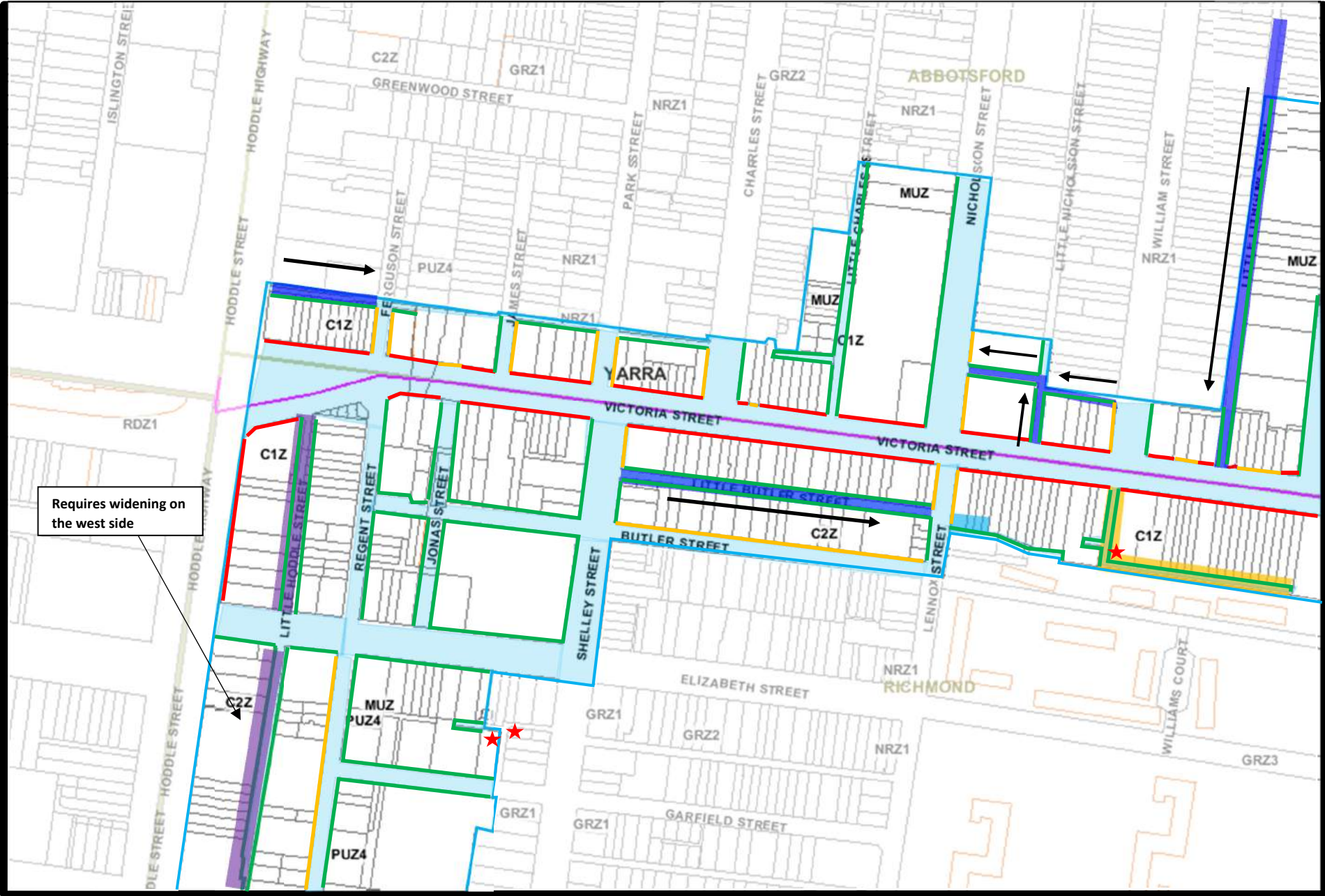


Appendix E: Access Management Plans

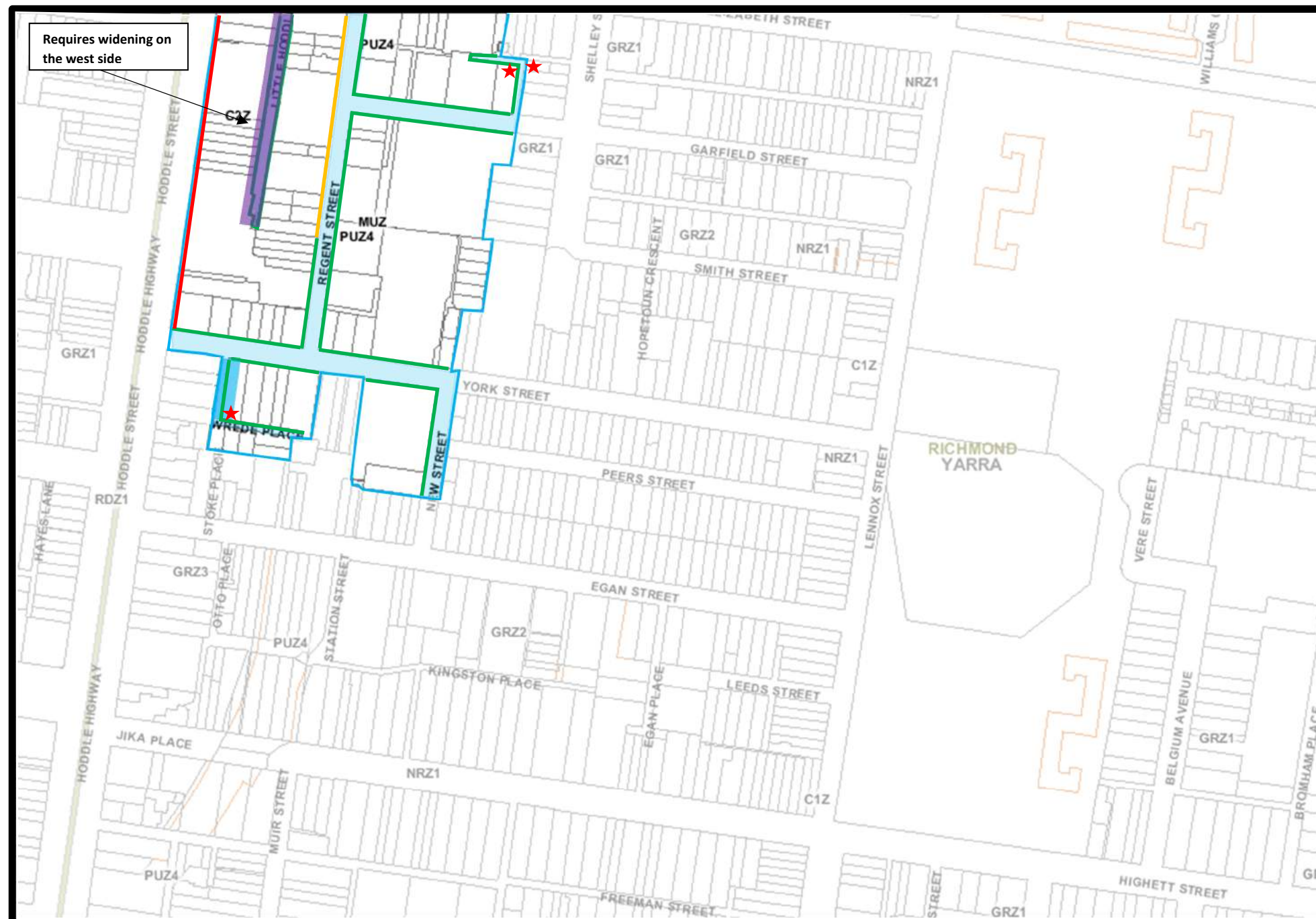


Legend – Recommended Access Controls

- Study Area Boundary
- Access Prohibited
- Access Not Preferred
- Access Preferred

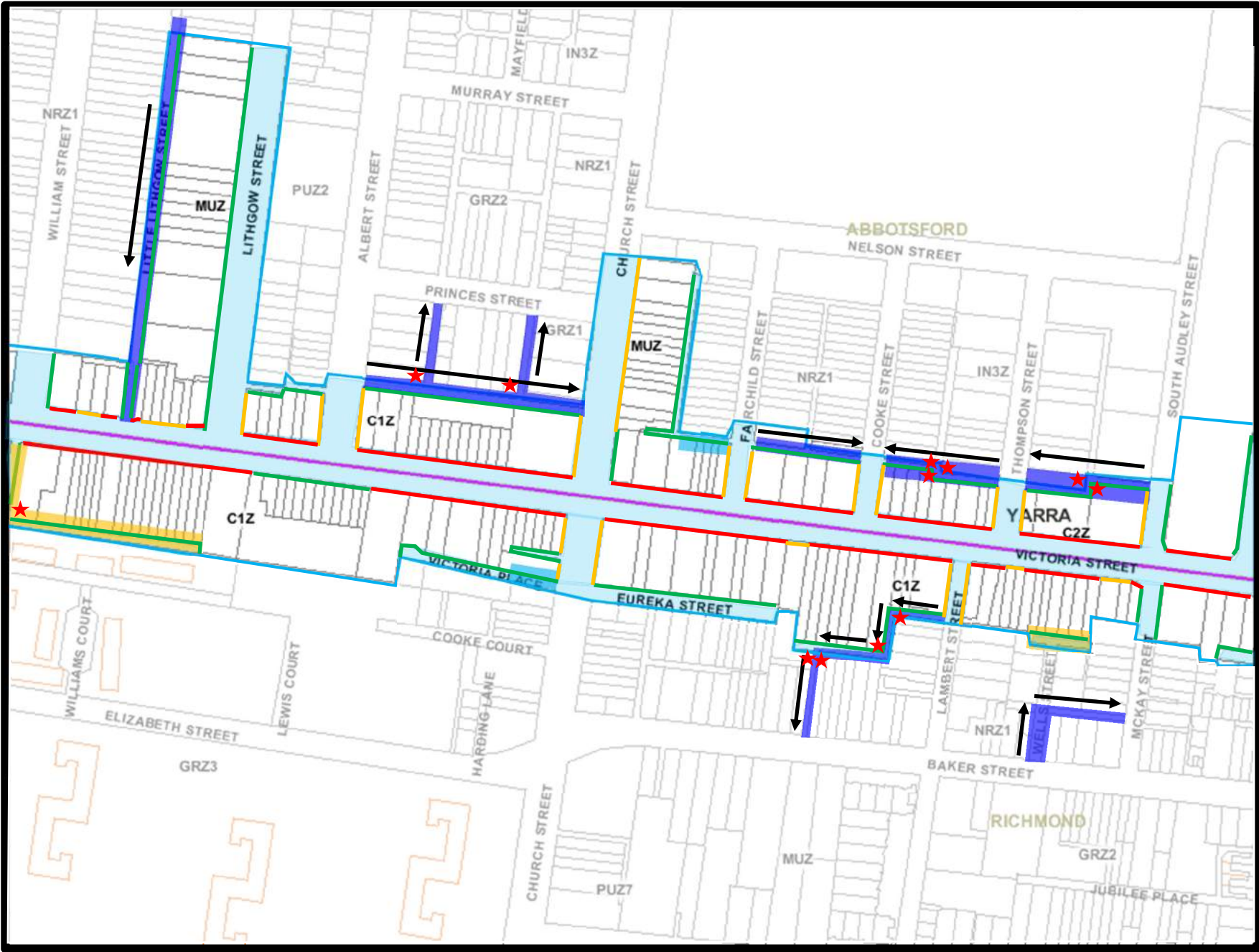
Legend – Recommended Changes

- One-way (with indicative arrow)
- 6m wide shared zone
- 6m wide road
- Passing area
- Splay



Legend – Recommended Access Controls	
—	Study Area Boundary
—	Access Prohibited
—	Access Not Preferred
—	Access Preferred

Legend – Recommended Changes	
—	One-way (with indicative arrow)
—	6m wide shared zone
—	6m wide road
—	Passing area
★	Splay

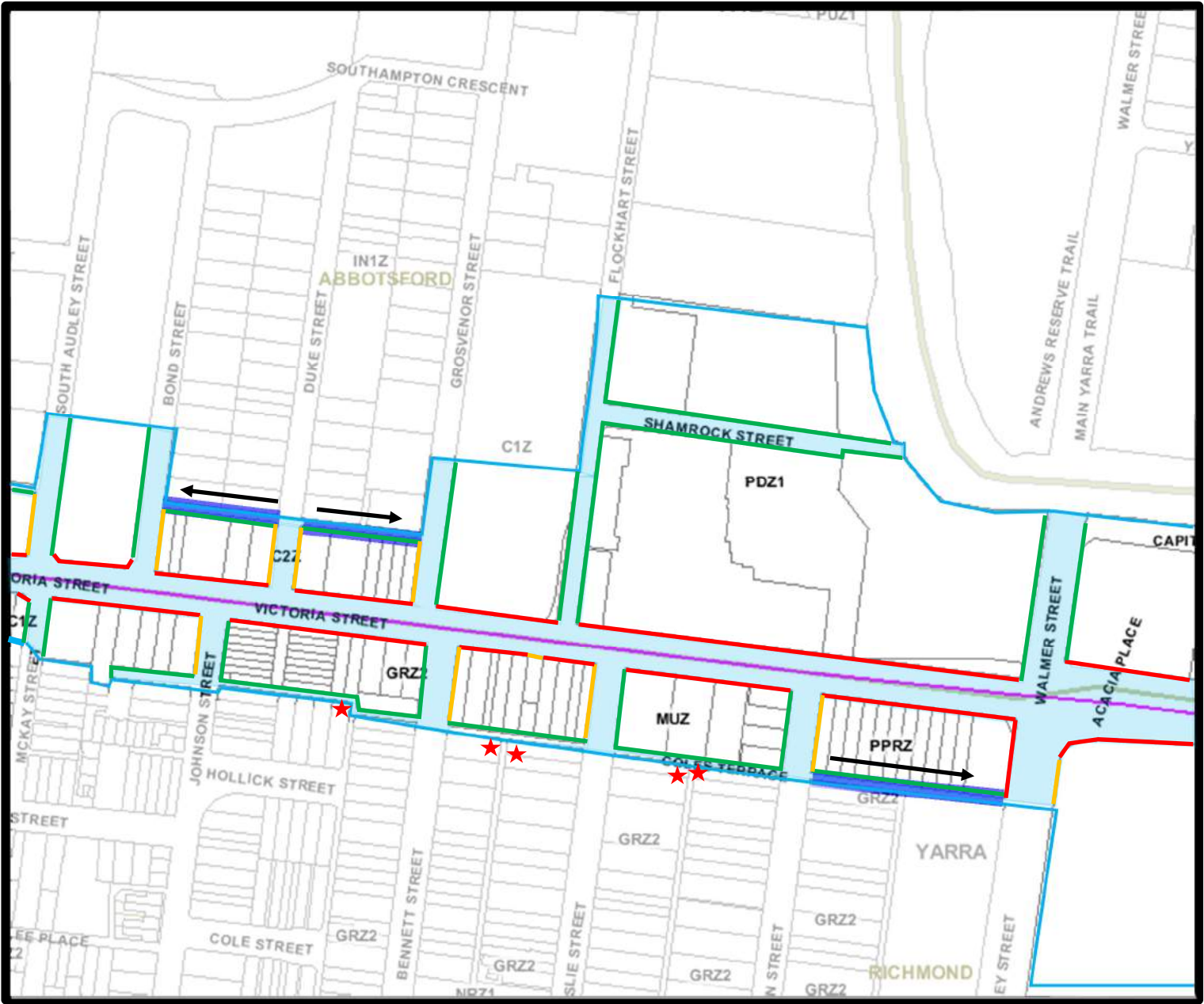


Legend – Recommended Access Controls

- Study Area Boundary
- Access Prohibited
- Access Not Preferred
- Access Preferred

