

Attachment 1 - PLN17/0635 - 317 & 326 - 348 Church Street Richmond - Site Location Plan

SUBJECT LAND: 371 Church Street Richmond ↑ North  **Subject Site**

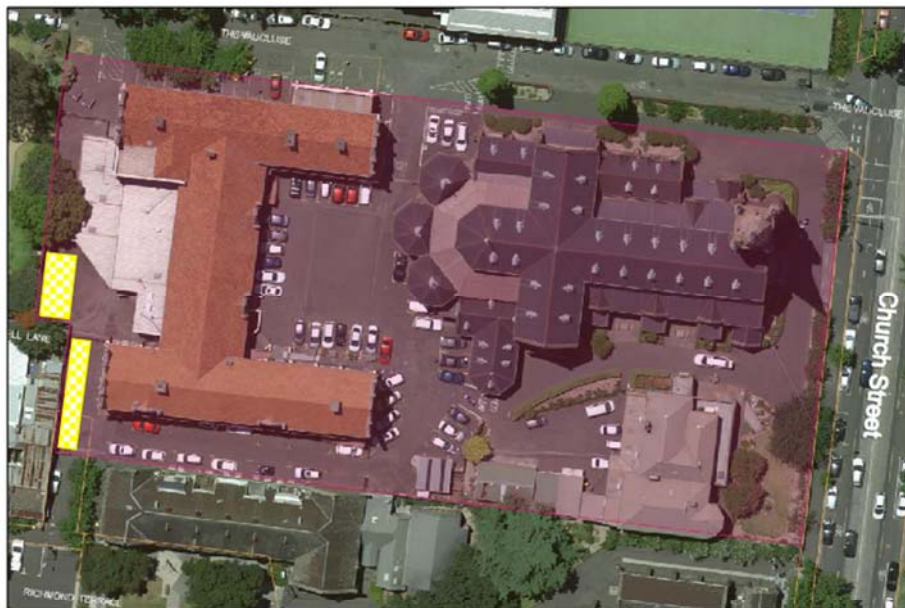
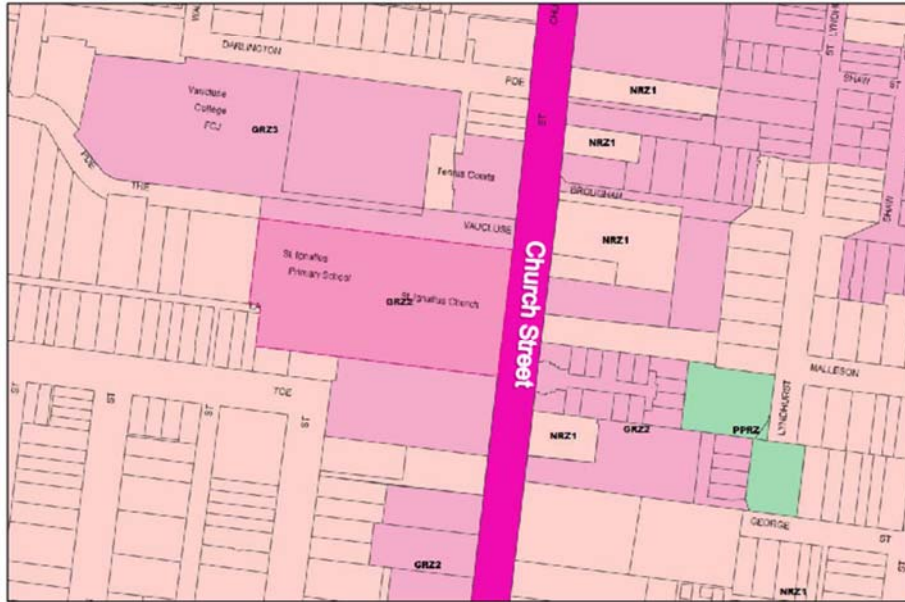
Location of Proposed Childcare Centre



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SUBJECT LAND: 326 – 348 Church Street Richmond ↑ North ■ Subject Site

Location of Proposed Car Parking Spaces



Aerial view of No. 326 – 348 Church Street Richmond - car parking spaces highlighted in yellow

Attachment 2 - PLN17/0635 - 371 & 326 - 348 Church Street Richmond - Heritage advice

**City of Yarra
Heritage Advice**

Application No.:	<i>PLN17/0635</i>
Address of Property:	371 Church Street, Richmond
Planner:	<i>Nish Goonetilleke</i>
Yarra Planning Scheme References:	<p><i>STATE POLICY:</i></p> <ul style="list-style-type: none"> • <i>Clause 15.03 Heritage</i> <p><i>LOCAL POLICY:</i></p> <ul style="list-style-type: none"> • <i>Clause 21.05-1 Built Form (Heritage)</i> • <i>Clause 43.01 Heritage Overlay</i> • <i>Clause 22.02 Development Guidelines for sites subject to the Heritage Overlay</i>
Heritage Overlay No. & Precinct:	<i>HO315 – Church Street Precinct Schedule to the heritage overlay: Paint controls</i>
Level of significance:	<i>Non-contributory (Appendix 8, City of Yarra Review of Heritage Overlay Areas 2007- updated Feb 2017)</i>
General description:	<i>Full demolition for the construction of a two storey childcare centre including new crossover and new fence treatment.</i>
Drawing Nos.:	<i>Set of 9 x A1 drawings prepared by InSite Architects, received by Council and date stamped 10 November 2017</i>

DESCRIPTION OF THE HO315 AREA

The precinct is historically significant as one of the first parts of Richmond to be subdivided and developed, as expressed by early buildings like Messenger House 333 Church Street, from the 1840s. It is also historically significant as the chosen site for a high number of important 19th and early 20th century ecclesiastical and civic buildings, and some Melbourne landmarks, as well as substantial residential buildings that were attracted to the area by its elevated topography, high amenity and proximity to churches. (Criterion A)

The precinct is also historically and socially significant, as the site of key civic or institutional buildings in Richmond from the 19th century through to the 1920s (i.e. The Richmond RSL Hall), and the commercial development extending up Church Street from the Swan Street and Bridge Road shopping areas with shops dating from the late 19th and early 20th centuries, as part of the cultural context of Victorian and Edwardian-era life on the hill. (Criterion A)

The precinct is aesthetically significant for its impressive collection of architecture from the late nineteenth and early twentieth century such as the William Wardell-designed St Ignatius Roman Catholic Church, which is a well-known and prominent landmark across the metropolitan area. (Criterion E)

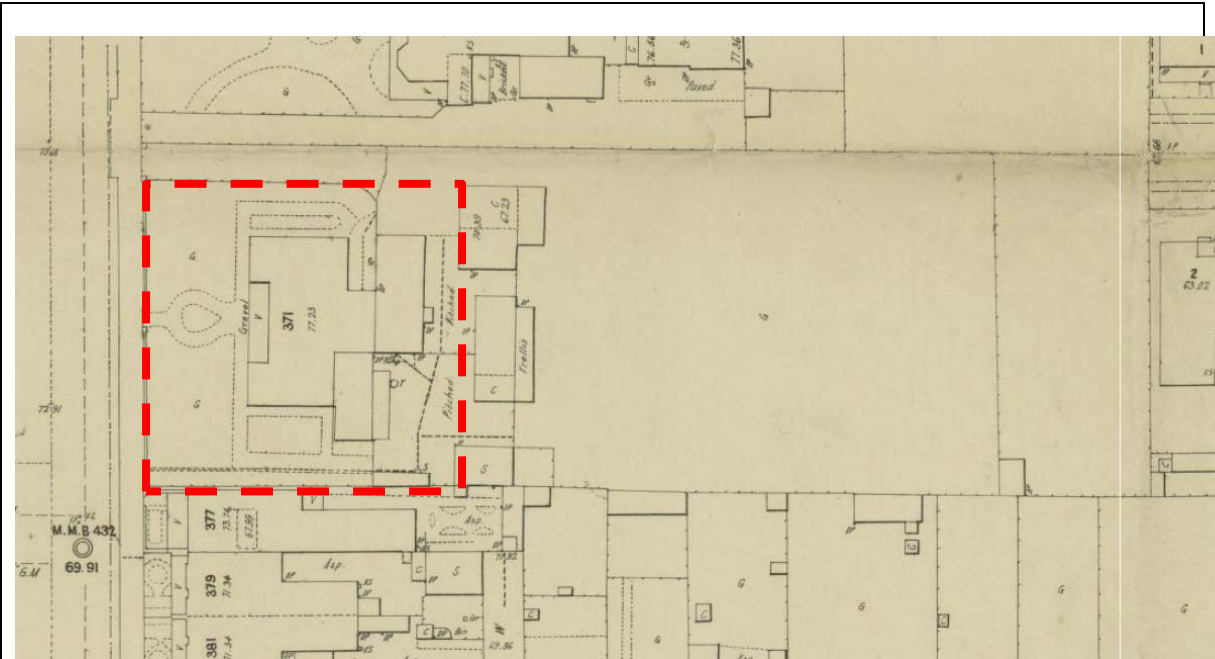
CONTEXT DESCRIPTION:

The subject site is a square allotment with principal frontage to Church Street and no rear or side access. There is a walkway to the north which allows views to the property from No.361 Church Street. It is located on the eastern side of the street, between Elm Grove to the south and Brougham Street to the north.

