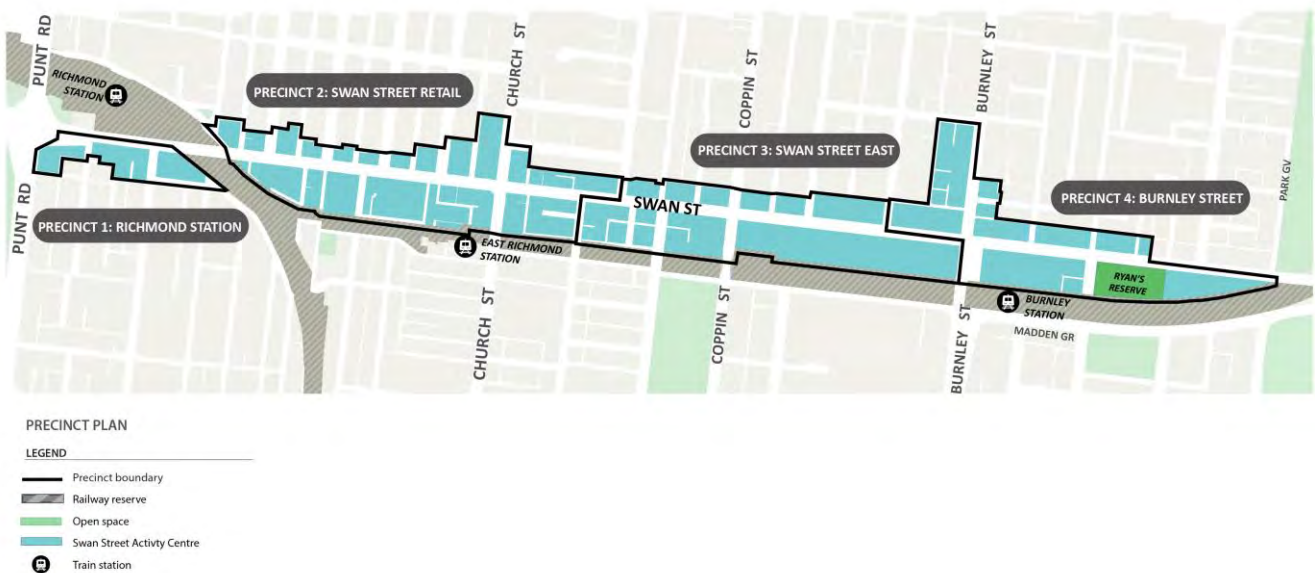


Figure 1: Swan Street Activity Centre

##### What the amendment does

The Amendment C191 ~~proposes to~~ implements the recommendations of the following strategic planning work:

- Built form principles and objectives of the *Swan Street Activity Centre Built Form Framework*, September 2017 (Framework). The framework reviews and builds upon the strategic work undertaken by Council to date, including the Swan Street Structure Plan, January 2014 (Structure Plan); and
- Heritage recommendations of the *Swan Street Built Form Study Heritage Assessments & Analysis*, October 2017.

The Amendment C191 ~~seeks to~~:

- Inserts a new Schedule to Clause 43.02 Design and Development Overlay (~~DDO47~~DDO25) on a permanent basis, which applies to Precinct 1: Richmond Station ~~land~~ in the Swan Street Activity Centre ~~to guide built form within the centre~~.
- Inserts a new Schedule to Clause 43.02 Design and Development Overlay (DDO26) on a permanent basis, which applies to Precinct 2: Swan Street Retail in the Swan Street Activity Centre.

- Inserts a new Schedule to Clause 43.02 Design and Development Overlay (DDO27) on a permanent basis, which applies to Precinct 3: Swan Street East in the Swan Street Activity Centre.
- Inserts a new Schedule to Clause 43.02 Design and Development Overlay (DDO28) on a permanent basis, which applies to Precinct 4: Burnley Station in the Swan Street Activity Centre.
- Deletes Schedule 17 to Clause 43.02 Design and Development Overlay (DDO17), which applies to the Swan Street Activity Centre on an interim basis.
- Amends ~~Inserts a new Swan Street Activity Centre~~ Local Area Policy at Clause 21.12 to insert a new section on implement the Swan Street Activity Centre ~~Built Form Framework 2017 and~~ to guide land use and built form within the activity centre.
- Amends Clause 21.08 (Neighbourhoods) to make minor corrections and reflect the application of the DDO17 and the Swan Street Activity Centre Local Area Policy.
- Amends Clause 21.11 Reference Documents to include the ~~Swan Street Activity Centre Built Form Framework 2017~~, *Swan Street Built Form Study Heritage Assessments & Analysis*, October 2017 (GJM Heritage) and *Yarra High Streets: Statements of Significance*, ~~October 2017~~ December 2020 (GJM Heritage) as reference documents in the planning scheme.
- Amends the Schedule to Clause 72.04 Ddocuments ~~l~~incorporated in this Planning Sscheme ~~Table of documents incorporated in this scheme~~ to include the new incorporated document titled "City of Yarra Database of Heritage Significant Areas (~~January 2019~~ December 2020)".
- Amends Clause 22.02 Development Guidelines for Sites Subject to the Heritage Overlay to include the *Swan Street Built Form Study Heritage Assessments & Analysis*, October 2017 (GJM Heritage) and *Yarra High Streets: Statements of Significance*, ~~October~~ December 2017-2020 (GJM Heritage) as reference documents.
- Amends the Schedule to Clause 43.01 Heritage Overlay and Map No. 8HO to apply heritage controls to the following 15 places on a permanent basis:
  - 30-42 Swan Street, Cremorne (new Swan Street West Precinct HO524)
  - 273A Swan Street, Richmond (new HO522)
  - 323-325 Swan Street, Richmond (new HO523)
  - 223-239 Swan Street, Richmond (include into Swan Street Precinct HO335)
- Amends Planning Scheme Map No. 8HO to remove the following ungraded 4 places from the Heritage Overlay in the activity centre:
  - The rear car park of 57-61 Swan Street (the Corner Hotel) (HO332)
  - 375 Punt Road, Cremorne (Wellington Street Precinct ~~HO364~~ HO293)
  - 377 Punt Road, Cremorne (Wellington Street Precinct ~~HO364~~ HO293)
  - 416-418 Church Street, Richmond (Church Street Precinct HO315)
  - Units 5-0 / 3 Green Street, Richmond (Swan Street Precinct HO335)
- Amends Planning Scheme Map No. 8HO by removing the Heritage Overlay (HO332) from 57-61 Swan Street (the Corner Hotel) and replacing with Heritage Overlay (HO335), excluding the rear car park.
- Amends Map Nos. 8DDO and 9DDO to ~~apply delete interim~~ Schedule 17 to the Design and Development Overlay to land in the Swan Street Activity Centre.
- Amends Map Nos. 8DDO and 9DDO to apply Schedules 25, 26, 27 and 28 to the Design and Development Overlay to land in the Swan Street Activity Centre.

- Amends s Map No. 8ZN and 9ZN to rezone land within the Commercial 2 Zone (C2Z) to Commercial 1 Zone (C1Z) to facilitate additional retail and housing in the activity centre.
- Amends s Map Nos. 8EAO and 9EAO to apply the Environmental Audit Overlay to potentially contaminated land in the Swan Street Activity Centre.
- Amends s Maps Nos. 8EAO, 8ZN, 9ZN and 8HO to correct a number of mapping anomalies.

## Strategic assessment of the amendment

### Why is the amendment required?

Amendment C191 ~~yara is required to~~ manages s and responds s to increased development activity along the Swan Street Activity Centre area.

As a designated major activity centre, Swan Street, Richmond is required to play a significant role in achieving the directions of Plan Melbourne 2017-2050, in relation to both housing and employment. Council's recently adopted Housing Strategy also seeks to direct new housing to areas within or close to activity areas that have good access to public transport, open space, and other services and limit housing growth in established residential areas, consistent with Plan Melbourne, state and regional policy

Swan Street, Richmond is a highly intact turn of the century 'high street' of consistent scale and architectural quality. ~~The~~ ~~Currently, the centre does not have~~ permanent built form planning controls ~~to~~ ensure that new development has regard to the potential impacts on the heritage significance of Swan Street, or the surrounding low scale (heritage) residential neighbourhoods.

### Design and Development Overlay Schedule ~~17-s 25, 26, 27, and 28~~

Amendment C191 ~~yara proposes to~~ implements s the built form recommendations of the Swan Street Activity Centre Built Form Framework, September 2017 (framework) through the introduction of Schedules ~~17-25, 26, 27 and 28~~ to the Design and Development Overlay (~~DDO17~~ DDO25, DDO26, DDO27 and DDO28) on a permanent basis. The framework ~~has been~~ was prepared by Tract (urban design) with extensive input from GJM Heritage (heritage) and Traffix Group (access and movement). It implements and builds upon the vision and key principles for change outlined in the Swan Street Structure Plan and provides s a strong strategic basis for the future planning of the area.

~~DDO25, DDO26, DDO27 and DDO28 each include precinct specific controls~~ ~~DDO17 includes general and precinct controls that respond to the changing context along Swan Street.~~ Mandatory heights are ~~proposed to be~~ applied to the majority of Precinct 2 in response to the Precinct's intact heritage streetscape and also to Precincts 3 and 4 to protect the amenity of adjoining low scale residential areas. The framework ~~is~~ was a comprehensive piece of strategic work that demonstrates s mandatory controls are appropriate in this context and necessary to achieve the preferred built form outcomes.

Discretionary height controls ~~are proposed to be~~ are applied to land outside of the Heritage Overlay, where there are no sensitive interfaces, for example, land on the south side of Swan Street that is in close proximity to the railway line and commercial land (Precincts 1, 3 and 4). These are appropriate locations for discretionary controls to provide more flexibility to accommodate contextual variations and innovative design. Where discretionary building heights ~~are were proposed in DDO17 applied,~~ a range of performance-based provisions ~~would~~ need to be met to ensure appropriate development.

Importantly, the DDO schedules provides s built form certainty where there are heritage, residential amenity, and public realm sensitivities and allows s for some discretion where there are fewer site constraints. Amendment C191 ~~yara will~~ facilitates s development appropriate to a major activity centre, whilst ensuring that new development is site responsive, and improved amenity outcomes are achieved.

### Heritage Overlay

Amendment C191 ~~yara~~ gives effect to the heritage recommendations of the Swan Street Built Form Study Heritage Assessments & Analysis (GJM Heritage, October 2017), including applying the Heritage Overlay to 15 places of heritage significance and removing 4 places from the Heritage Overlay, as they do not contribute to the significance of their broader heritage precincts.

The assessment ~~has also~~ recommended that Appendix 8 of the Yarra Planning Scheme be amended to identify 3 places as 'not contributory' to the Swan Street Precinct, Richmond (HO335) and the Richmond Hill Precinct (HO332) as they have undergone substantial alterations, which have irrevocably reduced their heritage significance.

The assessment ~~has also~~ recommended ~~removing the place at 57 Swan Street (the Corner Hotel) from the Richmond Hill Precinct (HO332) and including 57-61 Swan Street it as an individually significant place~~ within the Swan Street Precinct (HO335), ~~as the hotel has formed an important part of the social and reactional history of Swan Street.~~

#### Rezoning Commercial 2 Zone to Commercial 1 Zone

The land along Swan Street ~~is was~~ zoned a combination of C1Z and C2Z. Residential development is prohibited on land within the C2Z. ~~It is proposed to rezone a~~ All remaining C2Z land along Swan Street ~~will are to be has now been rezoned~~ to C1Z. This rezoning ~~is was~~ required to provide the opportunity for residential development along the length of Swan Street. Rezoning the remaining C2Z land to C1Z ~~was is~~ appropriate as it ~~will~~ provides an incentive for further renewal and mixed-use redevelopment within the centre, consistent with State and metropolitan planning policy.

#### Environmental Audit Overlay

The ~~proposed~~ rezoning of the land from C2Z to C1Z ~~would~~ allows for residential uses, which are defined as sensitive uses. ~~It is proposed that t~~ The Environmental Audit Overlay (EAO) ~~will be is was~~ applied to land ~~(which is to be transitioned to a sensitive use)~~ assessed as having high potential for contamination.

~~The City of Yarra considers that it is important that the application of the an EAO~~ The application of an EAO ~~must~~ be undertaken in accordance with relevant policy, including The Department of Sustainability and Environment (DSE) General Practice Note – Potentially Contaminated Land (PPN 30). As such, Council ~~has undertaken~~ ~~undertook~~ an assessment (in-line PPN30) to determine the risk of contamination on a site-by-site basis ~~and has subsequently amended the proposed application of the EAO.~~

The application of the EAO ~~is was~~ based on:

- Site inspections to determine current land uses and activities;
- Whether the EAO already applies to some or all of the site;
- Current Zoning of a site;
- City of Richmond rate register: Years 1920-21, 1945-46, 1960-61, 1971-72;
- Sands & McDougall's directory of Victoria - Melbourne & suburban profession & trade directory: Years 1900, 1905, 1910, 1920, 1930, 1940, 1950, 1955, 1960 and 1970;
- Review of the EPA Priority Site Register; and
- A site's proximity to land uses and activities with high potential for contamination.