Legend – Recommended Changes

- One-way (with indicative arrow)
- 6m wide shared zone
- 6m wide road
- Passing area
- Splay

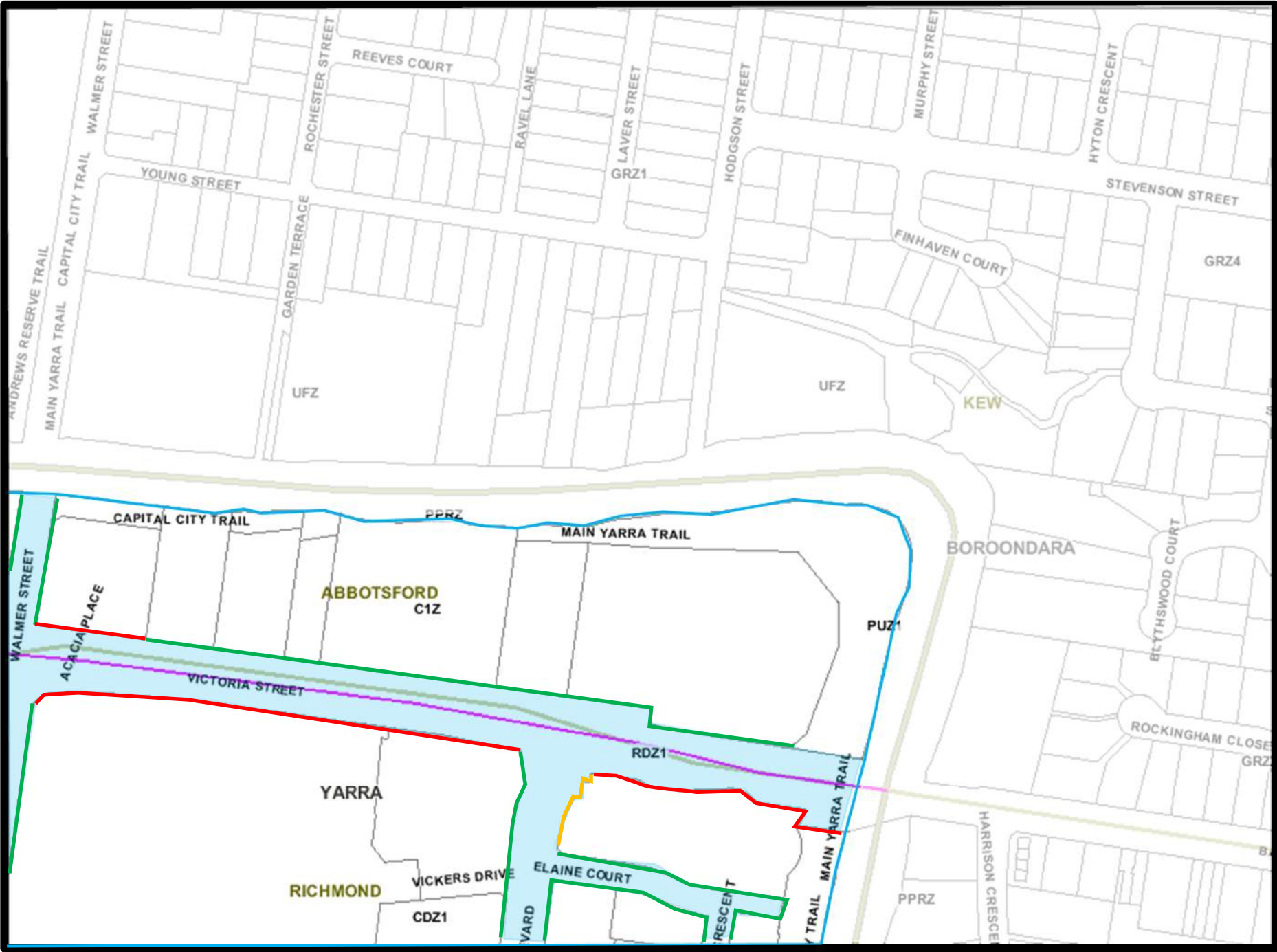


Legend – Recommended Access Controls

- Study Area Boundary
- Access Prohibited
- Access Not Preferred
- Access Preferred

Legend – Recommended Changes

- One-way (with indicative arrow)
- 6m wide shared zone
- 6m wide road
- Passing area
- Splay

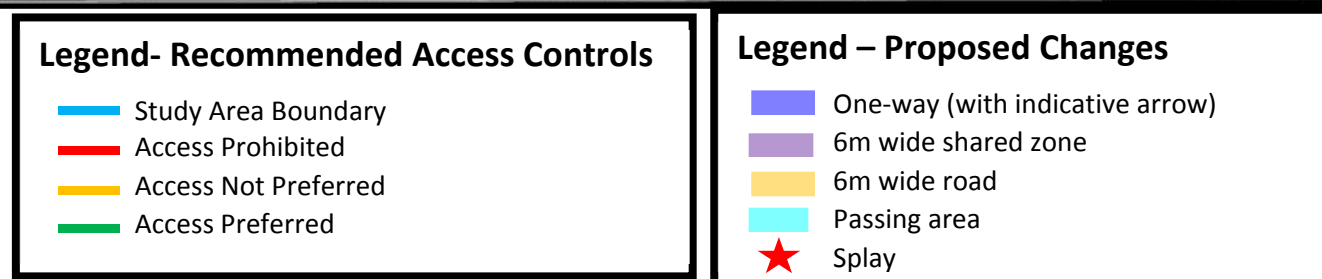


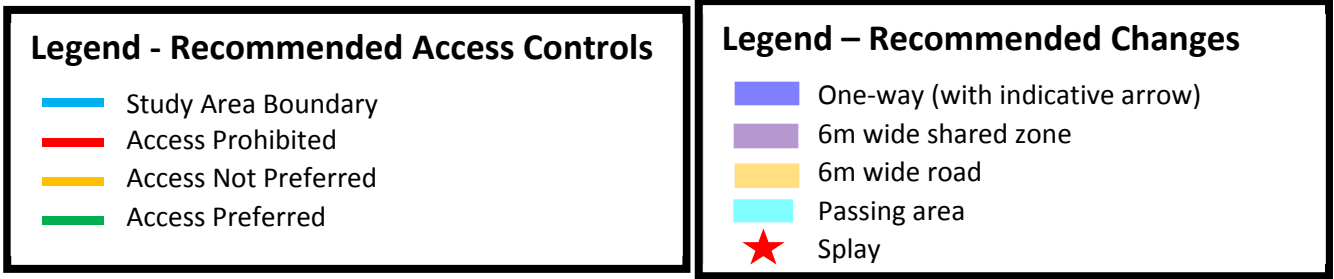
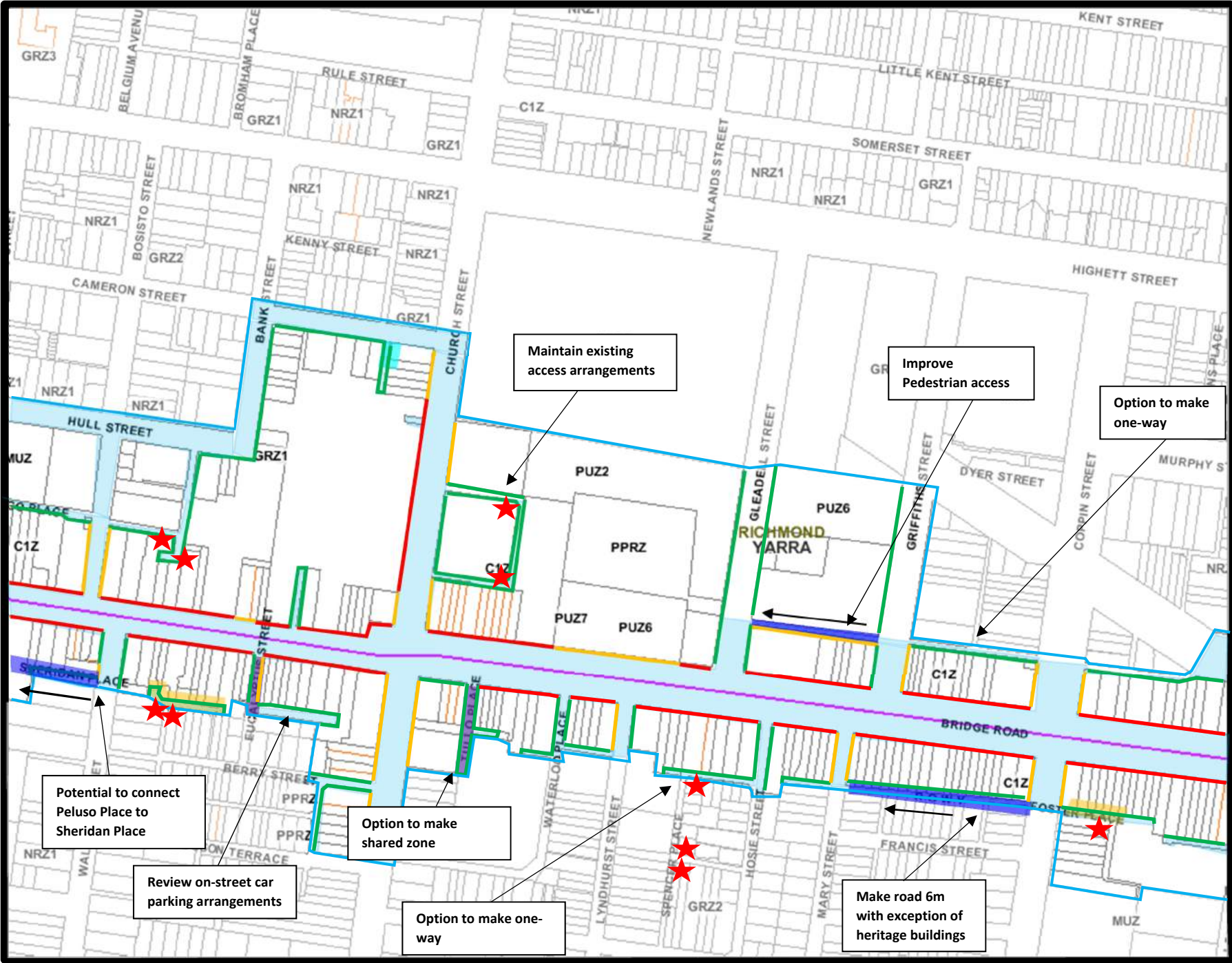
Legend – Recommended Access Controls

- Study Area Boundary
- Access Prohibited
- Access Not Preferred
- Access Preferred

Legend – Recommended Changes




- One-way (with indicative arrow)
- 6m wide shared zone
- 6m wide road
- Passing area
- Splay












Legend – Recommended Access Controls

-  Study Area Boundary
- Access Prohibited
-  Access Not Preferred
-  Access Preferred

Legend – Recommended Changes

-  One-way (with indicative arrow)
-  6m wide shared zone
-  6m wide road
-  Passing area
-  Splay

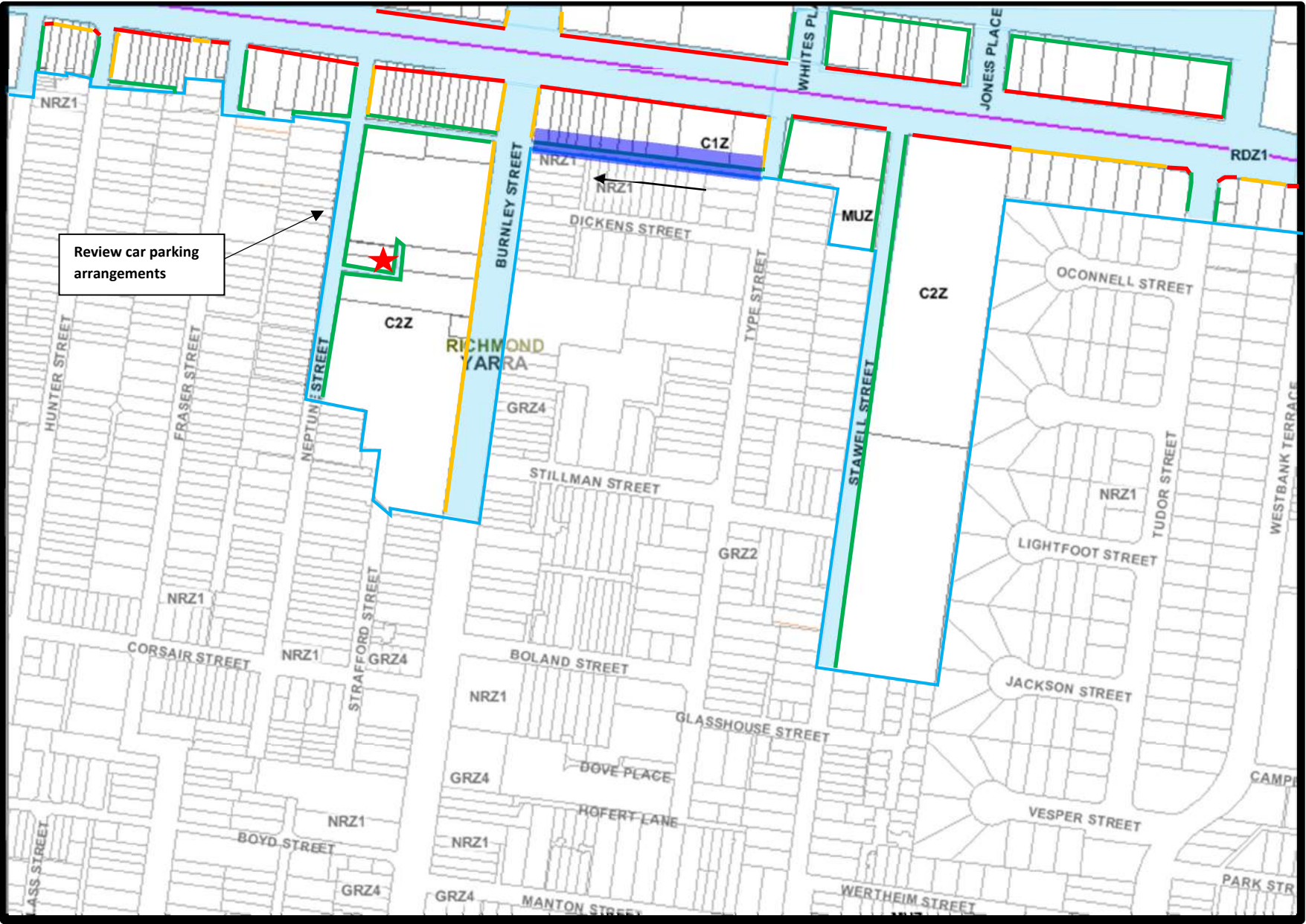


Legend – Recommended Access Controls

- Study Area Boundary
- Access Prohibited
- Access Not Preferred
- Access Preferred

Legend – Recommended Changes

- One-way (with indicative arrow)
- 6m wide shared zone
- 6m wide road
- Passing area
- Splay



Legend – Recommended Access Controls

- Study Area Boundary
- Access Prohibited
- Access Not Preferred
- Access Preferred