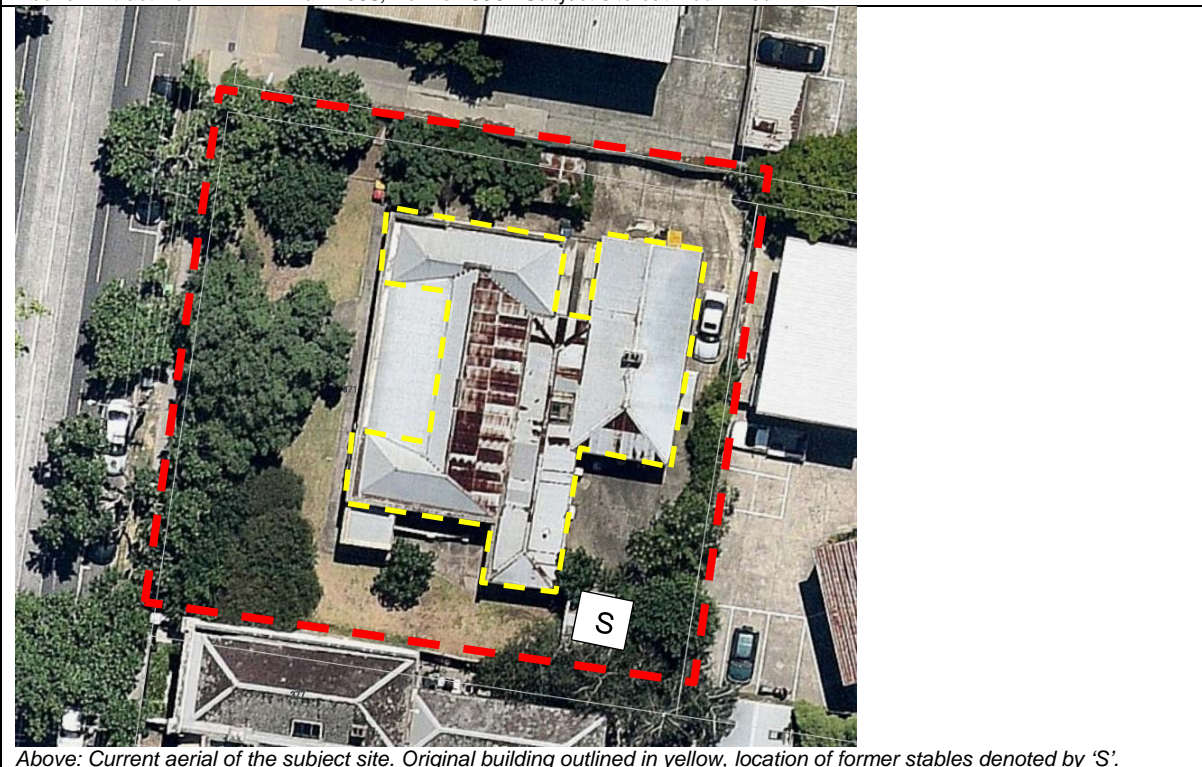
Historically the site appears to have originally been a Victorian-era villa. The MMBW shows a building footprint which is still extant today, although the original building has been substantially altered and is barely recognisable. The site is contained by a cast iron palisade fence set in a large bluestone base which steps down toward the south.

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Immediately adjoining the subject site to the south is a row of four two-storey Victorian-era terrace houses which are all individually significant. To the north is a block of two-storey apartment buildings which are non-contributory. Despite the high number of individually significant properties in the area, immediately across from the subject site are a number of non-contributory apartment buildings. Large and well established trees help to conceal non-contributory properties in the summer.



Above: Extract from MMBW Plan 1065, from c. 1896. Subject site outlined in red.



Above: Current aerial of the subject site. Original building outlined in yellow, location of former stables denoted by 'S'.

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Above: View towards subject site from Church Street



Above: View towards subject site from across Church Street, as seen on Google Streetview (in winter, for clarity).



Above: View towards subject site from walkway to the north.



Above: View towards properties south of the subject site as seen from across Church Street. Subject site denoted by red dot.

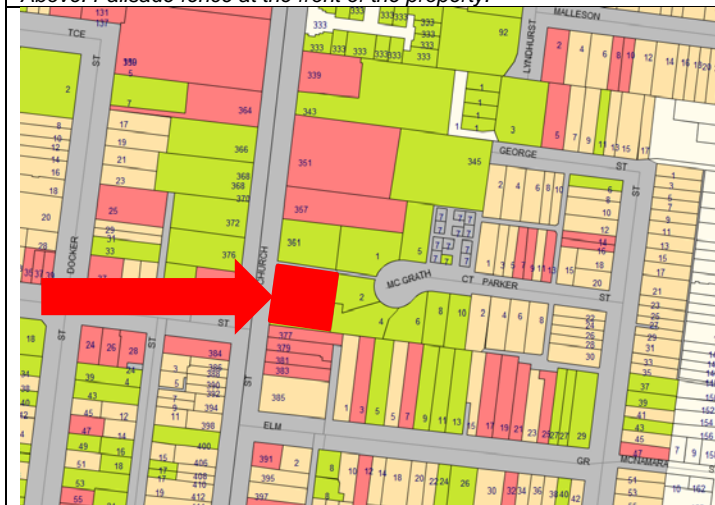
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Above: View towards properties north of the subject site as seen from across Church Street. Subject site denoted by red dot.



Above: Palisade fence at the front of the property.



Left: Extract from Yarra GIS showing individually significant (pink), contributory (cream) and non-contributory (green) buildings within the vicinity of the subject site (red).

ASSESSMENT OF PROPOSED WORKS:

Comments regarding proposed demolition:

The extent of demolition proposed by this application includes full demolition of all structures on the site, excluding the palisade fence.

The key consideration for assessing this aspect of the works is whether the proposed demolition will **adversely** affect the significance of the heritage building or the **broader** heritage precinct.

Clause 22.02-5.1 of the Yarra Planning Scheme generally encourages the retention of a building in a heritage place, unless

- The building is identified as being **not contributory**.

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No new evidence has been produced to suggest that the property's status should be changed to contributory. Apart from the fence, it appears that much of the main building has been so unsympathetically altered that that its original character is beyond recognition from Church Street.

Although restoration of a heritage place is encouraged by Clause 22.02-5.3, this is dependent on evidence to support its accuracy. It is believed that despite a number of period features scattered within the building, evidence to support the accuracy of any restoration or reconstruction of the original building is not known to exist.

Full demolition of the existing building is supported as it will not adversely affect the broader heritage precinct.

Palisade fencing:

The proposal requires partial demolition of the existing front fence to allow vehicular access to the site. The original access point for vehicles to the property was along the northern boundary of the site. This area is now outside of the title boundaries. The request for a point of vehicular access to the site is not considered unreasonable. The most appropriate location for an opening would be at either end of existing fence. The proposal to remove a section of palisade fencing at the southern end is therefore considered acceptable. The section of fencing removed should however be retained on site in a meaningful location shown on the approved drawings.

Comments regarding new development, alterations and additions:

The extent of new works proposed by this application includes development of a two-storey childcare facility.

The key consideration for assessing this aspect of the works is whether the proposed development will adversely affect the significance, character or appearance of the broader heritage precinct.

Setbacks:

The proposed front setback for the new development will be 7.8 metres from the front title boundary, which is approximately 2metres behind the verandah of immediately adjoining individually significant buildings. The setback is for an open outdoor plan area (deck). The façade is setback over 14.5metres from the front title boundary.

The proposed setback would allow individually significant buildings to dominate in this section of Church Street, and would maintain a significant garden setback as originally designed. **The proposed front setback is supported.**

The proposed side setbacks for the new development will be 1.8 metres from the northern boundary and over 5.9 metres from the southern boundary (to allow for a driveway to car parking). **The proposed side setbacks are supported** as ensuring new fabric is sufficiently setback from individually significant fabric; and for reducing the width of the new development.

However the wide setback will create a new 'void' which was traditionally landscaped area contained behind a traditional fence (which needs to be demolished in this zone to allow access). **It is strongly recommended that the driveway be constructed using a permeable driveway** (see image below) to help reduce the amount of bare concrete in this zone. A permeable driveway will also help to assist with stormwater management on the site.



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Above: examples of permeable driveway that would reduce the visual impact of a large hard standing area for cars on the streetscape

Scale/height:

The proposed facade height for the new development will be 6.3metres. Immediately surrounding individually significant properties are between 6metres and 9metres tall, not including the churches which are much larger. **The scale of the new development is supported** as being appropriate for, and recessive within the historic context.

Appearance:

Clause 22.02-5.7.1 of the Yarra Planning Scheme encourages the design of new development to:

- Respect the pattern, rhythm, orientation to the street, spatial characteristics, fenestration, roof form, materials and heritage character of the surrounding historic streetscape.
- Be articulated and massed to correspond with the prevailing building form of the heritage place or contributory elements to the heritage place.
- Be visually recessive and not dominate the heritage place.
- Be distinguishable from the original historic fabric.
- Not remove, cover, damage or change original historic fabric.
- Not obscure views of principle façades.
- Consider the architectural integrity and context of the heritage place or contributory element

A contemporary design approach has been adopted for the proposed new building. This approach is not unacceptable provided that adequate respect is given to the heritage character of the surrounding area.

Surrounding contributory properties are generally:

- Masonry with painted or rendered finish; or bluestone walls
- Slate or sheet metal roofing
- Timber windows and doors
- Cast iron balustrades
- Cast iron and bluestone palisade, or timber picket, fences
- Have well established gardens.

The proposed external materials for the new development will be: grey rendered walls, glass and timber batten balustrades, aluminium copper feature awnings and dark coloured windows and roof. Overall the material palette is light and recessive and the feature aluminium cladding will be setback over 14.5metres from the front title boundary, so won't overly detract from the HO area. **The material palette is supported.**

The plans note that there is a landscape plan for the outdoor play area at the front of the property. A landscaped area in this zone is supported for maintaining a deep garden setback, and for detracting from contemporary materials behind.

Regarding new secondary fence:

New fences are required around the site, as it is a childcare centre. The plans note that a new fence constructed of clear Perspex on steel posts is to be built behind the existing palisade fence. It is strongly recommended that the new fence be setback from the original fence so that the space between the two can be planted out and eventually conceal the appearance of the secondary fence.

Regarding the proposed bin store:

The location of the proposed bin store will be forward of the main building, i.e. within the front setback of the property. This is not considered an appropriate location for such a built structure as front setbacks

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within this part of Church Street were traditionally reserved for garden landscaping. The proposed bin store will be clearly visible from the street and not in character with the street where such structures are usually setback either to the side or rear of the main building.

The location of the proposed bin store must be setback behind the alignment of the main building at ground level.