~~T~~ Council ~~considers that t~~ the application of the Environmental Audit Overlay (EAO) to the nominated sites ~~is was~~ required to address any potential site contamination issues to meet the requirements of Ministerial Direction No. 1 – Potentially Contaminated Land. The potential for contamination results from past and current industrial activities in on these sites.

The ~~introduction of an~~ EAO ~~would~~ requires an environmental audit to be undertaken on the land and for certificate or statement to be obtained from an environmental auditor in accordance with the Environment Protection Act 1970. The audit is carried out prior to the construction of the buildings and works in association with a dwelling or other sensitive use.

#### Correction of mapping errors

Amendment C191 ~~yara proposes to~~ corrects a number of historical mapping errors. There ~~are was~~ a large number of mapping anomalies in the Swan Street Activity Centre where two zones ~~app~~ applied to a single site. The existence of dual-zones occurs for a number of reasons, mostly due to historical mapping errors. Dual zoned sites should be corrected in order to 'provide for the fair, orderly,



economic and sustainable use and development of land' in accordance with the objectives (in part) of the Planning and Environment Act 1987. ~~Once~~ Now that the anomalies ~~are~~ have been corrected, the land can be developed and used in accordance with the zone provisions.

### **How does the amendment implement the objectives of planning in Victoria?**

The Amendment implements the objectives in Section 4 of the Planning and Environment Act 1987 (the Act), in particular:

- a) *to provide for the fair, orderly, economic and sustainable use, and development of land;*
- c) *to secure a pleasant, efficient and safe working, living and recreational environment; and*
- d) *to conserve and enhance those buildings, areas or other places which are of scientific, aesthetic, architectural or historical interest, or otherwise of special cultural value;*
- g) *to balance the present and future interests of all Victorians.*

The Amendment ~~will~~ facilitates housing growth as well as economic growth and creates a more economically viable mixed-use activity centre that has economic benefits for the local area.

### **How does the amendment address any environmental, social and economic effects?**

The Amendment ~~seeks to~~ integrates relevant environmental, social and economic factors in the interests of net community benefit and sustainable development.

The Amendment ~~is expected to~~ generate positive social and economic benefits as it ~~will~~ facilitates development within a major activity centre, providing opportunities for economic development and housing growth. The Amendment ~~will also~~ responds to the local demand for housing and provide housing and employment in a location, which has strong access to public transport infrastructure and social services.

The Amendment addresses any environmental effects or risks through the application of the Environmental Audit Overlay.

### **Does the amendment address relevant bushfire risk?**

The land affected by the Amendment is not located within an area of identified bushfire risk.

### **Does the amendment comply with the requirements of any Minister's Direction applicable to the amendment?**

The Amendment complies with Ministerial Direction No. 1 in addressing the risk from potentially contaminated land.

The Amendment is consistent with the Ministerial Direction on the Form and Content of Planning Schemes under section 7(5) of the Act and Direction 11 - Strategic Assessment under Section 12(2) of the Act. During the Amendment process, Direction No. 15 – The Planning Scheme Amendment Process, which sets times for completing steps in the Amendment process, ~~was~~ will also be followed.

The Amendment ~~has also been~~ was prepared with regard to Ministerial Direction No. 9 Metropolitan Planning Strategy (which refers to Plan Melbourne 2017-2050). Plan Melbourne 2017-2050 identifies a vision for the future of Melbourne and objectives and outcomes sought for the city, with directions identified to achieve the desired outcomes and objectives.

The Amendment is consistent with the following Directions contained in Plan Melbourne 2017-2050:

- Direction 1.1 – Create a city structure that strengthens Melbourne's competitiveness for jobs and investment, which seeks to strengthen the competitiveness of Melbourne's employment land. The Amendment provides appropriate policy direction for the planning and development of the Swan Street Activity Centre to ensure that the activity centre continues to meet community needs.

- Direction 5.1- Create a city of 20-minute neighbourhoods which aims to cluster new housing in activity centres and other places that offer good access to jobs, services and public transport and includes a policy for local governments to prepare structure plans for activity centres to accommodate growth. The amendment ~~will~~ facilitates the renewal of a major activity centre which ~~will~~ improve local employment, housing and commercial opportunities.

The Amendment is consistent with Ministerial Direction 19, as the City of Yarra (as the planning authority) sought the written views of the EPA about the potential impacts of the proposed amendment on the environment, amenity, and human health and has included a statement, in this Explanatory Report, of how the proposed amendment addresses the final views of the EPA.

### **How does the amendment support or implement the Planning Policy Framework and any adopted State policy?**

The amendment supports and implements relevant objectives of the State Planning Policy Framework (SPPF) including:

Clause 11 – Settlement provides context and implements the key principles of Plan Melbourne 2017-2050, which include providing for housing choice and affordability by planning for expected housing needs and providing for reduced ongoing living costs by increasing housing supply near public transport and services. It also encourages the consolidation of residential activities within existing urban areas and development in existing residential areas. The Amendment provides a framework for the orderly planning and high-quality development of the Swan Street Activity Centre in a manner consistent with the directions of Plan Melbourne 2017-2050.

Clause 15 – Built Environment and Heritage seeks to create urban environments that are safe, functional and provide good quality environments with a sense of place and cultural identity. This Clause also sets out the importance of ensuring the conservation of places, which have identified heritage significance. The Amendment supports this clause by providing appropriate built form guidance to ensure that development is site responsive and appropriate in the context of heritage places.

Clause 16 – Housing emphasises the importance of providing enough quality and diverse housing that meets the growing diverse needs of Victorians in locations in or close to activity centres and sites that offer good access to jobs, services and transport. It requires councils to identify areas that offer opportunities for more medium and high-density housing near employment and transport in Metropolitan Melbourne. The Amendment provides strategic guidance on the appropriate scale of development including housing in the Activity Centre.

Clause 17 – Economic Development seeks to encourage development which meets the community's needs for retail, entertainment, office and other commercial services and provides a net community benefit in relation to accessibility, efficient infrastructure use and the aggregation and sustainability of commercial facilities. The Amendment supports this clause by facilitating opportunities for a mix of office, retail, and residential uses throughout the centre.

Clause 18 – Transport promotes the creation of a safe and sustainable transport system and promotes the use of sustainable personal transport. The Amendment implements the objectives of this clause by facilitating development in an Activity Centre, which is well serviced by public transport.

### **How does the amendment support or implement the Local Planning Policy Framework, and specifically the Municipal Strategic Statement?**

The vision for the City of Yarra in the existing Municipal Strategic Statement in relation to built form is that by 2020, all new development will demonstrate design excellence.

The Amendment is consistent with and facilitates the following Clauses of the Local Planning Policy Framework:

- *Clause 21.04-1 - Accommodation and Housing*
- *Objective 1 - To accommodate forecast increases in population.*

- *Strategy 1.2 Direct higher density residential development to Strategic Redevelopment Sites identified at clause 21.08 and other sites identified through any structure plans or urban design frameworks.*
- *Clause 21.04-2 - Activity centres*
  - *Objective 4 - To maintain a balance between local convenience and regional retail roles in Yarra's activity centres.*
    - *Strategy 4.1 Increase the range of retail, personal and business services, community facilities, and recreation activities, within individual centres.*
    - *Strategy 4.3 Support the role of all activity centres, including Neighbourhood Activity Centres, in providing local day-to-day needs of residents of all abilities.*
  - *Objective 5 - To maintain the long term viability of activity centres.*
    - *Strategy 5.2 Support land use change and development that contributes to the adaptation, redevelopment and economic growth of existing activity centres.*
    - *Strategy 5.3 Discourage uses at street level in activity centres which create dead frontages during the day.*
    - *Strategy 5.4 Permit residential development that does not compromise the business function of activity centres.*
- *Clause 21.05-1 - Heritage*
  - *Objective 14 - To protect and enhance Yarra's heritage places.*
    - *Strategy 14.1 Conserve, protect and enhance identified sites and areas of heritage significance including pre-settlement ecological heritage.*
    - *Strategy 14.2 Support the restoration of heritage places.*
    - *Strategy 14.3 Protect the heritage skyline of heritage precincts.*
    - *Strategy 14.6 Protect buildings, streetscapes and precincts of heritage significance from the visual intrusion of built form both within places and from adjoining areas.*
- *Clause 21.05-2 - Urban design*
  - *Objective 16 - To reinforce the existing urban framework of Yarra.*
    - *Strategy 16.2 Maintain and strengthen the preferred character of each Built Form Character Type within Yarra.*
  - *Objective 19 - To create an inner city environment with landscaped beauty.*
    - *Strategy 19.1 Require well resolved landscape plans for all new development.*
    - *Strategy 19.2 Encourage opportunities for planting suitable trees and landscape areas in new development.*
  - *Objective 20 - To ensure that new development contributes positively to Yarra's urban fabric.*
    - *Strategy 20.1 Ensure development is designed having particular regard to its urban context and specifically designed following a thorough analysis of the site, the neighbouring properties and its environs.*
  - *Objective 21 - To enhance the built form character of Yarra's activity centres.*
    - *Strategy 21.1 Require development within Yarra's activity centres to respect and not dominate existing built form.*

- *Strategy 21.2 Require new development within an activity centre to consider the context of the whole centre recognising that activity centres may consist of sub-precincts, each of which may have a different land use and built form character.*
- *Strategy 21.3 Support new development that contributes to the consolidation and viability of existing activity centres.*

### Does the amendment make proper use of the Victoria Planning Provisions?

The Amendment makes suitable use of the Victoria Planning Provisions (VPP) through the selection of appropriate tools to achieve guidance for future land use and development outcomes for the centre.

### How does the amendment address the views of any relevant agency?

Council sought the views of the then Department of Planning and Community Development (now DELWP) VicRoads, Department of Transport (DoT) and Yarra Trams in the drafting of the Swan Street Structure Plan which informs the Amendment. The views of VicRoads and Transport for Victoria ~~have also been were~~ sought in the drafting of the Design and Development Overlay as it relates to vehicular access.

In accordance with Ministerial Direction No. 19, the City of Yarra, as the planning authority, sought the written views of the EPA about the potential impacts of the ~~proposed~~ amendment on the environment, amenity, and human health. The Environment Protection Authority Victoria (EPA) ~~is was~~ generally supportive of the ~~proposed~~ Amendment, subject to the City of Yarra addressing the further comments made in respect to the matrix of land contamination and associated application of the EAO. The City of Yarra ~~has~~ updated its Potential Contamination of Sites in the Swan Street Major Activity Centre - Historic and Current Land Use Assessment Matrix in accordance with the EPA's comments.

### Does the amendment address relevant requirements of the Transport Integration Act 2010?

The Amendment is consistent with the requirements of the Transport Integration Act 2010 and ~~will~~ facilitates development outcomes that promote the principles of transit-oriented development.

Particular consideration ~~has been was~~ given to ensuring that vehicular movements do not impact on the Principal Public Transport Network.

### Resource and administrative costs

The Amendment ~~will have has~~ some impact on the general operation of Council's statutory planning department as it ~~will~~ facilitates some new forms of development and land use.

The application of planning controls ~~is considered to~~ provides a more consistent assessment of planning permit applications. This is considered to ultimately reduce costs by providing more certainty to the community.

### Where you may inspect this amendment

~~The Amendment is available for public inspection, free of charge, during office hours at the following places:~~

<p><del>Planning Counter</del>  <del>Richmond Town Hall</del>  <del>333 Bridge Road</del>  <del>Richmond VIC 3121</del></p>	<p><del>Service Counter</del>  <del>Richmond Library</del>  <del>415 Church Street</del>  <del>Richmond VIC 3121</del></p>
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The Amendment can ~~also~~ be inspected free of charge at the Department of Environment, Land, Water and Planning's website at ~~<https://www.planning.vic.gov.au/schemes-and-amendments/browse-planning-schemes>~~ ~~[www.planning.vic.gov.au/public-inspection](http://www.planning.vic.gov.au/public-inspection)~~ and at the City of Yarra's website at [www.yarracity.vic.gov.au/amendmentC191](http://www.yarracity.vic.gov.au/amendmentC191).

## ~~Submissions~~

~~Any person who may be affected by the Amendment may make a submission to the planning authority. Submissions about the Amendment must be received by Monday 15 April 2019.~~

~~A submission must be sent to:~~

~~Strategic Planning Unit~~

~~Yarra City Council~~

~~PO Box 168~~

~~Richmond VIC 3121~~

~~Or via email: [info@yarracity.vic.gov.au](mailto:info@yarracity.vic.gov.au) (Subject line: Swan Street Amendment C191 – Submission)~~

~~Panel hearing dates~~

~~In accordance with clause 4(2) of Ministerial Direction No.15 the following panel hearing dates have been set for this amendment:~~

~~directions hearing: Week commencing 7 October 2019~~

~~panel hearing: Week commencing 11 November 2019~~

NB: Given the size of the document, only relevant extracts from the *Database of Heritage Significant Areas* are shown.