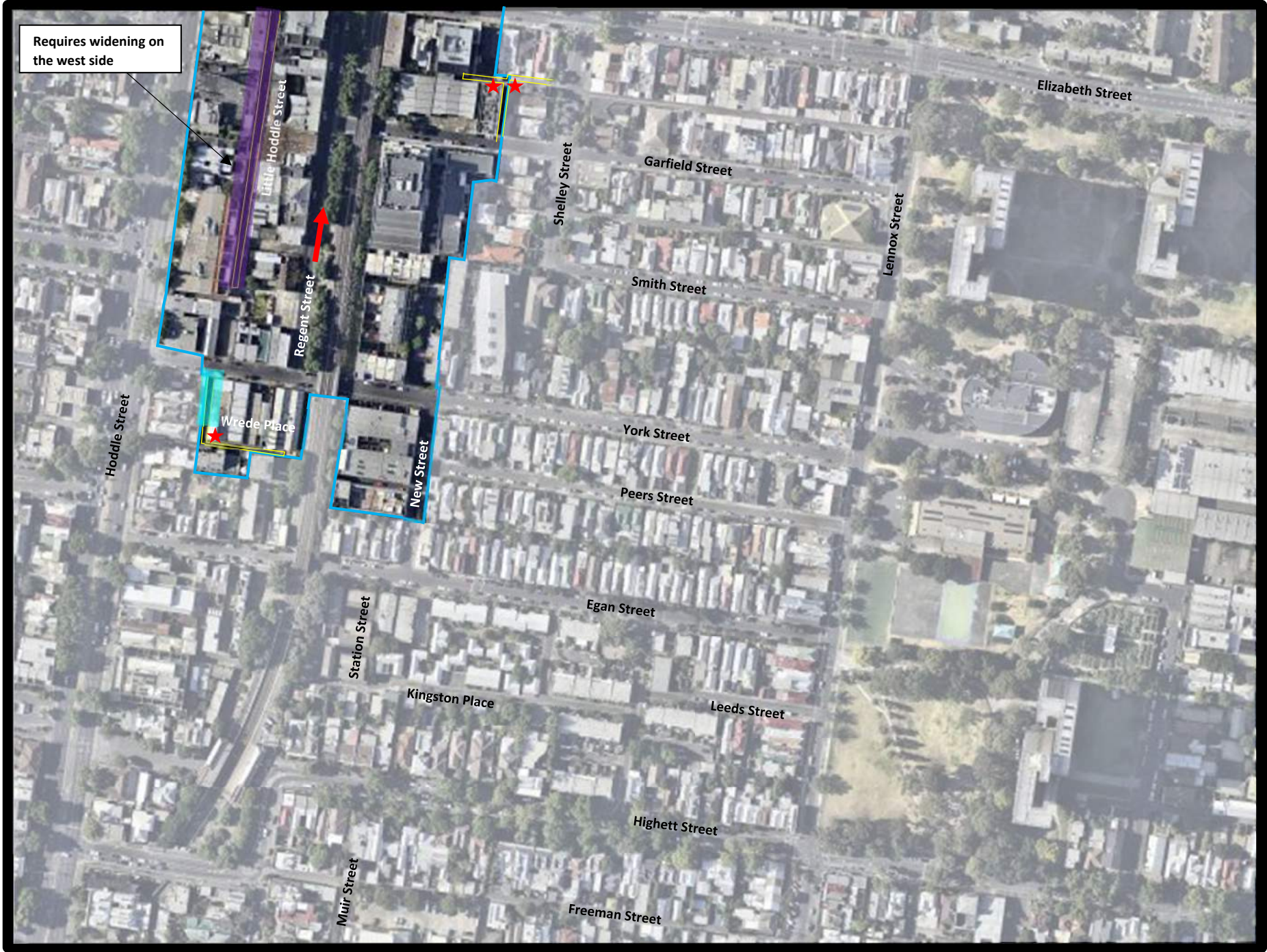
Legend

- One-way (with indicative arrow)
- 6m wide shared zone
- 6m wide road
- Passing area
- Splay

Appendix F: Proposed Traffic Management Plans



Legend – Recommended Changes		Legend – Existing Conditions	
	One-way (with indicative arrow)		Study Area Boundary
	6m wide shared zone		Traffic Signals
	6m wide road		Pedestrian Signals
	Passing area		Threshold Treatment
	Splay		No Entry (Exit Only)
			Right Turn Ban
			Left-turn Only
			One-way
			No Through Road Blockade

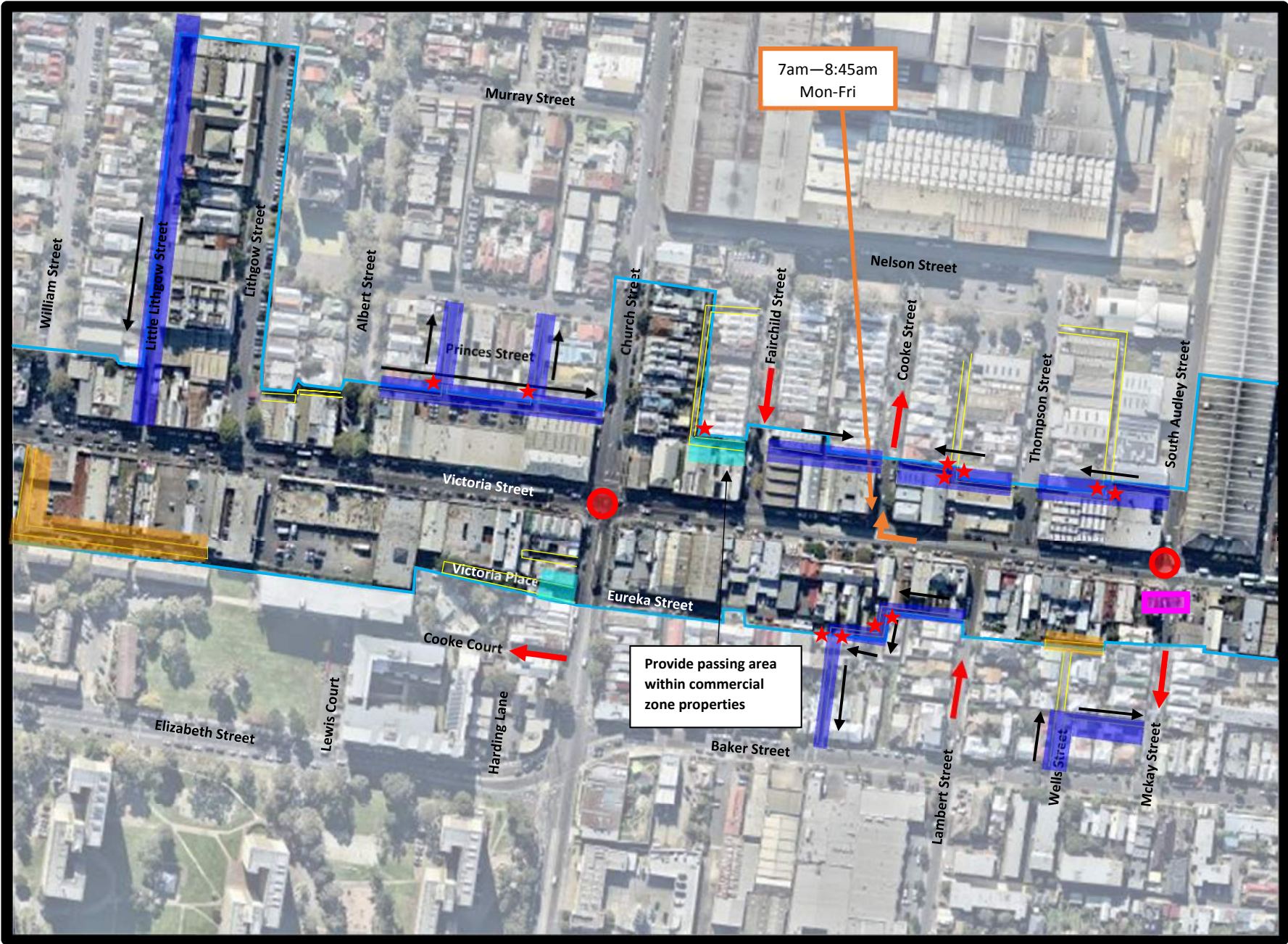


Legend – Recommended Changes

- One-way (with indicative arrow)
- 6m wide shared zone
- 6m wide road
- Passing area
- Splay

Legend – Existing Conditions

- Study Area Boundary
- Traffic Signals
- Pedestrian Signals
- Threshold Treatment
- No Entry (Exit Only)
- Right Turn Ban
- Left-turn Only
- One-way
- No Through Road Blockade

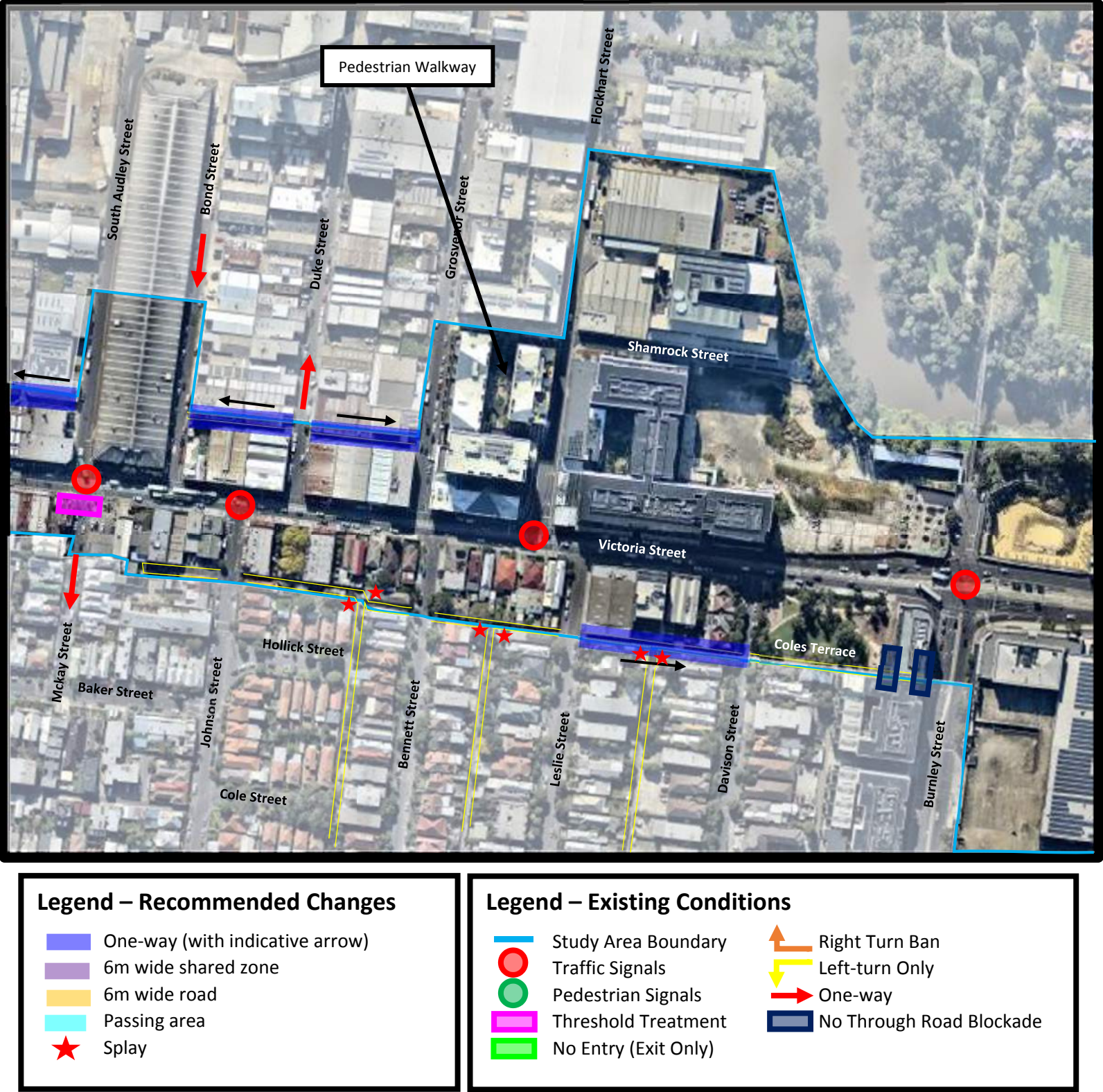


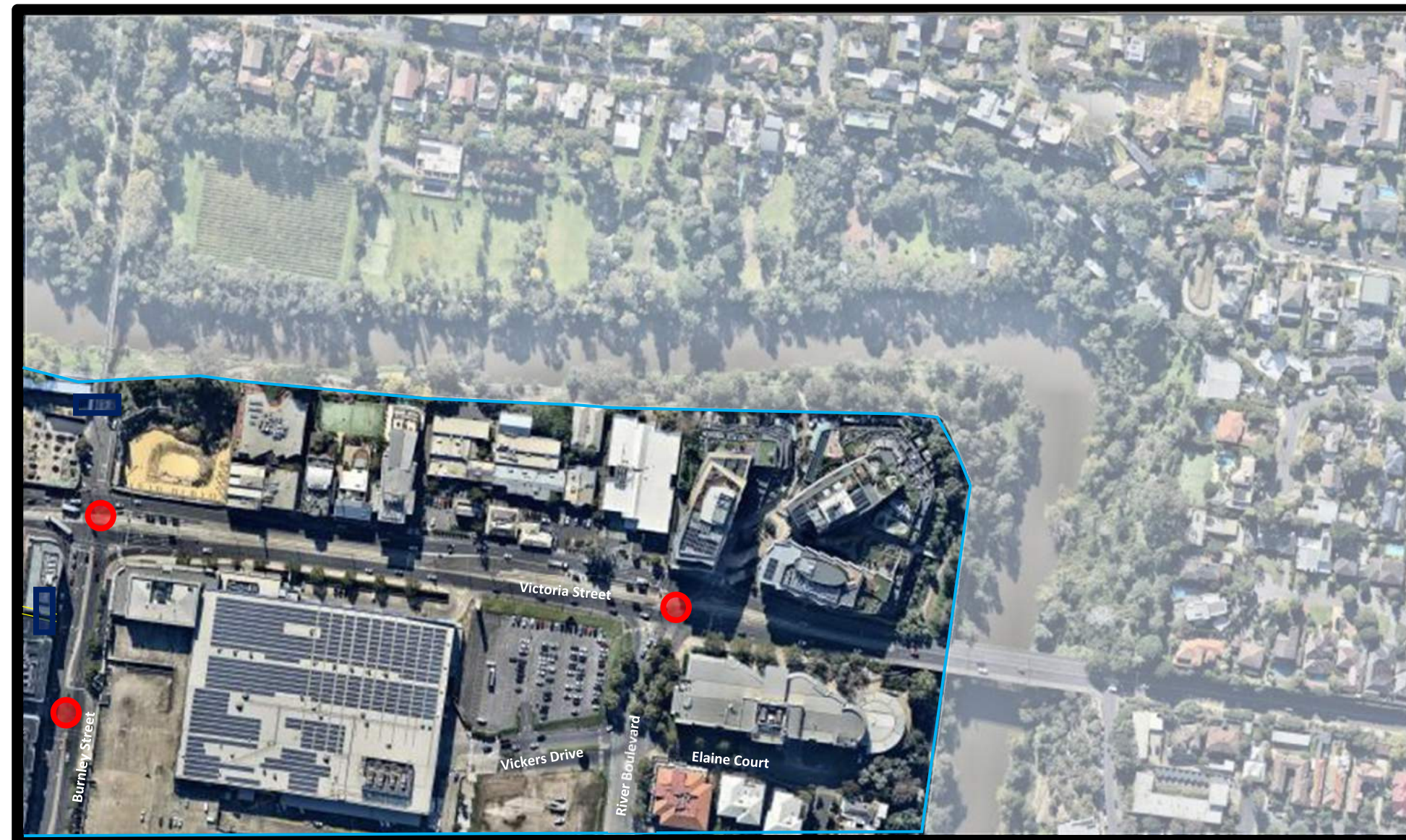
Legend – Recommended Changes

- One-way (with indicative arrow)
- 6m wide shared zone
- 6m wide road
- Passing area
- Splay