RECOMMENDATIONS:

On heritage grounds the works proposed in this application may be approved subject to the following conditions:

- 1) That the section of original fencing to be removed must be retained on site in a meaningful location shown on the approved drawings;
- 2) That the proposed new front fence be setback from the original palisade fence so that the space between the two can be planted out and eventually conceal the appearance of the secondary fence;
- 3) That that the proposed driveway and car park be constructed using a permeable material that softens the visual impact of the hard standing area from the street;
- 4) That the location of the proposed bin store must be setback behind the alignment of the main building at ground level.

SIGNED:



Ruth Redden

DATED: 31 January 2018

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23 January 2018

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City of Yarra
PO Box 168
Richmond VIC 3121

Attention: Nish Goonetilleke

Dear Nish

**371 & 326-348 Church Street, Richmond
Planning Assessment Acoustical Review
PLN 17/0635**

SLR Consulting Australia Pty Ltd (SLR) has been retained by the City of Yarra to provide a review of the acoustic assessment report for the childcare centre proposed for 371 & 326-348 Church Street, Richmond.

Details of the report are as follows:

- Title: Proposed childcare Centre – 371 Church Street, Richmond
- Reference: 3OU-17-0133-DRP-633253-1
- Date: 5 October 2017
- Prepared by: Vipac Engineering and Scientists (Vipac)

1 Project Details

(Sections 1 to 4 of the acoustic report)

The proposal is for a new 87 placement, two level childcare centre to be constructed at 371 Church Street Richmond.

The breakdown of children numbers into age groups is provided.

Noise sensitive receivers are identified, and the receiver height (number of storeys) is provided.

The report has been prepared to address voice noise impacts from outdoor play areas of the childcare centre.

The locations of the outdoor play areas are shown in the drawings referred to in the acoustic report. The proposal includes:

- A large (216 m²) ground floor outdoor play area adjacent to Church Street
- A smaller (89 m²) ground floor outdoor play area in the south east corner of the site

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- A large (314 m²) first floor outdoor play area wrapping around the western and southern sides of the building.

SLR Comment:

The site, the proposed use and the development is adequately described and the nearest noise sensitive receivers have been clearly identified. We note, however, that the building at 361 Church Street is four levels high to the eastern end of the site (the report identifies the building at this address as being 3 storey).

The proposed hours of operation for the childcare centre are not stated, although Vipac recommend that outdoor play be restricted to the hours of 9 am to 5 pm. The planning report for the project indicates that the application is for 6:30 am to 7 pm Monday to Friday.

Noise impacts to and from the site are not explicitly identified, however the report has been prepared to address impacts from children's voice noise.

Noise impacts from vehicle movements (particularly during the early morning drop off period), and from mechanical plant (e.g. air conditioning plant and kitchen exhaust fans), are typically also addressed in assessments of noise from childcare centres. Noise impacts to the centre from ambient sources such as road traffic are also usually considered.

2 Background Noise Monitoring

(Section 5 of the acoustic report)

An attended measurement of background noise was conducted on 19 September 2017 between 10 am and 11 am in the front garden of the subject site at 371 Church Street. The measured background noise level was 51 dBA L₉₀.

SLR Comment:

The noise measurement was undertaken outside peak hour, during a time when impacts from voice noise are likely to be greatest. Consequently the limited attended measurement (as opposed to an extended period of unattended noise monitoring) is a reasonable approach to quantifying background noise levels for the purpose of setting noise limits.

This location appears reasonable for the setting of noise limits to locations equally exposed to road traffic noise. However, dwellings to the rear of the site, e.g. R3 and R4, and to a lesser extent R2 and the rear of R6, are not as exposed to Church Street, and consequently the background levels at these locations may be lower. Additional attended measurements towards the rear of the subject site would have assisted in determining the background noise levels at receiver locations more distant and/or more shielded from Church Street.

3 Childcare Centre Assessment Standards and Noise Limits

(Section 6 of the acoustic report)

Children's voice noise is proposed to be assessed to an Leq target of 'background + 5 dB' for extended periods of outdoor play, and 'background + 10 dB' for up to two hours of outdoor play. This approach is consistent with the Association of Australian Acoustical Consultants (AAAC) 'Guideline for Childcare Care Centre Acoustic Assessment (2013).

The identified noise limits are 56 dBA Leq for extended periods of play, and 61 dBA Leq if the outdoor areas are used less than 2 hours per day.

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SLR Comment:

There is some consensus among acoustical consultants that the AAAC guideline noise limit for extended periods of outdoor play is inappropriately stringent. The resultant noise limits can be extremely difficult to meet and can effectively preclude outdoor play in some instances. They can also be lower than the limits applied to commercial noise. In our reviews of childcare centres for the City of Yarra we have approved noise limits of 'background + 10 dB' and SEPP N-1 limits for daytime use of outdoor play areas. However, as indicated above, lower background noise levels are likely to be relevant for a number of receivers. These may offset the less stringent criteria we have proposed.

4 Voice Noise Assessment

(Sections 6 and 7 of the acoustic report)

Noise from the outdoor play area has been modelled using the SoundPlan noise modelling package.

Noise data for children in the outdoor play area has been sourced from the AAAC guideline for childcare centres. The mid-point in the AAAC recommended range of noise levels has been used in the modelling.

The following acoustic barriers have been incorporated in the model:

- 2.5 m high along the northern boundary (both ground floor and first floor)
- 1.8 m around the first floor outdoor play area
- 2.0 m high along the eastern boundary, adjacent to the small ground floor outdoor play area
- 1.8 m high along the western side of the large ground floor outdoor play area.

A 3-D image of the noise model, showing the sources as extended area sources, is included in Appendix B.

Modelling has been undertaken for two scenarios:

- All children playing outside (assessed to 'background + 10 dB'), and
- 50% of children outside (assessed to 'background + 5 dB').

The predicted noise levels at receiver locations are up to 60 dBA Leq for all children playing outside, and 57 dBA Leq for 50% of children. The predicted levels comply with the identified noise limits for the scenarios modelled, however Vipac note that an exceedance of 4 dB may occur if all children are outside for extended periods.

SLR Comment:

The child voice noise data used by Vipac is not representative of 'worst case'. This approach is reasonable provided that the assessment is moderately conservative in other areas, such as in the identification of noise limits, prediction methodology, modelling scenarios and compliance margin.

Our indicative calculations suggest similar voice noise levels at receiver locations to those presented by Vipac.

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5 Other Matters**5.1 Road Traffic Noise to the Childcare Centre**

Noise to the outdoor play areas of the childcare centre is not directly addressed in the acoustic report. The AAAC guideline recommends that traffic noise in outdoor play areas should not exceed 55 dBA Leq,1hr. While an explicit assessment of noise impacts to the childcare outdoor play areas has not been conducted, it would appear unlikely that the levels would exceed 55 dBA Leq,1hr given the noise barriers proposed by Vipac. As such, further consideration of this issue is not necessary from our perspective.

5.2 Vehicle Noise from the Childcare Centre

Noise from vehicles and voice as children are dropped off at the centre is not assessed. For this centre, which proposes to operate during the 6:30 am to 7 am period, consideration should be given to the issue of sleep disturbance.

However, the child drop off carpark will be accessed via Church Street, and the carpark itself is in close proximity to this major road and tram line. Given that ambient noise levels are currently high, noise impacts from use of the carpark to existing dwellings would seem unlikely. As such, further consideration of this issue is not necessary from our perspective.

5.3 Mechanical Services Noise

Noise from mechanical services associated with the project should meet SEPP N-1 limits at all existing dwellings. The project is likely to include a number of air conditioning condenser units and a kitchen exhaust fan. Given that the site is surrounded by overlooking dwellings, there is potential for equipment to be located such that SEPP N-1 limits are exceeded. We recommend that the noise limits be identified in the report and that indicative advice is provided regarding management of noise. The SEPP N-1 zoning levels could be used to provide an adequate indication of noise limits, or the limits could be based on further background noise monitoring (the current background noise monitoring conducted at the front of the site is not sufficient for determining SEPP N-1 noise limits to other receiver locations).

6 SLR Summary

The acoustic report prepared for the childcare centre proposed for 371 Church Street generally addresses the issues of noise from the proposed use. Moderately high noise barriers have been proposed, and Vipac recommend that the outdoor areas are only used between the hours of 9 am and 5 pm. We have raised several issues with respect to the report and these should ideally be addressed. However the issues raised may not have significant implications for the overall design. A summary of the items we have raised is provided below:

- The background noise levels used to set noise limits are, in our opinion, inappropriate for receivers that are more distant or more shielded from Church Street than the measurement location. Measurements relative to these receivers should ideally be undertaken. This may involve measuring closer to the eastern boundary of the site with the microphone on a pole, or measuring at ground level, set as far back from Church Street as far as possible, but still with a limited line of sight to the road (the more critical elevated receivers all have some exposure to traffic noise from Church Street).
- The 'background + 5 dB' target proposed, while in accordance with the AAAC guideline document for voice noise from childcare centres, is more stringent than we have accepted on other City of Yarra projects. We have approved targets of 'background + 10 dB' for extended use of outdoor play areas in recognition that outdoor play is important to children's health, and that the 'background + 5 dB' target cannot practically be met in many situations. We have also accepted assessment of noise from outdoor play areas to SEPP N-1 based noise limits.

SLR Consulting Australia Pty Ltd

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- The mid-point (rather than the upper level) of the AAAC voice level range for children has been used by Vipac in the assessment (many consultants use the upper level). This approach removes a level of conservatism from the assessment. It is acceptable provided that the assessment is not overly optimistic in other areas. This approach would, for example be reasonable from our perspective provided that noise limits were predicted to be met for at least 80% of children playing outside.
- Assessment should also be conducted to the fourth floor receivers at 361 Church Street.

In summary, we recommend that compliance with 'background + 10 dB' targets to be demonstrated with noise limits based on revised background noise levels at more shielded locations, assuming at least 80% of children are playing outside. The use of the mid-point for the sound power levels is considered reasonable if this approach is adopted.

An indicative assessment should also be provided in the report for mechanical plant noise. This assessment should include identification of noise limits and general guidance for achieving those limits, such as an indication of suitable locations for plant and equipment, and the potential for noise barriers to be required. We recognise that a full assessment of plant and equipment is unlikely to be able to be conducted during this stage of the design.