# CITY OF YARRA

## Database of Heritage Significant Areas

*January 2019 December 2020*

Formerly

*'Appendix 8 - City of Yarra review of Heritage Overlay Areas, Graeme Butler and Associates (2007)'*



The following is an extract from the City of Yarra Heritage Database listing the heritage status of properties within each Heritage Overlay. As requested by the City of Yarra, the table is arranged in heritage overlay number order and then address order within that group. Victorian Heritage Register (VHR) properties have a second listing within Heritage Overlay Areas as recognition of their contribution to each area.

## Data fields in City of Yarra Heritage Database extract

### Name

Typically as place type only i.e. *Shop and Residence*, if not a house/residence or with no known historical name

### Address

Street name and number, suburb

### City of Yarra property number

The City of Yarra Property number as at 2005-2018

### Significance

(From given range as below)

Significance	Definition
<b>Unknown</b>	Insufficient data to allow an assessment from the public domain.
<b>Not Contributory</b>	Not contributory to identified cultural value of heritage overlay area as stated in the Statement of Significance.
<b>Contributory</b>	Contributory to identified cultural values of heritage overlay areas as stated in the Statement of Significance.
<b>Individually Significant</b>	Aesthetically, historically, scientifically, and/or socially significant at the Local level and contributory or complementary to the Heritage Overlay Area.
<b>Victorian Heritage Register</b>	On the Victorian Heritage Register (VHR) as aesthetically, historically, scientifically, and/or social significant at the State level and contributory or complementary to the Heritage Overlay Area.

### Date range

Estimated primary creation date of typically publically visible fabric only.

## HO335 - Swan Street Precinct, Richmond

HO335	Kipling	Street	7	Cremorne	Apartment	303370	Not contributory	1950-2010
HO335	Swan	Street	57-61	Richmond	Corner Hotel	190660	<del>Not contributory</del> Individually Significant	1966?
HO335	Swan	Street	63	Richmond	Shop	190655	Not contributory	1925-1935?
HO335	Swan	Street	65	Richmond	Shop	190650	Not contributory	1925-1935?
HO335	Swan	Street	69	Richmond	H.E. McNaughton ironmongery & residence, former	190645	Individually Significant	1924
HO335	Swan	Street	213A	Richmond	Vacant land	288595	Not contributory	Unknown
HO335	Swan	Street	215	Richmond	Shop & residence	288590	Contributory	1850-1890
HO335	Swan	Street	216	Cremorne	State Bank, Former	157930	Victorian Heritage Register	1907
HO335	Swan	Street	217	Richmond	Shop	191010	Not contributory	Unknown
HO335	Swan	Street	218	Cremorne	Shop & residence	157935	Not Contributory	1850-1890
HO335	Swan	Street	219	Richmond	Shop & residence	191005	Individually Significant	1870-1890
HO335	Swan	Street	220	Cremorne	Bank of Melbourne	157940	Not contributory	1980-2000
HO335	Swan	Street	221	Richmond	Shop	190995	Not contributory	1950-1960?
HO335	Swan	Street	223	Richmond	Shop	191000	Not contributory	1950s-1960
HO335	Swan	Street	224	Cremorne	Shop & residence	157945	Individually Significant	1870-1890
HO335	Swan	Street	225	Richmond	Office	500380	Not contributory	2000-2010
HO335	Swan	Street	226	Cremorne	Shop & residence	157950	Individually Significant	1915-1925
HO335	Swan	Street	228	Cremorne	Shop & residence	157965	Individually Significant	1870-1890
HO335	Swan	Street	229	Richmond	Shop & residence	415990	Contributory	1914-1918
HO335	Swan	Street	230	Richmond	Shop & residence	157970	Individually Significant	1888
HO335	Swan	Street	231	Richmond	Shop & residence	416080	Contributory	1914-1918
HO335	Swan	Street	232	Richmond	Shop & residence	157980	Individually Significant	1870-1890
HO335	Swan	Street	233	Richmond	Shop & residence	190975	Contributory	1860-1870
HO335	Swan	Street	234	Richmond	Shop & residence	157990	Individually Significant	1870-1890
HO335	Swan	Street	235	Richmond	Shop & residence	190970	Contributory	1860-1870
HO335	Swan	Street	236	Richmond	M.Ball & Co, former part	158000	Individually Significant	1870-1880
HO335	Swan	Street	237	Richmond	Shop & residence	190965	Contributory	1880-1890
HO335	Swan	Street	238	Richmond	M.Ball & Co, former part	158005	Individually Significant	1860-1870
HO335	Swan	Street	239	Richmond	Shop & residence	190960	Contributory	1880-1890
HO335	Swan	Street	240	Richmond	M.Ball & Co, former	158010	Individually Significant	1860-1870
HO335	Swan	Street	242-244	Richmond	Shop	158015	Not contributory	1950-1960?
HO335	Swan	Street	246	Richmond	Shop & residence	158020	Contributory	1900-1915
HO335	Swan	Street	248	Richmond	Shop & residence	158025	Contributory	1900-1915
HO335	Swan	Street	250	Richmond	Whitehorse Hotel, former	158030	Individually Significant	1845-1855

## HO524 - Swan Street West Precinct

HO524	Swan	Street	30	Cremorne	Shop	157620	Contributory	1901-1918
HO524	Swan	Street	34	Cremorne	Shop	318580	Contributory	1875-1901
HO524	Swan	Street	36	Cremorne	Shop	157635	Contributory	1860-1875
HO524	Swan	Street	40-42	Cremorne	Shop	157645	Contributory	1901-1918



Yarra High Streets (Swan Street): Statements of Significance: Reference Document

December~~October~~ 2017 2020

This reference document contains the Statement of Significance for all Heritage Precincts and Individually Significant Places (where a Statement of Significance has been prepared) within:

The ~~Swan Street Built Form Review Study Area (City of Yarra & Tract 2017)~~ the Swan Street Major Activity Centre.

## INDEX

### Precincts

	<b>Precinct HO No.</b>	<b>Precinct Name</b>	<b>Page No.</b>
1.	HO309	Bendigo Street Precinct, Richmond	1
2.	HO315	Church Street Precinct, Richmond	2
3.	HO332	Richmond Hill Precinct	4
4.	HO335	Swan Street Precinct, Richmond	9
5.	HO364	Wellington Street Precinct	11
6.	HO474	Burnley Street Precinct, Richmond	15
7.	HO524	Swan Street West Precinct	16

### Individually Significant Places

	<b>Individual HO No. / VHR No.</b>	<b>Name</b>	<b>Address</b>	<b>Page No.</b>
1.	HO245	House	234 Coppin Street, Richmond	18
2.	HO285	Former Central Club Hotel	291 Swan Street, Richmond	18
3.	HO286	Former Burnley Theatre	365 Swan Street, Richmond	18
4.	HO288 <b>VHR H732</b>	Former State Bank	214-216 Swan Street, Richmond	18
5.	HO294	House	15 Wellington Street, Cremorne	18
6.	HO357 <b>VHR H48</b>	Former Richmond South Post Office	90-92 Swan Street, Richmond	19
7.	HO360 <b>VHR H2184</b>	Dimmeys	140-160 Swan Street, Richmond (Cremorne)	19
8.	HO405	The Greyhound Hotel, later Depot Hotel, now Precinct Hotel	60-62 Swan Street, Richmond	19
9.	HO429	-	400-402 Burnley Street, Richmond	20
10.	HO440	Swan Street Drill Hall	309 Swan Street, Richmond	20
11.	HO441	-	319 Swan Street, Richmond	21
12.	HO522	Shop and Residence	273A Swan Street, Richmond	22
13.	HO523	Pair of Shops	323-325 Swan Street, Richmond	22

### Individually Significant Places within a Precinct

Precinct HO No.	Name	Address	Page No.	
<del>1.</del>	<del>HO332</del>	<del>Corner Hotel</del>	<del>57-61 Swan Street, Richmond</del>	<del>24</del>
<u>2.1.</u>	HO335	Shops and Residences	454-456 Church Street, Cremorne	24
<u>3.2.</u>	HO335	H. E. McNaughton ironmongery & residence, former	69 Swan Street, Richmond	24
<u>4.3.</u>	HO335	National Bank of Australasia	105 Swan Street, Richmond	<del>24</del> <u>5</u>
<u>5.4.</u>	HO335	Shop and residence	232-234 Swan Street, Richmond	<del>25</del> <u>24</u>
<u>6.5.</u>	HO335	M. Ball & Co, former	236-240 Swan Street, Richmond	25
<u>7.6.</u>	HO335	Whitehorse Hotel, former	250-252 Swan Street, Richmond	25
<u>8.7.</u>	HO474	Shop and Residence	413-415 Swan Street, Richmond	25
<u>8.</u>	<u>HO474</u>	<u>Bank of Australasia (Former)</u>	<u>377 Burnley Street, Richmond</u>	<u>26</u>
<u>9.</u>	<u>HO474</u>	<u>Shop and Residence</u>	<u>380 Burnely Street, Richmond</u>	<u>26</u>

# STATEMENTS OF SIGNIFICANCE

## PRECINCTS

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### 1. Bendigo Street Precinct, Richmond (HO309)

Source: Graeme Butler & Associates (2007, 2013), *City of Yarra Review of Heritage Overlay Areas* Updated by GJM Heritage (2017), *Swan Street Built Form Review*

#### ***What is significant?***

The Bendigo Street Heritage Overlay Area was part of Crown Allotment 16 as sold to J M Chisholm in 1840. Plans from 1895 show that, by then, three-quarters of the area was developed with housing, mostly in Park Grove, Kimber, Brooks and Survey Streets. The Wertheim Piano Factory (HO224), later the GTV9 television studios, dominated the area from the Edwardian-era. The factory was once the largest piano factory in Australia, occupying a four acre site, complete with its own power generator and tramline. Designed c1909 by architect Nahum Barnet, the buildings are important heritage elements in the streetscape.

Nearby Richmond Park (now Burnley Park) was the pleasure ground for this area as well as the rest of inner Melbourne and now forms the eastern boundary of the area.

#### Main development era

The main development era evident in the heritage overlay is that of the Victorian and Edwardian-eras, with a contribution from well preserved inter-war buildings and individually significant places of all eras, such as the former Wertheim Piano Factory.

#### Contributory elements

The contributory buildings in the Bendigo St Heritage Overlay Area include mainly (but not exclusively) small attached and detached Victorian-era and Edwardian-era one-storey houses, but with some well preserved residential examples from the immediate post First-War era, having typically:

- Consistent building scale and setbacks
- Pitched gabled or hipped roofs, with some façade parapets;
- One storey wall heights;
- Weatherboard, face brick (red, bichrome and polychrome), bluestone, or stucco walls;
- Corrugated iron roof cladding, Marseilles pattern terra-cotta tiles, with some slate roofing;
- Chimneys of either stucco finish (with moulded caps) or of matching face brickwork with corbelled capping courses;
- Post-supported verandah or porch elements facing the street;
- Less than 40% of the street wall face comprised with openings such as windows and doors; and
- Front gardens, originally bordered by typically timber picket front fences or wire fences (inter-war) of around 1m height; also
- Corner shops and residences with large display windows and zero boundary setbacks.

Public infrastructure, expressive of the Victorian and Edwardian-eras such as stone pitched road paving, kerbs and channels, and asphalt paved footpaths

#### ***How is it significant?***

HO309 Bendigo Street Heritage Overlay Area, Richmond is **aesthetically and historically** significant to the City of Yarra (National Estate Register [NER] Criteria E1, A4)

#### ***Why is it significant***

The Bendigo Street Heritage Overlay Area is significant for:

## PRECINCTS

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- Its substantially intact single-storey Victorian-era housing that varies between modest and ornate;
  - Edwardian house examples, particularly in Swan and Moore Streets, being both typical and highly decorated Edwardian dwelling types, complemented by the significant Edwardian-era former Wertheim Piano Factory;
  - The retention of early materials and elements in the public domain, such as street construction, and the retention of many bluestone laneways;
  - The demonstration of a typical 19th century suburban area with its attached and detached housing stock and corner shops, that has been subsequently lost in other parts of the inner suburbs; and
  - The consistency of building scale and setbacks, creating cohesive and homogeneous streetscapes that are enhanced by mature plane tree avenue plantings.
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### 2. Church Street Precinct, Richmond (HO315)

Source: Graeme Butler & Associates (2012), *City of Yarra Heritage Gaps Review (Review 1)*

#### ***What is significant?***

##### Subdivision

The Church Street Heritage Overlay Area includes parts of the first suburban Crown Allotments sold in the City, being Crown Portions 20, 21, 26 and 27, each 25 acres in area. Allotments 21 and 22 were purchased in 1839 by Rev. Joseph Docker (squatter), Allotment 20 by Charles Williams (auctioneer), and Allotment 26 by W H Yaldwyn (squatter, banker).