Legend – Existing Conditions

- Study Area Boundary
- Traffic Signals
- Pedestrian Signals
- Threshold Treatment
- No Entry (Exit Only)
- Right Turn Ban
- Left-turn Only
- One-way
- No Through Road Blockade













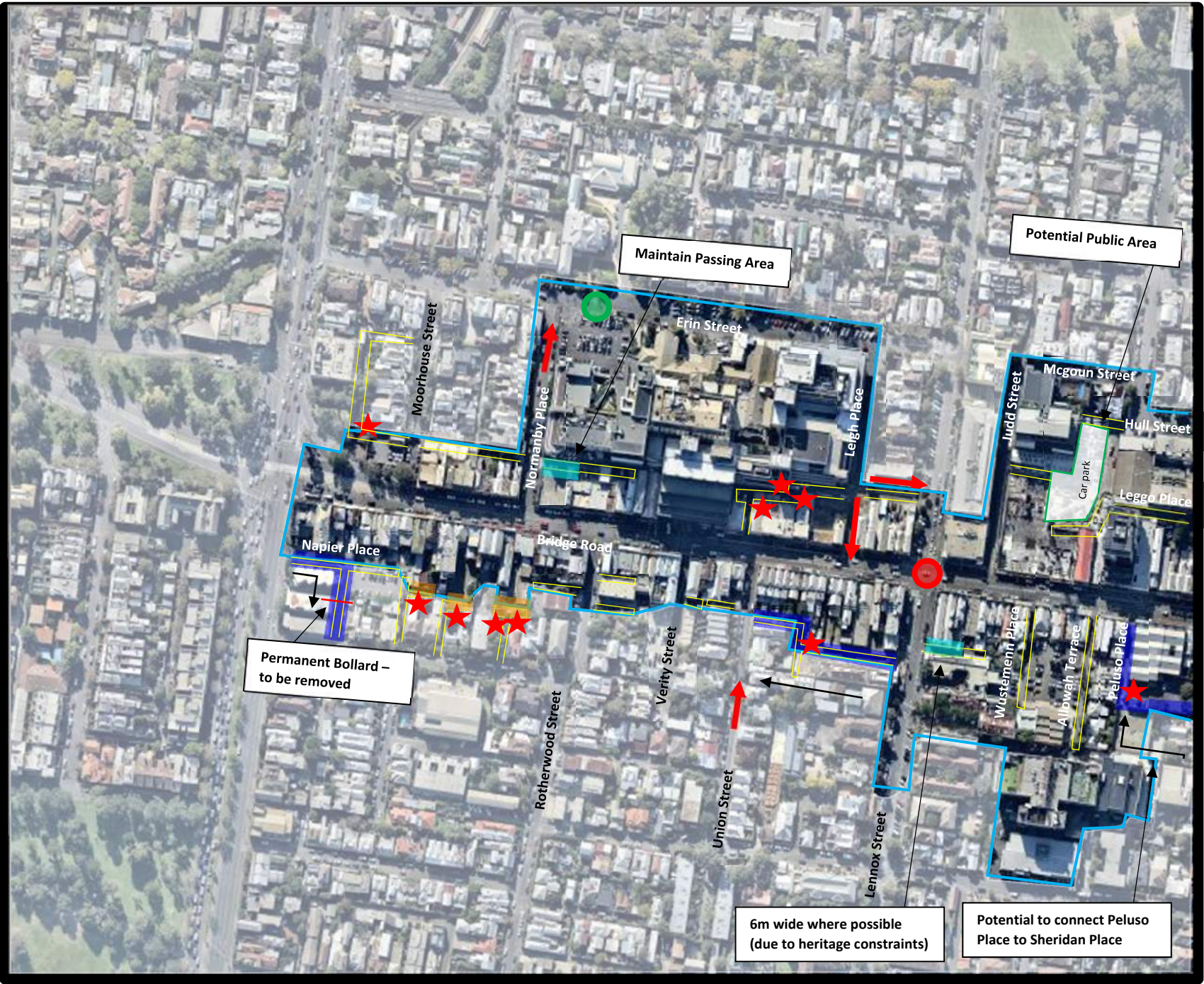


Legend – Recommended Changes

-  One-way (with indicative arrow)
-  6m wide shared zone
-  6m wide road
-  Passing area
-  Splay

Legend – Existing Conditions

- 
- | | | | |
|---|----------------------|---|--------------------------|
|  | Study Area Boundary |  | Right Turn Ban |
|  | Traffic Signals |  | Left-turn Only |
|  | Pedestrian Signals |  | One-way |
|  | Threshold Treatment |  | No Through Road Blockade |
|  | No Entry (Exit Only) | | |

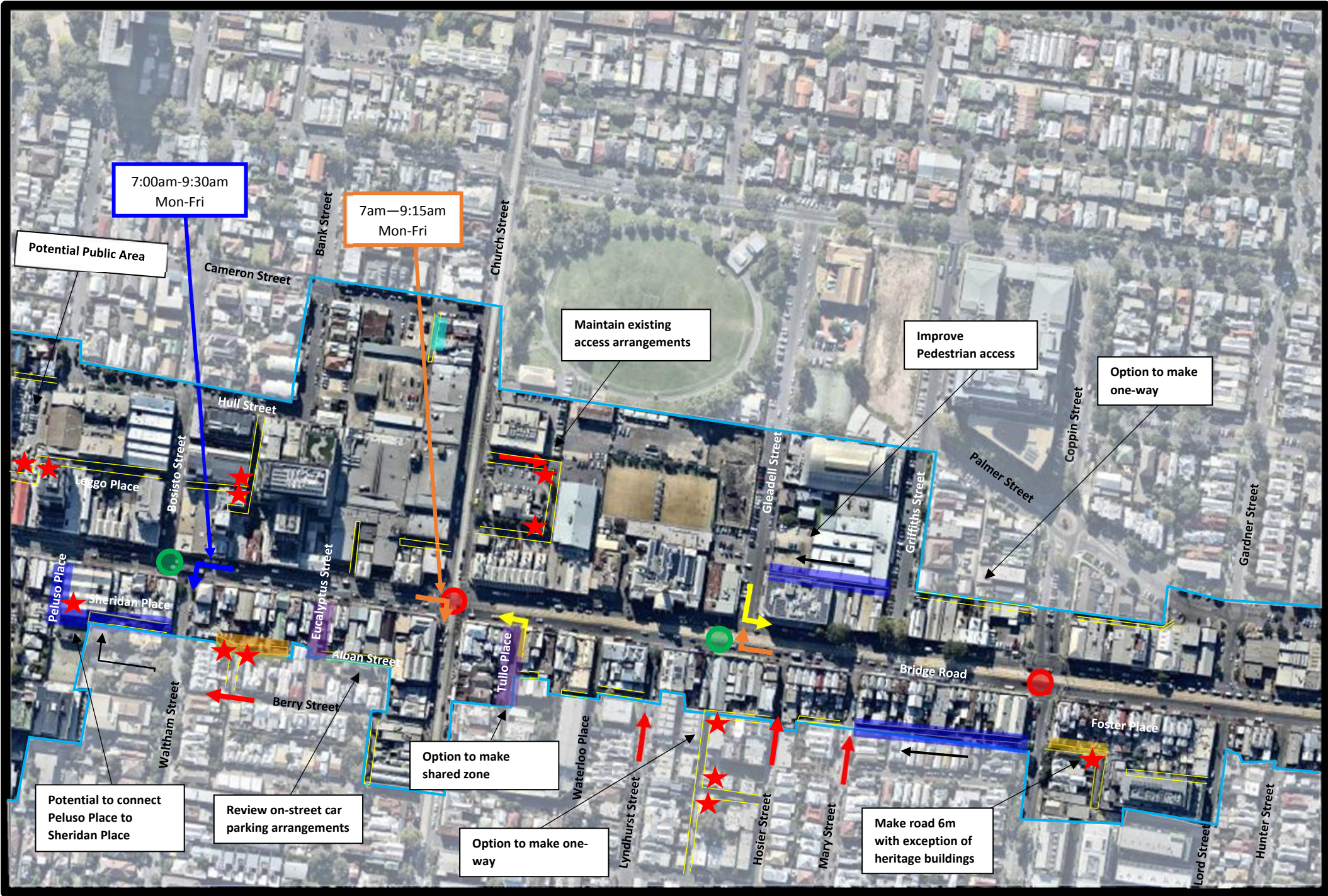


Legend – Recommended Changes

- One-way (with indicative arrow)
- 6m wide shared zone
- 6m wide road
- Passing area
- Splay

Legend – Existing Conditions

- Study Area Boundary
- Traffic Signals
- Pedestrian Signals
- Threshold Treatment
- No Entry (Exit Only)
- Right Turn Ban
- Left-turn Only
- One-way
- Left Turn Ban
- U-Turn Ban

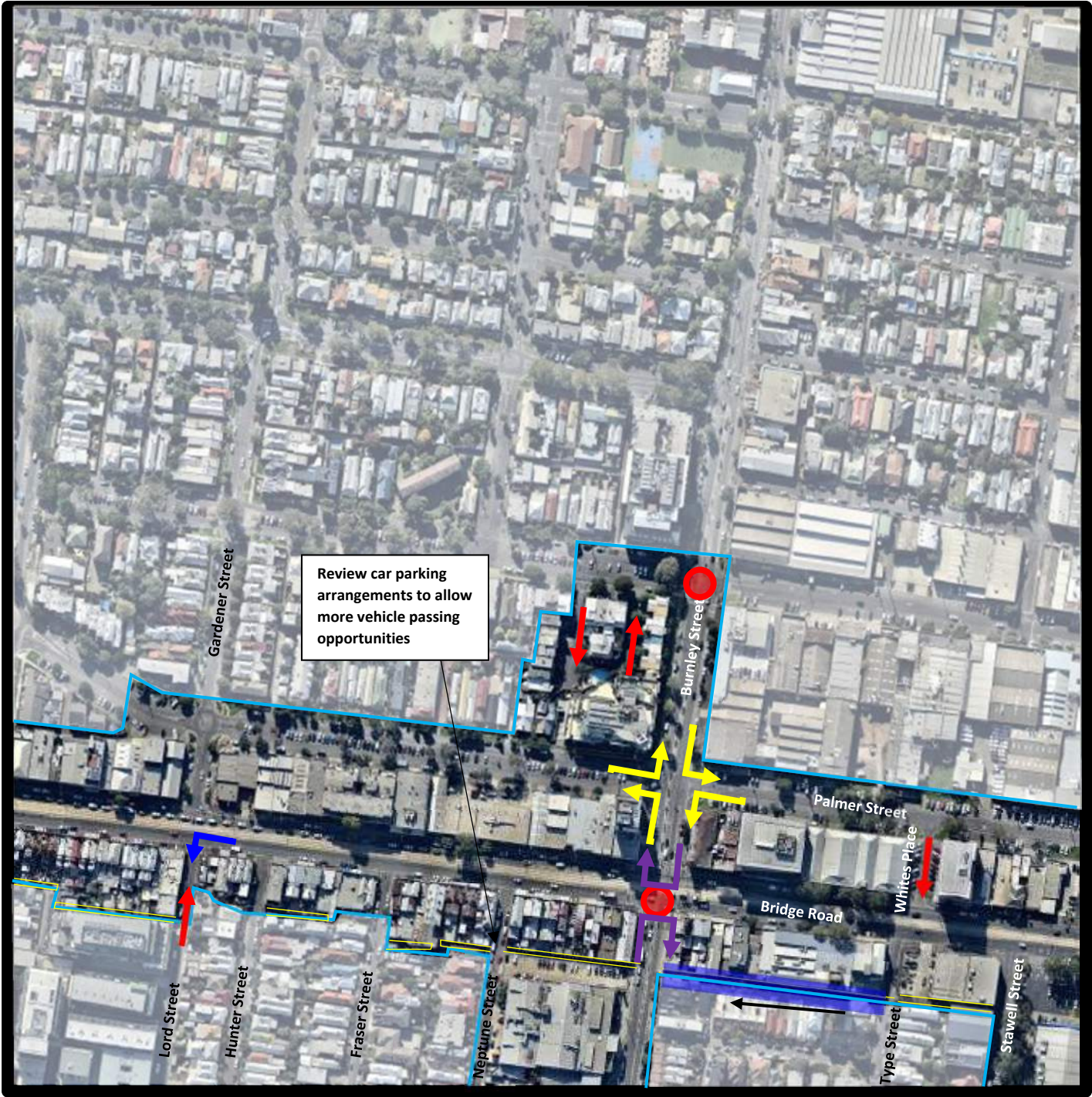


Legend – Recommended Changes

- One-way (with indicative arrow)
- 6m wide shared zone
- 6m wide road
- Passing area
- Splay

Legend – Existing Conditions

- Study Area Boundary
- Traffic Signals
- Pedestrian Signals
- Threshold Treatment
- No Entry (Exit Only)
- Right Turn Ban
- Left-turn Only
- One-way
- Left Turn Ban
- U-Turn Ban