Yours faithfully,



Dianne Williams
Associate – Acoustics

Checked by: JA

Attachment 4 - PLN17/0635 - 371 & 326 - 348 Church Street Richmond - Engineering comments



MEMO

To: Artemis Bacani
From: Nish Goonetilleke
Date: 28 February 2018
Subject: Application No: PLN17/0635
 Description: Child Care Centre
 Site Address: 371 Church Street, Richmond

I refer to the above Planning Application received on 1 December 2017 and the accompanying Traffic Impact report prepared by Ratio Consultants in relation to the proposed development at 371 Church Street, Richmond. Council's Engineering Services unit provides the following information:

CAR PARKING PROVISION

Proposed Development

Under the provisions of Clause 52.06-5 of the Yarra Planning Scheme, the development's parking requirements are as follows:

Proposed Use	Quantity/ Size	Statutory Parking Rate	No. of Spaces Required	No. of Spaces Allocated
Child Care Centre	87 Children	0.22 spaces to each child	19	15

The development has a parking shortfall of four spaces. To reduce the number of car parking spaces required under Clause 52.06-5 (including to reduce to zero spaces), the application for the car parking reduction must be accompanied by a Car Parking Demand Assessment.

Car Parking Demand Assessment

In reducing the number of parking spaces required for the proposed development, the Car Parking Demand Assessment would assess the following:

- *Car Parking Demand.*
Car parking associated with child care centres is generally long-stay parking for employees and short-stay/drop-off and pick-up parking for parents/guardians. It is agreed that the majority of parents would drop their children off at a childcare centre between 7.00am and 9:30am and collect their children between 3:00pm and 6pm.
- *Empirical Rates for Child Care Centres.*
For the empirical rate of a child care centre use, the *Traffic Impact Assessment* report for a similar development at 186 Esplanade West and 85 Liardet Street, Port Melbourne has been referenced. The development consists of a Family and Children's Centre for 135 children. Ratio Consultants had conducted a number of parking and questionnaire surveys at various childcare facilities in Vermont, Nunawading, and Box Hill South. Based on the survey results, Ratio Consultants adopted a car parking rate of 0.12 parent spaces per child and 0.08 staff spaces per child. Applying this rate for the proposed development would generate a parking demand of 10 parent spaces per child and six staff spaces per child. The rates adopted by

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Ratio Consultants are consistent with the rates generated by other traffic consultants ranging between 0.19 to 0.21 spaces per child.

The development's short-term parking for drop-off/pick-up of children would be accommodated in the seven on-site spaces. Any overflow with the development's short-term parking would be accommodated in the surrounding road network. The long-stay parking demand would be accommodated at an off-site facility within the grounds of St Ignatius Church. A total of seven spaces are provided. Staff would be fully aware with the lack of all-day parking in the Richmond area. This would be a disincentive for some staff to drive to the site. The site's proximity to public transport nodes would encourage some staff to catch public transport to the site.

- *Availability of Public Transport in the Locality of the Land.*
The site is within walking distance of tram services along Church Street, Bridge Road and Swan Street. Train services can be obtained from East Richmond and Richmond railway stations.

Appropriateness of Providing Fewer Spaces than the Likely Parking Demand

Clause 52.06 lists a number of considerations for deciding whether the required number of spaces should be reduced. For the subject site, the following considerations are as follows:

- *Availability of Car Parking.*
Ratio Consultants has commissioned car parking surveys on Wednesday 7 November 2016 between 7.00am and 10.00am, and 3.00pm and 6.30pm. The survey encompassed all suitable public car parking spaces near the site and includes sections of Church Street, Gipps Street, Elm Grove, and Charlotte Street. The times and extent of the survey is considered appropriate. A parking inventory of 153 spaces was identified. The survey results indicate that the peak parking occupancy during the AM survey was observed at 10.00am with 127 spaces occupied or 26 spaces vacant. During the PM survey, the peak parking occupancy occurred at 3.30pm with 16 spaces occupied or 17 vacant spaces. The data suggests that the shortfall of four spaces for the development could be accommodated in the surrounding streets.
- *Relevant Local Policy or Incorporated Document.*
The proposed development is considered to be in line with the objectives contained in Council's *Strategic Transport Statement*. The site is ideally located with regard to sustainable transport alternatives and the reduced provision of on-site car parking would potentially discourage staff and visitors to commute to the site by motor vehicle.

Adequacy of Car Parking

From a traffic engineering perspective, the waiver of four spaces is considered appropriate in the context of the development and the surrounding area. Any shortfall with the developments short-term parking could be accommodated in the surrounding streets.

Engineering Services has no objection to the waiver of car parking.

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TRAFFIC GENERATION

The traffic generation for the site adopted by Ratio Consultants is as follows:

Proposed Use	Adopted Traffic Generation Rate	Daily Traffic	Peak Period	
			AM	PM
371 Church Street (Child Car Centre Car Park)	AM Peak is 0.43 trips per child PM Peak is 0.40 trips per child	Not Provided	62	62
326 Church Street (St Ignatius Church Car Park)	AM Peak is 1.0 trip per space PM Peak is 1.0 trip per space	Not Provided	8	8
Total		-	70	70

The development would have 70 vehicle trips per peak hour (an average of one vehicle trip every 51 seconds).

The volume of traffic generated by the development during the peak AM and PM periods are not unduly high and could be accommodated on the local road network without adversely impacting on its operation.

There is no objection to the provision of a left-in/left-out access arrangement.

DEVELOPMENT LAYOUT DESIGN

Layout Design Assessment

Item	Assessment
Access Arrangements	
Development Entrance	The proposed 3.1 metre wide entrance satisfies the Australian/New Zealand Standard AS/NZS 2890.1:2004.
Vehicle Crossing	Open Space have provided comments regarding the removal of the street tree to accommodate the proposed vehicle crossing. An arborist report is required for the removal of the street tree prior to the construction of the vehicle crossing.
Visibility	A visibility triangle has been provided on the north side of the entrance. The applicant has proposed a convex mirror on the north side of the entrance to improve visibility of pedestrians along the footpath.
Car Parking Modules	
Regular Car Spaces	The regular spaces measure 2.6 metres by 4.9 metres to satisfy AS/NZS 2890.1:2004.
Accessible Car Space	The accessible car space and associated shared area comply with AS/NZS 2890.6:2009.

Attachment 4 - PLN17/0635 - 371 & 326 - 348 Church Street Richmond - Engineering comments

Aisle Width	A minimum aisle width of 6.5 metres has been provided to also satisfy AS/NZS 2890.1:2004.
Clearance to Walls	A minimum clearance of 300 mm to be provided for car spaces adjacent to walls. To be dimensioned on the plans.

Layout Design Assessment

Item	Assessment
Access Arrangements	
Column Depths and Setbacks	Not dimensioned on the plans.
Floor to Ceiling Clearance Height	A minimum floor to ceiling height of 2.89 metres has been provided for the undercroft parking spaces to satisfy <i>Design standard 1 – Accessways</i> .
Swept Path Diagram – Car Spaces	The swept path diagrams for the B85 design vehicle entering and exiting the car spaces are considered satisfactory.

Design Items to be Addressed

Item	Details
Vehicle Crossing	The applicant must liaise with VicRoads regarding the proposed crossing in Church Street. The crossing must satisfy Council's engineering requirements and VicRoads' geometric requirements.
Visibility	The visibility triangle is to be dimensioned on the drawings. Any plants/shrubs along the north side of the entrance must be kept at a reasonable height to maintain sight visibility.
Clearance to Walls	To be dimensioned on the drawings.
Column Depths and Setbacks	To be dimensioned on the plans.

IMPACT ON COUNCIL ROAD ASSETS

The construction of the new buildings, the provision of underground utilities and construction traffic servicing and transporting materials to the site will impact on Council assets. Trenching and areas of excavation for underground services invariably deteriorates the condition and integrity of footpaths, kerb and channel, laneways and road pavements of the adjacent roads to the site.

It is essential that the developer rehabilitates/restores laneways, footpaths, kerbing and other road related items, as recommended by Council, to ensure that the Council infrastructure surrounding the site has a high level of serviceability for residents, employees, visitors and other users of the site.

ENGINEERING CONDITIONS

Vehicle Crossing

Attachment 4 - PLN17/0635 - 371 & 326 - 348 Church Street Richmond - Engineering comments

- The vehicle crossing on the east side of Church Street must be constructed to Council's current standards and VicRoads' geometric requirements, and at the Permit Holder's cost.
- The applicant must undertake a ground clearance check of the proposed vehicle crossing using the B99 design vehicle. The ground clearance check must be submitted to Council's Construction Management unit for assessment and approval.

Road Asset Protection

- Any damaged roads, footpaths and other road related infrastructure adjacent to the development site as a result of the construction works, including trenching and excavation for utility service connections, must be reconstructed to Council's satisfaction and at the developer's expense.

Construction Management Plan

- A Construction Management Plan must be prepared and submitted to Council. The Plan must be approved by Council prior to the commencement of works. A detailed dilapidation report should detail and document the existing and post construction conditions of surrounding road infrastructure and adjoining private properties.

Impact of Assets on Proposed Development

- Any services poles, structures or pits that interfere with the proposal must be adjusted, removed or relocated at the owner's expense after seeking approval from the relevant authority.
- Areas must be provided inside the property line and adjacent to the footpath to accommodate pits and meters. No private pits, valves or meters on Council property will be accepted.

NON-PLANNING ADVICE FOR THE APPLICANT

Legal Point of Discharge

The applicant must apply for a Legal Point of Discharge under Regulation 610 – Stormwater Drainage of the *Building Regulations 2006* from Yarra Building Services unit. Any storm water drainage within the property must be provided and be connected to the nearest Council pit of adequate depth and capacity (legal point of discharge), or to Council's satisfaction under Section 200 of the *Local Government Act 1989* and Regulation 610.

Clearances from Electrical Assets

Overhead power lines run along the east side of Church Street, close to the property line.