The Waltham Street and Darlington Parade areas were subdivided in 1853 and further subdivided in the 1880s. Docker had subdivided his two allotments by 1853, with plans for a model village set out on the flat below his townhouse at 370 Church Street. By the turn of the century, most of the Heritage Overlay Area was developed. The topography of the Heritage Overlay Area, the highest point in Richmond, attracted both the churches and the wealthier colonists with the result that the majority of the earliest residences were of a more substantial nature compared with other sections of Richmond.

This area has remained one of the most prestigious parts of Richmond for residential development. As an example, Howard Lawson's Elmhurst Flat block of 1934 aimed to tap into this prestigious residential location, paralleling with his significant Hollywood style Beverley Hills and Stratton Heights Flats, sited across the Yarra River at South Yarra.

##### Large villas

By 1855, villas with large gardens and orchards had been established in Church Street between Brougham and Elm Streets. Early houses which survive include Doery House (353 Church Street) and Messenger House (333 Church Street, formerly Stonehenge), the latter being built prior to 1843 for Captain John Roach<sup>4</sup> and remodelled in the Edwardian period.

##### Major church complexes

Three major church complexes were established in the Heritage Overlay Area in the mid-1800s. St Stephen's Anglican Church (1850-1876) at 360 Church Street was designed by Blackburn and Newson on land donated to the church by the Rev. Joseph Docker and is one of the earliest bluestone churches built in Victoria. The Wesleyans began the construction of a temporary timber chapel (later the schoolhouse) in 1853, bluestone chapel in 1858, and added a schoolhouse (1871) and a parsonage (1876). St Ignatius' Roman Catholic Church (326) was built in stages between 1867 and 1928, to a design by prominent architect William Wardell, with the bluestone Presbytery added in 1872.

## PRECINCTS

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### Civic buildings

Other non-residential developments in the Heritage Overlay Area included the former Richmond United Friendly Society Dispensary (1884; 294 Church Street), and the Hibernian Hall (1872; 316 Church Street), which was built as a temperance hall. The Richmond RSL was built in 1922, as an expression of the continuing premier civic status of this part of Church Street in the 20th century. The Richmond Library is the most recent civic development in the area.

### Commercial development

Commercial development extended north from the major thoroughfares of Swan St and south from Bridge Road in the late Victorian and Edwardian-eras.

### Main development era

The main development period evident in the heritage overlay is that of the Victorian and Edwardian-period, with a contribution from some well preserved inter-war buildings and individually significant places of all eras. Large houses, religious and public buildings from this period are the key elements in the heritage overlay.

### Contributory elements

The Church Street Heritage Overlay contributory elements include (but not exclusively) large detached Victorian-era and Edwardian-era and small attached Victorian-era, one and two storey houses having typically:

- pitched gabled or hipped roofs, with some façade parapets;
- face brick (red, bichrome and polychrome) or stucco walls;
- corrugated iron, unglazed Marseilles pattern terra-cotta tiles, and slate roofing;
- chimneys of either stucco finish (with moulded caps) or of matching face brickwork with corbelled capping courses;
- typically with post-supported verandah elements facing the street;
- less than 40% of the street wall face comprised with openings such as windows and doors; and
- front gardens, originally bordered typically by timber picket front fences of around 1m height;

Contributory elements also include shops and residences of the Victorian and Edwardian-eras, with:

- display windows and recessed entries;
- zero boundary setbacks;
- mainly one storey scale;
- attached rectilinear plan form, a parapeted roofline; and

Substantial Victorian-era ecclesiastical buildings with:

- free standing rectilinear form
- pitched roofs, some towers and spires positioned to be visible from a distance;
- fenced yards, with potential use of timber or iron pickets and a stone base for the frontage fence;
- two storey and greater wall heights;
- stone, masonry or stuccoed masonry facades, slate or tiled roofs; and
- less than 40% of the street wall face comprised with openings such as windows and doors.

Contributory elements also include:

- Public infrastructure, expressive of the Victorian and Edwardian-eras such as stone pitched road paving, kerbs and channels, and asphalt paved footpaths - mainly in side streets; and

## PRECINCTS

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- Many significant buildings within the Heritage Overlay Area have their own heritage overlay (HO241, HO242, and the St Ignatius complex) but nevertheless are contributory to the Church St Heritage Overlay Area.

### ***How is it significant?***

HO315 Church Street Heritage Overlay Area, Richmond is aesthetically and historically significant to the City of Yarra (National Estate Register [NER] Criteria E1, A4).

### ***Why is it significant?***

The Church Street Heritage Overlay Area is significant:

- As one of the first parts of Richmond to be subdivided and developed, as expressed by early buildings like Messenger House 333 Church Street, from the 1840s;
- As the chosen site for a high number of individually significant 19th and early 20th century buildings set in grounds and including early ecclesiastical and civic buildings, and some Melbourne landmarks, as well as substantial residential buildings that were attracted to the area by its elevated topography, high amenity and proximity to churches;
- As the site of key civic or institutional buildings in Richmond from the 19th century through to the 1920s (i.e. The Richmond RSL Hall); and
- For its significant architecture such as the William Wardell designed St. Ignatius Roman Catholic Church as a well known and prominent landmark across the metropolitan area.

The heritage character of the precinct is also supported by the commercial development extending up Church St from the Swan St and Bridge Road shopping areas with shops dating from the late 19th and early 20th centuries, as part of the cultural context of Victorian and Edwardian-era life on the hill.

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### **3. Richmond Hill Precinct (HO332)**

Source: Graeme Butler & Associates (2007, 2013), *City of Yarra Review of Heritage Overlay Areas* [see Study for footnote references]

#### **ALL SUB-PRECINCTS**

##### ***What is significant?***

##### Subdivision types

The land which now comprises the Richmond Hill Area was purchased from the Crown in 1839 and consisted of six portions each of 25 acres in the parish of Jika Jika (CPs 21-26). The purchasers were Dr. Farquhar McCrae (CP24, surgeon), Rev. Joseph Docker (CP21-22, squatter), W H Yaldwyn (CP26, squatter, banker), Henry Gordon Brock (CP25), and John McNall (CP23, Melbourne's first butcher)<sup>96</sup>.

From 1840 onwards, Dr. McCrae sold parts of his allotment (at the north west corner of the heritage overlay) to prominent Melbourne figures, including Alexander McCrae, Alfred Woolley, Charles Williams, George Arden, and Thomas Strode (proprietors of the *Port Phillip Gazette*).<sup>97</sup> He also subdivided off 36 half acre blocks, creating Rowena Parade, Sherwood and Rotherwood Streets. Further subdivisions took place in the 1870s and 1880s.

Yaldwyn sold his allotment (at the north-east corner of the heritage overlay) to William Meek in 1840 and, by 1853, the Waltham Street and Darlington Parade areas were subdivided, and further subdivided in the 1880s. The Vaucluse area was laid out in the 1860s and a private street was created to link Rowena Parade and Church Street, for the use of abutting landholders only. The Vaucluse, thought to be the only Victorian-era private street in the inner suburbs, has its eastern entry at St Ignatius Church. Large houses were erected on this section, including Richmond Hill for James Henty

## PRECINCTS

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(Waltham Street, 1851) and a two-storey Italianate house for distinguished architect, James Blackburn Jr.

Docker had subdivided his two allotments by 1853 (at the south-east corner of the heritage overlay), creating a model village (Village of Clifton) set out as 365 lots on a grid plan designed by Charles Laing. Its regular layout was unlike the other haphazardly planned private subdivisions in the area. The village was bordered by Wangaratta St (inclusive) on the west, the Richmond Terrace service lane on the north, the Church St on the east and Swan Street on the south<sup>98</sup>. By 1857 Docker still owned many of the houses built on these allotments. His own townhouse was at 370 Church Street. By the 1880s, this area was almost fully developed. (See South Residential Sub-precinct)

Brock's allotment (at the north-centre of the heritage overlay) was purchased by overlander John Gardiner<sup>99</sup> who erected a villa on it when he retired from his grazing activities to take up the position of managing director of the Port Phillip Bank in 1840.<sup>100</sup> By 1853, Lennox Street, Rowena Parade and Goodwood Street had been laid out. Further subdivisions occurred in the 1870s and 1880s.

The allotment purchased by McNall (at the south-west corner of the heritage overlay) had undergone minor subdivision by 1853. McNall erected his house there, and his widow continued to live there following his death in the 1850s.<sup>101</sup> In 1859, the railway was established, and Richmond Station was constructed at the south-west corner of the allotment. The land had undergone major subdivision by the 1870s and 1880s.

### Major industry development

In the early 20th century the expansion of manufacturing across the nation was reflected in new industrial centres that were developed close to rail and road transport, as distinct from the Victorian-era industrial development that had occupied coastal sites or river and creek banks. Near to the transport hub of Richmond Railway Station and busy Punt Road, grew a distinctive group of factories and warehouses, mainly associated with the growth of the clothing manufacturing industry in the City and dating substantially from the early decades of the 20th century. This area had previously been residential.<sup>102</sup>

The Australian Knitting Mills Ltd. (41-43 Stewart St, knitted goods manufacturers) complex was at the centre of the Stewart-Tanner Streets industrial area development area. It was developed from 1912 when the supply for World War One uniforms was an impetus for its expansion and had another major building addition in 1922-5.<sup>103</sup> The company created the 'Golden Fleece' and 'Kookaburra' brands.

### Inter-war residential estates

Set away from this concentrated industrial strip, but related historically to it, was the Pelaco factory (1922-1928), located on a hill on the former site of a Victorian-era mansion.

Houses in Fordham Court and the associated part of Goodwood Street were built speculatively in 1939 for Mrs B.E. Fordham of Yarradale Rd., Toorak, on land sold by Pelaco.<sup>104</sup> These houses provide a distinctive enclave within the surrounding Victorian and Edwardian-era development.

## **A. RICHMOND HILL HERITAGE OVERLAY AREA (WEST SUB-AREA)**

### Main development era

The main development period evident in the Richmond Hill west sub-area of the Heritage Overlay Area is that of the Victorian era with a substantial contribution from the Edwardian-period. There is also a contribution from some well preserved inter-war buildings and individually significant places of all eras.

### Contributory elements



## PRECINCTS

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The Heritage Overlay Area contributory elements include (but not exclusively) mainly small, detached and some attached Victorian-era and Edwardian-era houses having typically:

- Pitched gabled or hipped roofs, with some façade parapets,
- One storey wall heights but with some two storey house rows,
- Face brick (red, dichrome and polychrome), weatherboard, or stucco walls;
- Corrugated iron roof and slate cladding, some unglazed Marseilles pattern terra-cotta tiles;
- Chimneys of either stucco finish (with moulded caps) or of matching face brickwork with corbelled capping courses;
- Post-supported verandah elements facing the street, set out on two levels as required with cast-iron detailing for Victorian-era houses and typically timber for Edwardian-era;
- Less than 40% of the street wall face comprised with openings such as windows and doors; and
- Small front gardens, originally bordered by typically timber picket front fences of around 1m height.

Contributory elements also include:

- Corner shops and residences with display windows and zero boundary setbacks.
- Well preserved buildings including typically one storey buildings from the pre Second War era.
- Public infrastructure, expressive of the Victorian and Edwardian-eras such as bluestone pitched road paving, crossings, stone kerbs, channels, and asphalt paved footpaths.

### **B. RICHMOND HILL HERITAGE OVERLAY AREA (NORTH SUB-AREA)**

Main development era

The main development period of the Richmond Hill north sub-area of the Heritage Overlay Area is that of the Victorian-era with a substantial contribution from the Edwardian-period. There is also a contribution from some well preserved inter-war buildings and individually significant places of all eras.

Contributory elements

The Heritage Overlay Area contributory elements include (but not exclusively) mainly detached and some attached Victorian-era and Edwardian-era mainly one and two -storey houses having typically:

- Pitched gabled or hipped roofs, with some façade parapets,
- One and two storey wall heights but with some two storey house rows,
- Face brick (red, dichrome and polychrome), or stucco walls;
- Corrugated iron roof and slate cladding, some Marseilles pattern terra-cotta tiles;
- Chimneys of either stucco finish (with moulded caps) or of matching face brickwork with corbelled capping courses;
- Post-supported verandah elements facing the street, set out on two levels as required with cast-iron detailing for Victorian-era houses;
- Less than 40% of the street wall face comprised with openings such as windows and doors; and
- Front gardens, originally bordered by typically timber or iron picket front fences of around 1m height.

Contributory elements also include:

- Corner shops and residences with display windows and zero boundary setbacks.
- Well preserved buildings including typically one storey buildings from the pre Second War era.
- Public infrastructure, expressive of the Victorian and Edwardian-eras such as bluestone pitched road paving, crossings, stone kerbs, channels, and asphalt paved footpaths.

### **C. RICHMOND HILL HERITAGE OVERLAY AREA (PELACO, FORDHAM COURT/PART GOODWOOD STREET SUB-AREA)**

## PRECINCTS

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### Main development era

The main development period of the Richmond Hill Fordham Court/Goodwood Street sub-area of the Heritage Overlay Area is that of the Inter-war era.