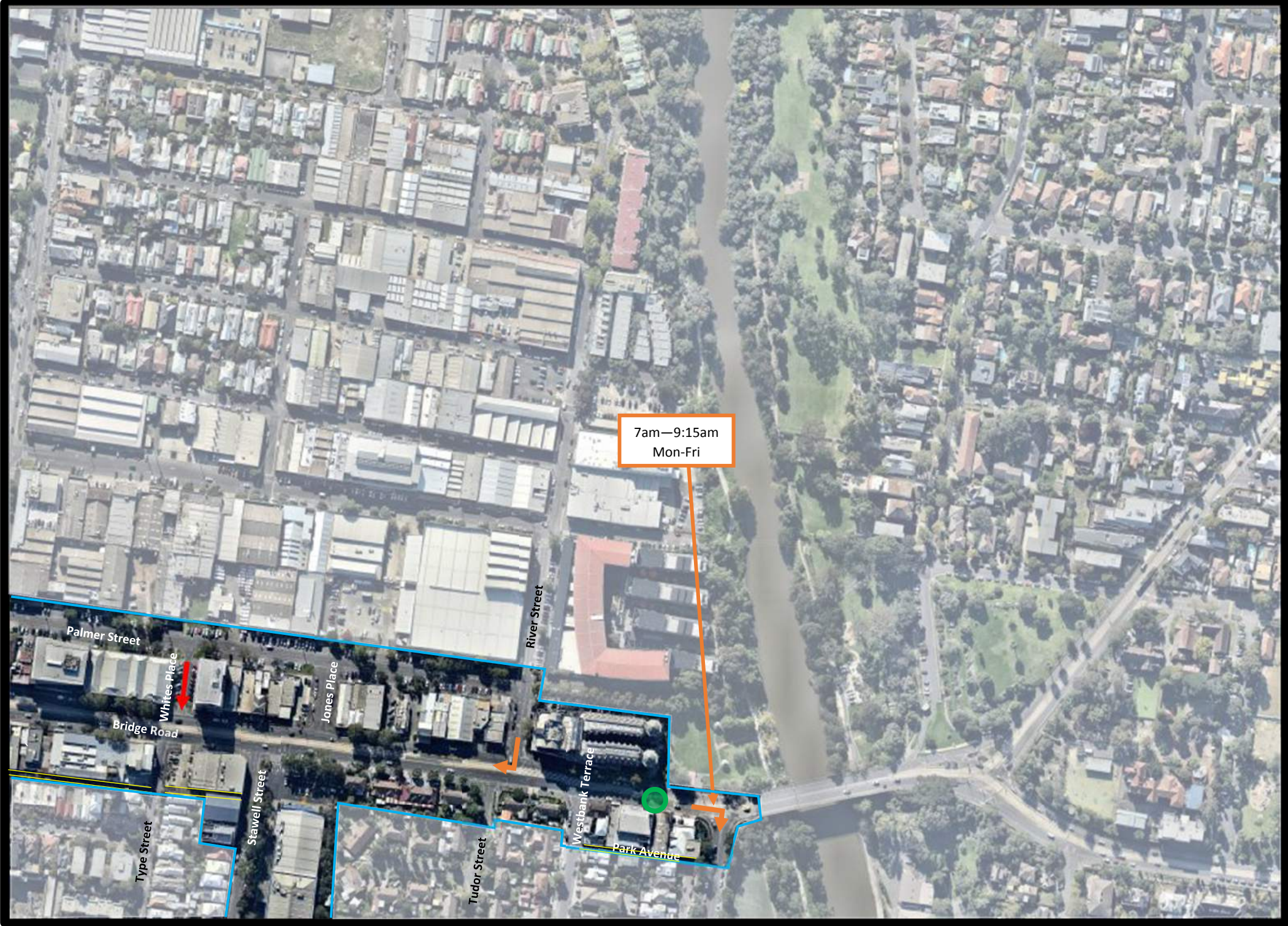


Legend – Recommended Changes

- One-way (with indicative arrow)
- 6m wide shared zone
- 6m wide road
- Passing area
- Splay

Legend – Existing Conditions

- Study Area Boundary
- Traffic Signals
- Pedestrian Signals
- Threshold Treatment
- No Entry (Exit Only)
- Right Turn Ban
- Left-turn Only
- One-way
- Left Turn Ban
- U-Turn Ban



Legend – Recommended Changes

- One-way (with indicative arrow)
- 6m wide shared zone
- 6m wide road
- Passing area
- Splay

Legend – Recommended Changes

- Study Area Boundary
- Traffic Signals
- Pedestrian Signals
- Threshold Treatment
- No Entry (Exit Only)
- Right Turn Ban
- Left-turn Only
- One-way
- Left Turn Ban
- U-Turn Ban



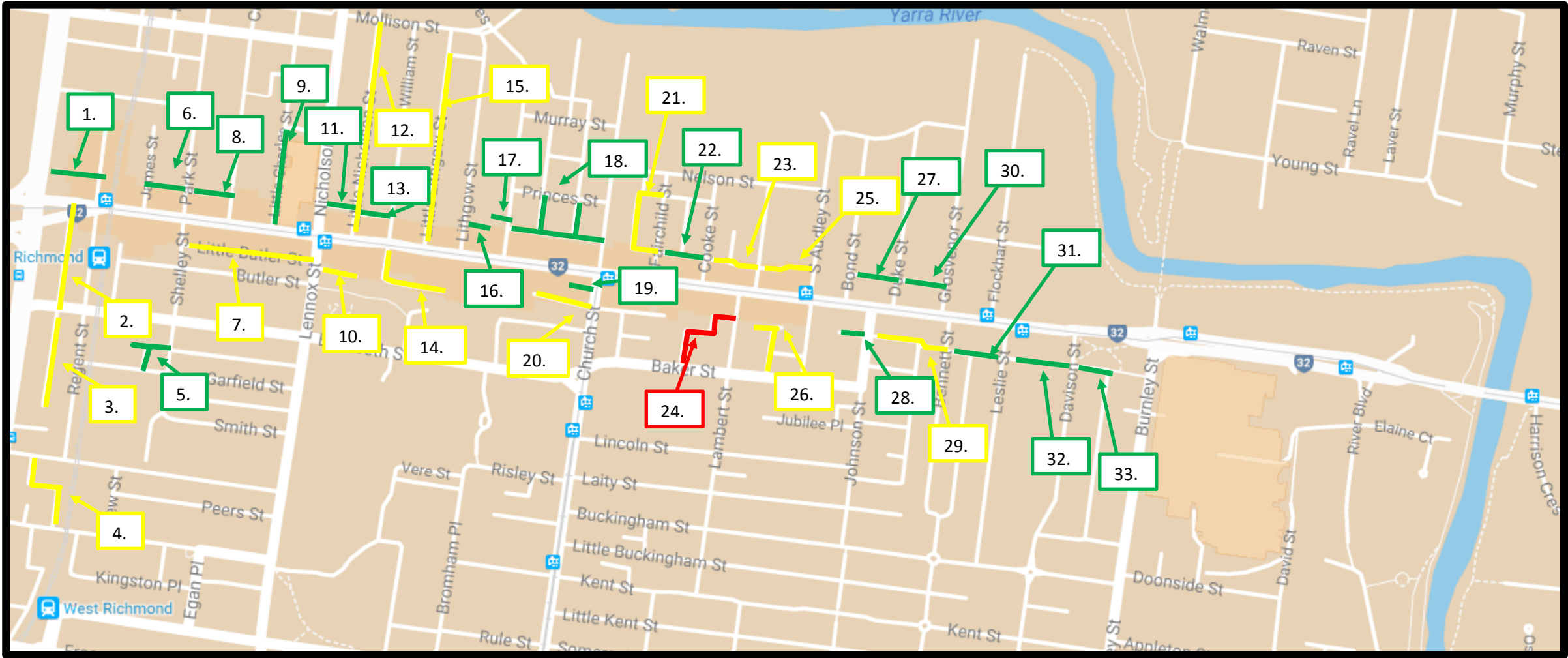
Legend – Recommended Changes

- One-way (with indicative arrow)
- 6m wide shared zone
- 6m wide road
- Passing area
- Splay

Legend – Existing Conditions

- Study Area Boundary
- Traffic Signals
- Pedestrian Signals
- Threshold Treatment
- No Entry (Exit Only)
- Right Turn Ban
- Left-turn Only
- One-way
- Left Turn Ban
- U-Turn Ban

Appendix G: ROW Recommendations




Legend

- Unconstrained Laneway
- Partially Constrained
- Highly Constrained

Appendix G

ROW Recommendations




Street Name	Description	Recommended changes	Photo
1: ROW (from Hoddle Street to Ferguson Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> Carriageway width – 2.85m-3.6m Traffic management – Two-way Parking – No parking Footpaths – No footpaths Material – Asphalt Layout Features – continuous, generally straight <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> Short, straight and connected at both ends. 	<p>One way in the eastbound direction</p> <p>Reason:</p> <p>Narrow width does not allow for passing. This may cause conflict at Hoddle Street between entering and exiting vehicles.</p> <p>The recommendation for one-way eastbound flow directs traffic away from Hoddle Street and eliminates vehicle conflict.</p> <p>Widening laneway challenging given multiple narrow properties accessed via ROW.</p>	

Appendix G

ROW Recommendations




Street Name	Description	Recommended changes	Photo
2: Little Hoddle Street (from Elizabeth Street to Victoria Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 4.6m • Road reservation – 5.95m • Traffic management – Two-way • Parking – No parking • Footpaths – Narrow kerbside/footpath on both sides • Material – Asphalt • Layout features – continuous, straight <p>Constraints: Partially constrained</p> <ul style="list-style-type: none"> • Single lane for two-way traffic • Long length, some development potential • Could be made two-way by creating a shared zone and removing the footpaths 	<p>Create shared zone for two-way flow using the whole carriageway width.</p> <p>Reason:</p> <p>Currently the carriageway too narrow for two-way traffic. Road reserve is wide enough to accommodate two-way traffic flow by removing the footpath to create a shared zone provides for vehicles and pedestrians.</p> <p>The current footpaths are inadequate for pedestrians and a shared zone would better serve all road users while supporting higher traffic volumes.</p>	

Appendix G

ROW Recommendations



Street Name	Description	Recommended changes	Photo
3: Little Hoddle Street (from Elizabeth Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 3.7m-4.8m • Road Reserve – 4.85m-6m • Traffic management – Two-way • Parking – Parking along sections of the east side of the laneway • Footpaths – Narrow kerbing/path • Material – Asphalt • Layout features – dead end, straight, narrows down towards the south <p>Constraints: Partially Constrained</p> <ul style="list-style-type: none"> • Long • Narrower than 6m without road reserve • Parking Arrangements limit two-way flow 	<p>Provide shared area by setting back properties on west side of Little Hoddle Street.</p> <p>Reason:</p> <p>The Built Form Framework indicates that the properties on either side of the laneway have high development potential. Council has indicated a desire to limit vehicle access to Regent Street to improve the public realm.</p> <p>Widening the ROW to 6m is necessary to accommodate the additional development potential given the dead-end nature of the ROW. This laneway should be widened by setbacks of developments on the west side of the lane.</p>	

Appendix G

ROW Recommendations




Street Name	Description	Recommended changes	Photo
4: Wrede Place (from York Street to Egan Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 3.4m-3.85m • Traffic management – Two-way • Parking – No parking • Footpaths – No footpaths • Material – Bluestone in sections and asphalt in sections • Layout features – continuous, s-shaped, no splays <p>Constraints: Partially Constrained</p> <ul style="list-style-type: none"> • Lack of splays makes navigating corners difficult 	<p>Provide passing area at intersection with York Street at #2 York Street.</p> <p>Provide splays on south-west corner of #2 York Street and north-east corner of #30A Wrede Place.</p> <p>Reason:</p> <p>Providing a passing area will minimise conflicts within the lane. Fully 6m carriageway not considered necessary given development potential of abutting land.</p> <p>Splays required to will increase manoeuvrability around corners.</p>	

Appendix G

ROW Recommendations




Street Name	Description	Recommended changes	Photo
5: ROW (from Shelley Street to Garfield Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.2m-3.95m• Traffic management – Two-way• Parking – No parking• Footpaths – No footpaths• Material – Asphalt• Layout features – continuous with a 90 degree bend and extending dead end section to the west, splays on south-east corner <p>Constraints: Unconstrained laneway</p> <p>Short and connected at both ends.</p>	No changes required. Largely built out.	

Appendix G

ROW Recommendations



Street Name	Description	Recommended changes	Photo
6: ROW (from James Street to Park Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3m• Traffic management – Two-way, must turn right at Park Street• Parking – Shared off-street car park on south side of ROW• Footpaths – No footpaths• Material – Asphalt with bluestone kerbing• Layout features – continuous, straight <p>Constraints: Unconstrained laneway</p> <p>Short, straight and connected at both ends.</p>	<p>No changes required.</p> <p>Relatively short length means that vehicle conflicts are likely to be minimal and easily managed by drivers.</p>	

Appendix G

ROW Recommendations




Street Name	Description	Recommended changes	Photo
7: Little Butler Street (from Shelly Street to Lennox Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 2.7m-3m • Road reservation – 3.95m-4.75m • Traffic management – Two-way • Parking – kerbside parallel both sides • Footpaths – No footpaths • Materials – Asphalt • Layout features – continuous, straight <p>Constraints: Partially constrained</p> <ul style="list-style-type: none"> • Long length • Inability to easily widen for 2-way traffic flow • Could be made one-way 	<p>One way in the eastbound direction</p> <p>Reason:</p> <p>Narrow width does not allow for vehicle passing. Relatively long length and high number of abutting properties increases likelihood of vehicle conflict.</p> <p>One-way arrangement recommended over increasing width due to number of abutting properties.</p>	

Appendix G

ROW Recommendations



Street Name	Description	Recommended changes	Photo
8: ROW (from Park to Charles)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.1m• Traffic management – Two-way• Parking – Shared off-street car park on south side and west end of ROW• Footpath – No footpath• Material – Concrete• Layout features – continuous, straight <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none">• Unconstrained due to short length	<p>No changes required.</p> <p>Relatively short length means that vehicle conflicts are likely to be minimal and easily managed by drivers.</p>	

Appendix G

ROW Recommendations





Street Name	Description	Recommended changes	Photo
9: Little Charles Street (from Victoria Street to Little Charles Close)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 3.5m • Road reservation – 5.15m • Traffic management – One-way (southbound) • Parking – No parking • Footpath – Narrow path on east side, with traversal onto road required at power poles • Material – Asphalt <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> • Unconstrained due to one-way nature 	<p>No changes required.</p> <p>Existing one-way arrangement.</p>	

Appendix G

ROW Recommendations



Street Name	Description	Recommended changes	Photo
10: ROW (from Lennox Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 3.5m • Traffic management – Two-way • Parking – Car Park at east end • Footpath – No footpaths • Material – Concrete • Layout features – slight bend to the south <p>Constraints: Partially Constrained</p> <ul style="list-style-type: none"> • Constrained due to dead end 	<p>Provide passing area at entrance, with setback to #136 Victoria Street.</p> <p>Reason:</p> <p>To minimise conflict within laneway.</p>	
11: ROW (from Nicholson Street to Little Nicholson Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 4.55m • Traffic management – Two-way • Parking – No Parking • Footpath – No footpaths • Material – Concrete <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> • Unconstrained due to short length 	<p>Make one-way in the westbound direction</p> <p>Reason:</p> <p>By making the ROW one-way in the westbound direction, and Little Nicholson one-way in the northbound direction, a loop is created, which will minimise vehicle conflict.</p> <p>Relatively high development potential of abutting land.</p>	

Appendix G

ROW Recommendations



Street Name	Description	Recommended changes	Photo
12: Little Nicholson Street (from Victoria Street to Mollison Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 4.9m • Traffic management – Two-way • Parking – No parking • Footpath – No footpath • Material – Concrete • Layout Features – loading activity occurs frequently, blocking traversal of ROW <p>Constraints: Partially constrained</p> <ul style="list-style-type: none"> • Long length • Insufficient for 2-way flow • Could be made one-way 	<p>Provide a one-way section between Victoria Street and ROW #11 and #13 in the northbound direction.</p> <p>Reason: Little Nicholson Street is not wide enough to accommodate two-way vehicle flow. In order to minimise conflict one-way traffic flow should be provided northbound, where vehicles can either continue along Little Nicholson Street, or turn left onto ROW #11 to exit.</p>	

Appendix G

ROW Recommendations




Street Name	Description	Recommended changes	Photo
13: ROW (from Little Nicholson Street to William Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 2.95m • Traffic management – Two-way • Parking – No parking • Footpath – No footpath • Material – Bluestone • Layout features – narrow <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> • Unconstrained due to short length, continuous 	<p>Make one-way in the westbound direction</p> <p>Reason: Carriageway is not wide enough for two-way traffic flow. A one-way in the westbound direction will allow vehicles to continue along ROW #11.</p>	