The developer needs to ensure that the building has adequate clearances from overhead power cables, transformers, substations or and other electrical assets where applicable. Energy Safe Victoria has published an information brochure, *Building Design Near Powerlines*, which can be obtained from their website:

<http://www.esv.vic.gov.au/About-ESV/Reports-and-publications/Brochures-stickers-and-DVDs>

Regards

Artemis Bacani
Roads Development Engineer
Engineering Services Unit

Attachment 5 - PLN17/0635 - 317 & 326 - 348 Church Street Richmond - VicRoads Comments



25 June 2018

Yarra City Council
PO Box 168
RICHMOND VIC 3121
Attention: Nish Goonetilleke

Dear Ms Goonetilleke,

PLANNING APPLICATION No.: PLN17/0635
VICROADS REFERENCE NO: 23829/17
PROPERTY ADDRESS: 371 CHURCH STREET, RICHMOND

Section 55 – No objection subject to conditions

Further to our response dated 29 March 2018 in relation to the above application VicRoads have received amended plans (refer copy). Subsequently VicRoads wish to withdraw their objection to this proposal.

If Council regards the proposed development favourably, VicRoads would require that the following conditions be included in any Notice of Decision to issue a Planning Permit or Planning Permit:

1. *Before the use approved by this permit commences the following roadworks on Church Street, Richmond must be completed at no cost to and to the satisfaction of the Responsible Authority and or the Roads Corporation:*
 - a. *The installation of signs, line marking, a left painted arrow out and associated road works permitting only "left turn in" and "left turn out" vehicular access from Church Street.*
 - b. *Modification or removal of any existing car parking spaces and associated road works on the east side of Church Street to accommodate the new vehicular access arrangement.*

Note: Separate consent may be required from VicRoads (the Roads Corporation) under the Road Management Act 2004 for buildings and works (i.e. canopies and architectural features/ projections) undertaken outside the title boundary within a Road Zone Category 1 (i.e. Church Street). Please contact VicRoads prior to commencing any works.

Notwithstanding the above, the VicRoads would encourage Council to consider the potential for the remaining single on street car space located between the vehicle crossover and the tram stop to be converted to a short stay car space during the operating hours of the child care centre to compliment the drop off/pick up arrangement on the site.

Once Council makes its decision, please forward a copy of the decision to VicRoads as required under Section 66 of the Planning and Environment Act 1987.

Attachment 5 - PLN17/0635 - 317 & 326 - 348 Church Street Richmond - VicRoads Comments

Should you have any enquiries regarding this matter, please contact me on 03 9313 1148 or
Gillian.Menegas@roads.vic.gov.au

Yours sincerely

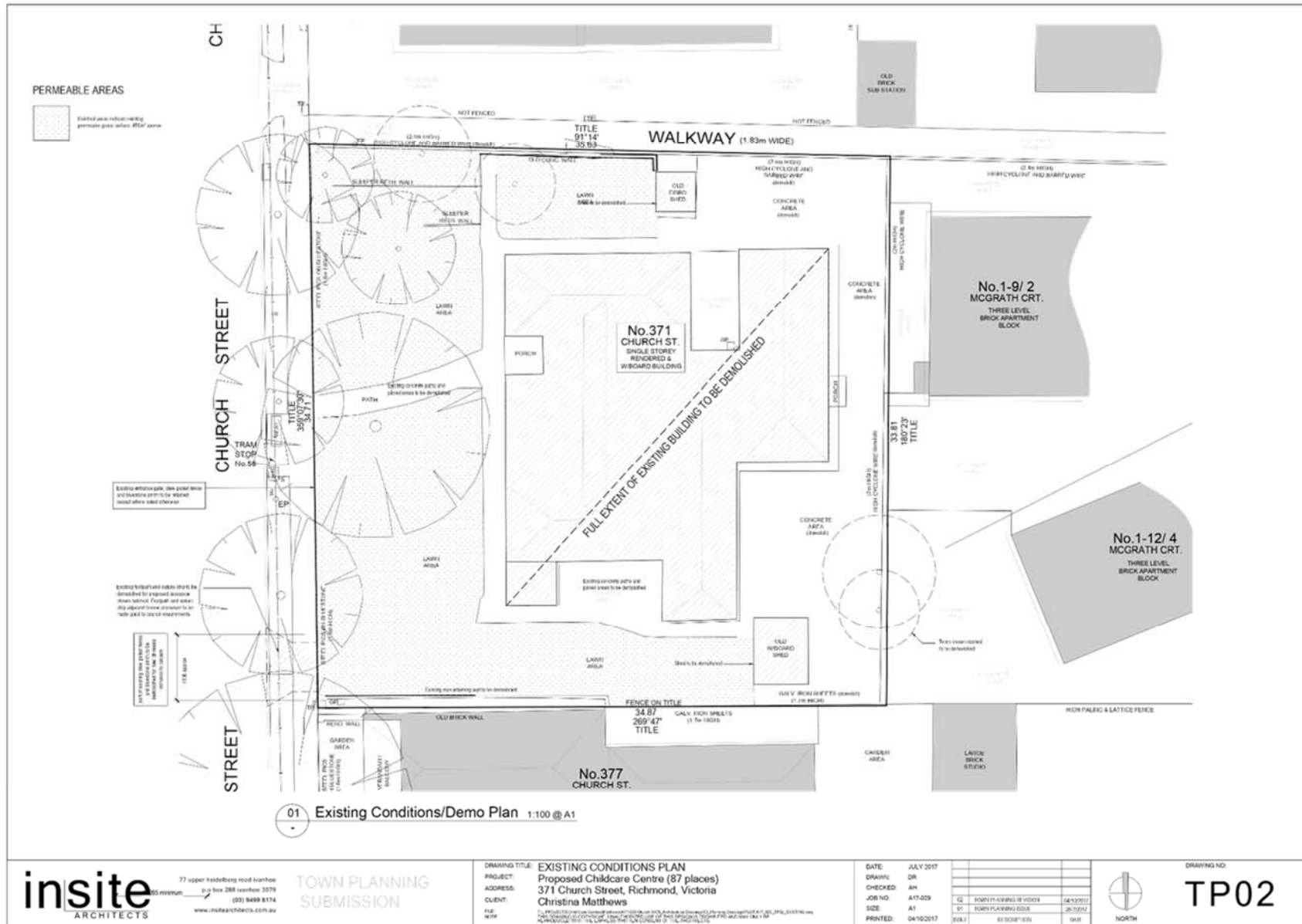
GILLIAN MENEGAS

Principal Planner- Statutory Planning Department (Central)

Attachment 6 - PLN17/0635 - 371 and 326 - 348 Church Street Richmond - Decision Plans



Attachment 6 - PLN17/0635 - 371 and 326 - 348 Church Street Richmond - Decision Plans



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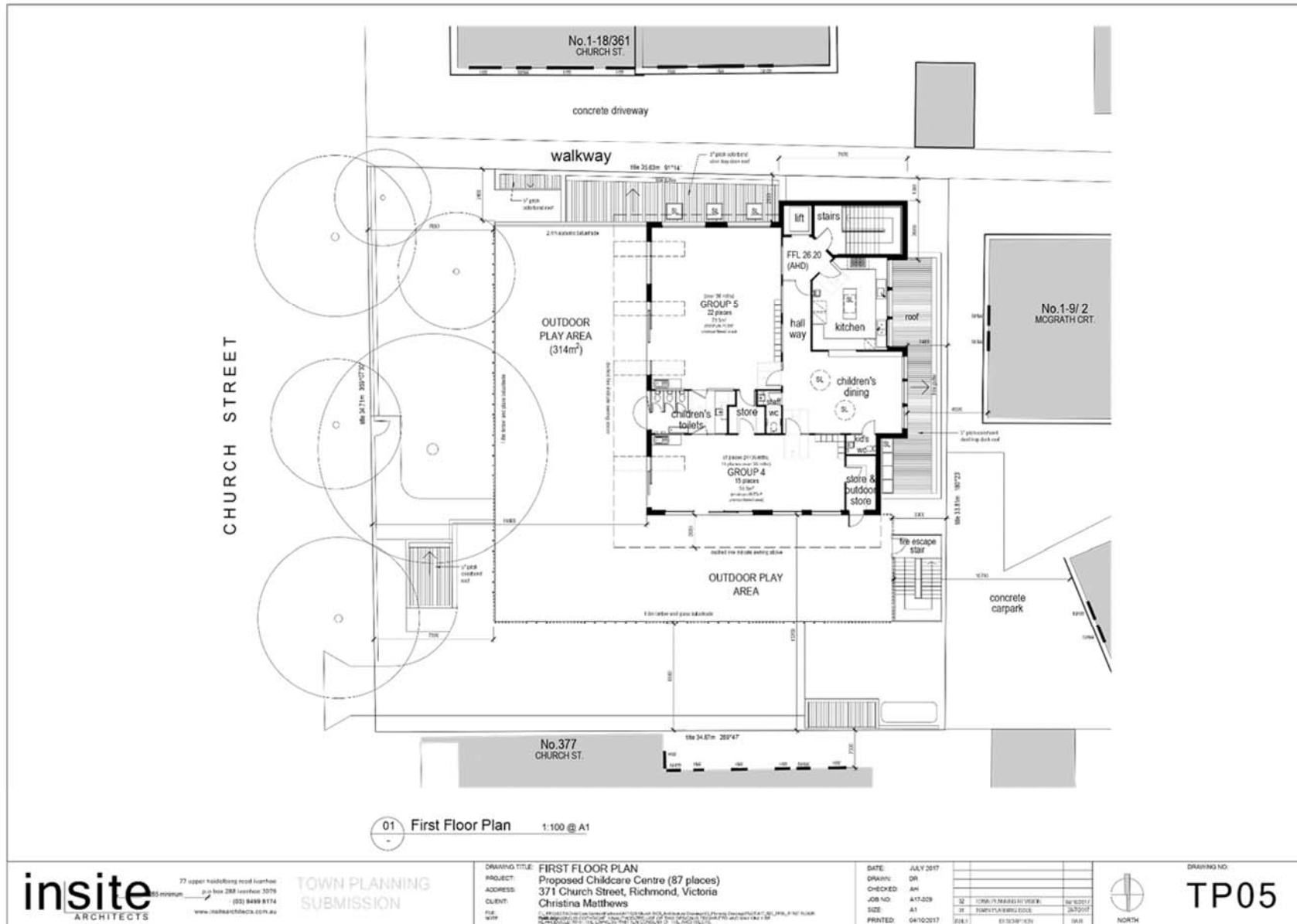
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PROJECT: Proposed Childcare Centre (87 places)
ADDRESS: 371 Church Street, Richmond, Victoria
CLIENT: Christina Matthews