### Contributory elements

Contributory buildings in the Fordham Court/Goodwood Street sub-area are typically:

- Residential;
- Single storey;
- Constructed in brick and rendered masonry;
- Of a hipped and gabled tiled roof form, some with a single hipped or gabled roof form over the two dwellings to appear as single larger houses;
- Semi detached duplexes;
- Set in relatively deep garden setbacks; and
- Fenced originally with front fences of low (500-700mm panel height) brick and rendered masonry pier and panel walls.

And

- Public infrastructure, part expressive of the Victorian and Edwardian-eras such as asphalt paved footpaths but most of the kerb and channel is concrete, as typical of the inter-war period.
- Former Pelaco factory (c1922) as an icon in the area, but unrelated in scale and form to the inter-war housing nearby.

## D. RICHMOND HILL HERITAGE OVERLAY AREA (SOUTH SUB-AREA)

### Main development era

The main development period evident in south sub-area of the Richmond Hill Heritage Overlay Area is that of the Victorian-era. There is also a contribution from the Edwardian-era and some well preserved inter-war buildings and individually significant places of all eras.

### Contributory elements

The Heritage Overlay Area contributory elements include (but not exclusively) mainly detached and some attached Victorian-era (some early Victorian-era), one-storey houses having typically:

- Steeply pitched gabled or hipped roofs;
- One storey wall heights;
- Weatherboard (square or bead edge), face brick (dichrome), stone, or stucco walls;
- Corrugated iron roof and slate cladding;
- Chimneys of either stucco finish (with moulded caps) or of matching face brickwork with corbelled capping courses;
- Simple post-supported timber verandah elements facing the street;
- Less than 40% of the street wall face comprised with openings such as windows and doors; and
- Front gardens, sometimes minimal, originally bordered by typically timber picket front fences of around 1m height.

Contributory elements also include:

- Corner shops and residences with display windows and zero boundary setbacks;
- Well preserved buildings, including typically one storey buildings from the pre Second War era.
- A regular subdivision plan with alternating wide principal streets and narrow service streets; and
- Public infrastructure, expressive of the Victorian and Edwardian-eras such as bluestone pitched road paving, crossings, stone kerbs, channels, and asphalt paved footpaths.

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### E. RICHMOND HILL HERITAGE OVERLAY AREA (SOUTH INDUSTRIAL SUB-AREA)

Main development era

Main development period in the Richmond Hill south industrial sub-area of the Heritage Overlay Area is of the Edwardian-era and inter-war period.

Contributory elements

Contributory elements include (but not exclusively) industrial buildings from the early decades of the 20th century with typically:

- Wall heights ranging from 1-5 levels;
- Façade parapets, sometimes with proprietary logos, with pitched roofs behind;
- Limited or no setback from street frontages; and
- Walls of face red brick (some over-painted) with rendered bands.

Contributory elements also include:

- Public infrastructure, part expressive of the Victorian and Edwardian-eras such as some bluestone pitching and asphalt paved footpaths.

#### ***How is it significant?***

HO332 Richmond Hill Heritage Overlay Area as a whole is aesthetically and historically significant to the City of Yarra (National Estate Register [NER] Criteria E1, A4)

#### ***Why is it significant***

### A. The Richmond Hill Heritage Overlay Area (west sub-area)

The Richmond Hill Heritage Overlay Area (west sub-area) is significant:

- As an illustration of a typical early private subdivision and development in the City, shown by the irregular street patterns, widely varying allotment sizes, and unusual house siting (onto lanes);
- As one of the earliest private development areas in the City for worker housing, dating from the 1840s, as expressed today by the many early small cottages linked historically to developers who resided in the more salubrious elevated parts of this area;

### B. The Richmond Hill Heritage Overlay Area (north sub-area)

The Richmond Hill Heritage Overlay Area (north) is significant:

- For the well preserved, well-situated and sometimes large Victorian and Edwardian period houses and for the contribution of well preserved buildings from the inter-war period and individually significant buildings from all periods;
- As one of the early private housing development areas for the wealthy, evolving from the 1840s, and associated with some of Melbourne's most prominent early settlers as seen in their gracious residences, elevated siting and proximity to religious centres of most denominations;
- As particularly indicative (like the west sub-area) of Victorian-era private subdivision with irregular street patterns and lot sizes, and one private road; and
- For the views, from both inside and outside the area, to The Vaucluse Convent, the church spires and towers of St Ignatius and the utilitarian four storey red brick Pelaco factory, with its early neon sign above.

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### **C. The Richmond Hill Heritage Overlay Area (former Pelaco factory, Fordham Court/Goodwood Street sub-area)**

The Richmond Hill Heritage Overlay Area (Pelaco, Fordham Court/Goodwood Street sub-area) is significant:

- For the well preserved and visually cohesive groups of inter-war and early post WW2 residential development
- For the historical and visual links of this inter-war development with the adjacent inter-war landmark Pelaco Factory;
- For the contribution of the iconic Pelaco factory complex and illuminated sign, that is well-preserved externally, large in scale and set prominently on the hill as, historically, a symbol of Richmond's special role in the development of key manufacturing centres in the first half of the 20th century.

### **D. The Richmond Hill Heritage Overlay Area (south residential sub-area)**

The Richmond Hill Heritage Overlay Area (south) is significant:

- As one of the earliest private development areas in the City for worker housing, dating initially from the 1840s but extending in the gold-era of the 1850s, as early small cottages set on small allotments;
- For the historical links of the street layout and subsequent housing development with the Docker family and the estate's designer, the noted architect and surveyor, Charles Laing;
- As an illustration of a rare type of model private subdivision and development in the Melbourne area in the form of a planned 'village', as shown by the regular street patterns, similar allotment sizes, and the originally residential land use.

### **E. The Richmond Hill Heritage Overlay Area (south industrial sub-area)**

The Richmond Hill Heritage Overlay Area (south industrial sub-area) is significant:

- As a distinctive and visually related group of externally well-preserved factories and warehouses, associated with the growth of the clothing manufacturing industry in the City, dating from the early decades of the 20th century and symbolic of Richmond's special role in the development of key manufacturing centres in the first half of the 20th century.
- For the precinct's strategic location, next to major transport links (railway, Punt Road) and the resultant distinctive angled siting of key buildings to face the railway.

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## **4. Swan Street Precinct, Richmond (HO335)**

Source: GJM Heritage (2017), *Swan Street Built Form Study: Heritage Assessments & Analysis*

### ***What is significant?***

Swan Street, Richmond from Botherambo Street to Brighton Street (nos. 63-221 & 90-272), Church Street, south of Swan Street to the railway line (nos. 421-425 & 454-468), Byron Street (nos. 2-8 & 140-160) and Kipling Street (nos. 1, 2, 2A, 3, 5 & 7).

### ***History***

Swan Street was designated as a road reserve in Hoddle's Crown survey of 1837. Allotments on the north side of Swan Street were sold from 1839 and the south side from 1846. By 1857 a number of commercial businesses had been established in Swan Street, particularly at the west end, and traders included butchers, drapers, fruiterers, tailors, shoemakers, hairdressers and hoteliers. Amongst the

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early hotels was the Swan Inn after which the street was named, and the predecessor of the present three-storey Swan Hotel (1889) at the corner of Church Street.

Swan Street ran from Punt Road to the Survey Paddock (now Burnley Park) before extension to the Yarra River sometime between 1874 and 1888. In 1859 a railway line from Princes Bridge to Punt Road provided access to Swan Street and this was extended to Church Street (East Richmond Station) and beyond to the present Burnley Park the following year. Running south of, and parallel to, Swan Street, this railway line provided transport to and from the city. In 1916 an electric tram service provided additional transport along Swan Street, encouraging further development along the length of Swan Street.

The commercial western end of Swan Street experienced a development boom in the 1880s and much of the present commercial building stock, particularly between the railway line (which was raised above Swan Street in 1887) and the Church Street intersection, was constructed at this time. This included a number of predominantly two-storey shops with residences above, including the extensive Perrins Building, 128-38 Swan Street (1884), shops opposite at 95-97 Swan Street (1885) and a highly decorative group of buildings at 224-234 Swan Street.

Swan Street was sealed in 1901 and construction of commercial buildings continued on allotments at the western end of Swan Street. Substantial and distinctive commercial and public buildings, such as the Maples Piano Showroom and the Richmond South Post Office in 1905, and Dimmeys and the former State Savings Bank of Victoria in 1907 added an Edwardian presence to the existing Victorian character of the precinct. A highly prominent clock tower with ball, added to the Dimmeys store in the 1910s, became a landmark feature in Swan Street.

Swan Street's long history as a commercial centre has also seen it play a central role in the social life of Richmond. For over 150 years, the street has provided the community with social and recreational opportunities, with the long-term commercial success of the street being predicated upon its attractiveness as a social space. It is this rich social legacy that binds the various phases of the street's development together, and which continues today to attract new residents and visitors.

### *Description*

The main development period of the Swan Street Precinct is from the 1880s to 1920 and the streetscape dates largely from this period. A small number of buildings from both earlier and later periods make a contribution to the precinct. A number of individually significant buildings are contained within the precinct, dating predominantly from the 1880s-1920 period.

Buildings are predominantly attached Victorian and Edwardian shops and residences. They are predominantly two-storey with some three/four and single-storey shops interspersed.

Elements that contribute to the significance of the place include (but are not limited to):

- A variety of simple and highly decorative façade parapets, with pitched roofs behind
- No front or side setbacks
- Face red brick (including polychrome) or rendered walls
- Rendered window frames, sills and hoods to upper stories
- Rendered ornament and incised decoration to upper stories
- Brickwork with corbelled capping courses
- Original post-supported street verandahs
- Strong horizontal lines formed by parapets, cornices, string courses
- Repetitive upper floor fenestration patterns

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- Consistent two-storey scale with some one and three-storey buildings
- Shop fronts with display windows, timber or tiled plinths, and entry recesses
- Some red brick storage or stable buildings at the rear or side lane frontage
- Corner buildings with secondary façades to side streets or lanes
- Splayed corners to buildings at intersections
- Corrugated iron and slate roof cladding
- Bluestone pitched road paving, crossings, stone kerbs and channels and asphalt paved footpaths.

### ***How is it significant?***

Swan Street, Richmond is of local historical, architectural and aesthetic significance to the City of Yarra.

### ***Why is it significant?***

The Swan Street Precinct is a major 'High Street' in the City of Yarra and has functioned continuously as one of Richmond's two key commercial centres since the 1840s. It retains a substantial collection of intact commercial buildings, predominantly from the Victorian and Edwardian periods, including shops and associated residences, hotels and other commercial and civic buildings. Together these buildings demonstrate the development of this major 'High Street', particularly from the 1880s to c1920 when substantial growth in the street occurred, and are illustrative of the enduring role the street has played in the economic and social life of Richmond since the establishment of the suburb [Criterion A].

The highly intact streetscape of the Swan Street Precinct clearly demonstrates the principal characteristics of a major turn of the century 'High Street' in the City of Yarra. Typical characteristics, including predominantly two-storey Victorian and Edwardian wall heights, parapeted rendered or red brick facades with repetitive upper floor fenestration, and ground floor shopfronts, are displayed in the original forms, fabric and detailing of many of the buildings. The streetscapes on the south side of Swan Street between the former Richmond Post Office and Church street, on the north side of Swan Street between Carroll and Dando Streets, and the west side of Church Street between Swan Street and the railway line are particularly intact and consistent in their architectural form and expression [Criterion D].

The Swan Street Precinct contains a number of landmark and individually significant buildings which are well-considered and carefully detailed examples of commercial and civic buildings. These include, among others, the former State Savings Bank of Victoria (216 Swan Street); the former Richmond South Post Office (90-92 Swan Street); large retail stores such as Maples Pianos (122-126 Swan Street); Dimmeys (140-48 Swan Street); and M Ball & Co (236-240 Swan Street), as well as hotels such as the Swan Hotel (425 Church Street). The clock tower and ball of the Dimmeys building is of particular prominence and is a local landmark. Variations in façade detailing throughout the precinct, including parapet ornamentation, balustrading and pediments, incised and applied decoration and polychromatic brickwork, make an aesthetic contribution to the overall character of the precinct [Criterion E].

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## 5. Wellington Street Precinct (H0364)

Source: Graeme Butler & Associates (2012), *City of Yarra Heritage Gaps Review (Review 1)*

### ***What is significant?***

Historical background

In 1839, two years after the first land sales in the township reserve of Melbourne, Crown allotments were auctioned in Richmond, Fitzroy and Collingwood. These allotments were mainly intended for development as farmlets. However many of the purchases in Richmond were speculative for, very soon, allotments were subdivided and advertised for sale in the "Port Phillip Patriot". The first was

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William Wilton's Crown allotment 46 which was to be sold in one or more acre lots. In 1840, at a subdivision sale of Dr. Farquhar McCrae's allotment 24, the auctioneer described Richmond as "...the abode of aristocracy, wealthy and retired opulence..." and 36 half-acre blocks were sold.