Appendix G

ROW Recommendations


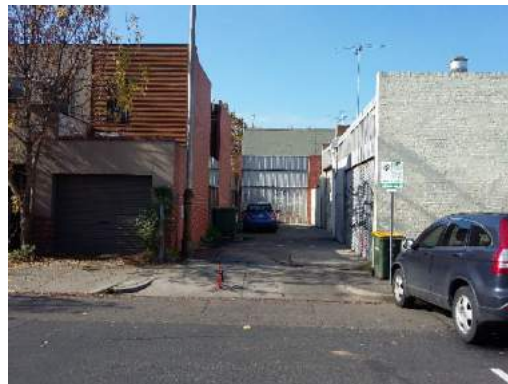


Street Name	Description	Recommended changes	Photo
14: ROW (from Victoria Street to END, opposite William Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 4.75m for north-south section and 3m for east-west section • Traffic management – Two-way • Parking – No parking • Footpath – No footpath • Material – Concrete • Layout Features – Splay provided at bend, over land of 176 Victoria Street <p>Constraints: Partially constrained</p> <ul style="list-style-type: none"> • Single Lane • Length • 90 degree bend • Some development potential • Would require widening for two-way traffic, particularly north-south leg 	<p>Provide 6m two-way road for full length, with setback to all properties on the north and east side of the ROW.</p> <p>Review need for separate pedestrian path on north-south leg.</p> <p>Reason:</p> <p>Development potential of the laneway is high and vehicles cannot currently pass one another without relying on private lane. Blind corner also creates conflict.</p> <p>This laneway is also a pedestrian route. This may require further width or implementation of a shared zone on the north-south leg.</p>	

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ROW Recommendations



Street Name	Description	Recommended changes	Photo
15: Little Lithgow Street (from Victoria Street to Mollison Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> Carriageway width – 5.1m Traffic management – Two-way Parking – No parking Footpath – No footpath Material – Asphalt <p>Constraints: Partially Constrained</p> <ul style="list-style-type: none"> Slightly too narrow for two-way traffic flow 	<p>One-way in the southbound direction.</p> <p>Reason:</p> <p>Little Lithgow is slightly too narrow to allow two-way traffic flow. The long length of the lane creates a problem with conflict.</p>	
16: ROW (from Lithgow Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> Carriageway width – 5.4m Traffic management – Two-way Parking – No parking Footpath – No footpath Material – Concrete <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> Sufficient width for two-way traffic flow 	<p>No changes required.</p> <p>Short length and width means vehicle conflict would be minimal.</p>	

Appendix G

ROW Recommendations




Street Name	Description	Recommended changes	Photo
17: ROW (from Albert Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.2m• Traffic management – Two-way• Parking – No parking• Footpath – No footpath• Material – concrete <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none">• Unconstrained due to short length	<p>No changes required.</p> <p>Short length and width means vehicle conflict would be minimal.</p>	A photograph of a narrow, short laneway between buildings. The ground is paved with concrete and has some fallen leaves. There are buildings on both sides, and a car is visible in the distance. The laneway is very short and narrow, illustrating the existing conditions.

Appendix G

ROW Recommendations


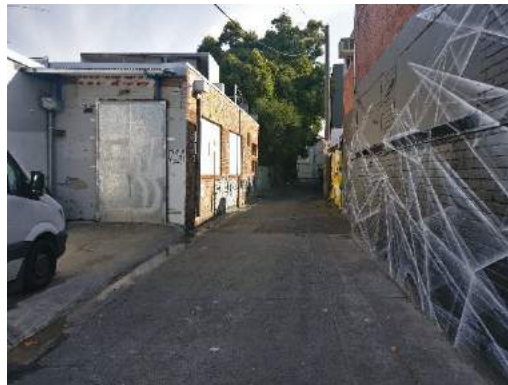


Street Name	Description	Recommended changes	Photo
18: ROW (from Albert Street to Church Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 4.4m• Traffic management – Two-way, right turn only at Fairchild Street• Parking – Car park at midpoint of ROW• Footpath – No footpath• Material – Concrete• Layout features – there is are two connecting north-south ROWs extending northerly <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none">• Unconstrained due to being continuous, could be one-way	<p>One-way eastbound from Albert Street to Church Street. North-south sections to be one-way northbound.</p> <p>Reason:</p> <p>High development potential. One-way arrangement addresses vehicle conflict issues.</p>	 A photograph of a narrow laneway. On the left is a red brick building with a black roller door. On the right is a white building with a balcony. The laneway is paved and leads to a street at the end.

Appendix G

ROW Recommendations




Street Name	Description	Recommended changes	Photo
19: ROW (from Church Street to End)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> Carriageway width – 3.05m Traffic management – Two-way Parking – No parking Footpath – No footpath Material – Asphalt <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> Unconstrained due to short length 	No changes required due to short length.	
20: Victoria Place (from Church Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> Carriageway width – 4.75m, 5.7m aisle for western car park Traffic management – Two-way Parking – Parking provided in car park at western end Footpath – No footpath Material – Concrete <p>Constraints: (Partially constrained)</p> <ul style="list-style-type: none"> Dead end Some development potential 	<p>Provide passing area at entrance with setback to #6 Church Street.</p> <p>Reason:</p> <p>Connects directly to Church Street (arterial road) and a passing area eliminates vehicle conflict at this critical location.</p>	

Appendix G

ROW Recommendations




Street Name	Description	Recommended changes	Photo
21: ROW (from Fairchild Street to Fairchild Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 3m • Traffic management – Two-way, must enter via right turn from Fairchild, exit via left turn to Fairchild • Parking – No parking • Footpath – No footpath • Material – Bluestone • Layout features – connects to ROW extending north-south that loops back to Fairchild Street <p>Constraints: Partially constrained</p> <ul style="list-style-type: none"> • No splay • Low development potential • Single lane • Length • Bends 	<p>Provide passing area at southern connection to Fairchild Street with setback to #463 and #465 Victoria Street.</p> <p>Reason:</p> <p>A passing area to accommodate the development potential of the properties adjacent to Victoria Street.</p>	

Appendix G

ROW Recommendations




Street Name	Description	Recommended changes	Photo
22: ROW (from Fairchild to Cooke Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> Carriageway width – 2.9m Traffic management – Two-way, must travel south on Fairchild Street, and north on Cooke Street Parking – No parking Footpath – No footpath Material – Asphalt Layout features – there is a ROW that extends northerly, where there are no splays, making it difficult to traverse due to the narrow width <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> Unconstrained due to short length, continuous 	<p>One way in the eastbound direction.</p> <p>Reason:</p> <p>The road is only wide enough for one-way flow, and due to the one-way restrictions already in place on Fairchild Street and Cooke Street, the eastbound direction is most appropriate.</p>	

Appendix G

ROW Recommendations




Street Name	Description	Recommended changes	Photo
23: ROW (from Cooke Street to Thompson Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 3.1m-3.8m • Road Reservation – 3.1m-4.7m • Traffic management – Two-way • Parking – No parking • Footpath – No footpath • Material – Asphalt • Layout Features – there is a kink in the ROW at the midpoint, which is also where a northerly ROW also connects, the 4.2m width of the connecting ROW provides space to navigate this kink <p>Constraints: Partially constrained</p> <ul style="list-style-type: none"> • An improved splay would assist with the kink in the ROW, especially for service vehicles 	<p>One way in the westbound direction.</p> <p>Splays on #1 & #6 Cooke Street and #493 Victoria Street.</p> <p>Reason:</p> <p>The road is only wide enough for one-way flow, and due to the one-way restrictions already in place on Cooke Street and Thompson Street, the westbound direction is most appropriate.</p> <p>Splays required to improve vehicle access at corners.</p>	

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ROW Recommendations




Street Name	Description	Recommended changes	Photo
24: ROW (from Lambert Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 2.8m-4m • Traffic management – Two-way • Parking –No parking • Footpath – No footpath • Material – Asphalt and bluestone • Layout features – There are a number of bends in the ROW. Splays are provided in the narrower sections, but not for bends connecting to the 4m width section. The ROW also connects to Baker Street in the south <p>Constraints: Highly constrained</p> <ul style="list-style-type: none"> • Length, number of properties • Narrow • Bends with without splays • Properties at corners are outside of the study boundary 	<p>One-way from Lambert Street to Baker Street</p> <p>Splays required at corners of #2 Lambert Street, #332 Victoria Street, #31 Baker Street and #24 Eureka Street.</p> <p>Reason:</p> <p>The lane is narrow and has significant potential for conflict due to having a number of 90° corners. Splays will need to be provided to make the lane fully trafficable.</p>	

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ROW Recommendations




Street Name	Description	Recommended changes	Photo
25: ROW (from Thompson Street to South Audley Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 3.6m-3.7m • Traffic management – Two-way • Parking – Car park on the north side of the ROW, behind 2 Thompson Street • Footpath – No footpath • Material – Asphalt • Layout features – There is a kink in the middle of the ROW, where there is another northerly connected ROW. Potentially challenging to navigate the kink <p>Constraints: Partially constrained</p> <ul style="list-style-type: none"> • Kink • Lack of Splays 	<p>One way in the westbound direction</p> <p>Provide splay at #523 Victoria Street.</p> <p>Property setback for #2 Thompson Street will need to be maintained in order to facilitate movement.</p> <p>Reason:</p> <p>The road is only wide enough for one-way flow. One-way westbound encourages drivers to enter local road network at South Audley Street traffic signals.</p> <p>Splays will need to be provided in order to facilitate movement.</p>	

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ROW Recommendations





Street Name	Description	Recommended changes	Photo
26: ROW (East-west ROW connected to Wells Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 4.7m-4.85m • Traffic management – Two-way • Parking – No parking • Footpath – No footpath • Material – Asphalt • Layout Features – connects to the northern end of Wells Street. No splays are provided at the intersection <p>Constraints: Partially constrained</p> <ul style="list-style-type: none"> • 90 degree bends • Lack of splays 	<p>Widen to 6m for properties abutting Victoria Street.</p> <p>Make Wells Street one-way northbound from Baker Street to the east-west ROW connecting to McKay Street.</p> <p>East-west ROW one-way eastbound.</p> <p>Reasons:</p> <p>Widening ROW to 6m for properties abutting Victoria Street facilitates vehicle access to all properties and reduces vehicle conflict.</p> <p>One-way arrangement reduces vehicle conflict within Wells Street without need to widen street.</p>	

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ROW Recommendations




Street Name	Description	Recommended changes	Photo
27: ROW (from Bond Street to Duke Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> Carriageway width – 3.4m Traffic management – Two-way, Bond Street is one-way northerly and Duke Street is one-way southerly Parking – No Parking Footpath – No Footpath Material – Asphalt <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> Unconstrained due to short length, continuous 	<p>Change to one-way westbound.</p> <p>Reasons:</p> <p>The road is only wide enough for one-way flow, and due to the one-way restrictions already in place on Duke Street and Bond Street, the westbound direction is most appropriate.</p>	
28: ROW (from Johnson Street to END, on west side of Johnson Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> Carriageway width – 4.55m-6.35m Traffic management – Two-way Parking – No parking Footpath – No footpath Material – Asphalt <p>Constraints: (Unconstrained laneway)</p> <ul style="list-style-type: none"> Unconstrained due to short length 	<p>No changes required due to width and short length.</p>	

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ROW Recommendations



Street Name	Description	Recommended changes	Photo
29: ROW (from Johnson Street to Bennett Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 2.95m-3.7m• Traffic management – Two-way• Parking – No Parking• Footpath – No Footpath• Material – Asphalt• Layout features – There is a kink in the ROW, which also connects to a southerly ROW. There is a splay on the south-west side of the intersection <p>Constraints: Partially constrained</p> <ul style="list-style-type: none">• Kink	<p>No changes required due to low development potential.</p> <p>Splays required at bends.</p>	

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ROW Recommendations



Street Name	Description	Recommended changes	Photo
30: ROW (from Duke Street to Grosvenor Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.4m• Traffic management – Two-way• Parking – No parking• Footpath – No footpath• Material – Asphalt <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none">• Unconstrained due to short length, continuous	<p>Change to one-way eastbound.</p> <p>Reasons:</p> <p>The road is only wide enough for one-way flow, and due to the one-way restrictions already in place on Duke Street, the eastbound direction is most appropriate.</p>	A photograph of a narrow alleyway. The walls on the left are covered in graffiti and have a 'VACC' sign. A blue building is on the right. The ground is paved.

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ROW Recommendations




Street Name	Description	Recommended changes	Photo
31: Coles Terrace (from Bennett Street to Leslie Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 2.7m-2.9m• Traffic management – Two-way• Parking – No parking• Footpath – No parking• Material – Bluestone• Layout features – There is a connecting southerly ROW of 3.05m width with a splay on the south-west corner of the intersection <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none">• Unconstrained due to short length, continuous	<p>No changes required except splays at corner due to low development potential.</p> <p>Can be made one-way in future, if necessary.</p>	A photograph of a narrow, paved laneway. On the left is a white wall with a house behind it. On the right is a wooden fence. A road sign is visible on the fence. The sky is blue with some clouds.

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ROW Recommendations



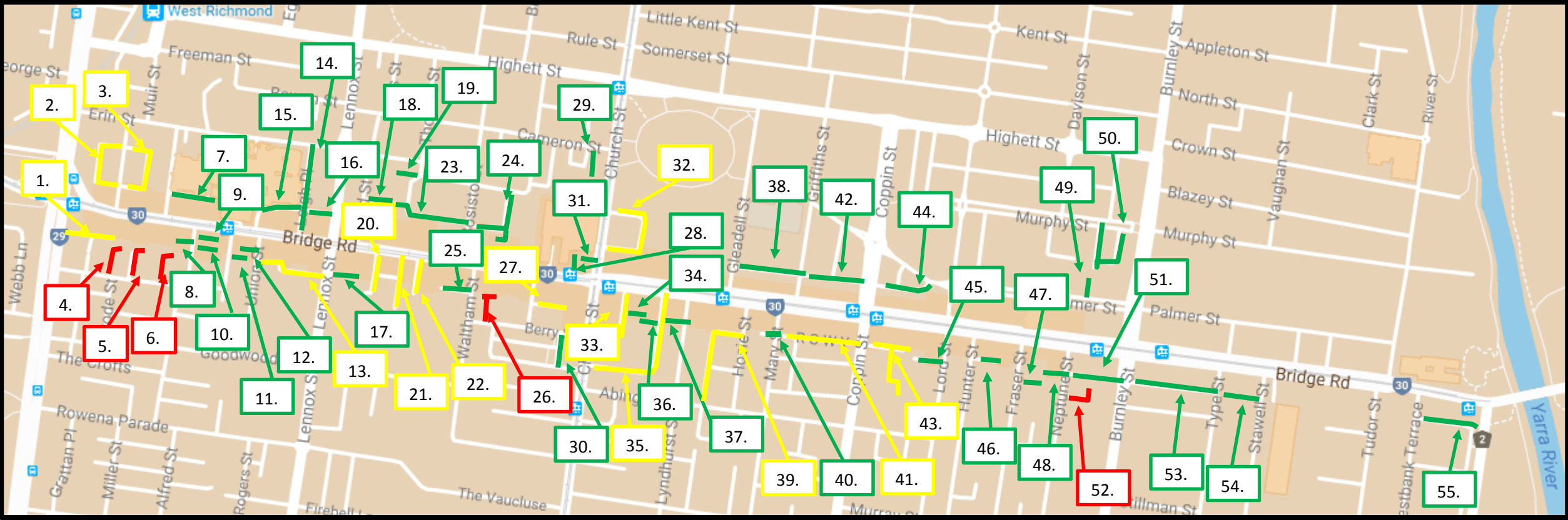
Street Name	Description	Recommended changes	Photo
32: Coles Terrace (from Leslie Street to Davidson Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 3.15m • Traffic management – Two-way • Parking – No parking • Footpath – No footpaths • Material – Bluestone • Layout features - There is a connecting southerly ROW of 2.85m width with a slight splay on each corner. Corner is still quite difficult to traverse due to narrow width, and shallow depth of splay <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> • Unconstrained due to short length, continuous 	<p>One-way in the eastbound direction.</p> <p>Reason:</p> <p>Due to development potential of laneway, change to one-way flow to minimise vehicle conflicts.</p>	

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ROW Recommendations



Street Name	Description	Recommended changes	Photo
33: Coles Terrace (from Davidson Street to Burnley Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.6m-4.6m• Traffic management – Bollards prevent vehicles from entering/exiting ROW at Burnley Street• Parking – No parking• Footpath – No footpaths• Material – Asphalt• Layout features - There is a connecting southerly ROW of 3.05m width with no splays. Low vegetation and kerbing on the northern side of the ROW allow for the vehicle body to overhang. <p>Constraints: Unconstrained laneway</p> <p>Unconstrained due to short length, low development potential</p>	No changes required due to low development potential.	A photograph showing a narrow, paved laneway leading from a residential street. On the left is a chain-link fence with some vegetation behind it. On the right is a red brick house with a gabled roof and a small porch. A black trash bin is visible on the right side of the laneway. The ground is paved with asphalt.