DATE: JULY 2017
DRAWN: DR
CHECKED: AH
JOB NO: A17-009
SITE: A1
PRINTED: 04/10/2017

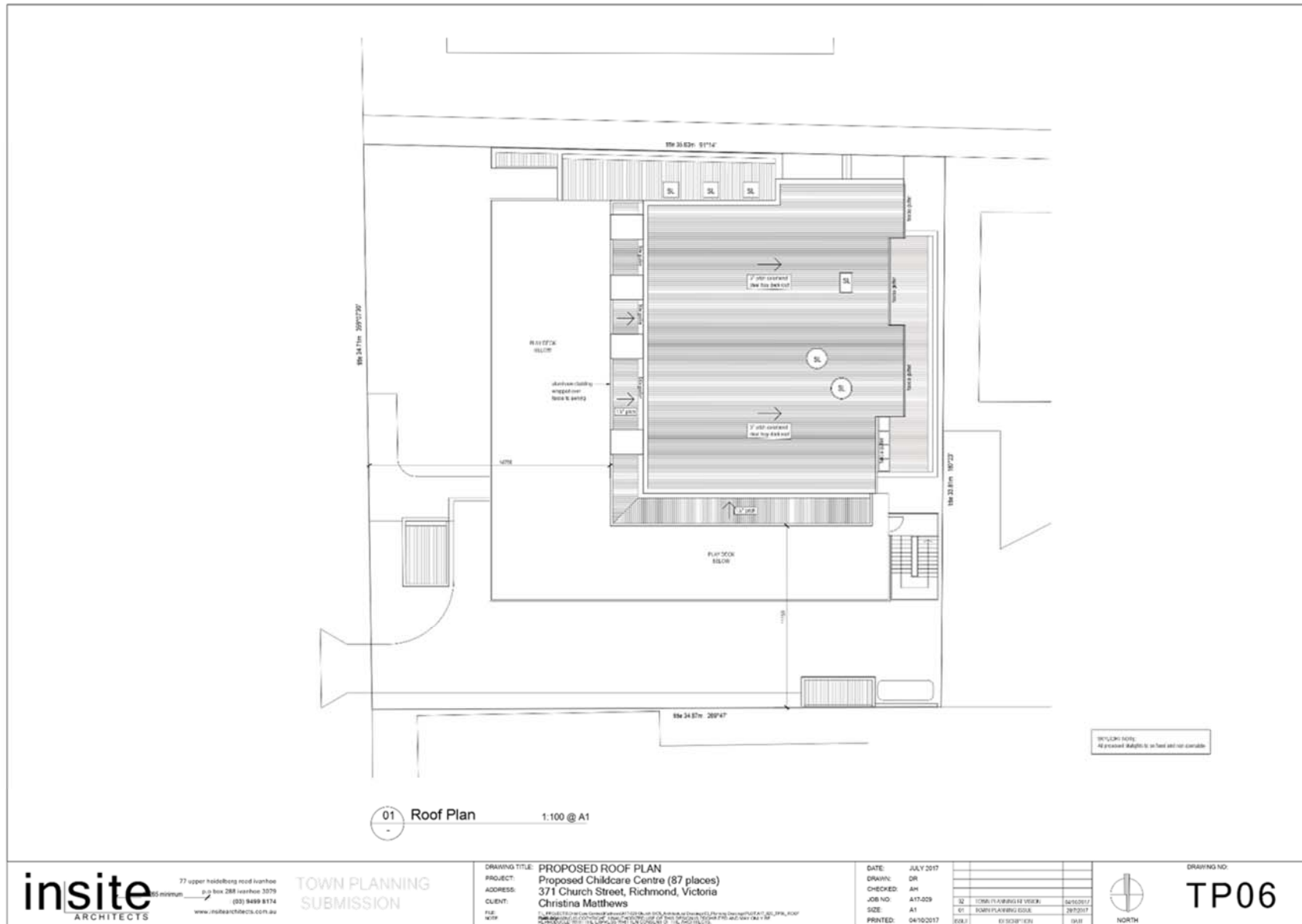


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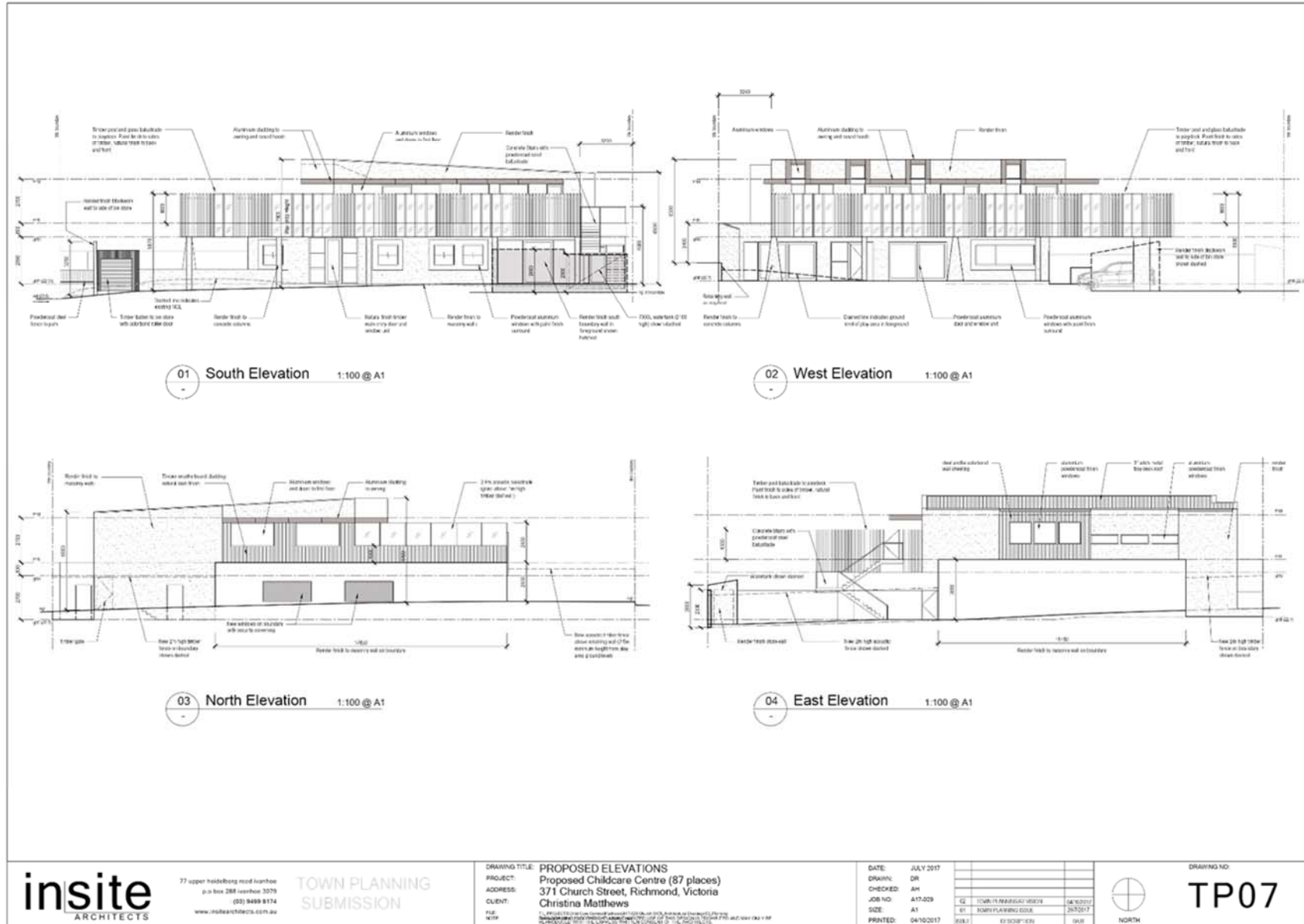
Attachment 6 - PLN17/0635 - 371 and 326 - 348 Church Street Richmond - Decision Plans



Attachment 6 - PLN17/0635 - 371 and 326 - 348 Church Street Richmond - Decision Plans



Attachment 6 - PLN17/0635 - 371 and 326 - 348 Church Street Richmond - Decision Plans