This was the boom period leading up to the recession of the early 1840s. As a sign of the times, subdivisions on the Richmond flats were advertised in 1842 as "...well deserving public attention among the working class", in contrast to earlier advertising of the higher parts of Richmond as for gentlemen only.

By the mid 1840s the depression had ended and resumption of the Immigration Act resulted in a new influx of workers. The sale of Crown allotments recommenced in Richmond in 1845 and by 1851 a further fifteen Crown Portions were sold. Reserves were also created for police purposes (Crown allotments 13-15), and for churches, recreation, produce market, schools and a mechanics' institute (Crown allotment 35). Thirty-one quarry sites were set aside on Crown allotments 9 to 15 where they abutted the river. The only other clay pits shown are at the locality of Yarraberg which David Mitchell operated in Crown Portion 42, off Burnley St.

Richmond's population in 1846 was 4029. At this time, Fitzroy and Collingwood were also being rapidly subdivided, St. Kilda and Port Melbourne were fashionable picnic spots and Williamstown a busy port. The village at Brighton was the leading pleasure resort, and Heidelberg a prosperous farming community. East Melbourne was little built upon until after 1848 when Bishop Perry chose a site there for the Anglican Bishop's Palace. This gave an impetus to building and the Richmond area went ahead as a select and convenient one in which to live. In 1852 North Melbourne, St. Kilda, South Melbourne, Port Melbourne, Essendon, Remington, Carlton and Hawthorn were laid out. Melbourne's population had trebled by 1853 with people returning from the goldfields, while in Richmond major residential subdivisions had occurred in the north and west. Within the next four years, men who established their suburban villas on the Richmond hills included senior Government officials, Alexander McCrae and William Hull; newspaper proprietors Thomas Strode, George Cavanaugh and George Arden; merchants Patrick Welsh, David Stodart Campbell and Alfred Woolley; and the bankers William Highett and John Gardiner. Their "...comfortable, if not architecturally stylish villas began to dot the place".

Richmond was created a separate municipality in 1855. The survey maps of Magee and Kearney show that at this time many of the existing major streets had been laid out but that almost all buildings, with the exception of those in the Yarraberg area to the northeast, were concentrated in the western half of Richmond, near to Melbourne town and the railway route: large suburban villas and gardens on the hill, and cottages on small blocks in the north and south, often in areas of relatively intense development isolated to individual streets. The factors influencing the location of the earliest development appear to have been a preference for high ground and a position on government roads, especially at cross roads.

Richmond's population in 1857 was 9,029 with 2,161 houses and five architects. The electors' roll for 1856-7 indicates an established retail and service trade in Swan Street and Bridge Road - butchers, drapers, shoemakers, hotels, fruiterers, tailors, hairdressers, grocers and blacksmiths.

With separation from Melbourne in 1855, Richmond, along with Collingwood, became exempt from the 'Melbourne Building Act' of 1849 which controlled building and subdivision standards. Developers were free to plan streets, reduce frontages and build what they liked. Closer development of Richmond was also encouraged by the railway which was extended to Brighton from Melbourne by 1859, and by horse drawn omnibuses which connected Richmond with Melbourne along Bridge Road.

Melbourne's population in 1861 was 37,000 (including Carlton and East Melbourne); Richmond, Collingwood and Fitzroy each had about 12,000, Prahran 10,000, South Melbourne 9,000, North

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Melbourne 7,000 and St. Kilda 6,000. Development was apparent along Punt Road c.1860, with little development in south-east Richmond was in 1869. Unemployment was a major issue during the 1860s and in 1862 the Richmond Council sought the repeal of the 'Yarra Pollution Prevention Act' of 1855 (which forbade fellmongeries, starch and glue factories, and boiling down works discharging waste into the Yarra River upstream from Melbourne) so that the river frontages could be opened to manufacturing. By 1865 a quarry, stone crushing mill, fellmongery and abattoir had been established on the river flats in Burnley, and by the 1870's a panoramic view of Richmond carried the caption 'Industry in Arcady'.

As with Melbourne and its other suburbs, the most active period of development in Richmond was in the 1870s and 1880s. The eastern half of the town was partly subdivided by 1874 and by 1888 most subdivision patterns were complete, the major exception being Cole's paddock on Victoria Street. Richmond was proclaimed a town in 1872 and a city in 1882. Its population in 1880 was 23,395 and in 1890 it was 38,797. The residential development trend was a marked increase from the 1850s, steeply rising until c1881 and then a plateau into the 1890s Great Depression. The rate books list 52 industrial establishments in 1880.

Houses constructed between Federation and World War One make up a substantial proportion of Richmond's building stock particularly in the eastern half of the city. Cole's paddock was subdivided by this time.

Encouraged by high tariff protection, new factories and stores were also being established, most notably Bryant & May, Wertheim's piano factory, Dimmey's Model Store, Ruwolt, Rosella, Moore Paragon and Mayall's tannery. By 1919 there were nine tanneries.

This industrial expansion continued after World War One when small gaps in the urban development were filled by inter-war housing estate and Wren's race course was changed to public housing. The Second War was the end of the first wave of urban development in Richmond and hence forms a perceptible period in the historic environment that is the basis for proposed heritage precincts in Richmond, Cremorne and Burnley.

### Specific history

This area is part of the 17 acre Crown Portion (CP) 1 sold to Messrs William Burnley, David Lyons and Matthew Cantler in 1849: land sales started immediately in the south-west corner with subdivided lots going to Burnley, Thomas King and Mitchell, Black & Follett. Inspired by the major population increase caused by gold finds in the Colony, William Burnley began to sell more house lots further north in 1852 from an estate plan that included the formation of today's Rout and Blanche Streets running east-west, joined by Wellington Street running north-south. Lots of 32-35 feet width faced onto Wellington Street and backed onto CP2 on the east or the 15 feet wide Huckerby Street, on the west, that Burnley had created to serve the lots facing Punt Road.

The 1853 plan shows Jessie and Cremorne Streets in place within Crown Portions 1&2 and the 1855 plan has buildings distributed across the north part of the Crown Portions, along Wellington (as far as Blanche) and Cremorne Streets, while to the south they front only Cremorne Street. The 1874 plan shows the north part of Wellington, Huckerby, Blanche and Jessie Streets in this precinct. King, Mitchell, Black and Follett's block, created from the south part of Crown Portion 1 in 1849, was to eventually contain the residential subdivision of Melrose and Kelso Streets in 1884 (Lodged Plan 605) with lots of 33-45 feet frontages. This estate is shown on the Tuxen 1888 plan, along with added streets such as Rout, but still there was no continuity for Wellington Street which stopped at Blanche St. The MMBW Plan 911 of 1896 showed the extension of Wellington Street southwards, past Blanche Street, but named as Melrose Street.



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### Description

The Wellington Street, Cremorne Heritage Precinct is a largely Victorian-era residential area centred on Wellington Street and extends north from the riverside industrial precinct south of Gough Street to the commercial strip of Swan Street West on the north. The arbitrary crank in the line of Wellington Street shows the two development phases (early and late Victorian-era) and exemplifies the piecemeal nature of private development, generally, in the Richmond area.

The haphazard street alignments generate unexpected house groups and vistas. There is the long weatherboard cottage row in Gough Place that now faces out across a large development site towards Punt Road: its unbroken roofline is another testimony of how Richmond, as well as being planned on a free-market basis, was also outside of the building laws that initially applied to the other inner suburbs of Melbourne (Melbourne Building Act). The construction of small, weatherboard and brick cottages in the narrow confines of the early Huckerby and Jessie Streets is another illustration of this evasion of standard building codes. The Richmond Conservation Study (1985) notes of Cremorne Cottage, at 50 Jessie Street: '...Similar size building shown in similar location on Lands Dept 1855 Map of Richmond...'. Other early houses such as 375-377 Punt Road can also be traced back in plan form to the 1855 survey map, adjoining the Rout Street entry to the precinct. A small Wellington Street house row (66-68) had rare and early brick-nogged wall construction as an indication of early construction techniques in this precinct. This method of wall construction involves brickwork placed between timber frame members and overlaid with weatherboard providing for an uncommon and environmentally sound building method. The Australian Architecture Index cites two other brick nogged houses nearby in Cremorne St auctioned in 1879.

The precinct has a number of individually significant Victorian-era buildings and building rows within its boundaries.

### Key buildings

Key buildings include late Victorian-era houses like the row-house pair at 397- 395 Punt Road, described in the 1990s as:

*'... A double-storey, rendered, Boom terrace pair, set back with a parapet. The centrepiece is (set) between abstracted Doric pilasters, supported by scrolls. Cornice and frieze-mould has vermiculated corbels; also to verandah, these on scrollbrackets. The skillion verandah, between wing-walls, has cast-iron lace valence and Composite posts, with first-storey balustrade in an unusual pattern of panels between balusters. Ground-window is tripartite with Tuscan fluted mullions. Doors have fan and sidelights. Chimneys have corbelled brick-bands' as an illustration of the range of ornament that was used in the late 19th century... '*

More typical, late Victorian-era masonry row houses line Wellington Street. Balino Cottage at 44 Wellington St, is an exception:

*'A characteristic double-fronted, symmetrical, rendered, Boom cottage, on the street line, with rich decoration. There is a balustraded parapet between piers, surmounted by balloons. The centrepiece has a scallop-shell in a round arch, with acroterion. Piers are supported by small scroll-brackets. A frieze and cornice-mould is supported by brackets, between festoons. The parapet and verandah wing-wall corbels are vermiculated, the latter on scroll-brackets. The brickwork beneath the verandah was exposed, decorated with diamond ceramic tiles. The tripartite window has barleysugar Tuscan mullions and bluestone cill and fanlight over door. The verandah is convex, with cast-iron posts, lace-valence and brackets. There are encaustic geometric tiles. The chimney has deep rendered Classical mould'...*

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Edwardian-era development is seen in houses facing Kelso Street (5, 9) as well as the former grocer's shop at 12 Kelso Street (Peter Byrne's shop in 1904 and Marcus Steel's in 1920), providing the sense of a self contained domain where provisions were available to householders within walking distance. Intermixed with these are the numerous Victorian-era houses, mainly weatherboard clad, with corrugated iron clad hipped roofs (but with some parapeted forms such as 17 Kelso Street) and little in the way of front gardens. Cremorne Court Flats in Punt Road and the Old English style house Teragram in Wellington Street, represent the well-preserved inter-war buildings that make up a minority of sites in the precinct.

### Main development period

The main development period evident in the heritage overlay is that of the Victorian and Edwardian-eras, with a contribution from well preserved interwar buildings and individually significant places of all eras.

### Contributory elements

Contributory elements include mainly (but not exclusively) Victorian-era and Edwardian-era houses, with some well preserved residential examples from the immediate post First-War era, having typically:

- pitched gabled or hipped roofs;
- one storey wall heights;
- weatherboard, face brick, or stucco wall cladding; corrugated iron, with some slate roofing;
- chimneys of either stucco finish (with moulded caps) or of matching face brickwork with capping courses;
- post-supported verandah elements facing the street;
- less than 40% of the street wall face comprised with openings such as
- windows and doors; and
- front gardens, bordered by low front fences, typically of timber picket for the Victorian and Edwardian-eras.

Contributory elements also include public infrastructure, expressive of the Victorian and Edwardian-eras such as stone pitched road paving, kerbs and channels, and asphalt paved footpaths.

### ***How is it significant?***

The Wellington Street, Cremorne, Heritage Precinct is aesthetically and historically significant (National Estate Register Criteria E1, A4) to the locality of Cremorne and the City of Yarra.

### ***Why is it significant?***

Wellington Street, Cremorne, Heritage Precinct is significant:

- As a well defined area of Victorian and Edwardian-era houses that matches the major growth periods in Richmond's and the City's housing history, complementing the existing adjoining Cremorne Heritage Overlay Area and individual heritage overlays within its boundaries;
- For some distinctive house groups such as in Gough Place, well preserved inter-war examples such as Cremorne Court Flats, and significant individual house examples; and
- For its role as one of the first development plans launched in Richmond, with some houses reflecting the 1850s estates.

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## 6. Burnley Street Precinct, Richmond (HO474)

Source: Context Pty Ltd (2014), *Heritage Gap Study, Review of Central Richmond*. \_\_\_\_\_

### ***What is significant?***

## PRECINCTS

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The Burnley Street Precinct, comprising 345-389 & 370-404 Burnley Street and 395-419 Swan Street, Richmond is significant. The following buildings and features contribute to the significance of the precinct:

- The buildings constructed from c.1880 to c.1940, as shown on the precinct map.
- The overall consistency of building form (buildings with roofs concealed by parapets, with residential quarters above if two storey and behind if single storey), materials and detailing (front walls of stucco with decorative parapets, some original timber or metal-framed shopfronts with timber or tiled stallboards and recessed entries), and siting (no front and side setbacks).
- The landmark qualities of the former Bank of Australasia.
- The nineteenth century subdivision pattern comprising regular allotments served by rear bluestone laneways.