Legend

- Unconstrained Laneway
- Partially Constrained
- Highly Constrained

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ROW Recommendations




Street Name	Description	Recommended Changes	Photo
1: Napier Lane (from Hoddle Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.85m• Trafficable width – 4.3m• Traffic management – Two-way• Parking – Car park attached to eastern end of lane• Footpaths – No footpaths• Material – Bluestone• Layout features – There is a connecting ROW to the south which connects to Sherwood Street, however bollards block access. <p>Constraints: Partially Constrained</p> <ul style="list-style-type: none">• Limited Carriageway	<p>Remove road block in the north-south connecting ROW to allow for one-way flow from Hoddle Street to Sherwood Street.</p> <p>Reason:</p> <p>If the road block is removed, then one-way flow can be achieved and should be directed away from Hoddle Street.</p> <p>This eliminates vehicle conflict without the need for widening.</p>	 A photograph of a narrow residential street, Napier Lane. The street is paved with asphalt and has white dashed lines. On the left, there is a building with graffiti. On the right, there is a white wall and some trees. The street appears to be blocked at the end by a road block.

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ROW Recommendations




Street Name	Description	Recommended Changes	Photo
2: ROW (from west side Moorhouse Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.15m• Traffic management – Two-way• Parking – No parking• Footpaths – No footpaths• Material – Bluestone• Layout features –there is a connecting northbound ROW which loops back to Moorhouse Street, with splays at the corners <p>Constraints: Partially Constrained</p> <ul style="list-style-type: none">• Lack of passing opportunities• Lack of sight distance around bends.	No changes required due to short length serving properties to be developed.	

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ROW Recommendations



Street Name	Description	Recommended Changes	Photo
3: ROW (from east end of Moorhouse Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.2-3.25m• Traffic Management – Two-way• Parking – Car park at east end of ROW• Footpaths – No footpaths• Material – Bluestone• Layout features – connecting ROW to the north which loops back to Moorhouse Street, with splays on each corner <p>Constraints: Partially Constrained</p> <ul style="list-style-type: none">• Lack of passing opportunities• Lack of sight distance around bends.	No changes required due to short length serving properties to be developed.	 A photograph of a narrow, paved alleyway. On the left is a brick building with a white picket fence. On the right is a red building with a metal gate and two green trash bins. The alleyway is paved with dark material and leads towards a brighter area in the distance.

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ROW Recommendations




Street Name	Description	Recommended Changes	Photo
4: ROW (East-West section of westernmost ROW from Sherwood Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 2.7m-3m• Road reservation – 3.95m-4.75m• Traffic management – Two-way• Parking – kerbside parallel both sides• Footpaths – No footpaths• Materials – Asphalt• Layout features – connected to ROW at the south, of width 3.6m, with no splays provided. <p>Constraints: Highly Constrained</p> <ul style="list-style-type: none">• Single lane• No Splays at T-intersection• Limited potential to widen critical north-south link	<p>Increase width of road to 6m for east-west section by setting back properties along Bridge Road.</p> <p>Reason:</p> <p>In order to facilitate rear vehicle access to properties fronting Bridge Road, an increased setback is necessary. This manages vehicle conflict in the laneway by providing space for vehicles to pass and facilitates vehicle turning at the bend of the ROW.</p>	A photograph showing a narrow residential street. On the left, a silver car is parked. On the right, there is a house with a red brick wall and a green fence. The street is paved with asphalt and has some fallen leaves on it.

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ROW Recommendations




Street Name	Description	Recommended Changes	Photo
5: ROW (East-West section of middle ROW from Sherwood Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 4.6m • Traffic management – Two-way • Parking – No Parking • Footpath – No footpath • Material – Asphalt • Layout features – connected to ROW at the south, of width 3.5m, with no splays provided. <p>Constraints: Highly Constrained</p> <ul style="list-style-type: none"> • Single lane • No Splays at T-intersection • Limited potential to widen critical north-south link 	<p>Increase width of road to 6m for east-west section by setting back properties along Bridge Road.</p> <p>Reason:</p> <p>In order to facilitate rear vehicle access to properties fronting Bridge Road, an increased setback is necessary. This manages vehicle conflict in the laneway by providing space for vehicles to pass and facilitates vehicle turning at the bend of the ROW.</p>	

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ROW Recommendations





Street Name	Description	Recommended Changes	Photo
6: ROW (easternmost ROW from Sherwood Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 2.75m • Traffic management – Two-way • Parking – No parking • Footpath – No footpath • Material – Asphalt • Layout features – Narrow width and bend at north end. Setback property on western side. <p>Constraints: Highly Constrained</p> <ul style="list-style-type: none"> • Single lane • No Splays at T-intersection • Limited potential to widen critical north-south link 	<p>Increase width of road to 6m for east-west section by setting back properties along Bridge Road.</p> <p>Reason:</p> <p>In order to facilitate rear vehicle access to properties fronting Bridge Road, an increased setback is necessary. This manages vehicle conflict in the laneway by providing space for vehicles to pass and facilitates vehicle turning at the bend of the ROW.</p>	

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ROW Recommendations





Street Name	Description	Recommended Changes	Photo
7: ROW (from Normanby Place to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 3.3m, widens at intersection with Normanby Place • Traffic management – Two-way • Parking – No Parking • Footpath – No footpaths • Material – Asphalt • Layout features – Hospital uses this ROW <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> • Passing area at entrance to laneway 	<p>No Changes.</p> <p>Maintain existing passing area at entrance.</p>	
8: ROW (from west side of Rotherwood Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 5.3m • Traffic management – Two-way • Parking – No Parking • Footpath – No footpaths • Material – Bluestone <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> • Wide enough for two-way traffic flow • Short length 	<p>No changes required due to short length.</p>	

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ROW Recommendations





Street Name	Description	Recommended Changes	Photo
9: ROW (from east side of Rotherwood Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 3.05m • Traffic management – Two-way • Parking – No parking • Footpath – No footpath • Material – Asphalt • Layout features – short and narrow <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> • Short • Limited development potential 	No changes required due to short length.	
10: ROW (from east side of Rotherwood Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 3.8m • Traffic management – Two-way • Parking – Car park on south side • Footpath – No footpath • Material – Concrete <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> • Short • Limited development potential 	No changes required due to short length.	

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ROW Recommendations




Street Name	Description	Recommended Changes	Photo
11: ROW (from Verity Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 6.05m • Traffic management – Two-way • Parking – Open tandem parking for adjacent properties • Footpath – No footpath • Material – Concrete <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> • Short <p>Limited development potential</p>	No changes required due to short length.	
12: ROW (West side of Union Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 3m • Traffic management – Two-way, No Entry to Union Street from Bridge Road • Parking – No parking • Footpath – No footpath • Material – Bluestone <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> • Short • Limited development potential 	No changes required due to short length.	

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ROW Recommendations



Street Name	Description	Recommended Changes	Photo
13: ROW (East side of Union Street to Lennox Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.7m-3.75m• Traffic management – Two-way, No Entry to Union Street from Bridge Road• Parking – No parking• Footpath – No footpath• Material – Asphalt and Bluestone• Layout features – there is a kink involving two 90 degree bends. A splay is provided on one side of the northern bend <p>Constraints: Partially Constrained</p> <ul style="list-style-type: none">• Non-functional kink breaks laneway into two parts	<p>Make one-way from Lennox Street to Union Street.</p> <p>Reasons:</p> <p>Passing area is not possible, due to heritage buildings.</p>	 A photograph showing a narrow, paved laneway between two heritage-style buildings. The building on the left is a light-colored stone wall with a dark door. The building on the right is a white-painted stone building with a blue door and a small balcony. The laneway is flanked by high walls and has a dark, paved surface.

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ROW Recommendations




Street Name	Description	Recommended Changes	Photo
14: Leigh Place (from Bridge Road to Erin Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 5.7m• Road reserve – 9m• Traffic management – Two-way for northern section, One-way for southern section connecting to Bridge Road• Parking – No parking• Footpath – Footpath on west side• Material – Asphalt <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none">• One-way	No changes required.	A photograph showing a narrow asphalt road (Leigh Place) running alongside a modern building with large glass windows. The road is flanked by a green fence on the left and a sidewalk on the right. The sky is blue with some clouds.

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ROW Recommendations





Street Name	Description	Recommended Changes	Photo
15: ROW (from Leigh Place to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.55m• Traffic management – Two-way• Parking – No parking• Footpath – No footpath• Material – Asphalt• Layout features – Slight kink at the middle, still easily traversable <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none">• Short	No changes required.	

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ROW Recommendations




Street Name	Description	Recommended Changes	Photo
16: Corns Place (from Leigh Place to Lennox Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> Carriageway width – 3.1m Traffic management – Two-way, must turn left at Leigh Place Parking – Car park at midpoint of ROW Footpath – No footpath Material – Asphalt <p>Constraints: Unconstrained</p> <ul style="list-style-type: none"> Short Continuous Could be made one-way 	No changes required.	
17: ROW (from Lennox Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> Carriageway width – 3.5m Traffic management – Two-way Parking – No parking Footpath – No footpath Material – Asphalt <p>Constraints: (Unconstrained laneway)</p> <ul style="list-style-type: none"> Short Low development potential 	<p>Passing area required at entrance to manage vehicle conflict onto Lennox Street by a setback of #132 Bridge Road.</p> <p>Heritage building at #132 Bridge Road a potential constraint.</p> <p>Reason:</p> <p>Avoid vehicle conflict and queuing on Lennox Street.</p>	

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ROW Recommendations



Street Name	Description	Recommended Changes	Photo
18: ROW (from Judd Street to Carpark)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 6m (including mountable kerbing)• Traffic management – Two-way• Parking – No parking• Footpath – Mountable footpath on south side• Material – Asphalt <p>Constraints: (Unconstrained laneway)</p> <ul style="list-style-type: none">• Short <p>Mountable kerbing allows for two-way passing</p>	No changes required.	 A photograph showing a narrow, paved laneway or alleyway. On the left side, there are several orange traffic cones and white plastic water-filled barriers used for traffic management. The right side is a dark, overhanging structure, possibly a building entrance or a large overhang. The ground is dark asphalt. In the background, some industrial buildings and a clear blue sky are visible.

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ROW Recommendations




Street Name	Description	Recommended Changes	Photo
19: ROW (from Hull Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.15m• Traffic management – Two-way• Parking – Parking provided in car park at southern end• Footpath – No footpath• Material – Asphalt <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none">• Short• Small number of adjacent properties	No changes required.	A photograph showing a narrow residential street or laneway. On the left, a white car is parked. In the foreground, the front of a blue car is visible. The street is paved with asphalt. On the right, there is a brick building with a white fence in front of it. In the background, a modern multi-story building is visible.

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ROW Recommendations




Street Name	Description	Recommended Changes	Photo
20: Wustemenn Place (from Bridge Road to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 2.65m• Traffic management – Two-way• Parking – Parking provided in car park at southern end• Footpath – No footpath• Material – Asphalt• Layout Features – Narrow width, shares car park with Allowah Terrace <p>Constraints: Partially constrained</p> <ul style="list-style-type: none">• Lack of passing area• Dead end• Could be connected to Allowah Terrace	No changes.	

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ROW Recommendations



Street Name	Description	Recommended Changes	Photo
21: Allowah Terrace (from Bridge Road to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 2.6m • Traffic management – Two-way • Parking – Parking provided in car park at southern end • Footpath – No footpath • Material – Bluestone • Layout features – Narrow width, shares car park with Wustemenn Place <p>Constraints: Partially constrained</p> <ul style="list-style-type: none"> • Lack of passing area • Dead end • Could be connected to Wustemenn Place 	No changes.	

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ROW Recommendations



Street Name	Description	Recommended Changes	Photo
22: Peluso Place (from Bridge Road to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 4.1m-4.85m • Traffic management – Two-way • Parking – Parking provided in car park at southern end • Footpath – No footpath • Material – Asphalt <p>Constraints: Partially constrained</p> <ul style="list-style-type: none"> • Lack of passing area • Dead end 	<p>Connect to Sheridan Place and make one-way westbound.</p> <p>Reason:</p> <p>Heritage buildings prevent providing a passing area. A one-way arrangement will limit vehicle conflict on Bridge Road with access to Sheridan Place.</p>	

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ROW Recommendations





Street Name	Description	Recommended Changes	Photo
23: Leggo Place (from Bosisto Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 7.6m• Traffic management – Two-way• Parking – Large Car park at western end• Footpath – No footpath• Material – Asphalt• Layout Features – Has a kink at the end, and connects to a large car park <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none">• Sufficient width for two-way traffic	No changes required, sufficient width for two-way operation.	

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ROW Recommendations



Street Name	Description	Recommended Changes	Photo
24: ROW (from Bosisto Street to Hull Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 3.5m-4.3m • Traffic management – Two-way • Parking –No parking • Footpath – No footpath • Material – Asphalt • Layout features – Already ‘built out’ to a large degree <p>Constraints: Unconstrained Laneway</p> <ul style="list-style-type: none"> • Properties already developed 	No changes required, laneway already ‘built out’.	
25: Sheridan Place (from Waltham Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 3.55m • Traffic management – Two-way • Parking – No parking • Footpath – No footpath • Material – Asphalt <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> • Short 	<p>Connect to Peluso Place and make one-way.</p> <p>Reason:</p> <p>Heritage buildings prevent providing a passing area. A one-way arrangement will limit vehicle conflict on Bridge Road/Lennox Street.</p>	

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ROW Recommendations



Street Name	Description	Recommended Changes	Photo
26: ROW (from Berry Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.05m-3.3mm• Traffic management – Two-way, Berry Street is one-way (westbound)• Parking – No parking• Footpath – No footpath• Material – Bluestone• Layout Features – has a T-intersection at the northern end, with splays on both corners. <p>Constraints: Highly constrained</p> <ul style="list-style-type: none">• Length• T-shape	<p>Provide a width of 6m for the east-west section by setting back properties fronting Bridge Road.</p> <p>Reason:</p> <p>This widening allows for vehicle access to properties fronting Bridge Road, vehicle passing and vehicle access around the 'T' intersection of the ROW.</p>	A photograph showing a narrow, paved alleyway. On the left is a tall wooden fence, and on the right is a white-painted house with a dark roof. The alleyway leads towards a street in the background.