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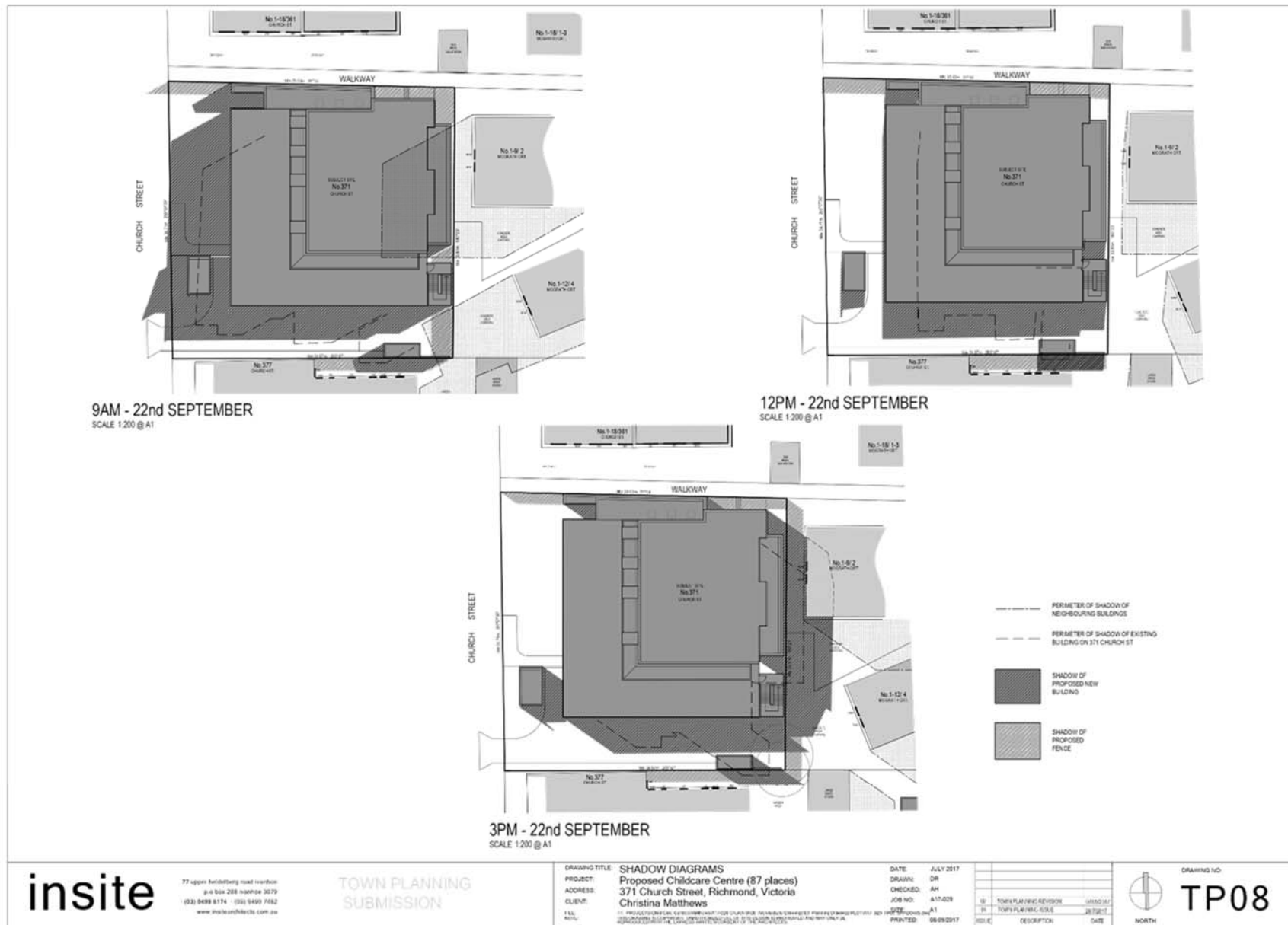
DRAWING TITLE: PROPOSED ELEVATIONS
PROJECT: Proposed Childcare Centre (87 places)
ADDRESS: 371 Church Street, Richmond, Victoria
CLIENT: Christina Matthews
FILE NAME: C:\Projects\170635\170635.dwg

DATE: JULY 2017
DRAWN: DR
CHECKED: AH
JOB NO: A17-009
SCALE: A1
PRINTED: 04/10/2017



DRAWING NO:
TP07

Attachment 6 - PLN17/0635 - 371 and 326 - 348 Church Street Richmond - Decision Plans



Attachment 6 - PLN17/0635 - 371 and 326 - 348 Church Street Richmond - Decision Plans

The image displays four architectural elevations of a proposed childcare centre: West, North, South, and East. A legend on the right side of the page lists the materials, finishes, and colors used in the design. The elevations show a modern building with a mix of materials including render, timber, and aluminium. The legend provides specific details for each material and finish.

Proposed West Elevation

Proposed North Elevation

Proposed South Elevation

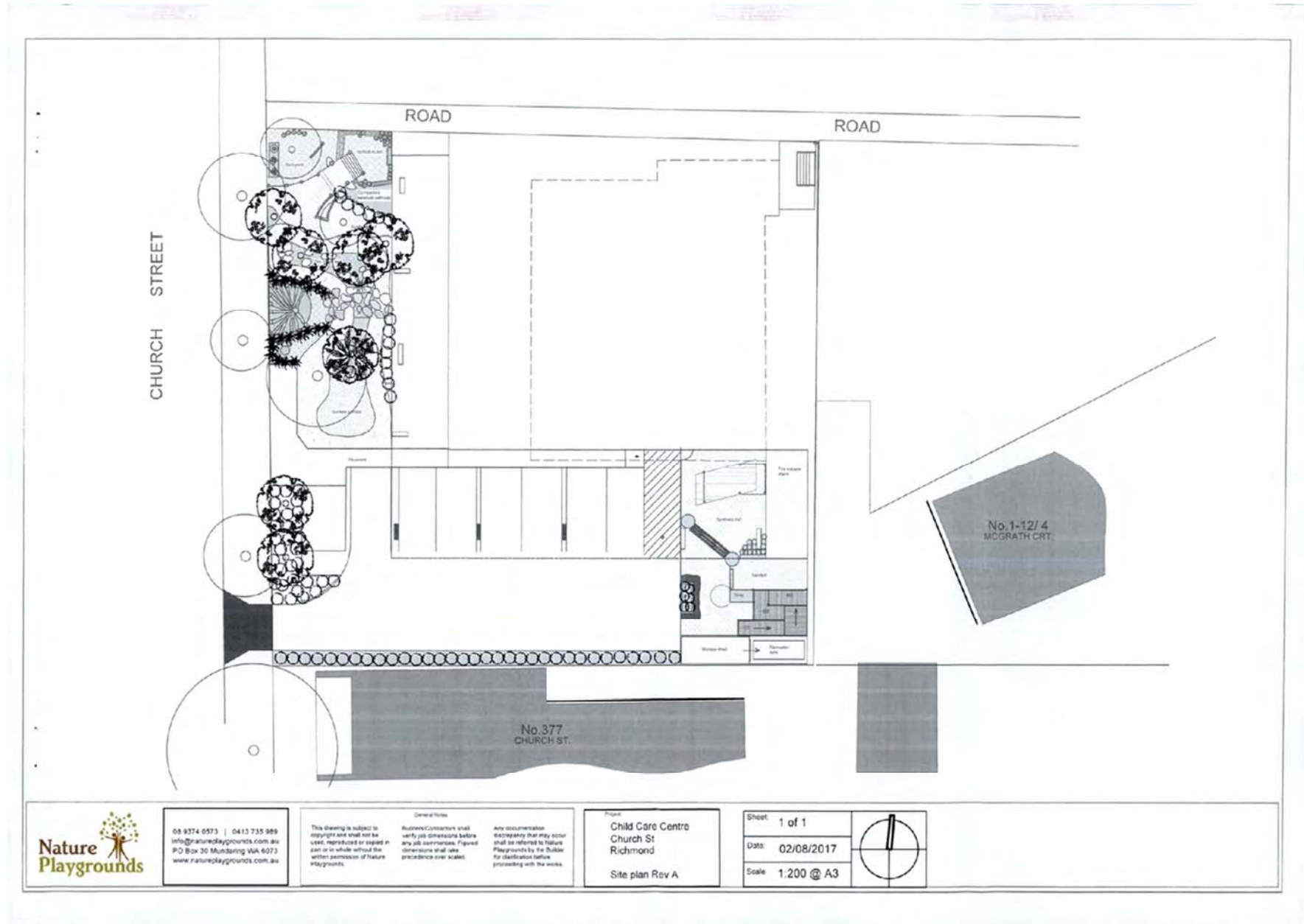
Proposed East Elevation

- RENDER FINISH - DULUX 'SILVER CITY'
- TIMBER BALUSTRADE POSTS - NATURAL TO BACK AND FRONT, PAINT FINISH TO SIDE FACES
- TIMBER BATTEN WALL CLADDING - NATURAL FINISH
- ALUMINIUM CLADDING - 'COPPER'
- POWDERCOAT ALUMINIUM DOORS AND WINDOWS - DULUX 'MONUMENT'
- FEATURE WINDOW REVEALS - DULUX 'CORAL BLOSSOM' & 'MISTY MOSS'
- COLORBOND ROOFING, GUTTERS AND CAPPING - 'MONUMENT'

Proposed Childcare Centre Exterior Materials, Finishes & Colours
371 CHURCH ST, RICHMOND VICTORIA

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Attachment 6 - PLN17/0635 - 371 and 326 - 348 Church Street Richmond - Decision Plans



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 www.natureplaygrounds.com.au

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General Notes

Business/Contractors shall verify all dimensions before any job commences. Figure dimensions shall take precedence over scales.

Any documentation excessively that may occur shall be referred to Nature Playgrounds by the Builder for clarification before proceeding with the works.