The following places are Individually Significant and have their own statement of significance:

- Former Bank of Australasia, 377 Burnley Street,
- Former shop and residence, 380 Burnley Street,
- Shops and residences, 400-402 Burnley Street, and
- Shops and residences, 413-415 ~~Burnley~~ Swan Street.

Non-original alterations and additions to the Contributory buildings shown on the precinct map, and the buildings at 381 & 382 Burnley Street, and 411 Swan Street are Not Contributory.

### ***How is it significant?***

The Burnley Street Precinct is of local historic and architectural significance to the City of Yarra.

### ***Why is it significant?***

Historically, the precinct is a representative example of a shopping centre serving local needs that developed in response to the significant population growth of Richmond in the late nineteenth and early twentieth centuries. The terrace row at nos. 345-67 Burnley Street provides evidence of the first significant phase of residential development that created the need for the centre in the late nineteenth century. (Criterion A)

The location, close to a railway station, and around a major intersection and the mix of single and double storey commercial premises, some with original shopfronts, the corner hotel and former bank are all characteristic of these local centres. The mix of residential and commercial premises is also typical with places such as nos. 373 and 375 Burnley Street that combine a Victorian house with an Edwardian shopfront demonstrating the evolution of the precinct from residential to commercial. (Criterion D)

The precinct contains late Victorian, Federation, and Inter-war shops and one hotel with consistent and characteristic parapeted form, siting and detailing, including some original shopfronts, which contrast with the Victorian residential terrace row. The former Bank of Australasia is notable as a landmark within the precinct. (Criterion E)

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## 7. Swan Street West Precinct (HO524)

Source: GJM Heritage (2017), *Swan Street Built Form Study: Heritage Assessments & Analysis*

### ***What is significant?***

## PRECINCTS

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The Swan Street West Precinct comprising 30-42 Swan Street, Cremorne.

Elements that contribute to the significance of the place include (but are not limited to):

- The original external form, materials and detailing of the four buildings
- The high level of integrity of the buildings to their late nineteenth and early twentieth century design.

Later (post 1940) alterations and additions to the rear and shopfront are not significant.

***How is it significant?***

The Swan Street West Precinct is of local historical and architectural significance to the City of Yarra.

***Why is it significant?***

The Swan Street West Precinct is illustrative of historical development along a major, early commercial thoroughfare in the City of Yarra. As the only remaining group of intact commercial buildings from the Victorian and Edwardian periods west of the Swan Street railway bridge, this precinct demonstrates the commercial development at the west entrance of the major Swan Street 'High Street' up to the 1920s [Criterion A].

The small but intact Swan Street West Precinct clearly demonstrates the principal characteristics of a major Victorian and Edwardian 'High Street' in the City of Yarra. Typical characteristics, including parapeted facades with repetitive upper floor fenestration, rendered facades and ground floor shopfronts, are displayed in the variety of original forms, fabric and detailing of the four buildings [Criterion D].

## INDIVIDUALLY SIGNIFICANT PLACES

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1. **House**  
**234 Coppin Street, Richmond (HO245)**

Source: Allom Lovell & Associates (1998), *City of Yarra Heritage Review*

234 Coppin Street, Richmond, is of local architectural significance. The house is representative of the late 19th century double-storey form, and is a good example of the Italianate style. The house is substantially intact, retaining most of the original cement render and cast iron decoration, and is notable for its unusual broken pedimented parapet.

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2. **Former Central Club Hotel**  
**291 Swan Street, Richmond (HO285)**

Source: Allom Lovell & Associates (1998), *City of Yarra Heritage Review*

The former Central Club Hotel is of local architectural significance. The hotel is a good example of the late 19th century Italianate Boom style, notable for its lively asymmetrical facade composition and florid Victorian Mannerist cement rendered decoration. The three-storey building is a significant local landmark in Swan Street. Its association with the locally prominent Cremean family and their involvement in Catholic politics is of local historical interest.

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3. **Former Burnley Theatre**  
**365 Swan Street, Richmond (HO286)**

Source: Allom Lovell & Associates (1998), *City of Yarra Heritage Review*;  
Updated by GJM Heritage (2017), *Swan Street Built Form Heritage Review*

The former Burnley Theatre is of local social and architectural significance. Until its closure in 1958, the theatre was popular with the residents of Richmond as a social gathering place. The Swan Street facade, although not of individual architectural importance, contributes to the streetscape and is a notable local landmark.

Additional contributory elements:

- Capacious building with wide symmetrical façade to Swan Street
  - Strong horizontal lines define levels
  - Front façade has pattern of windows and engaged columns with wider central bay
  - Located on a corner with undecorated side façade to Edinburgh Street
  - Large gabled hall behind front façade.
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4. **Former State Bank**  
**214-216 Swan Street, Richmond (HO288, VHR H732)**

Refer to Victorian Heritage Database for Statement of Significance

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5. **House**  
**15 Wellington Street, Cremorne (HO294)**

Source: Allom Lovell & Associates (1998), *City of Yarra Heritage Review*;  
Updated by GJM Heritage (2017), *Swan Street Built Form Heritage Review*

## INDIVIDUALLY SIGNIFICANT PLACES

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The house at 15 Wellington Street, Richmond, is of local architectural significance. Although the overall form of the house is relatively common, this building is distinguished by its highly unusual Dutch gables, and also by the unusual proportions of the fenestration to the projecting bay. The house is an important heritage element in Wellington Street.

Works in 2016/2017 have resulted in the re-rendering of external walls, removal and replacement of cast iron friezes and balustrading and the reinstatement of a previously bricked in ground level window opening.

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6. **Former Richmond South Post Office**  
**90-92 Swan Street, Richmond (HO357, VHR H48)**

Refer to Victorian Heritage Database for Statement of Significance

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7. **Dimmeys**  
**140-160 Swan Street, Richmond (Cremorne) (HO360, VHR H2184)**

Refer to Victorian Heritage Database for Statement of Significance

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8. **The Greyhound Hotel, later Depot Hotel, now Precinct Hotel**  
**60-62 Swan Street, Richmond (HO405)**

Source: Graeme Butler & Associates (2012), *City of Yarra Heritage Gaps Review (Review 1)*;  
Updated by GJM Heritage (2017), *Swan Street Built Form Heritage Review*

***What is significant?***

The Greyhound Hotel, later Depot Hotel and Precinct Hotel has been the site of a public house and gathering place since the 1850s-60s (as 52 Swan St) when licensees included John Davies and William Perrin. The Swan Street West area had consolidated by the 1890s with continuing commercial uses evolving around the new Bowling Club Hotel. The former Bowling Club Hotel, at 36-38 Swan Street, was licensed at another Richmond location to one John Smith in 1868, with a Miss Julia Topey keeping a hotel on this site by the 1880s. The hotel was kept by a M Fitzgerald in the early 1900s but as the new century progressed, the hotel had been delicensed such that by 1920, the only hotel that had survived in this part of Swan Street was the Greyhound Hotel at the Cremorne Street corner (and rebuilt in 1926). Meanwhile shops had occupied the former Bowling Club Hotel. The hotel is a 2 storey Neo-Grec style building, with a simple and boldly executed cemented facade. A deep projecting cornice with brackets underscores the raised parapet entablature with its symmetry to the two elevations expressed through panels, with the focus at the splayed corner. Leadlight glass is evident as is the upper level and ground level joinery (part).

Additional contributory elements include:

- Two-storey wall height
- Zero front setback
- Regular pattern of rectangular fenestration at upper level
- Corner building which address both Swan Street and Cremorne Street by continuing the pattern of openings and parapet detailing
- Splayed corner
- Strong horizontal lines defining levels.

***How is it significant?***

## INDIVIDUALLY SIGNIFICANT PLACES

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The building is aesthetically, socially and historically significant (National Estate Register Criteria E1, G1, A4) to Richmond.

### ***Why is it significant?***

The building is significant: - for its good representation of a key period in the City's history, being as a site, the initial growth of commercial Swan St and as a building, the rebirth of hotel development in the 1920s after the Licence Reduction Board had eliminated other less salubrious pubs; and - as a well-preserved example of a Greek revival style public house in the local context.

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### 9. **400-402 Burnley Street, Richmond (HO429)**

Source: Lovell Chen (2012), *City of Yarra Heritage Gaps Study (Heritage Gaps Amendment two)*

### ***What is significant?***

The property at 400-402 Burnley Street, Richmond, dates from 1912, and comprises a pair of two storied Federation red brick shops with combined residences. No. 402 is the larger of the two, with both building components matching in terms of design. The buildings have transverse gable roof forms, and rear roof pitches concealed by brick parapet walls to form asymmetrical side gables. On the east façade to Burnley Street, both buildings are walled in exposed face brick with two oriel gabled bays to the first floors. The bays and the transverse gable roofs are clad in terracotta tiles; the expressed central and side walls are topped by stepped moulded brackets with orbs; and the gables to the bays are half-timbered with roughcast stucco, crown four-light bay windows, and have apron panels clad in roughcast stucco. The ground floor shopfronts are also original or early.

### ***How is it significant?***

The property at 400-402 Burnley Street, Richmond, is of local historical and aesthetic/architectural significance.

### ***Why is it significant?***

The property at 400-402 Burnley Street, Richmond, dates from 1912, and is of local historical significance. It is associated with the later development of eastern Richmond, in an area of Burnley Street, near the junction with Swan Street, which had developed by the early twentieth century into an established retail precinct. The property is also of local aesthetic/architectural significance. It is a well preserved and substantially externally intact example of a Federation two-storey combined residence and shop, in exposed face brick. It is distinguished by the transverse gable roof form, the prominent oriel gabled bays to the first floors, the substantially intact ground floor shopfronts, and the asymmetrical side gable evident on the south elevation which was increasingly common in early twentieth century shop design. Other elements of note include the roof decoration, brackets with orbs, half-timbered gables to the oriel bays, and the oriel apron panels clad in roughcast stucco.

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### 10. **Swan Street Drill Hall 309 Swan Street, Richmond (HO440)**

Source: Lovell Chen (2012), *City of Yarra Heritage Gaps Study (Heritage Gaps Amendment two)*

### ***What is significant?***

The property at 309 Swan Street, Richmond, is a rectangular shaped allotment with three buildings dating from 1916 and 1937. The buildings comprise the large former drill hall abutting the Duke Street (west) boundary; the narrower storage building abutting the Lord Street (east) boundary; and a smaller brick building between these to the Swan Street boundary. Both the hall and storage shed date from 1916 and are single storey, gable-roofed and gable-ended structures, clad in corrugated

## INDIVIDUALLY SIGNIFICANT PLACES

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galvanised iron. Both buildings retain most of their original window framing, with some exceptions. The space between the two original buildings is concreted over. The third building dates from 1937, and is a single storey, overpainted brick building, with a transverse gable roof. The 1916 drill hall is the largest building on the site, has the distinctive 'drill hall' form, and was the focus of the original training operations and subsequent Commonwealth use. The 1916 storage shed, although original, and the later 1937 building, are less distinguished elements, and more utilitarian in character and form.

### ***How is it significant?***

The property at 309 Swan Street, Richmond, is of local historical and architectural significance.

### ***Why is it significant?***

The property at 309 Swan Street, Richmond, was established as a drill hall complex in 1916 and is of local historical significance. It dates from the period when compulsory military service was introduced, and a large number of drill halls were constructed in Australia. Richmond men between the ages of eighteen and thirty, who were called upon to enlist for military service in World War One, did so at the Swan Street drill hall. They were also given some training on the site. Later Commonwealth uses included housing the 2/11th Field Regiment in the 1940s; the Department of Technical Engineering copywriting section, and the Training Depot for the Australian Army's Royal Corps of Australian Electrical and Mechanical Engineers in the 1950s; and cadet battalions in the 1970s. The former drill hall building is also of local architectural significance for demonstrating some of the principal characteristics of World War One drill halls. These include the simple gabled form, albeit carried over a large building, and the large internal space. The galvanised and overpainted corrugated iron cladding is also typical of suburban and regional drill halls around the country. In addition, it is a comparatively externally intact example of a drill hall of this era.

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## 11. 319 Swan Street, Richmond (HO441)

Source: Lovell Chen (2012), *City of Yarra Heritage Gaps Study (Heritage Gaps Amendment two)*

### ***What is significant?***

No. 319 Swan Street, Richmond, was constructed in 1889 as a two storey corner shop and residence. The building is rectangular in plan form with a chamfered corner entry and a hipped roof clad in galvanized corrugated steel, with two stuccoed chimneys with cornices. The first floor is largely externally intact, with original detailing including moulded stringcourses, double-hung sashes with stilted segmental arches, and moulded architraves with accentuated keystones. Other details include half-fluted pilasters (piers) which support a dentilled entablature and above that a continuous bracketed cornice and parapet with waisted balustrading. The chamfered corner is capped with a triangular pediment on two broad piers, enclosing a cartouche panel and topped by an orb finial. At ground level, the shop front has been altered. The rear portion of 319 Swan Street, facing Lord Street, is largely externally intact, albeit more simply detailed than the corner shop component. Beyond this wing is an adjoining contemporary development which is not of heritage significance.