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ROW Recommendations




Street Name	Description	Recommended Changes	Photo
27: Alban Street (from Eucalyptus Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> Carriageway width – 5.8m Traffic management – Two-way Parking – Parking along the north side of Alban Street Footpath – No Footpath Material – Asphalt <p>Constraints: Partially constrained</p> <ul style="list-style-type: none"> Wide enough for two-way traffic Parking arrangements make two-way traffic flow unachievable 	<p>Review on-street car parking arrangements.</p> <p>Reason:</p> <p>Review car parking as under current arrangements, two-way flow is not achievable when vehicles are parked within Alban Street.</p> <p>Changes should be made when or if required, given development potential from Alban Street is low.</p>	
28: ROW (from Bridge Road to END, opposite Eucalyptus Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> Carriageway width – 3.65m Traffic management – Two-way Parking – No parking Footpath – No footpath Material – Asphalt <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> Short Low development potential 	<p>No changes required. A redevelopment of Richmond Plaza is unlikely to use this lane for vehicle access.</p>	

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Street Name	Description	Recommended Changes	Photo
29: Henry Street (from Cameron Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.9m• Traffic management – Two-way, speed humps• Parking – No parking• Footpath – No footpath• Material – Asphalt <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none">• Short	<p>Passing area required at entrance using #196-198 Church Street.</p> <p>Reason:</p> <p>Significant development potential off this laneway is likely to require a passing area.</p>	

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Street Name	Description	Recommended Changes	Photo
30: ROW (from Berry Street to Hodgson Terrace)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 4.15m• Traffic management – Two-way, Berry Street is one-way (westbound)• Parking – No parking• Footpath – No footpath• Material – Bluestone• Layout features – Berry Street is a narrow street (3.5m road), and a splay is provided on the southeast corner of the intersection with the ROW to assist movement. <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none">• Short• Low development potential	<p>No changes required due to short length and low development potential.</p>	

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Street Name	Description	Recommended Changes	Photo
31: ROW (from Church Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.7m• Traffic management – Two-way• Parking – No parking• Footpath – No footpath• Material – Asphalt <p>Constraints: Unconstrained laneway</p> <p>Short</p>	No changes required due to short length.	

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Street Name	Description	Recommended Changes	Photo
32: ROW (from Church Street to Church Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 4m-4.7m • Traffic management – ROW is entry only for the northern section, however, an exit lane is provided via adjacent McDonalds car park, so is considered two-way for all practical purposes. • Parking – No parking • Footpath – No footpath • Material – Asphalt • Layout features – There are two 90 degree turns which loop the ROW back to Church Street. Splays are provided at each bend, and the ROW has enough width to allow for unimpeded turning. <p>Constraints: Partially constrained</p> <ul style="list-style-type: none"> • Narrow • U-shaped • Lack of passing without 'McDonalds' site, however surrounding McDonalds site means that access issues could be easily resolved with re-development 	<p>Maintain current layout in any future development of the MacDonalDs site.</p> <p>Reason:</p> <p>Current layout which includes the land of #227-235 (MacdonalDs) allows for two-way flow and prevents conflicts on Church Street.</p>	

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Street Name	Description	Recommended Changes	Photo
33: Tullo Place (from Bridge Road to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 3.95m-4.55m • Road reserve – 6.2m-6.8m • Traffic management – Two-way, no right turn at Bridge Road • Parking – No Parking • Footpath – Footpath on west side • Material – Asphalt • Layout features – There is a connecting ROW on the east side of the road, with no splays provided <p>Constraints: Partially Constrained</p> <ul style="list-style-type: none"> • Lack of passing area • Could be converted shared zone for two-way traffic (footpath removed) 	<p>Option to create shared zone for vehicles and pedestrians.</p> <p>Reason:</p> <p>Footpath can be removed to allow for a carriageway width that provides for two-way traffic flow while improving the pedestrian environment. Passing area reduces vehicle conflict at Bridge Road.</p>	

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Street Name	Description	Recommended Changes	Photo
34: ROW (from Tullo Place to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.3m• Traffic management – Two-way• Parking – No parking• Footpath – No footpath• Material – Asphalt• Layout features – Connected to Tullo Place, with no splays provided <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none">• Short• Low development potential	No changes required due to short length.	 A photograph showing a narrow asphalt laneway. On the left is a brick wall, and on the right is a white picket fence. The laneway leads to a green door at the end of the path.

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Street Name	Description	Recommended Changes	Photo
35: Waterloo Place (from Bridge Road to Church Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 4.4m • Road reserve – 6.2m • Traffic management – Two-way • Parking – No parking • Footpath – Narrow footpaths on both sides • Material – Asphalt • Layout features – Waterloo Place has a 90 degree bend connecting it from Bridge Road to Church Street. A splay is provided at the bend on the northwest corner. There are also two ROWs connected to Waterloo Place <p>Constraints: Partially Constrained</p> <ul style="list-style-type: none"> • Lack of two-way passing opportunities • Length • Could be made one-way 	<p>No changes required.</p> <p>This laneway abuts properties largely outside of the study area. It has sufficient width for future conversion into a shared zone or to be made one-way, if required.</p>	

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Street Name	Description	Recommended Changes	Photo
36: ROW (from Waterloo Place to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 3.5m (with additional property boundary setback of 2.55m) • Traffic management – Two-way • Parking – Private parking on south side within property setback • Footpath – No footpath • Material – Bluestone • Layout features – A property boundary setback allows for turning into ROW from Waterloo Place <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> • Short 	No changes required due to short length.	

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Street Name	Description	Recommended Changes	Photo
37: ROW (from Waterloo Place to Lyndhurst Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.2m• Traffic management – Two-way, Lyndhurst Street is one-way (northbound)• Parking – No parking• Footpath – No footpath• Material – Bluestone• Layout features – A splay on the southeast corner of Waterloo Place and the ROW is provided to assist turning. <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none">• Short• Continuous	No changes required due to short length.	

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Street Name	Description	Recommended Changes	Photo
38: ROW (from Gleadell Street to Griffiths Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.15m• Traffic management – Two-way• Parking – No parking• Footpath – No footpath• Material – Asphalt <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none">• Continuous• Straight• Could be one-way	<p>One way in the westbound direction.</p> <p>Reason:</p> <p>The road is only wide enough for one-way flow. One-way westbound provides a higher level of access compared to eastbound due to existing turn bans at Gleadell Street.</p> <p>One way reduces vehicle conflict. We understand Council expects high pedestrian volumes in the future with the new school opening soon.</p>	A photograph showing a narrow street scene. On the left, there is a metal gate or fence. On the right, there is a modern, multi-story building with large windows. A car is visible on the street to the left of the gate.

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Street Name	Description	Recommended Changes	Photo
39: Spencer Place (from Hosie Street to Abinger Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 3.45m-3.8m • Traffic management – Two-way, Hosie Street is one-way (northbound) • Parking – No parking • Footpath – No footpath • Material – Asphalt and bluestone • Layout features – There is a 90 degree bend in Spencer Place, with a splay provided on the southeast corner. There is another connecting ROW, which connects back to Hosie Street, with a splay also provided. <p>Constraints: Partially Constrained</p> <ul style="list-style-type: none"> • Long • Lack passing opportunities 	<p>No changes required.</p> <p>A future change to one-way operation is a potential option, if required.</p>	

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Street Name	Description	Recommended Changes	Photo
40: Pandoleon Lane (from Mary Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.65m• Traffic management – Two-way, Mary street is one-way (northbound)• Parking – No parking• Footpath – No footpath• Material – Asphalt <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none">• Short• Low development potential	No changes required.	

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Street Name	Description	Recommended Changes	Photo
41: ROW (from Mary Street to Coppin Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.2m• Traffic management – Two-way, Mary Street is one-way (northbound)• Parking – No parking• Footpath – No footpath• Material – Asphalt• Layout features – Straight, limited splays on intersecting ROWs. <p>Constraints: Partially Constrained</p> <ul style="list-style-type: none">• Length• No passing area• Continuous, could be one-way	<p>One-way in the westbound direction.</p> <p>Reason:</p> <p>Lane is long with no passing opportunities.</p> <p>This allows for current one-way arrangements on Mary Street to be maintained.</p>	A photograph of a narrow, paved street flanked by brick buildings. The street is straight and appears to be a residential or commercial lane. The sky is overcast.

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Street Name	Description	Recommended Changes	Photo
42: ROW (from Griffiths Street to Coppin Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 3.1m • Traffic management – Two-way, must enter and exit via left on Coppin Street • Parking – No parking • Footpath – No footpath • Material – Asphalt • Layout features – There is a connecting ROW to the north, with splays provided on both corners of the intersection. <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> • Continuous • Straight • Could be one-way 	<p>No changes required due to low development potential.</p> <p>Can be made one-way in future if required.</p>	

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Street Name	Description	Recommended Changes	Photo
43: Foster Place (from Coppin Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.15m-3.45m• Traffic management – Two-way• Parking – No parking• Footpath – No footpath• Material – Asphalt• Layout features – There is a connecting ROW of 4m width to the south, with no splays provided. <p>Constraints: Partially Constrained</p> <ul style="list-style-type: none">• Lack of passing area on east-west link• T intersection	<p>Provide 6m passing area where possible, avoiding heritage buildings.</p> <p>Reason:</p> <p>Lane is narrow and does not allow for two-way flow. A passing area cannot be provided at the entrance due to a heritage building.</p>	A photograph showing a narrow residential street. On the left is a yellow building with a green trash bin. On the right is a heritage-style building with a red roof and a green trash bin. The street is paved with asphalt and has a white crosswalk in the foreground.

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Street Name	Description	Recommended Changes	Photo
44: ROW (from Coppin Street to Palmer Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.7m-5.7m• Traffic management – Two-way• Parking – No parking• Footpath – No footpath• Material – Asphalt• Layout features – There is a connecting ROW to the north, with no splays provided at the intersection, however, properties on the south are set back. <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none">• Continuous• Short	<p>No changes required.</p> <p>Can be made one-way in future if required.</p>	

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Street Name	Description	Recommended Changes	Photo
45: ROW (from Lord Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 3.55m, widened by adjacent development • Traffic management – Two-way, Lord Street is one-way (northbound) • Parking – No parking • Footpath – No footpath • Material – Asphalt <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> • Provides two-way traffic 	No changes required due to short length and effective widening has already taken place.	
46: ROW (from Hunter Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 3m • Traffic management – Two-way • Parking – No parking • Footpath – No footpath • Material – Asphalt <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> • Short • Low development potential 	No changes required due to short length.	

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Street Name	Description	Recommended Changes	Photo
47: ROW (from Hunter Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 4.4m • Traffic management – Two-way • Parking – No parking • Footpath – No footpath • Material – Concrete • Layout features – appears to have been consumed as private property <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> • Short <p>Low development potential</p>	No changes required due to short length.	
48: ROW (from Neptune Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 3m • Traffic management – Two-way • Parking – No parking • Footpath – No footpath • Material – Asphalt <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> • Short • Low development potential 	<p>No changes required.</p> <p>Parking arrangements at Neptune Street could be reviewed to allow for more passing opportunities.</p>	

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Street Name	Description	Recommended Changes	Photo
49: ROW (from Palmer Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 3.45m • Traffic management – Two-way • Parking – No parking • Footpath – No footpath • Material – Concrete <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none"> • Short <p>Low development potential</p>	No changes required.	
50: Birch Square (from Murphy Street to Murphy Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none"> • Carriageway width – 6.1m-9m • Traffic management – One-way in an anticlockwise direction • Parking – Parking on north side of east-west section • Footpath – No footpath • Material – Asphalt <p>Constraints: Unconstrained laneway</p> <p>Already one-way to minimise vehicle conflict</p>	No changes required.	

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Street Name	Description	Recommended Changes	Photo
51: ROW (from Neptune Street to Burnley Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3m• Traffic management – Two-way, must exit/enter left at Burnley• Parking – No parking• Footpath – No footpath• Material – Asphalt <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none">• Continuous• Straight• Could be one-way	No changes required due to low development potential.	

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Street Name	Description	Recommended Changes	Photo
52: ROW (from Neptune Street to END)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.15m• Traffic management – Two-way• Parking – No parking• Footpath – No footpath• Material – Asphalt• Layout features – ROW bends 90 to the north with no spays provided. North-south section is not trafficable and requires splays <p>Constraints: Highly Constrained</p> <p>Requires splays on the corners</p>	Splays required on #23 to make trafficable.	 A photograph showing a narrow alleyway between two-story brick buildings. The ground is paved asphalt. A white sign with a large 'S' is visible on the left building. The alleyway appears to be a dead-end or a very narrow passage.

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Street Name	Description	Recommended Changes	Photo
53: ROW (from Burnley Street to Type Street)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.4m• Traffic management – Two-way• Parking – No parking• Footpath – No footpath• Material – Asphalt• Layout features – There is a connecting ROW to the south, with no splays provided at the intersection. <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none">• Continuous• Straight• Could be one-way	<p>One-way in the westbound direction.</p> <p>Reason:</p> <p>Long laneway width no passing opportunities.</p>	 A photograph of a street intersection. A 'ONE WAY' sign is visible on a pole, pointing left. The street is paved with asphalt and has white lane markings. There are trees and buildings in the background.

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Street Name	Description	Recommended Changes	Photo
54: ROW (from Type Street END)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 5m• Traffic management – Two-way• Parking – No parking• Footpath – Footpath on south side• Material – Asphalt <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none">• Continuous• Straight• Could be one-way	No changes required due to low development potential and short length. Can be made one-way in future if required.	 A photograph of a narrow asphalt laneway. On the left, there is a black metal fence and a building with a corrugated metal roof. On the right, there is a white fence and a building with a brick wall. A white car is parked at the end of the laneway.

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Street Name	Description	Recommended Changes	Photo
55: Park Avenue (east-west section abutting Bridge Road properties from Westbank Terrace to bend)	<p>Existing Conditions:</p> <ul style="list-style-type: none">• Carriageway width – 3.65m• Traffic management – Two-way• Parking – No parking• Footpath – Footpath on south side• Material – Asphalt <p>Constraints: Unconstrained laneway</p> <ul style="list-style-type: none">• Continuous• Straight <p>Could be one-way</p>	No changes required due to low development potential. Can be made one-way in future if required.	

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Street Name	Description	Recommended Changes	Photo
Eucalyptus Street	<p>Existing Conditions:</p> <ul style="list-style-type: none"> Carriageway width – 3.45m-5.95m Road Reserve – 5.95m Traffic management – Two-way Parking – No Parking Footpaths – Narrow footpath on both sides Material – Asphalt <p>Layout features – Road provides passing area at intersection with Bridge Road, however road narrows soon after, providing no other opportunities for passing.</p>	<p>Review conversion into a true shared zone where pedestrians and vehicles share road space and allow two vehicles to pass one another, particularly at Bridge Road.</p>	
Neptune Street	<p>Existing Conditions:</p> <ul style="list-style-type: none"> Carriageway width – 7.1m Road Reserve – 9.8m Traffic management – Two-way Parking – Parallel parking on both sides Footpaths – Narrow footpath on both sides Material – Asphalt <p>Layout features – Parking on each side of the road only allows for one-way traffic flow.</p>	<p>On-street car parking arrangements to be reviewed.</p> <p>Reason:</p> <p>Carriageway currently allows parking on both sides of the road and a single lane for two-way traffic. Development potential accessing Neptune Street might necessitate removing some on-street parking to provide passing areas along Neptune Street.</p>	