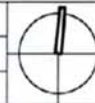
Project

Child Care Centre
 Church St
 Richmond
 Site plan Rev A

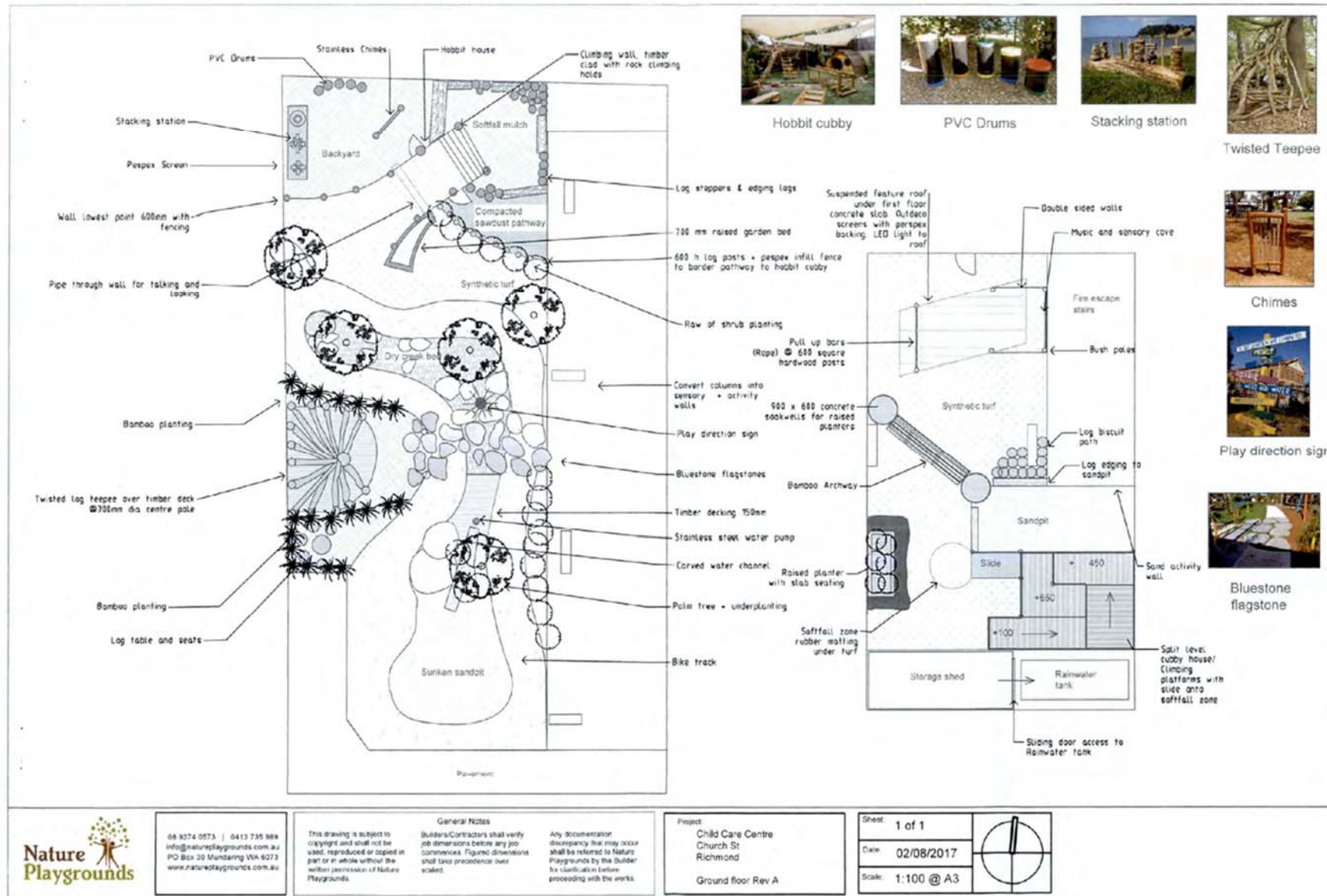
Sheet 1 of 1

Date: 02/08/2017

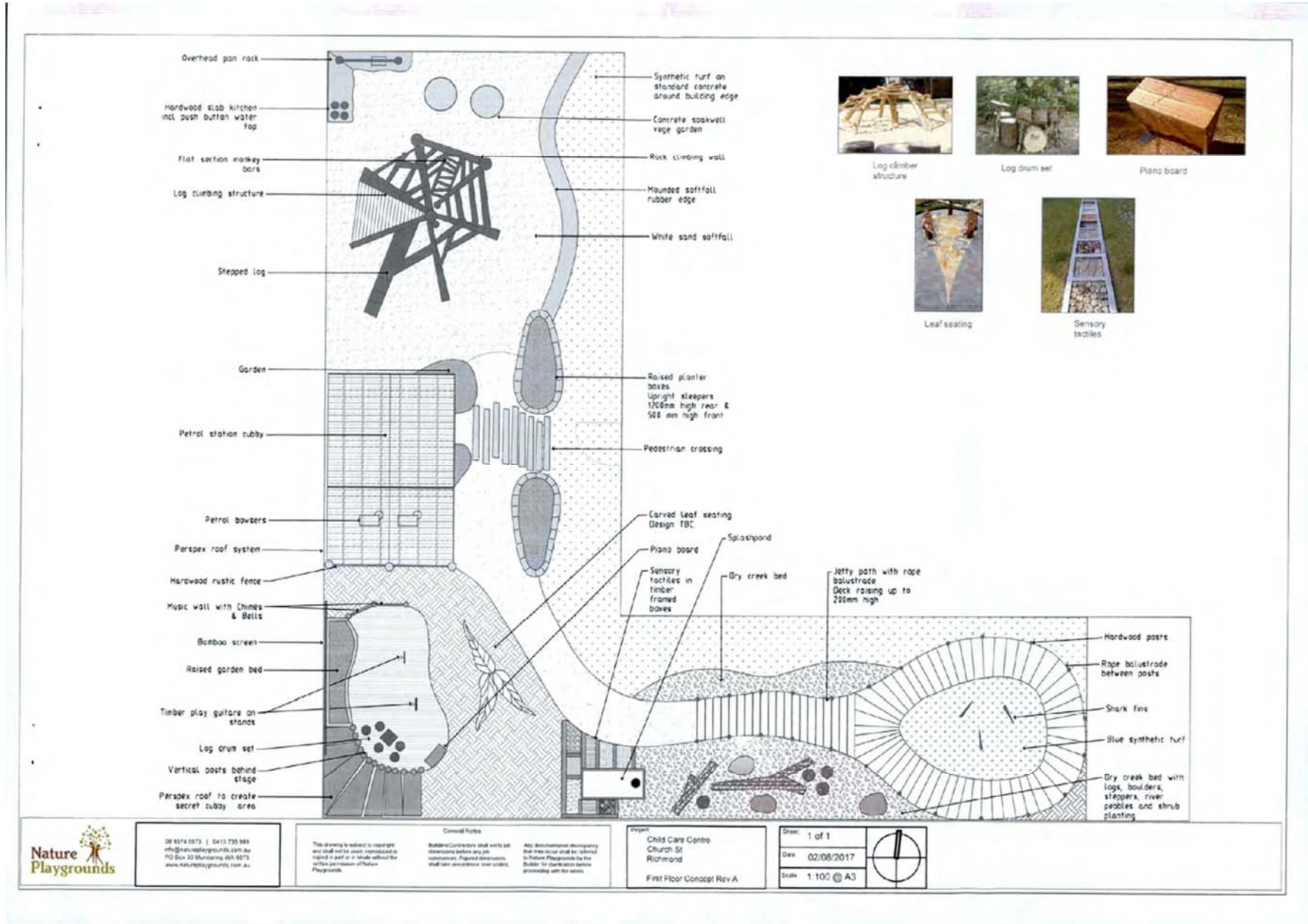
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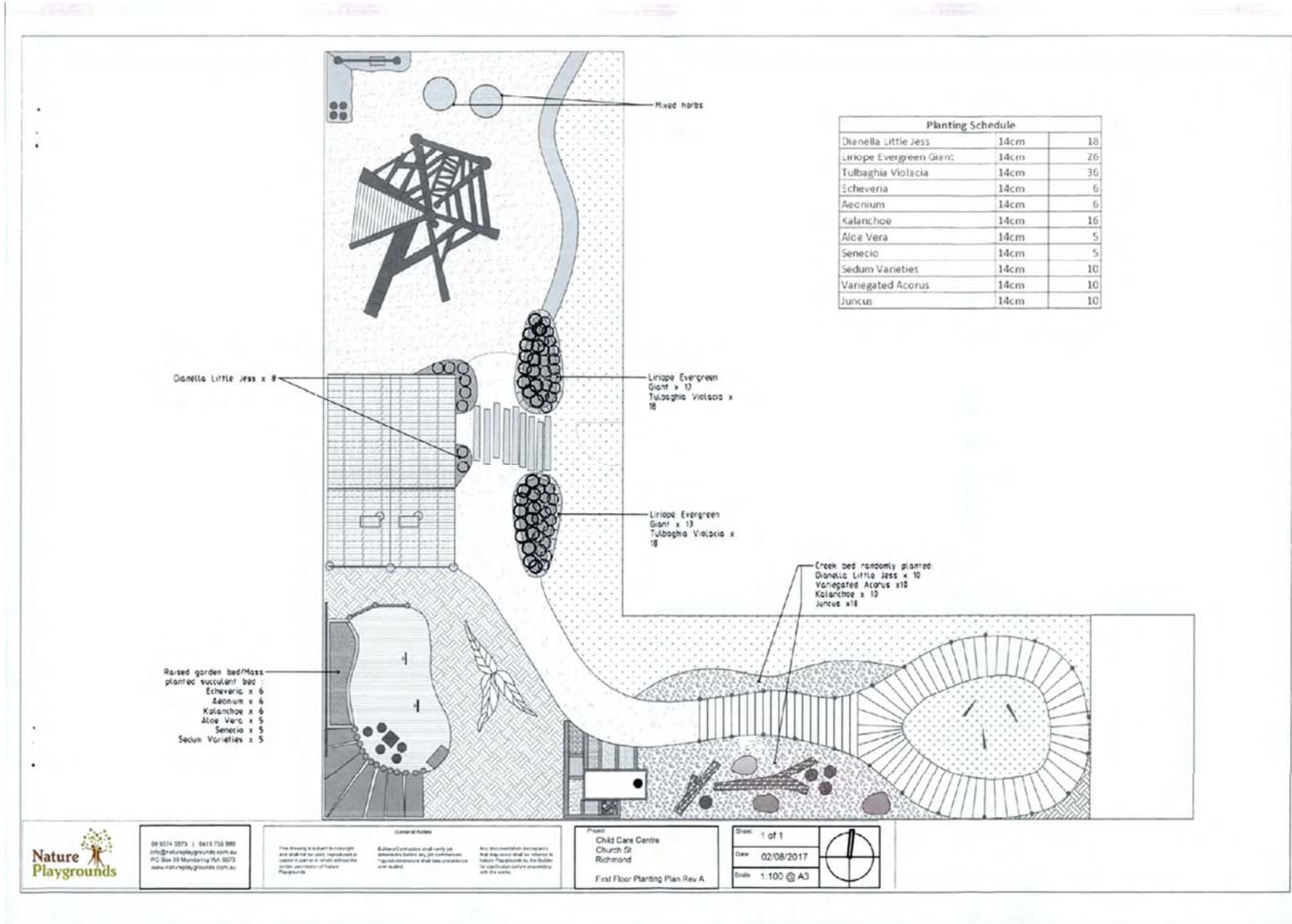
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Attachment 6 - PLN17/0635 - 371 and 326 - 348 Church Street Richmond - Decision Plans



Attachment 6 - PLN17/0635 - 371 and 326 - 348 Church Street Richmond - Decision Plans



Attachment 7 - PLN17/0635 - 317 & 326 - 348 Church Street Richmond - Sketch Plan 05.06.2018