### ***How is it significant?***

No. 319 Swan Street, Richmond, is of local historical and aesthetic/architectural significance.

### ***Why is it significant?***

No. 319 Swan Street is of local historical significance, as a combined shop and residence constructed in 1889 for James Davison, a baker. At that time commercial development was being consolidating in Swan Street; the 1880s date is also consistent with the core period of commercial building construction in the street. Unusually, the bakery operation in the building was sustained until the



## INDIVIDUALLY SIGNIFICANT PLACES

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mid-1970s. No. 319 Swan Street is also of local aesthetic/architectural significance. While the building is broadly consistent with many nineteenth century two-storey shop and house combinations in inner suburban main road locations, it is distinguished by the complexity and vigour of the first floor elevations in particular, and the reasonably intact side elevation to Lord Street. The segmentally arched first floor windows are usual in terraced shops, enlivened here by the Corinthian pilasters with cornice breakfronts above. The parapet and corner pediment, with baluster waisting, dentil mouldings and bracketing, are also largely intact. The chamfered corner enhances the streetscape presentation.

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### 12. **Shop and Residence** **273A Swan Street, Richmond (HO522)**

Source: GJM Heritage (2017), *Swan Street Built Form Study: Heritage Assessments & Analysis*

#### ***What is significant?***

The two-storey corner shop and residence at 273A Swan Street, Richmond, built c1885-90. Elements that contribute to the significance of the place include (but are not limited to):

- The building's original external form, materials and detailing
- The building's high level of integrity to its original design.

Later (post 1900) alterations and additions to the rear, shopfront and eastern side are not significant.

#### ***How is it significant?***

The two-storey corner shop and residence at 273A Swan Street, Richmond is of local historical and architectural significance to the City of Yarra.

#### ***Why is it significant?***

The two-storey corner shop and residence at 273A Swan Street, Richmond is illustrative of historical development that occurred along a major, early commercial thoroughfare in the City of Yarra, particularly in the 'boom' period of the 1880s [Criterion A].

The two-storey corner shop and residence at 273A Swan Street, Richmond is a fine, intact and representative example of a Victorian shop and residence. It displays typical features of the Victorian style popular in the 1880s in Richmond and across Melbourne more broadly, including a parapeted façade with repetitive ground and upper floor fenestration, articulated facades to both streets with splayed corner, rendered facades and ground floor shopfronts [Criterion D].

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### 13. **Pair of Shops** **323-325 Swan Street, Richmond (HO523)**

Source: GJM Heritage (2017), *Swan Street Built Form Study: Heritage Assessments & Analysis*

#### ***What is significant?***

The two-storey pair of shops and residences at 323-325 Swan Street, Richmond, built in 1886. Elements that contribute to the significance of the place include (but are not limited to):

- The building's original external form, materials and detailing
- The building's high level of integrity to its original design.

Later (post 1900) alterations and additions to the rear and shopfront are not significant.

#### ***How is it significant?***

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## INDIVIDUALLY SIGNIFICANT PLACES

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The pair of shops and residences at 323-325 Swan Street, Richmond is of local historical and architectural significance to the City of Yarra.

***Why is it significant?***

The pair of shops and residences at 323-325 Swan Street, Richmond is illustrative of historical development that occurred along a major, early commercial thoroughfare in the City of Yarra, particularly in the 'boom' period of the 1880s [Criterion A].

The commercial premises at 323-325 Swan Street, Richmond are a fine, intact and representative example of a pair of Victorian shops and residences. They display typical features of the Victorian architectural style popular in the 1880s in Richmond and across Melbourne more broadly, including a parapeted facade with repetitive upper floor fenestration, rendered facades and ground floor shopfronts [Criterion D].

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## INDIVIDUALLY SIGNIFICANT PLACES WITHIN PRECINCTS

Some 'Individually Significant' places within heritage precincts have a Statements of Significance. Those places that have one are listed below.

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### **1. Corner Hotel**

#### **57-61 Swan Street, Richmond (within HO335)**

Source: GJM Heritage (2017), *Swan Street Built Form Study: Heritage Assessments & Analysis*

#### ***What is significant?***

The two-storey hotel known as the Corner Hotel, 57-61 Swan Street, Richmond, built in the 1960s.

#### ***How is it significant?***

The Corner Hotel, 57-61 Swan Street, Richmond is of local historical and social significance to the City of Yarra.

#### ***Why is it significant?***

The Corner Hotel, Richmond has made a strong contribution to the commercial and social life of Richmond from its establishment in the early 1870s and an important and highly influential contribution to the music industry as an important live music venue since its rebuilding in the mid 1960s and renovation in the 1990s [Criterion A].

The Corner Hotel, Richmond is of particular social significance for its long-term and continued use as a live music venue. It is a well-established and well-known venue which is considered amongst the most pre-eminent in the City of Yarra and the broader community [Criterion G].

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### **2.1. Shops and Residences**

#### **454-456 Church Street, Cremorne (within HO335)**

Source: O'Connor, John & Coleman, Roslyn et al. (1985), *Richmond Conservation Study : undertaken for the City of Richmond*

A distinctive group of gabled Edwardian shops by the distinguished architect Nahum Barnet.

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### **3.2. H. E. McNaughton ironmongery & residence, former**

#### **69 Swan Street, Richmond (within HO335)**

Source: Graeme Butler & Associates (2007, 2013), *City of Yarra Review of Heritage Overlay Areas*

#### ***What is significant?***

The former H.E. McNaughton ironmongery and residence at 69 Swan Street, Richmond, was created in 1924 for Henry Ernest McNaughton and has a close association with him. The place has a good integrity to its creation date. Fabric from the creation date at the H.E. McNaughton ironmongery and residence is locally significant within the City of Yarra, compared to other similar places from a similar era.

#### ***How is it significant?***

The H.E. McNaughton ironmongery and residence, former at 69 Swan Street, Richmond is architecturally significant to the locality of Richmond and the City of Yarra.

#### ***Why is it significant?***

## INDIVIDUALLY SIGNIFICANT PLACES WITHIN PRECINCTS

Some 'Individually Significant' places within heritage precincts have a Statements of Significance. Those places that have one are listed below.

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The H.E. McNaughton ironmongery and residence is significant as a well preserved inter-war parapeted brick and stucco shop and residence with original or early shopfront and the owner's name cemented in bas-relief on the parapet.

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### 4.3. **National Bank of Australasia 105 Swan Street, Richmond (within HO335)**

Source: O'Connor, John & Coleman, Roslyn et al. (1985), *Richmond Conservation Study : undertaken for the City of Richmond*

This suburban branch of the National Bank of Australasia was erected in 1886-7 by the notable bank architect Albert Purchas. Its imposing three storey height, prominent corner location, and fine architectural detailing are important components of the building's significance.

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### 5.4. **Shops and residences 232 & 234 Swan Street, Richmond (within HO335)**

Source: O'Connor, John & Coleman, Roslyn et al. (1985), *Richmond Conservation Study : undertaken for the City of Richmond*

A rare example of a pair of small shops designed in a boom style classicism, completely intact above verandah level.

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### 6.5. **M. Ball & Co, former 236 - 240 Swan Street, Richmond (within HO335)**

Source: O'Connor, John & Coleman, Roslyn et al. (1985), *Richmond Conservation Study : undertaken for the City of Richmond*

A Richmond landmark used as a drapery since 1871, retaining some original shop fittings and a now rare aerial cash conveyor.

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### 7.6. **Whitehorse Hotel, former 250-252 Swan Street, Richmond (within HO335)**

Source: O'Connor, John & Coleman, Roslyn et al. (1985), *Richmond Conservation Study : undertaken for the City of Richmond*

A rare example of an early 1850's stone commercial building designed in a colonial Regency style.

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### 8.7. **Shops and Residences 413 - 415 Swan Street, Richmond (within HO474)**

Source: Lovell Chen (2012), *City of Yarra Heritage Gaps Study (Heritage Gaps Amendment two)*

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***What is significant?***

## INDIVIDUALLY SIGNIFICANT PLACES WITHIN PRECINCTS

Some 'Individually Significant' places within heritage precincts have a Statements of Significance. Those places that have one are listed below.

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Nos 413-415 Swan Street, Richmond is a Victorian two storey brick terrace pair of shops and combined residences. The ground floor shopfronts have been modified, but the rendered first floor facades are intact, with no. 413 remaining unpainted save for the c.1920s 'Monopole Magnum Cigars' sign. Intact details include double-hung sash windows, window architraves, brackets, supporting stringcourse and mouldings, plus pedimented parapets incorporating panels, brackets, cornices and mouldings, urn pedestals and dividers. The buildings have been extended to the rear; these later elements are not of heritage significance.

### ***How is it significant?***

Nos 413-415 Swan Street, Richmond is of local historical and aesthetic/architectural significance.

### ***Why is it significant?***

Nos 413-415 Swan Street is of local historical significance, as a pair of two storey brick shops and combined residences constructed for carpenter, J Wood in 1892. This was just after the period of commercial development consolidation in Swan Street, and unusually during the severe economic Depression of the early 1890s. The buildings were subsequently occupied by a range of retailers and commercial operations, typical for a main street of the late nineteenth and early twentieth century. These included sellers of dairy produce, a greengrocer, butcher, hairdresser and tobacconist. The subject property is also of local aesthetic/architectural significance. It is a well-preserved pair of two storey shops/residences, broadly consistent with many similar nineteenth century commercial buildings in inner suburban main road locations. However, the first floor facades retain their original form and detailing; in particular, the upper level to no. 415 has remained unpainted. This façade is further distinguished through the retention of the 'Monopole Magnum Cigars' sign. Both first floors also have prominent and well-detailed arched pedimented parapets.

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### **8. Bank of Australasia (Former)** **377 Burnley Street, Richmond (within HO474)**

Source: Context Pty Ltd (2014), *Heritage Gap Study, Review of Central Richmond*.

### **What is significant?**

The former Bank of Australasia, erected in 1889 to a design by the prominent bank designer, Anketell Henderson, of the firm Reed, Henderson and Smart, at 377 Burnley Street, Richmond is significant. It is a freestanding symmetrical two-storey rendered brick building. The ground floor facade has banded rusticated walls, and a central entrance door flanked by tripartite shallow arched windows, which are timber-framed with fixed central lights and double-hung sidelights. The first floor has a loggia-style balcony; the projecting entrance section extends up through the first floor and has an arched opening to the facade. There are three openings in the wall behind. The loggia has a simple patterned balustrade, and a skillion-profile roof; the wing walls have arched openings and their copings terminate in small pediments. There is a prominent parapet with a wide moulded cornice. There is also a string course below the loggia.

Non-original alterations and additions to the building are not significant.

### **What is significant?**

The former Bank of Australasia, at 377 Burnley Street, Richmond, is of local architectural significance.

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## INDIVIDUALLY SIGNIFICANT PLACES WITHIN PRECINCTS

Some 'Individually Significant' places within heritage precincts have a Statements of Significance. Those places that have one are listed below.

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### Why is it significant?

Architecturally, the building is a good example of an austere Classically-styled building which is in contrast to much of the more flamboyant boom style Italianate designs of the period. It is an important landmark within the Burnley Street commercial precinct. The building is an important work in the oeuvre of prominent bank architect, Anketell Henderson. Henderson was an important protagonist of the austere classical style of bank architecture of the 1880s. (Criteria D, E & H)

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### 9. Shops and Residences

#### 380 Burnley Street, Richmond (within HO474)

Source: Context Pty Ltd (2014), *Heritage Gap Study, Review of Central Richmond*.

### What is significant?

The shop and residence, constructed by 1888, at 380 Burnley Street, Richmond is significant. This is a single storey late Victorian shop, one of a pair with no.378. The parapet has a moulded cornice above a stringcourse, each framed by vermiculated corbels inset with lions-heads. The semi-circular pediment has a small acroterion and is flanked by small scrolls, with small scrolls adjacent to the low piers at either end of the parapet that appear to have once supported orbs or urns. The shopfront has timber-framed windows with fixed display windows below highlights, and panelled timber stallboards over a bluestone plinth. The window frames are slender, which is typical of the period, and the recessed central entry enhances the symmetry of the building.

Non-original alterations and additions to the building and signage are not significant.

### How is it significant?

The shop and residence at 380 Burnley Street, Richmond is of local historic, architectural and aesthetic significance to the City of Yarra.

### Why is it significant?

Historically, it is significant as one of the oldest surviving shops in Burnley Street and is associated with the beginnings of the transformation of the southern Burnley Street into a local shopping centre during the late nineteenth century. (Criterion A)

It is also significant as a representative example of a shop with typical late Victorian form and detailing including the parapet ornamentation and a timber-framed shopfront with stallboards and a recessed central entry below the pediment that emphasises the symmetry of the building. It is notable for its high degree of intactness, which includes the original shopfront. While many late Victorian shops survive in Richmond very few retain their original shopfront. (Criteria B, D & E)

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